

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS

**STATE OF TEXAS
DEPARTMENT OF TRANSPORTATION**

**PLANS OF PROPOSED
STATE HIGHWAY IMPROVEMENT**

FEDERAL AID PROJECT NUMBER
NH 2B24 (072)
CSJ 0482-01-036, ETC.

NET LENGTH OF PROJECT = 52,400 FEET = 9.924 MILES
 ROADWAY = 52,265 FEET = 9.899 MILES
 BRIDGE = 135 FEET = 0.026 MILES

**JIM HOGG, ETC.
SH 285**

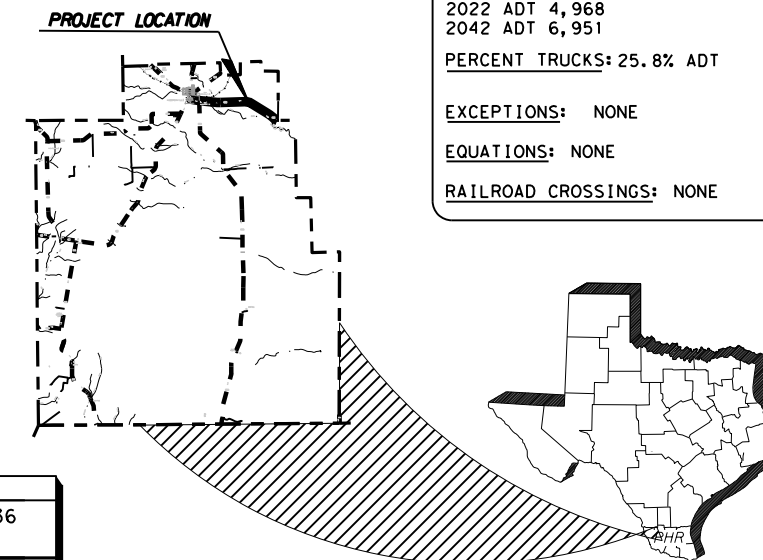
FROM: FM 1017
TO: JIM HOGG/BROOKS COUNTY LINE

FOR THE CONSTRUCTION OF WIDENING AND REHABILITATION OF EXISTING HIGHWAY (SUPER 2 HIGHWAY)
CONSISTING OF: CEMENT TREATED SUBGRADE, CEMENT TREATED FLEX BASE, DRAINAGE STRUCTURES,
ASPHALT CONCRETE PAVEMENT, SIGNING AND PAVEMENT MARKINGS.

CONT	SECT	JOB	HIGHWAY
0482	01	036, Etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG, Etc.		1

PROJECT DATA

DESIGN SPEED:
75 MPH
HIGHWAY FUNCTIONAL CLASS:
MINOR ARTERIAL
TRAFFIC VOLUMES:
2022 ADT 4,968
2042 ADT 6,951
PERCENT TRUCKS: 25.8% ADT
EXCEPTIONS: NONE
EQUATIONS: NONE
RAILROAD CROSSINGS: NONE



FINAL PLANS

DATE OF LETTING: _____
 DATE WORK BEGAN: _____
 DATE WORK COMPLETED: _____
 DATE WORK ACCEPTED: _____
 FINAL CONTRACT COST: \$ _____
 CONTRACTOR: _____

LIST OF APPROVED FIELD CHANGES, CHANGE ORDERS & SUPPLEMENTAL AGREEMENTS:

THIS IS TO CERTIFY THAT ALL CONSTRUCTION SUBSTANTIAL WORK WAS PERFORMED IN ACCORDANCE WITH THE PLANS SPECIFICATIONS AND CONTRACT. ALL PROPOSED CONSTRUCTION WAS COMPLETED UNLESS OTHERWISE NOTED.

FRANCISCO CANTU, P.E. _____ DATE _____
 ROMA AREA ENGINEER

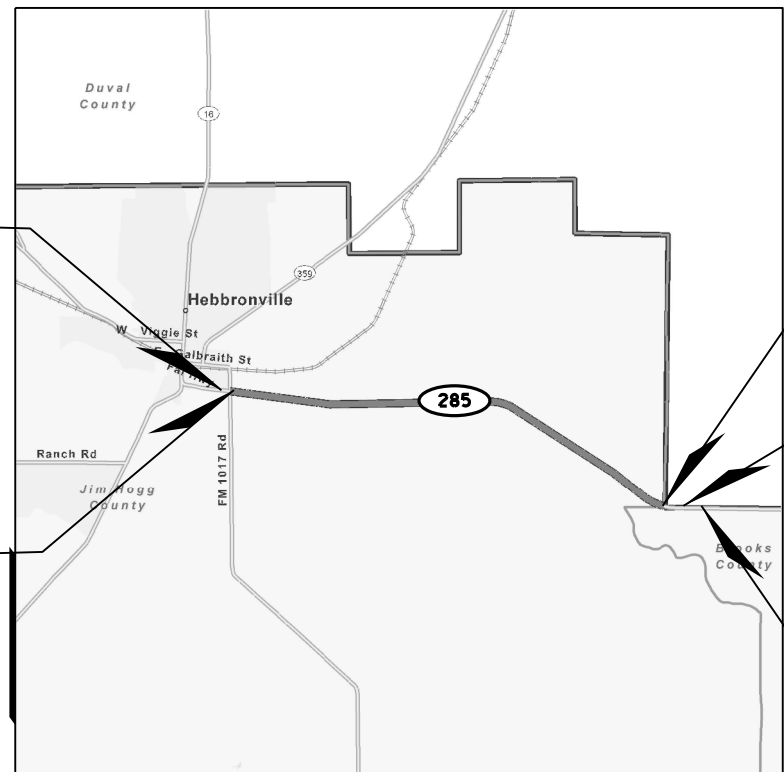
BEGIN INCIDENTAL CONSTRUCTION
 CSJ: 0482-01-036
 STA: 73+35.00
 REF MRKR: 482+1.93
 DFO: 0.560
 LAT: 27.299224
 LONG: -98.669943
 MP: 2.560

END INCIDENTAL CONSTRUCTION
BEGIN PROJECT
 CSJ: 0482-01-036
 STA: 96+00.00
 REF MRKR: 484+0.36
 DFO: 0.989
 LAT: 27.298275
 LONG: -98.663075
 MP: 2.989

END PROJECT
 CSJ: 0482-01-036
BEGIN PROJECT
 CSJ: 0482-02-035
 STA: 585+00.00
 REF MRKR: 496+0.00
 DFO: 10.216
 LAT: 27.265317
 LONG: -98.523291
 MP: 0.16

END PROJECT
 CSJ: 0482-02-035
 STA: 620+00.00
 REF MRKR: 496+0.663
 DFO: 10.879
 LAT: 27.265359
 LONG: -98.523342
 MP: 0.775

END INCIDENTAL CONSTRUCTION
 CSJ: 0482-02-035
 STA: 642+10.00
 REF MRKR: 496+1.081
 DFO: 11.297
 LAT: 27.264781
 LONG: -98.505758
 MP: 1.241



LOCATION MAP NOT TO SCALE

TDLR INSPECTION NOT REQUIRED

SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION ON NOVEMBER 1, 2014 AND SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS, SHALL GOVERN ON THIS PROJECT: REQUIRED CONTRACT PROVISIONS FOR ALL FEDERAL-AID CONSTRUCTION CONTRACTS (FORM FHWA 1273, OCTOBER 23, 2023).

RECOMMENDED FOR LETTING: DATE: 2/29/2024

SUBMITTED FOR LETTING: DATE: 2/29/2024

DocuSigned by:
Pedro R. Alvarez
DISTRICT ENGINEER

DocuSigned by:
Romualdo Mena Jr
DISTRICT CENTRAL DESIGN SUPERVISOR

FILE: pw:\txdot\project\wiseon\ine.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 TITLE SHEET.dgn
DATE: 2/12/2024 7:31:11 AM

INDEX OF SHEETS

GENERAL

- 1 TITLE SHEET
- 2 INDEX OF SHEETS
- 3-6 PROJECT LAYOUT
- 7 EXISTING TYPICAL SECTIONS
- 8-10 PROPOSED TYPICAL SECTIONS
- 11-17 SUMMARY OF ESTIMATED QUANTITIES
- 18-21 ESTIMATE & QUANTITY SHEETS
- 22-29 GENERAL NOTES
- 30-37 EARTHWORK SUMMARY

TRAFFIC CONTROL PLAN

- 38 TCP COVER SHEET
- 39-40 TCP GENERAL NOTES
- 41-45 TCP SEQUENCE OF CONSTRUCTION
- 46 TCP SIGN INVENTORY
- 47-49 TCP ADVANCED WARNING SIGNS LAYOUT
- 50-51 TCP ESTIMATED QUANTITIES
- 52-54 CRASH CUSHION SUMMARY SHEET
- 55-58 TCP PHASE LAYOUTS
- 59-62 TCP PHASE I TYPICAL SECTIONS
- 63-67 TCP PHASE I STEP I
- 68-72 TCP PHASE I STEP II
- 73-76 TCP PHASE II TYPICAL SECTIONS
- 77-80 TCP PHASE II STEP I
- 81-85 TCP PHASE II STEP II
- 86 TCP PHASE II STEP II STAGE A
- 87 TCP PHASE II STEP II STAGE B
- 88 TCP PHASE II STEP II STAGE APP
- 89 TCP PHASE II STEP II STAGE BPP
- 90-94 TCP PHASE II STEP III
- 95-97 TCP PHASE III TYPICAL SECTIONS
- 98-101 TCP PHASE III STEP I
- 102-105 TCP PHASE III STEP II
- 106-109 TCP PHASE III STEP III
- 110-112 TCP PHASE IV TYPICAL SECTIONS
- 113-115 TCP PHASE IV STEP I
- 116-119 TCP PHASE IV STEP II
- 120-123 TCP PHASE IV STEP III
- 124-127 TCP PHASE V TYPICAL SECTIONS
- 128-131 TCP PHASE V STEP I
- 132-135 TCP PHASE V STEP II
- 136-139 TCP PHASE V STEP III
- 140-141 TCP PHASE VI TYPICAL SECTIONS

TRAFFIC CONTROL PLAN STANDARDS

- >> 142-153 [S] BC (1)- 21 THRU BC (12)- 21
- >> 154 [S] TCP (2-1) - 18
- >> 155 [S] TCP (2-3) - 23
- >> 156 [S] TCP (2-8) - 23
- >> 157 [S] WZ (STPM) - 23
- >> 158 [S] WZ (UL) - 13
- >> 159 [S] WZ (RCD) - 13
- >> 160-161 [S] WZ (BTS-1) - 13 & WZ (BTS-2) - 13
- >> 162 [S] WZ (BRK) - 13
- >> 163-164 [S] SSCB(2)-10
- >> 165 [S] ABSORB (M) - 19
- >> 166 [S] SLED - 19
- >> 167 [S] TREATMENT FOR VARIOUS EDGE CONDITIONS

ROADWAY DETAILS

- 168 ROADWAY COVER SHEET
- 169-174 SURVEY CONTROL DATA
- 175-180 HORIZONTAL & VERTICAL CONTROL
- 181 ROADWAY DATA SHEET
- 182-225 PLAN AND PROFILE
- 226 CONCRETE MAT FOR UTILITY CROSSING
- 227 DRIVEWAY TABLE

ROADWAY DETAILS STANDARDS

- >> 228 [D] DRIVEWAY PROFILE DETAILS
- >> 229 [D] DRIVEWAY DETAILS PRIVATE (RESIDENTIAL-COMMERCIAL)
- >> 230 [D] DRIVEWAY DETAILS PUBLIC (COUNTY ROAD-CITY STREET)

DRAINAGE DETAILS

- 231 DRAINAGE COVER SHEET
- 232-236 DRAINAGE AREA MAPS
- 237 HYDRAULIC DATA
- 238 CULVERT CROSSING STA. 99+10 & STA. 101+51
- 239 CULVERT CROSSING STA. 111+00 & STA. 121+00
- 240 CULVERT CROSSING STA. 158+55 & STA. 164+40
- 241 CULVERT CROSSING STA. 219+76 & STA. 259+51
- 242 CULVERT CROSSING STA. 273+78 & STA. 285+13
- 243 CULVERT CROSSING STA. 311+00 & STA. 360+24
- 244 CULVERT CROSSING STA. 392+01 & STA. 415+36
- 245 CULVERT CROSSING STA. 442+00 & STA. 456+74
- 246 CULVERT CROSSING STA. 473+79 & STA. 491+99
- 247 CULVERT CROSSING STA. 509+85 & STA. 530+01
- 248 CULVERT CROSSING STA. 573+99 & STA. 606+22

DRAINAGE DETAILS STANDARDS

- ** 249 [D] CEMENT STABILIZED BACKFILL DETAILS
- ** 250 [D] MISCELLANEOUS PIPE DETAILS
- ** 251 [S] PIPE AND BOX GROUTED CONNECTIONS FOR PRECAST STRUCTURES (PBGC)
- ** 252-253 [S] SETP -CD
- ** 254 [S] SETP -PD
- ** 255 [S] PSET -RC
- ** 256 [S] PSET -RP
- ** 257 [S] (FW-0)
- ** 258 [S] SCP - 5

BRIDGE

- 259 BRIDGE COVER SHEET
- 260 BRIDGE CLASS CULVERT TYPICAL SECTIONS
- 261 RAIL ANCHORAGE CURB (MOD) RETROFIT LAYOUT

BRIDGE STANDARDS

- // 262-263 [S] RAC-R (OMIT SHEET 263)
- // 264 [S] GF (31) - 19
- // 265-266 [S] GF (31) TR - TL3 - 20
- // 267 [S] SGT (12S) 31 - 18
- // 268 [S] SGT (15) 31 - 20
- // 269-271 [S] TYPE T223
- // 272 [S] SCP - 6

SIGNING & DELINEATION

- 273 SIGNING & DELINEATION COVER SHEET
- 274-295 SIGNING & DELINEATION LAYOUTS
- 296-302 SUMMARY OF SMALL SIGNS
- 303 SMALL SIGN DETAILS
- 304 SUMMARY OF SIGNS TO BE REMOVED

SIGNING & DELINEATION STANDARDS

- >> 305-307 [S] TSR (3) - 13 THRU TSR (5) - 13
- >> 308 [S] SMD (GEN) - 08
- >> 309 [S] SMD (SLIP - 1) -08
- >> 310 [S] SMD (SLIP - 2) - 08
- >> 311 [S] SMD (SLIP - 3) - 08
- >> 312-315 [S] D&OM (1), (2), (3) AND (4) - 20

PAVEMENT MARKINGS

- 316 PAVEMENT MARKINGS COVER SHEET
- 317-338 PAVEMENT MARKING LAYOUTS

PAVEMENT MARKING STANDARDS

- >> 339 [S] TS2 (PL-2) - 23
- >> 340 [S] PM (1) - 22
- >> 341 [S] PM (2) - 22
- >> 342 [S] PM (3) - 22
- >> 343 [S] CENTERLINE BUFFER TWO-LANE ROADWAYS
- >> 344 [S] CENTERLINE BUFFER MULTI-LANE ROADWAYS
- >> 345 [S] RS (3) - 23
- >> 346 [S] RS (4) - 23

ENVIRONMENTAL ISSUES

- 347 ENVIRONMENTAL COVER SHEET
- 348-349 STORMWATER POLLUTION PREVENTION PLAN (SWP3)
- 350-351 ENVIRONMENTAL PERMITS, ISSUES AND COMMITMENTS (EPIC)
- 352-354 TPWD BMP'S
- 355-376 SW3P LAYOUTS

ENVIRONMENTAL ISSUES STANDARDS

- >> 377 [D] TECL - 17 (PHR)
- >> 378-379 [D] BIRD EXCLUSION DETAILS
- >> 380 [S] EC (1) - 16
- >> 381 [S] EC (2) - 16
- >> 382 [S] EC (3) - 16
- >> 383-385 [S] EC (9) - 16

MISCELLANEOUS

- 386 MISCELLANEOUS COVER SHEET
- 387-408 REMOVAL LAYOUTS

[S] STATE STANDARDS
[D] DISTRICT STANDARDS

DATE: 2/8/2024 9:01:31 AM
FILE: P:\xtdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 INDEX.dgn



**THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ABOVE HAVE BEEN SELECTED BY ME OR UNDER MY SUPERVISION AND ARE APPLICABLE TO THIS PROJECT.

DocuSigned by:

Arlene Garcia

P. E.

2/8/2024

ARLE

215F41A211C24AF...

DATE



**THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ABOVE HAVE BEEN SELECTED BY ME OR UNDER MY SUPERVISION AND ARE APPLICABLE TO THIS PROJECT.

DocuSigned by:

Filiberto Ramos

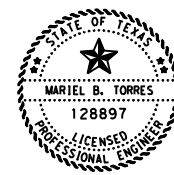
P. E.

2/8/2024

FI

97AAD53D9FDA403...

DATE



**THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ABOVE HAVE BEEN SELECTED BY ME OR UNDER MY SUPERVISION AND ARE APPLICABLE TO THIS PROJECT.

DocuSigned by:

Mariel Torres

P. E.

2/8/2024

MAR

C9702DF8C67B467...

DATE

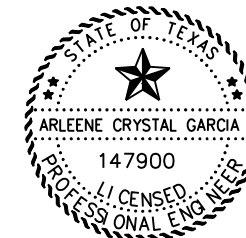
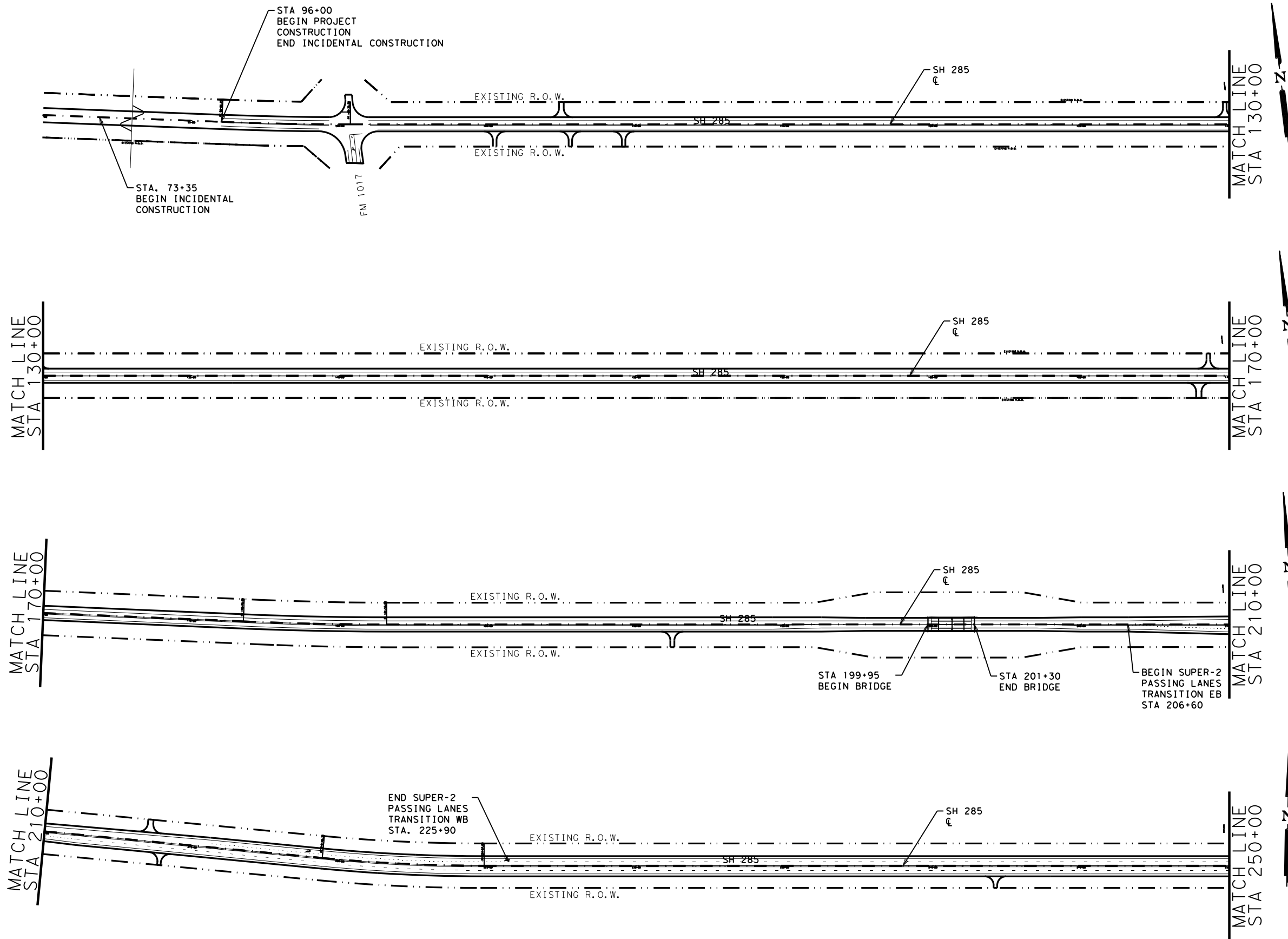
Pharr District Central Design

**SH 285
INDEX OF SHEETS**

SHEET 1 OF 1

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	2

DATE: 2/7/2024 10:48:54 AM
 FILE: D:\t\tdot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 PROJECT LAYOUT



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

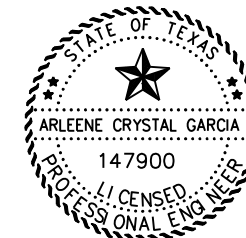
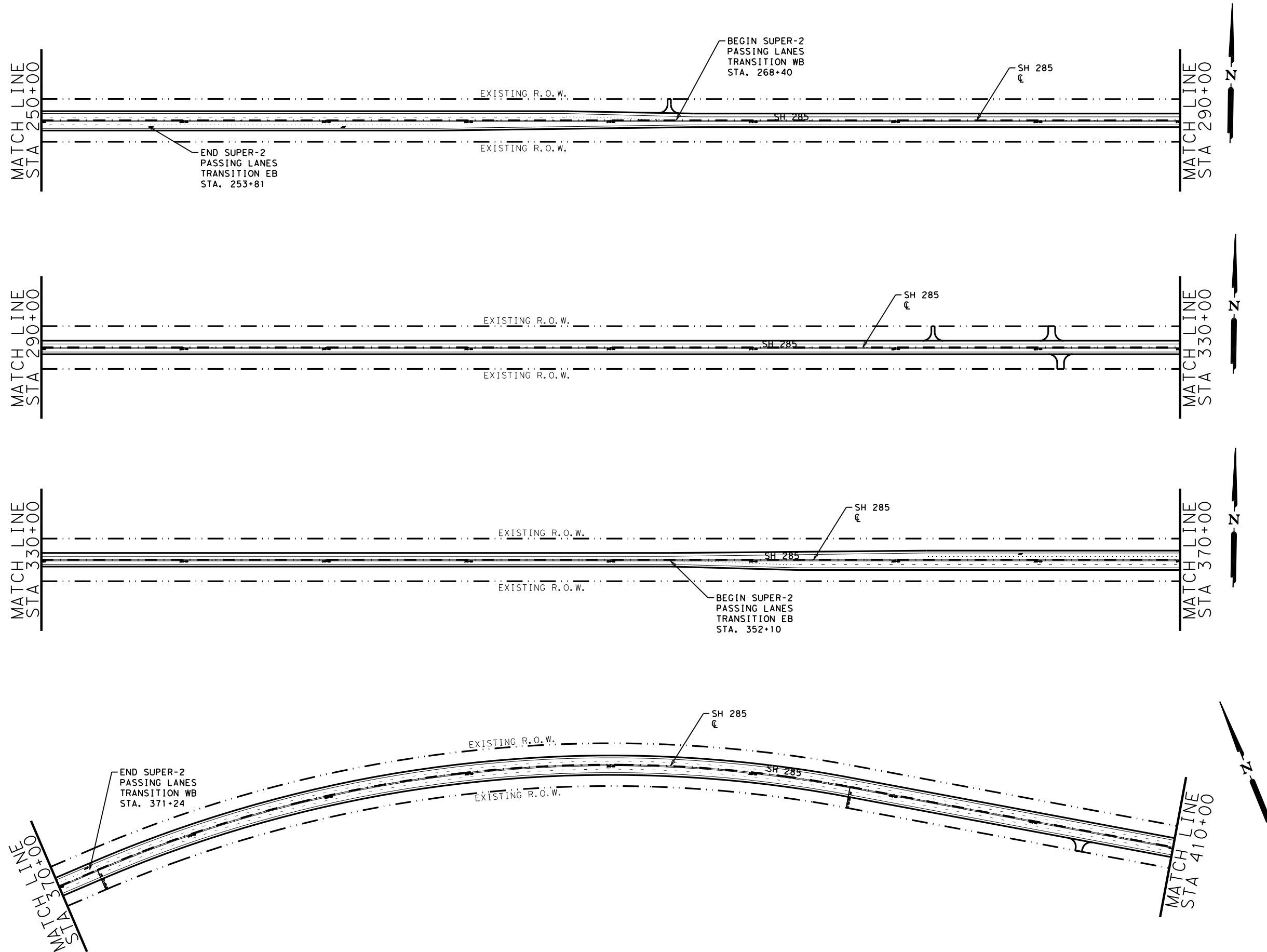


SH 285 PROJECT LAYOUT

SCALE = 1" = 350' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		3

DATE: 2/7/2024 10:48:57 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 PROJECT LAYOUT



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

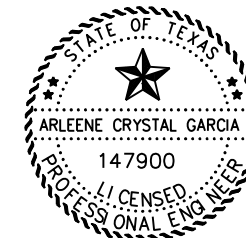
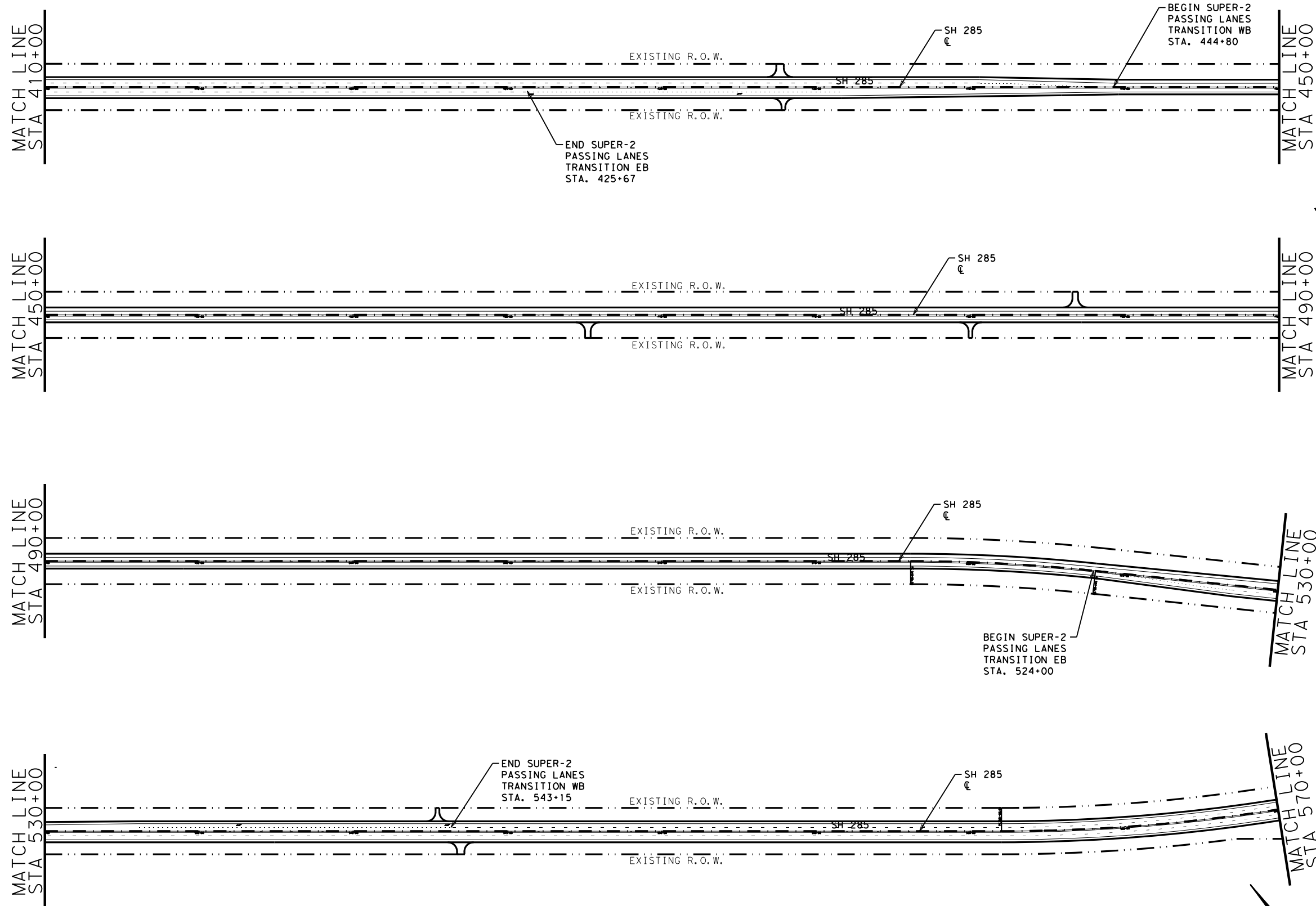


SH 285 PROJECT LAYOUT

SCALE = 1" = 350' SHEET 2 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	4

DATE: 2/7/2024 10:48:59 AM
 FILE: D:\xtdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 PROJECT LAYOUT



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

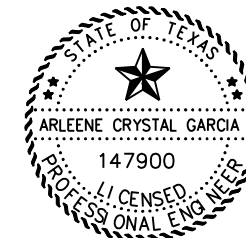
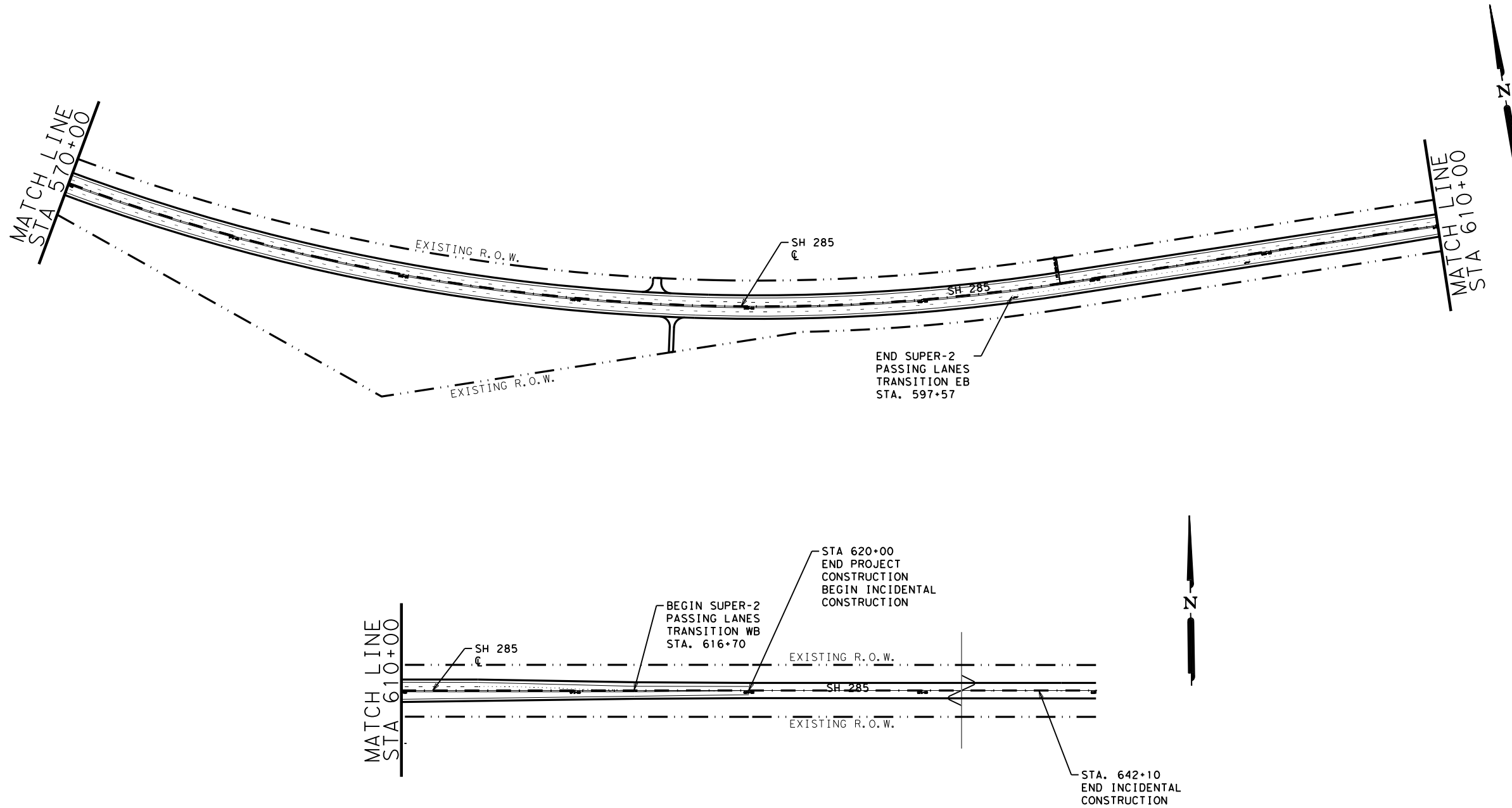


SH 285 PROJECT LAYOUT

SCALE= 1"=350' SHEET 3 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.
PHR	JIM HOGG		5

DATE: 2/7/2024 10:49:01 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 PROJECT LAYOUT



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

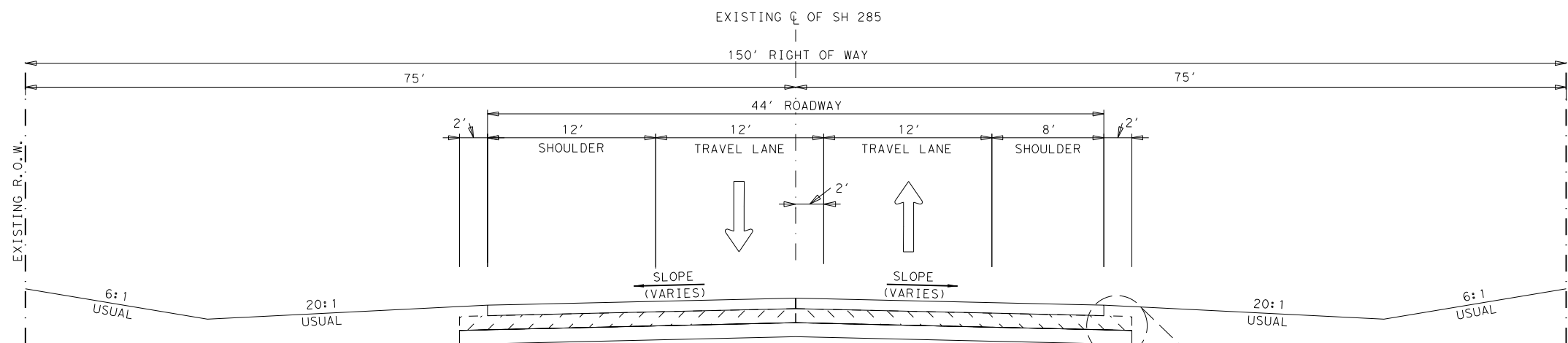


**SH 285
PROJECT LAYOUT**

SCALE = 1" = 350' SHEET 4 OF 4

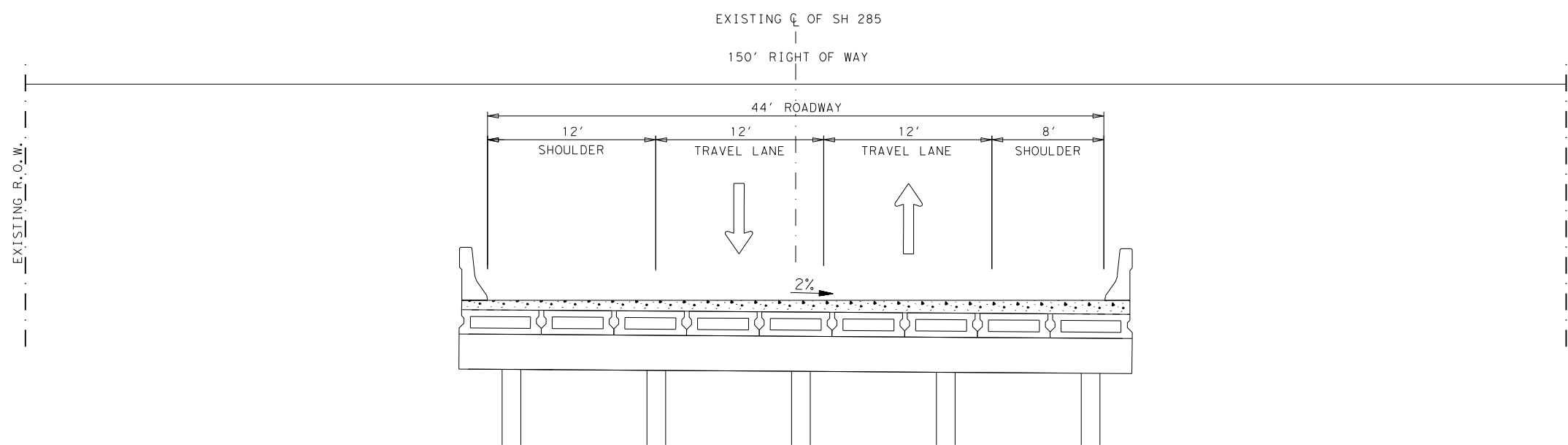
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	6

DATE: 2/7/2024 10:49:07 AM
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285 EXISTING TYPICALS



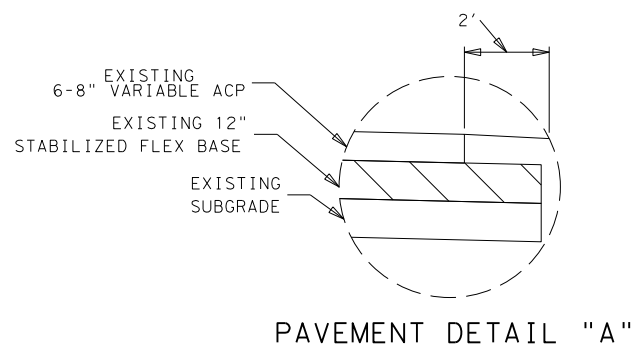
**SH 285
 EXISTING TYPICAL SECTION**

STA 96+00 TO STA 199+95
 STA 201+30 TO STA 585+00
 STA 585+00 TO STA 620+00



**SH 285
 EXISTING TYPICAL SECTION**

STA 199+95 TO STA 201+30



LEGEND

- ℄ - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R.O.W. - RIGHT OF WAY
- SHLDR - SHOULDER

GENERAL NOTES:

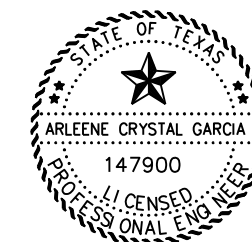
THE EXISTING ACP SHALL BE SALVAGED IN ACCORDANCE WITH ITEM 305. ALL SURPLUS RAP WILL REMAIN AS PROPERTY OF THE CONTRACTOR, THIS MATERIAL MAY BE REUSED IN THE NEW PAVEMENT STRUCTURE.

THE EXISTING BASE MATERIAL SHALL BE SALVAGED IN ACCORDANCE WITH ITEM 251, ALL EXCESS BASE MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR.

STATIONING IS APPROXIMATE AND ARE FOR INFORMATION PURPOSES ONLY, FIELD CONDITIONS MAY VARY.

STATIONING IS BASED ON SH 285 PROPOSED ALIGNMENT.

e% - DENOTES EXISTING SUPERELEVATION RATE



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
 EXISTING
 TYPICAL SECTIONS**

N. T. S.		SHEET 1 OF 1	
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		7

DATE: 2/12/2024 7:31:41 AM
 FILE: \\phr\dot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 PROPOSED TYPICALS.dgn

LEGEND

- ℄ - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R.O.W. - RIGHT OF WAY
- SHLDR - SHOULDER

NOTES:

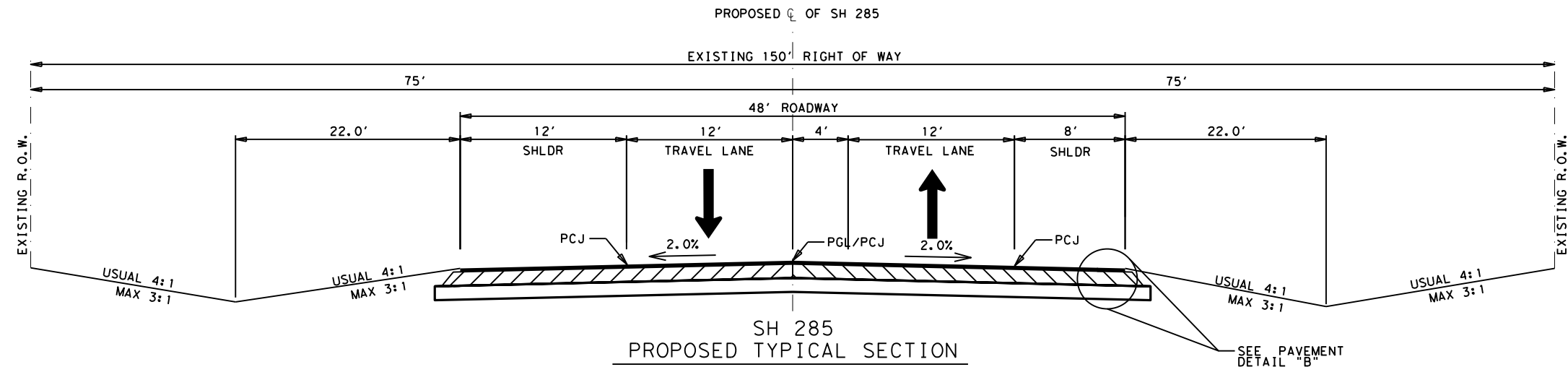
A MINIMUM OF 4" OF THE PROPOSED BASE LAYER SHALL BE NEW. IF SALVAGE AND DETOUR MATERIAL PERMITS, 8" OF THE PROPOSED BASE LAYER SHALL BE USED FROM SALVAGE MATERIAL REMOVED UNDER ITEM 105 AND REWORKED AS PER PERTINENT ITEM 251.

EXISTING BRIDGE CLASS CULVERT STA. 142+70 TO STA. 143+10 TO REMAIN IN PLACE. SEE BRIDGE SECTION ON PLANS FOR ADDITIONAL DETAILS ABOUT PROPOSED RETROFIT RAIL TO BE ADDED IN THIS LOCATION.

WHERE REQUIRED BY FIXTURES OR UNUSUAL CONDITIONS, THE GOVERNING SLOPES MAY BE VARIED WHEN SPECIFICALLY DIRECTED BY THE ENGINEER.

WHERE POSSIBLE AND UNLESS OTHERWISE DIRECTED BY THE ENGINEER, PERMISSIBLE CONSTRUCTION JOINTS SHALL FALL ON STRIPING LINES AS SHOWN ON THE PAVEMENT MARKINGS LAYOUTS.

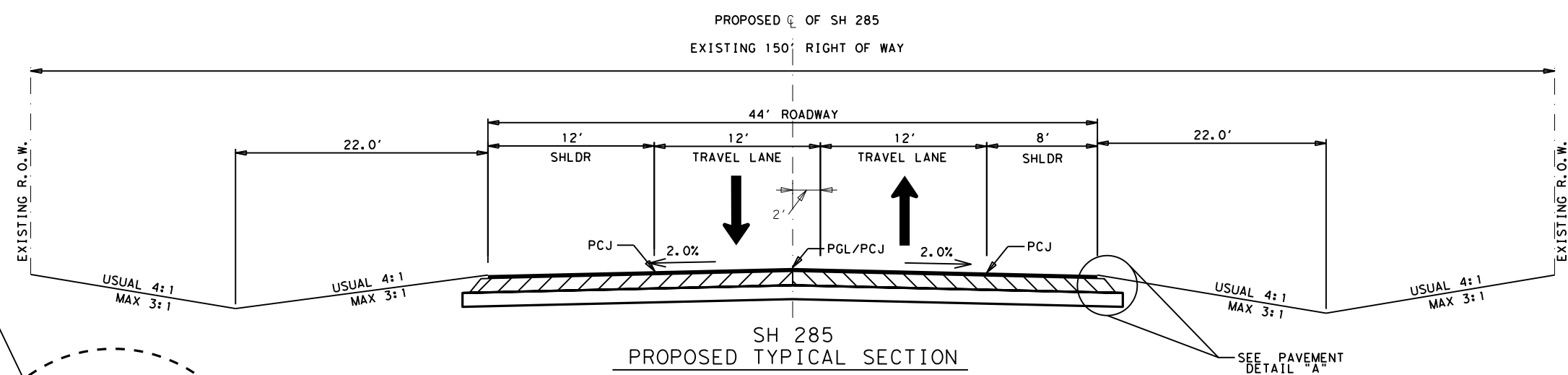
ALL GRADING SHALL BE DONE WITHING R.O.W. LIMITS UNLESS OTHERWISE NOTED ON PLANS. TOPSOIL TO BE SCARIFIED, SALVAGED, AND STOCKPILED IN THE R.O.W.



- SH 285 PROPOSED TYPICAL SECTION
- STA 96+00 TO STA 99+34
 - STA 99+34 TO STA 101+27 (FM 1017 INTERSECTION) **
 - STA 101+27 TO STA 142+70
 - STA 142+70 TO STA 196+25
 - STA 196+25 TO STA 197+75
 - STA 205+00 TO STA 206+60
 - STA 206+60 TO STA 211+10 (48' TO 68' TRANSITION)
 - STA 272+90 TO STA 352+10
 - STA 352+10 TO STA 361+10 (48' TO 68' TRANSITION)
 - STA 444+80 TO STA 524+00
 - STA 524+00 TO STA 533+00 (48' TO 68' TRANSITION)
 - STA 616+70 TO STA 620+00 (48' TO 68' TRANSITION)

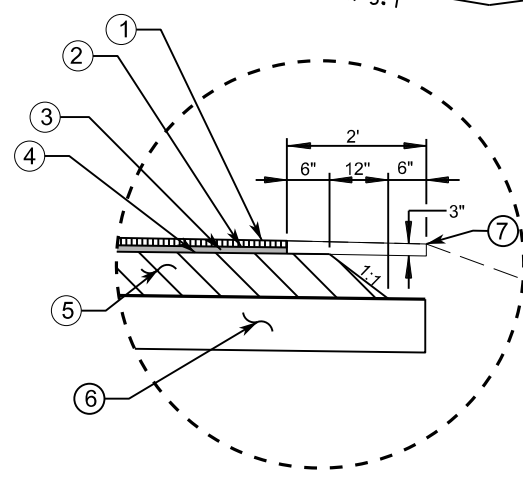
SEE PAVEMENT
DETAIL "B"

SEE PAVEMENT
DETAIL "B1"
FOR FM 1017 INTERSECTION



- SH 285 PROPOSED TYPICAL SECTION
- STA 197+75 TO STA 199+95
 - STA 201+30 TO STA 203+50
 - STA 203+50 TO STA 205+00 (44' TO 48' TRANSITION)

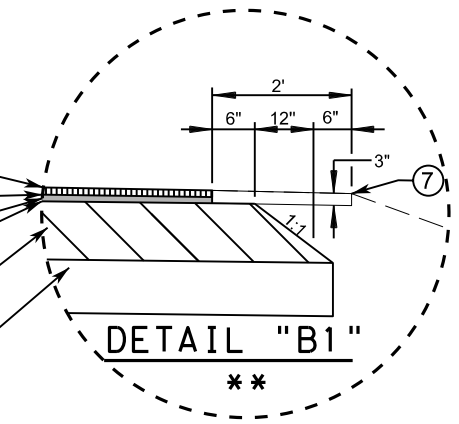
SEE PAVEMENT
DETAIL "A"



DETAIL "B"

LEGEND:

- ① PROPOSED 2.0" SP-C PG 76-22 SAC A (NO RAP/RAS)
- ② PROPOSED BONDING COURSE
- ③ PROPOSED 2.5" SP-B PG 76-22 SAC B
- ④ PROPOSED MC-30 PRIME COAT
- ⑤ PROPOSED 12" TY E GR 4 W/ 2% CEMENT ADMIX BY WT FLEX BASE (2 EQ LIFTS)
- ⑥ PROPOSED 12" STABILIZED SUBGRADE W/ 2% CEMENT ADMIX BY WT.
- ⑦ PROPOSED TY "A" BACKFILL (ITEM 134)



DETAIL "B1"

- PROPOSED 2.0" SP-C PG 76-22 SAC A (NO RAP/RAS)
- PROPOSED BONDING COURSE
- PROPOSED 2.5" SP-B PG 76-22 SAC A
- PROPOSED MC-30 PRIME COAT
- PROPOSED 16" TY E GR 4 W/ 2% CEMENT ADMIX BY WT FLEX BASE (2 EQ LIFTS)
- PROOF ROLL SUBGRADE (ADD MOISTURE) BEFORE PROOF ROLLING



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

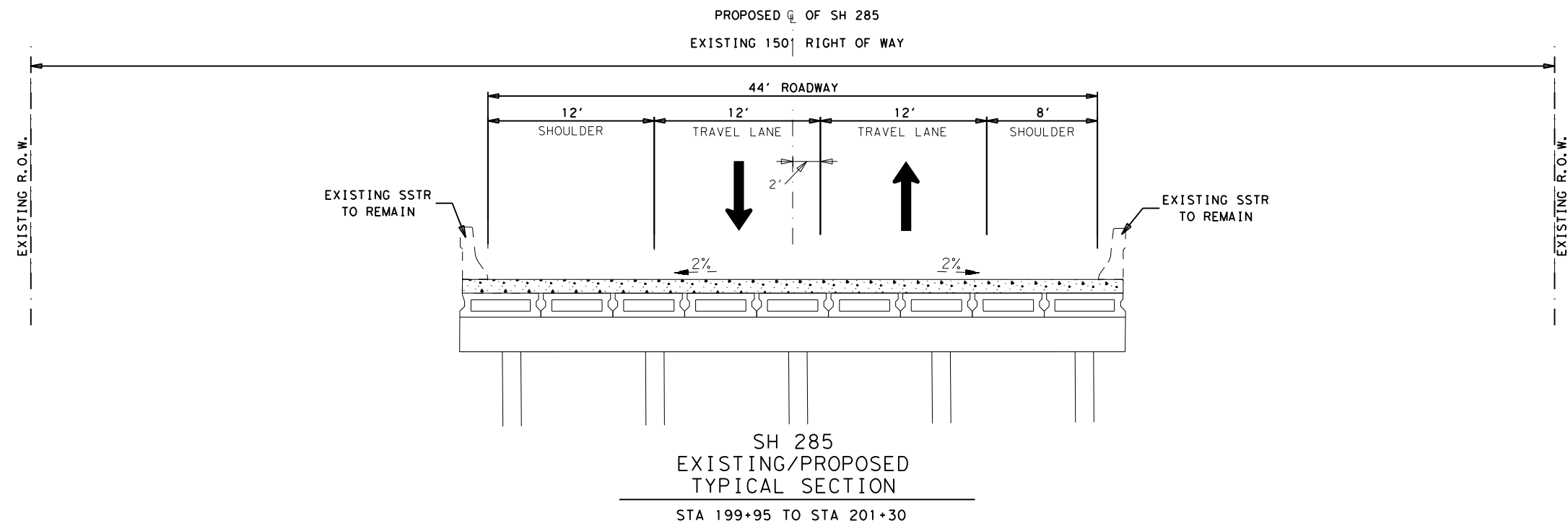
Texas Department of Transportation

SH 285 PROPOSED TYPICAL SECTIONS

SHEET 1 OF 3

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	8

DATE: 2/7/2024 10:49:12 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 PROPOSED TYPICALS.dgn



LEGEND

- CL - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R.O.W. - RIGHT OF WAY
- SHLDR - SHOULDER

NOTES:

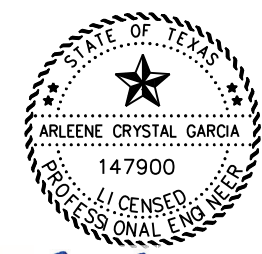
A MINIMUM OF 4" OF THE PROPOSED BASE LAYER SHALL BE NEW. IF SALVAGE AND DETOUR MATERIAL PERMITS, 8" OF THE PROPOSED BASE LAYER SHALL BE USED FROM SALVAGE MATERIAL REMOVED UNDER ITEM 105-6020 AND REWORKED AS PER PERTINENT ITEM 251-6158.

THERE SHALL BE NO WORK DONE ON BRIDGE SECTION APART FROM REMOVING AND REPLACING THE METAL BEAM GUARD FENCE.

WHERE REQUIRED BY FIXTURES OR UNUSUAL CONDITIONS, THE GOVERNING SLOPES MAY BE VARIED WHEN SPECIFICALLY DIRECTED BY THE ENGINEER.

WHERE POSSIBLE AND UNLESS OTHERWISE DIRECTED BY THE ENGINEER, PERMISSIBLE CONSTRUCTION JOINTS SHALL FALL ON STRIPING LINES AS SHOWN ON THE PAVEMENT MARKINGS LAYOUTS.

ALL GRADING SHALL BE DONE WITHIN R.O.W. LIMITS UNLESS OTHERWISE NOTED ON PLANS. TOPSOIL TO BE SCARIFIED, SALVAGED, AND STOCKPILED IN THE R.O.W.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

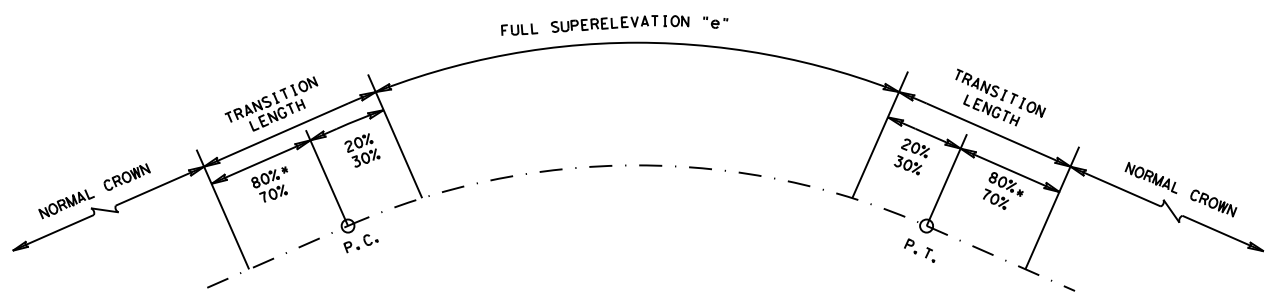


**SH 285
 PROPOSED
 TYPICAL SECTIONS**

SHEET 2 OF 3				
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		9

DATE: 2/7/2024 10:49:13 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1 - General\SH 285 PROPOSED TYPICALS.dgn

SUPERELEVATION DATA CL SH 285



CURVE ID	P.C. STA	P.T. STA	RADIUS (FT)	e%	STATION LIMITS FULL "e"	TRANSITION LENGTHS AT P.C.			TRANSITION LENGTHS AT P.T.		
						BEGIN STA	END STA	LENGTH	BEGIN STA	END STA	LENGTH
CURVE 08	176+74.99	181+58.98	11,459.16	2.2%	177+21.49 TO 181+12.48	175+66.49	177+21.49	155	181+12.48	182+67.48	155
* CURVE 11	219+40.36	224+87.03	5,729.58	3.8%	219+99.96 TO 224+27.43	217+01.96	219+99.96	298	224+27.43	227+25.43	298
* CURVE 14	371+47.04	398+45.71	4,583.66	4.6%	372+14.84 TO 397+77.91	368+75.84	372+14.84	339	397+77.91	401+16.91	339
CURVE 17	518+05.58	524+05.58	5,729.58	3.8%	518+69.78 TO 523+41.38	516+55.78	518+69.78	214	523+41.38	525+55.38	214
CURVE 20	561+00.00	599+00.00	5,729.58	3.8%	561+89.40 TO 598+10.60	558+91.40	561+89.40	298	598+10.60	601+08.6	298

AS PER TABLE 2-4 MINIMUM RADAI AND SUPERELEVATION RATES FOR HIGH-SPEED OR NON-URBAN FACILITIES, $e_{max} = 6\%$
 AS PER TABLE 2-8 PORTION OF SUPERELEVATION TRANSITION LOCATED ON THE TANGENT

LEGEND

- CL - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R.O.W. - RIGHT OF WAY
- SHLDR - SHOULDER

NOTES:

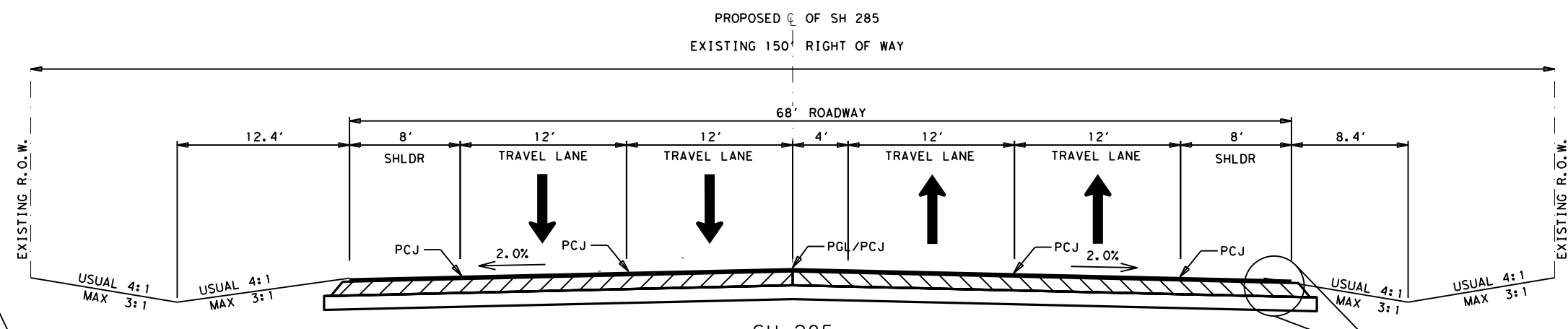
A MINIMUM OF 4" OF THE PROPOSED BASE LAYER SHALL BE NEW. IF SALVAGE AND DETOUR MATERIAL PERMITS, 8" OF THE PROPOSED BASE LAYER SHALL BE USED FROM SALVAGE MATERIAL REMOVED UNDER ITEM 105-6020 AND REWORKED AS PER PERTINENT ITEM 251-6158.

THERE SHALL BE NO WORK DONE ON BRIDGE SECTION APART FROM REMOVING AND REPLACING THE METAL BEAM GUARD FENCE.

WHERE REQUIRED BY FIXTURES OR UNUSUAL CONDITIONS, THE GOVERNING SLOPES MAY BE VARIED WHEN SPECIFICALLY DIRECTED BY THE ENGINEER.

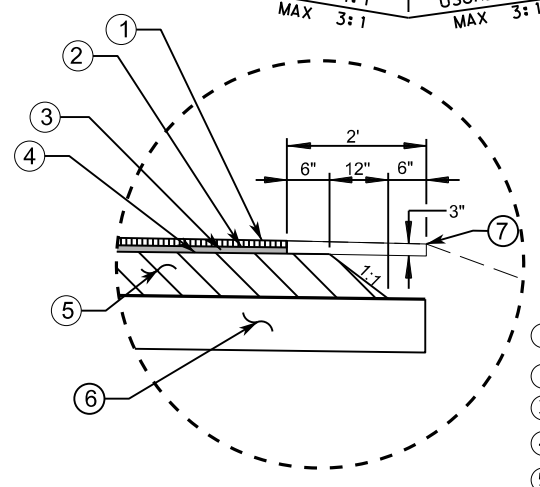
WHERE POSSIBLE AND UNLESS OTHERWISE DIRECTED BY THE ENGINEER, PERMISSIBLE CONSTRUCTION JOINTS SHALL FALL ON STRIPING LINES AS SHOWN ON THE PAVEMENT MARKINGS LAYOUTS.

ALL GRADING SHALL BE DONE WITHING R.O.W. LIMITS UNLESS OTHERWISE NOTED ON PLANS. TOPSOIL TO BE SCARIFIED, SALVAGED, AND STOCKPILED IN THE R.O.W.



SH 285 PROPOSED TYPICAL SECTION

- STA 211+10 TO STA 263+90
- STA 263+90 TO STA 272+90 (68' TO 48' TRANSITION)
- STA 361+10 TO STA 435+80
- STA 435+80 TO STA 444+80 (68' TO 48' TRANSITION)
- STA 533+00 TO STA 585+00
- STA 585+00 TO STA 607+70
- STA 607+70 TO STA 616+70 (68' TO 48' TRANSITION)



DETAIL "B"

LEGEND:

- ① PROPOSED 2.0" SP-C PG 76-22 SAC A (NO RAP/RAS)
- ② PROPOSED BONDING COURSE
- ③ PROPOSED 2.5" SP-B PG 76-22 SAC B
- ④ PROPOSED MC-30 PRIME COAT
- ⑤ PROPOSED 12" TY E GR 4 W/ 2% CEMENT ADMIX BY WT FLEX BASE (2 EQ LIFTS)
- ⑥ PROPOSED 12" STABILIZED SUBGRADE W/ 2% CEMENT ADMIX BY WT.
- ⑦ PROPOSED TY "A" BACKFILL (ITEM 134)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285 PROPOSED TYPICAL SECTIONS

SHEET 3 OF 3

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		10

DATE: 3/1/2024 10:56:40 AM
 FILE: P:\dot\project\wiseonline.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SH 285	SUMMARY OF REMOVAL ITEMS												
	104 6028	104 6054	496 6004	496 6005	496 6006	496 6007	496 6008	542 6001	542 6004	544 6003	*	*	*
	REMOVING CONC (MISC)	REMOVING CONCRETE (MOW STRIP)	REMOV STR (SET)	REMOV STR (WINGWAL L)	REMOV STR (HEADWAL L)	REMOV STR (PIPE)	REMOV STR (BOX CULVERT)	REMOVE METAL BEAM GUARD FENCE	RM MTL BM GD FENCE TRANS (THRIE-B EAM)	GUARDRAIL END TREATMENT (REMOVE)	REMOVE ASPHALT DRIVEWAY	REMOVE GRAVEL DRIVEWAY	REMOVE DIRT DRIVEWAY
SY	LF	EA	EA	EA	LF	LF	LF	EA	EA	SY	SY	SY	
SHEET 1 OF 22	9			2	4	138	76					500	
SHEET 2 OF 22	3				2	76					112		
SHEET 3 OF 22	10			4			152				60		
SHEET 4 OF 22	-										352		
SHEET 5 OF 22	-	872						475	4	4	210	111	
SHEET 6 OF 22	3			2			68						
SHEET 7 OF 22	3			2			76				1056		
SHEET 8 OF 22	17			2			132						
SHEET 9 OF 22	-												
SHEET 10 OF 22	10			2			66					548	97
SHEET 11 OF 22	-												
SHEET 12 OF 22	13			2			66						
SHEET 13 OF 22	7			2			71						
SHEET 14 OF 22	17			2			132				202		
SHEET 15 OF 22	11			2		30	66					308	
SHEET 16 OF 22	11			2	2	136	66					253	
SHEET 17 OF 22	14			2			132				242		
SHEET 18 OF 22	4			2			66						
SHEET 19 OF 22	14			2			132				309	75	
SHEET 20 OF 22	-												
SHEET 21 OF 22 (UP TO STA 585+00)	10			2			72						
CSJ 0482-01-036 TOTALS	156	872	0	32	8	380	1373	475	4	4	2543	1795	97
SHEET 21 OF 22 (FROM STA 585+00)											194	234	
SHEET 22 OF 22			2			160							
CSJ 0842-02-035 TOTALS	0	0	2	0	0	160	0	0	0	0	194	234	0
PROJECT TOTALS	156	872	2	32	8	540	1373	475	4	4	2737	2029	97

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY).

SH 285	SUMMARY OF ADDITIONAL PROJECT ITEMS				
	500 6001	502 6001	6001 6002	6185 6002	6185 6005
	MOBILIZATI ON	BARRICADES, SIGNS AND TRAFFIC HANDLING	PORTABLE CHANGEAB LE MESSAGE SIGN	TMA (STATIONA RY)	TMA (MOBILE OPERATION)
LS	MO	EA	DAY	DAY	
PROJECT TOTALS	1.00	74	4	1140	50

Pharr District Central Design

**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**

SHEET 1 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		11

DATE: 3/1/2024 10:56:41 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SUMMARY OF WORKZONE TRAFFIC CONTROL ITEMS																				
SH 285	508 6001	510 6003	512 6001	512 6025	512 6049	545 6019	545 6003	545 6005	662 6008	662 6037	662 6067	662 6071	662 6075	662 6080	662 6090	662 6098	662 6050	662 6096	677 6001	678 6002
	CONSTRUCTI NG DETOURS	ONE-WAY TRAF CONT (PORT TRAF SIG)	PORT CTB (FUR & INST) (SGL SLOPE) (TY 1)	PORT CTB (MOVE) (S GL SLP) (TY 1)	PORT CTB (REMOVE) (SGL SLP) (TY 1)	CRASH CUSH ATTEN (INSTL) (S (N) (TL3)	CRASH CUSH ATTEN (MOVE & RESET)	CRASH CUSH ATTEN (REMOVE)	WK ZN PAV MRK NON-REMOV (W) 6" (SL D)	WK ZN PAV MRK NON-REMOV (Y) 6" (SL D)	WK ZN PAV MRK REMOV (W) 6" (SL D)	WK ZN PAV MRK REMOV (W) 8" (SL D)	WK ZN PAV MRK REMOV (W) 24" (S LD)	WK ZN PAV MRK REMOV (W) (ARRO W)	WK ZN PAV MRK REMOV (W) (WORD)	WK ZN PAV MRK REMOV (Y) 6" (SL D)	WK ZN PAV MRK REMOV (REFL) TY II-A-A	WK ZN PAV MRK REMOV (Y) 6" (BR K)	ELIM EXT PAV MRK & MRKS (4")	PAV SURF PREP FOR MRK (6")
	SY	MO	LF	LF	LF	EA	EA	EA	LF	LF	LF	LF	LF	EA	EA	LF	EA	LF	LF	LF
PHASE I																				
PHASE I STEP I	-	1	3210	-	-	4	-	-	5010	-	751	-	40	-	-	7200	180	-	12530	270
PHASE I STEP II	-	-	-	3210	-	4	4	-	-	-	9640	-	-	-	-	10240	258	-	3560	540
STA. 189+20 TO STA. 215+60	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE II																				
PHASE II STEP I (DETOUR)	14779	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE II STEP II	-	-	5340	3210	-	2	10	-	18640	18640	7830	-	-	-	-	7830	665	-	24430	-
PHASE II STEP II STAGE A	-	-	300	-	-	2	1	-	7200	7200	320	-	36	-	-	640	16	-	4200	-
PHASE II STEP II STAGE B	-	-	-	300	-	-	5	-	-	-	8340	-	36	-	-	7920	18	-	3896	-
PHASE II STEP III	-	-	300	8550	-	-	12	-	-	-	21834	82	28	1	1	21834	654	-	-	-
STA. 96+00 TO STA. 189+20	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE III																				
PHASE III STEP I (DETOUR)	14093	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE III STEP II	-	-	150	8850	-	-	10	-	17980	17980	10240	-	-	-	-	10240	708	-	27060	-
PHASE III STEP III	-	-	-	8280	-	-	4	-	-	-	23420	-	-	-	-	23420	578	220	-	-
STA. 215+60 TO STA. 305+50	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE IV																				
PHASE IV STEP I (DETOUR)	22932	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE IV STEP II	-	-	7650	9000	-	-	12	-	34380	34380	8840	-	-	-	-	8840	1139	-	33460	-
PHASE IV STEP III	-	-	360	16550	-	-	12	4	-	-	37640	-	-	-	-	38320	944	-	2400	-
STA. 305+50 TO STA. 477+40	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE V																				
PHASE V STEP I (DETOUR)	13581	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE V STEP II	-	-	-	10120	-	-	5	-	20120	20120	8660	-	-	-	-	8660	725	-	25265	-
PHASE V STEP III	-	-	-	10610	-	-	5	5	-	-	24840	-	-	-	-	25520	621	-	2040	-
STA. 477+40 TO STA. 585+00 and 620+00 to 627+00	-	-	-	-	17310	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
CSJ 0482-01-036 TOTALS	65385	1	17310	78680	17310	12	80	9	103330	98320	162355	82	140	1	1	170664	6506	220	138841	810
PHASE V (585+00 TO 620+00)																				
PHASE V STEP I (DETOUR)	4803	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
PHASE V STEP II	-	-	-	3430	0	-	3	2	7000	7000	0	-	-	-	-	175	175	-	4355	-
PHASE V STEP III	-	-	-	3430	-	-	1	1	-	-	7000	-	-	-	-	7000	175	-	0	-
STA. 585+00 TO STA. 620+00	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
CSJ 0482-02-035 TOTALS	4803	0	0	6860	0	0	4	3	7000	7000	7000	0	0	0	0	7000	350	0	4355	0
PROJECT TOTALS	70188	1	17310	85540	17310	12	84	12	110330	105320	169355	82	140	1	1	177664	6856	220	143196	810

Pharr District Central Design

**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**

SHEET 2 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST COUNTY			SHEET NO.
	PHR JIM HOGG			12

DATE: 3/1/2024 10:56:43 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SUMMARY OF ROADWAY ITEMS												
SH 285	100	134	204	354	400	400	402	432	432	464	467	530
	6002	6001	6003	6021	6001	6011	6001	6045	6003	6003	6363	6005
	PREPARING ROW	BACKFILL (TY A)	SPRINKLING (DUST CONTROL)	PLANE ASPH CONC PAV (0" TO 2")	STRUCT EXCAV	SAND BACKFILL	TRENCH EXCAVATION PROTECTION	RIPRAP (MOW STRIP) (4 IN)	RIPRAP (CONC) (6 IN)	RC PIPE (CL III) (18 IN)	SET (TY II) (18 IN) (RCP) (6: 1) (P)	DRIVEWAYS (ACP)
	STA	STA	MG	SY	CY	CY	LF	CY	CY	LF	EA	SY
SHEET 1 OF 44	12	12	48	190	-	-	-	-	-	104	6	355
SHEET 2 OF 44	12	12	48	-	-	-	-	-	-	32	2	111
SHEET 3 OF 44	12	12	48	-	-	-	-	-	-	32	2	111
SHEET 4 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 5 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 6 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 7 OF 44	12	12	48	-	-	-	-	-	-	72	4	256
SHEET 8 OF 44	12	12	48	-	-	-	-	-	-	32	2	126
SHEET 9 OF 44	12	12	48	-	-	-	-	56	-	-	-	-
SHEET 10 OF 44	12	12	48	-	-	-	-	-	-	72	4	227
SHEET 11 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 12 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 13 OF 44	12	12	48	-	-	-	-	-	-	32	2	96
SHEET 14 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 15 OF 44	12	12	48	-	-	-	-	-	-	32	2	109
SHEET 16 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 17 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 18 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 19 OF 44	12	12	48	-	-	-	-	-	-	32	2	111
SHEET 20 OF 44	12	12	48	-	-	-	-	-	-	96	4	358
SHEET 21 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 22 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 23 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 24 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 25 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 26 OF 44	12	12	48	-	-	-	-	-	-	32	2	146
SHEET 27 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 28 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 29 OF 44	12	12	48	-	18	-	44	-	1.63	80	4	253
SHEET 30 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 31 OF 44	12	12	48	-	-	-	-	-	-	40	2	145
SHEET 32 OF 44	12	12	48	-	-	-	-	-	-	32	2	111
SHEET 33 OF 44	12	12	48	-	-	-	-	-	-	40	2	145
SHEET 34 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 35 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 36 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 37 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 38 OF 44	12	12	48	-	-	-	-	-	-	80	4	248
SHEET 39 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 40 OF 44	12	12	48	-	-	-	-	-	-	-	-	-
SHEET 41 OF 44 (UP TO STA 585+00)	9	9	36	-	-	-	-	-	-	-	-	-
						1.63						
CSJ 0482-01-036 TOTALS	489	489	1956	190	18	1.63	44	56	1.63	840	46	2908
SHEET 41 OF 44 (FROM STA 585+00)	3	3	12							80	4	336
SHEET 42 OF 44	12	12	48									
SHEET 43 OF 44	12	12	48									
SHEET 44 OF 44	8	8	32									
CSJ 0482-02-035 TOTALS	35	35	140	0	0	0	0	0		80	4	336
PROJECT TOTALS	524	524	2096	190	18	1.63	44	56	1.63	920	50	3244

SUMMARY OF BRIDGE ITEMS						NBI: 211250048201001				
SH 285	420	450	540	540	544					
	6136	6006	6001	6006	6001					
	CL C CONC (RAC-R)	RAIL (TY T223)	MTL W-BEAM GD FEN (TIM POST)	MTL BEAM GD FEN TRANS (THRIE-B EAM)	GUARDRAIL END TREATMENT (INSTALL)					
	CY	LF	LF	EA	EA					
RAIL RETROFIT LAYOUT	8	80	475	4	4					
PROJECT TOTALS	8	80	475	4	4					

Pharr District Central Design

**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**

SHEET 3 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST COUNTY			SHEET NO.
	PHR JIM HOGG			13

DATE: 3/15/2024 9:54:02 AM
 FILE: \\phr\dot\project\wiseonline.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SH 285	SUMMARY OF ROADWAY MATERIALS ITEMS																					
	&	&	&	&	&	105 6020	&	110 6001	132 6006	247 6225	251 6158	251 6210	275 6001	275 6032	275 6374	275 6005	305 6005	310 6009	3077 6033	3077 6010	3084 6001	
	AVAILABLE SALVAGE FLEX BASE OF EXISTING ROADWAY	## FLEX BASE FROM DETOUR TO BE REUSED	TOTAL FLEXBASE NEEDED (12")	INTERSECT ION TOTAL FLEXBASE NEEDED (16")	MINIMUM 4" REQUIRED NEW FLEX BASE	REMOVING STAB BASE & ASPH PAV (12")	REMOVING STAB BASE & ASPH PAV (12")	EXCAVATIO N (ROADWAY)	EMBANKMEN T (FINAL) (DENS CONT) (TY C)	FL BS (RDWY DEL) (TY E 4) (FNAL POS)	RWRK BS MTL (TY B) (12") (DC) (ORG POS)	RWRK BS MTL (TY B) (14") (DC) (ORG POS)	CEMENT	INTERSECTION CEMENT TREAT (NEW BASE) (16")	CMT TRT (MX EXST MTL & NW BS) (12") (DC)	CEMENT TREAT (EXIST MATL) (12 ")	SALV, HAUL & STKPL RCL APH PV (6 TO 8")	PRIME COAT (MC-30)	SP MIXES SP-C SAC-A PG76-22	SP MIXES SP-B SAC-B PG76-22	BONDING COURSE	
CY	CY	CY	CY	CY	SY	CY	CY	CY	CY	CY	CY	TON	SY	SY	SY	SY	GAL	TON	TON	GAL		
PHASE I STEP I	2639	-	2966		989	1987	662	143574.4	58474	989	1977	-	192	-	8898	9176	7637	1780			1229	
PHASE I STEP II	1601	-	2020		674	765	255	-	-	674	1346	-	132	-	6060	6338	4530	1212			824	
PHASE II STEP I	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
PHASE II STEP II	9067	-	9719		3240	7765	2588	-	-	3240	6479	-	628		29157	30165	26497	5832			4012	
PHASE II STEP II STAGE A	235	-	-	314	-	706	235	-	-	314	-	-	11	706	-	-	706	143			101	
PHASE II STEP II STAGE B	247	-	-	329	-	739	247	-	-	329	-	-	12	739	-	-	739	148			106	
PHASE II STEP III	6553	5380	7270	-	2479	19676	6559	-	-	2479	-	4791	478	-	22311	22845	18640	4463			2961	
PHASE III STEP I	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
PHASE III STEP II	9680	-	12755	-	4252	5303	1768	-	-	4252	7914	589	821	-	38264	39348	27994	7653			5300	
PHASE III STEP III	6320	4214	8557	-	2853	14505	4835	-	-	2853	1490	4214	554	-	25670	26669	17980	5134			3516	
PHASE IV STEP I	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
PHASE IV STEP II	17173	-	22184	-	7395	7153	2384	-	-	7395	14789	-	1427	-	66550	68460	49660	13310			9212	
PHASE IV STEP III	8817	9283	12226	-	4076	26452	8817	-	-	4076	-	8150	790	-	36677	38069	34380	7336			5029	
PHASE V STEP I (UP TO STA 585+00)	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
PHASE V STEP II (UP TO STA 585+00)	9874	-	13546	-	4516	2532	844	-	-	4516	8199	831	869	-	40638	41644	29062	8128			5576	
PHASE V STEP III (UP TO STA 585+00)	7516	6129	10172	-	3391	2205	735	-	-	3391	629	6152	658	-	30517	31812	21520	6103			4124	
PHASE VI	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	33591			20626
CSJ 0482-01-036 TOTALS	79722	25006	101415	643	33865	89788	29929	143574.4	58474	34508	42823	24727	6572	1445	304742	314526	239345	61242	33591	41990	20626	
PHASE V STEP I (585+00 TO 620+00)								12522.2	5781.4													
PHASE V STEP II (585+00 TO 620+00)	3685		4912		1637	4637	1546			1637	2973	302	318		14736	15236	10112	2948				
PHASE V STEP III (585+00 TO 620+00)	2518	2053	3357		1119	25414	8471			1119	208	2030	217		10070	10360	7000	2014				
PHASE VI																			2828	3534	1737	
CSJ 0482-02-035 TOTALS	6203	2053	8269	0	2756	30051	10017	12522.2	5781.4	2756	3181	2332	535	0	24806	25596	17112	4962	2828	3534	1737	
PROJECT TOTALS	85925	27059	109684	643	36621	119839	39946	156096.6	64255.4	37264	46004	27059	7107	1445	329548	340122	256457	66204	36419	45524	22363	

- ## REWORK (SALVAGE) MATERIAL IS TAKEN FROM DETOUR MATERIAL (##) AND EXISTING BASE (//) PAID UNDER ITEM 251
- * ALL RAP MATERIAL 256457 SY SHALL BECOME PROPERTY OF THE CONTRACTOR.
- BONDING COURSE RATE IS FOR ESTIMATING PURPOSES ONLY AND MAY BE MODIFIED BY ENGINEER AS PER PERTINENT ITEM 3077.
- & FOR CONTRACTOR INFORMATION ONLY (NON-PAY).
- EXCESS SALVAGE MATERIAL TO BE HAULED OFF AND SHALL BECOME PROPERTY OF THE CONTRACTOR AFTER PROJECT COMPLETION.
- EST. WEIGHT OF FLEXIBLE BASE = 3375 LBS/CY (COMPACTED DRY WEIGHT)
- EST. WEIGHT OF SUBGRADE = 2970 LBS/CY (COMPACTED)
- EST. APPL. RATE OF PRIME COAT(MC-30) = 0.20 GAL/SY
- EST. APPL. RATE OF SP MIXES = 114 LBS/SY/IN
- EST. APPL. RATE OF BONDING COURSE = 0.07 GAL/SY
- ITEM 105 SPECIFIED FOR EXISTING BASE THAT WILL NOT BE INCORPORATED INTO NEW FLEX BASE. CONTRACTOR CAN NOT REMOVE MATERIAL UNTIL THE END OF THE PROJECT OR UNDER AREA ENGINEER WRITTEN PERMISSION.

Pharr District Central Design

**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**

SHEET 4 OF 7


© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST COUNTY			SHEET NO.
	PHR JIM HOGG			14

DATE: 3/1/2024 10:56:45 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SUMMARY OF DRAINAGE ITEMS											
	400 6001	400 6005	400 6010	402 6001	420 6009	432 6002	462 6006	464 6060	466 6150	467 6390	467 6392
SH 285	* STRUCT EXCAV	CEM STABIL BKFL	STRUCT EXCAV (SPECIAL)	TRENCH EXCAVATI ON PROTECTI ON	CL A CONC (COLLAR)	RIPRAP (CONC) (5 IN)	CONC BOX CULV (5 FT X 2 FT)	RC PIPE (CL IV) (24 IN) (SPL)	WINGWALL (FW - 0) (HW=3 FT)	SET (TY II) (24 IN) (RCP) (4: 1) (C)	SET (TY II) (24 IN) (RCP) (5: 1) (C)
	CY	CY	CY	LF	EA	CY	LF	LF	EA	EA	EA
SHEET 1 OF 11											
STA. 99+10	45	29	17	-	-	-	-	68	-	-	2
STA. 101+51	82	49	28	-	-	-	-	136	-	2	-
SHEET 2 OF 11											
STA. 111+00	81	46	28	-	-	-	-	128	-	-	2
STA. 121+00	107	51	29	-	-	-	-	144	-	2	-
SHEET 3 OF 11											
STA. 158+55	104	39	22	-	-	51	72	-	2	-	-
STA. 164+40	122	37	21	-	-	71	68	-	2	-	-
SHEET 4 OF 11											
STA. 219+76	234	92	57	117	-	55	200	-	2	-	-
STA. 259+51	228	81	50	-	-	109	176	-	2	-	-
SHEET 5 OF 11											
STA. 273+78	266	70	44	-	-	55	152	-	2	-	-
STA. 285+13	248	37	21	-	-	95	68	-	2	-	-
SHEET 6 OF 11											
STA. 311+00	193	61	39	-	-	109	136	-	2	-	-
STA. 360+24	238	88	55	-	-	81	192	-	2	-	-
SHEET 7 OF 11											
STA. 392+01	160	54	31	122	-	51	100	-	2	-	-
STA. 415+36	278	120	77	-	-	111	276	-	2	-	-
SHEET 8 OF 11											
STA. 442+00	184	67	44	-	-	109	152	-	2	-	-
STA. 456+74	121	37	21	-	-	71	68	-	2	-	-
SHEET 9 OF 11											
STA. 473+39	194	51	30	-	-	-	-	144	-	-	2
STA. 491+99	182	63	39	-	-	109	136	-	2	-	-
SHEET 10 OF 11											
STA. 509+85	78	51	31	-	-	-	-	144	-	-	2
STA. 530+01	199	81	50	-	-	81	176	-	2	-	-
SHEET 11 OF 11											
STA. 573+99	304	96	59	121	-	55	208	-	2	-	-
TEMPORARY CULVERT CROSSING EXTENSION											
STA. 99+10	-	-	-	-	1	-	-	16	-	-	1
STA. 101+51	-	-	-	-	1	-	-	12	-	1	-
STA. 111+00	-	-	-	-	1	-	-	8	-	-	1
CSJ 0482-01-036 TOTALS	3648	1300	793	360	3	1213	2180	800	30	5	10
SHEET 11 OF 11											
STA 606+22	234	84	53			109	184		2		
CSJ 0482-02-035 TOTALS	234	84	53	0	0	109	184	0	2	0	0
PROJECT TOTALS	3882	1384	846	360	3	1322	2364	800	32	5	10

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY). SUBSIDIARY TO PERTINENT BID ITEMS.

Pharr District Central Design



**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**

SHEET 5 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST COUNTY			SHEET NO.
	PHR JIM HOGG			15

DATE: 3/14/2024 2:45:11 PM
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SH 285	SUMMARY OF EROSION CONTROL ITEMS										
	164 6023	164 6029	*	168 6001	506 6003	506 6011	506 6021	506 6024	506 6041	506 6043	5088 6001
	CELL FBR MLCH SEED(PER M)(RURAL) (CLAY)	CELL FBR MLCH SEED(TEMP) (WARM)	FERTILIZE R	VEGETATIV E WATERING	ROCK FILTER DAMS (INSTALL) (TY 3)	ROCK FILTER DAMS (REMOVE)	CONSTRUCT ION EXITS (INSTALL) (TY 2)	CONSTRUCT ION EXITS (REMOVE)	BIODEG EROSN CONT LOGS (IN STL (12"))	BIODEG EROSN CONT LOGS (REMOVE)	BIRD EXCLUSION METHOD
SY	SY	TON	MG	LF	LF	SY	SY	LF	LF	SF	
SHEET 1 OF 22	27389	27389	0.283	476			156	156	720	720	152
SHEET 2 OF 22	27051	27051	0.28	469					380	380	-
SHEET 3 OF 22	27200	27200	0.28	472	150	150			400	400	304
SHEET 4 OF 22	26931	26931	0.28	467			156	156	360	360	-
SHEET 5 OF 22	31694	31694	0.33	550					390	390	-
SHEET 6 OF 22	21839	21839	0.23	379			156	156	470	470	136
SHEET 7 OF 22	21764	21764	0.22	378					380	380	152
SHEET 8 OF 22	25467	25467	0.26	442					460	460	660
SHEET 9 OF 22	27198	27198	0.28	472					360	360	-
SHEET 10 OF 22	26728	26728	0.28	464			156	156	500	500	330
SHEET 11 OF 22	26484	26484	0.27	460					240	240	-
SHEET 12 OF 22	21888	21888	0.23	380					440	440	462
SHEET 13 OF 22	21844	21844	0.23	379					320	320	213
SHEET 14 OF 22	21719	21719	0.22	377					380	380	660
SHEET 15 OF 22	24458	24458	0.25	424					560	560	396
SHEET 16 OF 22	27055	27055	0.28	470					580	580	330
SHEET 17 OF 22	26944	26944	0.28	468			156	156	420	420	660
SHEET 18 OF 22	27113	27113	0.28	471					320	320	198
SHEET 19 OF 22	21954	21954	0.23	381					440	440	660
SHEET 20 OF 22	22189	22189	0.23	385					320	320	-
SHEET 21 OF 22 (UP TO STA 585+00)	10158	10158	0.11	176					120	120	360
CSJ 0482-01-036 TOTALS	515067	515067	5.333	8940	150	150	780	780	8560	8560	5673
SHEET 21 OF 22 (FROM STA 585+00)	11924	11924	0.12	207					360	360	-
SHEET 22 OF 22	21509	21509	0.22	373			156	156	320	320	-
CSJ 0482-02-035 TOTALS	33433	33433	0.34	580	0	0	156	156	680	680	0
PROJECT TOTALS	548500	548500	5.673	9520	150	150	936	936	9240	9240	5673

SH 285	SUMMARY OF SIGNING ITEMS							
	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060	
	ALUMINUM SIGNS (TY A)	IN SM RD SN SUP&AM TYS80(1) SA(P)	IN SM RD SN SUP&AM TYS80(1) SA(T)	IN SM RD SN SUP&AM TYS80(1) SA(U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND(BI)	REMOVE DELIN & OBJECT MARKER ASSMS	
SF	EA	EA	EA	EA	EA	EA		
SHEET 1 OF 22	59	9	7	2	16	6	6	
SHEET 2 OF 22	-	2	1	-	3	3	3	
SHEET 3 OF 22	-	-	-	-	-	7	7	
SHEET 4 OF 22	16	-	2	-	2	-	-	
SHEET 5 OF 22	10.5	3	1	-	3	-	-	
SHEET 6 OF 22	-	-	2	-	-	2	2	
SHEET 7 OF 22	-	1	2	-	1	2	2	
SHEET 8 OF 22	-	1	-	1	-	4	2	
SHEET 9 OF 22	-	-	-	-	-	-	-	
SHEET 10 OF 22	-	-	-	-	-	2	2	
SHEET 11 OF 22	-	2	-	1	1	-	-	
SHEET 12 OF 22	-	-	2	-	-	2	2	
SHEET 13 OF 22	-	-	-	-	-	2	2	
SHEET 14 OF 22	-	-	2	-	-	2	2	
SHEET 15 OF 22	-	2	-	1	1	2	3	
SHEET 16 OF 22	-	-	-	-	-	4	4	
SHEET 17 OF 22	-	-	-	-	-	2	2	
SHEET 18 OF 22	-	-	-	1	-	2	2	
SHEET 19 OF 22	-	1	1	-	-	2	2	
SHEET 20 OF 22	-	1	1	-	2	2	-	
SHEET 21 OF 22 (UP TO STA 585+00)	-	2	-	-	2	-	1	
CSJ 0482-01-036 TOTALS	85.5	24	21	6	31	46	44	
SHEET 21 OF 22 (FROM STA 585+00)	21	3	5	-	6	-	1	
SHEET 22 OF 22	-	2	-	1	-	2	2	
CSJ 0482-02-035 TOTALS	21	5	5	1	6	2	3	
PROJECT TOTALS	106.5	29	26	7	37	48	47	

PERMANENT SEEDING TO BE DONE AFTER THE ROADWAY CONSTRUCTION IS COMPLETE.

FERTILIZER QUANTITIES (TON) ARE BASED ON A RATE OF 100 LSB OF NITROGEN ACRE NPK 10-5-5.

VEGETATIVE WATERING APPLICATION RATE = 7,000 GAL/ACRE @ 12 CYCLES/1000(MG)

EROSION CONTROL LOG MAX LENGTH = 30 FEET

EROSION CONTROL LOG STD LENGTH = 10 FEET

EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER. CONTRACTOR SHALL CONSULT WITH THE FIELD ENGINEER BEFORE ANY EROSION CONTROL DEVICE IS INSTALLED.

* FOR CONTRACTOR INFORMATION ONLY.

Pharr District Central Design



**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**


SHEET 6 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		16

DATE: 3/1/2024 10:56:48 AM
 FILE: P:\txdot\project\wiseon\ine.com:TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH 285 SUMMARY OF ESTIMATED QUANTITIES

SH 285	SUMMARY OF PAVEMENT MARKING ITEMS																				
	662 6109	662 6111	666 6018	666 6021	666 6036	666 6048	666 6306	666 6318	666 6321	666 6343	668 6077	668 6083	668 6085	668 6116	672 6007	672 6009	672 6017	672 6018	678 6002	6038 6004	6038 6017
	WK ZN PAV MRK SHT TERM (TAB)TY W	WK ZN PAV MRK SHT TERM (TAB)TY Y-2	REFL PAV MRK TY I (W)6" (DO T) (100MIL)	REFL PAV MRK TY I (W)6" (LN DP) (100MIL)	REFL PAV MRK TY I (W)8" (SL D) (100MIL)	REFL PAV MRK TY I (W)24" (S LD) (100MIL)	RE PM W/RET REQ TY I (W)6" (BR K) (100MIL)	RE PM W/RET REQ TY I (Y)6" (BR K) (100MIL)	RE PM W/RET REQ TY I (Y)6" (SL D) (100MIL)	REF PROF PAV MRK TY (W)6" (S LD) (100MIL)	PREFAB PAV MRK TY C (W) (ARROW)	PREFAB PAV MRK TY C (W) (LNDP ARROW)	PREFAB PAV MRK TY C (W) (WORD)	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)	REFL PAV MRKR TY I-C	REFL PAV MRKR TY II-A-A	TRAFFIC BUTTON TY Y	TRAFFIC BUTTON TY B	PAV SURF PREP FOR MRK (6")	MULTIPOLY MER PAV MRK (W) (6") (S LD)	MULTIPOLY MER PAV MRK (Y) (6") (S LD)
EA	EA	LF	LF	LF	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	LF	LF	LF
SHEET 1 OF 22	8	404	-	-	98	55	-	845	2319	8298	1	-	1	1	5	118	465	1186	-	-	-
SHEET 2 OF 22	-	180	-	-	-	-	-	600	-	4800	-	-	-	-	30	-	960	-	-	-	-
SHEET 3 OF 22	-	180	-	-	-	-	-	600	-	4800	-	-	-	-	30	-	960	-	-	-	-
SHEET 4 OF 22	-	180	-	-	-	-	-	600	-	4800	-	-	-	-	30	-	960	-	-	-	-
SHEET 5 OF 22	178	230	114	-	-	-	80	115	3880	4530	-	-	-	4	93	776	184	540	270	270	-
SHEET 6 OF 22	610	240	-	255	-	-	910	-	4800	4800	-	2	-	-	45	60	960	-	-	-	-
SHEET 7 OF 22	523	240	-	207	-	-	990	-	4800	4800	-	2	-	-	50	60	960	-	-	-	-
SHEET 8 OF 22	236	236	114	48	-	-	160	50	4406	4800	-	-	-	8	83	921	40	-	-	-	-
SHEET 9 OF 22	-	270	-	-	-	-	-	400	3000	4800	-	-	-	-	114	600	360	-	-	-	-
SHEET 10 OF 22	-	180	-	-	-	-	-	600	-	4800	-	-	-	-	30	-	960	-	-	-	-
SHEET 11 OF 22	111	224	105	-	-	-	40	171	3436	4800	-	-	-	2	79	687	273	-	-	-	-
SHEET 12 OF 22	526	240	-	252	-	-	870	-	4800	4800	-	2	-	-	45	60	960	-	-	-	-
SHEET 13 OF 22	360	240	-	-	-	-	1200	-	4800	4800	-	-	-	-	60	60	960	-	-	-	-
SHEET 14 OF 22	471	240	-	108	-	-	1090	-	4800	4800	-	1	-	-	54	60	960	-	-	-	-
SHEET 15 OF 22	408	240	114	147	-	-	260	-	4800	4800	-	1	-	-	13	81	960	-	-	-	-
SHEET 16 OF 22	-	251	-	-	-	-	-	410	2556	4800	-	-	-	-	97	511	449	-	-	-	-
SHEET 17 OF 22	-	225	-	-	-	-	-	600	900	4800	-	-	-	-	63	180	780	-	-	-	-
SHEET 18 OF 22	36	215	48	-	-	-	-	250	2800	4800	-	-	-	-	83	560	400	-	-	-	-
SHEET 19 OF 22	650	240	64	255	-	-	710	-	4800	4800	-	2	-	-	34	60	960	-	-	-	-
SHEET 20 OF 22	360	240	-	-	-	-	1200	-	4800	4800	-	-	-	-	60	60	960	-	-	-	-
SHEET 21 OF 22 (UP TO 585+00)	165	110	-	-	-	-	550	-	2200	2200	-	-	-	28	28	440	-	-	-	-	-
SHEET 22 OF 22 (FROM 620+00 TO 642+10)	-	190	-	-	-	-	-	328	1800	4420	-	-	-	-	40	360	524	-	-	-	-
CSJ 0482-01-036 TOTALS	4642	4995	559	1272	98	55	8060	5569	65697	105848	1	10	1	1	408	1419	13180	8036	540	270	270
SHEET 21 OF 22 (FROM 585+00 TO 598+00)	195	130	12	-	-	-	650	-	2600	2600	-	1	-	-	32	32	520	-	-	-	-
SHEET 22 OF 22 (UP TO 620+00)	566	220	114	243	-	-	360	-	4400	4400	-	1	-	1	18	64	880	-	-	-	-
CSJ 0482-02-035 TOTALS	761	350	126	243	0	0	1010	0	7000	7000	0	2	0	1	50	96	1400	0	0	0	0
PROJECT TOTALS	5403	5345	685	1515	98	55	9070	5569	72697	112848	1	12	1	2	458	1515	14580	8036	540	270	270

Pharr District Central Design



**SH 285
SUMMARY OF
ESTIMATED
QUANTITIES**

SHEET 7 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		17



Estimate & Quantity Sheet

CONTROLLING PROJECT ID 0482-01-036

DISTRICT Pharr
HIGHWAY SH 285

COUNTY Brooks, Jim Hogg

CONTROL SECTION JOB				0482-01-036		0482-02-035		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00123534		A00204054			
COUNTY				Jim Hogg		Brooks			
HIGHWAY				SH 285		SH 285			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL	EST.	FINAL		
	100-6002	PREPARING ROW	STA	489.000		35.000		524.000	
	104-6028	REMOVING CONC (MISC)	SY	156.000				156.000	
	104-6054	REMOVING CONCRETE(MOW STRIP)	LF	872.000				872.000	
	105-6020	REMOVING STAB BASE & ASPH PAV (12")	SY	89,788.000		30,051.000		119,839.000	
	110-6001	EXCAVATION (ROADWAY)	CY	143,574.400		12,522.200		156,096.600	
	132-6006	EMBANKMENT (FINAL)(DENS CONT)(TY C)	CY	58,474.000		5,781.400		64,255.400	
	134-6001	BACKFILL (TY A)	STA	489.000		35.000		524.000	
	164-6023	CELL FBR MLCH SEED(PERM)(RURAL)(CLAY)	SY	515,067.000		33,433.000		548,500.000	
	164-6029	CELL FBR MLCH SEED(TEMP)(WARM)	SY	515,067.000		33,433.000		548,500.000	
	168-6001	VEGETATIVE WATERING	MG	8,940.000		580.000		9,520.000	
	204-6003	SPRINKLING (DUST CONTROL)	MG	1,956.000		140.000		2,096.000	
	247-6225	FL BS (RDWY DEL)(TY E GR 4)(FNAL POS)	CY	34,508.000		2,756.000		37,264.000	
	251-6158	RWRK BS MTL(TY B)(12")(DC)(ORG POS)	CY	42,823.000		3,181.000		46,004.000	
	251-6210	RWRK BS MTL (TY B)(14")(DC)(ORG POS)	CY	24,727.000		2,332.000		27,059.000	
	275-6001	CEMENT	TON	6,572.000		535.000		7,107.000	
	275-6005	CEMENT TREAT (EXIST MATL)(12")	SY	314,526.000		25,596.000		340,122.000	
	275-6032	CEMENT TREAT (NEW BASE) (16")	SY	1,445.000				1,445.000	
	275-6374	CMT TRT(MX EXST MTL & NW BS)(12")(DC)	SY	304,742.000		24,806.000		329,548.000	
	305-6005	SALV, HAUL & STKPL RCL APH PV (6 TO 8")	SY	239,345.000		17,112.000		256,457.000	
	310-6009	PRIME COAT (MC-30)	GAL	61,242.000		4,962.000		66,204.000	
	354-6021	PLANE ASPH CONC PAV(0" TO 2")	SY	190.000				190.000	
	400-6001	STRUCT EXCAV	CY	18.000				18.000	
	400-6005	CEM STABIL BKFL	CY	1,300.000		84.000		1,384.000	
	400-6010	STRUCT EXCAV (SPECIAL)	CY	793.000		53.000		846.000	
	400-6011	SAND BACKFILL	CY	1.630				1.630	
	402-6001	TRENCH EXCAVATION PROTECTION	LF	404.000				404.000	
	420-6009	CL A CONC (COLLAR)	EA	3.000				3.000	
	420-6136	CL C CONC (RAC-R)	CY	8.000				8.000	
	432-6002	RIPRAP (CONC)(5 IN)	CY	1,213.000		109.000		1,322.000	
	432-6003	RIPRAP (CONC)(6 IN)	CY	1.630				1.630	
	432-6045	RIPRAP (MOW STRIP)(4 IN)	CY	56.000				56.000	
	450-6006	RAIL (TY T223)	LF	80.000				80.000	
	462-6006	CONC BOX CULV (5 FT X 2 FT)	LF	2,180.000		184.000		2,364.000	
	464-6003	RC PIPE (CL III)(18 IN)	LF	840.000		80.000		920.000	
	464-6060	RC PIPE (CL IV) (24 IN) (SPL)	LF	800.000				800.000	
	466-6150	WINGWALL (FW - 0) (HW=3 FT)	EA	30.000		2.000		32.000	
	467-6363	SET (TY II) (18 IN) (RCP) (6: 1) (P)	EA	46.000		4.000		50.000	



Estimate & Quantity Sheet

CONTROLLING PROJECT ID 0482-01-036

DISTRICT Pharr
HIGHWAY SH 285

COUNTY Brooks, Jim Hogg

CONTROL SECTION JOB				0482-01-036		0482-02-035		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00123534		A00204054			
COUNTY				Jim Hogg		Brooks			
HIGHWAY				SH 285		SH 285			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL	EST.	FINAL		
	467-6390	SET (TY II) (24 IN) (RCP) (4: 1) (C)	EA	5.000				5.000	
	467-6392	SET (TY II) (24 IN) (RCP) (5: 1) (C)	EA	10.000				10.000	
	496-6004	REMOV STR (SET)	EA			2.000		2.000	
	496-6005	REMOV STR (WINGWALL)	EA	32.000				32.000	
	496-6006	REMOV STR (HEADWALL)	EA	8.000				8.000	
	496-6007	REMOV STR (PIPE)	LF	380.000		160.000		540.000	
	496-6008	REMOV STR (BOX CULVERT)	LF	1,373.000				1,373.000	
	500-6001	MOBILIZATION	LS	1.000				1.000	
	502-6001	BARRICADES, SIGNS AND TRAFFIC HANDLING	MO	74.000				74.000	
	506-6003	ROCK FILTER DAMS (INSTALL) (TY 3)	LF	150.000				150.000	
	506-6011	ROCK FILTER DAMS (REMOVE)	LF	150.000				150.000	
	506-6021	CONSTRUCTION EXITS (INSTALL) (TY 2)	SY	780.000		156.000		936.000	
	506-6024	CONSTRUCTION EXITS (REMOVE)	SY	780.000		156.000		936.000	
	506-6041	BIODEG EROSN CONT LOGS (IN STL) (12")	LF	8,560.000		680.000		9,240.000	
	506-6043	BIODEG EROSN CONT LOGS (REMOVE)	LF	8,560.000		680.000		9,240.000	
	508-6001	CONSTRUCTING DETOURS	SY	65,385.000		4,803.000		70,188.000	
	510-6003	ONE-WAY TRAF CONT (PORT TRAF SIG)	MO	1.000				1.000	
	512-6001	PORT CTB (FUR & INST)(SGL SLOPE)(TY 1)	LF	17,310.000				17,310.000	
	512-6025	PORT CTB (MOVE)(SGL SLP)(TY 1)	LF	78,680.000		6,860.000		85,540.000	
	512-6049	PORT CTB (REMOVE)(SGL SLP)(TY 1)	LF	17,310.000				17,310.000	
	530-6005	DRIVEWAYS (ACP)	SY	2,908.000		336.000		3,244.000	
	540-6001	MTL W-BEAM GD FEN (TIM POST)	LF	475.000				475.000	
	540-6006	MTL BEAM GD FEN TRANS (THRIE-BEAM)	EA	4.000				4.000	
	542-6001	REMOVE METAL BEAM GUARD FENCE	LF	475.000				475.000	
	542-6004	RM MTL BM GD FENCE TRANS (THRIE-BEAM)	EA	4.000				4.000	
	544-6001	GUARDRAIL END TREATMENT (INSTALL)	EA	4.000				4.000	
	544-6003	GUARDRAIL END TREATMENT (REMOVE)	EA	4.000				4.000	
	545-6003	CRASH CUSH ATTEN (MOVE & RESET)	EA	80.000		4.000		84.000	
	545-6005	CRASH CUSH ATTEN (REMOVE)	EA	9.000		3.000		12.000	
	545-6019	CRASH CUSH ATTEN (IN STL)(S)(N)(TL3)	EA	12.000				12.000	
	636-6001	ALUMINUM SIGNS (TY A)	SF	85.500		21.000		106.500	
	644-6027	IN SM RD SN SUP&AM TYS80(1)SA(P)	EA	24.000		5.000		29.000	
	644-6030	IN SM RD SN SUP&AM TYS80(1)SA(T)	EA	21.000		5.000		26.000	
	644-6033	IN SM RD SN SUP&AM TYS80(1)SA(U)	EA	6.000		1.000		7.000	
	644-6076	REMOVE SM RD SN SUP&AM	EA	31.000		6.000		37.000	
	658-6060	REMOVE DELIN & OBJECT MARKER ASSMS	EA	44.000		3.000		47.000	
	658-6100	IN STL OM ASSM (OM-2Z)(WFLX)GND(BI)	EA	46.000		2.000		48.000	



Estimate & Quantity Sheet

CONTROLLING PROJECT ID 0482-01-036

DISTRICT Pharr
HIGHWAY SH 285

COUNTY Brooks, Jim Hogg

CONTROL SECTION JOB				0482-01-036		0482-02-035		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00123534		A00204054			
COUNTY				Jim Hogg		Brooks			
HIGHWAY				SH 285		SH 285			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL	EST.	FINAL		
	662-6008	WK ZN PAV MRK NON-REMOV (W)6"(SLD)	LF	103,330.000		7,000.000		110,330.000	
	662-6037	WK ZN PAV MRK NON-REMOV (Y)6"(SLD)	LF	98,320.000		7,000.000		105,320.000	
	662-6050	WK ZN PAV MRK REMOV (REFL) TY II-A-A	EA	6,506.000		350.000		6,856.000	
	662-6067	WK ZN PAV MRK REMOV (W)6"(SLD)	LF	162,355.000		7,000.000		169,355.000	
	662-6071	WK ZN PAV MRK REMOV (W)8"(SLD)	LF	82.000				82.000	
	662-6075	WK ZN PAV MRK REMOV (W)24"(SLD)	LF	140.000				140.000	
	662-6080	WK ZN PAV MRK REMOV (W)(ARROW)	EA	1.000				1.000	
	662-6090	WK ZN PAV MRK REMOV (W)(WORD)	EA	1.000				1.000	
	662-6096	WK ZN PAV MRK REMOV (Y)6"(BRK)	LF	220.000				220.000	
	662-6098	WK ZN PAV MRK REMOV (Y)6"(SLD)	LF	170,664.000		7,000.000		177,664.000	
	662-6109	WK ZN PAV MRK SHT TERM (TAB)TY W	EA	4,642.000		761.000		5,403.000	
	662-6111	WK ZN PAV MRK SHT TERM (TAB)TY Y-2	EA	4,995.000		350.000		5,345.000	
	666-6018	REFL PAV MRK TY I (W)6"(DOT)(100MIL)	LF	559.000		126.000		685.000	
	666-6021	REFL PAV MRK TY I (W)6"(LNDP)(100MIL)	LF	1,272.000		243.000		1,515.000	
	666-6036	REFL PAV MRK TY I (W)8"(SLD)(100MIL)	LF	98.000				98.000	
	666-6048	REFL PAV MRK TY I (W)24"(SLD)(100MIL)	LF	55.000				55.000	
	666-6306	RE PM W/RET REQ TY I (W)6"(BRK)(100MIL)	LF	8,060.000		1,010.000		9,070.000	
	666-6318	RE PM W/RET REQ TY I (Y)6"(BRK)(100MIL)	LF	5,569.000				5,569.000	
	666-6321	RE PM W/RET REQ TY I (Y)6"(SLD)(100MIL)	LF	65,697.000		7,000.000		72,697.000	
	666-6343	REF PROF PAV MRK TY I(W)6"(SLD)(100MIL)	LF	105,848.000		7,000.000		112,848.000	
	668-6077	PREFAB PAV MRK TY C (W) (ARROW)	EA	1.000				1.000	
	668-6083	PREFAB PAV MRK TY C (W) (LNDP ARROW)	EA	10.000		2.000		12.000	
	668-6085	PREFAB PAV MRK TY C (W) (WORD)	EA	1.000				1.000	
	668-6116	PREFAB PAV MRK TY C(EVAC SYM, BLUE/WHT)	EA	1.000		1.000		2.000	
	672-6007	REFL PAV MRKR TY I-C	EA	408.000		50.000		458.000	
	672-6009	REFL PAV MRKR TY II-A-A	EA	1,419.000		96.000		1,515.000	
	672-6017	TRAFFIC BUTTON TY Y	EA	13,180.000		1,400.000		14,580.000	
	672-6018	TRAFFIC BUTTON TY B	EA	8,036.000				8,036.000	
	677-6001	ELIM EXT PAV MRK & MRKS (4")	LF	138,841.000		4,355.000		143,196.000	
	678-6002	PAV SURF PREP FOR MRK (6")	LF	1,350.000				1,350.000	
	3077-6010	SP MIXES SP-B SAC-B PG76-22	TON	41,990.000		3,534.000		45,524.000	
	3077-6033	SP MIXES SP-C SAC-A PG76-22	TON	33,591.000		2,828.000		36,419.000	
	3084-6001	BONDING COURSE	GAL	20,626.000		1,737.000		22,363.000	
	5088-6001	BIRD EXCLUSION METHOD	SF	5,673.000				5,673.000	
	6001-6002	PORTABLE CHANGEABLE MESSAGE SIGN	EA	4.000				4.000	
	6038-6004	MULTIPOLYMER PAV MRK (W)(6")(SLD)	LF	270.000				270.000	
	6038-6017	MULTIPOLYMER PAV MRK (Y)(6")(SLD)	LF	270.000				270.000	



Estimate & Quantity Sheet

CONTROLLING PROJECT ID 0482-01-036

DISTRICT Pharr
HIGHWAY SH 285

COUNTY Brooks, Jim Hogg

CONTROL SECTION JOB				0482-01-036		0482-02-035		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00123534		A00204054			
COUNTY				Jim Hogg		Brooks			
HIGHWAY				SH 285		SH 285			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL	EST.	FINAL		
	6185-6002	TMA (STATIONARY)	DAY	1,140.000				1,140.000	
	6185-6005	TMA (MOBILE OPERATION)	DAY	50.000				50.000	
	18	SAFETY CONTINGENCY: CONTRACTOR FORCE ACCOUNT WORK (PARTICIPATING)	LS	1.000				1.000	
		EROSION CONTROL MAINTENANCE: CONTRACTOR FORCE ACCOUNT WORK (PART)	LS	1.000				1.000	

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

2014 SPECS GENERAL NOTES:

General Requirements and Covenants to ITEMS 1 thru 9:

For all pits or quarries, comply with the “Texas Aggregate Quarry and Pit Safety Act.”

Provide on a weekly basis a list of equipment, including idle equipment, utilized on the project that week.

The 1-800 call services for utility locations do not include TxDOT facilities. Contact the Pharr District Signal Section (956-702-6225) for coordination regarding TxDOT underground lines.

ITEM 2: Instructions to Bidders

Contractor questions on this project are to be addressed to the following individual(s):

Francisco Cantu, P.E., Roma Area Engineer; Francisco.J.Cantu@txdot.gov
Danny Flores, P.E., Transportation Engineer; Danny.Flores@txdot.gov

Contractor questions will be accepted through email, phone, and in person by the above individuals. Questions may also be submitted via the Letting Pre-Bid Q&A web page. This webpage can be accessed from the Notice to Contractors dashboard located at the following Address:

<https://tableau.txdot.gov/views/ProjectInformationDashboard/NoticetoContractors>

All contractor questions will be reviewed by the Engineer. All questions and any corresponding responses that are generated will be posted through the same Letting Pre-Bid Q&A web page.

The Letting Pre-Bid Q&A web page for each project can be accessed by using the dashboard to navigate to the project you are interested in by scrolling or filtering the dashboard using the controls on the left. Hover over the blue hyperlink for the project you want to view the Q&A for and click on the link in the window that pops up.

Information found on TxDOT's FTP server will be considered for informational purposes only. [Index of /pub/txdot-info/Pre-Letting Responses/Pharr District/21-Pharr District \(Construction\) \(state.tx.us\)](https://pub.txdot-info/Pre-Letting%20Responses/Pharr%20District/21-Pharr%20District%20(Construction)/state.tx.us)

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 5: Control of the Work

The responsibility for the construction surveying on this contract will be in accordance with Article 5.9.1., “Method A.”

Prior to contract letting, bidders may obtain a free computerized transfer of files (from the Engineer’s office) that contains the earthwork information. If copies of the actual cross-sections in addition to, or instead of the electronic files are requested, they will be available at the Engineer’s office for borrowing by copying companies for the purpose of making copies for the bidder at the bidder’s expense.

When a precast or cast-in-place concrete element is included in the plans, a precast concrete alternate may be submitted in accordance with “Standard Operating Procedure for Alternate Precast Proposal Submission” found online at <https://www.txdot.gov/business/resources/highway/bridge/bridge-publications.html#design>. Acceptance or denial of an alternate is at the sole discretion of the Engineer. Impacts to the project schedule and any additional costs resulting from the use of alternates are the sole responsibility of the Contractor.

ITEM 6: Control of Materials

To comply with the latest provisions of Build America, Buy America Act (BABA Act) of the Bipartisan Infrastructure Law, the contractor must submit an original of the TxDOT Construction Material Buy America Certification Form for all items classified as construction materials. This form is not required for materials classified as a manufactured product.

Refer to the Buy America Material Classification Sheet for clarification on material categorization.

The Buy America Material Classification Sheet is located at the below link.

<https://www.txdot.gov/business/resources/materials/buy-america-material-classification-sheet.html> for clarification on material categorization.

ITEM 7: Legal Relations and Responsibilities

Roadway or Lane closures during the following key dates and/or special events are prohibited:

- National Holidays
- The day before a National Holiday
- During emergency events such as natural disasters or as directed by the Engineer
- Local Special Event

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 8: Prosecution and Progress

Working days will be computed and charged in accordance with Article 8.3.1.4. Standard Workweek.

Prepare progress schedules using the Critical Path Method (CPM).

ITEM 100: Preparing Right of Way

Preparation of right of way will be done in accordance with the construction phasing shown on the Traffic Control Plans. Performance of this item will not be allowed outside of the project's current construction phase without prior approval by the Engineer.

Removal of all existing vegetation and trees within the ROW will be subsidiary to prep ROW.

ITEM 132: Embankment

Embankment (DENS CONT) shall be Type C with a max. PI of 40. Material used as embankment material in the top two feet below the bottom of Flexible Base shall meet the following requirements based on preliminary tests and such other tests found necessary by the Engineer.

1. The material shall be such as to produce a well-bonded embankment and shall have a minimum PI of 8 and a maximum PI of 30.

It is the Contractor's responsibility to advise the Engineer of the location of the source sufficiently in advance to avoid delay.

ITEM 134: Backfilling Pavement Edges

Areas to be backfilled shall extend approximately 3-ft out from the edges of the proposed overlay. Final slopes shall be uniform and smooth. The 100-foot station payment includes backfilling of both sides.

Backfill Ty A shall not contain particles more than two inches in size and shall have a minimum PI of 10 and a maximum PI of 20.

Any additional backfill material necessary due to pre-existing edge conditions or to replace existing fill removed during blading operations will not be paid for directly. It will be considered subsidiary to this bid Item.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 160: Topsoil

Use topsoil as needed and directed by the Project Engineer for select problem areas. Unless otherwise approved by the Project Engineer, use topsoil from approved sources outside the right of way as per standard specifications. Existing topsoil is to be salvaged and retained for re-use on the project as topsoil.

ITEM 164: Seeding for Erosion Control

During drill seeding operations, application methods shall be in accordance with the method shown in the Standard Specification Book.

SS-1 Tacking Agent shall be a ratio of 2:1, two (Emulsion) to one (water) and applied at a rate of 0.05 gallons per square yard. The SS-1 Tacking Agent required for Drill Seed operations, will not be paid for directly, but will be subsidiary to Item 164 "Drill Seeding." Watering shall not be used with the Drill Seed Method. A biodegradable tacking agent may be used in lieu of the SS-1 tacking agent in accordance with the manufacturer's recommendations when approved by the Engineer.

Cool Season or Warm Season Grasses shall be included as part of Item 164 (See Table 3 and/or Table 4 in the Standard Specification Book or dates and seed type).

Seed mixture shall be as specified under Item 164.

ITEM 166: Fertilizer

Fertilizer rate is based on a rate of 100 Lbs. of Nitrogen per acre. The Nitrogen-Phosphorous Potassium (NPK) ratio shall include a minimum of 5% Phosphorous and 5% Potassium.

Fertilizer shall be homogenized.

ITEM 247: Flexible Base

Flexible Base Type E will be composed of caliche (argillaceous Limestone, calcareous or calcareous clay particles) and may contain stone, conglomerate, gravel, sand, or granular materials when these materials are in situ with the caliche.

Flexible Base (TY E GR 4) caliche shall conform to the following requirements:

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

Table 1: Gradation Requirements for Flexible Base

Retained on Sq. Sieve:	Percent Retained
2"	0
1/2"	20-60
No. 4	40-75
No. 40	70-90
Max. PI	15
Max. Wet Ball PI	15
Wet Ball Mill Max. Amount	50
Min. Comp. Strength PSI	150 at 15 PSI lateral pressure
Triaxial Test	Tex-117-E

The Wet Ball Test (Tex-116-E) shall be run and the Plasticity Index of the material passing the No.40 sieve shall be determined (Wet Ball PI).

Flexible Base (TY E GR 4) caliche shall meet minimum compressive strength specified on Table 1 Gradation Requirements for Flexible Base above.

The percent of density as determined by Compaction Ratio (Tex-113-E) for the new Flexible Base shall be a minimum of 98%.

The Contractor's attention is called to the fact that certain existing and/or proposed structures may be within the limits of the Flexible Base. It shall be the Contractor's responsibility to perform construction operations without damage to these structures.

For water added under Item 247, the sulfate content will not exceed 3000-ppm and the chloride content will not exceed 3000-ppm.

ITEM 251: Reworking Base Courses

Quantities of Flexible Base to be salvaged, shown on the typical sections, are for estimating purposes only. All acceptable base material encountered in existing base is to be salvaged as directed by the Engineer regardless of the quantities involved.

Salvaged base shall be used in the bottom course on any of the proposed roadway and/or turnout sections.

Salvaged base may be used on any of the proposed driveway sections.

All surplus salvage base not used on the project will remain the property of the Contractor, unless otherwise directed by Engineer.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

Proof roll the roadbed in accordance with Item 216, "Proof Rolling." Correct soft spots as directed.

ITEM 275: Cement Treatment (Road-Mixed)

The Contractor's attention is called to the fact that certain existing and/or proposed structures are within the limits of the cement-treated Subgrade. Unless otherwise directed by the Engineer, these structures shall be installed before the final rolling of this Subgrade. It shall be the Contractor's responsibility to perform the proper cement treating operation without damage to these structures.

The percent of density as determined by Tex-120-E for the new and salvage Flexible Base shall be a minimum of 98% for all courses.

Proof roll all constructed cement treated subgrade and bases courses in accordance with Item 216, "Proof Rolling." Correct soft spots as directed. Correction of soft spots in the subgrade or base courses will be at the Contractor's expense.

Contractor is to place an underseal and/or pavement course as indicated on plans within 14 calendar days of initial prime coat application. Otherwise, reapply prime coat as directed by the Engineer. Reapplication of the prime coat will be at the Contractor's expense.

ITEM 3096: Asphalts, Oils, and Emulsions

Temporary ramps/detours and driveways may use Performance Grade Binder 64-22.

ITEM 301: Asphalt Antistripping Agents

Hydrated Lime shall be added as an Antistripping additive between the rates of 1% minimum and 2.0% maximum by weight for Items 292, 3076, 3077, and 3080. If the Hamburg Wheel Test cannot be met within these limits, Liquid Antistripping agents as approved by the Engineer may be used in conjunction with lime for Items 3076, 3077, and 3080.

ITEM 310: Prime Coat

The Contractor shall exercise diligence in the application of asphalt by the use of flagging and rolling procedures to keep from spraying or splattering the traveling public with asphaltic material.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

Do not apply subsequent courses over the initial prime coat no earlier than 12 hours after the prime coat was applied, unless otherwise authorized or directed by the Engineer.

ITEM 3077: Superpave Mixtures

The Contractor shall exercise diligence in the application of "Bonding Course" by the use of flagging and rolling procedures to keep from spraying or splattering the traveling public with asphaltic material.

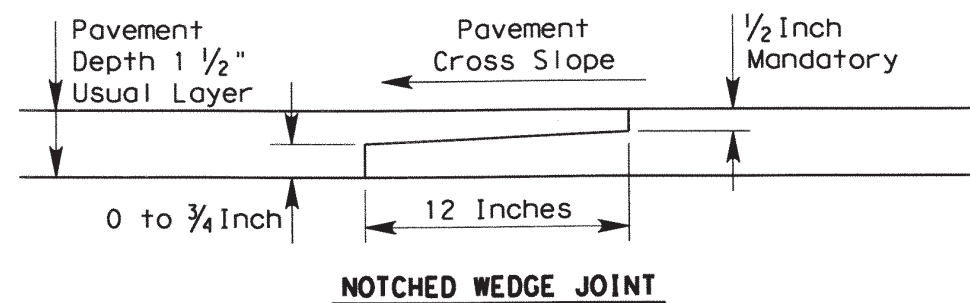
Blading (not to exceed more than 3-ft from the pavement edge) may also be necessary to clean dirt and grass from pavement edges and turnout areas as work under this bid Item. The cost of this blading will not be paid for directly but shall be considered subsidiary to this bid Item.

All surplus RAP from this project will remain the property of the Contractor.

Level-up will be placed before the surface course. An asphaltic concrete spreading and finishing machine and/or motor graders; when approved by the Engineer may be used to place the ACP level-up.

Aggregates used on shoulders and ramps are required to meet SAC requirements.

All unconfined longitudinal joints shall be constructed with a joint maker providing a maximum 1/2-inch vertical edge and a minimum 6:1 edge taper or as approved by the Engineer. The Engineer may waive this requirement when no impacts to the traveling public are foreseen.



The engineer may allow for variances to the dimensions shown.

Public and private driveways need to have a smooth vertical transition between the edge of pavement and the existing driveways. The Contractor is to add a vertical taper if needed which will be subsidiary to Item 3077.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

The use of RAP and RAS (recycled asphalt shingles) will not be allowed as part of the mix design for the final riding surface.

Use a release agent from the Department's MPL to clean and to coat the inside of truck beds for hauling equipment. Hauling equipment shall be cleaned prior to hauling material to job site. Submit a copy of the bill of lading to the Engineer as part of the QCP. Ensure the pavement is free from any spillage of hydraulic oil or diesel from construction equipment. The Department may reject trucks that contain any foreign material and suspend production if the pavement is contaminated by any pollutants mentioned above.

The percentage of RAS used in the total mix shall not exceed 3% when allowed.

SAC B aggregate must have material properties that require 10 or less on the magnesium sulfate soundness test and 20 or less on the Micro-Deval test.

ITEM 3084 – Bonding Course

The minimum application rates are listed in Table BC.

The target shear bond strengths are listed in Table BCS. The informational test cores shall be taken once a shift for first 5 lots of placement or a change to placement method of bonding course, bonding material, or hot mix material. The remaining informational test cores shall be taken once every 3 lots for surface mix. Informational tests are not required for non-surface mix beyond the first 5 lots unless there is a change to placement method of bonding course, bonding material, or hot mix material. Results from these informational tests will not be used for specification compliance.

Table BC

Material	Minimum Application Rate (gal. per square yard)
TRAIL – Emulsified Asphalt	0.06
TRAIL – Hot Asphalt	0.12
Spray Applied Underseal Membrane	0.10

Table BCS (For Informational Tests)

Material	Target Shear Bond Strength (Tex-249-F psi)
SMA – Stone-Matrix Asphalt	60.0
All Other Materials	40.0

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 400: Excavation and Backfill for Structures

If the Contractor elects to cut pavement (existing/detour) for structural work beyond that required by the construction phasing shown in the plans and approved by the Engineer, it shall be restored at his expense and backfilled to its original condition or better in accordance with Item 400.

Unless shown otherwise in the plans, use a 1-ft depth for Item 400 Structural Excavation (Special) for gravel bedding needed below drainage structures with unstable material.

Structural Excavation Special (Gravel):

Use durable natural stone when tested in accordance with Tex-411-A, has weight loss of no more than 18% after 5 cycles of magnesium sulfate solution. Provide gravel conforming to an aggregate Grade No. 1 as shown on Table 4 of Article 421.2.

ITEM 421: Hydraulic Cement Concrete

Provide Sulfate Resistant Concrete for all concrete piling and drilled shafts.

Provide equipment at the batch plant for determining the free moisture and/or absorption of aggregates in accordance with applicable TXDOT Test.

Provide the following items for concrete batch inspection in accordance with specifications outlined in DMS-10101, "Computer Equipment":

- (1) One Desktop Microcomputer or One Laptop Microcomputer
- (2) One Integrated Printer/Scanner/Copier/Fax Unit
- (3) Contractor-Furnished Software
- (4) Hardware

Submit to the Engineer for approval the project locations for all Portland Cement concrete washout areas prior to starting any concrete work.

Fiber Reinforced Concrete is not permitted.

ITEM 432: Riprap

Provide Class "A" concrete minimum for riprap aprons placed around all box culvert and pipe safety end treatments. Provide ¼-inch thick dummy joints at least every 15-ft for riprap aprons placed around box and pipe culverts.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

Do not use fiber reinforced concrete RIPRAP on side slopes equal to or steeper than 6:1 unless approved by the Engineer.

ITEM 462: Concrete Box Culverts and Drains

Provide joints in pre-cast concrete box culverts using any of the methods specified in Item 464, except mortar joints.

Provide pre-cast concrete boxes to expedite traffic handling unless otherwise shown on the plans.

Provide the Area Engineer with the casting schedule of all pre-cast concrete boxes prior to beginning any fabrication.

ITEM 464: Reinforced Concrete Pipe

Use tongue and groove pipe where the RCP extends into the lime treated subgrade. The 4-foot depth restriction for heavy equipment passage over pipe structures is voided. The Contractor will be responsible for any construction damage to these facilities.

Do not use mortar joints.

All reinforced concrete pipe shall include rubber gaskets unless shown otherwise on the plans or directed by the Engineer.

ITEM 466: Headwalls and Wingwalls

Do not use pre-cast headwalls/wingwalls.

ITEM 467: Safety End Treatment

All Type II SET's shall have riprap, Class "A" minimum, aprons as shown on the plans. The Contractor may submit an alternate precast SET design for approval by the Engineer.

ITEM 502: Barricades, Signs, and Traffic Handling

Shadow vehicles equipped with Truck-Mounted Attenuators are required for traffic handling. See notes for Item 6185: Truck Mounted Attenuator/Trailer Attenuator, for additional references pertaining to the TMAs.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

Replace/relocate all regulatory signs removed due to construction operations with the same sign on fixed support(s) immediately upon its removal. First obtain Project Engineer approval before removing any regulatory roadway sign. Required flaggers are to be available to direct traffic during sign intermediate down time.

Relocate any Directional Sign Assemblies removed during construction operations immediately upon their removal.

These signs shall be relocated to a location in accordance with the Latest Version of the "Texas Manual on Uniform Traffic Control Devices". In no case will a sign be removed without a replacement sign and support(s) being readily available and a location established. Removal and relocation of these signs required for traffic control will not be paid for directly but shall be considered subsidiary to Item 502.

From the beginning to the end of the project, all traffic control devices need to be in acceptable condition as per the Texas Quality Guidelines for Work Zone Traffic Control Devices.

The Contractor Force Account "Safety Contingency" that has been established for this project is intended to be utilized for work zone enhancements, to improve the effectiveness of the Traffic Control Plan, that could not be foreseen in the project planning and design stage. These enhancements will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent traffic management reviews on the project. The "Safety Contingency" is not intended to be used in lieu of bid Items established by the contract.

Remove and dispose of all litter, debris, objectionable material, excess materials that accumulate at the base of all traffic control devices as directed by the Engineer.

ITEM 504: Field Office and Laboratory

Furnish (1) Field Office (Type C).

The Contractor will furnish a Type D Structure (Asphalt Mix Laboratory) modified by the following.

Laboratory room:

The other room of this building will be used as a laboratory and will include access to a bathroom facility from the interior. The laboratory and bathroom facility will have the walls, ceiling and floor insulated such that the air temperature can always be maintained at 76 degrees Fahrenheit.

Furnish for the Department's use in the asphalt laboratory one (1) desktop computer.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 506: Temporary Erosion, Sedimentation, and Environmental Controls

Before starting each phase of construction, review with the Engineer the SW3P used for temporary erosion control as outlined on the plans. Before construction, place the temporary erosion and sedimentation control features as shown on the SW3P. Location of Construction Exits are to be approved by the Engineer. After completing earthwork operations, restore and reseed the disturbed areas in accordance with the Department's specifications for permanent or temporary erosion control. Before starting grading operations and during the project duration, place the temporary or permanent erosion control measures to prevent sediment from leaving the right of way.

The Contractor Force Account "Erosion Control Maintenance" that has been established for this project is intended to be utilized for work zone Best Management Practice (BMP) maintenance, to improve the effectiveness of the Environmental Controls that may need maintenance attention and/or require replacement while the project is still under the construction stage. These procedures will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent BMP management reviews on the project. The "Erosion Control Maintenance" is not intended to be used in lieu of bid Items established by the contract.

ITEM 508: Constructing Detours

Flexible Base, prime coat, and Asphaltic Concrete Pavement used for detours shall meet the requirements of Items 247, 310 and 3076 respectively, except for measurement and payment.

ITEM 512: Portable Traffic Barrier

During the various construction phases, provide drainage slots in every temporary concrete traffic barrier used for traffic control in order to handle temporary drainage. Provide any additional drainage measures needed as directed by the Engineer.

ITEM 530: Intersections, Driveways, and Turnouts

Prime coat shall meet the requirements of Item 310.

Public and private driveways need to have a smooth vertical transition tie-in between the proposed driveway and the existing driveway. The Contractor is to add a vertical taper if needed which will be subsidiary to Item 530.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 540: Metal Beam Guard Fence

The optional terminal anchor post with the terminal connector will be required as shown on the Metal Beam Guard Fence Standard.

Galvanize the rail elements supplied for this project using a Type II Zinc Coating.

ITEM 542: Removing Metal Beam Guard Fence

Dispose all metal beam guard fence materials unless shown otherwise in the plans.

ITEM 544: Guardrail End Treatments

Label "end treatment type" on backside of unit at time of installation.

ITEM 585: Ride Quality for Pavement Surfaces

Use Surface Test Type "B" for service roads and ramps.

Quality control results shall be submitted to TxDOT the next working day after each day's paving.

Pavement areas with public turnout intersections that carry major traffic volumes will not be subjected to inertial profiler testing. These areas shall be evaluated using the 10-ft. straightedge.

Diamond grinding shall be used to remove localized roughness.

Use Surface Test Type B pay adjustment schedule 1 to evaluate ride quality of the travel lanes in accordance with Item 585, "Ride Quality for Pavement Surfaces." This includes ramps and service road travel lanes.

ITEMS 636: Signs

Complete sign blanks and panels shall be handled and stored at the job site in such a manner that corners, edges and faces are not damaged. Finished sign blanks shall be stored in either a weatherproof warehouse or outside and off the ground in a vertical position. All paper, cardboard and chemically treated separators and packaging shall be removed prior to outside storage.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEM 644: Small Roadside Sign Assemblies

All signs shall be installed as shown in the plans and in accordance with the current edition of the "Texas Manual on Uniform Traffic Control Devices" and the "Sign Crew Field Book" (SCFB).

All signs shall be erected according to the locations shown on the signing layout sheets except that a sign may be shifted in order to secure a more desirable location. All sign locations will be staked as shown in the plans and as approved. It is the intent of the plans to erect all roadside traffic signs with the sign edge a minimum of 6 feet from the edge of the shoulder, or if none, 12 feet from the edge of the travel lane. In curb and gutter sections, the sign edge shall be a minimum of 2 feet from the face of the curb.

For this project, aluminum type sign blanks as provided for under Item 636 will be required for all proposed signing installed under Item 644. Aluminum sign blanks less than 7.5 square feet shall be 0.08-inch-thick, sign blanks 7.5 to 15 square feet shall be 0.100-inch-thick and sign blanks greater than 15 square feet shall be 0.125 inch thick.

All excess excavation shall be spread uniformly inside the right of way as directed and shall be included in the price of these Items.

Sign types which design details are not shown on the plans shall conform with the latest edition of the Department's "Standard Highway Sign Design for Texas" Manual.

Signs shown to be removed shall include the complete sign installation and separate the sign post at the concrete foundation. The concrete foundation shall be disposed in accordance with this bid Item. Except for concrete foundations, all removed sign panels, sign posts, and hardware shall remain then property of the Department. All removed sign installations shall be completely disassembled. All salvageable sections of sign panels shall be recycled by TxDOT. The removed sign material will be required to be hauled to the maintenance yard closest to the project. No signs shall be removed without prior approval.

ITEM 658: Delineator and Object Marker Assemblies

Delineator assemblies shall be installed 8 feet from the edge of the shoulder unless restricted by some obstruction, in which case, the delineator assembly shall be placed between 2 and 8 feet from the edge of the shoulder.

Bi-directional object markers shall be in accordance with the D&OM standard sheets. The Contractor is directed to the standards when instructed where and how to install the object markers.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

ITEMS 662 and 666: Work Zone Pavement Markings and Retroreflectorized Pavement Markings

All permanent pavement markings and work zone pavement markings for this project under these Items shall be 0.100 inches (100 mil) thick thermoplastic.

Any permanent pavement markings or non-removal work zone pavement markings lacking reflectivity in accordance with the requirements of Tex 828-B, or that fail to meet minimum retro reflectivity requirements for longitudinal pavement markings when required, will be addressed per the requirements of the specification. The roadway will be re-stripped at no additional compensation.

Pavement surface preparation for markings and markers will not be paid for directly but shall be considered subsidiary to Item 666.

Prior to any striping operations, an on-site coordination meeting between all the parties involved will be required to review striping details and requirements to ensure quality work.

The beads used on this project shall meet the requirements of Departmental Materials Specification DMS-8290, Glass Traffic Beads Texas Type II & III. Use a 50% Type II/ 50% Type III mix utilizing a double drop system with Type III beads dropped first.

ITEM 677: Eliminating Existing Pavement Markings and Markers

Asphalt and aggregate types and grades shall be as approved in writing when a surface treatment is used to eliminate existing pavement markings.

ITEM 5088: Bird Exclusion Methods

Contractor's attention is directed to the plan's EPIC sheets, Bird Exclusion Detail standard sheets and shall refer to the Migratory Bird Treaty Act requirements. Also, refer to the TPWD BMPSs sheets for specific adherence to the environmental requirements of the Best Management Practices.

ITEM 6185: Truck Mounted Attenuator/Trailer Attenuator

In addition to the shadow vehicles with truck mounted attenuator (TMA) that are specified as being required on the traffic control plan for the project, provide 2 additional shadow vehicle(s) with TMA as per TCP (2-1) -18 as detailed on General Note 5 of this standard sheet; or as per TCP (2-3) -23 as detailed on General Note 8 of this standard sheet.

Project Number:

County: Jim Hogg, Etc.

Control: 0482-01-036, Etc.

Highway: SH 285

Therefore, 3 total shadow vehicles with TMA will be required on this project for the type of work as shown on the plans. The Contractor will be responsible for determining if one or more of his construction operations will be ongoing at the same time and thus determine the total number of TMAs needed for the project.

DATE: 3/15/2024 9:49:03 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
96+00.0000 R	1	76.3	0.0	0	1	24.2	0.0	0	0
97+00.0000 R	1	51.0	235.6	235.6	1	19.0	80.1	80.1	155.6
98+00.0000 R	1	72.0	227.8	227.8	1	21.0	74.1	74.1	309.3
99+00.0000 R	1	27.0	183.3	183.3	1	27.0	88.9	88.9	403.7
100+00.0000 R	1	83.2	204.1	204.1	1	22.0	90.7	90.7	517.1
101+00.0000 R	1	106.2	350.7	350.7	1	22.0	81.5	81.5	786.3
102+00.0000 R	1	103.0	387.3	387.3	1	25.0	87.0	87.0	1086.6
103+00.0000 R	1	118.0	409.3	409.3	1	26.0	94.4	94.4	1401.4
104+00.0000 R	1	108.0	418.5	418.5	1	23.0	90.7	90.7	1729.2
105+00.0000 R	1	108.0	400.0	400.0	1	26.0	90.7	90.7	2038.4
106+00.0000 R	1	104.0	392.6	392.6	1	23.0	90.7	90.7	2340.3
107+00.0000 R	1	110.0	396.3	396.3	1	10.0	61.1	61.1	2675.5
108+00.0000 R	1	109.0	405.6	405.6	1	13.0	42.6	42.6	3038.4
109+00.0000 R	1	101.0	388.9	388.9	1	12.0	46.3	46.3	3381.0
110+00.0000 R	1	83.0	340.7	340.7	1	12.0	44.4	44.4	3677.3
111+00.0000 R	1	45.0	237.0	237.0	1	29.0	75.9	75.9	3838.4
112+00.0000 R	1	50.0	175.9	175.9	1	16.0	83.3	83.3	3931.0
113+00.0000 R	1	41.0	168.5	168.5	1	19.0	64.8	64.8	4034.7
114+00.0000 R	1	49.0	166.7	166.7	1	27.0	85.2	85.2	4116.2
115+00.0000 R	1	33.0	151.9	151.9	1	35.0	114.8	114.8	4153.3
116+00.0000 R	1	65.0	181.5	181.5	1	29.0	118.5	118.5	4216.2
117+00.0000 R	1	58.0	227.8	227.8	1	33.0	114.8	114.8	4329.2
118+00.0000 R	1	47.0	194.4	194.4	1	32.0	120.4	120.4	4403.3
119+00.0000 R	1	47.0	174.1	174.1	1	36.0	125.9	125.9	4451.4
120+00.0000 R	1	61.0	200.0	200.0	1	30.0	122.2	122.2	4529.2
121+00.0000 R	1	25.0	159.3	159.3	1	37.0	124.1	124.1	4564.4
122+00.0000 R	1	63.0	163.0	163.0	1	26.0	116.7	116.7	4610.7
123+00.0000 R	1	66.0	238.9	238.9	1	20.0	85.2	85.2	4764.4
124+00.0000 R	1	57.0	227.8	227.8	1	25.0	83.3	83.3	4908.8
125+00.0000 R	1	80.0	253.7	253.7	1	17.0	77.8	77.8	5084.7
126+00.0000 R	1	101.0	335.2	335.2	1	15.0	59.3	59.3	5360.7
127+00.0000 R	1	110.2	391.2	391.2	1	16.7	58.7	58.7	5693.1
128+00.0000 R	1	124.1	434.0	434.0	1	22.0	71.7	71.7	6055.4
129+00.0000 R	1	176.8	557.3	557.3	1	16.3	71.0	71.0	6541.7
130+00.0000 R	1	174.0	649.7	649.7	1	18.0	63.5	63.5	7127.9
131+00.0000 R	1	164.0	625.9	625.9	1	15.0	61.1	61.1	7692.7
132+00.0000 R	1	147.0	575.9	575.9	1	22.0	68.5	68.5	8200.1
133+00.0000 R	1	131.0	514.8	514.8	1	22.0	81.5	81.5	8633.4
134+00.0000 R	1	125.0	474.1	474.1	1	30.0	96.3	96.3	9011.2
135+00.0000 R	1	136.0	483.3	483.3	1	22.0	96.3	96.3	9398.3
136+00.0000 R	1	158.0	544.4	544.4	1	18.0	74.1	74.1	9868.6
137+00.0000 R	1	169.0	605.6	605.6	1	22.0	74.1	74.1	10400.1
138+00.0000 R	1	141.0	574.1	574.1	1	27.0	90.7	90.7	10883.4
139+00.0000 R	1	142.0	524.1	524.1	1	25.0	96.3	96.3	11311.2
140+00.0000 R	1	153.0	546.3	546.3	1	23.0	88.9	88.9	11768.6
141+00.0000 R	1	175.0	607.4	607.4	1	16.0	72.2	72.2	12303.8
142+00.0000 R	1	203.0	700.0	700.0	1	19.0	64.8	64.8	12939.0
143+00.0000 R	1	52.6	473.4	473.4	1	84.0	190.7	190.7	13221.7
144+00.0000 R	1	182.0	434.5	434.5	1	24.0	199.9	199.9	13456.2
145+00.0000 R	1	152.0	618.5	618.5	1	27.0	94.4	94.4	13980.3
146+00.0000 R	1	80.0	429.6	429.6	1	33.0	111.1	111.1	14298.8
147+00.0000 R	1	78.0	292.6	292.6	1	34.0	124.1	124.1	14467.3
148+00.0000 R	1	88.0	307.4	307.4	1	30.0	118.5	118.5	14656.2
149+00.0000 R	1	94.0	337.0	337.0	1	33.0	116.7	116.7	14876.6
150+00.0000 R	1	84.0	329.6	329.6	1	42.0	138.9	138.9	15067.3
151+00.0000 R	1	82.0	307.4	307.4	1	44.0	159.3	159.3	15215.5
152+00.0000 R	1	95.0	327.8	327.8	1	45.0	164.8	164.8	15378.5
153+00.0000 R	1	94.0	350.0	350.0	1	50.0	175.9	175.9	15552.5
154+00.0000 R	1	87.0	335.2	335.2	1	49.0	183.3	183.3	15704.4
155+00.0000 R	1	70.0	290.7	290.7	1	51.0	185.2	185.2	15809.9
156+00.0000 R	1	57.0	235.2	235.2	1	50.0	187.0	187.0	15858.1
157+00.0000 R	1	55.0	207.4	207.4	1	48.0	181.5	181.5	15884.0
158+00.0000 R	1	43.0	181.5	181.5	1	50.0	181.5	181.5	15884.0
159+00.0000 R	1	44.0	161.1	161.1	1	49.0	183.3	183.3	15861.8
160+00.0000 R	1	30.0	137.0	137.0	1	53.0	188.9	188.9	15809.9
161+00.0000 R	1	5.0	64.8	64.8	1	58.0	205.6	205.6	15669.2
162+00.0000 R	1	19.0	44.4	44.4	1	51.0	201.9	201.9	15511.8
163+00.0000 R	1	42.0	113.0	113.0	1	38.0	164.8	164.8	15459.9
164+00.0000 R	1	40.0	151.9	151.9	1	44.0	151.9	151.9	15459.9

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 1 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		30

DATE: 3/15/2024 9:49:35 AM
 FILE: \\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
165+00.0000	R	1	53.0	172.2	1	34.0	144.4	144.4	15487.7
166+00.0000	R	1	44.0	179.6	1	35.0	127.8	127.8	15539.6
167+00.0000	R	1	49.0	172.2	1	32.0	124.1	124.1	15587.7
168+00.0000	R	1	51.0	185.2	1	27.0	109.3	109.3	15663.6
169+00.0000	R	1	67.0	218.5	1	22.0	90.7	90.7	15791.4
170+00.0000	R	1	71.0	255.6	1	24.0	85.2	85.2	15961.8
171+00.0000	R	1	76.0	272.2	1	27.0	94.4	94.4	16139.6
172+00.0000	R	1	87.0	301.9	1	25.0	96.3	96.3	16345.1
173+00.0000	R	1	113.0	370.4	1	19.0	81.5	81.5	16634.0
174+00.0000	R	1	130.0	450.0	1	18.0	68.5	68.5	17015.5
175+00.0000	R	1	144.0	507.4	1	18.0	66.7	66.7	17456.2
176+00.0000	R	1	136.0	518.5	1	18.0	66.7	66.7	17908.1
177+00.0000	R	1	127.0	487.0	1	31.0	90.7	90.7	18304.4
178+00.0000	R	1	130.0	475.9	1	36.0	124.1	124.1	18656.2
179+00.0000	R	1	123.0	468.5	1	37.0	135.2	135.2	18989.6
180+00.0000	R	1	129.0	466.7	1	35.0	133.3	133.3	19322.9
181+00.0000	R	1	143.0	503.7	1	30.0	120.4	120.4	19706.2
182+00.0000	R	1	141.0	525.9	1	21.0	94.4	94.4	20137.7
183+00.0000	R	1	139.0	518.5	1	22.0	79.6	79.6	20576.6
184+00.0000	R	1	147.0	529.6	1	27.0	90.7	90.7	21015.5
185+00.0000	R	1	149.0	548.1	1	28.0	101.9	101.9	21461.8
186+00.0000	R	1	142.0	538.9	1	25.0	98.1	98.1	21902.5
187+00.0000	R	1	151.0	542.6	1	20.0	83.3	83.3	22361.8
188+00.0000	R	1	154.0	564.8	1	21.0	75.9	75.9	22850.7
189+00.0000	R	1	147.0	557.4	1	20.0	75.9	75.9	23332.2
190+00.0000	R	1	147.0	544.4	1	21.0	75.9	75.9	23800.7
191+00.0000	R	1	155.0	559.3	1	18.0	72.2	72.2	24287.7
192+00.0000	R	1	151.0	566.7	1	18.0	66.7	66.7	24787.7
193+00.0000	R	1	144.0	546.3	1	19.0	68.5	68.5	25265.5
194+00.0000	R	1	137.0	520.4	1	22.0	75.9	75.9	25709.9
195+00.0000	R	1	121.0	477.8	1	18.0	74.1	74.1	26113.6
196+00.0000	R	1	99.0	407.4	1	22.0	74.1	74.1	26447.0
197+00.0000	R	1	159.0	477.8	1	15.0	68.5	68.5	26856.2
198+00.0000	R	1	4.0	301.9	1	61.8	142.2	142.2	27015.9
199+00.0000	R	1	7.0	20.4	1	60.1	225.7	225.7	26810.6
202+00.0000	R	1	4.2	20.7	1	56.0	215.1	215.1	26616.2
203+00.0000	R	1	42.9	87.3	1	51.7	199.5	199.5	26504.0
204+00.0000	R	1	126.0	312.8	1	21.0	134.7	134.7	26682.1
205+00.0000	R	1	142.0	496.3	1	17.0	70.4	70.4	27108.1
206+00.0000	R	1	146.0	533.3	1	14.0	57.4	57.4	27584.0
207+00.0000	R	1	145.0	538.9	1	18.0	59.3	59.3	28063.6
208+00.0000	R	1	144.0	535.2	1	20.0	70.4	70.4	28528.4
209+00.0000	R	1	154.0	551.9	1	19.0	72.2	72.2	29008.1
210+00.0000	R	1	160.0	581.5	1	14.0	61.1	61.1	29528.4
211+00.0000	R	1	183.0	635.2	1	18.0	59.3	59.3	30104.3
212+00.0000	R	1	175.0	663.0	1	21.0	72.2	72.2	30695.1
213+00.0000	R	1	166.0	631.5	1	26.0	87.0	87.0	31239.5
214+00.0000	R	1	159.0	601.9	1	27.0	98.1	98.1	31743.2
215+00.0000	R	1	120.0	516.7	1	27.0	100.0	100.0	32159.9
216+00.0000	R	1	86.0	381.5	1	29.0	103.7	103.7	32437.7
217+00.0000	R	1	71.0	290.7	1	29.0	107.4	107.4	32621.0
218+00.0000	R	1	64.0	250.0	1	28.0	105.6	105.6	32765.5
219+00.0000	R	1	41.0	194.4	1	49.0	142.6	142.6	32817.3
220+00.0000	R	1	29.0	129.6	1	93.0	263.0	263.0	32684.0
221+00.0000	R	1	38.0	124.1	1	66.0	294.4	294.4	32513.6
222+00.0000	R	1	38.0	140.7	1	72.0	255.6	255.6	32398.8
223+00.0000	R	1	44.0	151.9	1	67.0	257.4	257.4	32293.2
224+00.0000	R	1	47.0	168.5	1	66.0	246.3	246.3	32215.5
225+00.0000	R	1	39.0	159.3	1	55.0	224.1	224.1	32150.6
226+00.0000	R	1	37.0	140.7	1	37.0	170.4	170.4	32121.0
227+00.0000	R	1	47.0	155.6	1	28.0	120.4	120.4	32156.2
228+00.0000	R	1	41.0	163.0	1	33.0	113.0	113.0	32206.2
229+00.0000	R	1	41.0	151.9	1	35.0	125.9	125.9	32232.1
230+00.0000	R	1	42.0	153.7	1	30.0	120.4	120.4	32265.5
231+00.0000	R	1	40.0	151.9	1	31.0	113.0	113.0	32304.3

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 2 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		31

DATE: 3/15/2024 9:50:06 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION		CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
232+00.0000	R	1	36.0	140.7	140.7	1	33.0	118.5	118.5	32326.6
233+00.0000	R	1	44.0	148.1	148.1	1	30.0	116.7	116.7	32358.1
234+00.0000	R	1	40.0	155.6	155.6	1	35.0	120.4	120.4	32393.2
235+00.0000	R	1	41.0	150.0	150.0	1	36.0	131.5	131.5	32411.8
236+00.0000	R	1	43.0	155.6	155.6	1	39.0	138.9	138.9	32428.4
237+00.0000	R	1	45.0	163.0	163.0	1	42.0	150.0	150.0	32441.4
238+00.0000	R	1	43.0	163.0	163.0	1	35.0	142.6	142.6	32461.8
239+00.0000	R	1	49.0	170.4	170.4	1	36.0	131.5	131.5	32500.6
240+00.0000	R	1	62.0	205.6	205.6	1	27.0	116.7	116.7	32589.5
241+00.0000	R	1	72.0	248.1	248.1	1	26.0	98.1	98.1	32739.5
242+00.0000	R	1	113.0	342.6	342.6	1	13.0	72.2	72.2	33009.9
243+00.0000	R	1	109.0	411.1	411.1	1	17.0	55.6	55.6	33365.5
244+00.0000	R	1	90.0	368.5	368.5	1	18.0	64.8	64.8	33669.2
245+00.0000	R	1	96.0	344.4	344.4	1	23.0	75.9	75.9	33937.7
246+00.0000	R	1	108.0	377.8	377.8	1	21.0	81.5	81.5	34234.0
247+00.0000	R	1	118.0	418.5	418.5	1	22.0	79.6	79.6	34572.9
248+00.0000	R	1	116.0	433.3	433.3	1	19.0	75.9	75.9	34930.3
249+00.0000	R	1	117.0	431.5	431.5	1	27.0	85.2	85.2	35276.6
250+00.0000	R	1	121.0	440.7	440.7	1	24.0	94.4	94.4	35622.9
251+00.0000	R	1	133.0	470.4	470.4	1	19.0	79.6	79.6	36013.6
252+00.0000	R	1	140.0	505.6	505.6	1	19.0	70.4	70.4	36448.8
253+00.0000	R	1	130.0	500.0	500.0	1	24.0	79.6	79.6	36869.2
254+00.0000	R	1	121.0	464.8	464.8	1	24.0	88.9	88.9	37245.1
255+00.0000	R	1	111.0	429.6	429.6	1	29.0	98.1	98.1	37576.6
256+00.0000	R	1	88.0	368.5	368.5	1	30.0	109.3	109.3	37835.8
257+00.0000	R	1	70.0	292.6	292.6	1	34.0	118.5	118.5	38009.9
258+00.0000	R	1	57.0	235.2	235.2	1	42.0	140.7	140.7	38104.3
259+00.0000	R	1	48.0	194.4	194.4	1	41.0	153.7	153.7	38145.1
260+00.0000	R	1	63.0	205.6	205.6	1	34.0	138.9	138.9	38211.8
261+00.0000	R	1	79.0	263.0	263.0	1	33.0	124.1	124.1	38350.6
262+00.0000	R	1	104.0	338.9	338.9	1	28.0	113.0	113.0	38576.6
263+00.0000	R	1	124.0	422.2	422.2	1	24.0	96.3	96.3	38902.5
264+00.0000	R	1	142.0	492.6	492.6	1	18.0	77.8	77.8	39317.3
265+00.0000	R	1	161.0	561.1	561.1	1	19.0	68.5	68.5	39809.9
266+00.0000	R	1	171.0	614.8	614.8	1	12.0	57.4	57.4	40367.3
267+00.0000	R	1	182.0	653.7	653.7	1	11.0	42.6	42.6	40978.4
268+00.0000	R	1	165.0	642.6	642.6	1	15.0	48.1	48.1	41572.9
269+00.0000	R	1	155.0	592.6	592.6	1	14.0	53.7	53.7	42111.8
270+00.0000	R	1	173.0	607.4	607.4	1	9.0	42.6	42.6	42676.6
271+00.0000	R	1	179.0	651.9	651.9	1	11.0	37.0	37.0	43291.4
272+00.0000	R	1	171.0	648.1	648.1	1	16.0	50.0	50.0	43889.5
273+00.0000	R	1	157.0	607.4	607.4	1	18.0	63.0	63.0	44434.0
274+00.0000	R	1	133.0	537.0	537.0	1	15.0	61.1	61.1	44909.9
275+00.0000	R	1	137.0	500.0	500.0	1	25.0	74.1	74.1	45335.8
276+00.0000	R	1	154.0	538.9	538.9	1	26.0	94.4	94.4	45780.3
277+00.0000	R	1	161.0	583.3	583.3	1	30.0	103.7	103.7	46259.9
278+00.0000	R	1	169.0	611.1	611.1	1	26.0	103.7	103.7	46767.3
279+00.0000	R	1	172.0	631.5	631.5	1	25.0	94.4	94.4	47304.3
280+00.0000	R	1	168.0	629.6	629.6	1	18.0	79.6	79.6	47854.3
281+00.0000	R	1	148.0	585.2	585.2	1	25.0	79.6	79.6	48359.9
282+00.0000	R	1	137.0	527.8	527.8	1	24.0	90.7	90.7	48796.9
283+00.0000	R	1	116.0	468.5	468.5	1	29.0	98.1	98.1	49167.3
284+00.0000	R	1	97.0	394.4	394.4	1	31.0	111.1	111.1	49450.6
285+00.0000	R	1	92.2	350.4	350.4	1	29.9	112.8	112.8	49688.3
286+00.0000	R	1	103.2	362.0	362.0	1	27.2	105.8	105.8	49944.4
287+00.0000	R	1	130.0	431.9	431.9	1	26.0	98.5	98.5	50277.7
288+00.0000	R	1	152.0	522.2	522.2	1	25.0	94.4	94.4	50705.5
289+00.0000	R	1	161.0	579.6	579.6	1	25.0	92.6	92.6	51192.6
290+00.0000	R	1	163.0	600.0	600.0	1	25.0	92.6	92.6	51700.0
291+00.0000	R	1	167.0	611.1	611.1	1	27.0	96.3	96.3	52214.8
292+00.0000	R	1	168.0	620.4	620.4	1	25.0	96.3	96.3	52738.9
293+00.0000	R	1	166.0	618.5	618.5	1	29.0	100.0	100.0	53257.4
294+00.0000	R	1	145.0	575.9	575.9	1	30.0	109.3	109.3	53724.0
295+00.0000	R	1	128.0	505.6	505.6	1	25.0	101.9	101.9	54127.7
296+00.0000	R	1	128.0	474.1	474.1	1	19.0	81.5	81.5	54520.3
297+00.0000	R	1	153.0	520.4	520.4	1	16.0	64.8	64.8	54975.9

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 3 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		32

DATE: 3/15/2024 9:50:43 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
298+00.0000	R	148.0	557.4	557.4	1	23.0	72.2	72.2	55461.1
299+00.0000	R	131.0	516.7	516.7	1	25.0	88.9	88.9	55888.9
300+00.0000	R	106.0	438.9	438.9	1	23.0	88.9	88.9	56238.9
301+00.0000	R	90.0	363.0	363.0	1	21.0	81.5	81.5	56520.3
302+00.0000	R	73.0	301.9	301.9	1	26.0	87.0	87.0	56735.2
303+00.0000	R	61.0	248.1	248.1	1	29.0	101.9	101.9	56881.4
304+00.0000	R	65.0	233.3	233.3	1	26.0	101.9	101.9	57012.9
305+00.0000	R	59.0	229.6	229.6	1	17.0	79.6	79.6	57162.9
306+00.0000	R	71.0	240.7	240.7	1	20.0	68.5	68.5	57335.2
307+00.0000	R	64.0	250.0	250.0	1	20.0	74.1	74.1	57511.1
308+00.0000	R	71.0	250.0	250.0	1	21.0	75.9	75.9	57685.2
309+00.0000	R	63.0	248.1	248.1	1	19.0	74.1	74.1	57859.2
310+00.0000	R	72.0	250.0	250.0	1	14.0	61.1	61.1	58048.1
311+00.0000	R	38.0	203.7	203.7	1	39.0	98.1	98.1	58153.7
312+00.0000	R	54.0	170.4	170.4	1	25.0	118.5	118.5	58205.5
313+00.0000	R	85.0	257.4	257.4	1	30.0	101.9	101.9	58361.1
314+00.0000	R	81.0	307.4	307.4	1	26.0	103.7	103.7	58564.8
315+00.0000	R	114.0	361.1	361.1	1	18.0	81.5	81.5	58844.4
316+00.0000	R	96.0	388.9	388.9	1	25.0	79.6	79.6	59153.7
317+00.0000	R	84.0	333.3	333.3	1	18.0	79.6	79.6	59407.4
318+00.0000	R	54.0	255.6	255.6	1	18.0	66.7	66.7	59596.3
319+00.0000	R	43.0	179.6	179.6	1	14.0	59.3	59.3	59716.6
320+00.0000	R	18.0	113.0	113.0	1	33.0	87.0	87.0	59742.6
321+00.0000	R	13.0	57.4	57.4	1	37.0	129.6	129.6	59670.3
322+00.0000	R	16.0	53.7	53.7	1	35.9	134.9	134.9	59589.1
323+00.0000	R	38.4	100.7	100.7	1	33.8	129.0	129.0	59560.8
324+00.0000	R	32.0	130.4	130.4	1	51.0	157.1	157.1	59534.1
325+00.0000	R	47.0	146.3	146.3	1	55.0	196.3	196.3	59484.1
326+00.0000	R	68.0	213.0	213.0	1	61.0	214.8	214.8	59482.2
327+00.0000	R	47.0	213.0	213.0	1	62.0	227.8	227.8	59467.4
328+00.0000	R	53.0	185.2	185.2	1	59.0	224.1	224.1	59428.5
329+00.0000	R	60.0	209.3	209.3	1	57.0	214.8	214.8	59423.0
330+00.0000	R	69.0	238.9	238.9	1	48.0	194.4	194.4	59467.4
331+00.0000	R	69.0	255.6	255.6	1	49.0	179.6	179.6	59543.4
332+00.0000	R	86.0	287.0	287.0	1	36.0	157.4	157.4	59673.0
333+00.0000	R	114.0	370.4	370.4	1	23.0	109.3	109.3	59934.1
334+00.0000	R	154.0	496.3	496.3	1	16.0	72.2	72.2	60358.2
335+00.0000	R	144.0	551.9	551.9	1	9.0	46.3	46.3	60863.7
336+00.0000	R	151.0	546.3	546.3	1	7.0	29.6	29.6	61380.4
337+00.0000	R	191.0	633.3	633.3	1	9.0	29.6	29.6	61984.1
338+00.0000	R	173.0	674.1	674.1	1	7.0	29.6	29.6	62628.5
339+00.0000	R	175.0	644.4	644.4	1	8.0	27.8	27.8	63245.2
340+00.0000	R	179.0	655.6	655.6	1	9.0	31.5	31.5	63869.3
341+00.0000	R	179.0	663.0	663.0	1	14.0	42.6	42.6	64489.7
342+00.0000	R	183.0	670.4	670.4	1	10.0	44.4	44.4	65115.6
343+00.0000	R	186.0	683.3	683.3	1	13.0	42.6	42.6	65756.3
344+00.0000	R	198.0	711.1	711.1	1	14.0	50.0	50.0	66417.4
345+00.0000	R	201.0	738.9	738.9	1	21.0	64.8	64.8	67091.5
346+00.0000	R	190.0	724.1	724.1	1	34.0	101.9	101.9	67713.7
347+00.0000	R	173.0	672.2	672.2	1	42.0	140.7	140.7	68245.2
348+00.0000	R	168.0	631.5	631.5	1	40.0	151.9	151.9	68724.8
349+00.0000	R	173.0	631.5	631.5	1	40.0	148.1	148.1	69208.2
350+00.0000	R	171.0	637.0	637.0	1	41.0	150.0	150.0	69695.2
351+00.0000	R	180.0	650.0	650.0	1	30.0	131.5	131.5	70213.7
352+00.0000	R	186.0	677.8	677.8	1	26.0	103.7	103.7	70787.8
353+00.0000	R	172.0	663.0	663.0	1	20.0	85.2	85.2	71365.6
354+00.0000	R	162.0	618.5	618.5	1	18.0	70.4	70.4	71913.7
355+00.0000	R	130.0	540.7	540.7	1	23.0	75.9	75.9	72378.5
356+00.0000	R	78.0	385.2	385.2	1	36.0	109.3	109.3	72654.5
357+00.0000	R	54.0	244.4	244.4	1	40.0	140.7	140.7	72758.2
358+00.0000	R	53.0	198.1	198.1	1	40.0	148.1	148.1	72808.2
359+00.0000	R	56.0	201.9	201.9	1	39.0	146.3	146.3	72863.7
360+00.0000	R	40.0	177.8	177.8	1	71.0	203.7	203.7	72837.8
361+00.0000	R	54.0	174.1	174.1	1	39.0	203.7	203.7	72808.2
362+00.0000	R	53.0	198.1	198.1	1	34.0	135.2	135.2	72871.1
363+00.0000	R	49.0	188.9	188.9	1	32.0	122.2	122.2	72937.8

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 4 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	33

DATE: 3/15/2024 9:51:10 AM
 FILE: \\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
364+00.0000	R	1	36.0	157.4	1	48.0	148.1	148.1	72947.1
365+00.0000	R	1	27.0	116.7	1	44.0	170.4	170.4	72893.4
366+00.0000	R	1	17.0	81.5	1	66.0	203.7	203.7	72771.1
367+00.0000	R	1	20.0	68.5	1	63.0	238.9	238.9	72600.8
368+00.0000	R	1	36.0	103.7	1	45.0	200.0	200.0	72504.5
369+00.0000	R	1	56.0	170.4	1	33.0	144.4	144.4	72530.4
370+00.0000	R	1	50.0	196.3	1	36.0	127.8	127.8	72598.9
371+00.0000	R	1	48.0	181.5	1	43.0	146.3	146.3	72634.1
372+00.0000	R	1	51.0	183.3	1	49.0	170.4	170.4	72647.1
373+00.0000	R	1	43.0	174.1	1	54.0	190.7	190.7	72630.4
374+00.0000	R	1	32.0	138.9	1	61.0	213.0	213.0	72556.3
375+00.0000	R	1	32.3	119.0	1	51.7	208.7	208.7	72466.6
376+00.0000	R	1	30.0	115.3	1	46.0	181.0	181.0	72400.9
377+00.0000	R	1	33.0	116.7	1	50.0	177.8	177.8	72339.8
378+00.0000	R	1	28.0	113.0	1	64.0	211.1	211.1	72241.7
379+00.0000	R	1	25.0	98.1	1	68.0	244.4	244.4	72095.4
380+00.0000	R	1	17.0	77.8	1	78.0	270.4	270.4	71902.8
381+00.0000	R	1	18.6	65.9	1	60.9	257.2	257.2	71711.5
382+00.0000	R	1	22.6	76.2	1	70.9	244.0	244.0	71543.7
383+00.0000	R	1	26.1	90.2	1	65.6	252.7	252.7	71381.2
384+00.0000	R	1	41.0	124.3	1	64.0	239.9	239.9	71265.6
385+00.0000	R	1	48.0	164.8	1	60.0	229.6	229.6	71200.8
386+00.0000	R	1	66.0	211.1	1	56.0	214.8	214.8	71197.1
387+00.0000	R	1	80.0	270.4	1	48.0	192.6	192.6	71274.9
388+00.0000	R	1	66.0	270.4	1	48.0	177.8	177.8	71367.5
389+00.0000	R	1	31.0	179.6	1	61.0	201.9	201.9	71345.3
390+00.0000	R	1	12.0	79.6	1	80.0	261.1	261.1	71163.8
391+00.0000	R	1	20.0	59.3	1	70.0	277.8	277.8	70945.3
392+00.0000	R	1	10.4	56.3	1	154.5	415.8	415.8	70585.8
393+00.0000	R	1	27.0	69.3	1	55.0	388.0	388.0	70267.1
394+00.0000	R	1	41.4	126.7	1	48.9	192.4	192.4	70201.4
395+00.0000	R	1	27.1	126.9	1	50.7	184.4	184.4	70143.9
396+00.0000	R	1	63.4	167.6	1	38.9	166.0	166.0	70145.5
397+00.0000	R	1	61.0	230.4	1	37.0	140.6	140.6	70235.4
398+00.0000	R	1	53.0	211.1	1	36.0	135.2	135.2	70311.3
399+00.0000	R	1	44.0	179.6	1	38.0	137.0	137.0	70353.9
400+00.0000	R	1	39.0	153.7	1	25.0	116.7	116.7	70390.9
401+00.0000	R	1	44.0	153.7	1	28.0	98.1	98.1	70446.5
402+00.0000	R	1	49.0	172.2	1	28.0	103.7	103.7	70515.0
403+00.0000	R	1	52.0	187.0	1	19.0	87.0	87.0	70615.0
404+00.0000	R	1	52.0	192.6	1	23.0	77.8	77.8	70729.8
405+00.0000	R	1	45.0	179.6	1	31.0	100.0	100.0	70809.4
406+00.0000	R	1	49.0	174.1	1	30.0	113.0	113.0	70870.5
407+00.0000	R	1	53.0	188.9	1	32.0	114.8	114.8	70944.6
408+00.0000	R	1	63.0	214.8	1	29.0	113.0	113.0	71046.5
409+00.0000	R	1	68.0	242.6	1	30.0	109.3	109.3	71179.8
410+00.0000	R	1	74.0	263.0	1	30.0	111.1	111.1	71331.6
411+00.0000	R	1	102.0	325.9	1	20.0	92.6	92.6	71565.0
412+00.0000	R	1	98.0	370.4	1	22.0	77.8	77.8	71857.6
413+00.0000	R	1	87.0	342.6	1	22.0	81.5	81.5	72118.7
414+00.0000	R	1	60.0	272.2	1	30.0	96.3	96.3	72294.6
415+00.0000	R	1	39.0	183.3	1	53.0	153.7	153.7	72324.2
416+00.0000	R	1	32.0	131.5	1	37.0	166.7	166.7	72289.1
417+00.0000	R	1	59.0	168.5	1	27.0	118.5	118.5	72339.1
418+00.0000	R	1	87.0	270.4	1	24.0	94.4	94.4	72515.0
419+00.0000	R	1	90.0	327.8	1	24.0	88.9	88.9	72753.9
420+00.0000	R	1	88.0	329.6	1	17.0	75.9	75.9	73007.6
421+00.0000	R	1	87.0	324.1	1	15.0	59.3	59.3	73272.4
422+00.0000	R	1	76.0	301.9	1	24.0	72.2	72.2	73502.0
423+00.0000	R	1	83.0	294.4	1	28.0	96.3	96.3	73700.2
424+00.0000	R	1	86.0	313.0	1	29.0	105.6	105.6	73907.6
425+00.0000	R	1	90.0	325.9	1	29.0	107.4	107.4	74126.1
426+00.0000	R	1	91.0	335.2	1	25.0	100.0	100.0	74361.3
427+00.0000	R	1	88.0	331.5	1	22.0	87.0	87.0	74605.7
428+00.0000	R	1	102.0	351.9	1	20.0	77.8	77.8	74879.8
429+00.0000	R	1	108.0	388.9	1	21.0	75.9	75.9	75192.8

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 5 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		34

DATE: 3/15/2024 9:51:38 AM
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE	
430+00.0000	R	1	130.0	440.7	440.7	1	16.0	68.5	68.5	75565.0
431+00.0000	R	1	105.0	435.2	435.2	1	15.0	57.4	57.4	75942.8
432+00.0000	R	1	63.0	311.1	311.1	1	33.0	88.9	88.9	76165.0
433+00.0000	R	1	30.0	172.2	172.2	1	53.0	159.3	159.3	76177.9
434+00.0000	R	1	70.0	185.2	185.2	1	32.0	157.4	157.4	76205.7
435+00.0000	R	1	29.0	183.3	183.3	1	48.0	148.1	148.1	76240.9
436+00.0000	R	1	44.0	135.2	135.2	1	39.0	161.1	161.1	76215.0
437+00.0000	R	1	66.0	203.7	203.7	1	35.0	137.0	137.0	76281.6
438+00.0000	R	1	48.0	211.1	211.1	1	39.0	137.0	137.0	76355.7
439+00.0000	R	1	45.0	172.2	172.2	1	38.0	142.6	142.6	76385.4
440+00.0000	R	1	45.0	166.7	166.7	1	28.0	122.2	122.2	76429.8
441+00.0000	R	1	35.0	148.1	148.1	1	22.0	92.6	92.6	76485.4
442+00.0000	R	1	36.0	131.5	131.5	1	57.0	146.3	146.3	76470.5
443+00.0000	R	1	32.0	125.9	125.9	1	16.0	135.2	135.2	76461.3
444+00.0000	R	1	23.0	101.9	101.9	1	26.0	77.8	77.8	76485.4
445+00.0000	R	1	28.0	94.4	94.4	1	20.0	85.2	85.2	76494.6
446+00.0000	R	1	17.0	83.3	83.3	1	27.0	87.0	87.0	76490.9
447+00.0000	R	1	6.6	43.8	43.8	1	28.3	102.4	102.4	76432.3
448+00.0000	R	1	5.7	22.9	22.9	1	36.6	120.1	120.1	76335.0
449+00.0000	R	1	6.0	21.7	21.7	1	61.0	180.7	180.7	76176.1
450+00.0000	R	1	10.0	29.6	29.6	1	67.0	237.0	237.0	75968.7
451+00.0000	R	1	6.0	29.6	29.6	1	67.0	248.1	248.1	75750.1
452+00.0000	R	1	10.0	29.6	29.6	1	66.0	246.3	246.3	75533.5
453+00.0000	R	1	12.0	40.7	40.7	1	77.0	264.8	264.8	75309.4
454+00.0000	R	1	18.0	55.6	55.6	1	69.0	270.4	270.4	75094.6
455+00.0000	R	1	27.0	83.3	83.3	1	52.0	224.1	224.1	74953.8
456+00.0000	R	1	35.0	114.8	114.8	1	43.0	175.9	175.9	74892.7
457+00.0000	R	1	29.0	118.5	118.5	1	42.0	157.4	157.4	74853.8
458+00.0000	R	1	51.0	148.1	148.1	1	44.0	159.3	159.3	74842.7
459+00.0000	R	1	69.0	222.2	222.2	1	38.0	151.9	151.9	74913.1
460+00.0000	R	1	69.0	255.6	255.6	1	35.0	135.2	135.2	75033.5
461+00.0000	R	1	72.0	261.1	261.1	1	29.0	118.5	118.5	75176.1
462+00.0000	R	1	61.0	246.3	246.3	1	27.0	103.7	103.7	75318.7
463+00.0000	R	1	52.0	209.3	209.3	1	26.0	98.1	98.1	75429.8
464+00.0000	R	1	42.0	174.1	174.1	1	30.0	103.7	103.7	75500.1
465+00.0000	R	1	36.0	144.4	144.4	1	27.0	105.6	105.6	75539.0
466+00.0000	R	1	34.0	129.6	129.6	1	22.0	90.7	90.7	75577.9
467+00.0000	R	1	36.0	129.6	129.6	1	19.0	75.9	75.9	75631.6
468+00.0000	R	1	77.0	209.3	209.3	1	16.0	64.8	64.8	75776.1
469+00.0000	R	1	52.0	238.9	238.9	1	24.0	74.1	74.1	75940.9
470+00.0000	R	1	55.0	198.1	198.1	1	17.0	75.9	75.9	76063.1
471+00.0000	R	1	56.0	205.6	205.6	1	20.0	68.5	68.5	76200.1
472+00.0000	R	1	65.0	224.1	224.1	1	19.0	72.2	72.2	76352.0
473+00.0000	R	1	47.0	207.4	207.4	1	10.0	53.7	53.7	76505.7
474+00.0000	R	1	52.0	183.3	183.3	1	8.0	33.3	33.3	76655.7
475+00.0000	R	1	62.0	211.1	211.1	1	6.0	25.9	25.9	76840.9
476+00.0000	R	1	54.0	214.8	214.8	1	7.0	24.1	24.1	77031.6
477+00.0000	R	1	45.0	183.3	183.3	1	15.0	40.7	40.7	77174.2
478+00.0000	R	1	47.0	170.4	170.4	1	9.0	44.4	44.4	77300.1
479+00.0000	R	1	55.0	188.9	188.9	1	8.0	31.5	31.5	77457.5
480+00.0000	R	1	58.0	209.3	209.3	1	7.0	27.8	27.8	77639.0
481+00.0000	R	1	50.0	200.0	200.0	1	15.0	40.7	40.7	77798.3
482+00.0000	R	1	49.0	183.3	183.3	1	12.0	50.0	50.0	77931.6
483+00.0000	R	1	49.0	181.5	181.5	1	11.0	42.6	42.6	78070.5
484+00.0000	R	1	31.0	148.1	148.1	1	27.0	70.4	70.4	78148.3
485+00.0000	R	1	23.0	100.0	100.0	1	52.0	146.3	146.3	78102.0
486+00.0000	R	1	11.0	63.0	63.0	1	69.0	224.1	224.1	77940.9
487+00.0000	R	1	6.6	32.5	32.5	1	51.9	223.8	223.8	77749.5
488+00.0000	R	1	11.0	32.5	32.5	1	52.0	192.4	192.4	77589.7
489+00.0000	R	1	12.0	42.6	42.6	1	36.0	163.0	163.0	77469.3
490+00.0000	R	1	27.0	72.2	72.2	1	38.0	137.0	137.0	77404.5
491+00.0000	R	1	32.0	109.3	109.3	1	37.0	138.9	138.9	77374.9
492+00.0000	R	1	15.0	87.0	87.0	1	61.0	181.5	181.5	77280.4
493+00.0000	R	1	22.0	68.5	68.5	1	32.0	172.2	172.2	77176.7
494+00.0000	R	1	26.0	88.9	88.9	1	32.0	118.5	118.5	77147.1
495+00.0000	R	1	37.0	116.7	116.7	1	30.0	114.8	114.8	77149.0

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 6 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		35

DATE: 3/15/2024 9:52:19 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
496+00.0000	R	39.0	140.7	140.7	1	30.0	111.1	111.1	77178.6
497+00.0000	R	53.0	170.4	170.4	1	26.0	103.7	103.7	77245.3
498+00.0000	R	38.0	168.5	168.5	1	29.0	101.9	101.9	77311.9
499+00.0000	R	24.0	114.8	114.8	1	42.0	131.5	131.5	77295.3
500+00.0000	R	16.0	74.1	74.1	1	45.0	161.1	161.1	77208.2
501+00.0000	R	11.0	50.0	50.0	1	29.9	138.7	138.7	77119.5
502+00.0000	R	6.5	32.5	32.5	1	38.6	126.9	126.9	77025.1
503+00.0000	R	5.6	22.6	22.6	1	39.6	144.9	144.9	76902.8
504+00.0000	R	6.0	21.6	21.6	1	52.0	169.7	169.7	76754.8
505+00.0000	R	11.0	31.5	31.5	1	47.0	183.3	183.3	76602.9
506+00.0000	R	24.0	64.8	64.8	1	41.0	163.0	163.0	76504.8
507+00.0000	R	40.0	118.5	118.5	1	32.0	135.2	135.2	76488.1
508+00.0000	R	38.0	144.4	144.4	1	36.0	125.9	125.9	76506.6
509+00.0000	R	26.0	118.5	118.5	1	36.0	133.3	133.3	76491.8
510+00.0000	R	31.0	105.5	105.5	1	26.5	115.8	115.8	76481.5
511+00.0000	R	54.2	157.7	157.7	1	25.3	96.0	96.0	76543.1
512+00.0000	R	32.0	159.6	159.6	1	37.0	115.4	115.4	76587.4
513+00.0000	R	37.0	127.8	127.8	1	28.0	120.4	120.4	76594.8
514+00.0000	R	40.0	142.6	142.6	1	24.0	96.3	96.3	76641.1
515+00.0000	R	17.0	105.6	105.6	1	36.0	111.1	111.1	76635.5
516+00.0000	R	13.0	55.6	55.6	1	42.0	144.4	144.4	76546.6
517+00.0000	R	20.0	61.1	61.1	1	32.0	137.0	137.0	76470.7
518+00.0000	R	23.0	79.6	79.6	1	30.0	114.8	114.8	76435.5
519+00.0000	R	27.0	92.6	92.6	1	25.0	101.9	101.9	76426.3
520+00.0000	R	39.0	122.2	122.2	1	24.0	90.7	90.7	76457.8
521+00.0000	R	50.0	164.8	164.8	1	17.0	75.9	75.9	76546.6
522+00.0000	R	36.0	159.3	159.3	1	29.0	85.2	85.2	76620.7
523+00.0000	R	34.0	129.6	129.6	1	26.0	101.9	101.9	76648.5
524+00.0000	R	43.0	142.6	142.6	1	19.0	83.3	83.3	76707.8
525+00.0000	R	67.0	203.7	203.7	1	18.0	68.5	68.5	76842.9
526+00.0000	R	83.0	277.8	277.8	1	25.0	79.6	79.6	77041.1
527+00.0000	R	76.0	294.4	294.4	1	30.0	101.9	101.9	77233.7
528+00.0000	R	56.0	244.4	244.4	1	28.0	107.4	107.4	77370.7
529+00.0000	R	46.0	188.9	188.9	1	35.0	116.7	116.7	77442.9
530+00.0000	R	41.0	161.1	161.1	1	89.0	229.6	229.6	77374.4
531+00.0000	R	45.0	159.3	159.3	1	35.0	229.6	229.6	77304.1
532+00.0000	R	49.0	174.1	174.1	1	30.0	120.4	120.4	77357.8
533+00.0000	R	52.0	187.0	187.0	1	32.0	114.8	114.8	77430.0
534+00.0000	R	37.0	164.8	164.8	1	39.0	131.5	131.5	77463.3
535+00.0000	R	33.0	129.6	129.6	1	47.0	159.3	159.3	77433.7
536+00.0000	R	31.0	118.5	118.5	1	49.0	177.8	177.8	77374.4
537+00.0000	R	47.0	144.4	144.4	1	36.0	157.4	157.4	77361.5
538+00.0000	R	56.0	190.7	190.7	1	38.0	137.0	137.0	77415.2
539+00.0000	R	67.0	227.8	227.8	1	38.0	140.7	140.7	77502.2
540+00.0000	R	100.0	309.3	309.3	1	24.0	114.8	114.8	77696.6
541+00.0000	R	107.0	383.3	383.3	1	22.0	85.2	85.2	77994.8
542+00.0000	R	93.0	370.4	370.4	1	27.0	90.7	90.7	78274.4
543+00.0000	R	94.0	346.3	346.3	1	17.0	81.5	81.5	78539.2
544+00.0000	R	73.0	309.3	309.3	1	26.0	79.6	79.6	78768.9
545+00.0000	R	53.0	233.3	233.3	1	36.0	114.8	114.8	78887.4
546+00.0000	R	62.0	213.0	213.0	1	30.0	122.2	122.2	78978.1
547+00.0000	R	57.0	220.4	220.4	1	35.0	120.4	120.4	79078.1
548+00.0000	R	64.0	224.1	224.1	1	30.0	120.4	120.4	79181.8
549+00.0000	R	78.0	263.0	263.0	1	25.0	101.9	101.9	79342.9
550+00.0000	R	76.0	285.2	285.2	1	28.0	98.1	98.1	79530.0
551+00.0000	R	74.0	277.8	277.8	1	33.0	113.0	113.0	79694.8
552+00.0000	R	99.0	320.4	320.4	1	26.0	109.3	109.3	79905.9
553+00.0000	R	113.0	392.6	392.6	1	27.0	98.1	98.1	80200.3
554+00.0000	R	121.0	433.3	433.3	1	23.0	92.6	92.6	80541.1
555+00.0000	R	131.0	466.7	466.7	1	19.0	77.8	77.8	80930.0
556+00.0000	R	134.0	490.7	490.7	1	17.0	66.7	66.7	81354.1
557+00.0000	R	115.0	461.1	461.1	1	19.0	66.7	66.7	81748.5
558+00.0000	R	109.0	414.8	414.8	1	17.0	66.7	66.7	82096.6
559+00.0000	R	118.0	420.4	420.4	1	14.0	57.4	57.4	82459.6
560+00.0000	R	97.0	398.1	398.1	1	17.0	57.4	57.4	82800.3
561+00.0000	R	75.0	318.5	318.5	1	27.0	81.5	81.5	83037.4

NOTE:

SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
EARTHWORK
SUMMARY

SHEET 7 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		36

DATE: 3/15/2024 9:52:55 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\1. General\SH285_Earthwork

BASELINE STATION	CUT SHRINK/SWELL FACTOR	STATION CUT AREA	STATION CUT VOLUME	ADJUSTED STATION CUT	FILL SHRINK/SWELL FACTOR	STATION FILL AREA	STATION FILL VOLUME	ADJUSTED STATION FILL	MASS ORDINATE
562+00.0000	R	1	55.0	240.7	240.7	1	36.0	116.7	83161.5
563+00.0000	R	1	42.0	179.6	179.6	1	55.0	168.5	83172.6
564+00.0000	R	1	31.9	136.8	136.8	1	31.7	160.6	83148.7
565+00.0000	R	1	30.4	115.4	115.4	1	40.7	134.2	83129.9
566+00.0000	R	1	26.2	104.8	104.8	1	49.4	166.9	83067.9
567+00.0000	R	1	21.4	88.2	88.2	1	45.7	176.2	82979.8
568+00.0000	R	1	5.7	50.2	50.2	1	44.0	166.1	82863.9
569+00.0000	R	1	41.0	86.5	86.5	1	43.0	161.1	82789.3
570+00.0000	R	1	61.0	188.9	188.9	1	39.0	151.9	82826.4
571+00.0000	R	1	66.0	235.2	235.2	1	44.0	153.7	82907.8
572+00.0000	R	1	66.0	244.4	244.4	1	41.0	157.4	82994.9
573+00.0000	R	1	57.0	227.8	227.8	1	48.0	164.8	83057.8
574+00.0000	R	1	32.0	164.8	164.8	1	155.0	375.9	82846.7
575+00.0000	R	1	92.0	229.6	229.6	1	47.0	374.1	82702.3
576+00.0000	R	1	99.0	353.7	353.7	1	41.0	163.0	82893.0
577+00.0000	R	1	108.0	383.3	383.3	1	39.0	148.1	83128.2
578+00.0000	R	1	139.0	457.4	457.4	1	34.0	135.2	83450.4
579+00.0000	R	1	154.0	542.6	542.6	1	33.0	124.1	83869.0
580+00.0000	R	1	144.0	551.9	551.9	1	33.0	122.2	84298.6
581+00.0000	R	1	117.0	483.3	483.3	1	34.0	124.1	84657.8
582+00.0000	R	1	72.0	350.0	350.0	1	40.0	137.0	84870.8
583+00.0000	R	1	67.0	257.4	257.4	1	46.0	159.3	84969.0
584+00.0000	R	1	59.0	233.3	233.3	1	50.0	177.8	85024.5
585+00.0000	R	1	76.0	250.0	250.0	1	44.0	174.1	85100.4
586+00.0000	R	1	95.0	316.7	316.7	1	38.0	151.9	85265.2
587+00.0000	R	1	124.0	405.6	405.6	1	33.0	131.5	85539.3
588+00.0000	R	1	141.0	490.7	490.7	1	35.0	125.9	85904.1
589+00.0000	R	1	160.0	557.4	557.4	1	34.0	127.8	86333.8
590+00.0000	R	1	180.0	629.6	629.6	1	33.0	124.1	86839.3
591+00.0000	R	1	176.0	659.3	659.3	1	34.0	124.1	87374.5
592+00.0000	R	1	145.0	594.4	594.4	1	39.0	135.2	87833.8
593+00.0000	R	1	132.0	513.0	513.0	1	41.0	148.1	88198.6
594+00.0000	R	1	119.0	464.8	464.8	1	41.0	151.9	88511.5
595+00.0000	R	1	95.0	396.3	396.3	1	50.0	168.5	88739.3
596+00.0000	R	1	80.0	324.1	324.1	1	59.0	201.9	88861.5
597+00.0000	R	1	74.0	285.2	285.2	1	66.0	231.5	88915.2
598+00.0000	R	1	81.0	287.0	287.0	1	59.0	231.5	88970.8
599+00.0000	R	1	145.0	418.5	418.5	1	36.0	175.9	89213.4
600+00.0000	R	1	112.0	475.9	475.9	1	35.0	131.5	89557.8
601+00.0000	R	1	89.0	372.2	372.2	1	38.0	135.2	89794.9
602+00.0000	R	1	62.0	279.6	279.6	1	53.0	168.5	89906.0
603+00.0000	R	1	53.0	213.0	213.0	1	58.0	205.6	89913.4
604+00.0000	R	1	52.0	194.4	194.4	1	65.0	227.8	89880.1
605+00.0000	R	1	50.0	188.9	188.9	1	73.0	255.6	89813.4
606+00.0000	R	1	55.0	194.4	194.4	1	79.0	281.5	89726.4
607+00.0000	R	1	64.0	220.4	220.4	1	61.0	259.3	89687.5
608+00.0000	R	1	57.0	224.1	224.1	1	57.0	218.5	89693.0
609+00.0000	R	1	57.0	211.1	211.1	1	53.0	203.7	89700.4
610+00.0000	R	1	61.0	218.5	218.5	1	45.0	181.5	89737.5
611+00.0000	R	1	77.0	255.6	255.6	1	41.0	159.3	89833.8
612+00.0000	R	1	106.0	338.9	338.9	1	36.0	142.6	90030.1
613+00.0000	R	1	117.0	413.0	413.0	1	35.0	131.5	90311.5
614+00.0000	R	1	129.0	455.6	455.6	1	33.0	125.9	90641.2
615+00.0000	R	1	127.0	474.1	474.1	1	32.0	120.4	90994.9
616+00.0000	R	1	112.0	442.6	442.6	1	32.0	118.5	91319.0
617+00.0000	R	1	91.0	375.9	375.9	1	33.0	120.4	91574.5
618+00.0000	R	1	73.0	303.7	303.7	1	32.0	120.4	91757.8
619+00.0000	R	1	52.0	231.5	231.5	1	35.0	124.1	91865.2
620+00.0000	R	1	0.0	96.3	96.3	1	30.0	120.4	91841.2

TOTAL 156,096.6 CY 64,255.4 CY

NOTE:
 SHRINKAGE AND SWELLING FACTORS WERE NOT CONSIDERED IN DETERMINING QUANTITIES. VOLUMES WERE MEASURED AS ORIGINAL AND FINAL POSITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING VALUES WHEN NEEDED.

VOLUMES SHOWN ARE IN CUBIC YARDS.

Pharr District Central Design



SH 285
 EARTHWORK
 SUMMARY

SHEET 8 OF 8

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	37

DATE: 3/4/2024 4:56:38 PM
FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP COVER.dgn

TRAFFIC CONTROL PLAN COVER SHEET

Pharr District Central Design



SH 285
TCP COVER SHEET

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		38

DATE: 3/4/2024 4:56:51 PM
FILE: P:\tdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP GENERAL NOTES.dgn

GENERAL NOTES AND SPECIFICATIONS DATA:

USE A POWER-BROOM WHEN CLEANING THE ROADWAY AS NEEDED.

REMOVE AND DISPOSE OF ALL MATERIAL NOT DEEMED SALVAGEABLE BY THE ENGINEER, UNLESS OTHERWISE SHOWN ON THE PLANS.

ON EXISTING PAVEMENT THAT WILL REMAIN IN PLACE, SAND BLAST OR SURFACE TREAT IN ORDER TO REMOVE EXISTING STRIPING.

DO NOT BLOCK DRAINAGE WHEN HANDLING AND STOCKPILING EXCAVATED MATERIAL.

MAINTAIN ACCESS TO DRIVEWAYS AND INTERSECTIONS THROUGH ALL PHASES OF CONSTRUCTION.

MAINTAIN POSITIVE DRAINAGE DURING ALL PHASES OF CONSTRUCTION.

ALWAYS COMPLETE THE PROPOSED DRIVEWAYS DURING THEIR TCP PHASE BEFORE SWITCHING TRAFFIC TO A NEW PHASE UNLESS DIRECTED BY THE ENGINEER.

TRAFFIC CONTROL DEVICES:

AT THE COMMENCEMENT OF THE PROJECT, ALL TRAFFIC CONTROL DEVICES SHALL BE IN ACCEPTABLE CONDITION, AND MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT, AS PER GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES AND FEATURES.

NOTIFY THE AREA ENGINEER (AE) IN WRITING (E-MAIL IS ACCEPTABLE) ONCE THE TRAFFIC CONTROL PLAN (TCP) AND ALL TRAFFIC CONTROL DEVICES HAVE BEEN INSTALLED AS PER PLANS ON THE PROJECT SO THAT THE DEPARTMENT'S RESPONSIBLE PERSON ACCOMPANIED BY THE CONTRACTOR'S RESPONSIBLE PERSON CAN CONDUCT AN INSPECTION ON THE SAID TCP AND TRAFFIC CONTROL DEVICES. COMMENCEMENT OF WORK WILL NOT BE AUTHORIZED OR ALLOWED UNTIL THE AE NOTIFIES THE CONTRACTOR IN WRITING TO PROCEED WITH WORK.

CONTRACTOR SHALL HAVE A SUFFICIENT AMOUNT OF TRAFFIC CONTROL DEVICES IN ACCEPTABLE CONDITION TO REPLACE ANY DAMAGED TRAFFIC CONTROL DEVICES WITHIN 24 HOURS OF NOTIFICATION.

PROVIDE ADDITIONAL SIGNS AND BARRICADES AS NECESSARY TO ADDRESS FIELD CONSTRUCTABILITY AND VISIBILITY. THESE ADDITIONAL SIGNS WILL BE CONSIDERED SUBSIDIARY TO ITEM 502.

REMOVE OR COMPLETELY COVER ALL EXISTING SIGNS WHICH ARE IN CONFLICT WITH THE TRAFFIC CONTROL PLAN.

ADJUST STOP SIGNS AS NEEDED ON INTERSECTING STREETS DURING THE VARIOUS CONSTRUCTION PHASES. DO NOT REMOVE ANY EXISTING STOP SIGNS UNTIL TEMPORARY SIGNS ARE IN PLACE.

COORDINATE THE TRAFFIC CONTROL PLAN AND THE VARIOUS SEQUENCES OF CONSTRUCTION WITH ADJACENT CONSTRUCTION PROJECTS IF APPLICABLE,

TO ENSURE THE UNINTERRUPTED AND SAFE FLOW OF TRAFFIC.

NOTIFY THE ENGINEER IN WRITING WHEN MAJOR TRAFFIC CHANGES ARE TO BE MADE. NOTIFICATIONS MUST BE GIVEN A MINIMUM OF THREE WORKING DAYS PRIOR TO THE CHANGE.

ALL WORK ZONE PAVEMENT MARKINGS FOR THIS PROJECT SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.

SAFETY:

PROTECT EXPOSED PITS THAT MUST REMAIN OPEN DURING NON-WORKING HOURS AS PER OSHA REQUIREMENTS.

PROJECT SPECIFIC NOTES:

THE TRAFFIC CONTROL PLAN AND VARIOUS PHASES AND SEQUENCES OF CONSTRUCTION SERVE AS A GUIDE FOR THE SAFE HANDLING OF TRAFFIC DURING CONSTRUCTION OF THE PROJECT'S ROADWAY, UTILITIES AND OTHER RELATED ITEMS. IT DOES NOT ATTEMPT TO ADDRESS EVERY ASPECT OF CONSTRUCTION THAT IS REQUIRED DURING EACH PHASE OF CONSTRUCTION. ALSO, THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF CONSTRUCTING THE COMPLETE ROADWAYS, UTILITIES AND OTHER RELATED ITEMS, AS NOTED ON THE PLANS AND SPECIFICATIONS.

IF NECESSARY, TEMPORARY LANE CLOSURES SHALL BE DONE DURING OFF-PEAK HOURS, BETWEEN 9:00 AM TO 3:00 PM AND AT NIGHT-TIME, OR AS DIRECTED BY THE ENGINEER. DURING THE DAY-TIME PEAK HOURS OF 6:00 AM TO 8:30 AM AND 3:00 PM TO 7:00 PM, THE CONTRACTOR SHALL MAINTAIN THE NUMBER OF LANES OPEN TO TRAFFIC SHOWN ON THE TRAFFIC CONTROL PLANS.

WHERE THE CONTRACTOR DESIRES TO MOVE ANY CONSTRUCTION EQUIPMENT NOT LICENSED FOR OPERATION ON PUBLIC HIGHWAYS OR ACROSS ANY PAVEMENT, HE/SHE SHALL PROTECT THE PAVEMENT FROM ALL DAMAGE AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL KEEP TRAVELED SURFACES USED IN HIS HAULING OPERATIONS CLEAR AND FREE OF DIRT, DEBRIS, OR OTHER MATERIAL. SURFACE CLEARING AND CLEANING OPERATIONS SHALL BE PERFORMED IMMEDIATELY UPON THE OCCURRENCE OF ANY DEBRIS ON THE TRAVEL LANES. FAILURE TO COMPLY CAN RESULT IN CONTRACTOR BEING PENALIZED BY THE ENGINEER.

THE CONTRACTOR SHALL MAINTAIN ALL INTERSECTING ROADS OPEN AND DRIVEABLE TO THE PUBLIC. IN AREAS WHERE EXISTING AND PROPOSED INTERSECTING ROADS DO NOT PROVIDE A SMOOTH TRAVEL WAY TRANSITION OR TURN RADIUS FOR TRUCK TRAFFIC, THE CONTRACTOR SHALL PLACE TEMPORARY RAP OR OTHER MATERIAL AS DIRECTED BY THE ENGINEER TO PROVIDE FOR A SMOOTH TRANSITION.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
GENERAL NOTES**

SHEET 1 OF 2

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		39

DATE: 3/4/2024 4:56:52 PM
FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP GENERAL NOTES.dgn

PROJECT SPECIFIC NOTES (CONTINUED):

INSTALL PROJECT LIMITS AND ADVANCE WARNING SIGNS, CROSSROADS BARRICADES/SIGNS AS SHOWN ON THE TRAFFIC CONTROL PLAN (TCP) AND IN ACCORDANCE WITH THE TMUTCD AND BC STANDARDS AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY PROPOSED ROADWAY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE PROJECT UNTIL COMPLETION AND ACCEPTANCE OF THE PROJECT BY TxDOT.

CHANGES TO MESSAGES ON PORTABLE CHANGEABLE MESSAGE SIGNS ARE PERMITTED WITH APPROVAL BY AREA ENGINEER. CONTRACTOR MUST MAKE USE OF TCP STANDARDS, BC STANDARDS AND WORKZONE STANDARDS FOR THIS OPERATION.

PLACE PCMS ONE WEEK PRIOR TO SETTING BARRICADES. COORDINATE WITH THE ENGINEER FOR CORRECT PLACEMENT/APPROVE MESSAGE TO BE DISPLAYED PRIOR TO PLACEMENT.

ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.

EXISTING SIGNS THAT ARE IN CONFLICT WITH THE PROPOSED WK ZN SIGNS SHALL BE REMOVED OR COVERED.

EXISTING STRIPING THAT IS IN CONFLICT WITH THE PROPOSED WK ZN PAVEMENT MARKINGS SHALL BE REMOVED.

REFER TO PUBLIC AND PRIVATE DRIVEWAY TABLES, PLAN LAYOUT, & SEQUENCE OF CONSTRUCTION FOR ADDITIONAL INFORMATION REGARDING PROPOSED DRIVEWAYS, RCP'S, AND SET'S.

CONTRACTOR MUST MAINTAIN ACCESS TO PUBLIC/PRIVATE DRIVEWAYS & CROSS STREETS DURING CONSTRUCTION USING ALL WEATHER MATERIALS, AND MUST COORDINATE WITH AFFECTED PROPERTY OWNERS REGARDING ACCESS 3 DAYS PRIOR TO CONSTRUCTION.

INSTALLATION AND REMOVAL OF TEMPORARY DRAINAGE STRUCTURES TO BE PAID UNDER ITEM 508-6001. DAMAGE TO DRAINAGE STRUCTURES WILL NEED TO BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

ALL DETOURS MUST BE CONSTRUCTED TO MATCH THE EXISTING ROADWAY CROSS SLOPE AND PROVIDE POSITIVE DRAINAGE. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING POSITIVE DRAINAGE AT ALL TIMES DURING CONSTRUCTION.

CONTRACTOR SHALL TEMPORARILY RELOCATE EXISTING MAILBOXES THAT WILL BE IN CONFLICT WITH TEMPORARY DETOUR AND PROPOSED ROADWAY. CONTRACTOR SHALL REMOVE MATERIAL AS NEEDED TO CONSTRUCT TEMPORARY DETOUR WITHIN THE SAME DAY OF PLACING DETOUR.

CONTRACTOR SHALL MINIMIZE UNEVEN LANE DIFFERENTIAL WHEN SHIFTING TRAFFIC BETWEEN PHASING ACCORDING TO "WZ(UL)-13."

EXCESS SALVAGE MATERIAL WILL BE AVAILABLE TO BE USED FOR THE CORRECTION OF SOFT SPOTS ENCOUNTERED ON THE PROJECT LIMITS OR AS APPROVED BY THE ENGINEER.

TO ACCOMMODATE THE VARIOUS PHASES OF CONSTRUCTION, CONTRACTOR WILL BE RESPONSIBLE FOR THE TEMPORARY ADJUSTMENTS AND RELOCATION OF EXISTING SIGNAL HEADS, POLES, PRECAST CONCRETE SAFETY BARRIER, SIGNING, AND ANY OTHER INCIDENTAL WORK NECESSARY TO PROVIDE FOR PROPER TRAFFIC SIGNAL OPERATION. THE ADJUSTMENTS AND RELOCATIONS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED SUBSIDIARY TO ITEM 502: "BARRICADES, SIGNS AND TRAFFIC HANDLING."

NO PHASE OF CONSTRUCTION SHALL START UNTIL COMPLETION OF THE PREVIOUS PHASE INCLUDING DRIVEWAYS, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

PROVIDE TEMPORARY ILLUMINATION AT TEMPORARY TRAFFIC SIGNAL TO SAFELY GUIDE TRAFFIC. PLACE ONE LUMINAIRE POST AT EACH TEMPORARY SIGNAL AREA OR ATTACHMENT TO THE TEMPORARY TRAFFIC SIGNAL. THE PORTABLE ILLUMINATION EQUIPMENT WILL BE POWERED WITH A GENERATOR OR AS APPROVED BY THE ENGINEER. PROVIDE BACKUP AND KEEP OPERATIONAL AND AVAILABLE ON THE JOB SITE AT ALL TIMES DURING TRAFFIC CONTROL. (TO BE CONSIDERED SUBSIDIARY TO ITEM 502, "BARRICADES, SIGNS AND TRAFFIC HANDLING"). TIMING FOR TEMPORARY SIGNALS SHALL BE PROVIDED BY CONTRACTOR AND APPROVED BY THE ENGINEER. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING TIMINGS AFTER IMPLEMENTATION.

DO NOT FACE LUMINAIRES AT UPCOMING TRAFFIC.

LIMIT THE LANE CLOSURE TO ONLY THE REQUIRED DISTANCE TO COVER THE HALF (0.5) MILE CONSTRUCTION WORKZONE AS SHOWN ON THE TCP LAYOUT. ONE LANE TWO-WAY TRAFFIC CONTROL WITH A TEMPORARY TRAFFIC SIGNAL WILL REMAIN IN PLACE OVERNIGHT AND DURING NON-WORKING HOURS UNTIL WORK IS COMPLETE. PROVIDE NECESSARY ILLUMINATION AS NOTED IN THE CONSTRUCTION SEQUENCE NOTES.

NO PHASE OF CONSTRUCTION SHALL START UNTIL COMPLETION OF THE PREVIOUS PHASE, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

RELOCATION OF EXISTING UTILITIES CONFLICTING WITH ANY PROPOSED ROADWAY AND DRAINAGE IMPROVEMENTS SHALL BE ADJUSTED PRIOR TO COMMENCING ANY PROPOSED IMPROVEMENTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY HORIZONTAL AND VERTICAL INFORMATION, THEREFORE THE CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES TO VERIFY UTILITIES PRIOR TO CONSTRUCTION.

ADEQUATE SIGNS AND BARRICADES SHALL BE INSTALLED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO OPENING ANY SECTION TO TRAFFIC. SIGNS ADDITIONAL TO THOSE REQUIRED BY THE BARRICADE STANDARDS OR DEEMED NECESSARY AND APPROVED BY THE ENGINEER WILL BE CONSIDERED SUBSIDIARY TO ITEM 502.

TEMPORARY EROSION CONTROL DEVICES MUST BE INSTALLED PRIOR TO THE START OF ANY CONSTRUCTION.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
TCP
GENERAL NOTES

SHEET 2 OF 2

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		40

DATE: 3/4/2024 4:56:57 PM
FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP SEQUENCE OF CONST.dgn

CONSTRUCTION SEQUENCE

NOTE:

NO PHASE OF CONSTRUCTION SHALL START UNTIL COMPLETION OF THE PREVIOUS PHASE, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

INSTALL PROJECT LIMITS AND ADVANCE WARNING SIGNS, CROSSROADS BARRICADES/SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP), IN ACCORDANCE WITH THE TMUTCD AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY PROPOSED ROADWAY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE PROJECT UNTIL COMPLETION AND ACCEPTANCE OF THE PROJECT BY TxDOT.

TO ACCOMMODATE THE VARIOUS PHASES OF CONSTRUCTION, THE CONTRACTOR WILL BE RESPONSIBLE FOR THE TEMPORARY ADJUSTMENTS AND RELOCATION OF EXISTING SIGNAL HEADS, POLES, LOW-PROFILE CONCRETE TRAFFIC BARRIER, SIGNING, AND ANY OTHER INCIDENTAL WORK NECESSARY TO PROVIDE FOR PROPER TRAFFIC SIGNAL OPERATION. THE ADJUSTMENTS AND RELOCATIONS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED SUBSIDIARY TO ITEM 502, "BARRICADES, SIGNS AND TRAFFIC HANDLING."

ALL INTERSECTIONS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES.

ADEQUATE SIGNS AND BARRICADES SHALL BE INSTALLED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO OPENING ANY SECTION TO TRAFFIC. SIGNS ADDITIONAL TO THOSE REQUIRED BY THE BARRICADE STANDARDS OR DEEMED NECESSARY AND APPROVED BY THE ENGINEER WILL BE CONSIDERED SUBSIDIARY TO ITEM 502.

TEMPORARY EROSION CONTROL DEVICES MUST BE INSTALLED PRIOR TO THE START OF ANY CONSTRUCTION.

PHASE I STEP I

1. PRIOR TO THE BEGINNING OF THIS PHASE, INSTALL TEMPORARY SIGNS, INCLUDING REDUCED SPEED LIMIT SIGNS, TRAFFIC CONTROL DEVICES, AND SW3P ITEMS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL BE IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE. CONSTRUCTION WORK WILL NOT BEGIN UNTIL THE ENGINEER GIVES APPROVAL OF THE ABOVE REFERENCED ITEMS.

2. CONSTRUCT DRAINAGE CULVERTS UTILIZING TxDOT TCP LATEST STANDARDS (TCP 2-1 THRU TCP 2-7). USING OPEN CUT METHOD, THE CONTRACTOR SHALL BACKFILL ALL OPEN-CUT SECTIONS AT THE END OF EACH WORK DAY, AND CUT & RESTORE EXISTING PAVEMENT. USE ONE LANE, TWO WAY TRAFFIC CONTROL. ALL DRAINAGE CULVERTS ON THE RESPECTIVE PHASE LIMITS MUST BE COMPLETED ON THEIR DESIGNATED PHASE.

CONSTRUCTION SEQUENCE (CONTINUED)

3. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.

4. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.

5. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.

6. CONTRACTOR SHALL CONSTRUCT THE NORTH PORTION OF SH 285 FROM STATIONS 189+20 TO STATION 215+60. DURING THIS CONSTRUCTION, THE SOUTH PORTION OF SH 285 WILL BECOME A 12' LANE WITH BI-DIRECTIONAL TRAFFIC THAT IS GUIDED WITH THE USE OF PORTABLE TRAFFIC SIGNALS.

7. CONTRACTOR SHALL ADD RETRO-FIT RAIL TO EXISTING BRIDGE CLASS CULVERT FROM STA. 142+70 TO STA. 143+10.

8. THERE SHALL BE NO CONSTRUCTION WITHIN THE EXISTING BRIDGE LIMITS FROM STATION 199+95 TO STATION 201+30.

9. ONCE PHASE I STEP I NORTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE I STEP II

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.

2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.

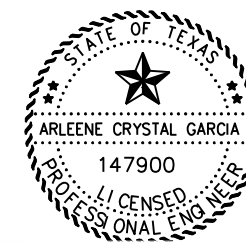
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.

4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE NORTH SIDE OF SH 285.

5. CONSTRUCT THE SOUTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 189+20 TO STATION 222+60, AS SHOWN IN THE TCP TYPICAL SECTIONS AND LAYOUT.

6. THERE SHALL BE NO CONSTRUCTION WITHIN THE EXISTING BRIDGE LIMITS WHICH ARE FROM STATION 199+95 TO STATION 201+30.

7. ONCE PHASE I STEP II SOUTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION LANE CONFIGURATION FROM STA. 189+20 TO STA. 222+60 USING REMOVABLE STRIPING, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285 TCP SEQUENCE OF CONSTRUCTION

SHEET 1 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	41

DATE: 3/4/2024 4:56:59 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP SEQUENCE OF CONST.dgn

CONSTRUCTION SEQUENCE (CONTINUED)

PHASE II STEP I

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. EXTEND EXISTING DRAINAGE STRUCTURES FOR STA 99+10, STA 101+51 AND STA 111+00 AS FAR AS NECESSARY TO ACCOMODATE TEMPORARY PAVEMENT. CULVERT EXTENSIONS SHALL BE PERMANENT/TEMPORARY IN NATURE DURING THIS CONSTRUCTION PHASE. INSTALL SET'S AND/OR HEADWALLS FOR ALL CROSS CULVERT LOCATIONS THROUGH ENTIRE PHASE LIMIT. FOLLOW LIMITS DEFINED IN TCP TYPICAL SECTIONS SHEET AND EXTEND STRUCTURES ON THE FIRST HALF OF THE ROADWAY.
3. CONTRACTOR SHALL CONSTRUCT THE TEMPORARY ROADWAY WIDENING DETOUR ON THE NORTH SIDE OF SH 285 AND MAINTAIN TRAVEL LANES OPEN AS SHOWN ON THE PROPOSED TRAFFIC CONTROL PLAN TYPICAL SECTIONS AND LAYOUTS FROM STATION 87+50 TO STATION 194+80. THE DETOUR WILL CHANGE FROM 10' TO 16' ON STATION 93+05 AND REMAIN 16' UNTIL STATION 103+85. FOR APPROPRIATE TRANSITIONS PLEASE REFER TO PHASE II TYPICAL SECTIONS.

PHASE II STEP II

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE NORTH SIDE OF SH 285.
5. CONSTRUCT THE SOUTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 96+00 TO STATION 189+20, AS SHOWN IN THE TCP TYPICAL SECTIONS AND LAYOUT.
6. ONCE SEGMENT FROM 96+00 TO 99+28 IS COMPLETED, CONSTRUCT THE LEFT PORTION OF THE ROADWAY ON THE INTERSECTION OF SH 285 AND FM 1017 FROM STATION 99+28 TO STATION 100+35 (STEP II STAGE A CONSTRUCTION.) REFER TO PHASE II STEP II STAGE A SHEET FOR PAVEMENT DESIGN AND ADDITIONAL DETAILS. OVERNIGHT CONSTRUCTION OPERATIONS SHOULD BE USED.
7. CONSTRUCT THE RIGHT PORTION OF THE ROADWAY ON THE INTERSECTION OF SH 285 AND FM 1017 FROM STATION 100+35 TO STATION 101+80 (STEP II STAGE B CONSTRUCTION.) REFER TO PHASE II STEP II STAGE B SHEET FOR PAVEMENT DESIGN AND ADDITIONAL DETAILS. OVERNIGHT CONSTRUCTION OPERATIONS SHOULD BE USED.

CONSTRUCTION SEQUENCE (CONTINUED)

8. ONCE PHASE II STEP II SOUTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE II STEP III

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.
5. CONSTRUCT THE NORTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 96+00 TO STATION 189+20, AS SHOWN IN THE TCP TYPICAL SECTIONS AND REMOVE TEMPORARY DETOUR PREVIOUSLY CONSTRUCTED ON PHASE II STEP I FROM STATION 87+50 TO STATION 193+80.

6. ONCE PHASE II STEP III NORTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION LANE CONFIGURATION FROM STA. 96+00 TO STA. 189+20 USING REMOVABLE STRIPING, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE III STEP I

1. CONSTRUCT DRAINAGE CULVERTS UTILIZING TXDOT TCP LATEST STANDARDS (TCP 2-1 THRU TCP 2-7). USING OPEN CUT METHOD, THE CONTRACTOR SHALL BACKFILL ALL OPEN-CUT SECTIONS AT THE END OF EACH WORK DAY, AND CUT & RESTORE EXISTING PAVEMENT. USE ONE LANE, TWO WAY TRAFFIC CONTROL. ALL DRAINAGE CULVERTS ON THE RESPECTIVE PHASE LIMITS MUST BE COMPLETED ON THEIR DESIGNATED PHASE.
2. CONSTRUCTION OF DRAINAGE CULVERTS SHALL BE DONE DURING OFF-PEAK HOURS, AT NIGHT TIME HOURS, OR DURING WEEKEND HOURS. CONTRACTOR MUST COORDINATE WITH ENGINEER AND TXDOT HEBBRONVILLE MAINTENANCE OFFICE FOR LANE CLOSURES DURING THIS WORK.
3. PRIOR TO THE BEGINNING OF THIS PHASE, INSTALL ADVANCED WARNING SIGNS, TRAFFIC CONTROL DEVICES, AND SW3P ITEMS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. CONSTRUCTION WORK WILL NOT BEGIN UNTIL THE ENGINEER GIVES APPROVAL OF THE ABOVE REFERENCED ITEMS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
SEQUENCE OF
CONSTRUCTION**

SHEET 2 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		42

DATE: 3/4/2024 4:57:00 PM
FILE: P:\tdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design\Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP SEQUENCE OF CONST.dgn

CONSTRUCTION SEQUENCE (CONTINUED)

4. CONTRACTOR SHALL CONSTRUCT THE TEMPORARY ROADWAY WIDENING DETOUR ON THE NORTH SIDE OF SH 285 AND MAINTAIN TRAVEL LANES OPEN AS SHOWN ON THE PROPOSED TRAFFIC CONTROL PLAN TYPICAL SECTIONS AND LAYOUTS FROM STATION 222+60 TO STATION 319+50.

PHASE III STEP II

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE NORTH SIDE OF SH 285.
5. CONSTRUCT THE SOUTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 215+60 TO STATION 312+50, AS SHOWN IN THE TCP TYPICAL SECTIONS AND LAYOUT.
6. ONCE PHASE III STEP II SOUTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE III STEP III

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.
5. CONSTRUCT THE NORTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 215+60 TO STATION 305+50, AS SHOWN IN THE TCP TYPICAL SECTIONS AND REMOVE TEMPORARY DETOUR PREVIOUSLY CONSTRUCTED ON PHASE III STEP I FROM STATION 208+60 TO STATION 305+50; TEMPORARY DETOUR FROM STATION 305+50 TO STATION 319+50 TO REMAIN FOR PHASE IV CONSTRUCTION.

CONSTRUCTION SEQUENCE (CONTINUED)

6. ONCE PHASE III STEP III NORTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50 USING NON-REMOVABLE STRIPING, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE IV STEP I

1. CONSTRUCT DRAINAGE CULVERTS UTILIZING TXDOT TCP LATEST STANDARDS (TCP 2-1 THRU TCP 2-7). USING OPEN CUT METHOD, THE CONTRACTOR SHALL BACKFILL ALL OPEN-CUT SECTIONS AT THE END OF EACH WORK DAY, AND CUT & RESTORE EXISTING PAVEMENT. USE ONE LANE, TWO WAY TRAFFIC CONTROL. ALL DRAINAGE CULVERTS ON THE RESPECTIVE PHASE LIMITS MUST BE COMPLETED ON THEIR DESIGNATED PHASE.
2. CONSTRUCTION OF DRAINAGE CULVERTS SHALL BE DONE DURING OFF-PEAK HOURS, AT NIGHT TIME HOURS, OR DURING WEEKEND HOURS. CONTRACTOR MUST COORDINATE WITH ENGINEER AND TXDOT HEBBRONVILLE MAINTENANCE OFFICE FOR LANE CLOSURES DURING THIS WORK.
3. PRIOR TO THE BEGINNING OF THIS PHASE, INSTALL ADVANCED WARNING SIGNS, TRAFFIC CONTROL DEVICES, AND SW3P ITEMS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. CONSTRUCTION WORK WILL NOT BEGIN UNTIL THE ENGINEER GIVES APPROVAL OF THE ABOVE REFERENCED ITEMS.
4. CONTRACTOR SHALL CONSTRUCT THE TEMPORARY ROADWAY WIDENING DETOUR ON THE NORTH SIDE OF SH 285 AND MAINTAIN TRAVEL LANES OPEN AS SHOWN ON THE PROPOSED TRAFFIC CONTROL PLAN TYPICAL SECTIONS AND LAYOUTS FROM STATION 319+50 TO STATION 491+40.

PHASE IV STEP II

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.
5. CONSTRUCT THE SOUTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 312+50 TO STATION 484+40, AS SHOWN IN THE TCP TYPICAL SECTIONS AND LAYOUT.
6. ONCE PHASE IV STEP II SOUTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
SEQUENCE OF
CONSTRUCTION**

SHEET 3 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		43

DATE: 3/4/2024 4:57:01 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design\Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP SEQUENCE OF CONST.dgn

CONSTRUCTION SEQUENCE (CONTINUED)

PHASE IV STEP III

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.
5. CONSTRUCT THE NORTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 305+50 TO STATION 477+40, AS SHOWN IN THE TCP TYPICAL SECTIONS AND REMOVE TEMPORARY DETOUR PREVIOUSLY CONSTRUCTED ON PHASE IV STEP I FROM STATION 319+50 TO STATION 477+40; TEMPORARY DETOUR FROM STATION 477+40 TO STATION 491+40 TO REMAIN FOR PHASE V CONSTRUCTION.
6. ONCE PHASE IV STEP III NORTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40 USING NON-REMOVABLE STRIPING, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE V STEP I

1. CONSTRUCT DRAINAGE CULVERTS UTILIZING TXDOT TCP LATEST STANDARDS (TCP 2-1 THRU TCP 2-7). USING OPEN CUT METHOD, THE CONTRACTOR SHALL BACKFILL ALL OPEN-CUT SECTIONS AT THE END OF EACH WORK DAY, AND CUT & RESTORE EXISTING PAVEMENT. USE ONE LANE, TWO WAY TRAFFIC CONTROL. ALL DRAINAGE CULVERTS ON THE RESPECTIVE PHASE LIMITS MUST BE COMPLETED ON THEIR DESIGNATED PHASE.
2. CONSTRUCTION OF DRAINAGE CULVERTS SHALL BE DONE DURING OFF-PEAK HOURS, AT NIGHT TIME HOURS, OR DURING WEEKEND HOURS. CONTRACTOR MUST COORDINATE WITH ENGINEER AND TXDOT HEBBRONVILLE MAINTENANCE OFFICE FOR LANE CLOSURES DURING THIS WORK.
3. PRIOR TO THE BEGINNING OF THIS PHASE, INSTALL ADVANCED WARNING SIGNS, TRAFFIC CONTROL DEVICES, AND SW3P ITEMS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. CONSTRUCTION WORK WILL NOT BEGIN UNTIL THE ENGINEER GIVES APPROVAL OF THE ABOVE REFERENCED ITEMS.
4. CONTRACTOR SHALL CONSTRUCT THE TEMPORARY ROADWAY WIDENING DETOUR ON THE NORTH SIDE OF SH 285 AND MAINTAIN TRAVEL LANES OPEN AS SHOWN ON THE PROPOSED TRAFFIC CONTROL PLAN TYPICAL SECTIONS AND LAYOUTS FROM STATION 491+40 TO STATION 627+00.

CONSTRUCTION SEQUENCE (CONTINUED)

PHASE V STEP II

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.
5. CONSTRUCT THE SOUTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 484+40 TO STATION 620+00, AS SHOWN IN THE TCP TYPICAL SECTIONS AND LAYOUT.
6. ONCE PHASE V STEP II SOUTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH WORK ZONE PAVEMENT MARKINGS, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.

PHASE V STEP III

1. INSTALL TEMPORARY SIGNS, AS SHOWN ON THE TRAFFIC CONTROL PLANS (TCP) AND/OR AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE ERECTED AND PLACED PRIOR TO COMMENCING ANY CONSTRUCTION AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE CONSTRUCTION PHASE.
2. REMOVE ALL EXISTING CONFLICTING STRIPING AND RAISED REFLECTIVE PAVEMENT MARKERS ON EXISTING ROADWAY WHICH ARE NOT BEING USED AND INSTALL PAVEMENT MARKINGS AS SHOWN ON TCP TYPICAL SECTIONS AND LAYOUTS.
3. INSTALL TEMPORARY TRAFFIC CONTROL CHANNELIZING DEVICES AS SHOWN IN TCP TYPICAL SECTIONS AND LAYOUT.
4. ONCE TRAFFIC CONTROL SETUP HAS BEEN COMPLETED EXISTING TRAFFIC SHALL BE SHIFTED TO THE SOUTH SIDE OF SH 285.
5. CONSTRUCT THE NORTH PORTION OF SH 285 ROADWAY TO ITS FINAL CONFIGURATION FROM STATION 477+40 TO STATION 620+00, AS SHOWN IN THE TCP TYPICAL SECTIONS AND REMOVE TEMPORARY DETOUR PREVIOUSLY CONSTRUCTED ON PHASE V STEP I FROM STATION 491+40 TO STATION 627+00.
6. ONCE PHASE V STEP III NORTH PORTION OF THE PROPOSED SH 285 ROADWAY IS COMPLETED, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00 USING NON-REMOVABLE STRIPING, INSTALL PROPOSED DRIVEWAY CULVERTS, SEED/SOD AND INSTALL EROSION CONTROL DEVICES AS SHOWN IN THE TRAFFIC CONTROL PLAN LAYOUTS AND SW3P TYPICAL LAYOUTS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
SEQUENCE OF
CONSTRUCTION**

SHEET 4 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		44

DATE: 3/4/2024 4:57:03 PM
FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP SEQUENCE OF CONST.dgn

CONSTRUCTION SEQUENCE (CONTINUED)

PHASE VI

1. PRIOR TO THE BEGINNING OF THIS PHASE, INSTALL ADVANCED WARNING SIGNS, TRAFFIC CONTROL DEVICES, AND SW3P ITEMS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. CONSTRUCTION WORK WILL NOT BEGIN UNTIL THE ENGINEER GIVES APPROVAL OF THE ABOVE REFERENCED ITEMS.
2. PERFORM MILLING/PLANING OPERATIONS FOR INCIDENTAL AREAS OF CONSTRUCTION FROM STATION TO STATION.
3. APPLY FINAL 2" LAYER OF ASPHALT AS SHOWN IN THE PROPOSED TYPICAL SECTIONS.
4. DELINEATE ALL COMPLETED ROADWAY SECTIONS TEMPORARILY USING SHORT TERM WORK ZONE PAVEMENT MARKINGS.
5. INSTALL PERMANENT PAVEMENT MARKINGS IN ACCORDANCE WITH TXDOT STANDARDS AND PROPOSED PAVEMENT MARKINGS LAYOUT.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design

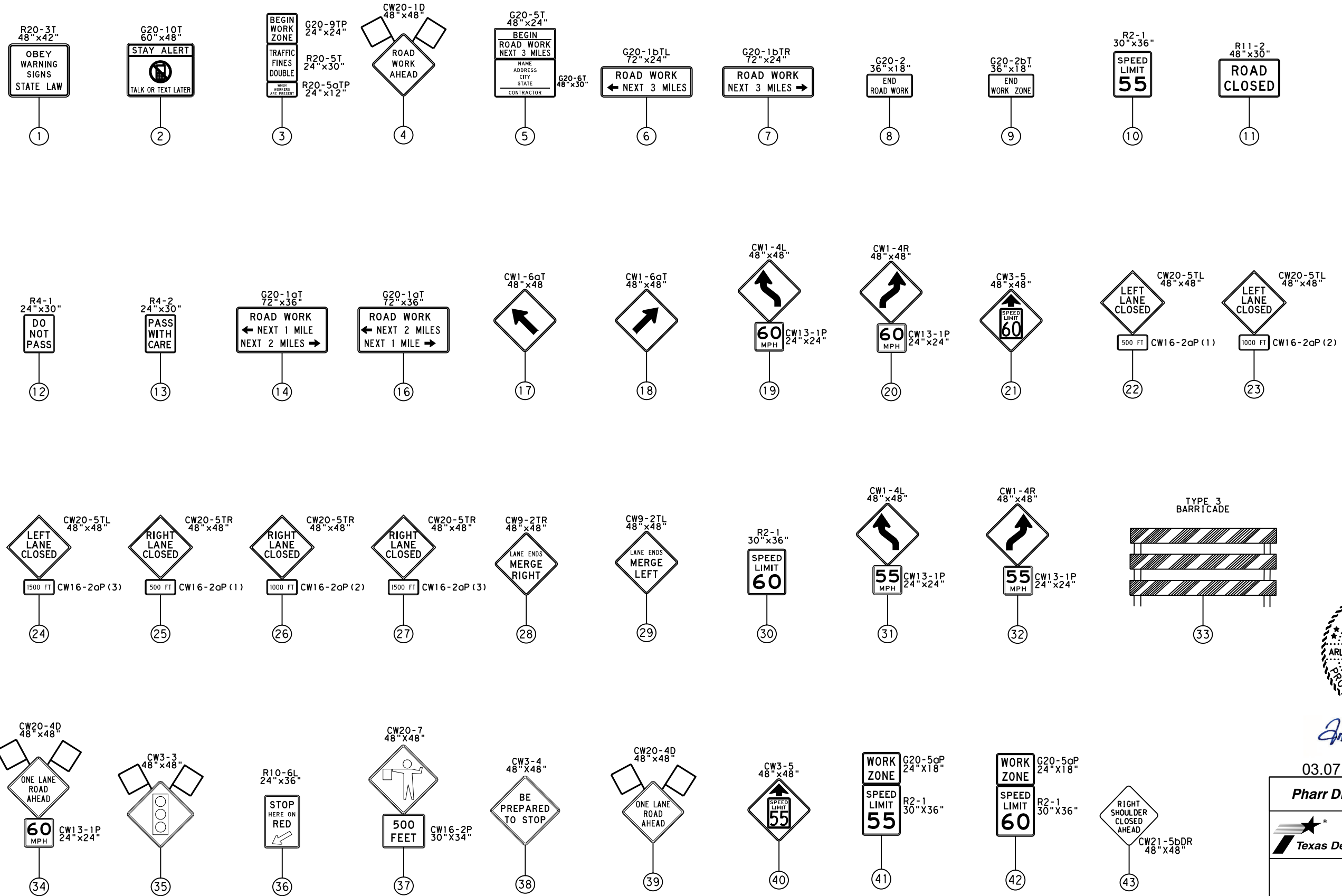


SH 285
TCP
SEQUENCE OF
CONSTRUCTION

SHEET 5 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
		DIST	COUNTY	SHEET NO.
	PHR		JIM HOGG	45

DATE: 3/4/2024 4:57:08 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP SIGN INVENTORY.dgn



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation
























**SH 285
TCP SIGN INVENTORY**

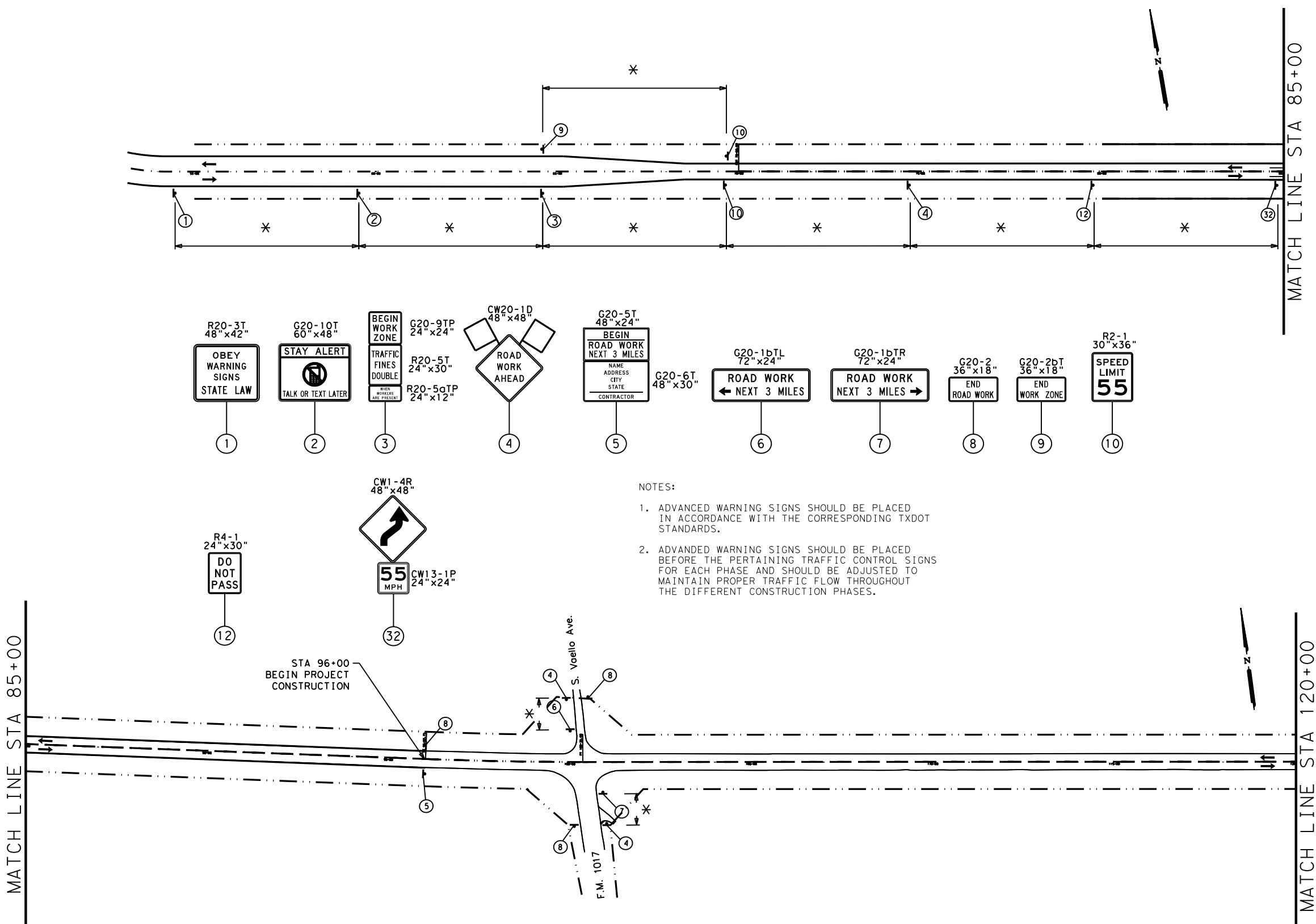
SHEET 1 OF 1

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	46

DATE: 3/4/2024 4:57:16 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 AWS.dgn

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG




- NOTES:
1. ADVANCED WARNING SIGNS SHOULD BE PLACED IN ACCORDANCE WITH THE CORRESPONDING TXDOT STANDARDS.
 2. ADVANCED WARNING SIGNS SHOULD BE PLACED BEFORE THE PERTAINING TRAFFIC CONTROL SIGNS FOR EACH PHASE AND SHOULD BE ADJUSTED TO MAINTAIN PROPER TRAFFIC FLOW THROUGHOUT THE DIFFERENT CONSTRUCTION PHASES.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

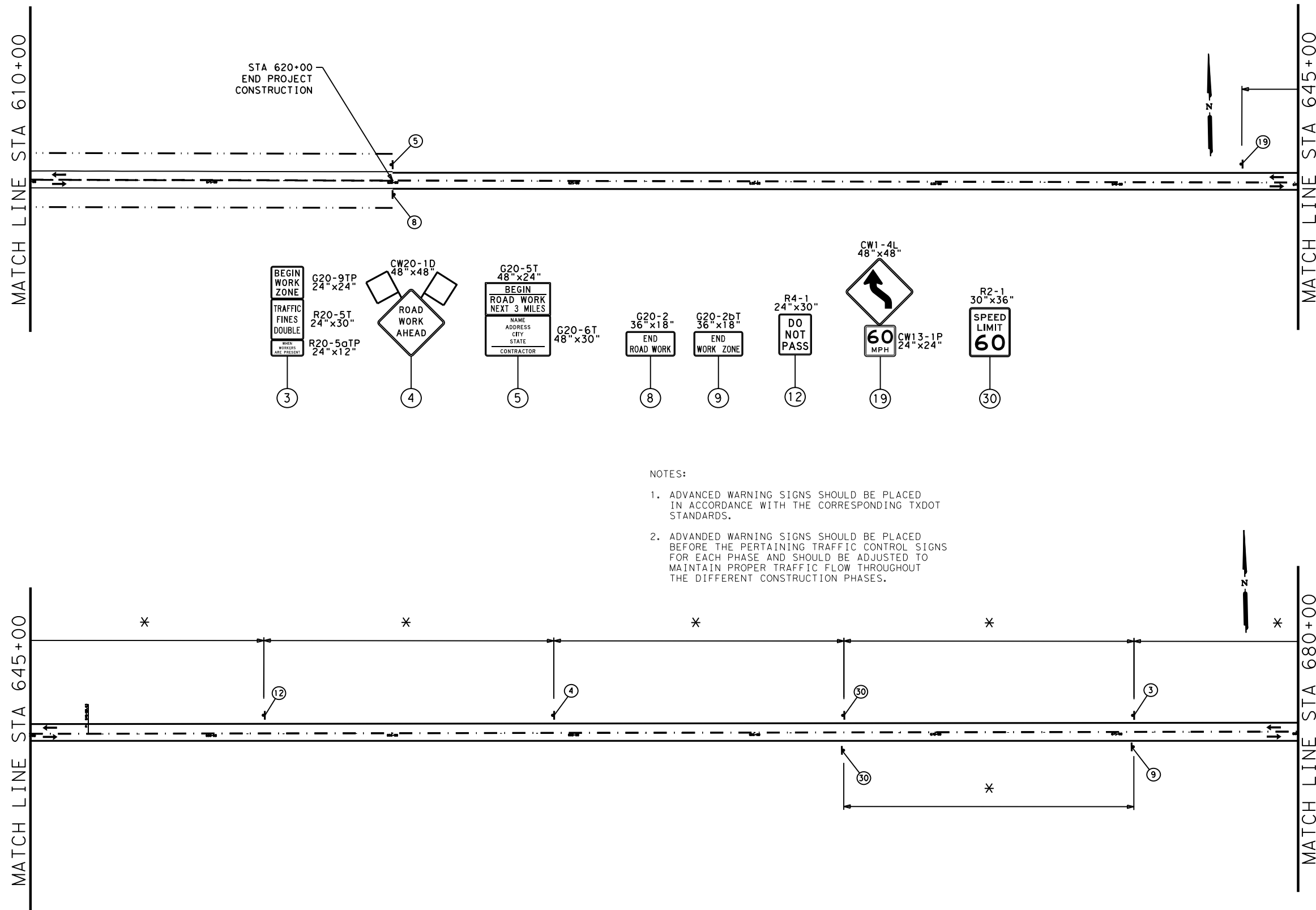
 **Texas Department of Transportation**

**SH 285
TCP
ADVANCED WARNING
SIGNS LAYOUT**

1" = 300' SHEET 1 OF 3

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
		DIST	COUNTY	SHEET NO.
	PHR		JIM HOGG	47

DATE: 3/4/2024 4:57:18 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 AWS.dgn



NOTES:

1. ADVANCED WARNING SIGNS SHOULD BE PLACED IN ACCORDANCE WITH THE CORRESPONDING TXDOT STANDARDS.
2. ADVANCED WARNING SIGNS SHOULD BE PLACED BEFORE THE PERTAINING TRAFFIC CONTROL SIGNS FOR EACH PHASE AND SHOULD BE ADJUSTED TO MAINTAIN PROPER TRAFFIC FLOW THROUGHOUT THE DIFFERENT CONSTRUCTION PHASES.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design

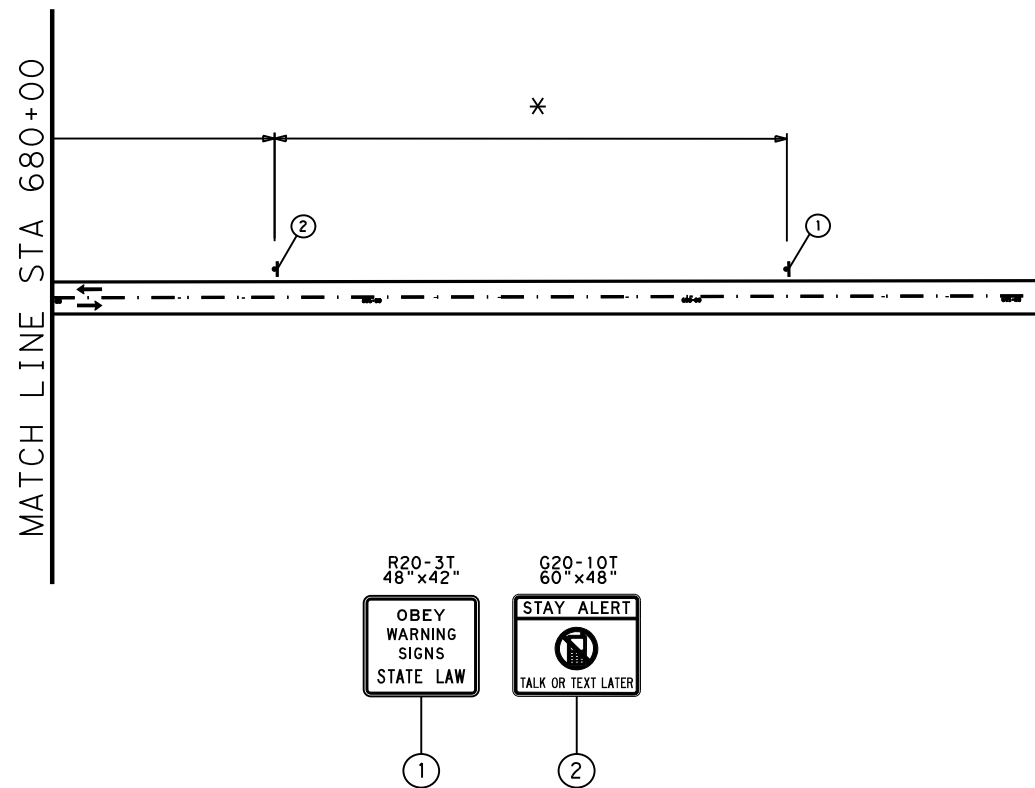


SH 285
 TCP
 ADVANCED WARNING
 SIGNS LAYOUT

1" = 300' SHEET 2 OF 3

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
PHR	JIM HOGG		48

DATE: 3/4/2024 4:57:21 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 AWS.dgn



NOTES:

1. ADVANCED WARNING SIGNS SHOULD BE PLACED IN ACCORDANCE WITH THE CORRESPONDING TXDOT STANDARDS.
2. ADVANCED WARNING SIGNS SHOULD BE PLACED BEFORE THE PERTAINING TRAFFIC CONTROL SIGNS FOR EACH PHASE AND SHOULD BE ADJUSTED TO MAINTAIN PROPER TRAFFIC FLOW THROUGHOUT THE DIFFERENT CONSTRUCTION PHASES.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK_ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK_ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK_ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK_ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK_ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK_ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK_ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK_ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK_ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK_ZONE PVMT MARK (REM) (W) (ARROW)
- WORK_ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 ADVANCED WARNING
 SIGNS LAYOUT


1" = 300' SHEET 3 OF 3

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
		DIST	COUNTY	SHEET NO.
	PHR		JIM HOGG	49

DATE: 3/4/2024 4:57:26 PM
 FILE: \\phr\dot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP ESTIMATED QUANTITIES

LOCATION	SHEET	FROM STATION	TO STATION	SUMMARY OF DETOUR		SUMMARY OF PORTABLE CONCRETE TRAFFIC BARRIER			SUMMARY OF CRASH CUSHION ATTENUATORS			SUMMARY OF WORK ZONE PAVEMENT MARKINGS								SUMMARY OF ELIMINATING EXISTING MARKINGS AND MARKERS						PAV SURF		
				354	508	512			545			6001	662								677						678	
				6045	6001	6001	6025	6049	6003	6005	6019	6002	6067	6071	6080	6090	6098	6096	6075	6050	6008	6037	6001	6003	6005	6007	6008	6012
PLAN E ASPH CONC PAV (2")	CONST RUCTIN G DETOUR	PORT CTB (FUR & INST) (SGL SLP) (TY I)	PORT CTB (MOVE) (SGL SLP) (TY I)	PORT CTB (REMOVE) (SGL SLP) (TY I)	CRASH CUSH ATTEN (MOVE & RESE T)	CRASH CUSH ATTEN (REMO VE)	CRASH CUSH ATTEN (INS TL) (S) (N) (TL3)	PORT. CHANG. MESS AGE SIGN	6" (W) SLD	8" (W) SLD	(W) ARROW	(W) WORD	6" (Y) SLD	6" (Y) BRK	24" (W) SLD	PAV MRK TY II-A-A	6" (W) SLD	WK ZN PAV MRK REMOV (Y) 6" (SLD)	ELIM EXT PAV MRK & MRKS (4")	ELIM EXT MRK & MRKS (8")	ELIM EXT MRK & MRKS (12")	ELIM EXT MRK & MRKS (24")	ELIM EXT PAV MRK & MRKS (AR ROW)	ELIM EXT PAV MRK & MRKS (WO RD)	PAV SURF PREP FOR MRK " 4"			
SY	SY	LF	LF	LF	EA	EA	EA	EA	LF	LF	EA	EA	LF	LF	LF	EA	LF	LF	LF	LF	LF	LF	EA	EA	EA			
PHASE 1																												
STEP 1	2 OF 5	160+00	183+00																									
	3 OF 5	183+00	207+00																									
	4 OF 5	207+00	231+00			2,145																						
	5 OF 5	231+00	243+00			1065																						
STEP 2	2 OF 5	162+00	186+00																									
	3 OF 5	186+00	210+00																									
	4 OF 5	210+00	234+00				1,990																					
	5 OF 5	234+00	258+00				1,220																					
PHASE 1 TOTAL				0	0	3,210	3,210	0	4	0	8	0	10,391														810	
PHASE 2																												
STEP 1	1 OF 4	69+00	93+00																									
	2 OF 4	93+00	166+00																									
	3 OF 4	166+00	180+00																									
	4 OF 4	180+00	204+00																									
STEP 2	1 OF 5	69+00	93+00																									
	2 OF 5	93+00	117+00																									
	3 OF 5	117+00	134+00																									
	4 OF 5	134+00	158+00																									
	5 OF 5	158+00	180+00																									
PHASE 2 TOTAL				0	14,779	5,940	12,060	0	27	0	4	0	38,324	82	1	1	38,224	0	100	1,353	25,840	25,840	32,526	0	0	0	0	0
PHASE 3																												
STEP 1	1 OF 4	186+00	210+00																									
	2 OF 4	210+00	315+00																									
	3 OF 4	319+00	343+00																									
	4 OF 4	343+00	350+00																									
STEP 2	1 OF 4	190+00	214+00																									
	2 OF 4	214+00	226+00																									
		226+00	262+00																									
		262+00	274+00																									
	3 OF 4	308+00	332+00																									
STEP 3	1 OF 4	190+00	214+00																									
	2 OF 4	214+00	226+00																									
		226+00	262+00																									
		262+00	274+00																									
	4 OF 4	325+00	333+00																									
PHASE 3 TOTAL				0	14,093	150	17,130	0	14	0	0	0	33,660	0	0	0	33,660	220	0	1,286	17,980	17,980	27,060	0	0	0	0	

Pharr District Central Design



**SH 285
TCP
ESTIMATED
QUANTITIES**


SHEET 1 OF 2

© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	SH 285
PHR	JIM HOGG			SHEET NO. 50

DATE: 3/4/2024 4:57:27 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP ESTIMATED QUANTITIES

LOCATION	SHEET	FROM STATION	TO STATION	SUMMARY OF DETOUR		SUMMARY OF PORTABLE CONCRETE TRAFFIC BARRIER					SUMMARY OF CRASH CUSHION ATTENUATORS			SUMMARY OF WORK ZONE PAVEMENT MARKINGS										SUMMARY OF ELIMINATING EXISTING MARKINGS AND MARKERS					PAV SURF
				354	508	512					545			662										677					678
				6045	6001	6001	6025	6049	6003	6005	6019	6002	6067	6071	6080	6090	6098	6096	6075	6050	6008	6037	6001	6003	6005	6007	6008	6012	6002
PLAN E ASPH CONC PAV (2")	CONSTRUCTING DETOUR	PORT CTB (FUR & INST) (SGL SLP) (TY)	PORT CTB (MOVE) (SGL SLP) (TY I)	PORT CTB (REMOVE) (SGL SLP) (TY I)	CRASH CUSH ATTEN (MOVE & RESET)	CRASH CUSH ATTEN (REMOVE)	CRASH CUSH ATTEN (INSTALL) (S) (N) (TL3)	PORT. CHANG. MESSAGE SIGN	6" (W) SLD	8" (W) SLD	(W) ARROW	(W) WORD	6" (Y) SLD	6" (Y) BRK	24" (W) SLD	PAV MRK TY II-A-A	6" (W) SLD	WK ZN PAV MRK REMOV (Y) 6"	ELIM EXT PAV MRK & MRKS (4")	ELIM EXT PAV MRK & MRKS (8")	ELIM EXT PAV MRK & MRKS (12")	ELIM EXT PAV MRK & MRKS (24")	ELIM EXT PAV MRK & MRKS (AR ROW)	ELIM EXT PAV MRK & MRKS (WO RD)	PAV SURF PREP FOR MRK "4"				
SY	SY	LF	LF	LF	EA	EA	EA	EA	LF	LF	EA	EA	LF	LF	LF	EA	LF	LF	LF	LF	LF	LF	EA	EA					
PHASE 4																													
STEP 2	1 OF 4	283+00	307+00							3,320				3,320			84			6,640									
	2 OF 4	307+00	363+00					3		1,100				1,100			280	10,100	10,100	6,700									
	3 OF 4	363+00	434+00		2,850	4,120		3																					
	4 OF 4	434+00	493+00		4,800			6			1,720			1,720			710	24,280	24,280	14,720									
STEP 3	1 OF 4	281+00	305+00							2,700				2,700			65			5,400									
	2 OF 4	305+00	363+00			5,510		9		11,600				11,600			290			2,400									
	3 OF 4	363+00	435+00			7,100		2																					
	4 OF 4	435+00	486+00			360	3,940	1		24,600				24,600			616												
PHASE 4 TOTAL				0	22,932	8,010	25,550	0	24	4	0	0	46,480	0	0	0	47,160	0	0	2,083	34,380	34,380	35,860	0	0	0	0		
PHASE 5																													
STEP 1	1 OF 4	470+00	494+00																										
	2 OF 4	509+00	585+00																										
	3 OF 4																												
	4 OF 4																												
STEP 2	1 OF 4	455+00	479+00											3,340			84			6,680									
	2 OF 4	479+00	535+00			5,120		3		1,080				1,080			280	10,120	10,120	6,680									
	3 OF 4	535+00	585+00			4,940		2																					
	4 OF 4					60				2,020				2,020			305	10,000	10,000	7,465									
STEP 3	1 OF 4	452+00	476+00							1,020				1,700			26			2,040									
	2 OF 4	476+00	535+00			5,700		3	5	11,800				11,800			295												
	3 OF 4	535+00	585+00			4,850		3																					
	4 OF 4					60				12,020				12,020			300												
PHASE 5 TOTAL				0	13,581	0	20,730	17,310	11	5	0	0	33,500	0	0	0	34,180	0	0	1,346	20,120	20,120	27,305	0	0	0	0		
CSJ 0482-01-036 TOTAL		0	65,385	17,310	78,680	17,310	80	9	12	0	0	162,355	82	1	1	170,664	220	140	6,506	103,330	98,320	138,841	0	0	0	0	0	810	
PHASE 5																													
STEP 1	1 OF 4																												
	2 OF 4	585+00	607+00																										
	3 OF 4	607+00	631+00																										
	4 OF 4	631+00	651+00																										
STEP 2	1 OF 4																												
	2 OF 4																												
	3 OF 4	585+00	607+00			2,130		3	2																				
	4 OF 4	607+00	631+00			1,300											175	7,000	7,000	4,355									
STEP 3	1 OF 4																												
	2 OF 4																												
	3 OF 4	585+00	607+00			2,130																							
	4 OF 4	607+00	631+00			1,300								7,000			7,000			175									
CSJ 0482-02-035 TOTAL		0	4,803	0	6,860	0	4	3	0	0	0	7,000	0	0	0	7,000	0	0	350	7,000	7,000	4,355	0	0	0	0	0		
PROJECT TOTAL		0	70,188	17,310	85,540	17,310	84	12	12	0	0	169,355	82	1	1	177,664	220	140	6,856	110,330	105,320	143,196	0	0	0	0	810		

Pharr District Central Design



**SH 285
TCP
ESTIMATED
QUANTITIES**

SHEET 2 OF 2

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		51

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

LOC. No.	TCP PHASE	PLAN SHEET NUMBER	LOCATION	STATION	TEST LEVEL	DIRECTION OF TRAFFIC (UNI/BI)	FOUNDATION PAD		BACKUP SUPPORT			AVAILABLE SITE LENGTH	CRASH CUSHION										
							PROPOSED MATERIAL	PROPOSED THICKNESS	DESCRIPTION	WIDTH	HEIGHT		INSTALL	REMOVE	MOVE/RESET	FROM LOC. #	L N	L W	R N	R W	S N	S W	
1*	PHASE 1 - STEP 1	63	WESTBOUND SH 285	142+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
2*	PHASE 1 - STEP 1	63	WESTBOUND SH 285	143+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
1	PHASE 1 - STEP 1	66	WESTBOUND SH 285	212+85	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
2	PHASE 1 - STEP 1	66	WESTBOUND SH 285	214+35	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
3	PHASE 1 - STEP 2	68	EASTBOUND SH 285	142+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1#							X	
4	PHASE 1 - STEP 2	68	EASTBOUND SH 285	143+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	2#							X	
5	PHASE 1 - STEP 2	70	EASTBOUND SH 285	188+60	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1							X	
6	PHASE 1 - STEP 2	70	EASTBOUND SH 285	190+40	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	2							X	
7	PHASE 1 - STEP 2	70	EASTBOUND SH 285	191+90	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
8	PHASE 1 - STEP 2	70	EASTBOUND SH 285	213+20	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
9	PHASE 1 - STEP 2	70	EASTBOUND SH 285	214+70	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
10	PHASE 1 - STEP 2	70	EASTBOUND SH 285	223+40	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
11	PHASE 2 - STEP 2	82	EASTBOUND SH 285	95+40	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	3							X	
12	PHASE 2 - STEP 2	82	EASTBOUND SH 285	99+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	4							X	
12A	PHASE 2 - STEP 2		EASTBOUND SH 285	100+14	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	12							X	
12B	PHASE 2 - STEP 2		FM 1017 INTERSECTION	*	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
12C	PHASE 2 - STEP 2		FM 1017 INTERSECTION	*	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"	1									X	
12D	PHASE 2 - STEP 2		EASTBOUND SH 285	102+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	5							X	
12E	PHASE 2 - STEP 2		EASTBOUND SH 285	99+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	12A							X	
12F	PHASE 2 - STEP 2		FM 1017 INTERSECTION	*	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		-	12B							X	
12G	PHASE 2 - STEP 2		FM 1017 INTERSECTION	*	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		-	12C							X	
12H	PHASE 2 - STEP 2		EASTBOUND SH 285	100+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	12D							X	
13	PHASE 2 - STEP 2	82	EASTBOUND SH 285	102+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	12H							X	
14	PHASE 2 - STEP 2	82	EASTBOUND SH 285	104+40	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	6							X	
15	PHASE 2 - STEP 2	82	EASTBOUND SH 285	106+05	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	7							X	
16	PHASE 2 - STEP 2	82	EASTBOUND SH 285	106+95	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	12F							X	
17	PHASE 2 - STEP 2	82	EASTBOUND SH 285	108+45	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	12G							X	
												SHEET TOTALS	10	-	17								

LEGEND:
 L=LOW MAINTENANCE
 R=REUSABLE
 S=SACRIFICIAL
 N=NARROW
 W=WIDE

*MOVE TO CONTRACTOR'S STOCKPILE LOCATION TEMPORARILY.
 TO BE PAID UNDER ITEM 545-6003

FOR DEFINITIONS SEE THE "CRASH CUSHION CATEGORIZATION CHART.PDF" AT THE DESIGN DIVISION (ROADWAY STANDARDS) WEBSITE. USE QUICK LINKS TO ACCESS ATTENUATORS / CRASH CUSHIONS SECTION.
<http://www.dot.state.tx.us/insdot/orgchart/cmd/cserve/standard/rdwylse.htm>

CRASH CUSHION SUMMARY SHEET

FILE: CCSS.dgn	DN: TxDOT	CK:	CK:
© TxDOT	CONT	SECT	JOB
REVISIONS	0482	01036, etc.	SH 285
	DIST	COUNTY	
	PHR	JIM HOGG	
	FEDERAL AID PROJECT		SHEET NO.
			52

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

LOC. NO.	TCP PHASE	PLAN SHEET NUMBER	LOCATION	STATION	TEST LEVEL	DIRECTION OF TRAFFIC (UNI/BI)	FOUNDATION PAD		BACKUP SUPPORT			AVAILABLE SITE LENGTH	CRASH CUSHION										
							PROPOSED MATERIAL	PROPOSED THICKNESS	DESCRIPTION	WIDTH	HEIGHT		INSTALL	REMOVE	MOVE/RESET		L N	L W	R N	R W	S N	S W	
															MOVE/RESET	FROM LOC. #							
18	PHASE 2 - STEP 2	82	EASTBOUND SH 285	108+75	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	8						X	
19	PHASE 2 - STEP 2	82	EASTBOUND SH 285	110+40	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	9						X	
20	PHASE 2 - STEP 2		EASTBOUND SH 285	168+15	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	10						X	
21	PHASE 2 - STEP 2		EASTBOUND SH 285	169+35	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"	1		1*							X	
22	PHASE 2 - STEP 2	84	EASTBOUND SH 285	189+75	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"	1		1*							X	
23	PHASE 2 - STEP 3	91	WESTBOUND SH 285	95+40	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	11						X	
24	PHASE 2 - STEP 3	91	WESTBOUND SH 285	99+60	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	12E						X	
25	PHASE 2 - STEP 3	91	WESTBOUND SH 285	101+10	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	13						X	
26	PHASE 2 - STEP 3	91	WESTBOUND SH 285	106+80	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	14						X	
27	PHASE 2 - STEP 3	91	WESTBOUND SH 285	108+20	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	15						X	
28	PHASE 2 - STEP 3		WESTBOUND SH 285	129+20	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	16						X	
29	PHASE 2 - STEP 3		WESTBOUND SH 285	130+70	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	17						X	
30	PHASE 2 - STEP 3		WESTBOUND SH 285	168+50	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	18						X	
31	PHASE 2 - STEP 3		WESTBOUND SH 285	170+00	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	19						X	
32	PHASE 2 - STEP 3	93	WESTBOUND SH 285	189+80	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1*	20						X	
33	PHASE 3 - STEP 2	103	EASTBOUND SH 285	221+90	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	21						X	
34	PHASE 3 - STEP 2		EASTBOUND SH 285	241+70	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	22						X	
35	PHASE 3 - STEP 2		EASTBOUND SH 285	242+90	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	23						X	
36	PHASE 3 - STEP 2	104	EASTBOUND SH 285	313+10	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	24						X	
37	PHASE 3 - STEP 3	107	WESTBOUND SH 285	215+00	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	33						X	
38	PHASE 3 - STEP 3	108	WESTBOUND SH 285	271+40	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	34						X	
39	PHASE 3 - STEP 3	108	WESTBOUND SH 285	272+70	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	35						X	
40	PHASE 3 - STEP 3	108	WESTBOUND SH 285	306+00	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	36						X	
41	PHASE 4 - STEP 2	117	EASTBOUND SH 285	311+90	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	37						X	
42	PHASE 4 - STEP 2		EASTBOUND SH 285	325+10	3	UNI			PCTB	24"	3' - 6"	20' - 11 3/4"			1	38						X	
												SHEET TOTALS	2	-	31								

*MOVE TO CONTRACTOR'S STOCKPILE LOCATION TEMPORARILY. TO BE PAID UNDER ITEM 545-6003

CRASH CUSHION SUMMARY SHEET

FOR DEFINITIONS SEE THE "CRASH CUSHION CATEGORIZATION CHART.PDF" AT THE DESIGN DIVISION (ROADWAY STANDARDS) WEBSITE. USE QUICK LINKS TO ACCESS ATTENUATORS / CRASH CUSHIONS SECTION.
<http://www.dot.state.tx.us/insdot/orgchart/cmd/cserve/standard/rdwylse.htm>

FILE: CCSS.dgn	DN: TxDOT	CK:	CK:
© TxDOT	CONT	SECT	JOB
REVISIONS	0482	01036, etc.	SH 285
	DIST	COUNTY	
	PHR	JIM HOGG	
	FEDERAL AID PROJECT	SHEET NO.	
			53

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

LOC. No.	TCP PHASE	PLAN SHEET NUMBER	LOCATION	STATION	TEST LEVEL	DIRECTION OF TRAFFIC (UNI/BI)	FOUNDATION PAD		BACKUP SUPPORT			AVAILABLE SITE LENGTH	CRASH CUSHION										
							PROPOSED MATERIAL	PROPOSED THICKNESS	DESCRIPTION	WIDTH	HEIGHT		INSTALL	REMOVE	MOVE/RESET	FROM LOC. #	L N	L W	R N	R W	S N	S W	
43	PHASE 4 - STEP 2		EASTBOUND SH 285	326+60	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	39						X	
44	PHASE 4 - STEP 2		EASTBOUND SH 285	406+90	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	40						X	
45	PHASE 4 - STEP 2		EASTBOUND SH 285	407+50	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	25						X	
46	PHASE 4 - STEP 2		EASTBOUND SH 285	433+30	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	26						X	
47	PHASE 4 - STEP 2	118	EASTBOUND SH 285	434+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	27						X	
48	PHASE 4 - STEP 2		EASTBOUND SH 285	466+70	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	28						X	
49	PHASE 4 - STEP 2		EASTBOUND SH 285	468+20	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1*	29						X	
50	PHASE 4 - STEP 2		EASTBOUND SH 285	479+30	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1*	30						X	
51	PHASE 4 - STEP 2		EASTBOUND SH 285	480+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1*	31						X	
52	PHASE 4 - STEP 2	118	EASTBOUND SH 285	485+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1*	32						X	
53	PHASE 4 - STEP 3	120	WESTBOUND SH 285	305+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	41						X	
54	PHASE 4 - STEP 3		WESTBOUND SH 285	320+60	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	42						X	
55	PHASE 4 - STEP 3		WESTBOUND SH 285	322+05	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	43						X	
56	PHASE 4 - STEP 3		WESTBOUND SH 285	324+75	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	44						X	
57	PHASE 4 - STEP 3		WESTBOUND SH 285	326+20	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1*	45						X	
58	PHASE 4 - STEP 3		WESTBOUND SH 285	433+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1*	46						X	
59	PHASE 4 - STEP 3		WESTBOUND SH 285	434+50	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	47						X	
60	PHASE 4 - STEP 3	122	WESTBOUND SH 285	478+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	48						X	
61	PHASE 5 - STEP 2	133	EASTBOUND SH 285	483+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	49						X	
62	PHASE 5 - STEP 2		EASTBOUND SH 285	542+60	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	50						X	
63	PHASE 5 - STEP 2		EASTBOUND SH 285	544+20	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	51						X	
64	PHASE 5 - STEP 2		EASTBOUND SH 285	588+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	52						X	
65	PHASE 5 - STEP 2		EASTBOUND SH 285	588+50	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	59						X	
66	PHASE 5 - STEP 2	134	EASTBOUND SH 285	620+60	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"			1	60						X	
67	PHASE 5 - STEP 3	137	WESTBOUND SH 285	476+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	61						X	
68	PHASE 5 - STEP 3	137	WESTBOUND SH 285	482+80	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	62						X	
69	PHASE 5 - STEP 3	137	WESTBOUND SH 285	484+00	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	63						X	
70	PHASE 5 - STEP 3		WESTBOUND SH 285	541+90	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	64						X	
71	PHASE 5 - STEP 3		WESTBOUND SH 285	543+20	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	57						X	
72	PHASE 5 - STEP 3	138	WESTBOUND SH 285	620+60	3	UNI			PCTB	24"	3'-6"	20' -11 3/4"		1	1	58						X	
												SHEET TOTALS	0	12	36								

*MOVE TO CONTRACTOR'S STOCKPILE LOCATION TEMPORARILY. TO BE PAID UNDER ITEM 545-6003

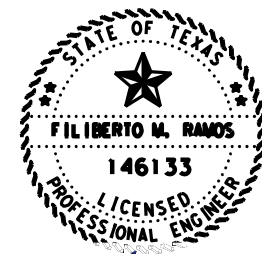
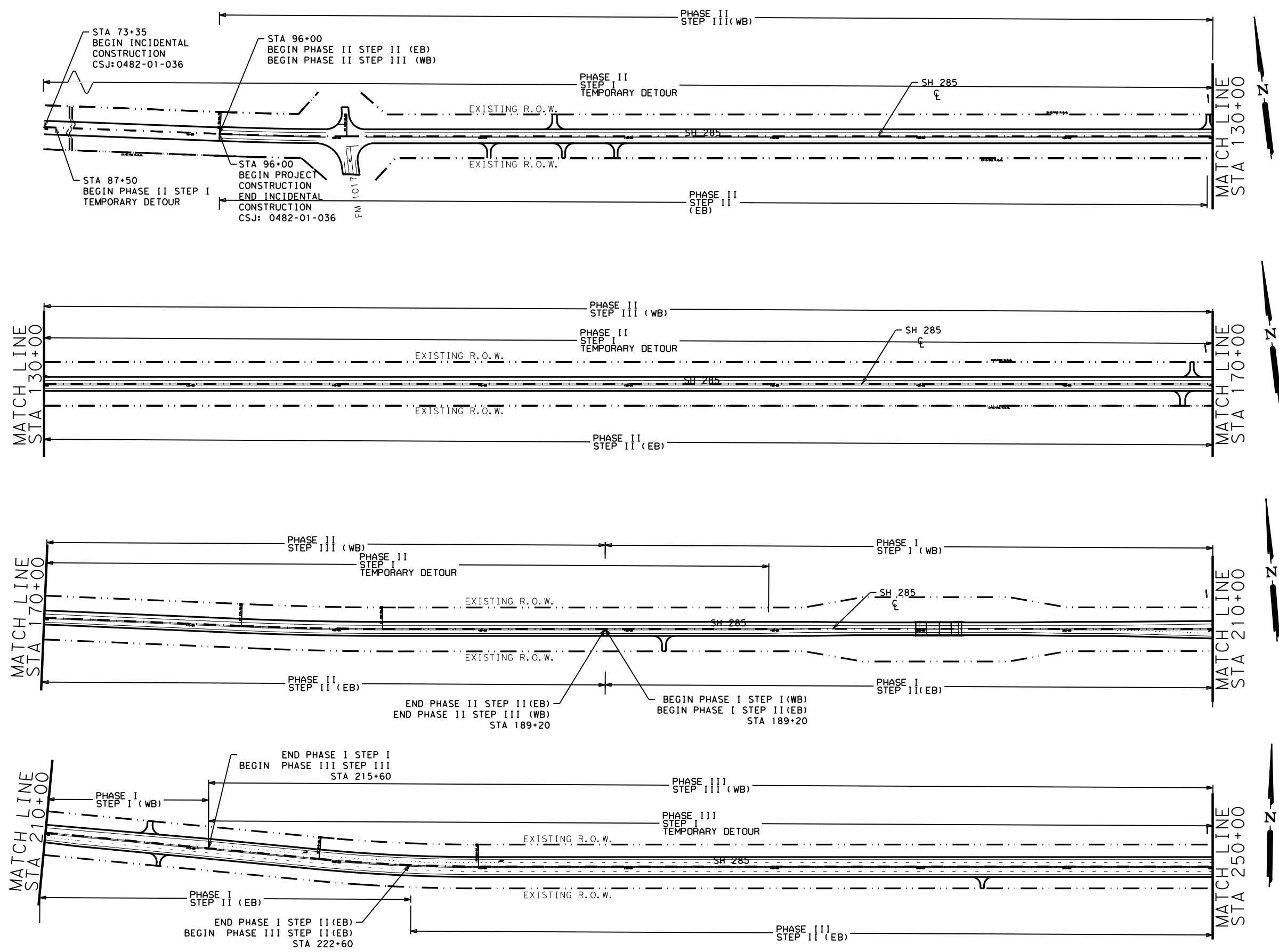
LEGEND:
 L=LOW MAINTENANCE
 R=REUSABLE
 S=SACRIFICIAL
 N=NARROW
 W=WIDE

FOR DEFINITIONS SEE THE "CRASH CUSHION CATEGORIZATION CHART.PDF" AT THE DESIGN DIVISION (ROADWAY STANDARDS) WEBSITE. USE QUICK LINKS TO ACCESS ATTENUATORS / CRASH CUSHIONS SECTION.
<http://www.dot.state.tx.us/insdot/orgchart/cmd/cserve/standard/rdwlyse.htm>

CRASH CUSHION SUMMARY SHEET

FILE: CCSS.dgn	DN: TxDOT	CK:	CK:
© TxDOT	CONT	SECT	JOB
REVISIONS	0482	01036, etc.	SH 285
	DIST	COUNTY	
	PHR	JIM HOGG	
	FEDERAL AID PROJECT	SHEET NO.	
			54

DATE: 3/4/2024 4:57:39 PM
 FILE: \\txdot\projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 PHASE LAYOUT



[Signature]
 6/2024

Pharr District Central Design

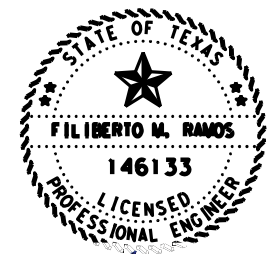
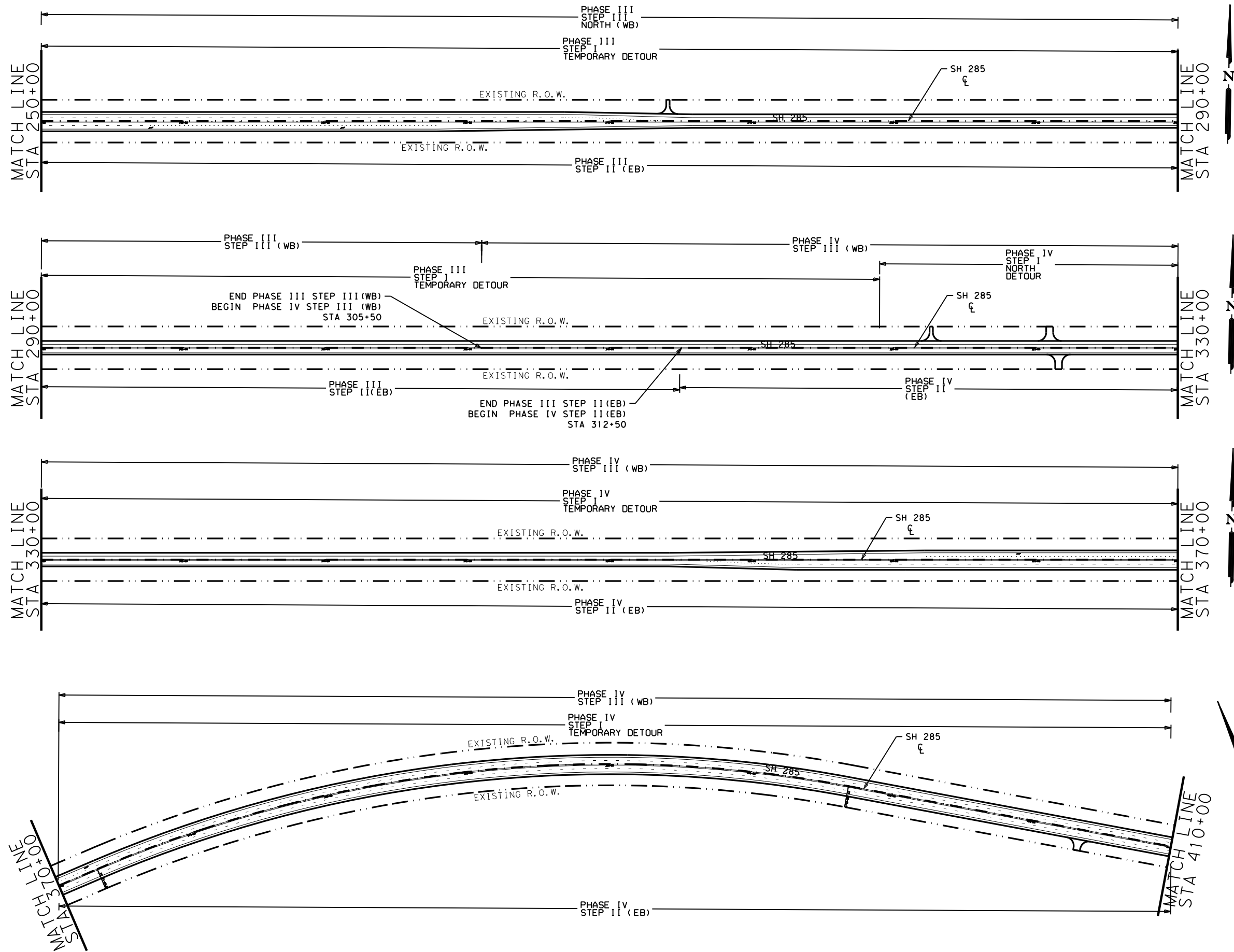
Texas Department of Transportation

**SH 285
 TCP
 PHASE LAYOUT**

SCALE = 1" = 350' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		55

DATE: 3/4/2024 4:57:41 PM
 FILE: P:\xtdot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 PHASE LAYOUT



Filiberto M. Ramos
 6/2024

Pharr District Central Design

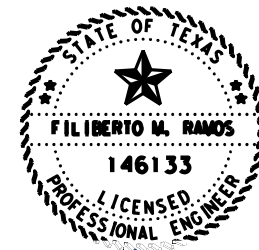
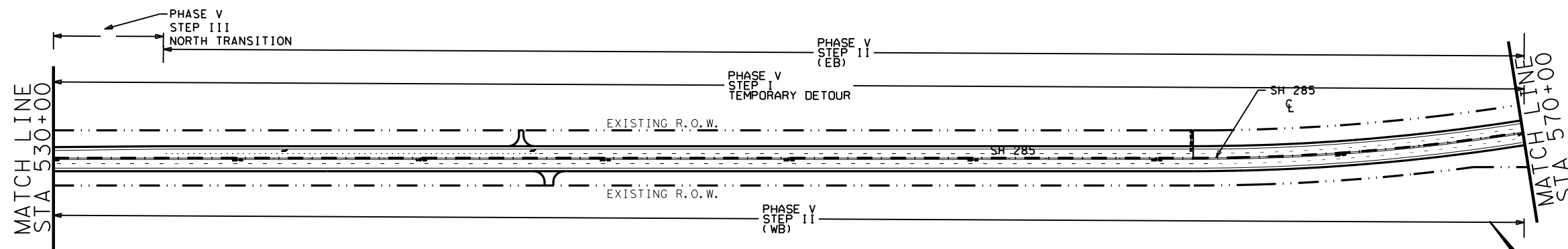
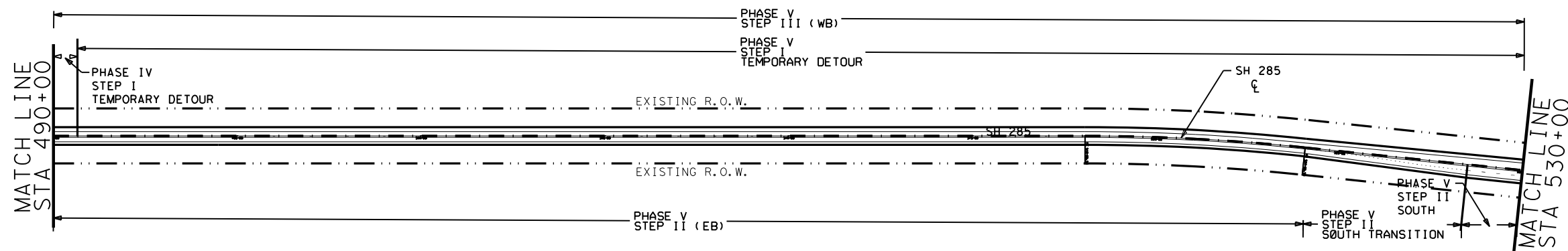
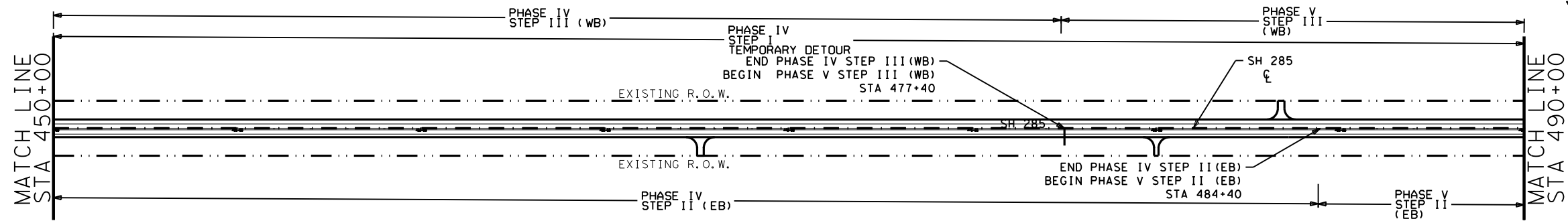
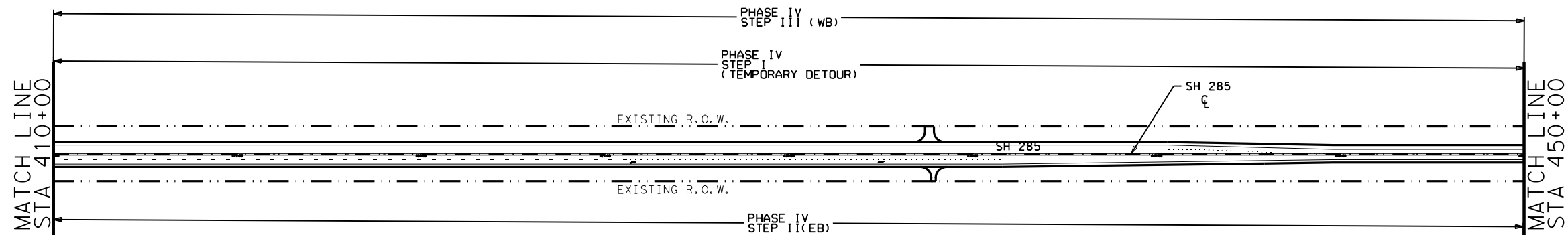
Texas Department of Transportation

**SH 285
 TCP
 PHASE LAYOUT**

SCALE = 1" = 350' SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	56

DATE: 3/4/2024 4:57:43 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 PHASE LAYOUT



Filiberto M. Ramos
 02/16/2024

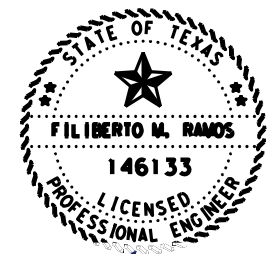
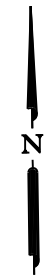
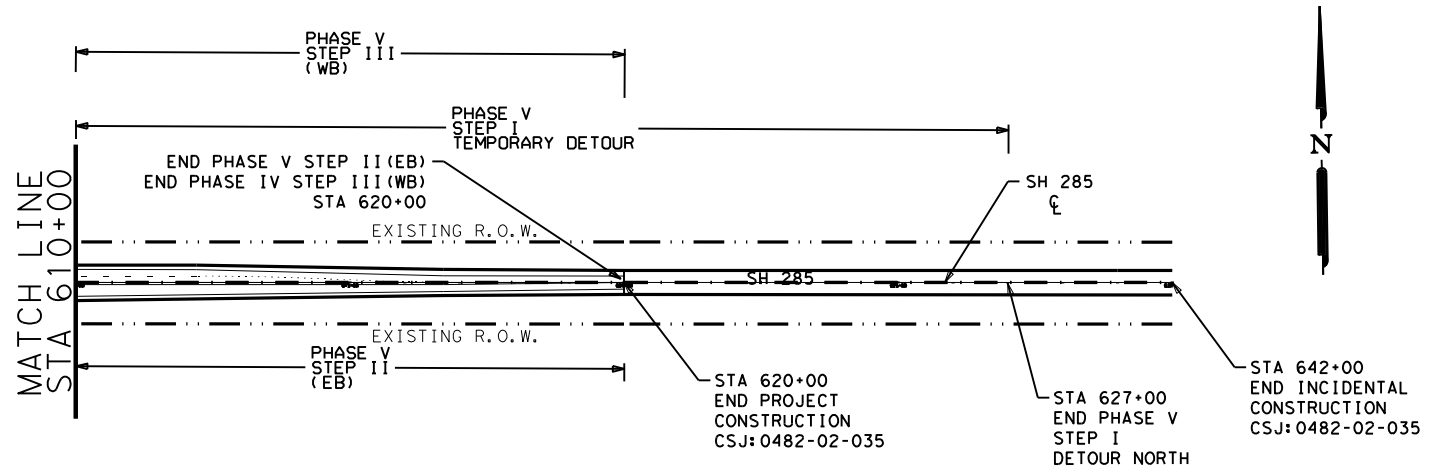
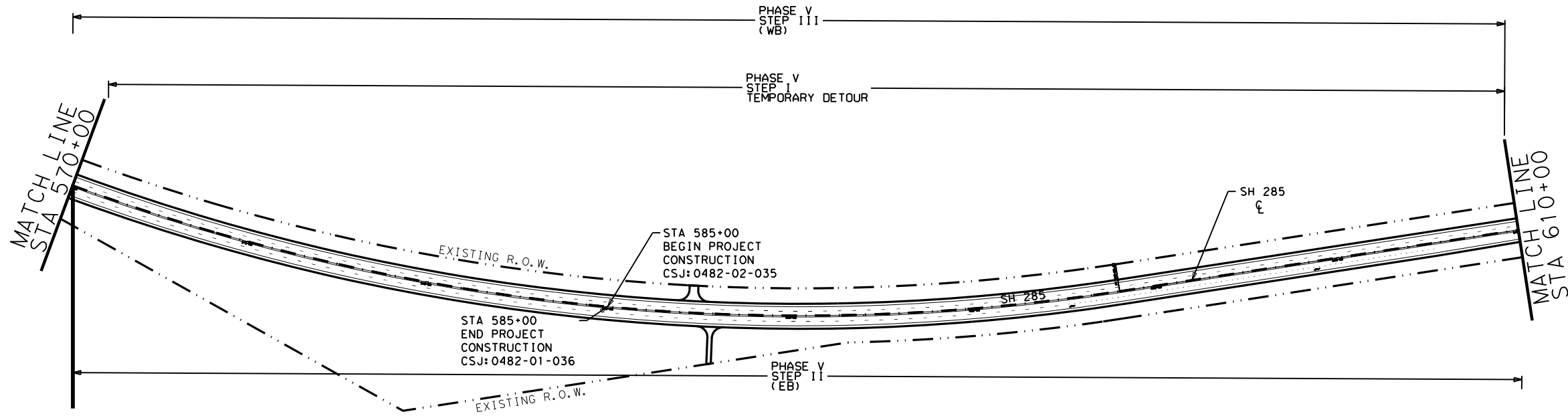
Pharr District Central Design
 Texas Department of Transportation

SH 285
 TCP
 PHASE LAYOUT

SCALE= 1"=350' SHEET 3 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	57

DATE: 3/4/2024 4:57:46 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 PHASE LAYOUT



[Signature]
 6/2024

Pharr District Central Design

Texas Department of Transportation

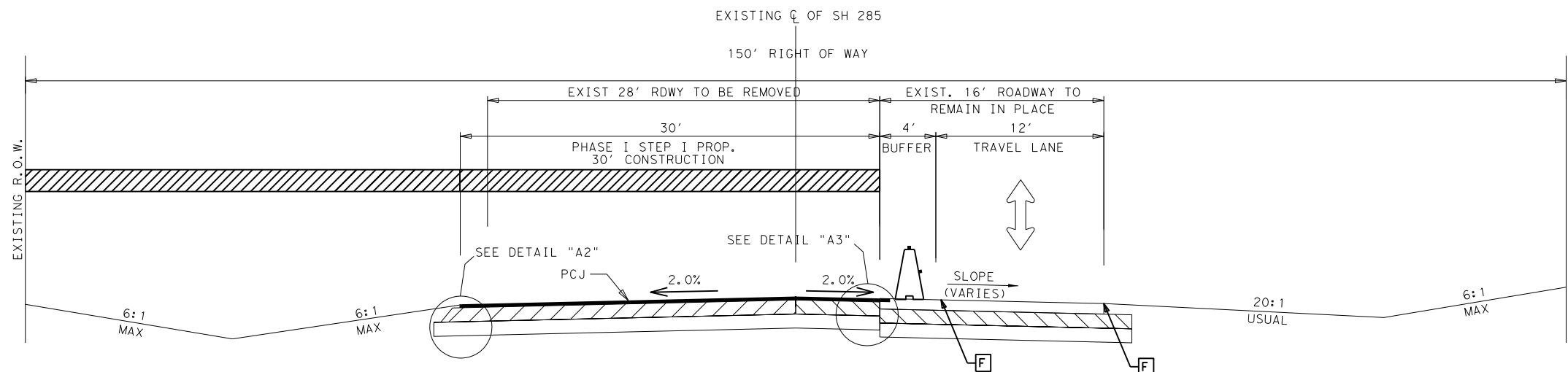
**SH 285
 TCP
 PHASE LAYOUT**

SCALE = 1" = 350' SHEET 4 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	58

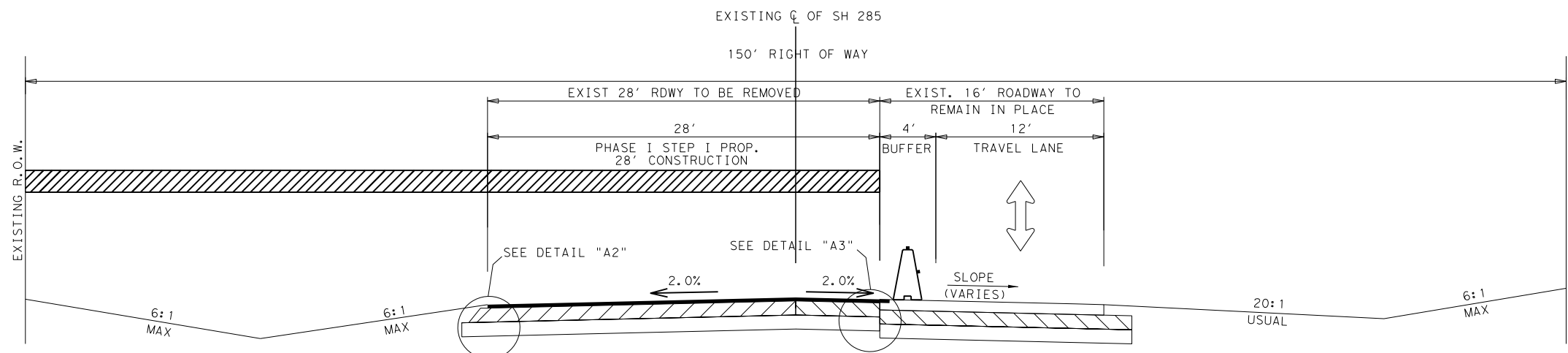
PHASE VI LIMITS ARE PROJECT LIMITS

DATE: 3/4/2024 4:57:58 PM
 FILE: \\phr\dot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH285 TCP PHASE I (BRIDGE) TYPICAL



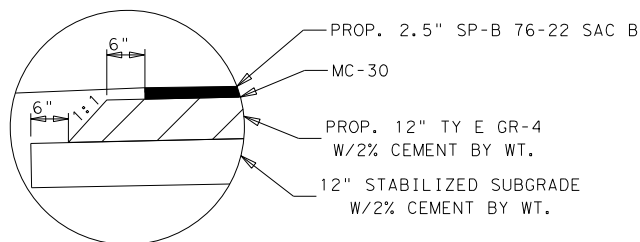
**SH 285
 PHASE I STEP I CONSTRUCTION**

STA 189+20 TO STA 196+25
 STA 196+25 TO STA 197+75 (FROM 30' TO 28' CONSTRUCTION)
 STA 205+00 TO STA 206+60
 STA 206+60 TO STA 215+60 (FROM 30' TO 38' CONSTRUCTION)

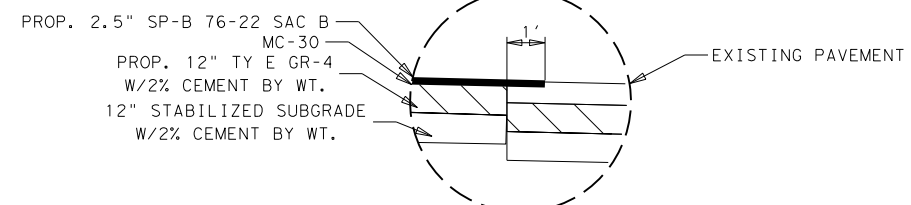


**SH 285
 PHASE I STEP I CONSTRUCTION**

STA 197+75 TO STA 199+95
 STA 201+30 TO STA 203+50
 STA 203+50 TO STA 205+00 (FROM 28' TO 30' CONSTRUCTION)



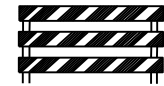














**PAVEMENT DETAIL "A2"
 N. T. S**



**PAVEMENT DETAIL "A3"
 N. T. S**

LEGEND

-  PLASTIC BARREL WITH REFLECTOR
-  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
-  TYPE 3 BARRICADE
-  CENTER LINE
-  PROFILE GRADE LINE
-  PERMISSIBLE CONSTRUCTION JOINT
-  RIGHT OF WAY
-  SHOULDER
-  WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
-  WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
-  PAV SURF PREP FOR MRK (4")
-  WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
-  WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

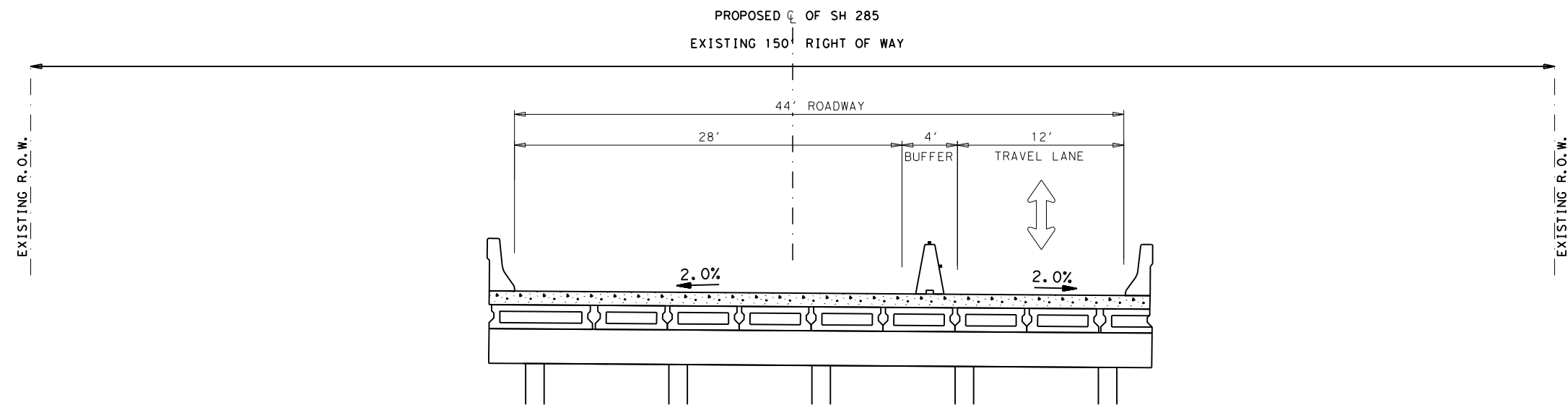
Pharr District Central Design



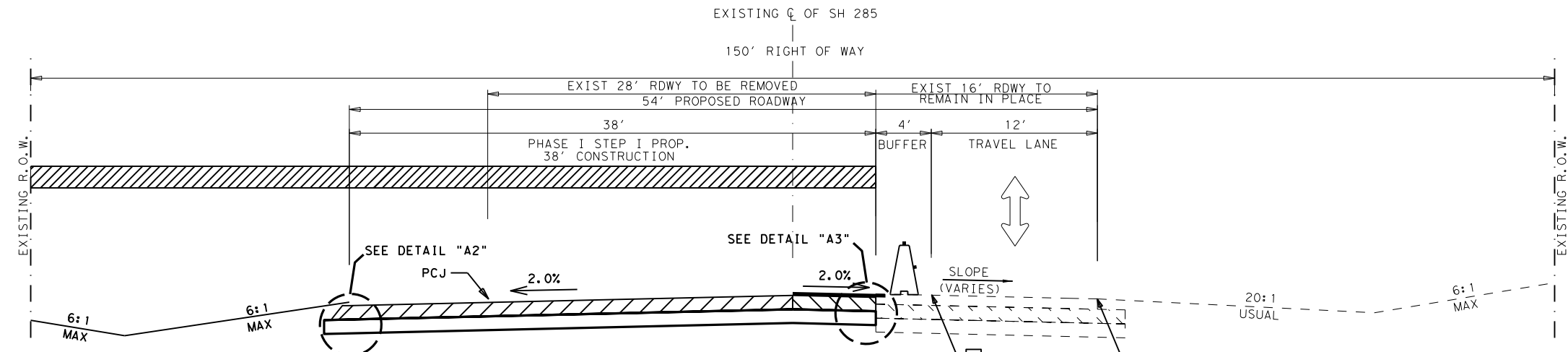
**SH 285
 TCP PHASE I
 TYPICAL SECTIONS**

N. T. S.		SHEET 1 OF 4	
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		59

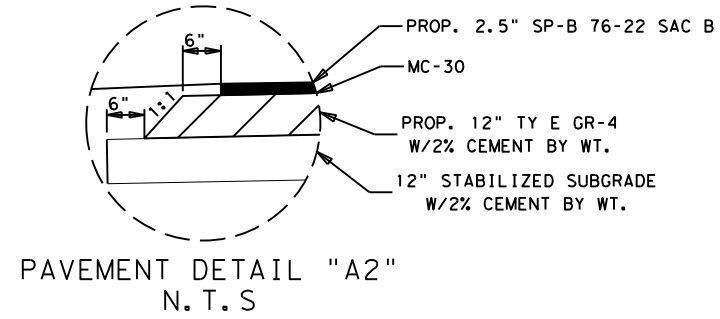
DATE: 3/4/2024 4:57:59 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH285 TCP PHASE I (BRIDGE) TYPICAL



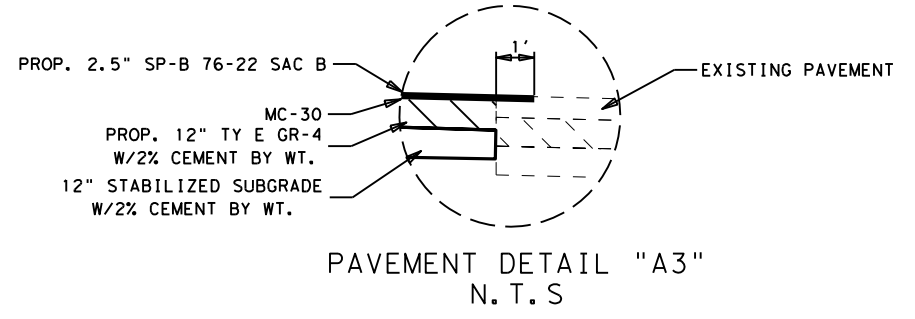
SH 285
 PHASE I STEP I CONSTRUCTION
 STA 199+95 TO STA 201+30



SH 285
 PHASE I STEP I CONSTRUCTION
 STA 215+60



PAVEMENT DETAIL "A2"
 N. T. S



PAVEMENT DETAIL "A3"
 N. T. S

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - CENTER LINE
 - PROFILE GRADE LINE
 - PERMISSIBLE CONSTRUCTION JOINT
 - RIGHT OF WAY
 - SHOULDER
 - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - PAV SURF PREP FOR MRK (4")
 - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

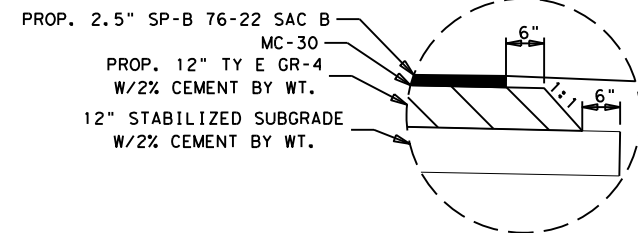
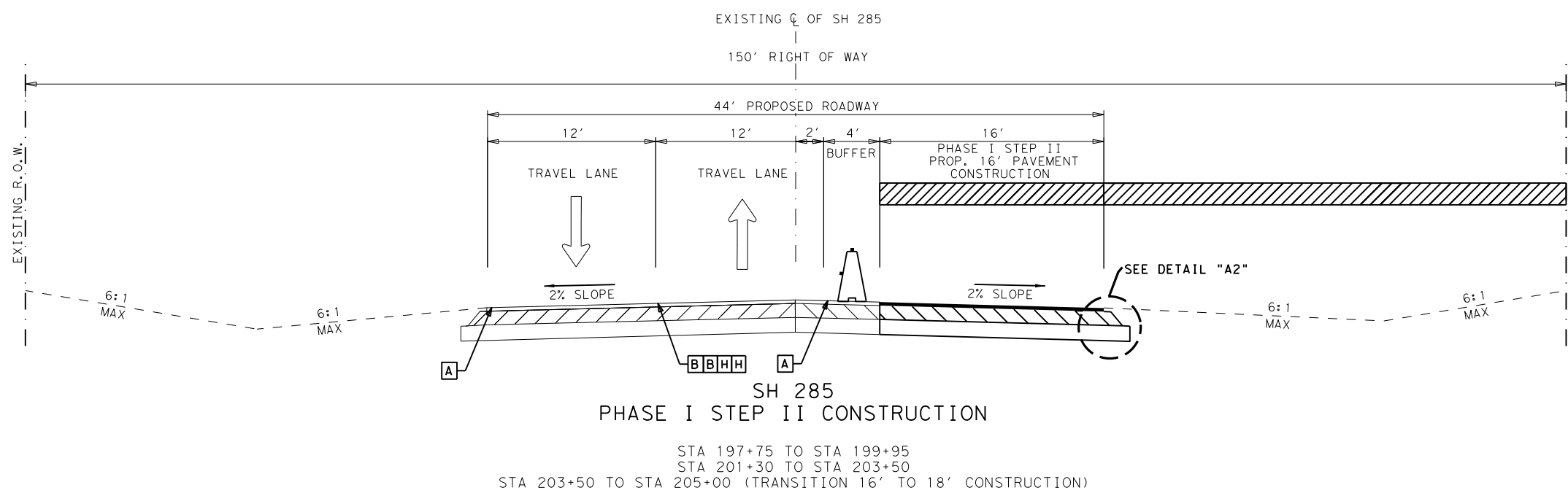
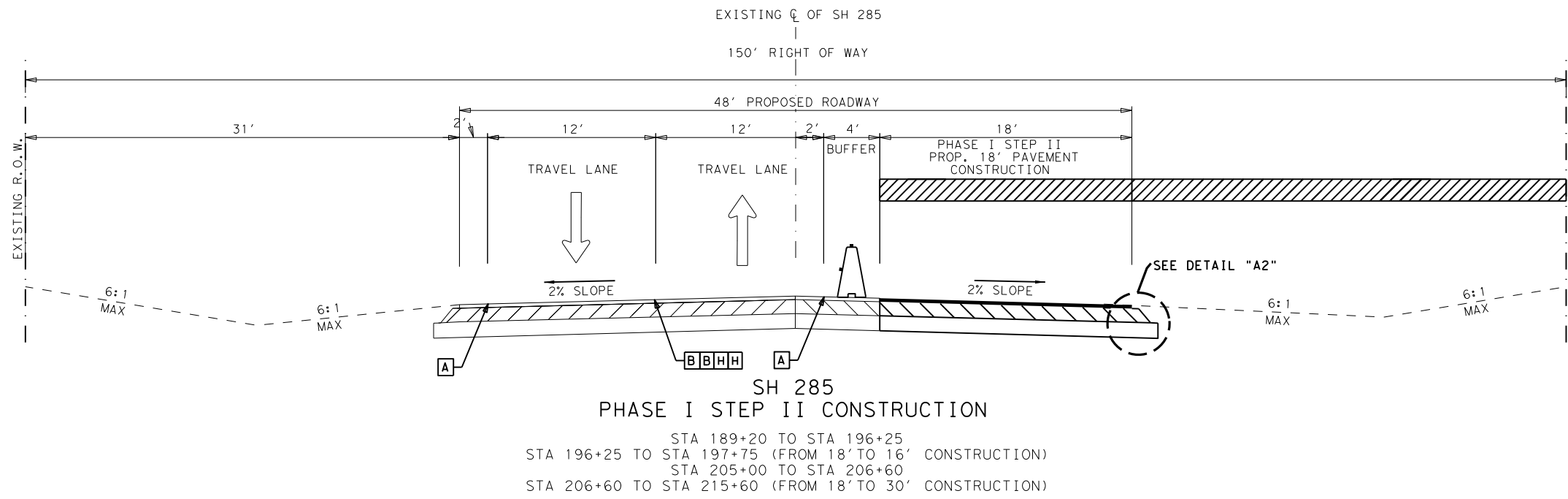
Pharr District Central Design



**SH 285
 TCP PHASE I
 TYPICAL SECTIONS**

N. T. S.		SHEET 2 OF 4	
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	60

DATE: 3/4/2024 4:58:00 PM
 FILE: pw:\txdot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH285 TCP PHASE I (BRIDGE) TYPICAL



PAVEMENT DETAIL "A2"
N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - \bar{C} - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

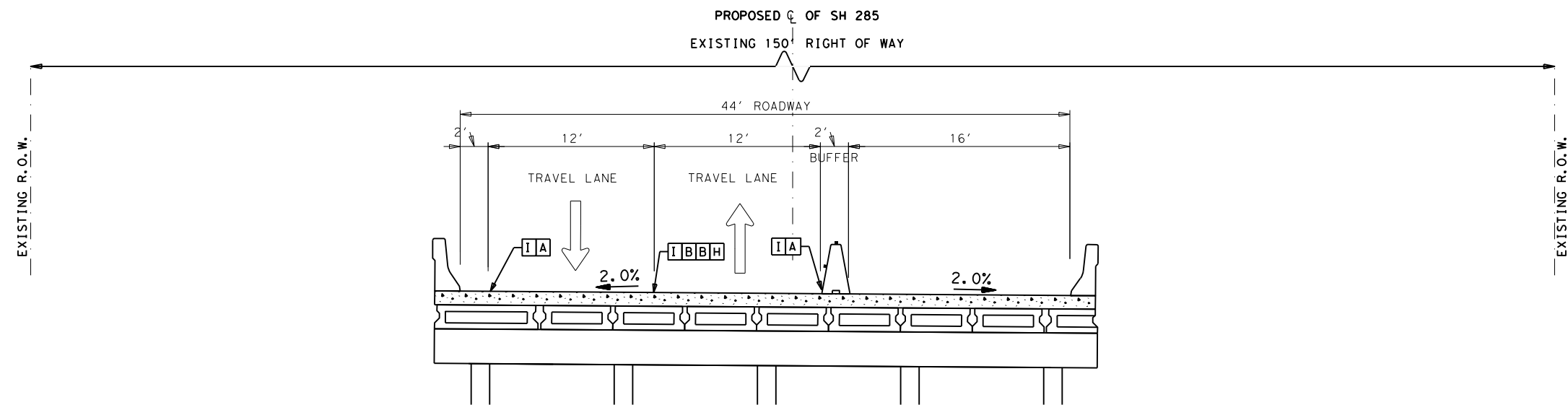
Pharr District Central Design



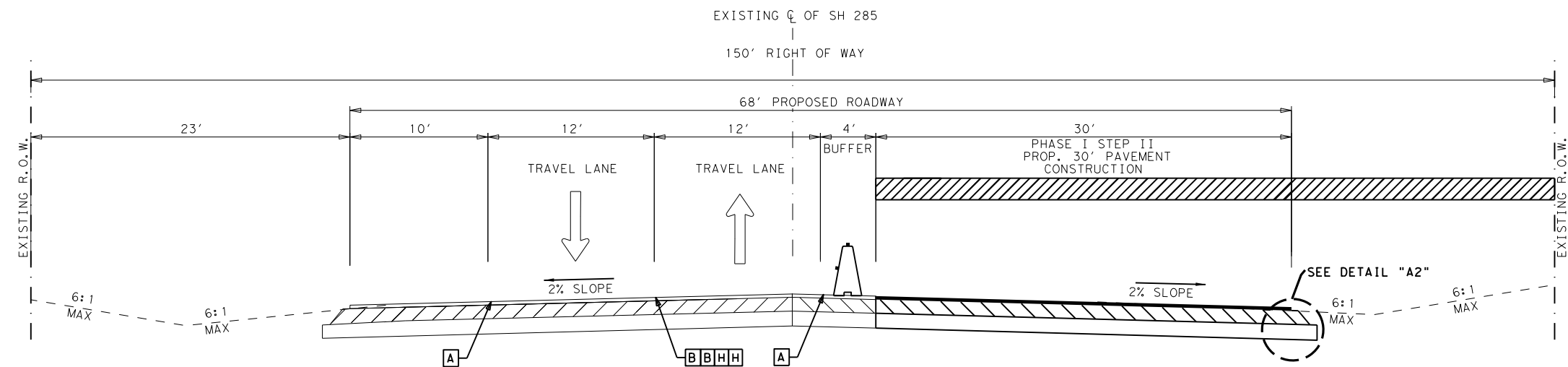
**SH 285
TCP PHASE I
TYPICAL SECTIONS**

N. T. S.		SHEET 3 OF 4	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO.
			61

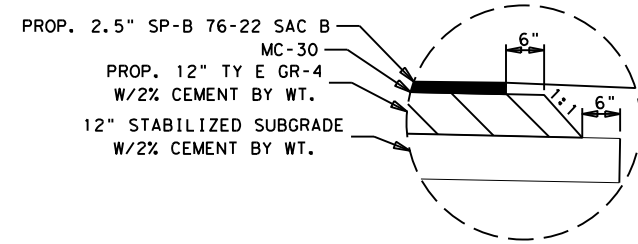
DATE: 3/4/2024 4:58:02 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH285 TCP PHASE I (BRIDGE) TYPICAL





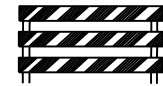












SH 285
 PHASE I STEP II CONSTRUCTION
 STA 199+95 TO STA 201+30



SH 285
 PHASE I STEP II CONSTRUCTION
 STA 215+60 TO STA 222+60



PAVEMENT DETAIL "A2"
 N. T. S.

- LEGEND**
-  PLASTIC BARREL WITH REFLECTOR
 -  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 -  TYPE 3 BARRICADE
 -  CENTER LINE
 -  PROFILE GRADE LINE
 -  PERMISSIBLE CONSTRUCTION JOINT
 -  RIGHT OF WAY
 -  SHOULDER
 -  WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 -  WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 -  WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 -  WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 -  PAV SURF PREP FOR MRK (4")
 -  WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 -  WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP PHASE I
 TYPICAL SECTIONS

N. T. S.		SHEET 4 OF 4	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO.
			62

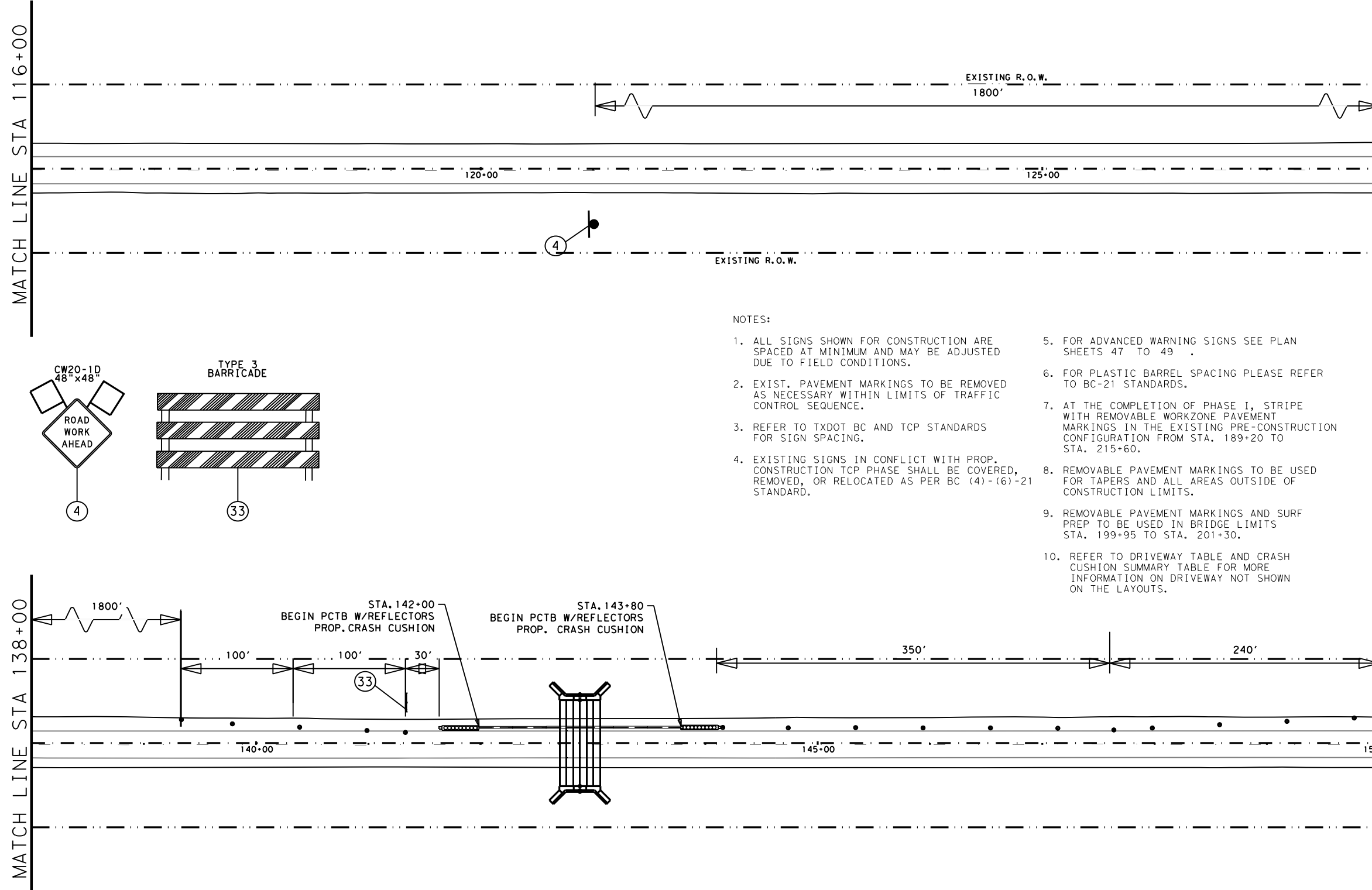
DATE: 3/4/2024 4:58:10 PM
 SHEET: PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP I (BRIDGE)

MATCH LINE STA 116+00

MATCH LINE STA 138+00

MATCH LINE STA 128+00

MATCH LINE STA 150+00

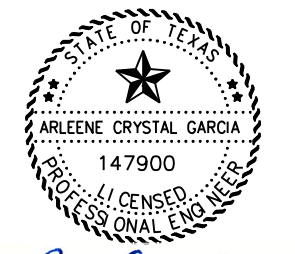


NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
10. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE I STEP I

























SCALE: 1" = 100' SHEET 1 OF 5

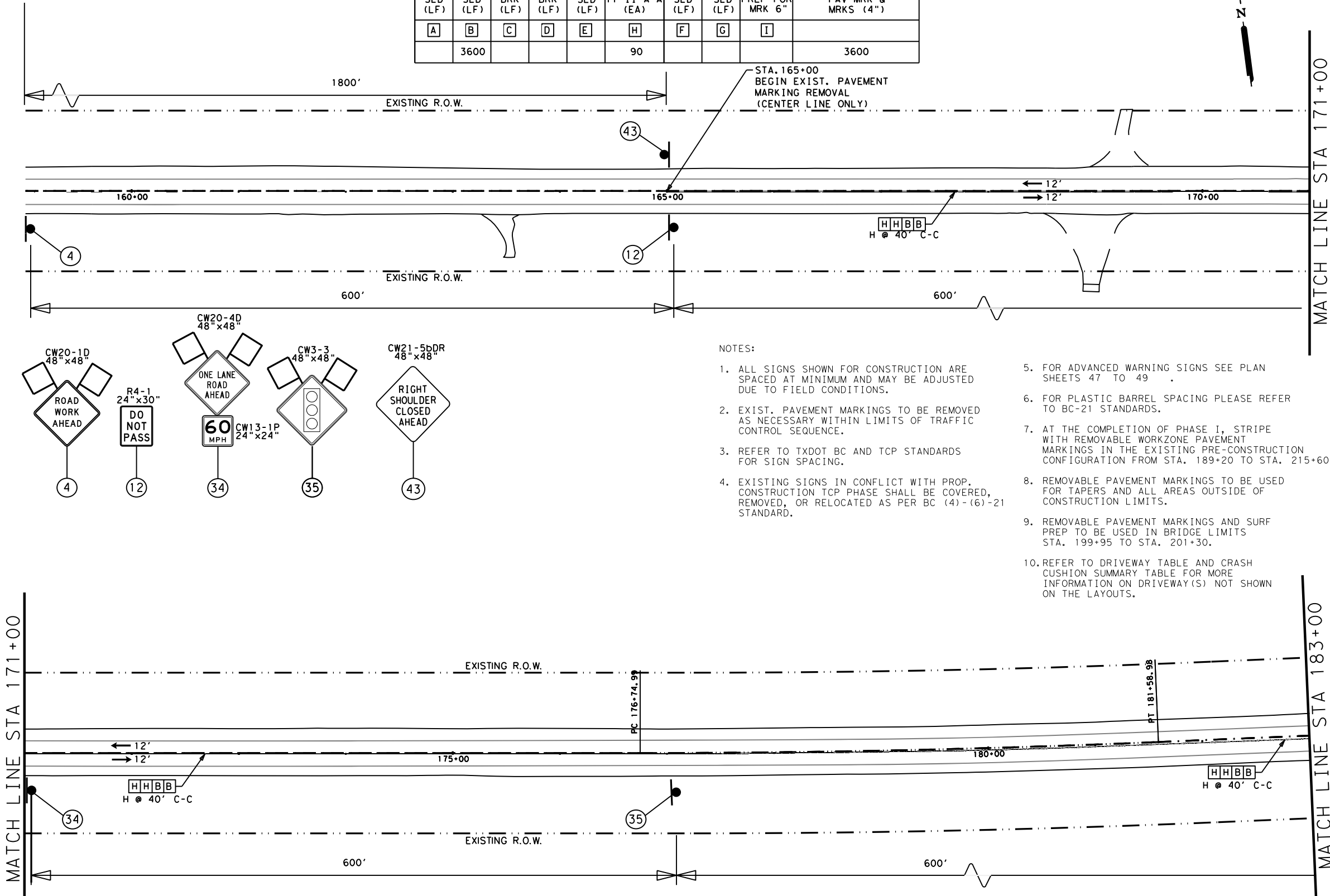
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		63

DATE: 3/4/2024 4:58:12 PM
 FILE: D:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP I BRIDGE

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
	3600				90				3600

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG




- NOTES:**
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
 - REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



Texas Department of Transportation

**SH 285
TCP
PHASE I STEP I**

SCALE: 1" = 100' SHEET 2 OF 5

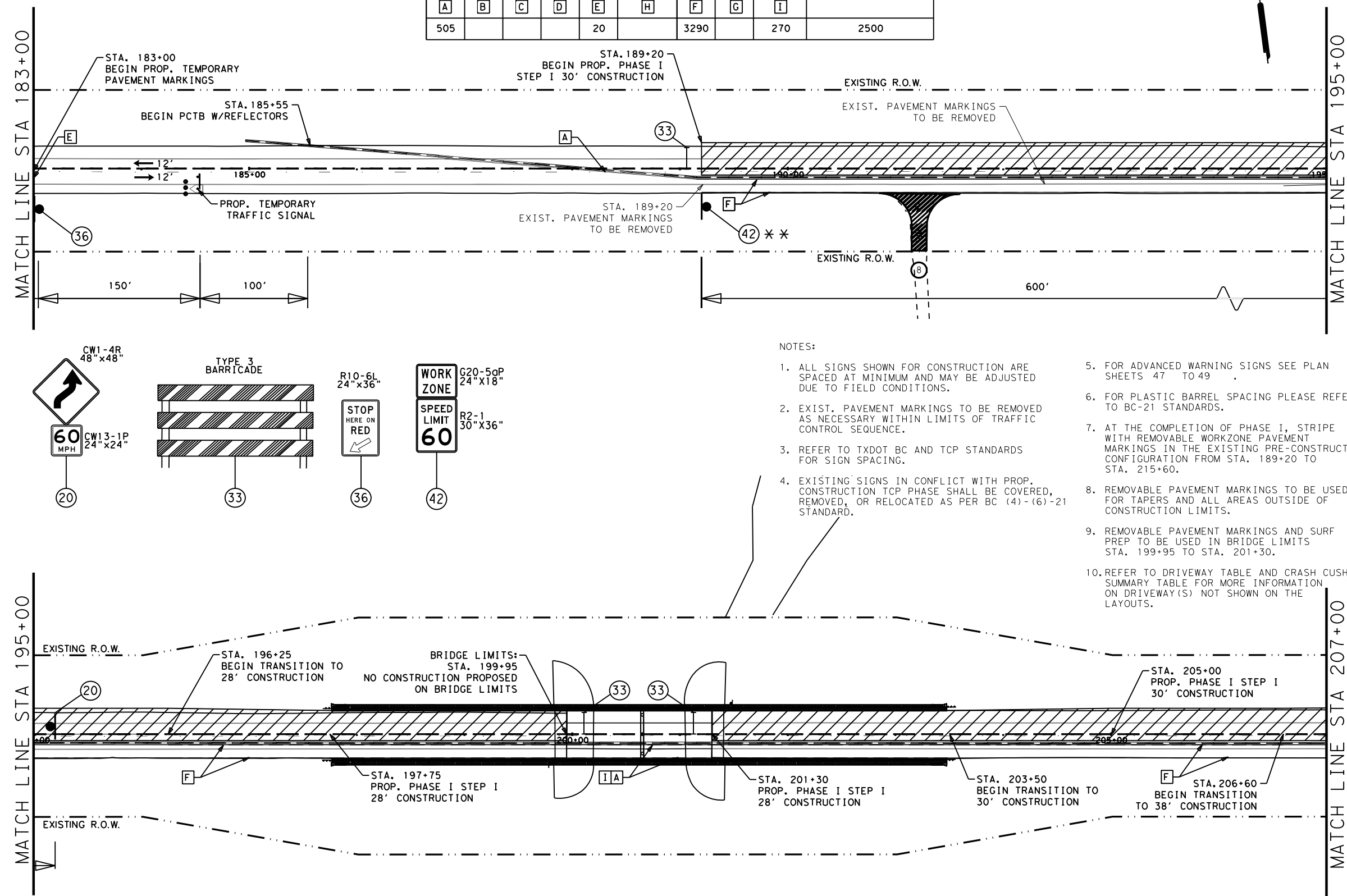
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	64	

DATE: 3/4/2024 4:58:14 PM
 FILE: D:\txdot\project\wison\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP I BRIDGE

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
505				20		3290		270	2500

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



- NOTES:**
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
 - REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE I STEP I**

SCALE: 1" = 100' SHEET 3 OF 5

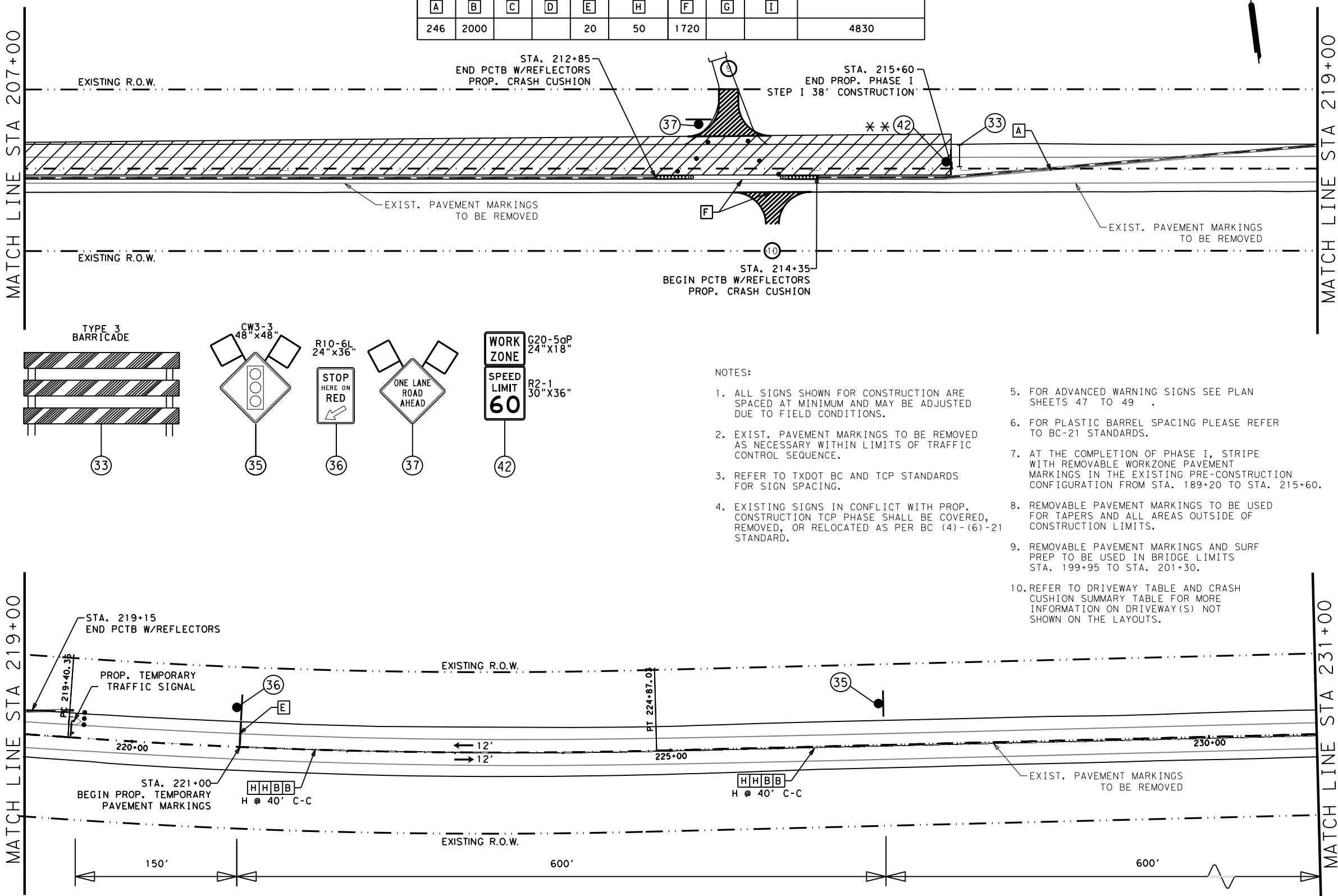
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	65	

DATE: 3/4/2024 4:58:16 PM
 FILE: D:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP I BRIDGE

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)		
(REMOVABLE)				(NON-REMOV)				678	677		
662	662	662	662	662	662	662	662	6008	6037	6002	677
6067	6098	6064	6096	6075	6050	6008	6037	6002			6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"			ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I			
246	2000			20	50	1720					4830

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
10. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE I STEP I**

























SCALE: 1" = 100' SHEET 4 OF 5

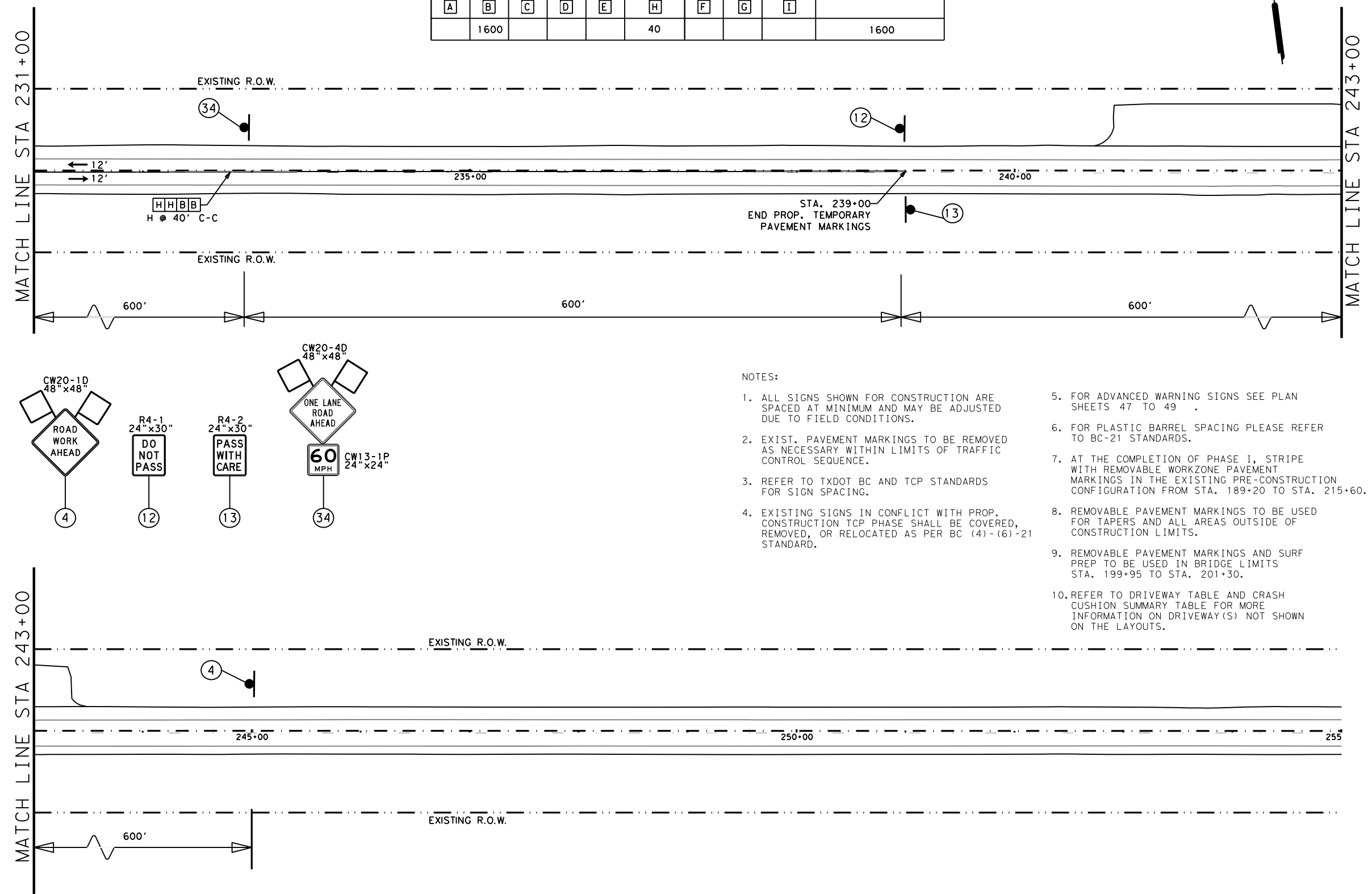
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	66	

DATE: 3/4/2024 4:58:18 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP I BRIDGE

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
	1600				40				1600

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:


1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
10. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



























**SH 285
TCP
PHASE I STEP I**

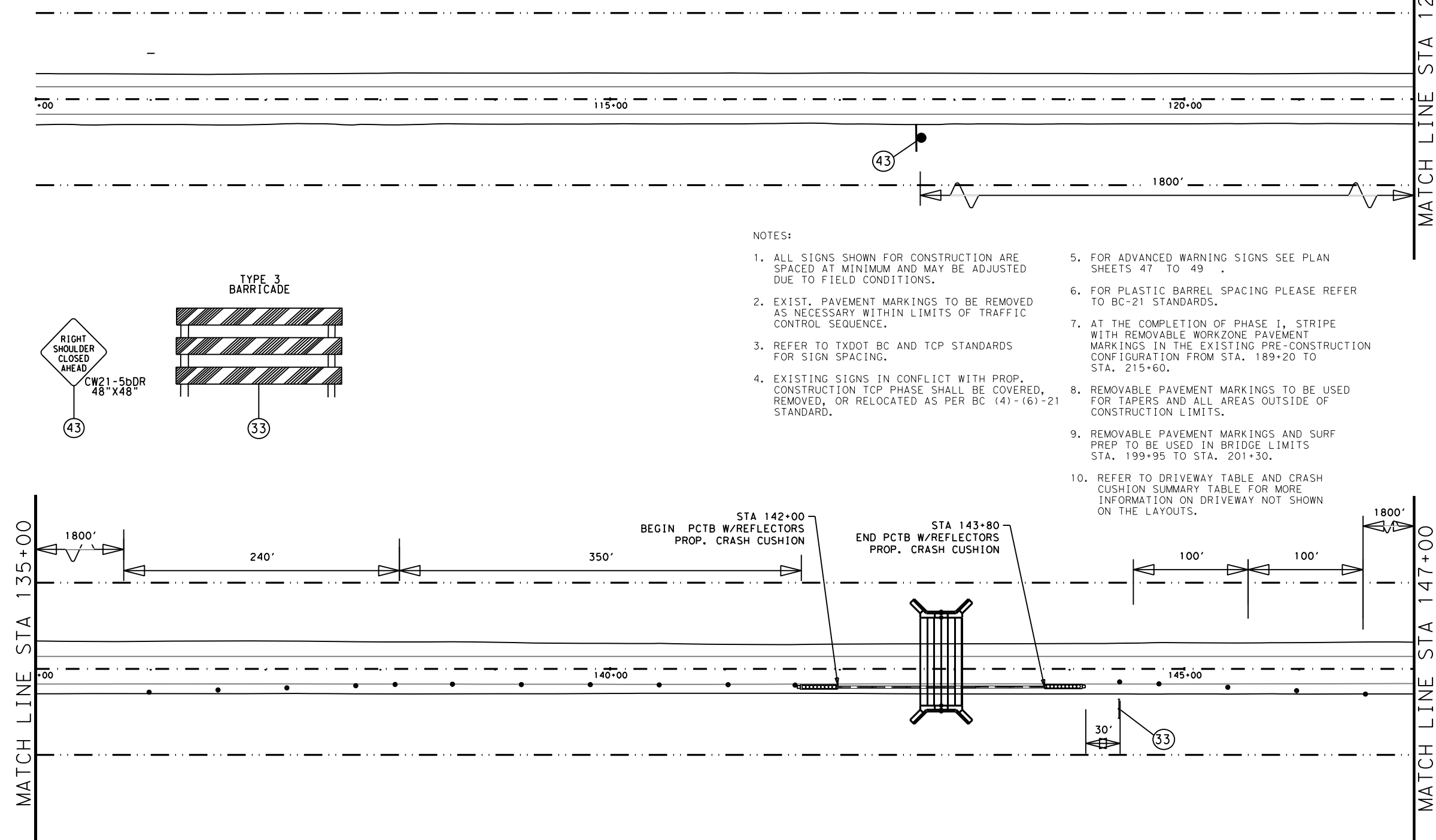
SCALE: 1" = 100' SHEET 5 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, et.c.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		67

DATE: 3/4/2024 4:58:24 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP II (BRIDGE)

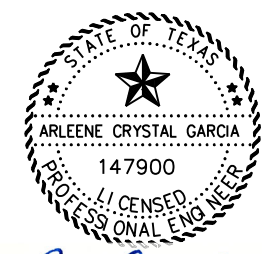
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
10. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE I STEP II

SCALE: 1" = 100' SHEET 1 OF 5

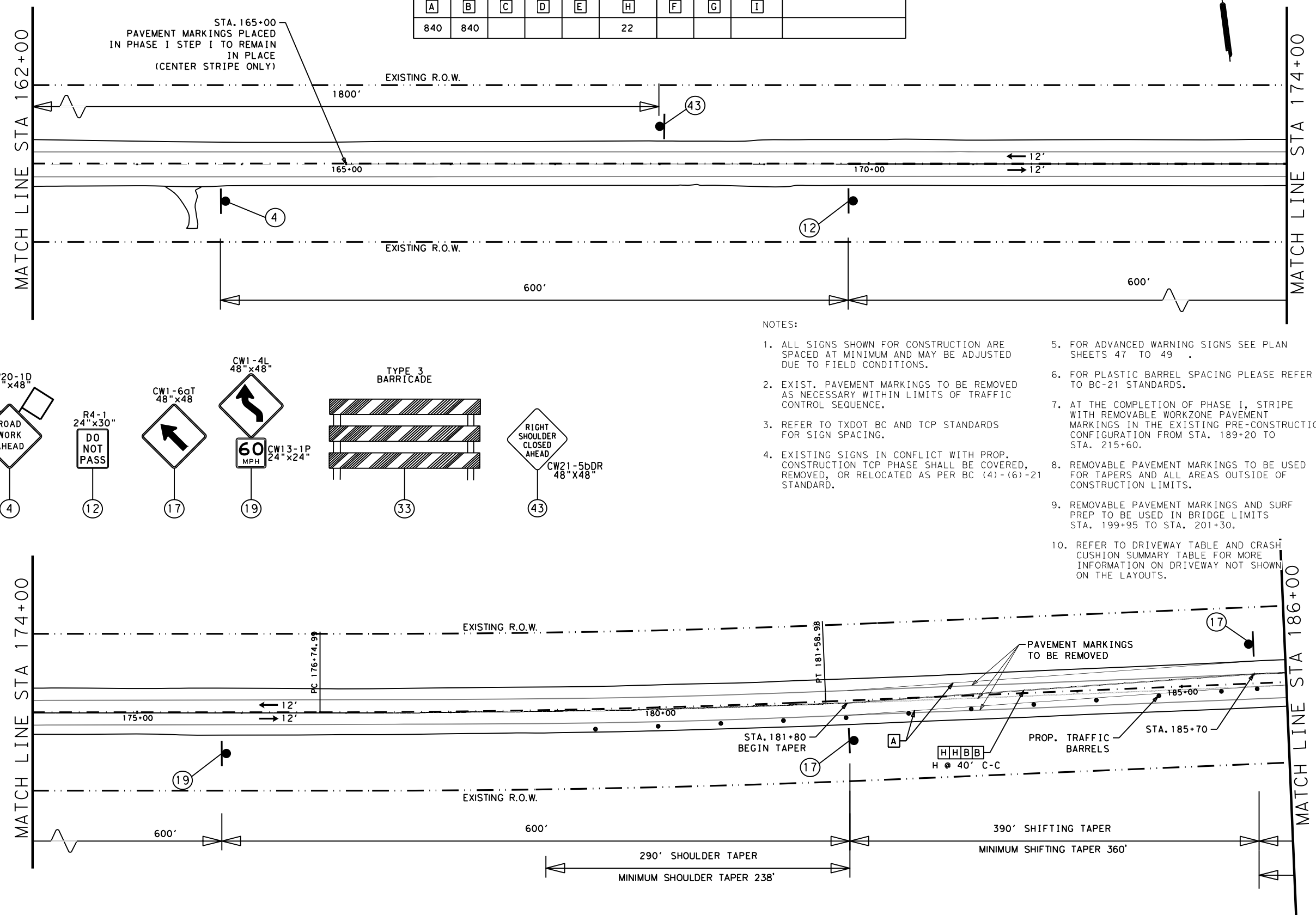
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	68	

DATE: 3/4/2024 4:58:26 PM
 FILE: \\txdot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP II (BRIDGE)

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
840	840				22				

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
10. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE I STEP II**

SCALE: 1" = 100' SHEET 2 OF 5

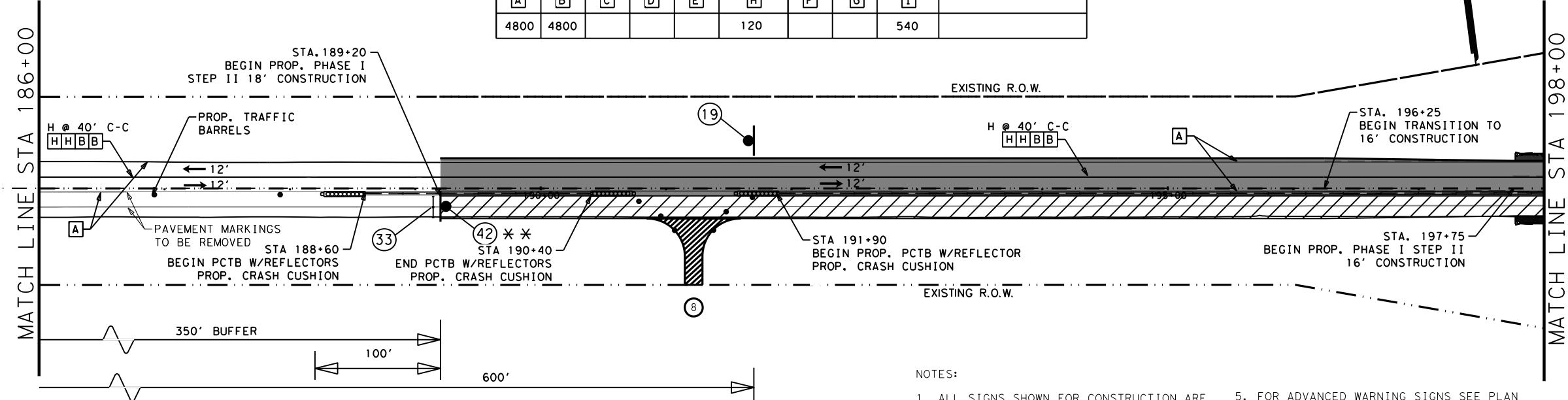
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	69	

DATE: 3/4/2024 4:58:28 PM
 FILE: \\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP II (BRIDGE)

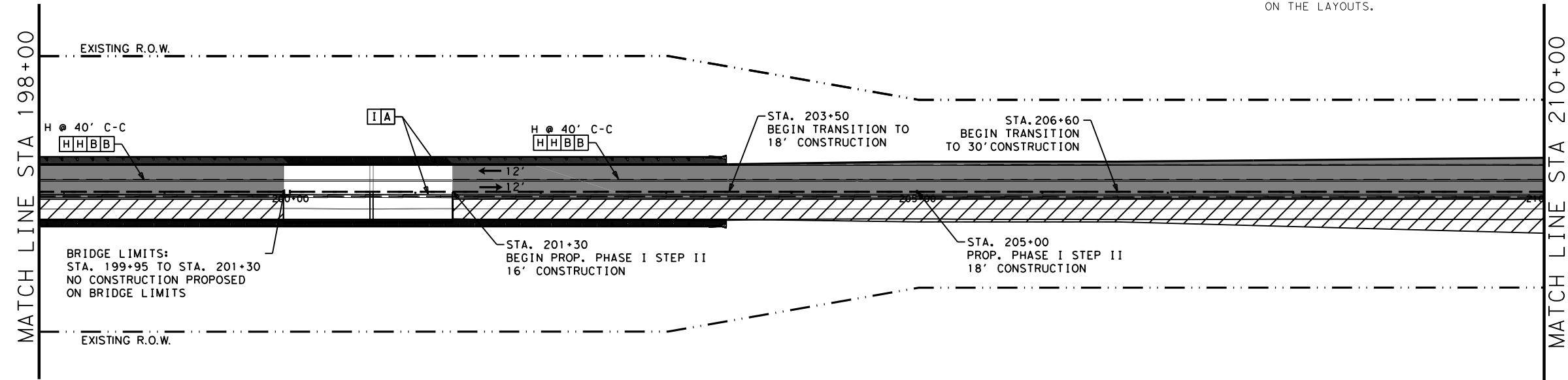
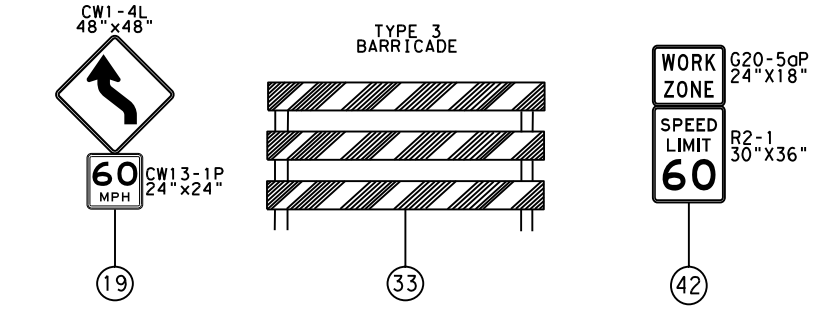
WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
4800	4800				120			540	

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



- NOTES:**
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.
 - REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TCP PHASE I STEP II

SCALE: 1" = 100' SHEET 3 OF 5

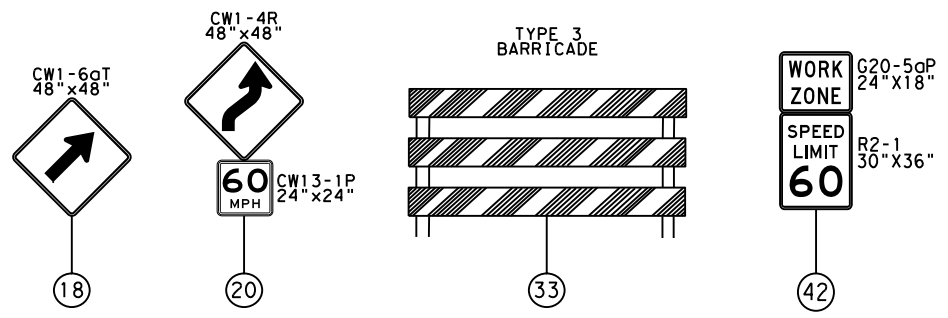
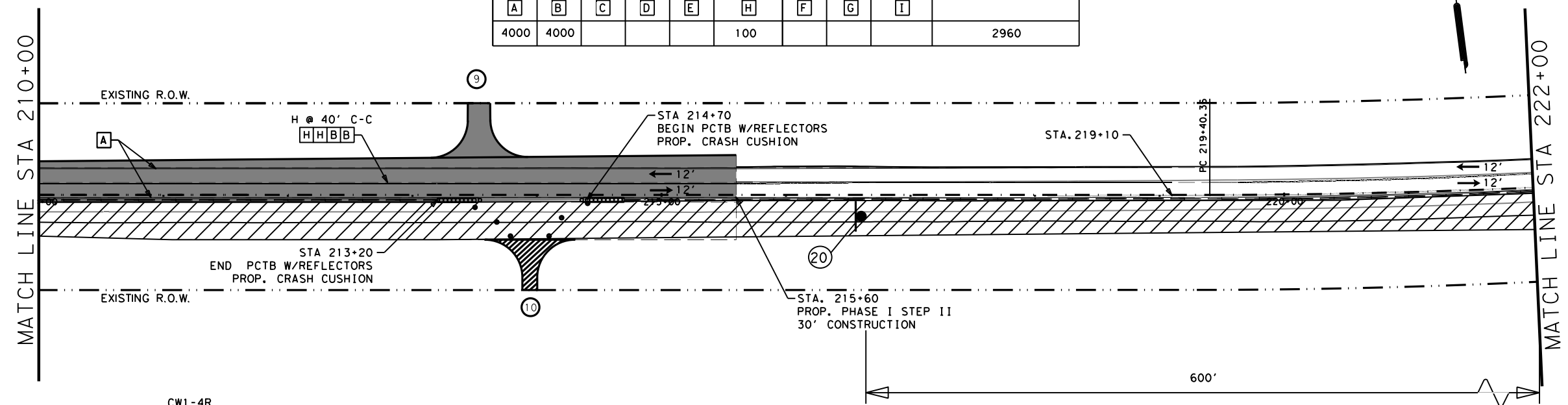
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		70

DATE: 3/4/2024 4:58:30 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP II (BRIDGE)

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)				678	677
662	662	662	662	662	662	662	6008	6037	6002
6067	6098	6064	6096	6075	6050				
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
4000	4000				100				2960

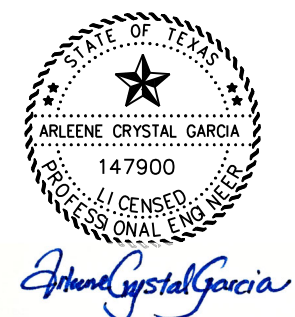
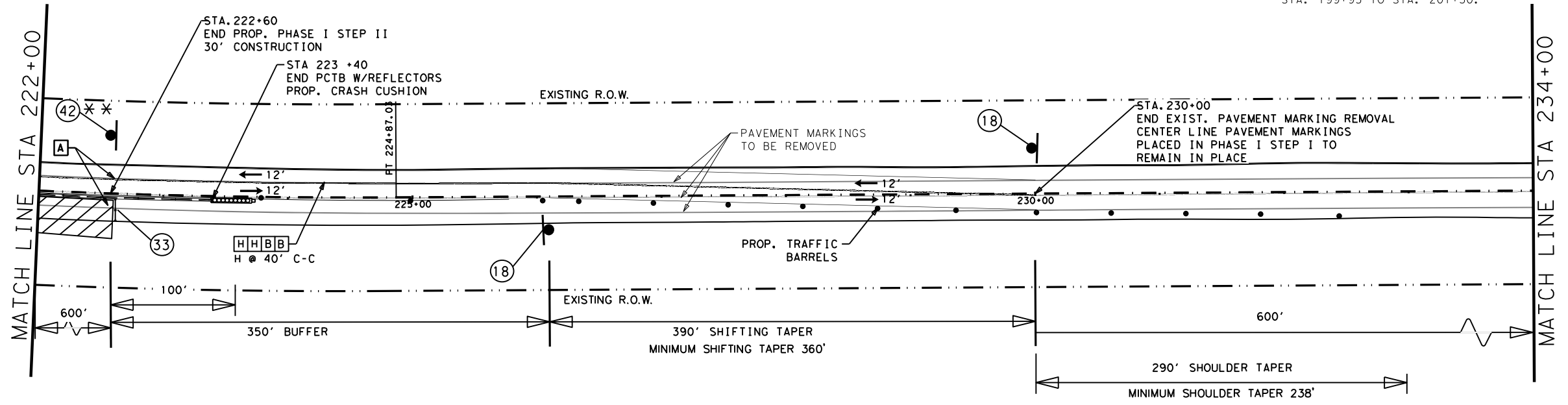
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(16)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.



03.07.2024

Pharr District Central Design



**SH 285
 TCP
 PHASE I STEP II**

SCALE: 1" = 100' SHEET 4 OF 5

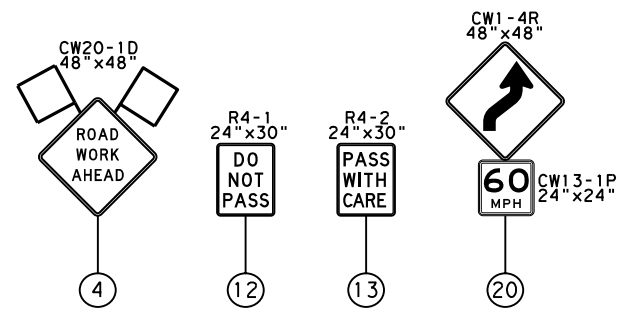
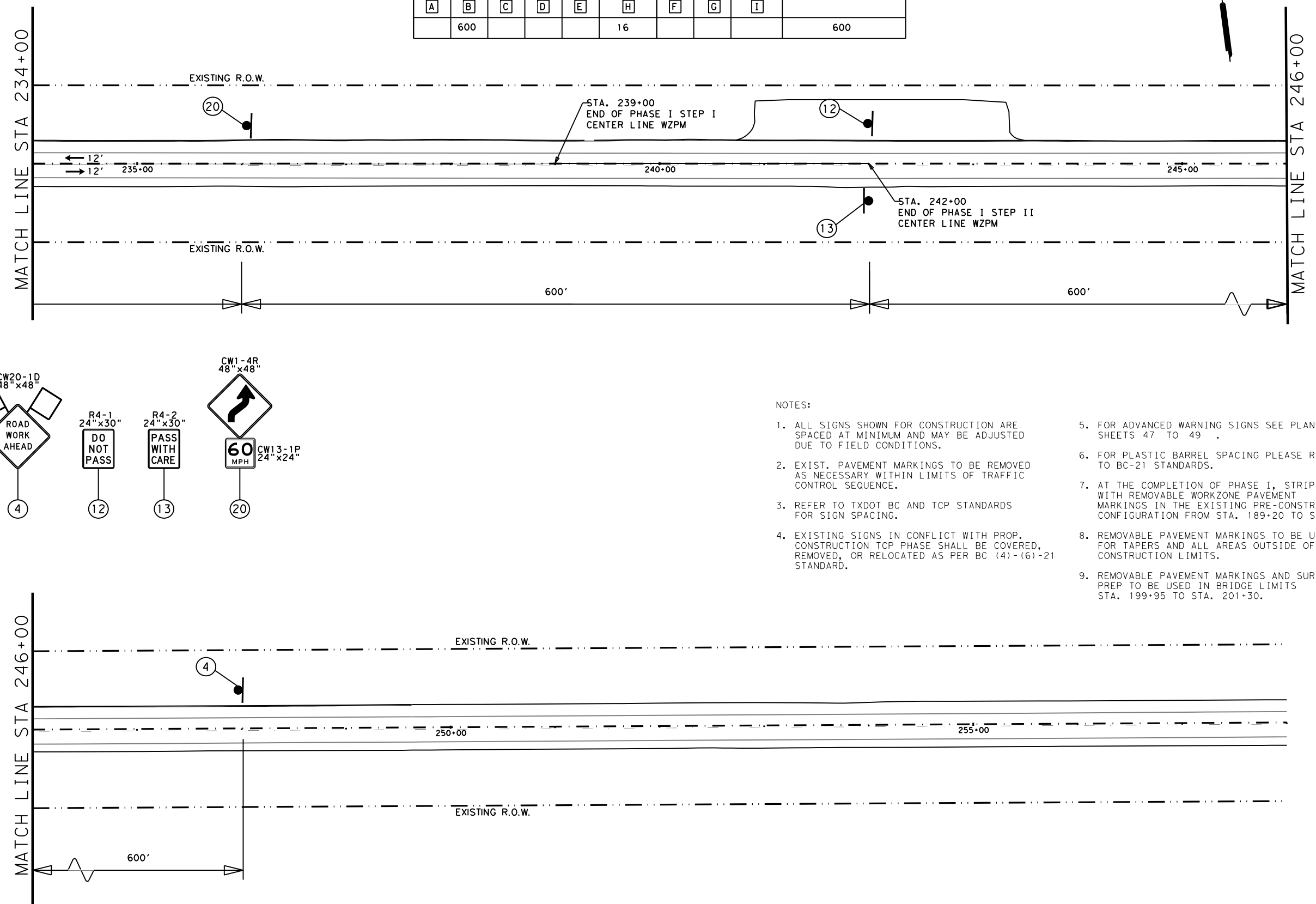
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	71	

DATE: 3/4/2024 4:58:32 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE I - BRIDGE\SH 285 TCP PHASE I STEP II (BRIDGE

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
	600				16				600

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE I, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 189+20 TO STA. 215+60.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REMOVABLE PAVEMENT MARKINGS AND SURF PREP TO BE USED IN BRIDGE LIMITS STA. 199+95 TO STA. 201+30.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design











SH 285
TCP
PHASE I STEP II








SCALE: 1"=100' SHEET 5 OF 5

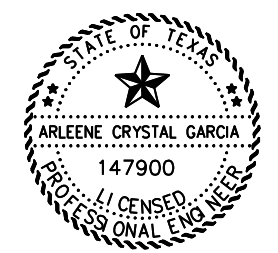
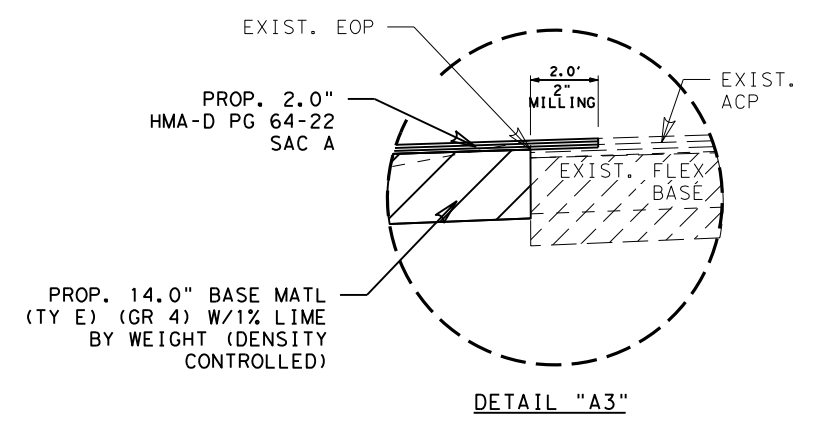
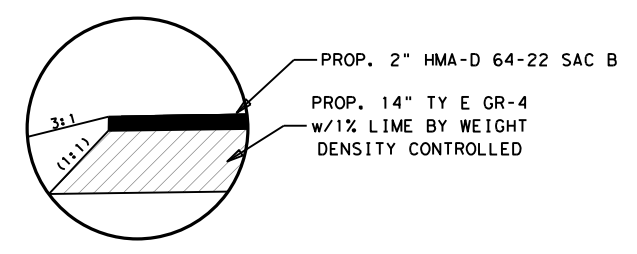
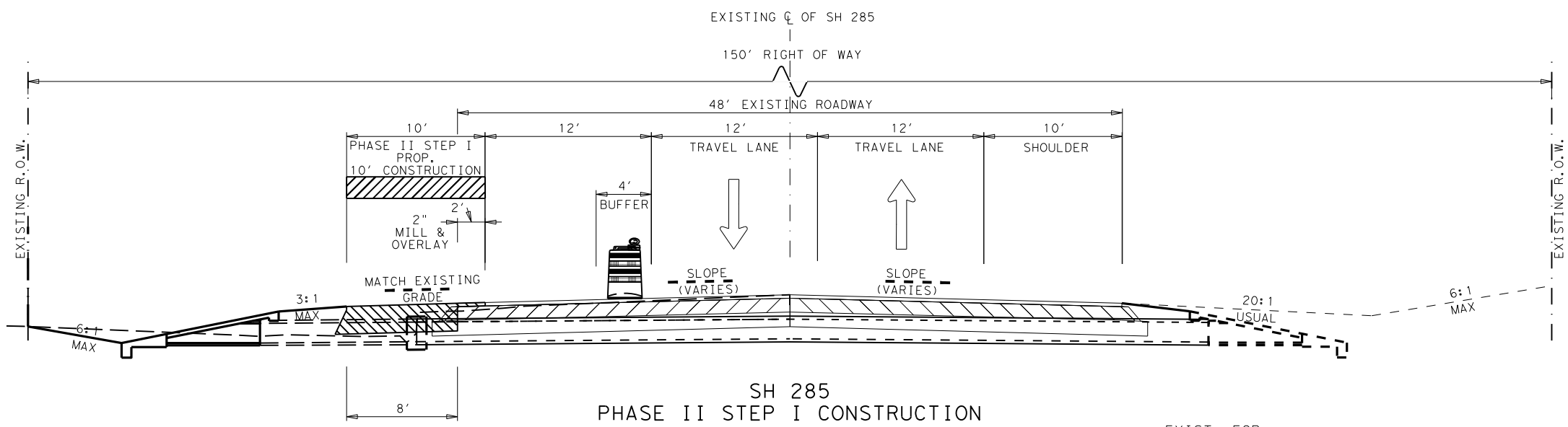
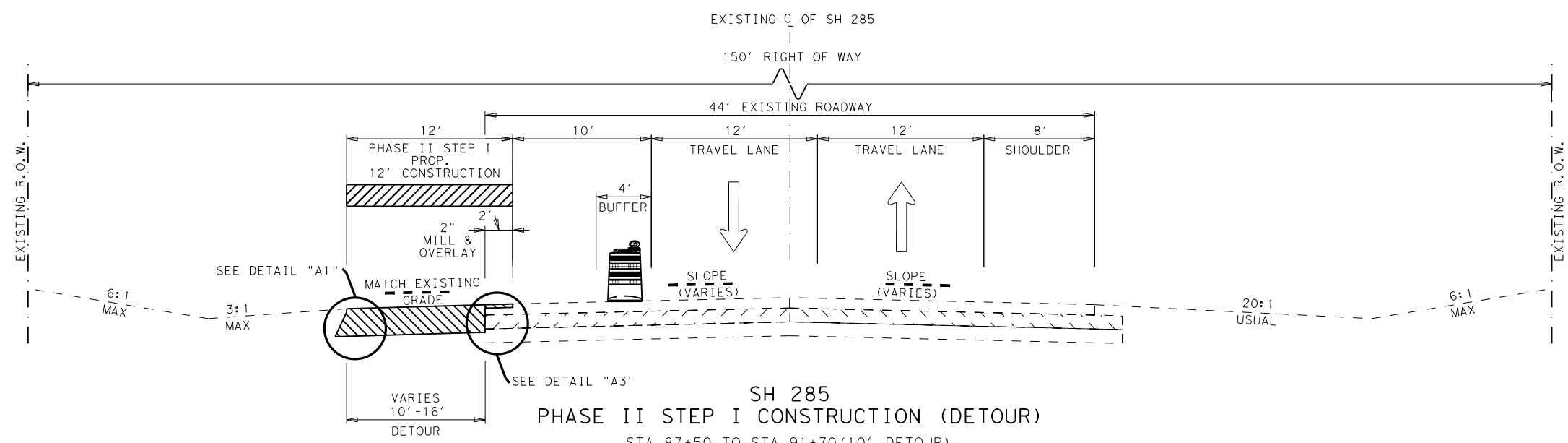
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		72

DATE: 3/4/2024 4:58:45 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II\SH285 TCP PHASE II TYPICALS.dgn

LEGEND

-  PLASTIC BARREL WITH REFLECTOR
-  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
-  TYPE 3 BARRICADE
-  CENTER LINE
-  PGL - PROFILE GRADE LINE
-  PCJ - PERMISSIBLE CONSTRUCTION JOINT
-  R.O.W. - RIGHT OF WAY
-  SHLDR - SHOULDER

-  A WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
-  B WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
-  F WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
-  G WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
-  I PAV SURF PREP FOR MRK (4")
-  H WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
-  WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

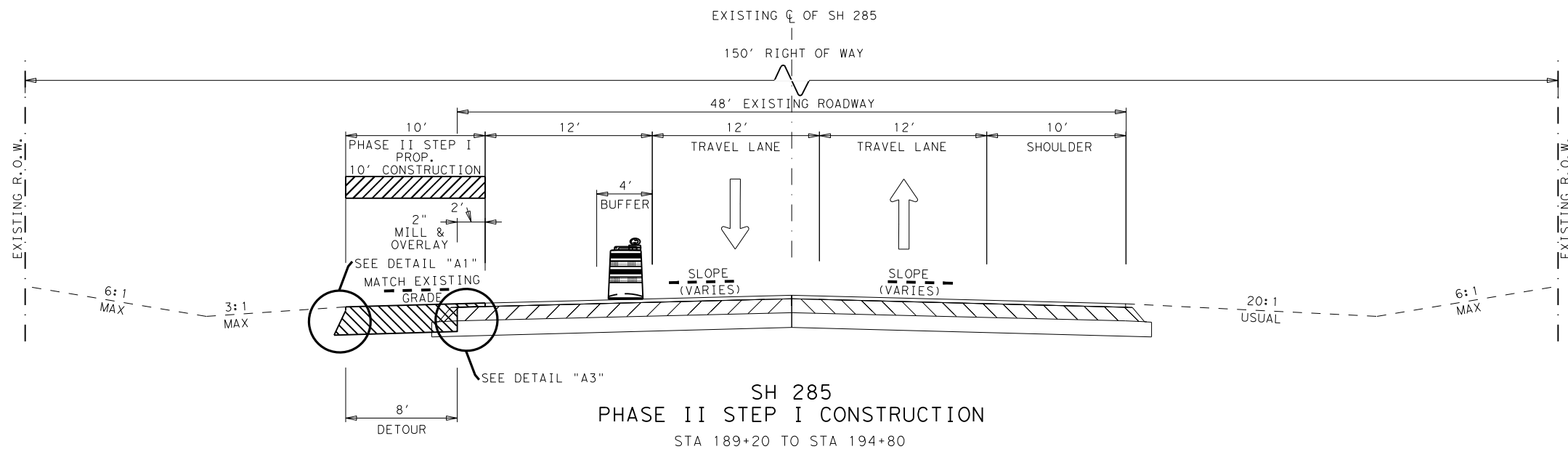


**SH 285
 TCP
 PHASE II
 TYPICAL SECTIONS**

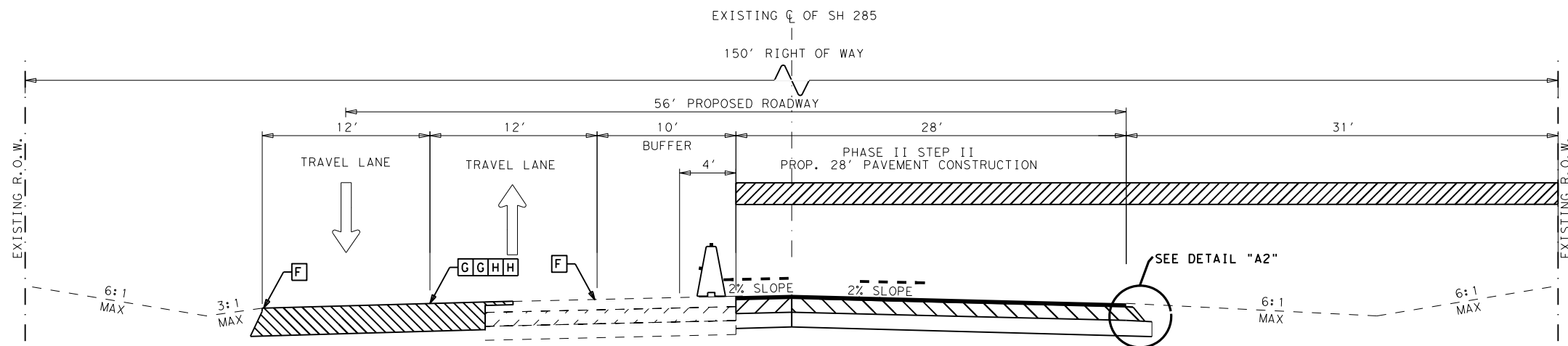
N. T. S. SHEET 1 OF 4

DS:	CK:	CONT	SECT	JOB	HIGHWAY
		0482	01	036, etc.	SH 285
DW:	CK:	DIST		COUNTY	SHEET NO.
		PHR		JIM HOGG	73

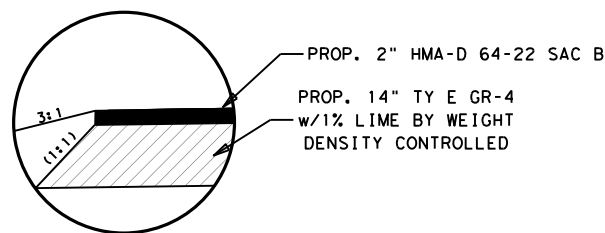
DATE: 3/4/2024 4:58:46 PM
 FILE: \\txdot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II\SH285 TCP PHASE II TYPICALS.dgn



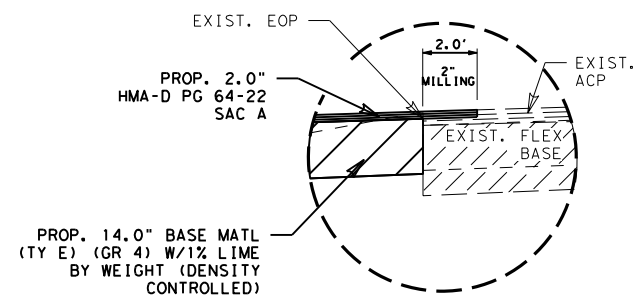
SH 285
 PHASE II STEP I CONSTRUCTION
 STA 189+20 TO STA 194+80



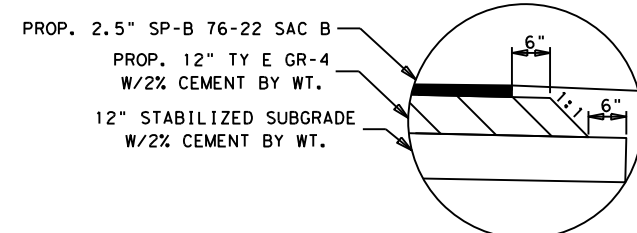
SH 285
 PHASE II STEP II CONSTRUCTION
 STA 96+00 TO STA 99+28
 STA 101+80 TO STA 103+85
 STA 103+85 TO 106+15 (TRANSITION FROM 18'-12' DETOUR)



PAVEMENT DETAIL "A1"

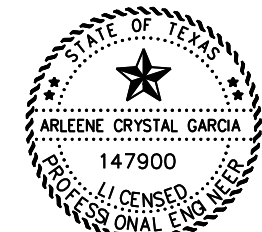


PAVEMENT DETAIL "A3"



PAVEMENT DETAIL "A2"
 N. T. S

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - ℄ - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - PAV SURF PREP FOR MRK (4")
 - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

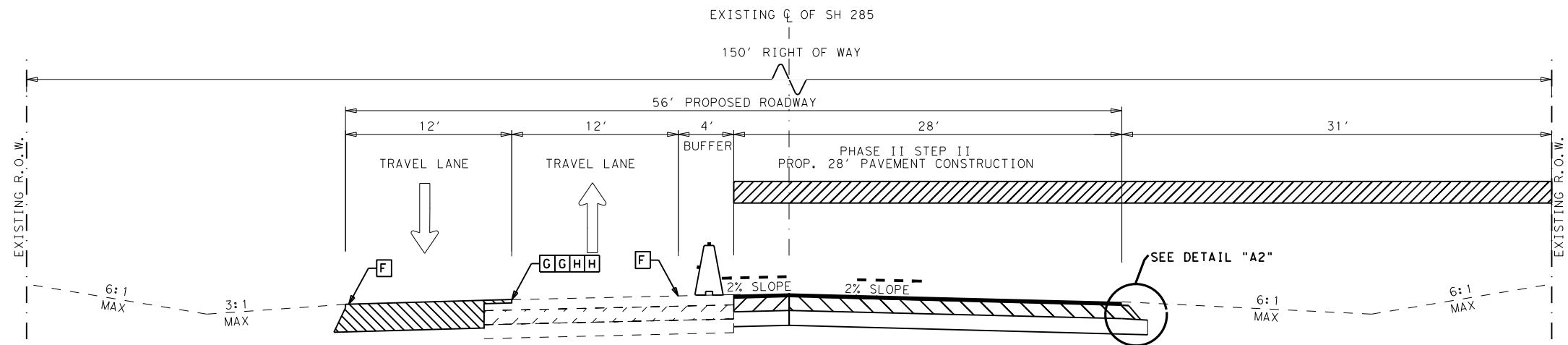
Texas Department of Transportation

**SH 285
 TCP
 PHASE II
 TYPICAL SECTIONS**

N. T. S. SHEET 2 OF 4

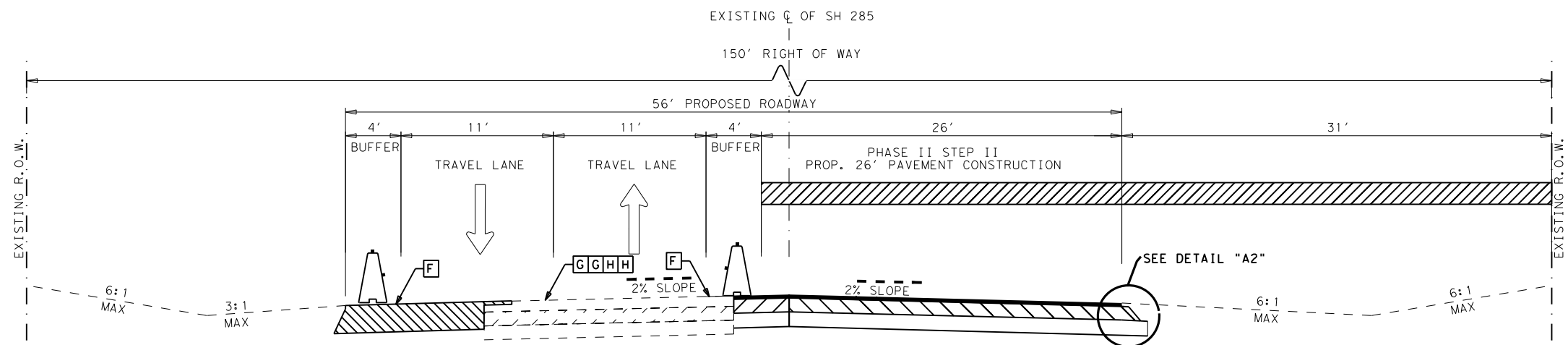
DS:	CK:	0482	01	036, etc.	SH 285
DW:	CK:	PHR	JIM HOGG		74

DATE: 3/4/2024 4:58:47 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II\SH285 TCP PHASE II TYPICALS.dgn



SH 285
 PHASE II STEP II CONSTRUCTION

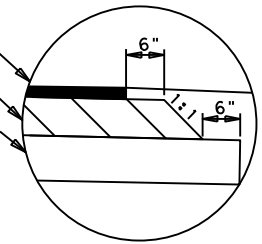
STA 106+15 TO STA 140+31
 STA 140+31 TO STA 142+70 (TRANSITION 28'-26' CONSTRUCTION)
 STA 145+52 TO STA 189+20



SH 285
 PHASE II STEP II CONSTRUCTION











STA 142+70 TO STA 143+10
 STA 143+10 TO STA 145+52 (TRANSITION 26'-28' CONSTRUCTION)

PROP. 2.5" SP-B 76-22 SAC B
 PROP. 12" TY E GR-4
 W/2% CEMENT BY WT.
 12" STABILIZED SUBGRADE
 W/2% CEMENT BY WT.



PAVEMENT DETAIL "A2"
 N. T. S

LEGEND

-  PLASTIC BARREL WITH REFLECTOR
-  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
-  TYPE 3 BARRICADE
- CL - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R.O.W. - RIGHT OF WAY
- SHLDR - SHOULDER
-  A WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
-  B WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
-  F WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
-  G WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
-  I PAV SURF PREP FOR MRK (4")
-  H WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
-  WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

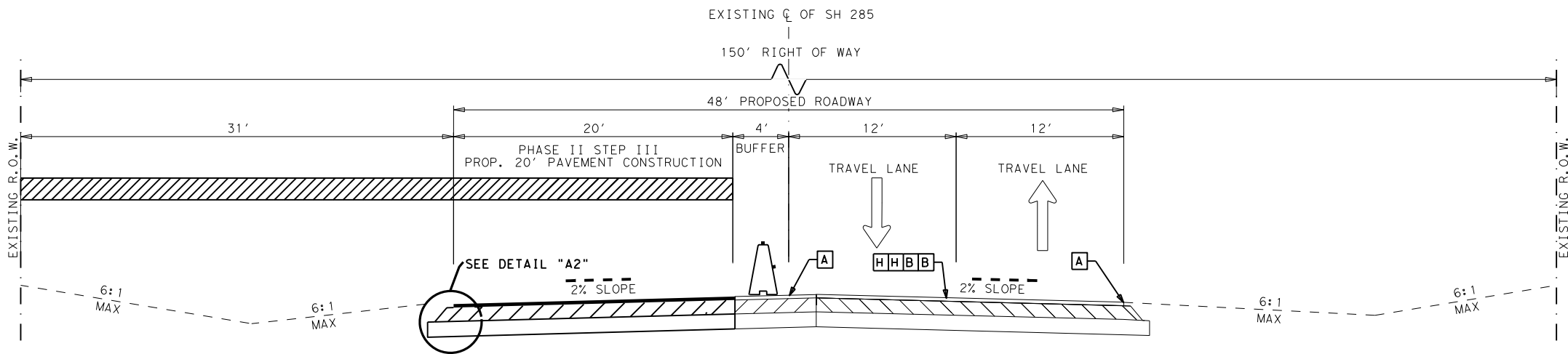
Pharr District Central Design



SH 285
 TCP
 PHASE II
 TYPICAL SECTIONS

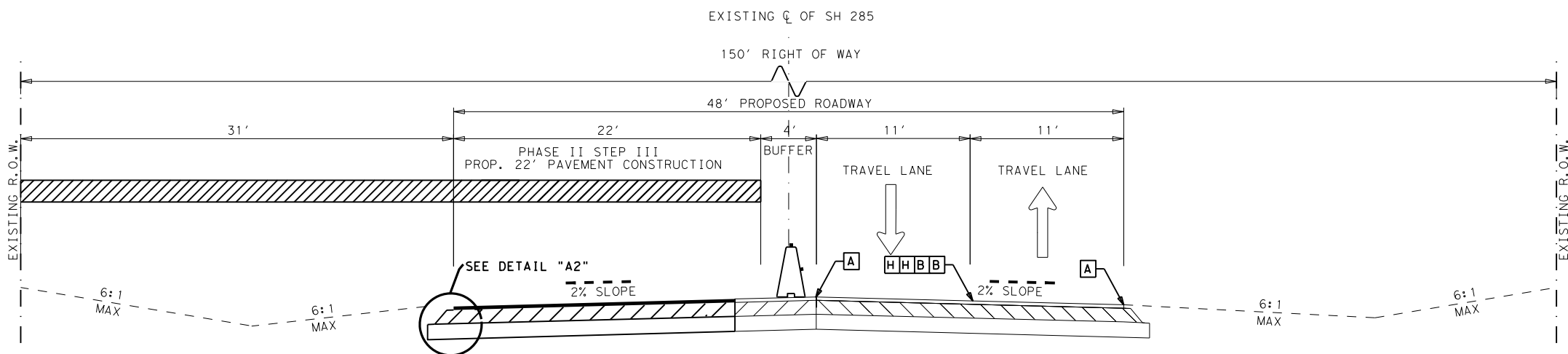
N. T. S.		SHEET 3 OF 4	
DS:	CK:	CONT:	SECT:
		0482	01
DIST:	COUNTY:	JOB:	HIGHWAY:
PHR	JIM HOGG	036, etc.	SH 285
DW:	CK:	SHEET NO.	
		75	

DATE: 3/4/2024 4:58:48 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II TYPICALS.dgn



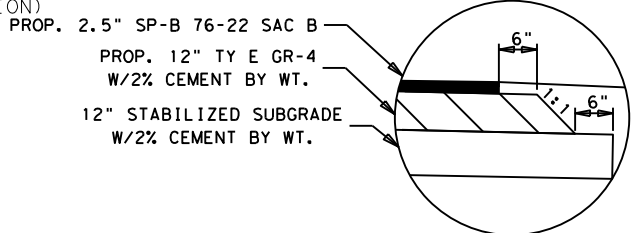
SH 285
 PHASE II STEP III CONSTRUCTION

STA 96+00 TO STA 140+31
 STA 140+31 TO STA 142+70 (TRANSITION 20' TO 22' CONSTRUCTION)
 STA 145+52 TO STA 189+20

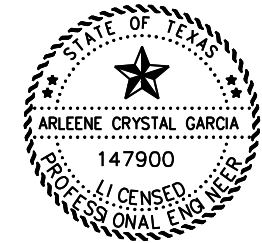


SH 285
 PHASE II STEP III CONSTRUCTION

STA 142+70 TO STA 143+10
 STA 143+10 TO STA 145+52 (TRANSITION 22' -20' CONSTRUCTION)



- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - ℄ - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - WORK ZONE PVT MARK (REM) 4" WHITE SOLID
 - WORK ZONE PVT MARK (REM) 4" YELLOW SOLID
 - WORK ZONE PVT MARK (NON-REM) 4" WHITE SOLID
 - WORK ZONE PVT MARK (NON-REM) 4" YELLOW SOLID
 - PAV SURF PREP FOR MRK (4")
 - WORK ZONE PVT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285
 TCP
 PHASE II
 TYPICAL SECTIONS

N. T. S. SHEET 4 OF 4

























DS:	CK:	0482	01	036, etc.	SH 285
DW:	CK:	PHR	JIM HOGG		76

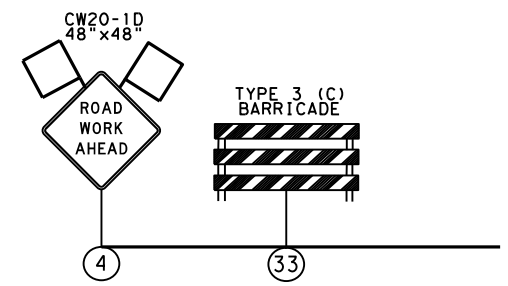
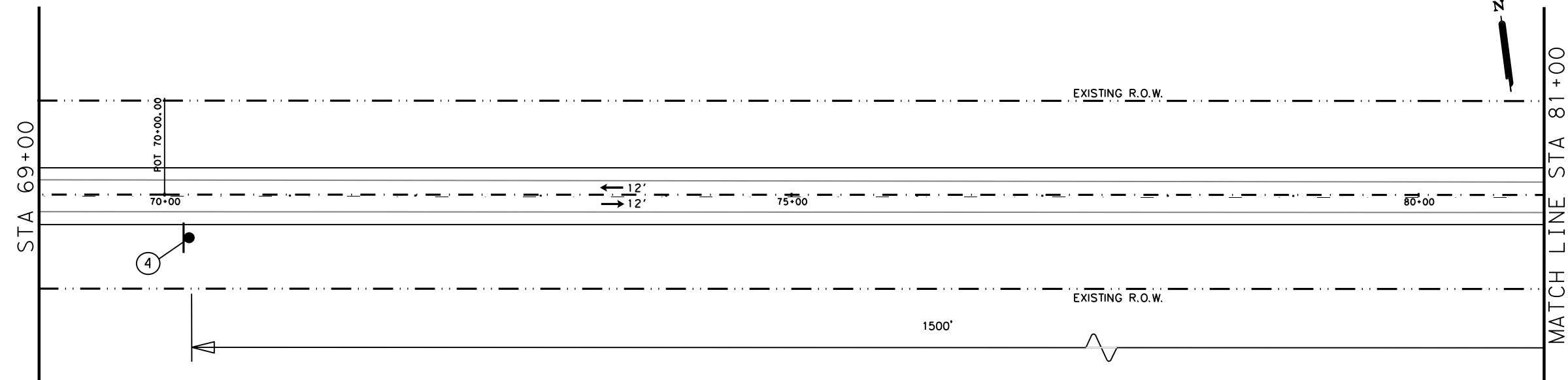
DATE: 3/4/2024 4:58:56 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II STEP I SHEETS

****NOTE TO CONTRACTOR:**

PHASE II STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 99+10, 101+51, 111+00, 121+00, 158+55 AND 164+40. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

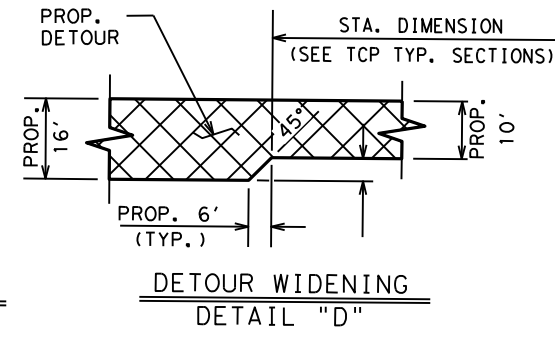
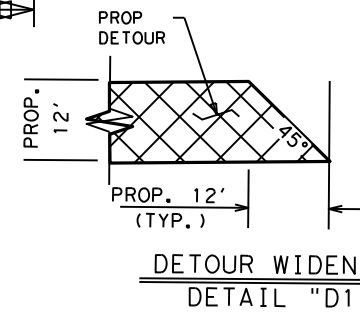
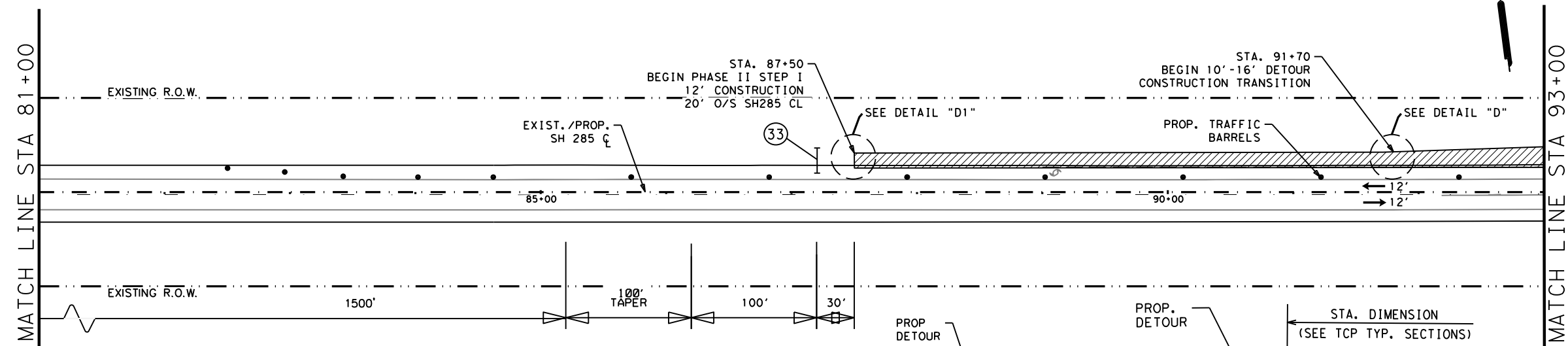
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:


1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN AND CHANNELIZING SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design


Texas Department of Transportation

**SH 285
TCP
PHASE II STEP I**

SCALE: 1" = 100' SHEET 1 OF 4

























CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	77

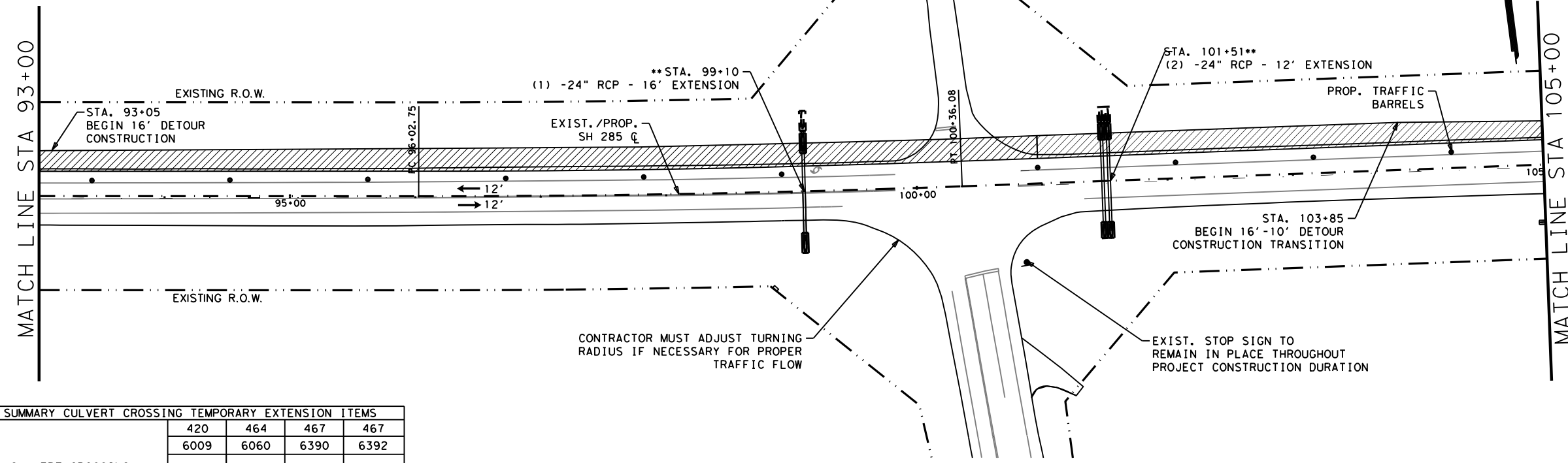
DATE: 3/4/2024 4:58:58 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP Phase II Step I SHEETS

****NOTE TO CONTRACTOR:**

PHASE II STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 99+10, 101+51, 111+00, 121+00, 158+55 AND 164+40. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

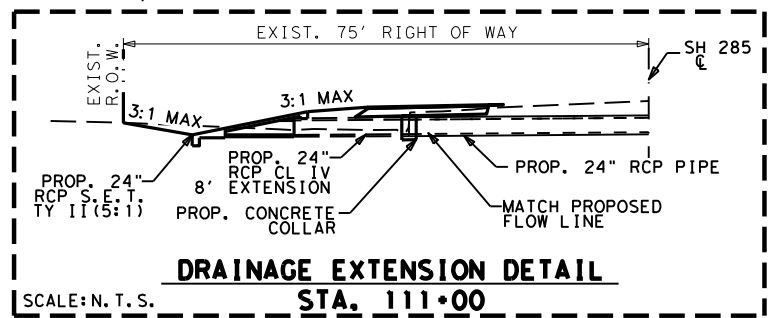
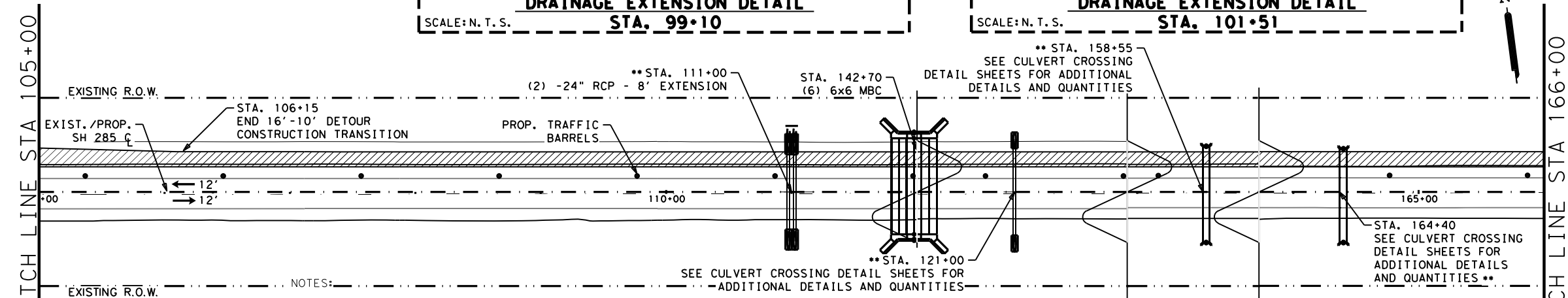
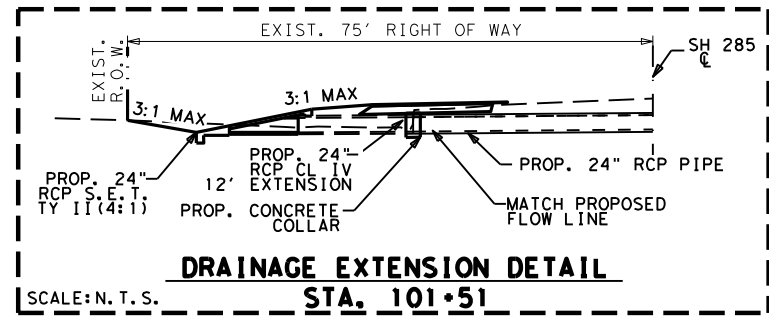
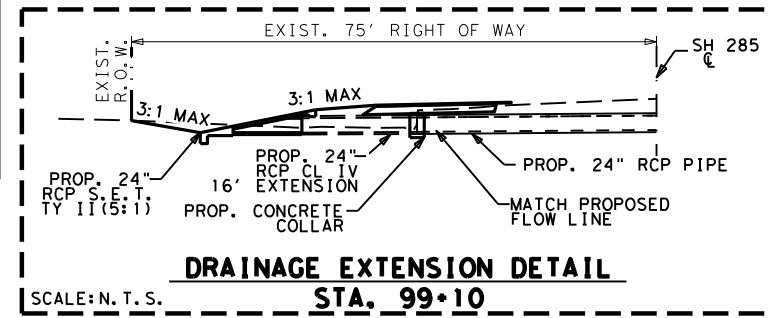
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



CULVERT CROSSING STATION	SUMMARY CULVERT CROSSING TEMPORARY EXTENSION ITEMS			
	420 6009	464 6060	467 6390	467 6392
	CONC. COLLAR	24" RCP (CL IV)	24" 4:1 SET (TY II)	24" 5:1 SET (TY II)
	(EA)	(LF)	(EA)	(EA)
STA. 99+10	1	16	-	1
STA. 101+51	1	12	1	-
STA. 111+00	1	8	-	1
TOTAL	3	36	1	2

NOTE: TOTAL QUANTITIES REFLECTED IN "SUMMARY OF DRAINAGE ITEMS" TABLE.



- NOTES:
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN AND CHANNELIZING SPACING
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS TO
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE II STEP I**

SCALE: 1" = 100' SHEET 2 OF 4

























© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
			COUNTY	SHEET NO.
	PHR		JIM HOGG	78

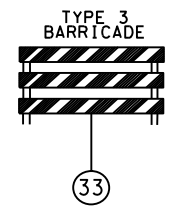
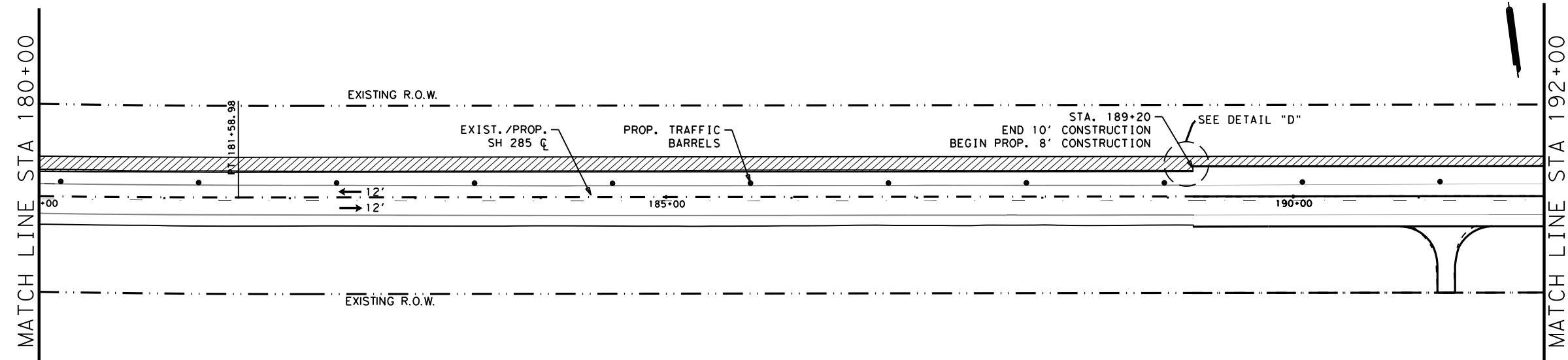
DATE: 3/4/2024 4:59:01 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II STEP I SHEETS

****NOTE TO CONTRACTOR:**

PHASE II STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 99+10, 101+51, 111+00, 121+00, 158+55 AND 164+40. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

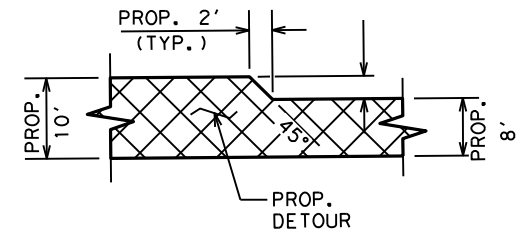
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG

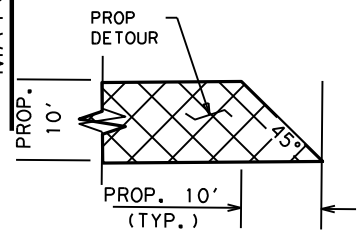
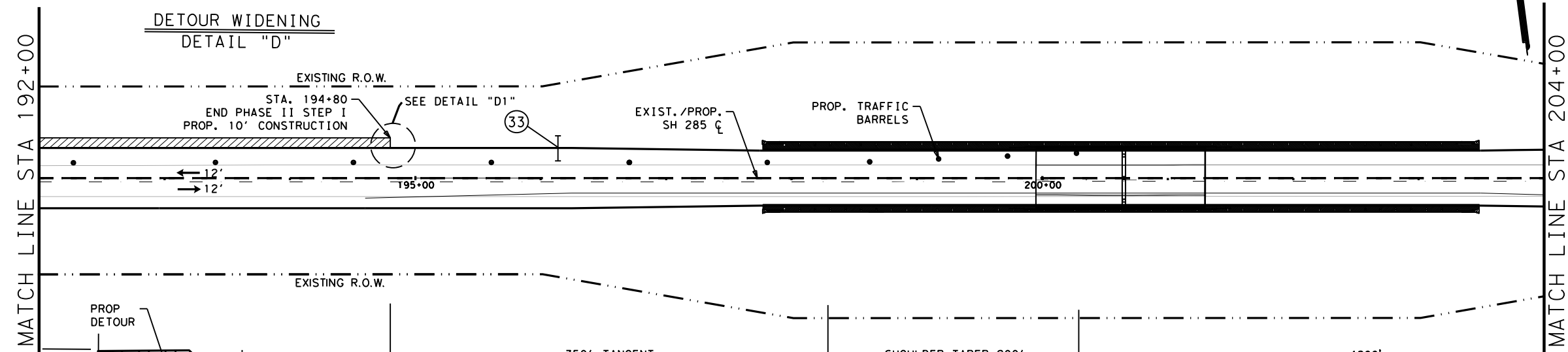


NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN AND CHANNELIZING SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



**DETOUR WIDENING
DETAIL "D"**



**DETOUR WIDENING
DETAIL "D1"**



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
PHASE II STEP I**

SCALE: 1" = 100' SHEET 3 OF 4

























CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		79

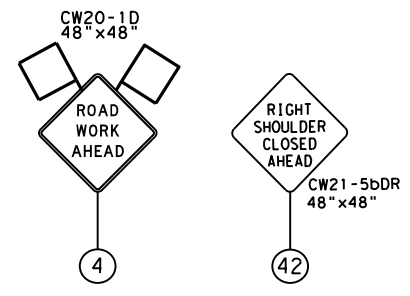
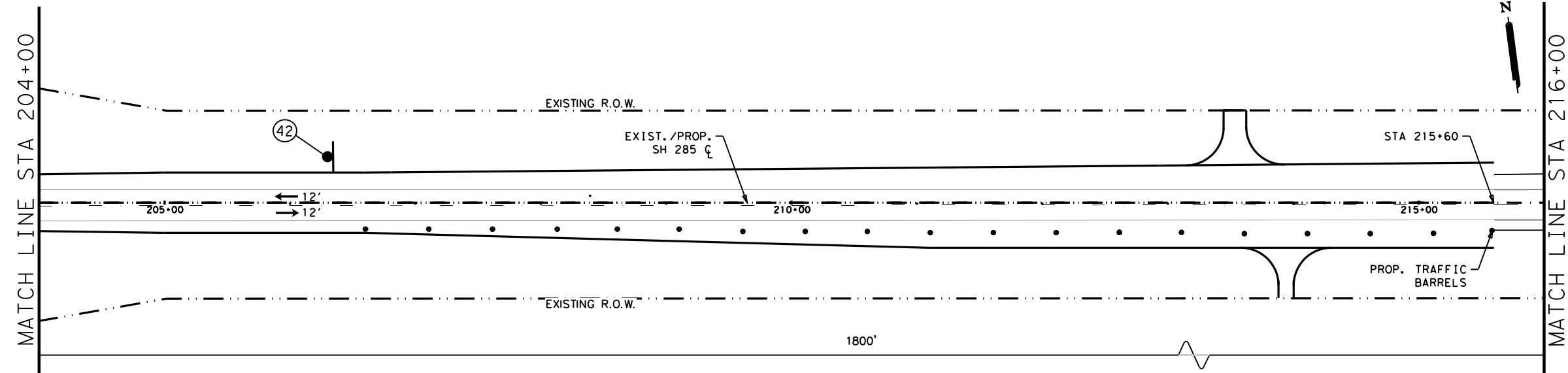
DATE: 3/4/2024 4:59:03 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP I SHEETS

****NOTE TO CONTRACTOR:**

PHASE II STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 99+10, 101+51, 111+00, 121+00, 158+55 AND 164+40. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

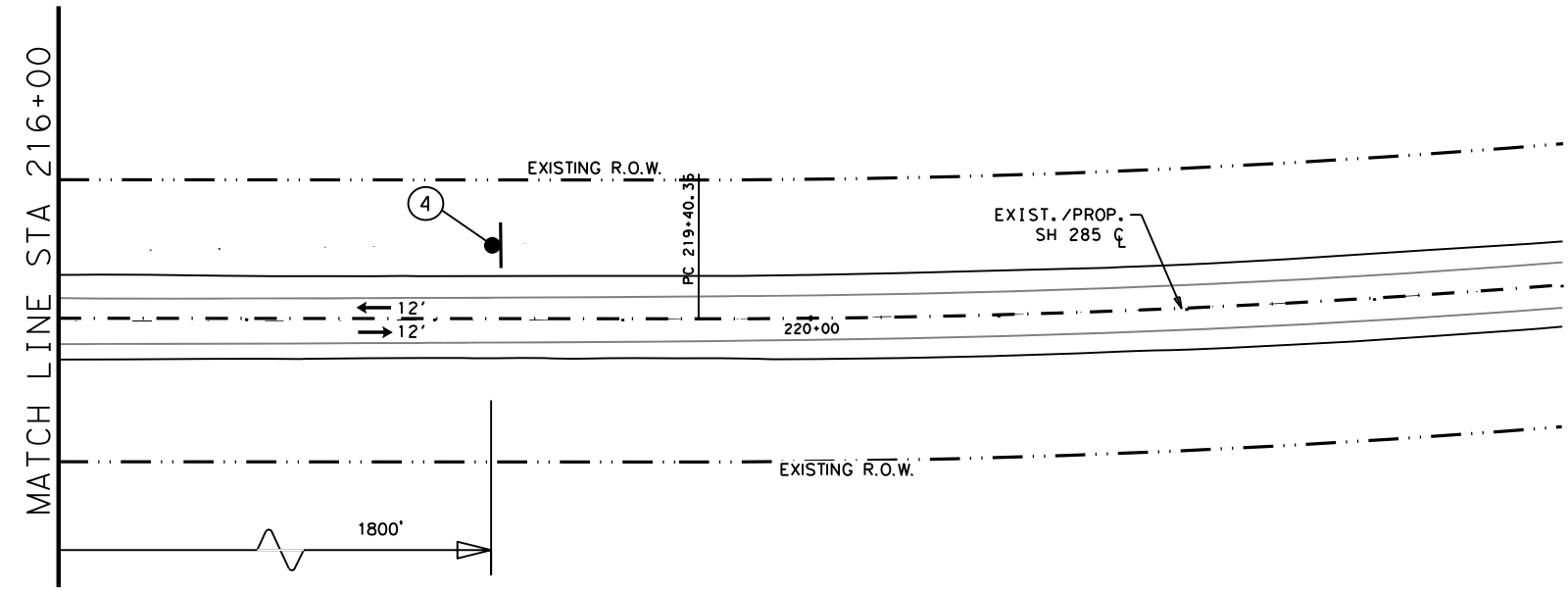
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN AND CHANNELIZING SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
PHASE II STEP I**

SCALE: 1" = 100' SHEET 4 OF 4

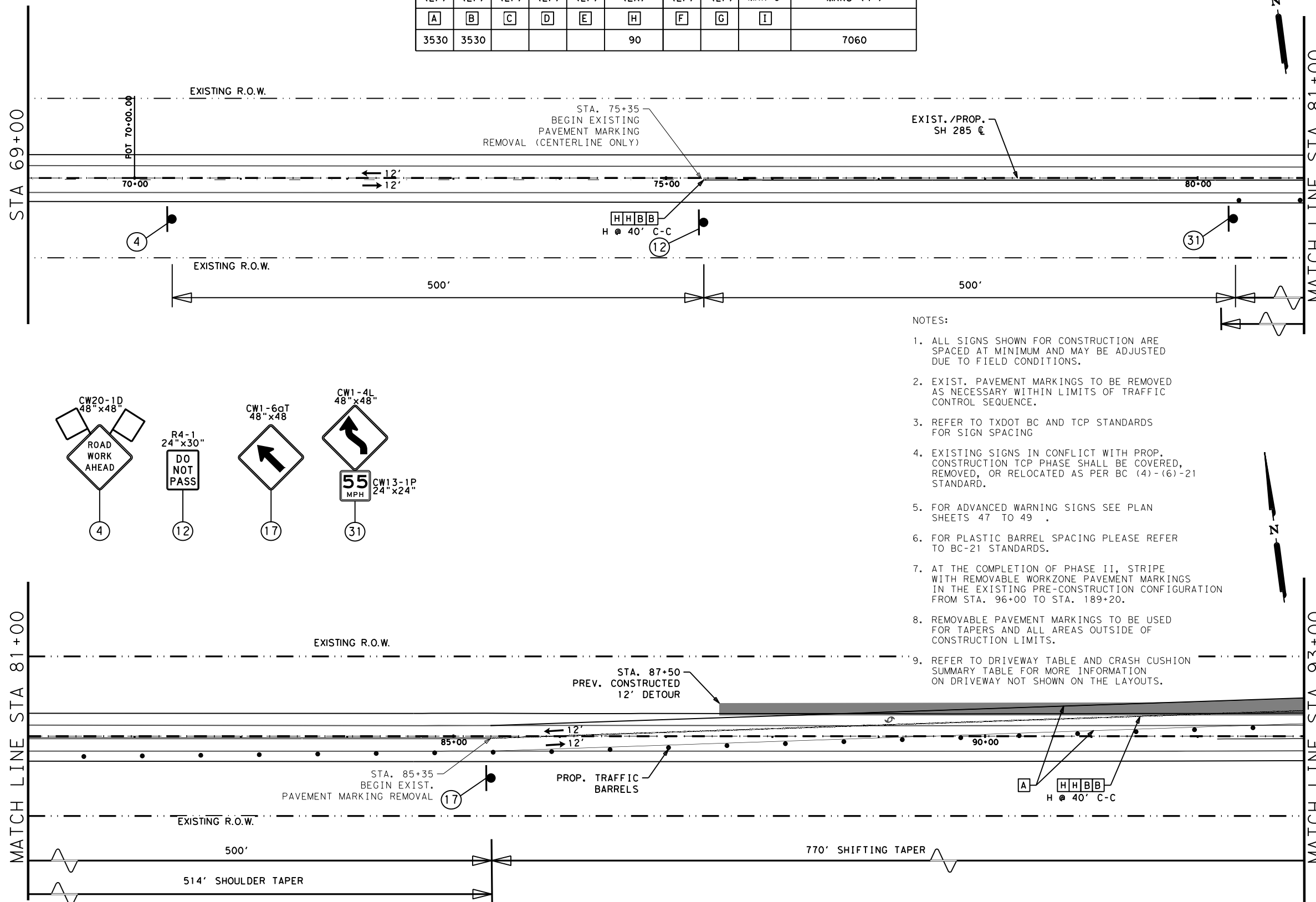
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	80	

DATE: 3/4/2024 4:59:08 PM
 FILE: P:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
3530	3530				90				7060

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TCP PHASE II STEP II

SCALE: 1" = 100' SHEET 1 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		81

DATE: 3/4/2024 4:59:10 PM
 FILE: \\txdot\project\wison\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP II SHEETS.dgn

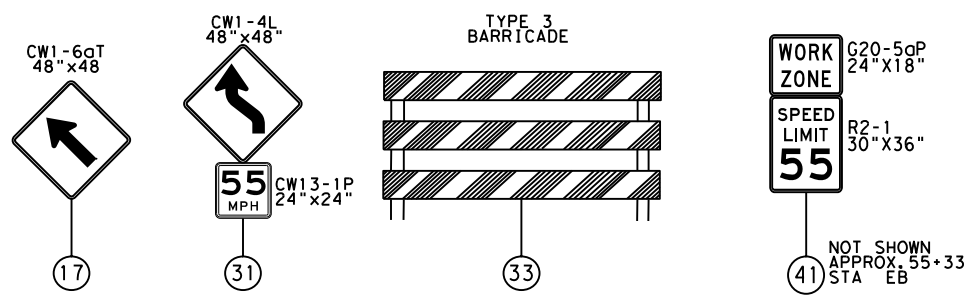
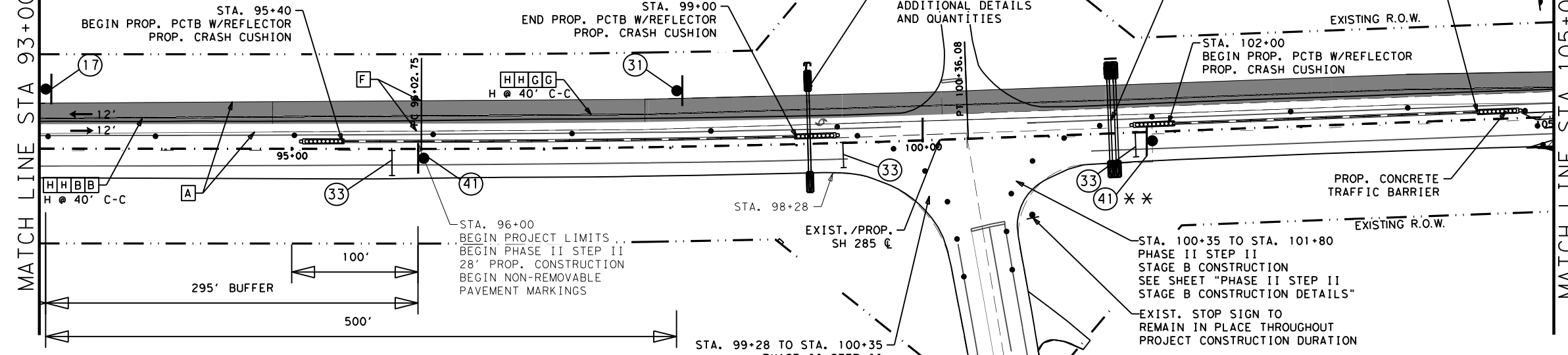
WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)				678	677
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
600	600				120	4200	4200		2900

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REF) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

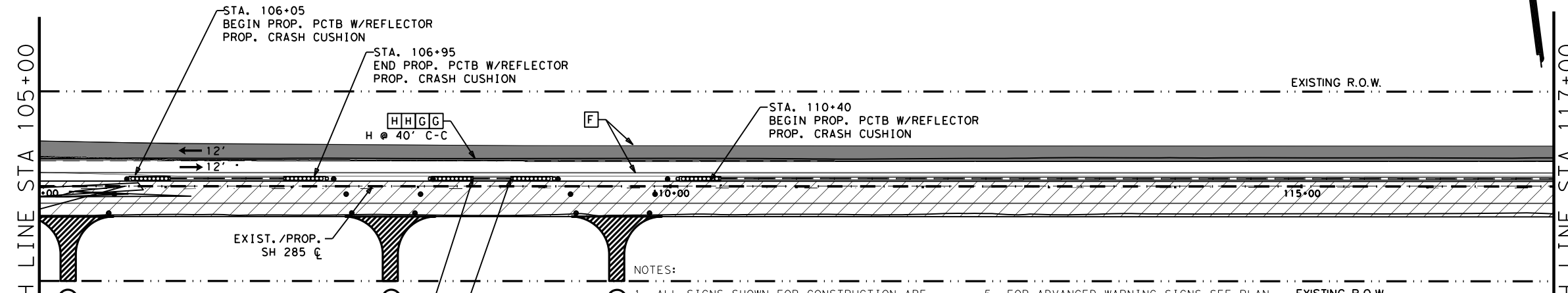
MATCH LINE STA 93+00

MATCH LINE STA 105+00

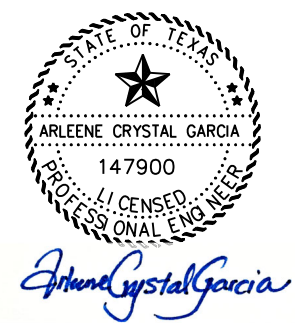


MATCH LINE STA 105+00

MATCH LINE STA 117+00



- NOTES:**
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE II STEP II**

SCALE: 1"=100' SHEET 2 OF 5

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	82	

DATE: 3/4/2024 4:59:12 PM
 FILE: P:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
					205	8200	8200		5800

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 117+00 TO STA. 134+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

MATCH LINE STA 134+00

MATCH LINE STA 146+00

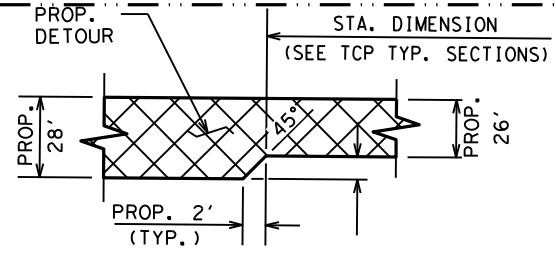
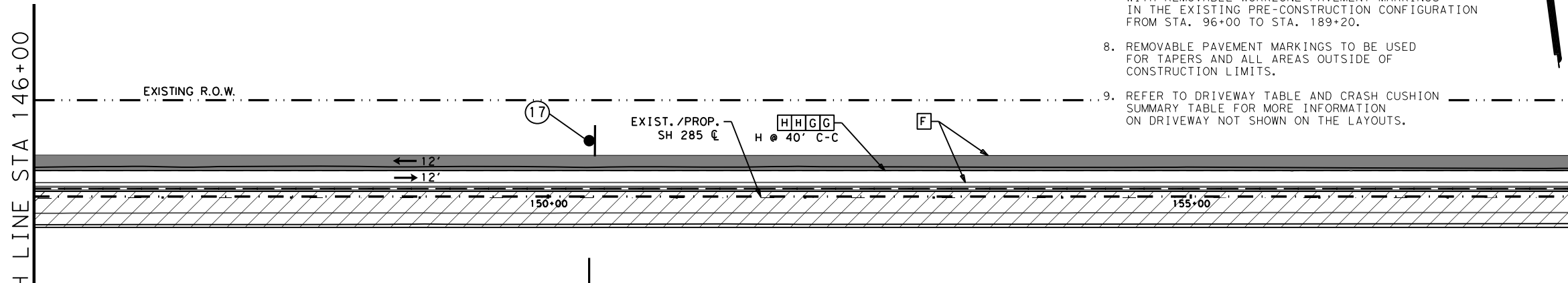


NOTES:

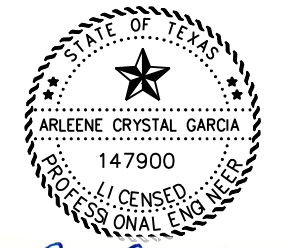
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.

MATCH LINE STA 146+00

MATCH LINE STA 158+00



DETOUR WIDENING
DETAIL "D"



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design
 Texas Department of Transportation

SH 285
TCP
PHASE II STEP II

SCALE: 1" = 100' SHEET 3 OF 5

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	83	

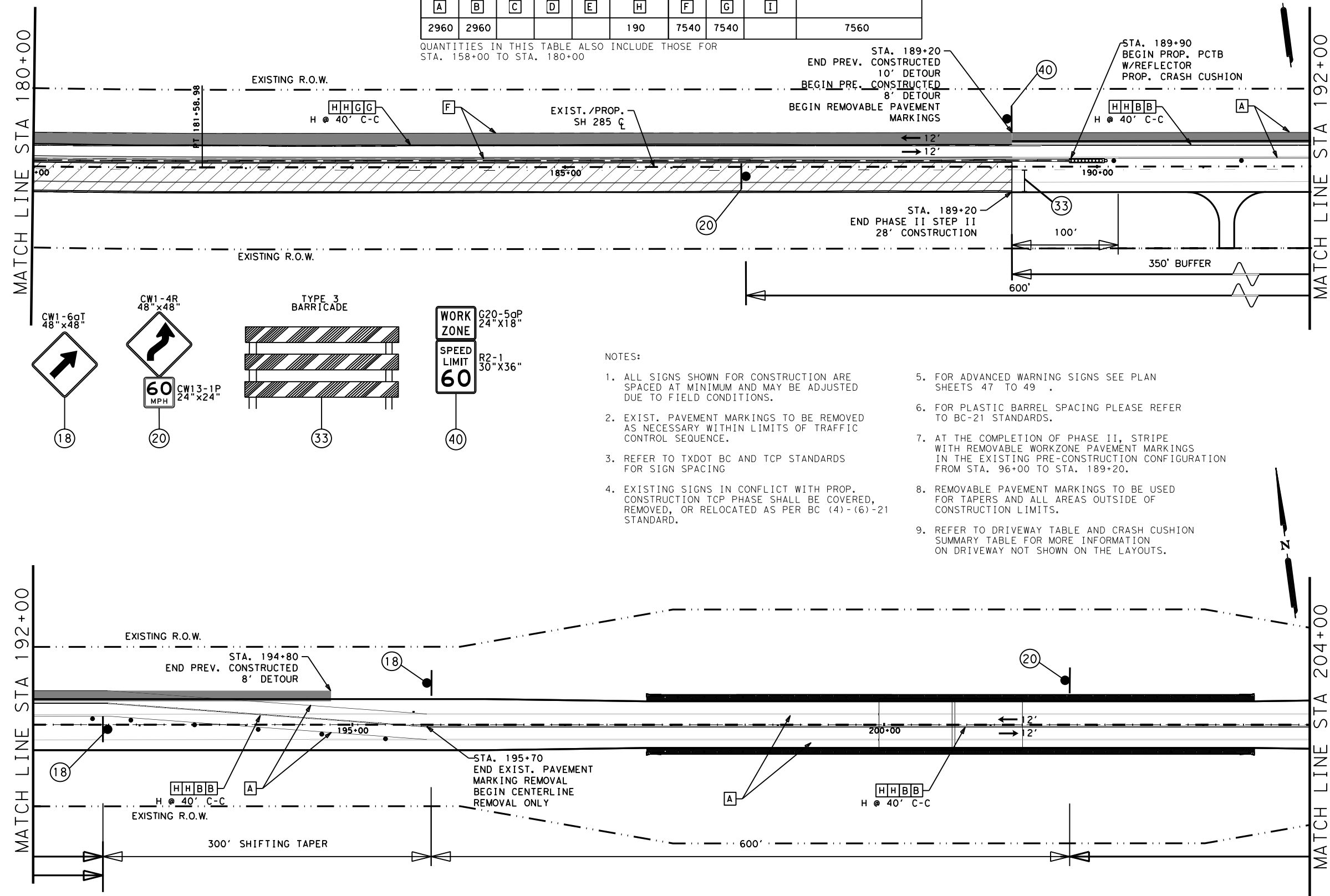
DATE: 3/4/2024 4:59:14 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)	
(REMOVABLE)				(NON-REMOV)				678	677	
662	662	662	662	662	662	662	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"		ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I		
2960	2960				190	7540	7540			7560

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 158+00 TO STA. 180+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING.
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TCP PHASE II STEP II

SCALE: 1" = 100' SHEET 4 OF 5

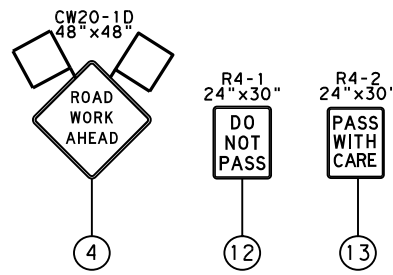
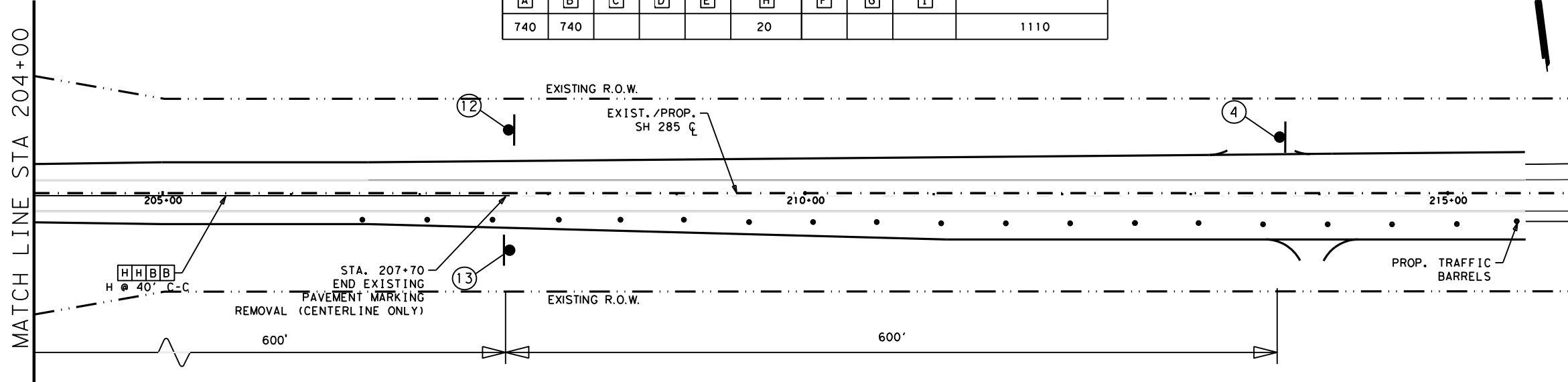
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	84	

DATE: 3/4/2024 4:59:16 PM
 FILE: \\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
740	740				20				1110

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE II STEP II**

SCALE: 1" = 100' SHEET 5 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		85

DATE: 2/16/2024 3:37:03 PM
 FILE: \\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II STEP II SHEET A SHEETS

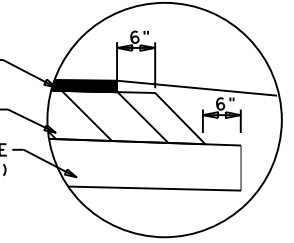
WORK ZONE PAVEMENT MARKINGS (ITEM 662)						PAV SURF PREP		ELIM. EXIST. PAV. MARKINGS (ITEM 677)	
(REMOVABLE)						(NON-REMOV)			
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
				36		7200	7200		3700

LEGEND

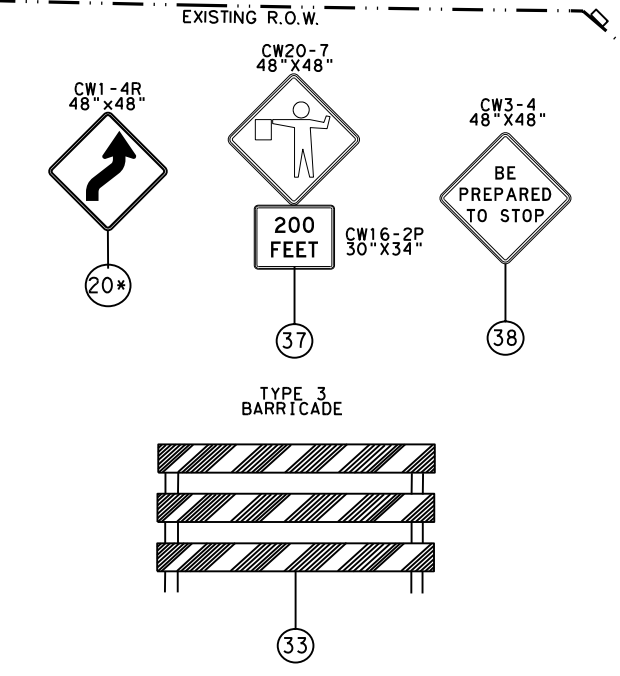
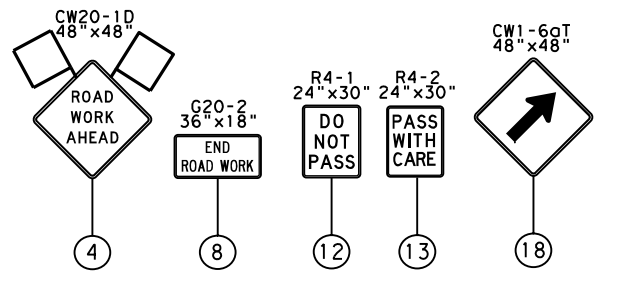
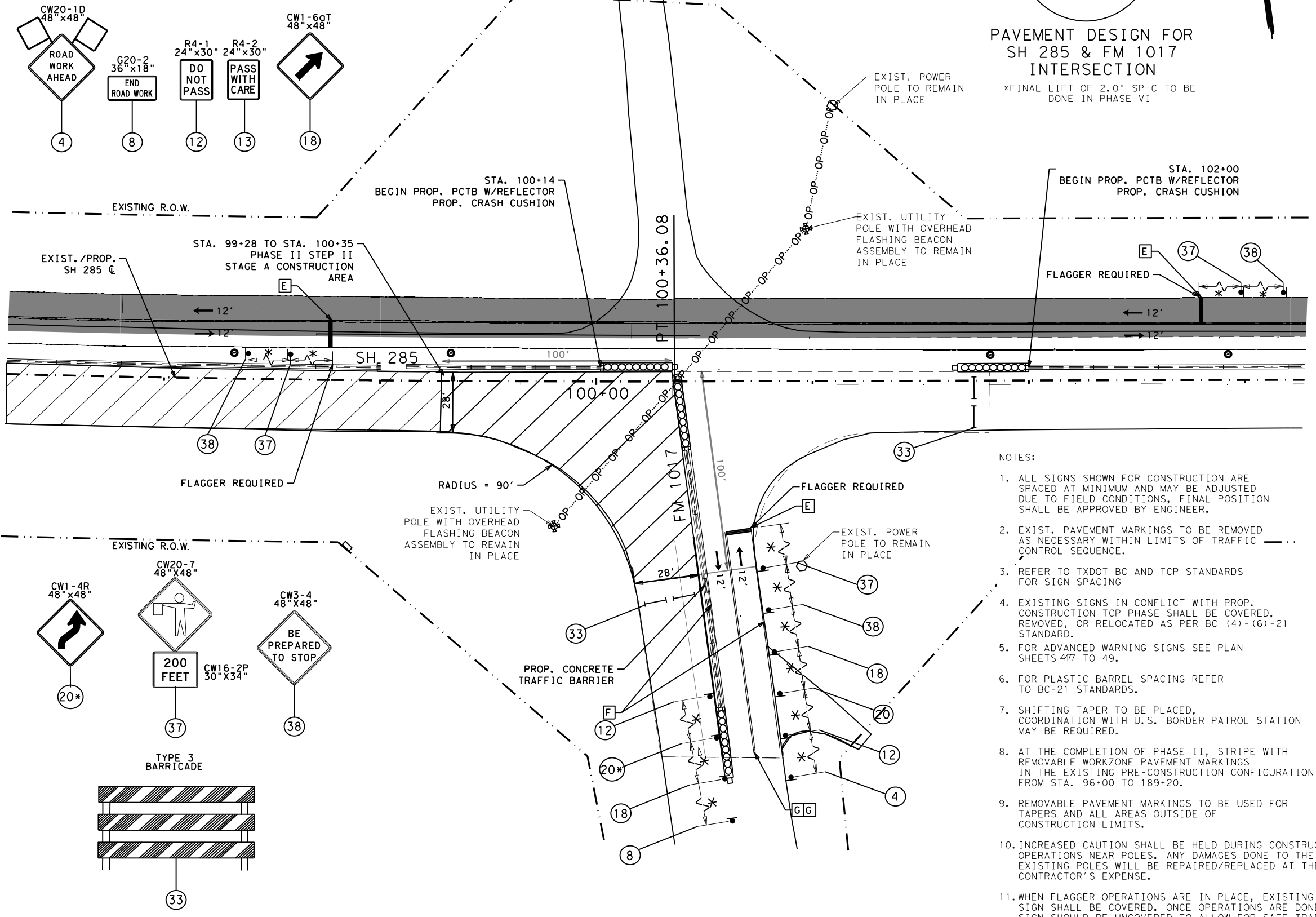
- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG

PAVEMENT DESIGN FOR SH 285 & FM 1017 INTERSECTION

2.5" SP-B PG 76-22 SAC B PRIME COAT (MC-30)
 16.0" TY-E GR-4 w/2% CEMENT BY WEIGHT
 PROOF ROLL SUBGRADE (ADD MOISTURE BEFORE PROOF ROLLING)

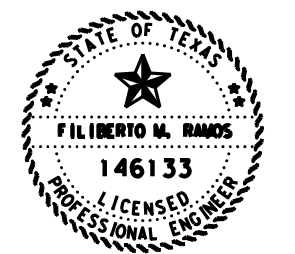


*FINAL LIFT OF 2.0" SP-C TO BE DONE IN PHASE VI



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS, FINAL POSITION SHALL BE APPROVED BY ENGINEER.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 447 TO 449.
6. FOR PLASTIC BARREL SPACING REFER TO BC-21 STANDARDS.
7. SHIFTING TAPER TO BE PLACED, COORDINATION WITH U.S. BORDER PATROL STATION MAY BE REQUIRED.
8. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO 189+20.
9. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
10. INCREASED CAUTION SHALL BE HELD DURING CONSTRUCTION OPERATIONS NEAR POLES. ANY DAMAGES DONE TO THE EXISTING POLES WILL BE REPAIRED/REPLACED AT THE CONTRACTOR'S EXPENSE.
11. WHEN FLAGGER OPERATIONS ARE IN PLACE, EXISTING STOP SIGN SHALL BE COVERED. ONCE OPERATIONS ARE DONE, STOP SIGN SHOULD BE UNCOVERED TO ALLOW FOR SAFE TRAFFIC FLOW.



Filiberto M. Ramos
 02/16/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TCP PHASE II STEP II STAGE A

SCALE: 1"=50' SHEET 1 OF 1

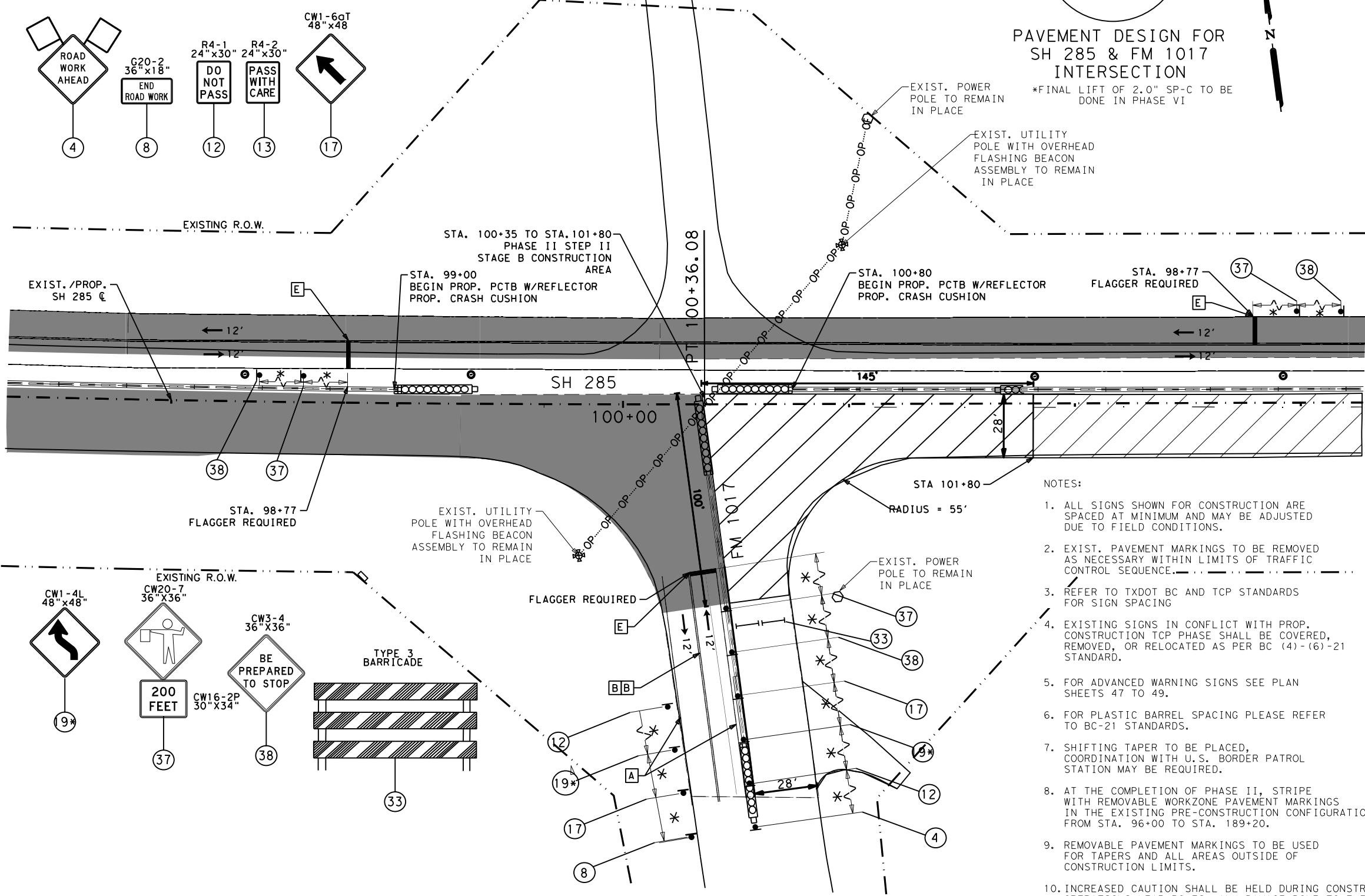
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	86	

DATE: 2/16/2024 3:37:08 PM
 FILE: \\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP II SHEET B SHEETS

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)								(NON-REMOV)	
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
7200	7200			36					3396

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



PAVEMENT DESIGN FOR SH 285 & FM 1017 INTERSECTION

*FINAL LIFT OF 2.0" SP-C TO BE DONE IN PHASE VI

NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. SHIFTING TAPER TO BE PLACED, COORDINATION WITH U.S. BORDER PATROL STATION MAY BE REQUIRED.
8. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
9. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
10. INCREASED CAUTION SHALL BE HELD DURING CONSTRUCTION OPERATIONS NEAR POLES. ANY DAMAGE DONE TO THE EXISTING POLES WILL BE REPAIRED/REPLACED AT THE CONTRACTOR'S EXPENSE.
11. WHEN FLAGGER OPERATIONS ARE IN PLACE, EXISTING STOP SIGN SHALL BE COVERED. ONCE OPERATIONS ARE DONE, STOP SIGN SHOULD BE UNCOVERED TO ALLOW FOR SAFE TRAFFIC FLOW.



Filiberto M. Ramos
 02/16/2024

Pharr District Central Design

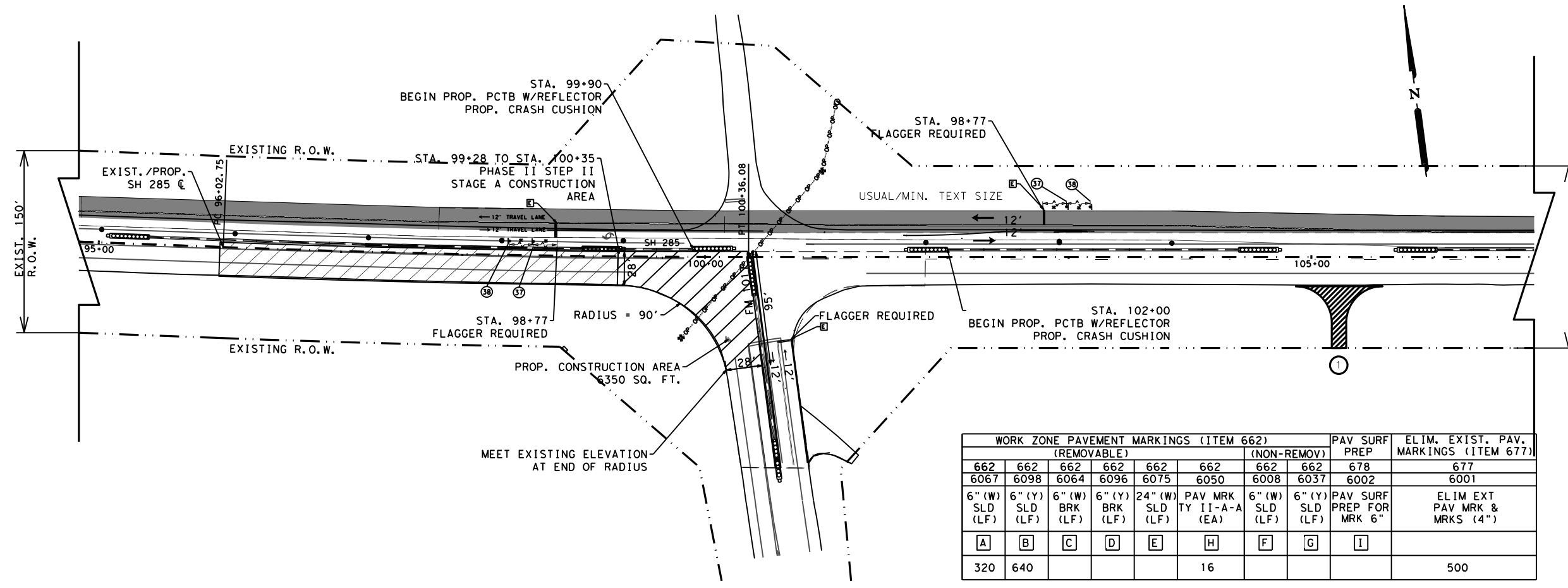
Texas Department of Transportation

SH 285 TCP PHASE II STEP II STAGE B

SCALE: 1"=50' SHEET 1 OF 1

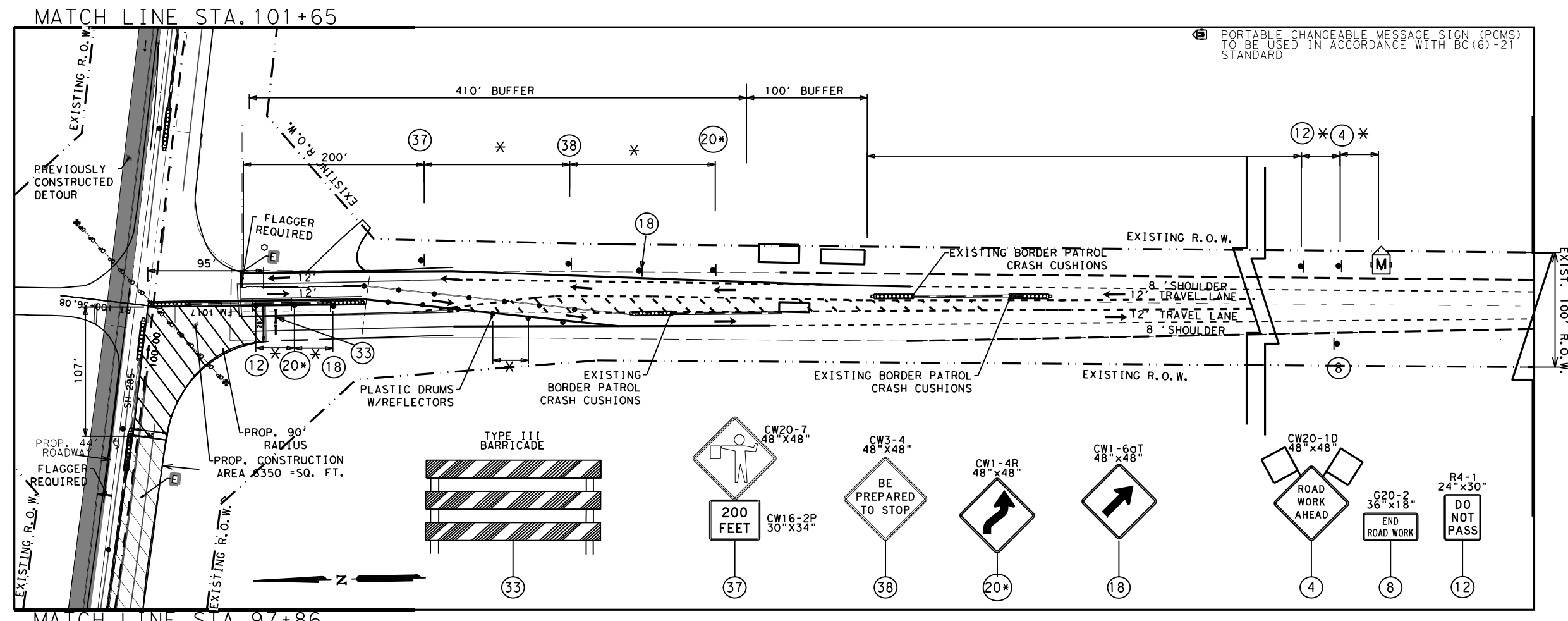
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		87

DATE: 2/16/2024 3:37:12 PM
 FILE: D:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP II Stage A

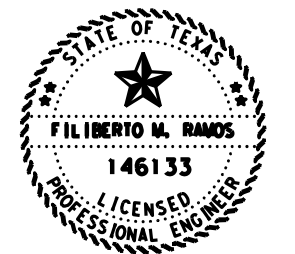


WORK ZONE PAVEMENT MARKINGS (ITEM 662)							PAV SURF PREP		ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)							(NON-REMOV)		
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MKR TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MKR & MRKS (4")
A	B	C	D	E	H	F	G	I	
320	640				16				500

- LEGEND**
- CONSTRUCTION AREA
 - TEMPORARY ROADWAY WIDENING
 - PREVIOUSLY CONSTRUCTED
 - CONCRETE TRAFFIC BARRIER (CTB)
 - CRASH CUSHION ATTENUATOR
 - TEMPORARY TRAFFIC SIGNAL
 - WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
 - WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
 - WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
 - WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
 - WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
 - WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
 - PAV SURF PREP FOR MRK (6")
 - WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
 - WORK ZONE PVMT MARK (REM) (W) (ARROW)
 - WORK ZONE PVMT MARK (REM) (W) (WORD)
 - DIRECTION OF TRAFFIC FLOW
 - TYPE 3 BARRICADE
 - CHANNELIZING DEVICES (PLASTIC DRUMS)
 - REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
 - CONSTRUCTION SIGN
 - FLAG
 - PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)



PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) TO BE USED IN ACCORDANCE WITH BC(6)-21 STANDARD



Filiberto M. Ramos
 02/16/2024

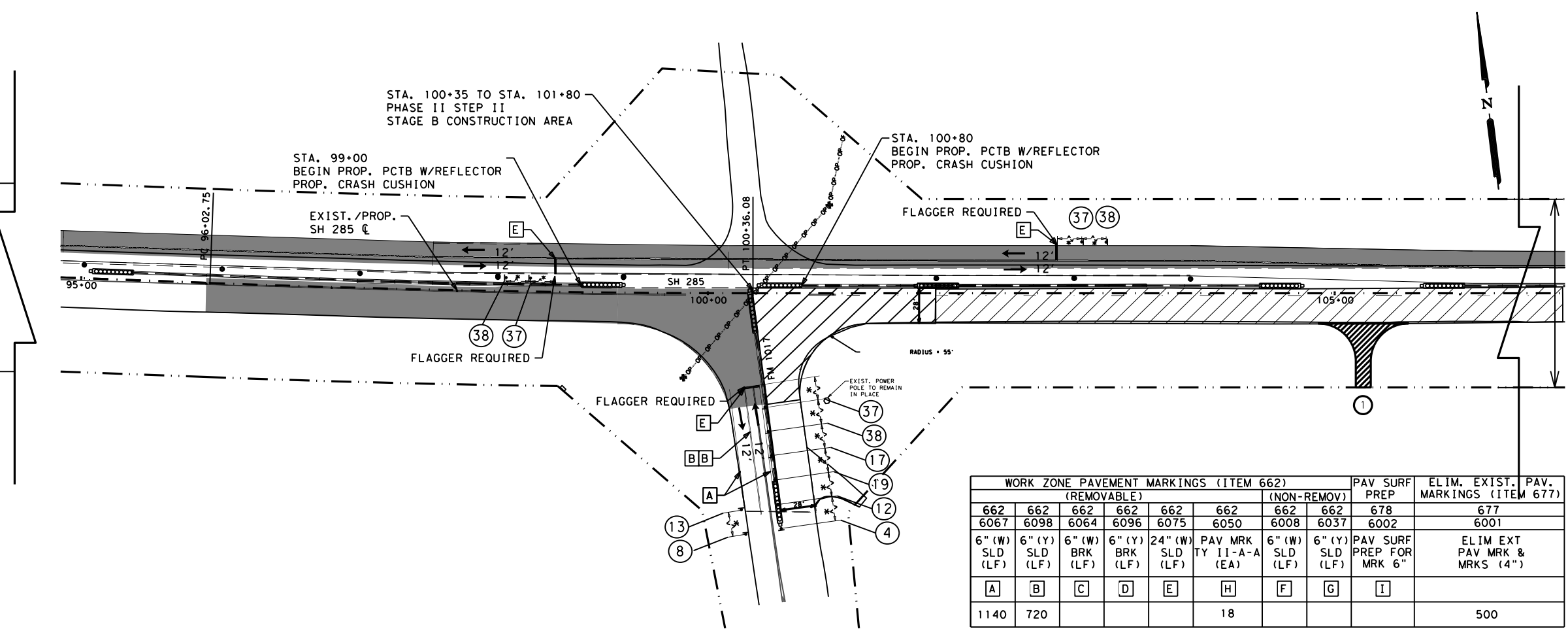
Pharr District Central Design

Texas Department of Transportation

**SH 285
 TCP
 PHASE II STEP II
 STAGE A**

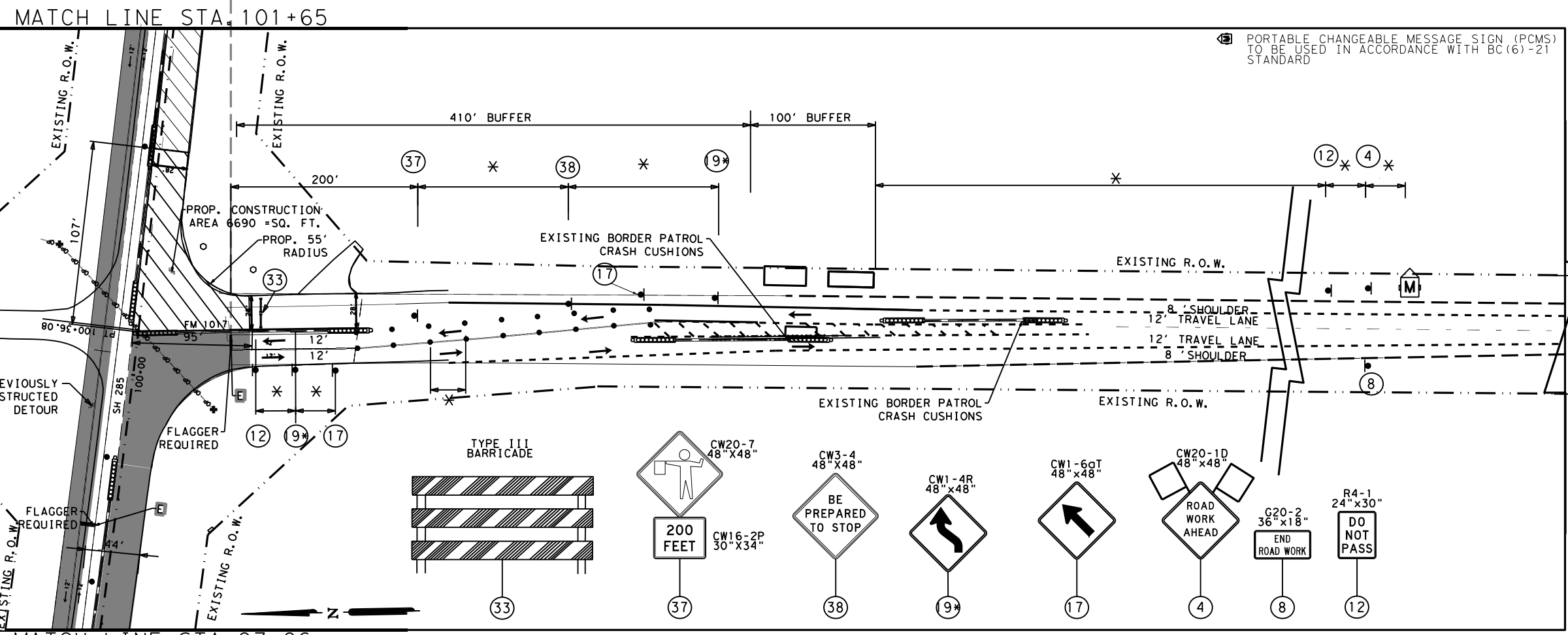
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	88

DATE: 2/16/2024 3:37:17 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP Phase II Step II Stage B Construction Area\150 - TCP Phase II Step II Stage B Construction Area\150 - TCP Phase II Step II Stage B Construction Area.dwg

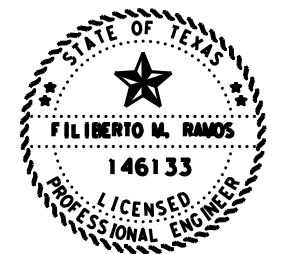
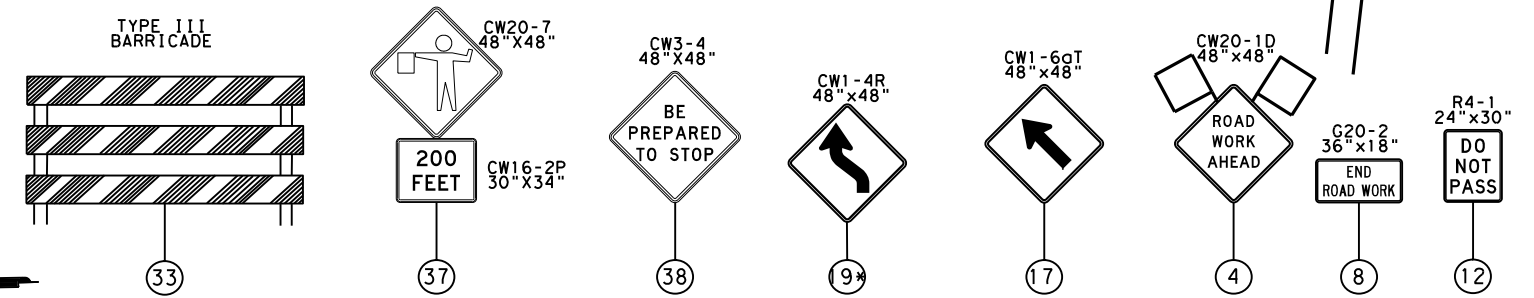


WORK ZONE PAVEMENT MARKINGS (ITEM 662)						PAV SURF PREP		ELIM. EXIST. PAV. MARKINGS (ITEM 677)	
(REMOVABLE)						(NON-REMOV.)			
662	662	662	662	662	662	662	662	677	
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1140	720				18				500

- LEGEND**
- CONSTRUCTION AREA
 - TEMPORARY ROADWAY WIDENING
 - PREVIOUSLY CONSTRUCTED
 - CONCRETE TRAFFIC BARRIER (CTB)
 - CRASH CUSHION ATTENUATOR
 - TEMPORARY TRAFFIC SIGNAL
 - WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
 - WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
 - WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
 - WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
 - WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
 - WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
 - PAV SURF PREP FOR MRK (6")
 - WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
 - WORK ZONE PVMT MARK (REM) (W) (ARROW)
 - WORK ZONE PVMT MARK (REM) (W) (WORD)
 - DIRECTION OF TRAFFIC FLOW
 - TYPE 3 BARRICADE
 - CHANNELIZING DEVICES (PLASTIC DRUMS)
 - REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
 - CONSTRUCTION SIGN
 - FLAG
 - PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)



PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) TO BE USED IN ACCORDANCE WITH BC(6)-21 STANDARD



Filiberto M. Ramos
02/16/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE II STEP II
STAGE B**

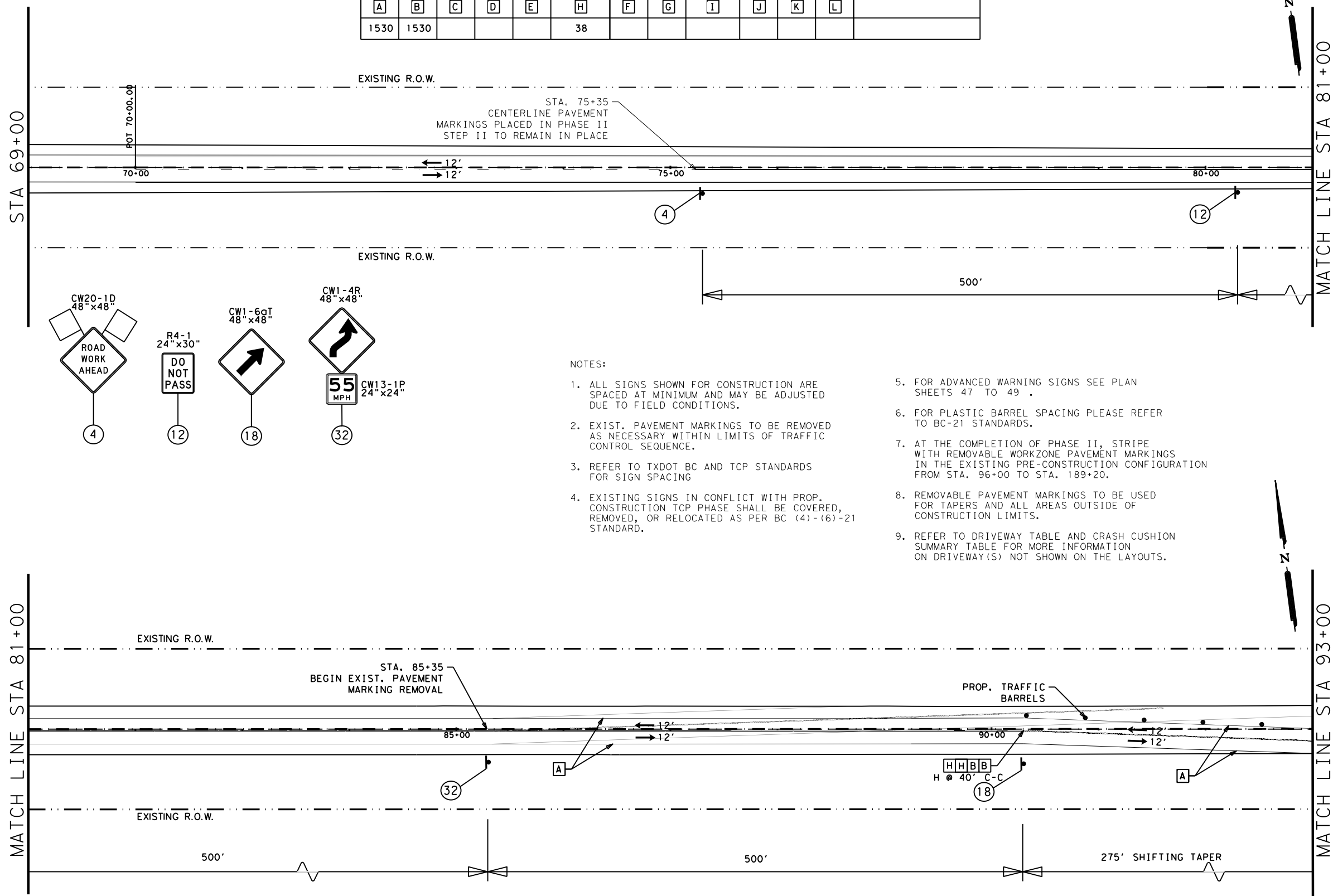
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	89

DATE: 3/4/2024 4:59:40 PM
 FILE: \\phr\project\wisonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)						PAV SURF PREP		WZ PM (ITEM 662)			ELIM. EXIST. PAV. MARKINGS (ITEM 677)	
(REMOVABLE)						(NON-REMOV)		(REMOVABLE)			677	
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	662 6071	662 6080	662 6090	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	8" (W) SLD (LF)	(W) ARROW (EA)	(W) WORD (EA)	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	J	K	L	
1530	1530				38							

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE II STEP III

SCALE: 1" = 100' SHEET 1 OF 5

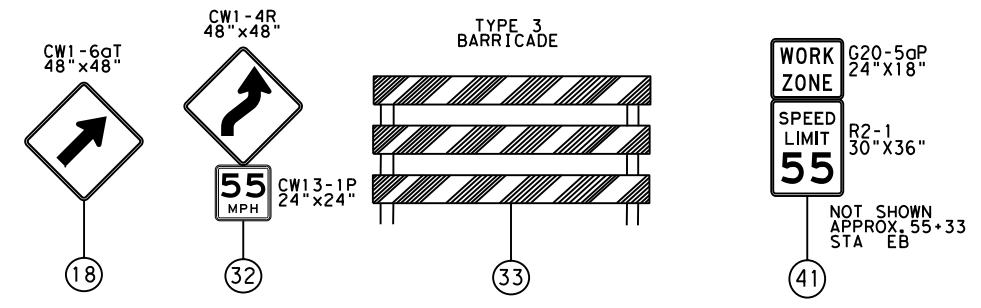
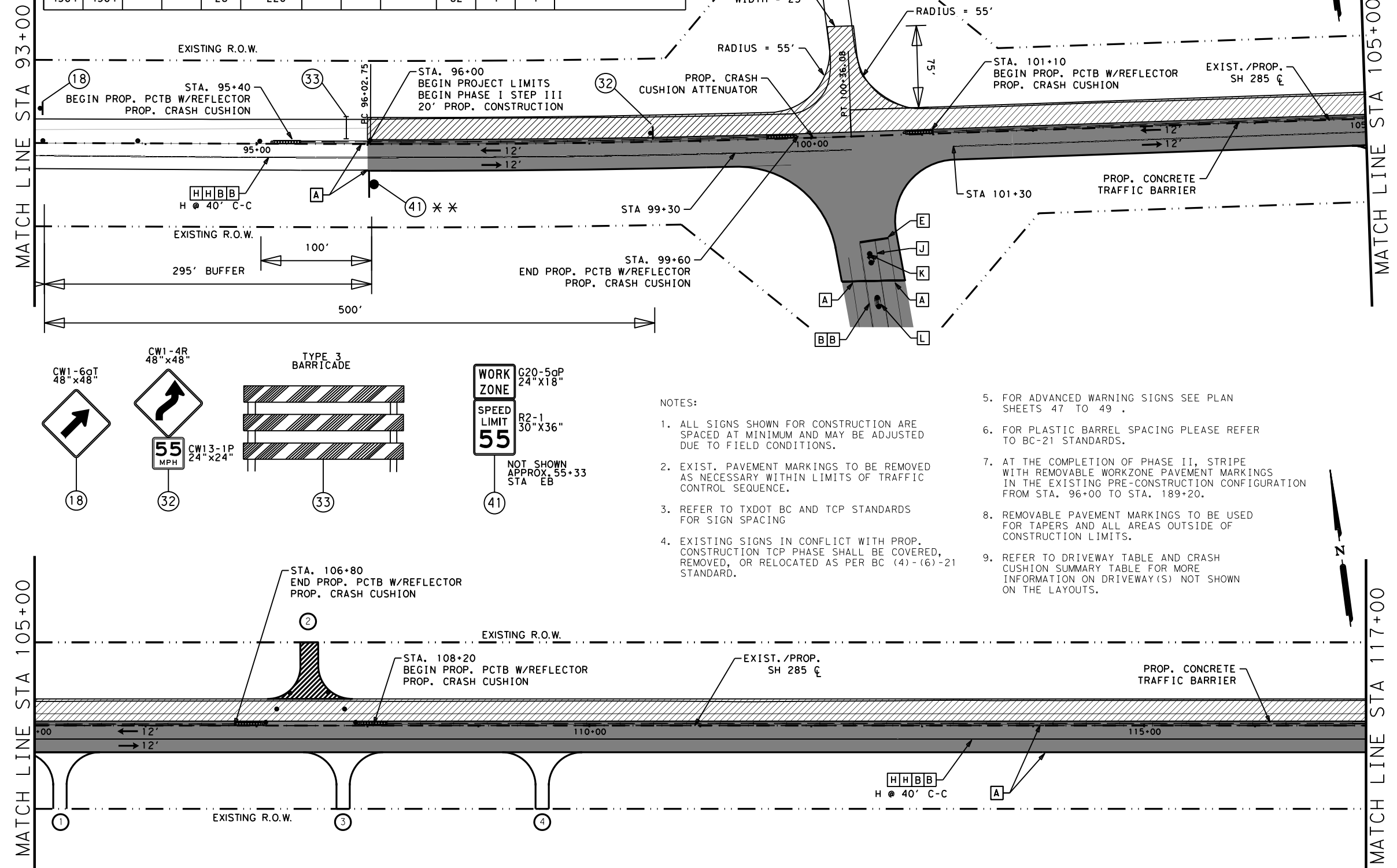
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		90

DATE: 3/4/2024 4:59:41 PM
 FILE: \\txdot\project\wisonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)						PAV SURF PREP		WZ PM (ITEM 662)			ELIM. EXIST. PAV. MARKINGS (ITEM 677)	
(REMOVABLE)						(NON-REMOV.)		(REMOVABLE)			(677)	
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	662 6071	662 6080	662 6090	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	8" (W) SLD (LF)	(W) ARROW (EA)	(W) WORD (EA)	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	J	K	L	
4564	4564			28	220				82	1	1	

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
PHASE II STEP III**

SCALE: 1" = 100' SHEET 2 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	91	

DATE: 3/4/2024 4:59:43 PM
 FILE: \\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE II STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	WZ PM (ITEM 662)			ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					(REMOVABLE)			
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	662 6071	662 6080	662 6090	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	8" (W) SLD (LF)	(W) ARROW (EA)	(W) WORD (EA)	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	J	K	L	
8200	8200				206							

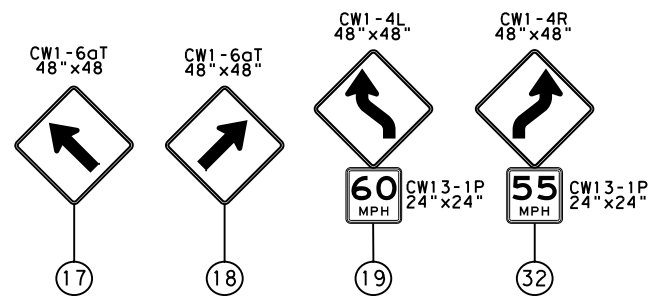
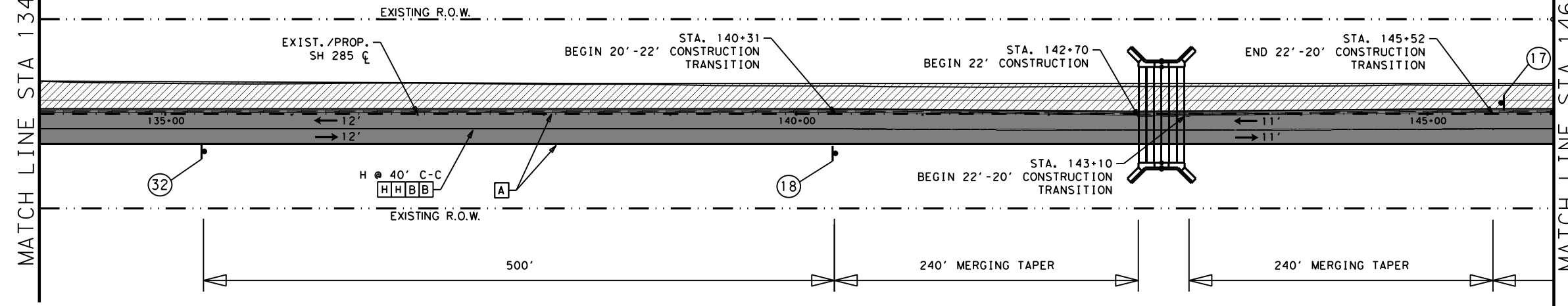
QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 117+00 TO STA. 134+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

MATCH LINE STA 134+00

MATCH LINE STA 146+00

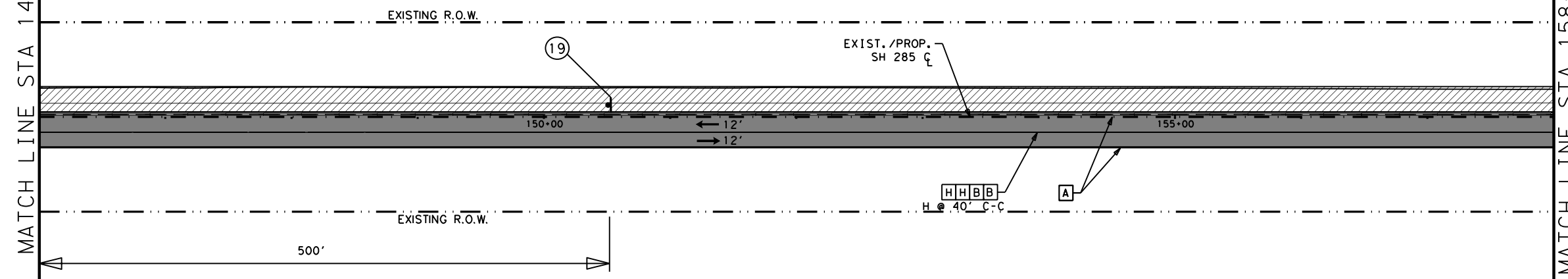


NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE(S) FOR MORE INFORMATION ON DRIVEWAY NOT SHOWN ON THE LAYOUTS.

MATCH LINE STA 146+00

MATCH LINE STA 158+00



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE II STEP III

SCALE: 1" = 100' SHEET 3 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	92	

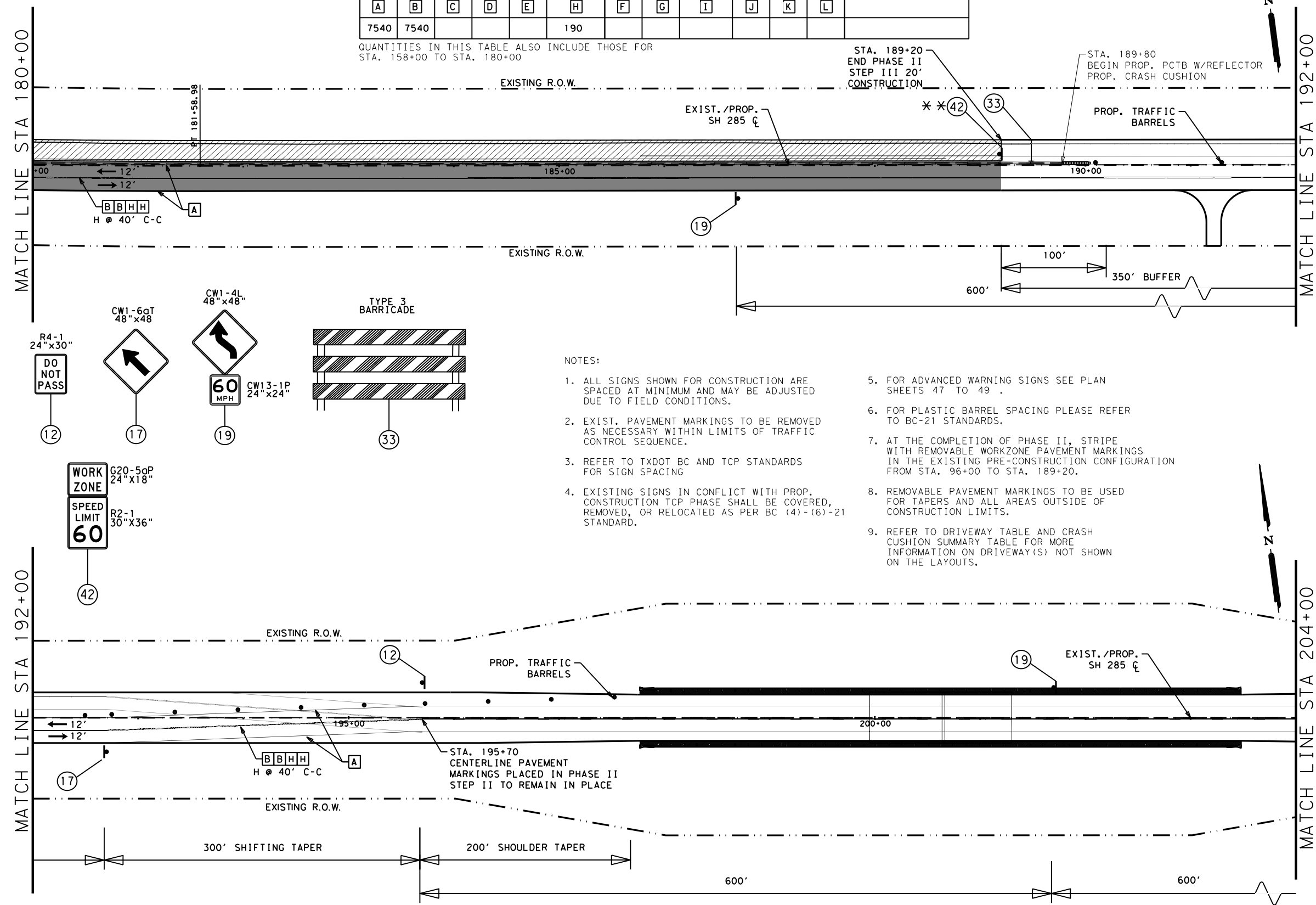
DATE: 3/4/2024 4:59:45 PM
 FILE: P:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE II STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	WZ PM (ITEM 662)			ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					(REMOVABLE)			
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	662 6071	662 6080	662 6090	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	8" (W) SLD (LF)	(W) ARROW (EA)	(W) WORD (EA)	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	J	K	L	
7540	7540				190							

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 158+00 TO STA. 180+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

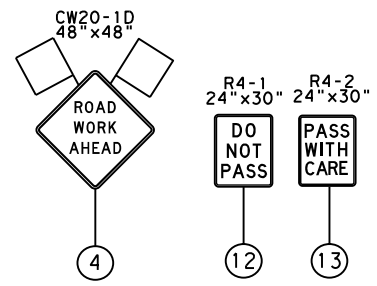
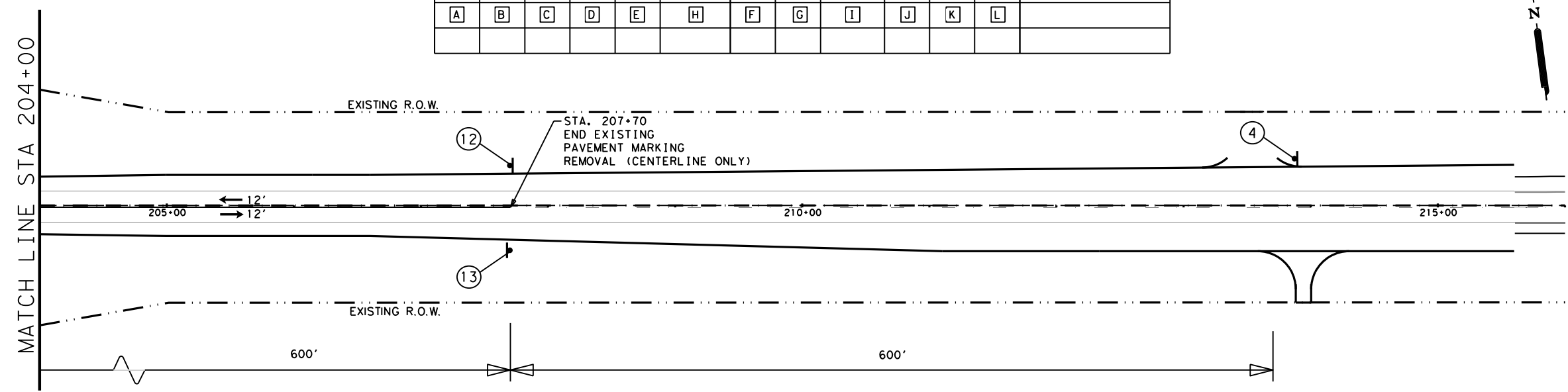


DATE: 3/4/2024 4:59:47 PM
 FILE: P:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase II\STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)							PAV SURF PREP	WZ PM (ITEM 662)			ELIM. EXIST. PAV. MARKINGS (ITEM 677)	
(REMOVABLE)							(NON-REMOV)	(REMOVABLE)			677	
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	662 6071	662 6080	662 6090	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	8" (W) SLD (LF)	(W) ARROW (EA)	(W) WORD (EA)	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	J	K	L	

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE II, STRIPE WITH REMOVABLE WORKZONE PAVEMENT MARKINGS IN THE EXISTING PRE-CONSTRUCTION CONFIGURATION FROM STA. 96+00 TO STA. 189+20.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arleene Crystal Garcia

03.07.2024

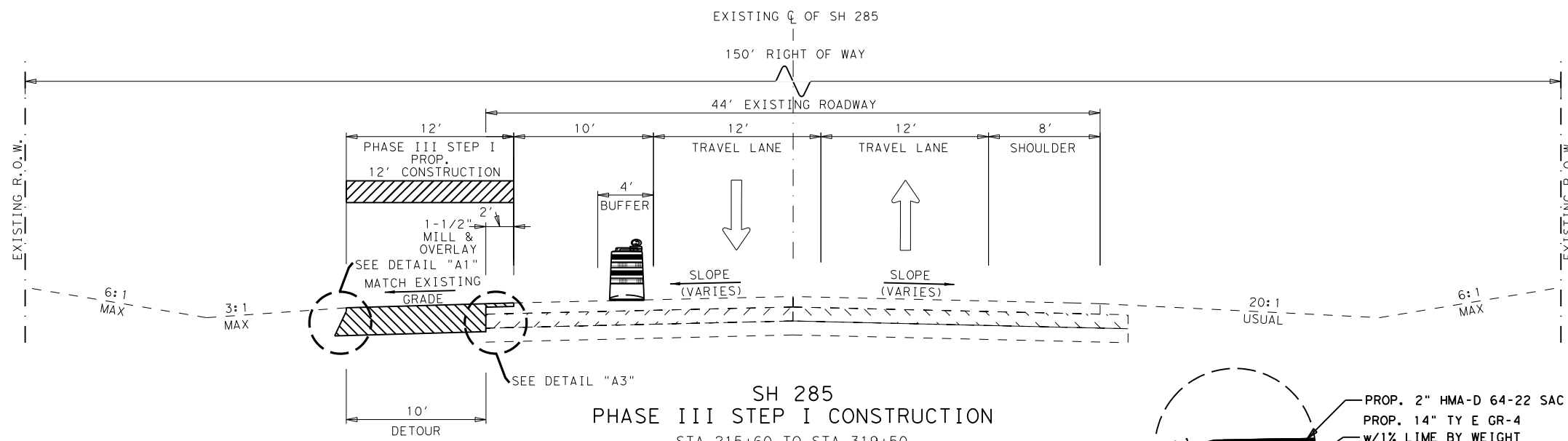
Pharr District Central Design



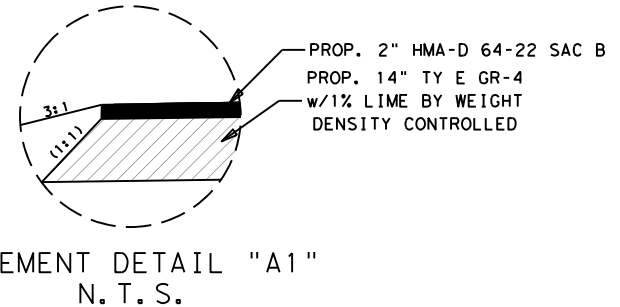
SH 285
 TCP
 PHASE II STEP III

SCALE: 1" = 100'		SHEET 5 OF 5	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO. 94

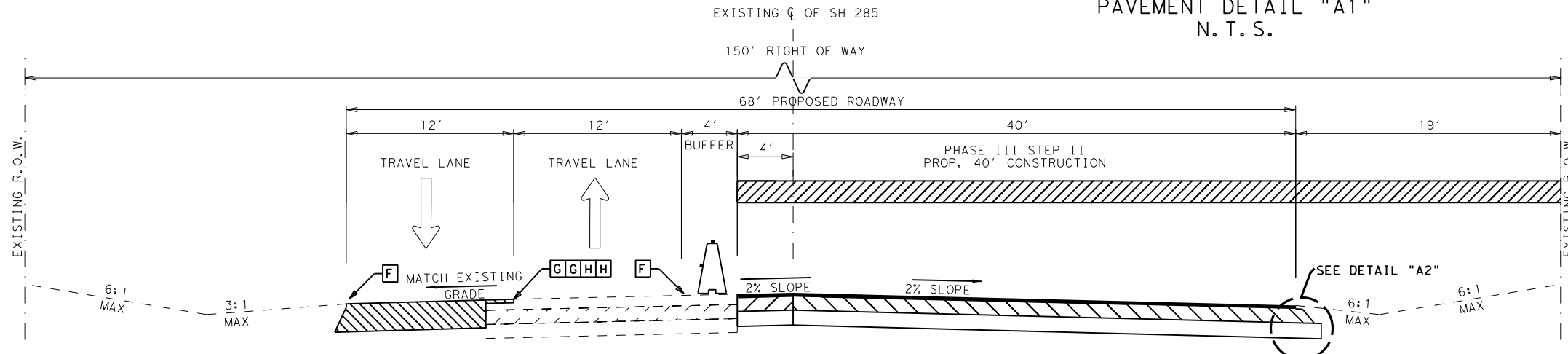
DATE: 3/4/2024 5:00:02 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE III\SH285 TCP PHASE III TYPICALS.dgn



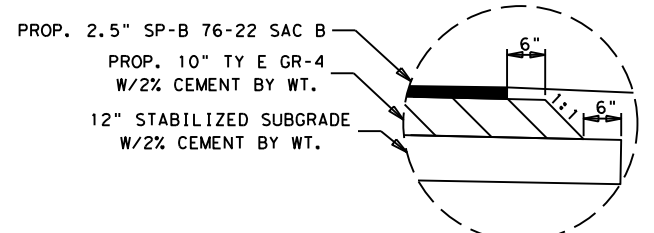
SH 285
 PHASE III STEP I CONSTRUCTION
 STA 215+60 TO STA 319+50



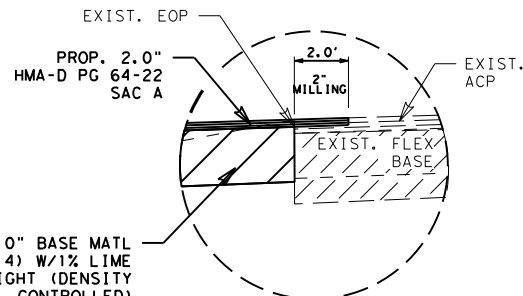
PAVEMENT DETAIL "A1"
 N. T. S.



SH 285
 PHASE III STEP II CONSTRUCTION
 STA 222+60 TO STA 263+90
 STA 263+90 TO STA 272+90 (40' TO 28' TRANSITION)

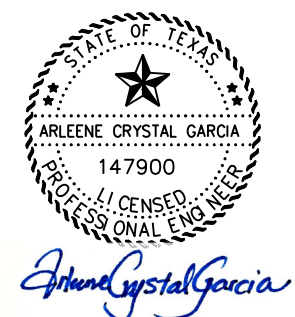


PAVEMENT DETAIL "A2"
 N. T. S.



PAVEMENT DETAIL "A3"
 N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - CENTER LINE
 - PROFILE GRADE LINE
 - PERMISSIBLE CONSTRUCTION JOINT
 - R. O. W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



03.07.2024

Pharr District Central Design

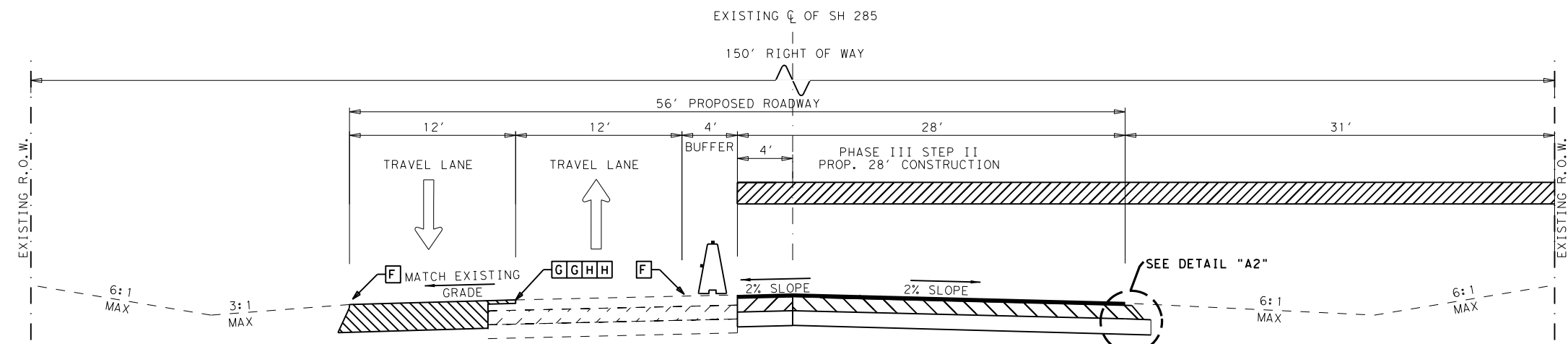
Texas Department of Transportation

SH 285
 TCP
 PHASE III
 TYPICAL SECTIONS

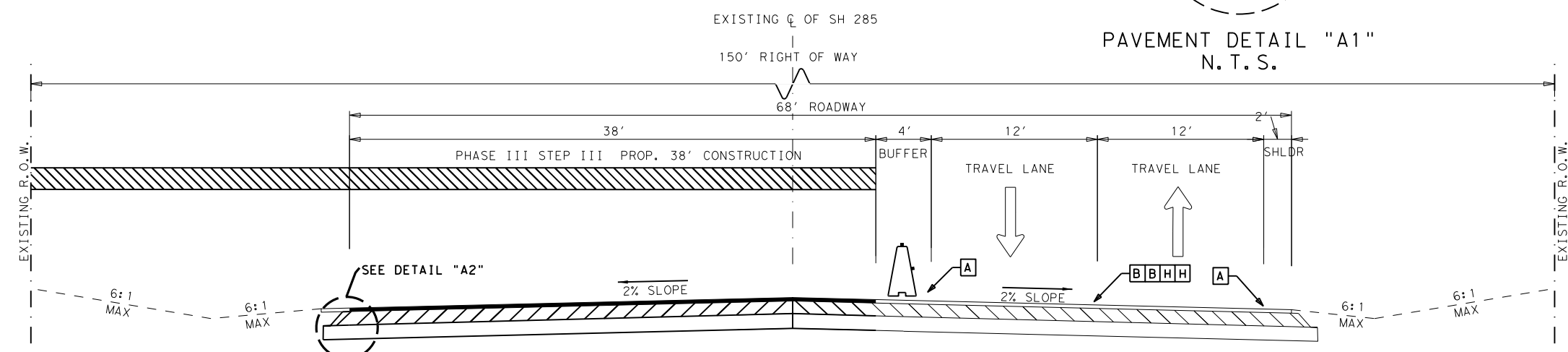
N. T. S. SHEET 1 OF 3

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	95	

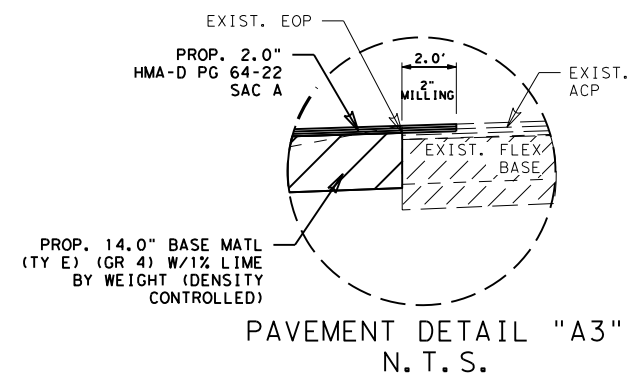
DATE: 3/4/2024 5:00:03 PM
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE III\SH285 TCP PHASE III TYPICALS.dgn



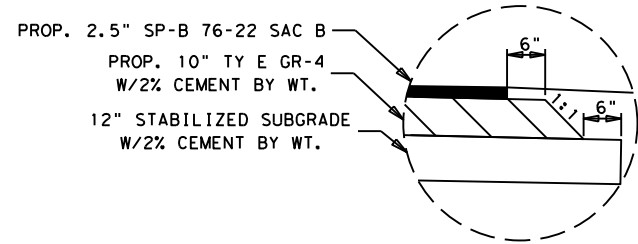
SH 285
 PHASE III STEP II CONSTRUCTION
 STA 272+90 TO STA 312+50



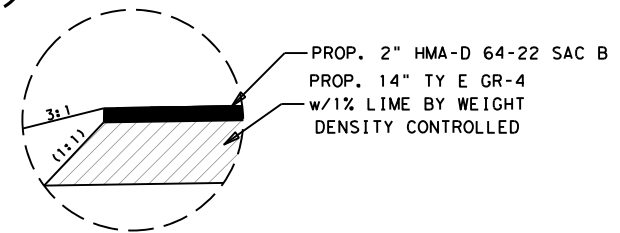
SH 285
 PHASE III STEP III CONSTRUCTION
 STA 215+60 TO STA 222+60 (38' CONSTRUCTION)



PAVEMENT DETAIL "A3"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.



PAVEMENT DETAIL "A1"
 N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - ℄ - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

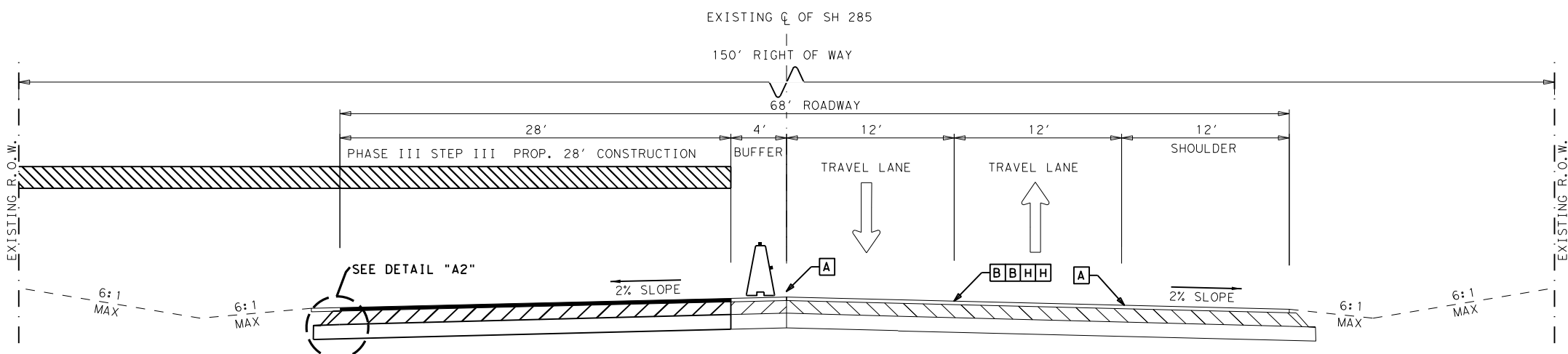
Pharr District Central Design



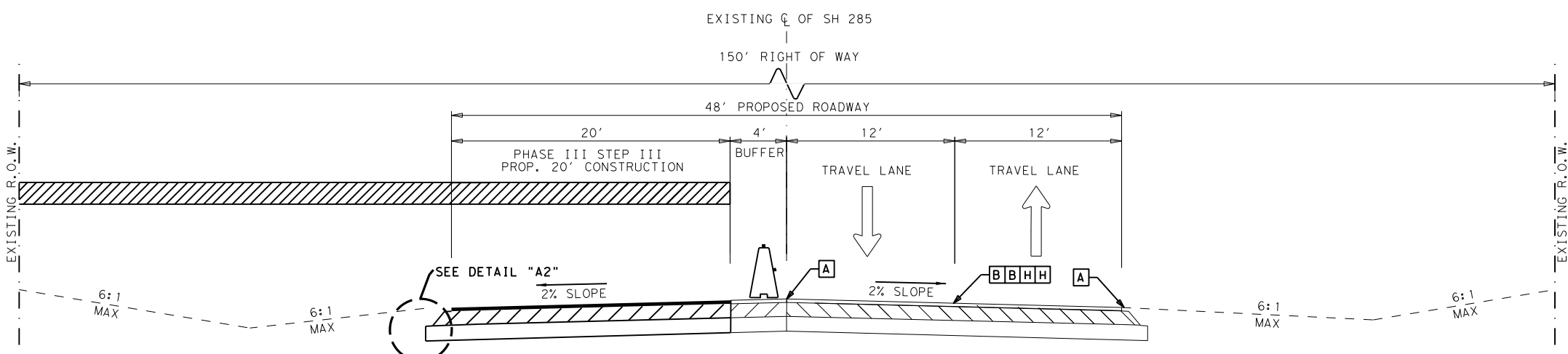
SH 285
 TCP
 PHASE III
 TYPICAL SECTIONS

N. T. S.		SHEET 2 OF 3	
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	96

DATE: 3/4/2024 5:00:04 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE III\SH285 TCP PHASE III TYPICALS.dgn

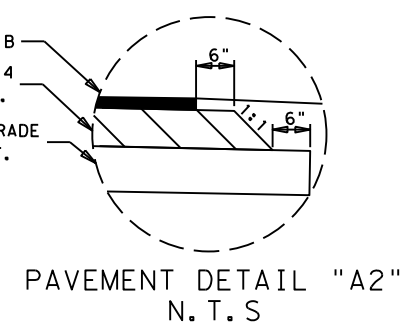


SH 285
 PHASE III STEP III CONSTRUCTION
 STA 222+60 TO STA 263+90 (28' CONSTRUCTION)
 STA 263+90 TO STA 272+90 (28' TO 20' CONSTRUCTION)



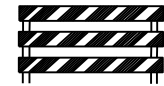














SH 285
 PHASE III STEP III CONSTRUCTION
 STA 272+90 TO STA 305+50

PROP. 2.5" SP-B 76-22 SAC B
 PROP. 10" TY E GR-4
 W/2% CEMENT BY WT.
 12" STABILIZED SUBGRADE
 W/2% CEMENT BY WT.



LEGEND

-  PLASTIC BARREL WITH REFLECTOR
-  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
-  TYPE 3 BARRICADE
-  CENTER LINE
-  PROFILE GRADE LINE
-  PERMISSIBLE CONSTRUCTION JOINT
-  R.O.W. - RIGHT OF WAY
-  SHLDR - SHOULDER
-  A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
-  B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
-  F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
-  G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
-  I - PAV SURF PREP FOR MRK (4")
-  H - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
-  WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



























SH 285
 TCP
 PHASE III
 TYPICAL SECTIONS

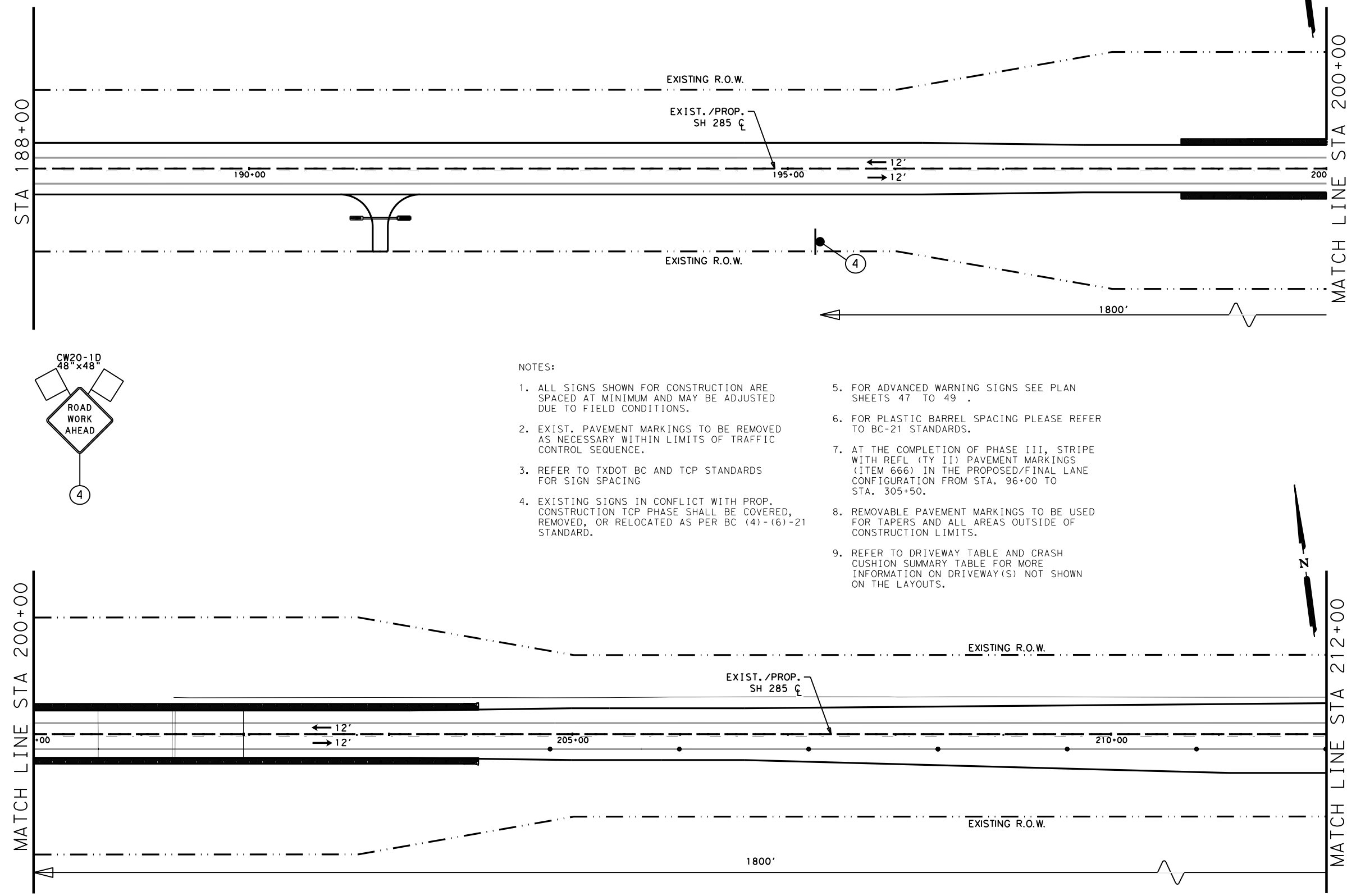
N. T. S.		SHEET 3 OF 3	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO.
			97

DATE: 3/4/2024 5:00:10 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP Phase III\SH 285 TCP Phase III Step I Sheets.dgn

****NOTE TO CONTRACTOR:**
 PHASE III STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 219+76, 259+51, 273+78, 285+13 & 311+00. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
 TCP
 PHASE III STEP I**

SCALE: 1" = 100' SHEET 1 OF 4

























CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
PHR		JIM HOGG	98

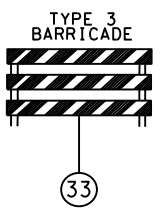
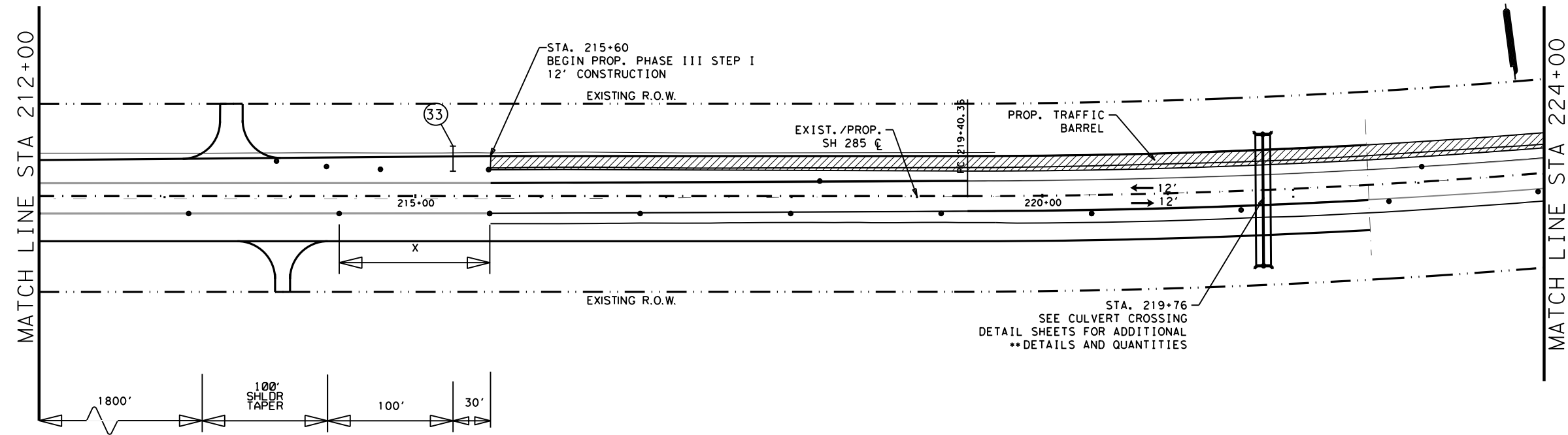
DATE: 3/4/2024 5:00:12 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE III STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 219+76, 259+51, 273+78, 285+13 & 311+00. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

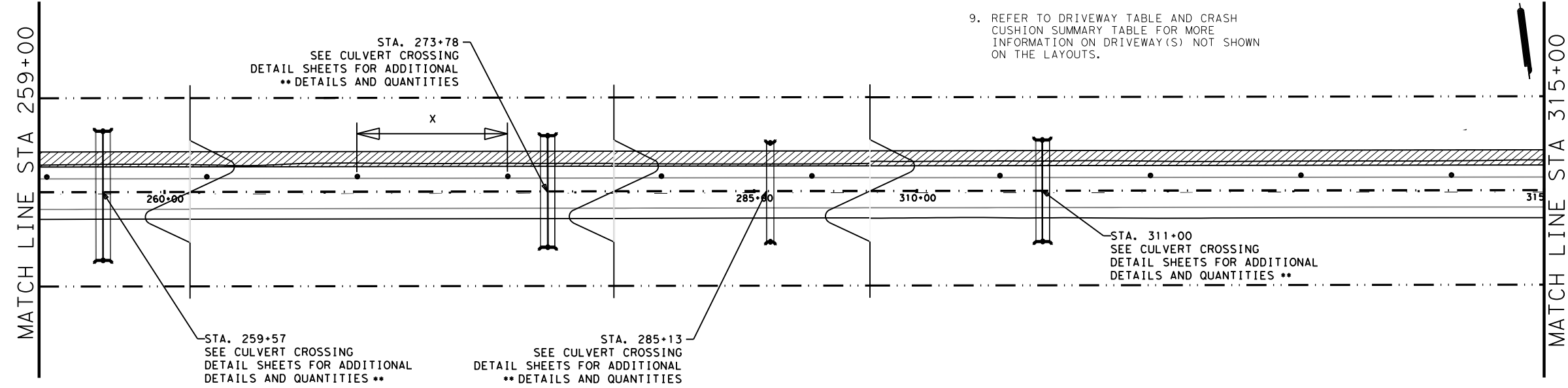
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
PHASE III STEP I**

SCALE: 1" = 100' SHEET 2 OF 4

























© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		99

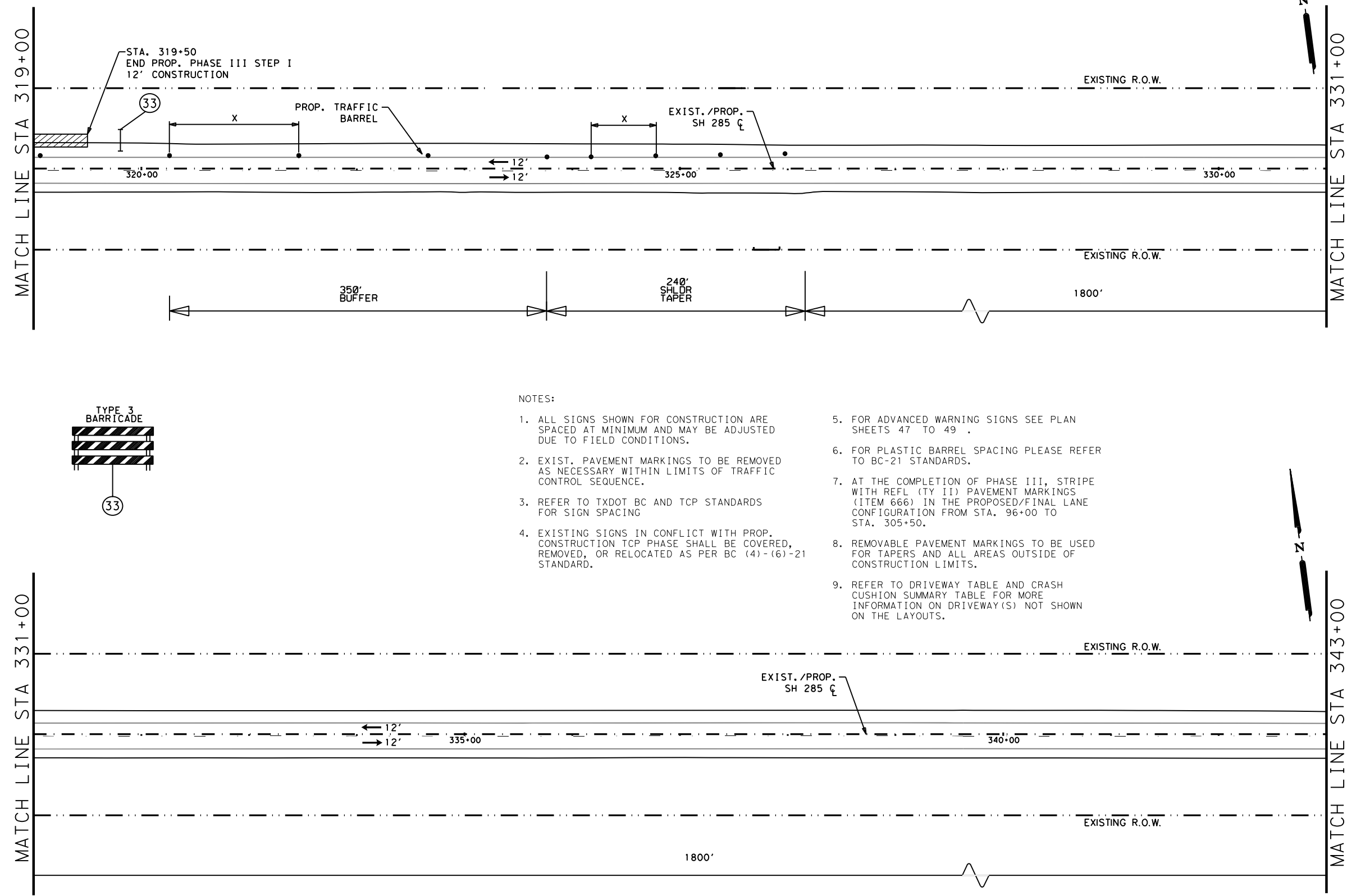
DATE: 3/4/2024 5:00:14 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE III STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 219+76, 259+51, 273+78, 285+13 & 311+00. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

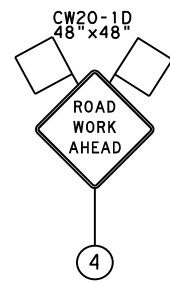
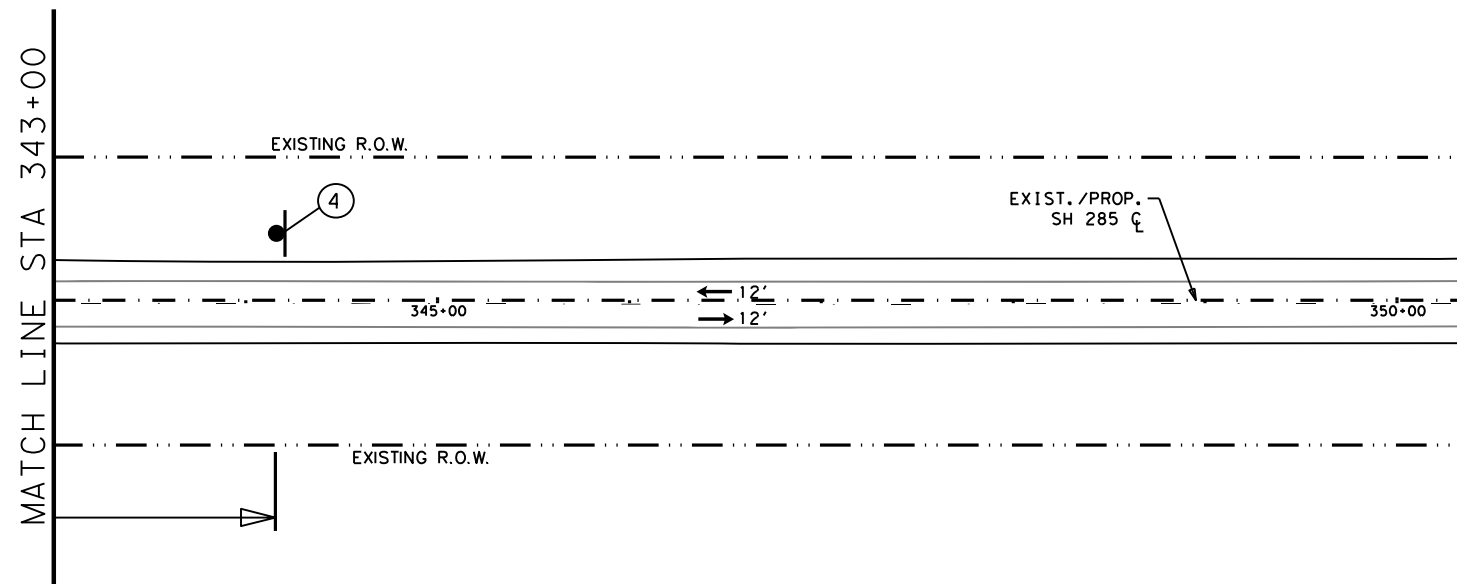


**SH 285
TCP
PHASE III STEP I**

SCALE: 1" = 100' SHEET 3 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	100	

DATE: 3/4/2024 5:00:16 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP I SHEETS.dgn



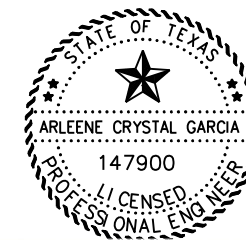
NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK_ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK_ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK_ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK_ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK_ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK_ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK_ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK_ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK_ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK_ZONE PVMT MARK (REM) (W) (ARROW)
- WORK_ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE III STEP I

SCALE: 1" = 100' SHEET 4 OF 4

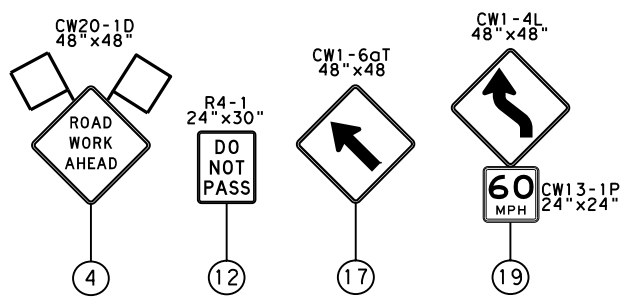
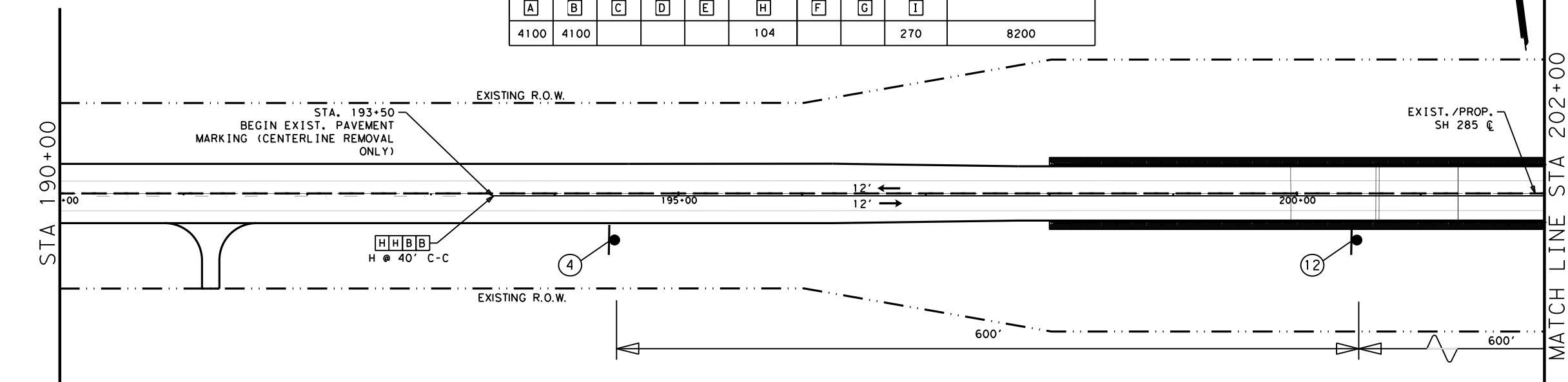
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
		DIST	COUNTY	SHEET NO.
	PHR		JIM HOGG	101

DATE: 3/4/2024 5:00:21 PM
 FILE: D:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
4100	4100				104			270	8200

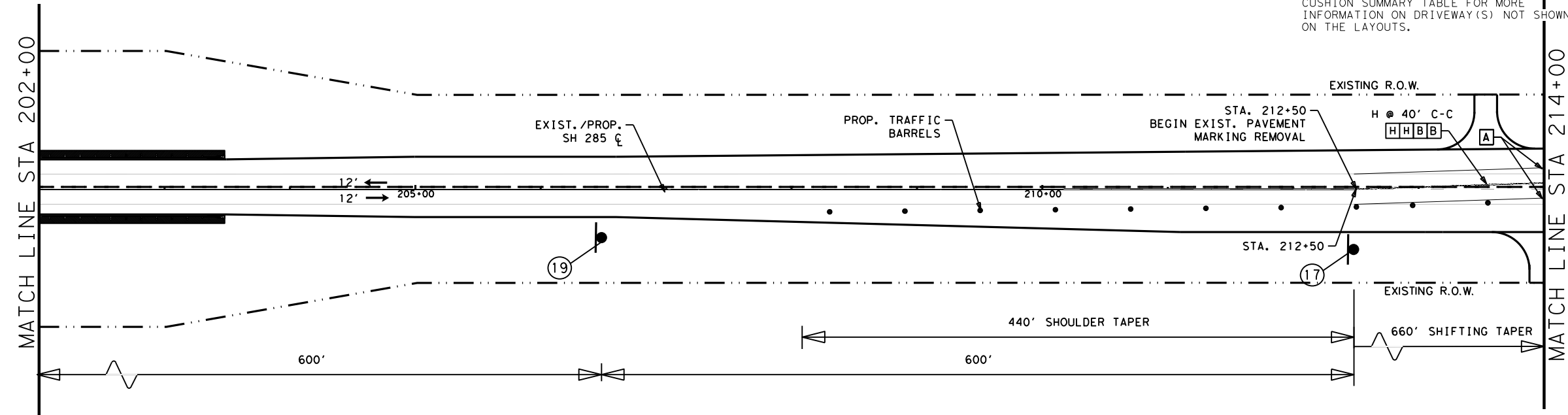
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC STANDARD SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP II**

SCALE: 1" = 100' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	102	

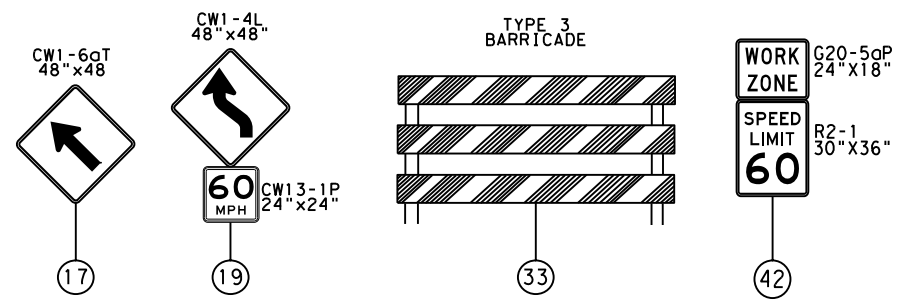
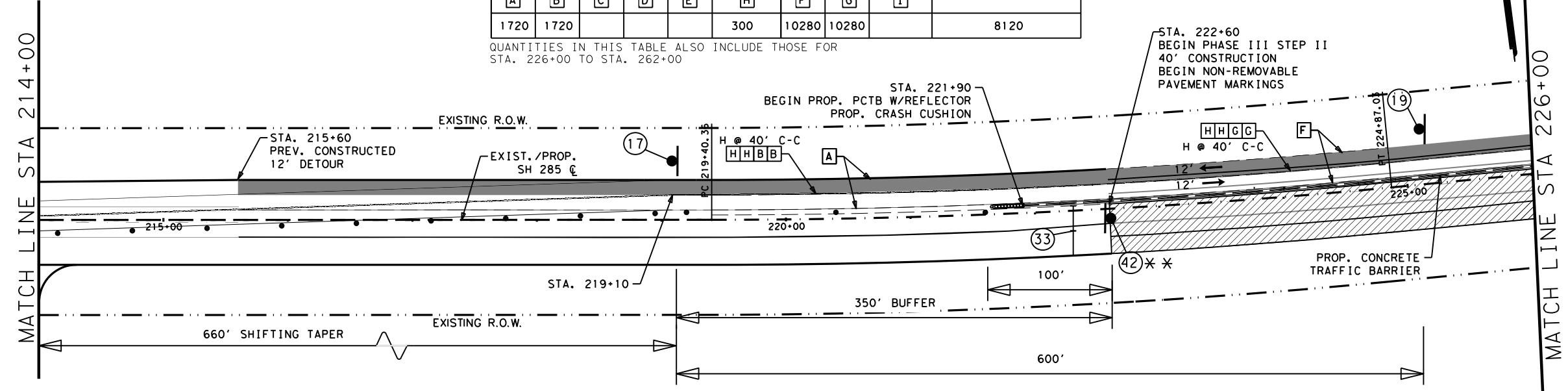
DATE: 3/4/2024 5:00:22 PM
 FILE: P:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1720	1720				300	10280	10280		8120

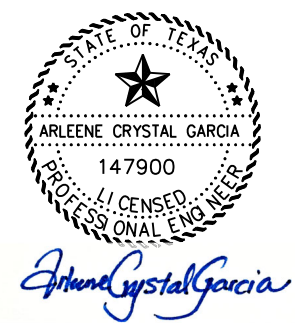
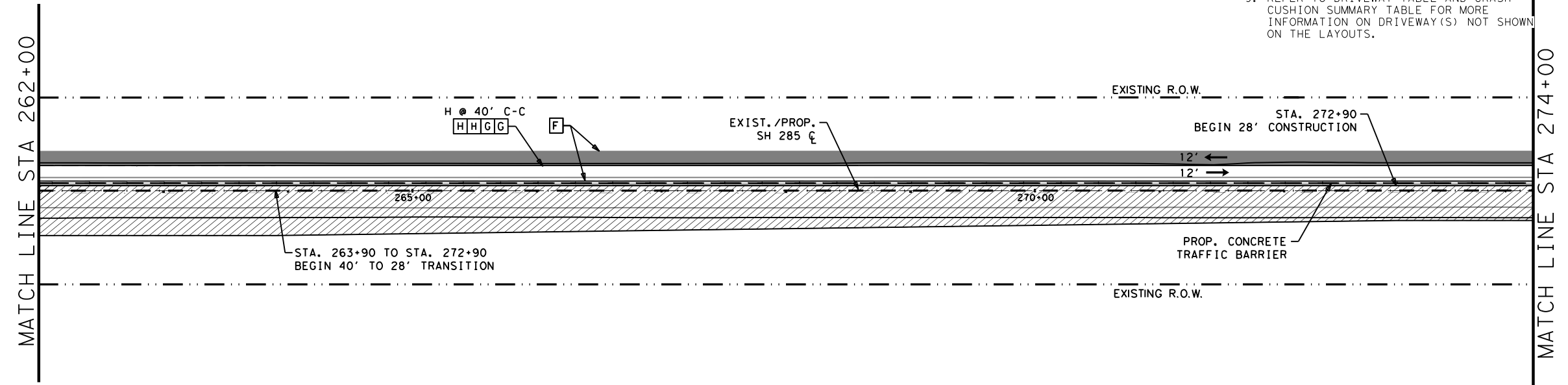
QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 226+00 TO STA. 262+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



- NOTES:
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP II**

SCALE: 1"=100' SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	103	

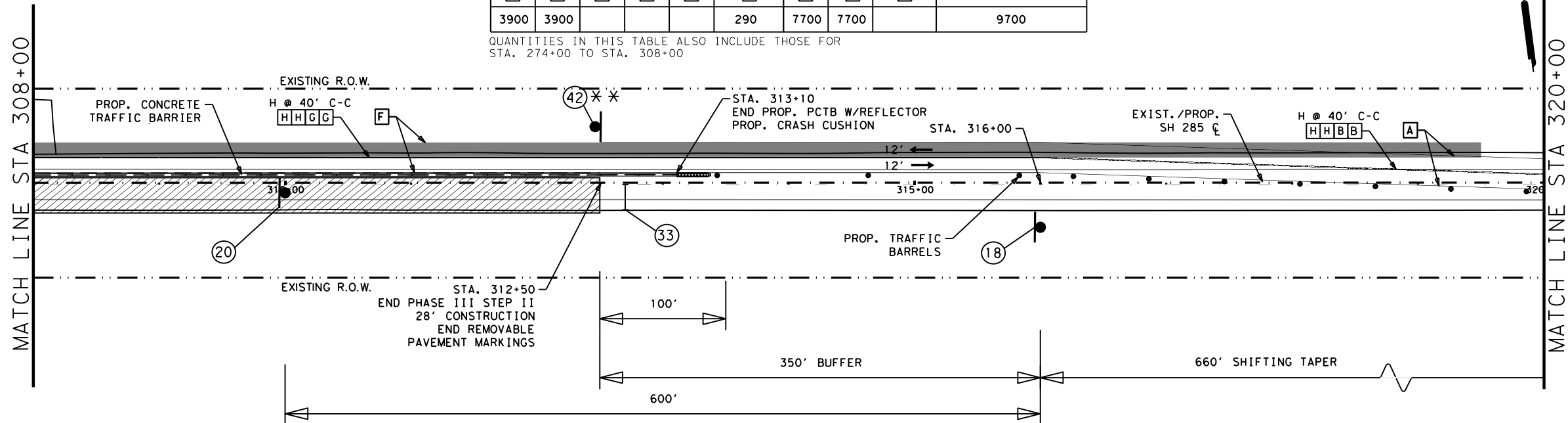
DATE: 3/4/2024 5:00:24 PM
 FILE: D:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)									PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)					(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	662 6002	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"		ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I		
3900	3900				290	7700	7700			9700

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 274+00 TO STA. 308+00

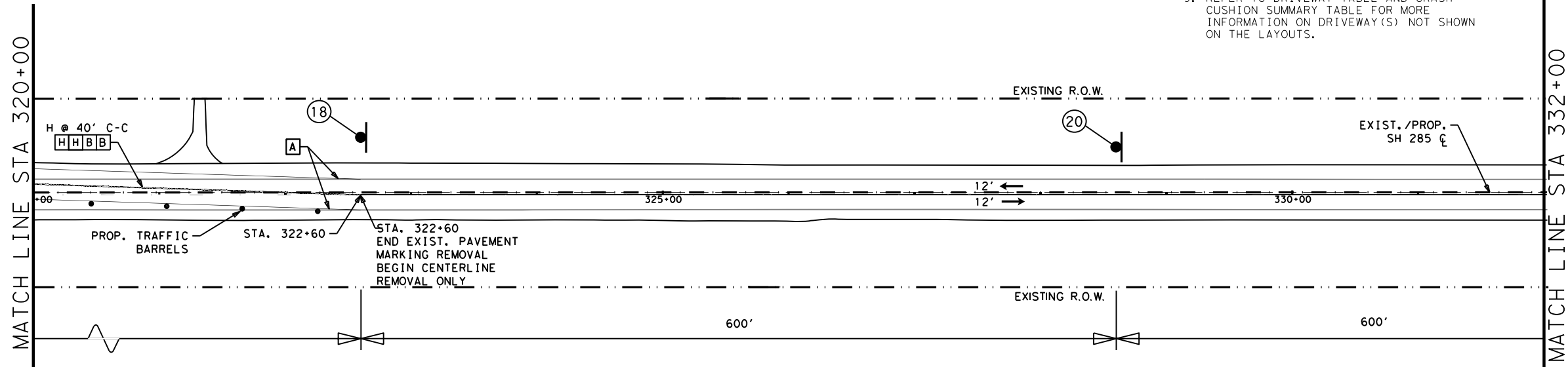
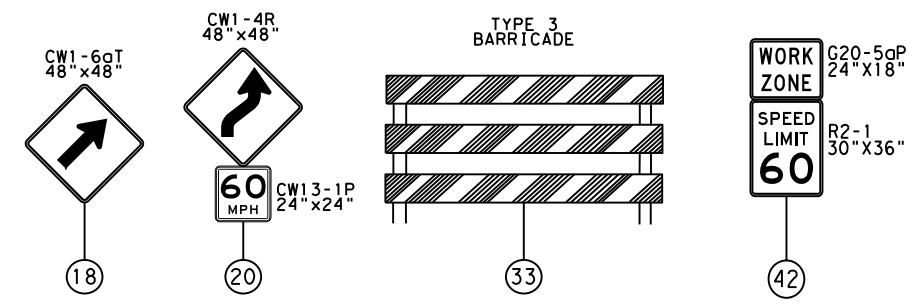
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP II**

SCALE: 1" = 100' SHEET 3 OF 4

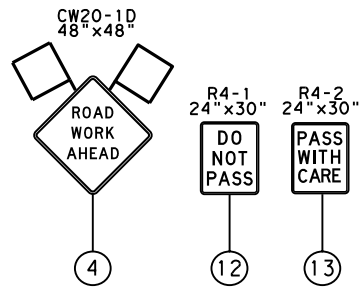
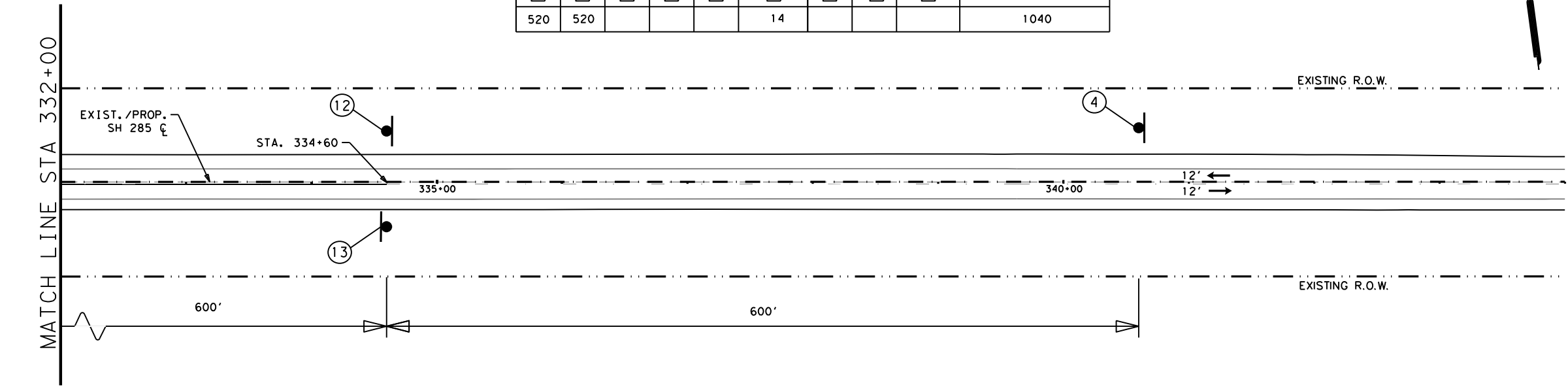
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	104	

DATE: 3/4/2024 5:00:26 PM
 FILE: P:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
520	520				14				1040

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP II**

SCALE: 1" = 100' SHEET 4 OF 4

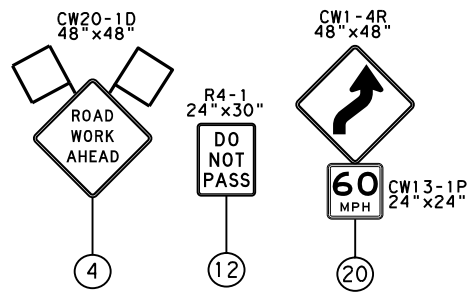
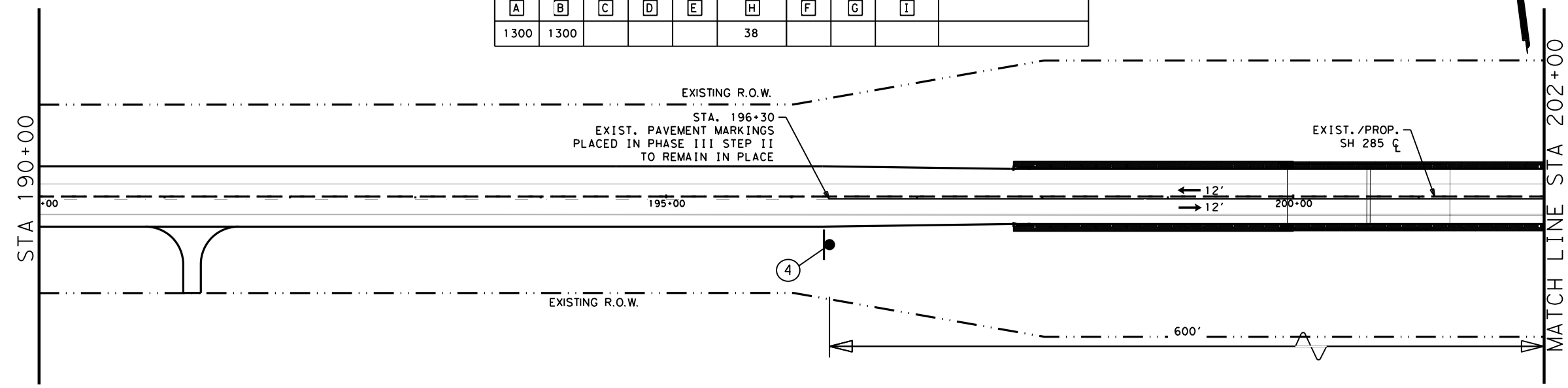
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		105

DATE: 3/4/2024 5:00:31 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase III\SH 285 TCP Phase III Step III Sheets.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1300	1300				38				

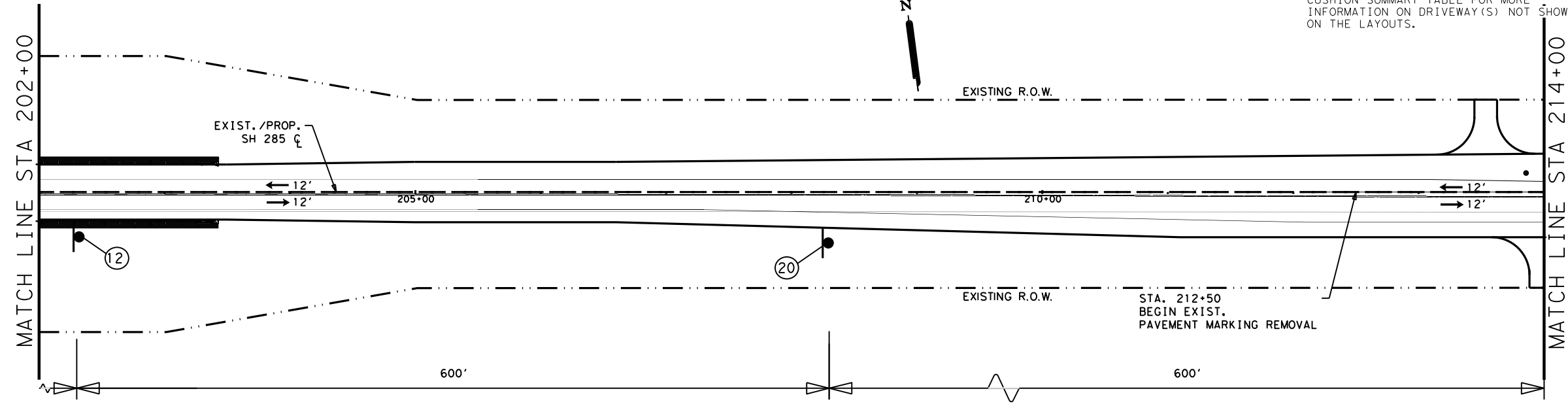
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP III**

SCALE: 1" = 100' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	106	

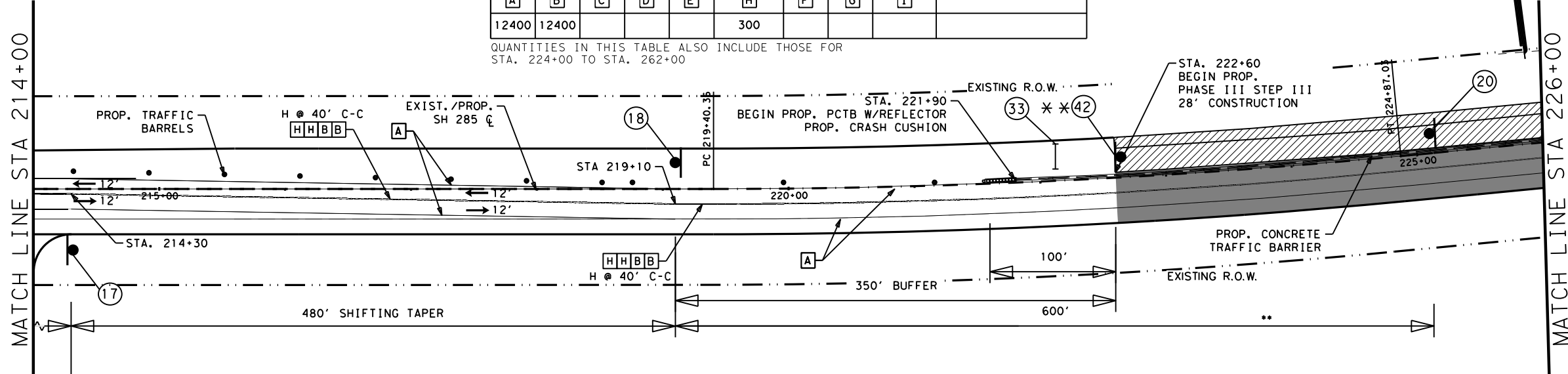
DATE: 3/4/2024 5:00:33 PM
 FILE: \\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP Phase III\SH 285 TCP Phase III Step III Sheets.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)									PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)					(NON-REMOV)					
662	662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"		ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I		
12400	12400				300					

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 224+00 TO STA. 262+00

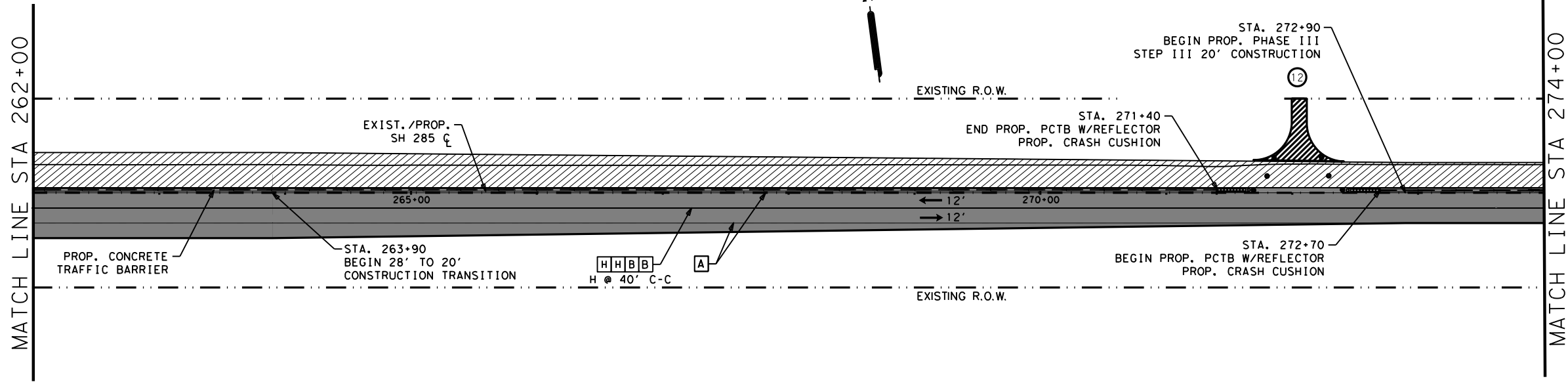
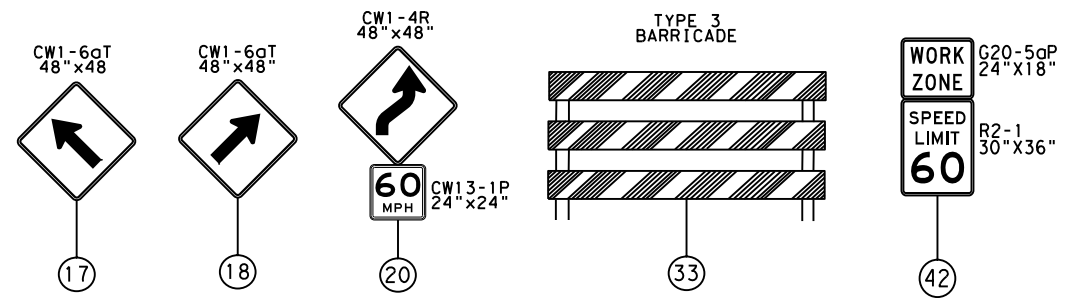
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP III**

SCALE: 1" = 100' SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		107

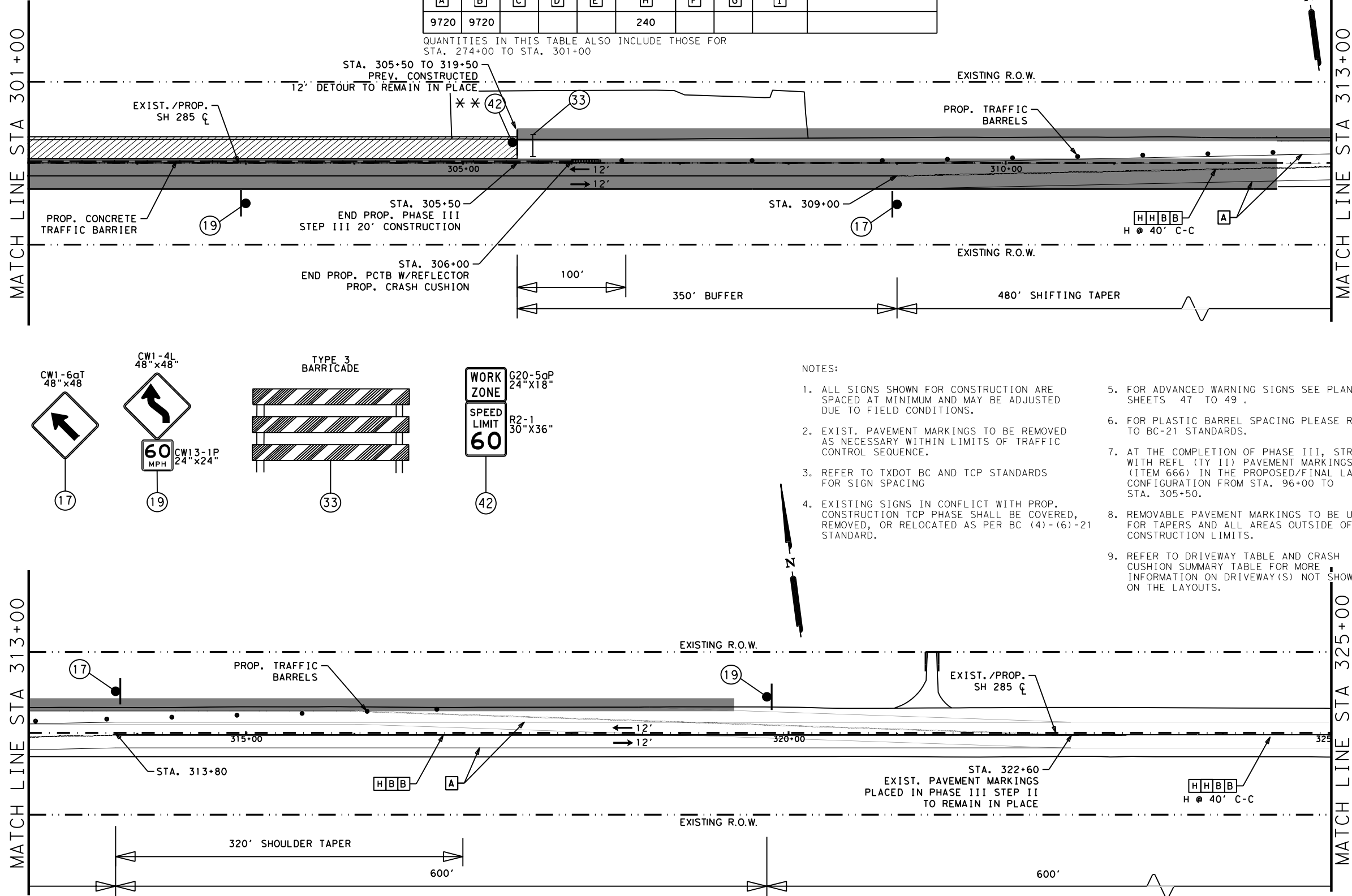
DATE: 3/4/2024 5:00:34 PM
 FILE: \\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE III\SH 285 TCP PHASE III STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
9720	9720				240				

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 274+00 TO STA. 301+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE III STEP III**

SCALE: 1" = 100' SHEET 3 OF 4

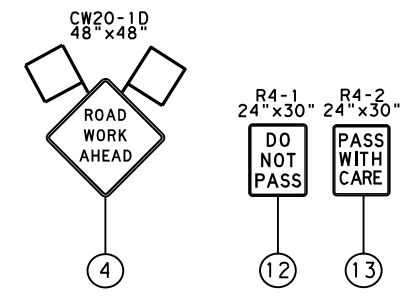
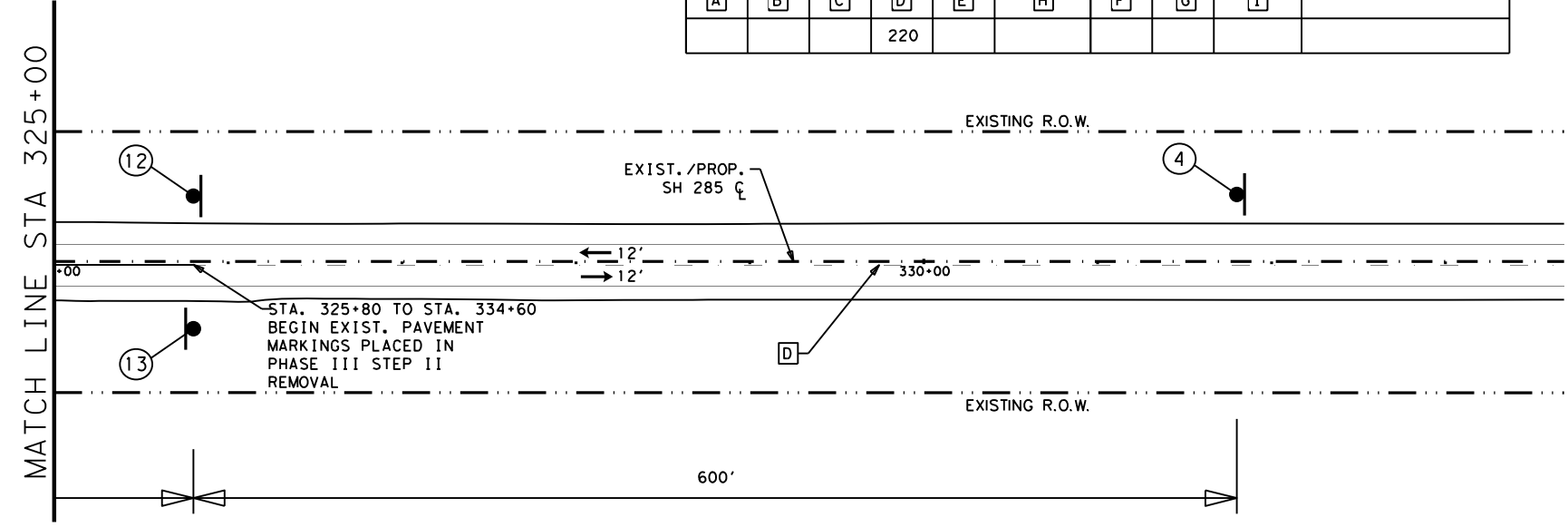
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	108	

DATE: 3/4/2024 5:00:36 PM
 FILE: \\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase III\SH 285 TCP Phase III Step III Sheets.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
			220						

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE III, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 96+00 TO STA. 305+50.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

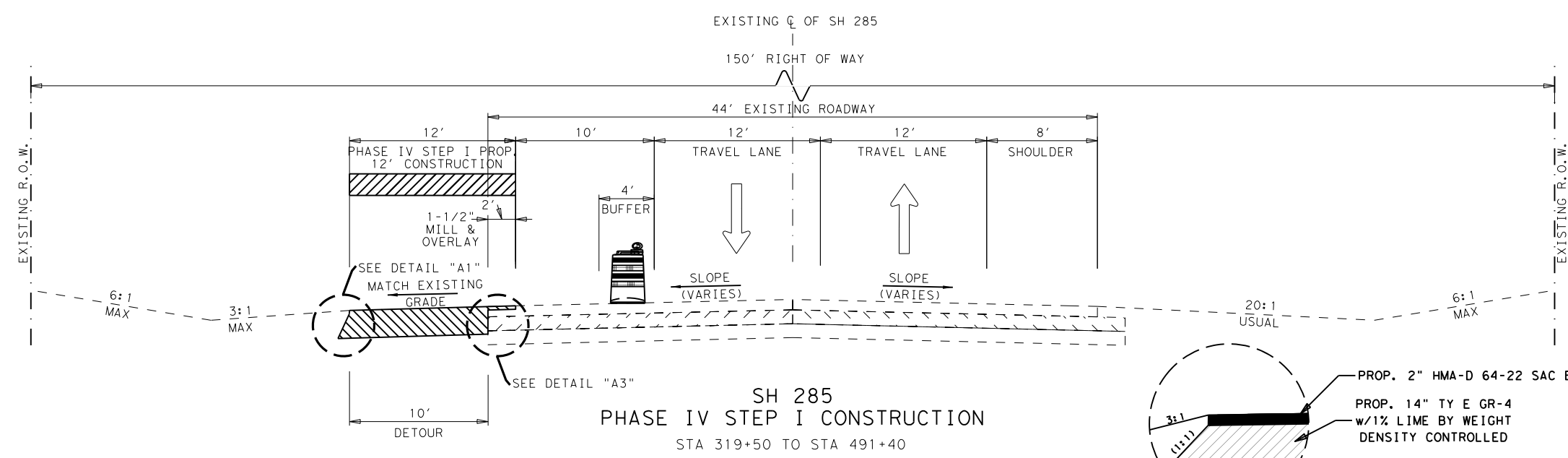
Texas Department of Transportation

**SH 285
TCP
PHASE III STEP III**

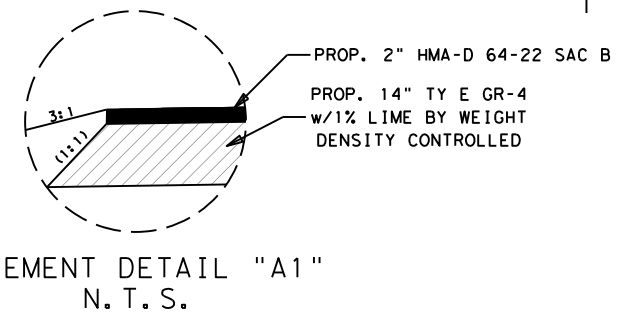
SCALE: 1" = 100' SHEET 4 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		109

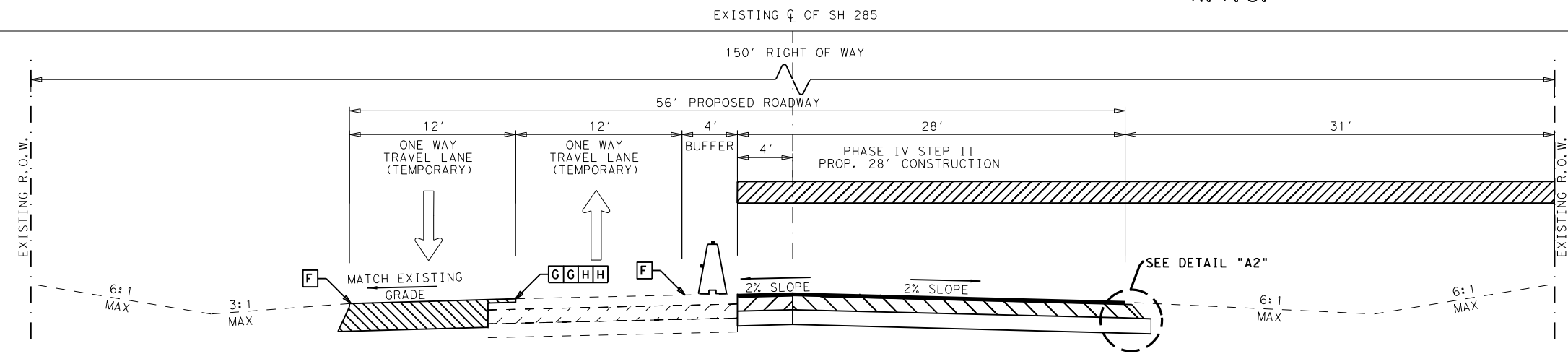
DATE: 3/4/2024 5:00:46 PM
 FILE: \\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE IV\SH285 TCP PHASE IV TYPICALS NEW.dgn



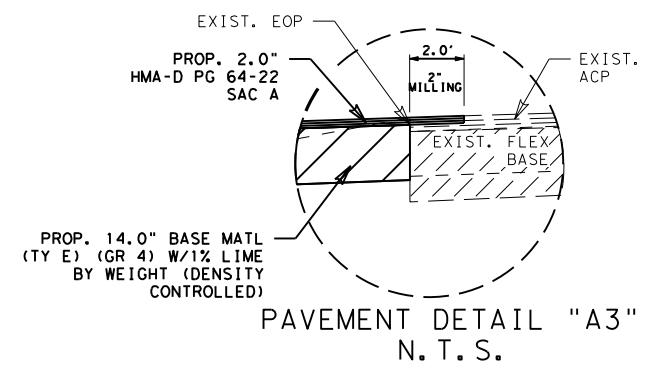
SH 285
 PHASE IV STEP I CONSTRUCTION
 STA 319+50 TO STA 491+40



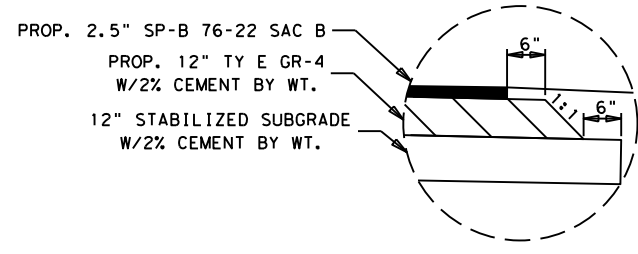
PAVEMENT DETAIL "A1"
 N. T. S.



SH 285
 PHASE IV STEP II CONSTRUCTION
 STA 312+50 TO STA 352+10
 STA 352+10 TO STA 361+10 (28' TO 40' TRANSITION)
 STA 444+80 TO STA 484+40



PAVEMENT DETAIL "A3"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - CENTER LINE
 - PROFILE GRADE LINE
 - PERMISSIBLE CONSTRUCTION JOINT
 - R. O. W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

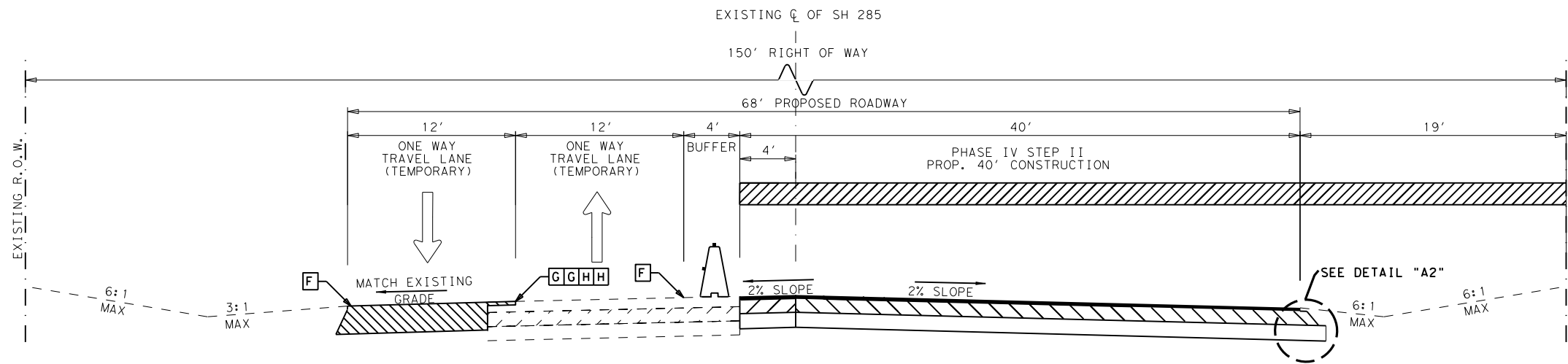
Pharr District Central Design



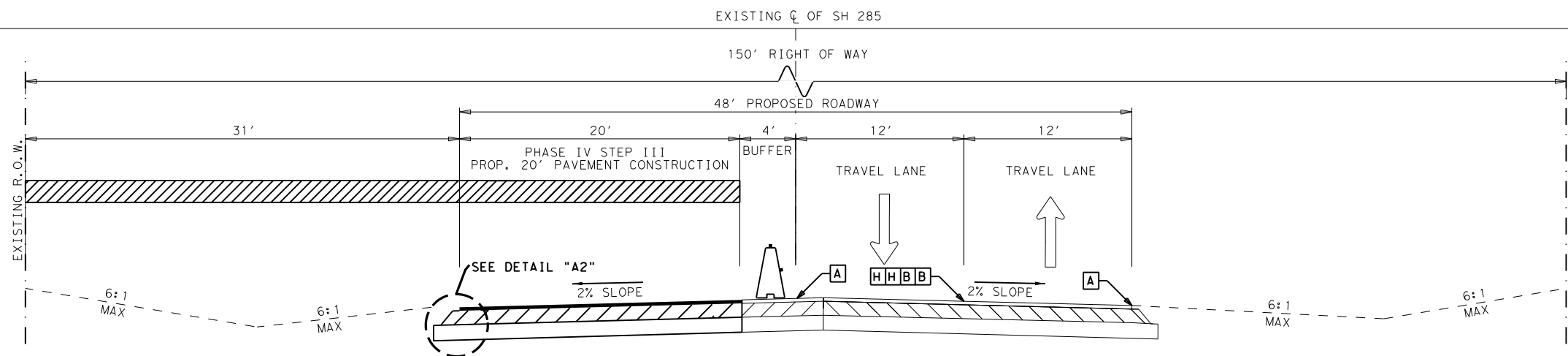
SH 285
 TCP
 PHASE IV
 TYPICAL SECTIONS

N. T. S.		SHEET 1 OF 3	
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	110

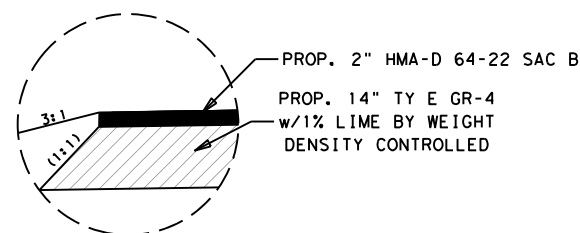
DATE: 3/4/2024 5:00:47 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE IV TYPICALS NEW.dgn



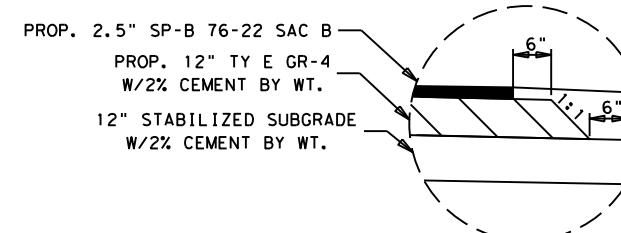
SH 285
 PHASE IV STEP II CONSTRUCTION
 STA 361+10 TO STA 435+80
 STA 435+80 TO STA 444+80 (40' TO 28' TRANSITION)



SH 285
 PHASE IV STEP III CONSTRUCTION
 STA 305+50 TO STA 352+10
 STA 352+10 TO STA 361+10 (20' TO 28' TRANSITION)
 STA 444+80 TO STA 477+40



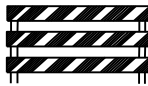









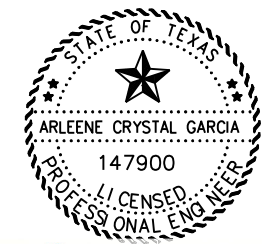
PAVEMENT DETAIL "A1"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.

LEGEND

-  PLASTIC BARREL WITH REFLECTOR
-  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
-  TYPE 3 BARRICADE
- ℄ - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R. O. W. - RIGHT OF WAY
- SHLDR - SHOULDER
-  WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
-  WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
-  PAV SURF PREP FOR MRK (4")
-  WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
-  WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

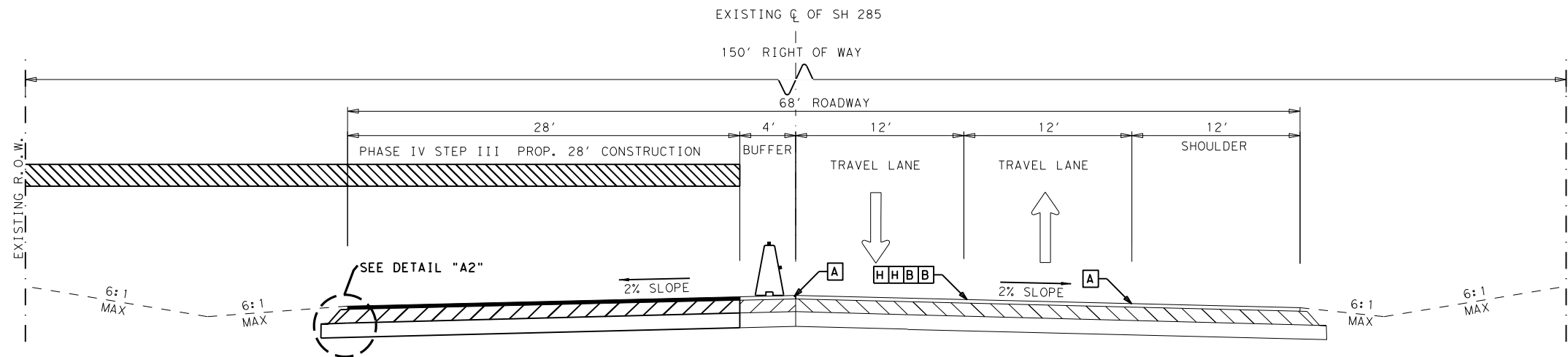
Pharr District Central Design



SH 285
 TCP
 PHASE IV
 TYPICAL SECTIONS

N. T. S.		SHEET 2 OF 3	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	111

DATE: 3/4/2024 5:00:48 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE IV TYPICALS NEW.dgn



SH 285
 PHASE IV STEP III CONSTRUCTION
 STA 361+10 TO STA 435+80
 STA 435+80 TO STA 444+80 (28' TO 20' TRANSITION)

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - CENTER LINE
 - PROFILE GRADE LINE
 - PERMISSIBLE CONSTRUCTION JOINT
 - RIGHT OF WAY
 - SHOULDER
 - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - PAV SURF PREP FOR MRK (4")
 - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arleene Crystal Garcia

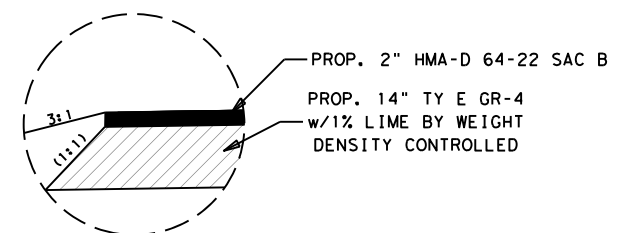
03.07.2024

Pharr District Central Design

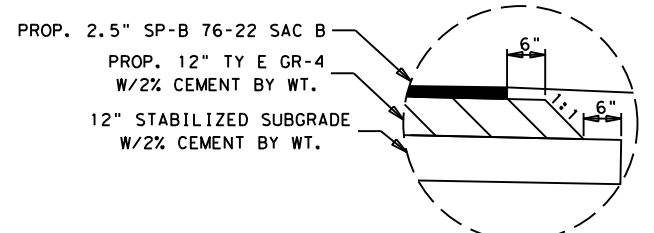


SH 285
 TCP
 PHASE IV
 TYPICAL SECTIONS

N. T. S.		SHEET 3 OF 3	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	112



PAVEMENT DETAIL "A1"
 N. T. S.



























PAVEMENT DETAIL "A2"
 N. T. S.

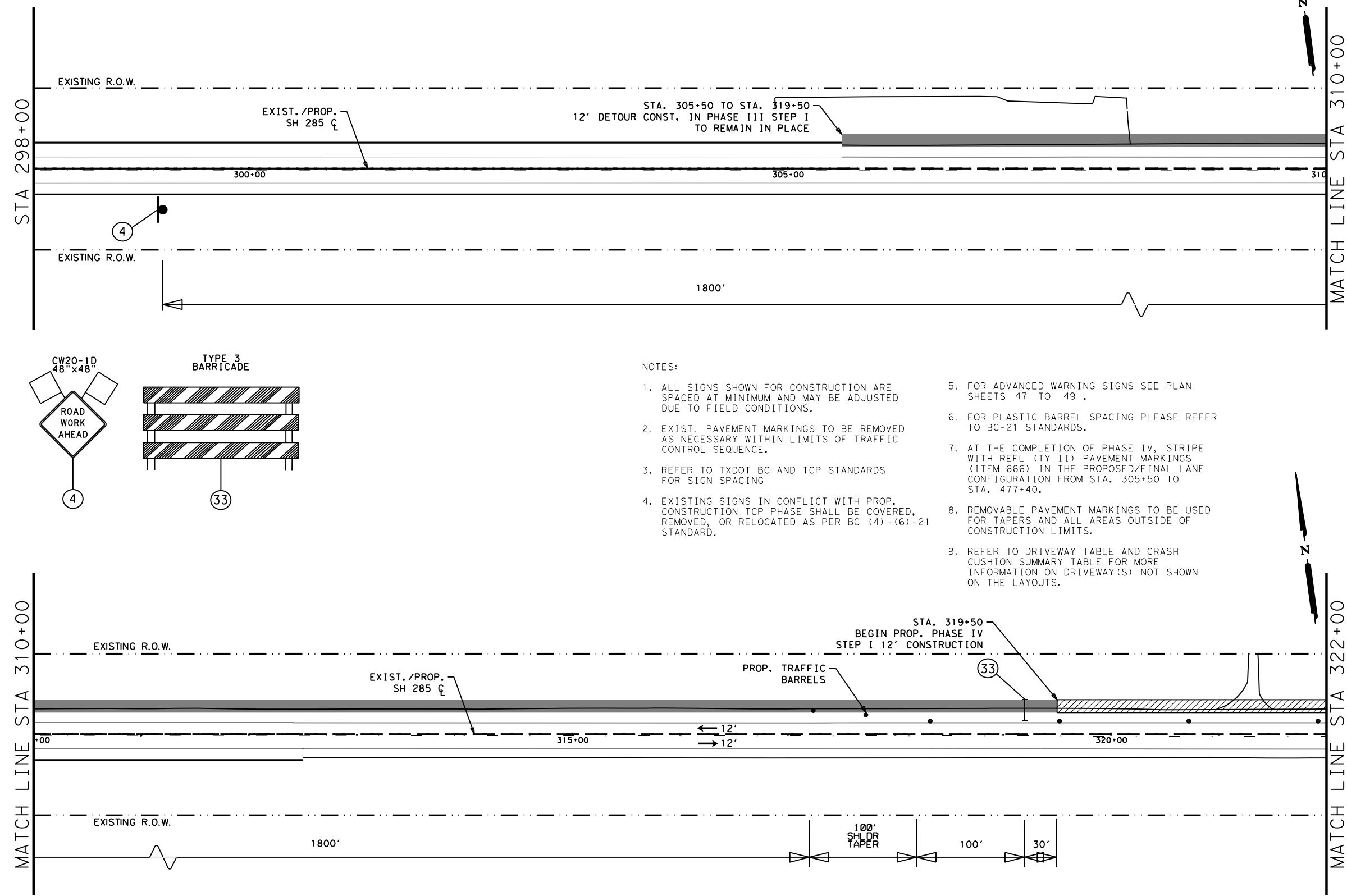
DATE: 3/4/2024 5:00:54 PM
 FILE: \\phr\design\projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step 1 SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE IV STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 360+24, 392+01, 415+36, 442+00, 456+74 AND 473+39. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

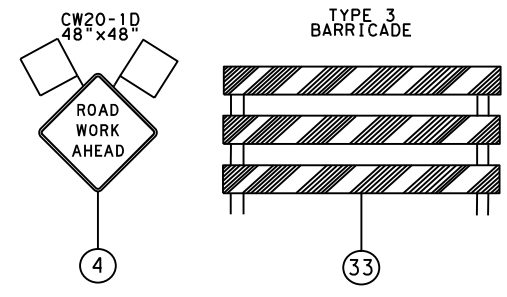
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK_ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK_ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK_ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK_ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK_ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK_ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK_ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK_ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK_ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK_ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK_ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:


1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



Texas Department of Transportation

**SH 285
TCP
PHASE IV STEP I**

SCALE: 1" = 100' SHEET 1 OF 3

























CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	113

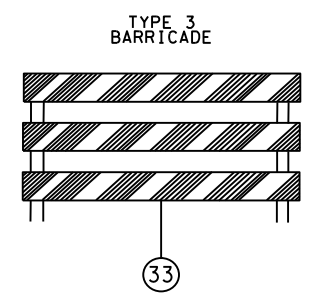
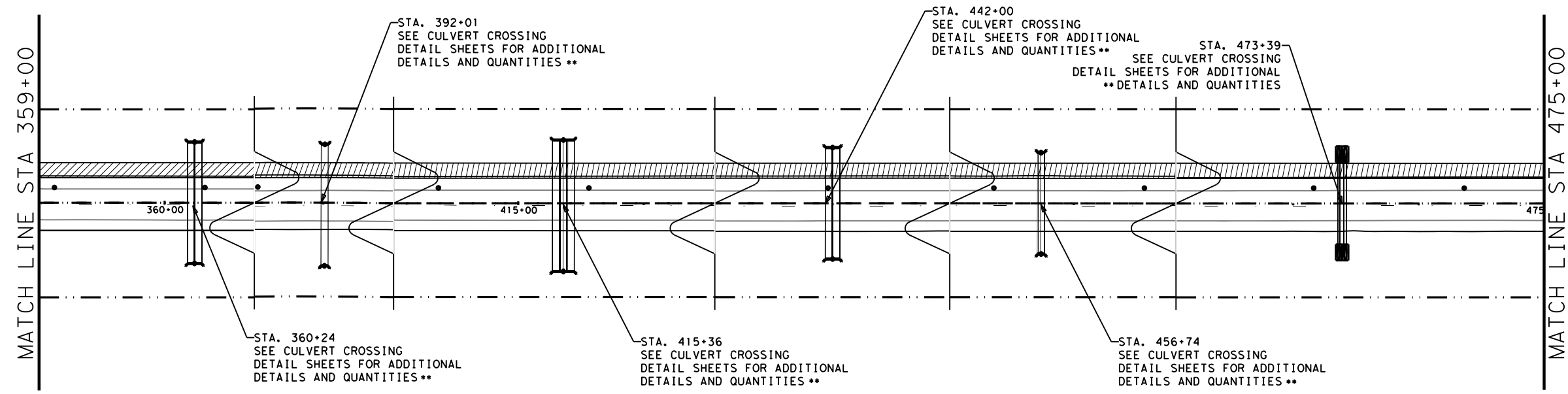
DATE: 3/4/2024 5:00:56 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE IV STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE IV STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 360+24, 392+01, 415+36, 442+00, 456+74 AND 473+39. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

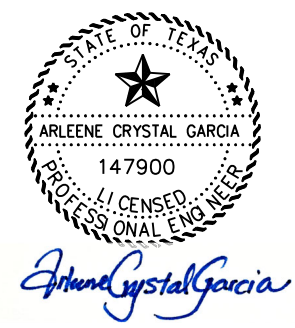
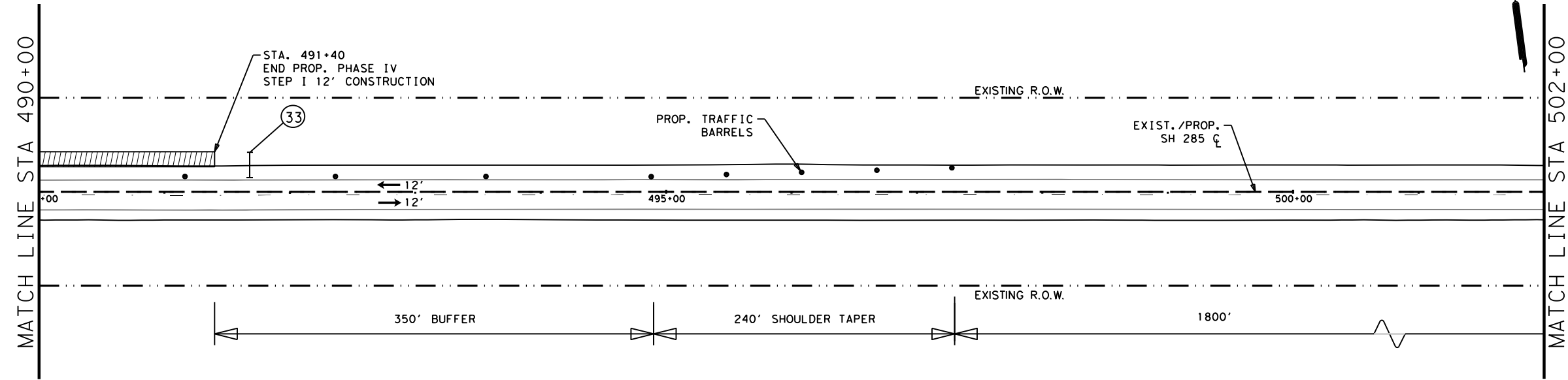
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
8. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design



**SH 285
 TCP
 PHASE IV STEP I**

SCALE: 1" = 100' SHEET 2 OF 3

























© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		114

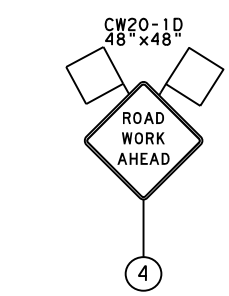
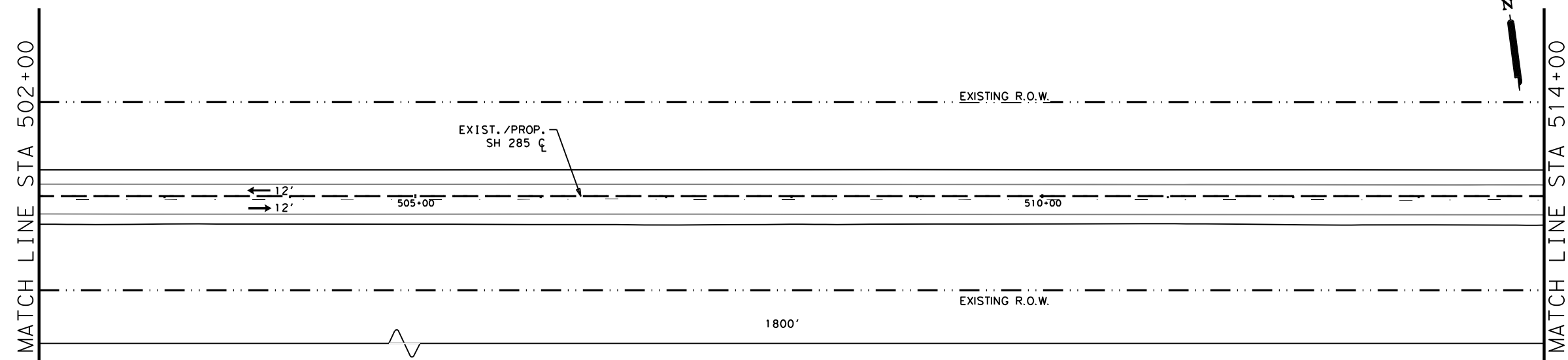
DATE: 3/4/2024 5:00:57 PM
 FILE: \\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE IV STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE IV STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 360+24, 392+01, 415+36, 442+00, 456+74 AND 473+39. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

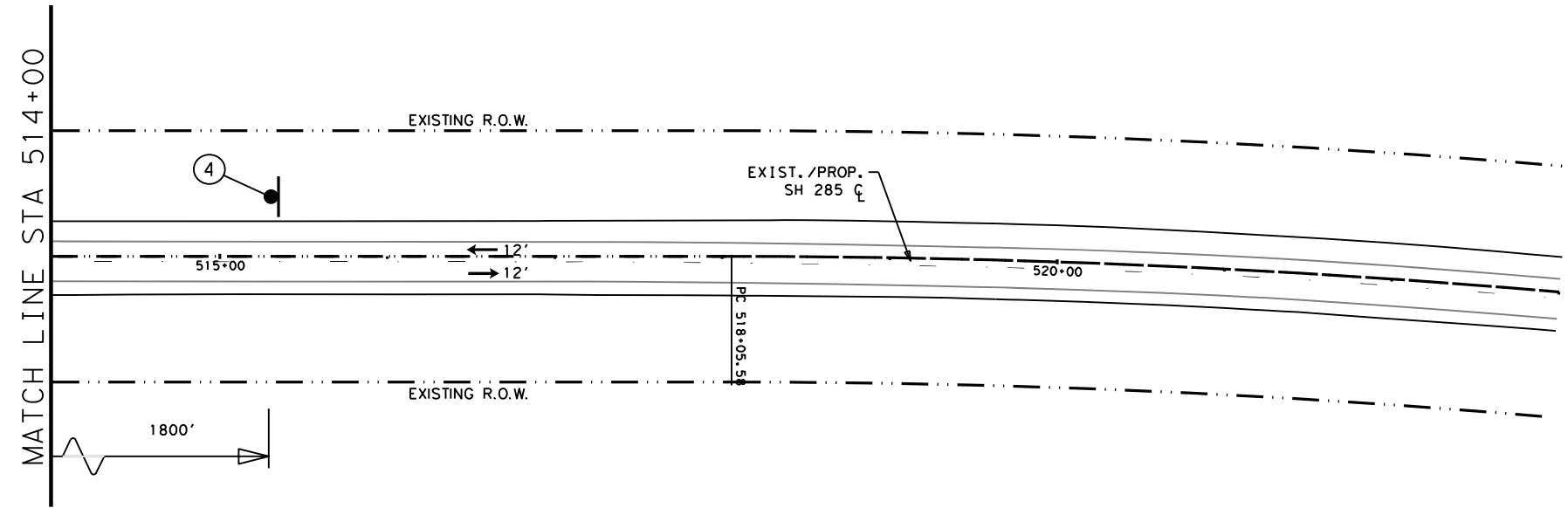
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
8. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
 TCP
 PHASE IV STEP I**

SCALE: 1" = 100' SHEET 3 OF 3

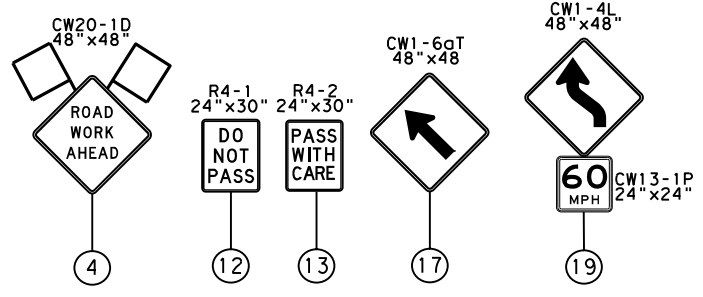
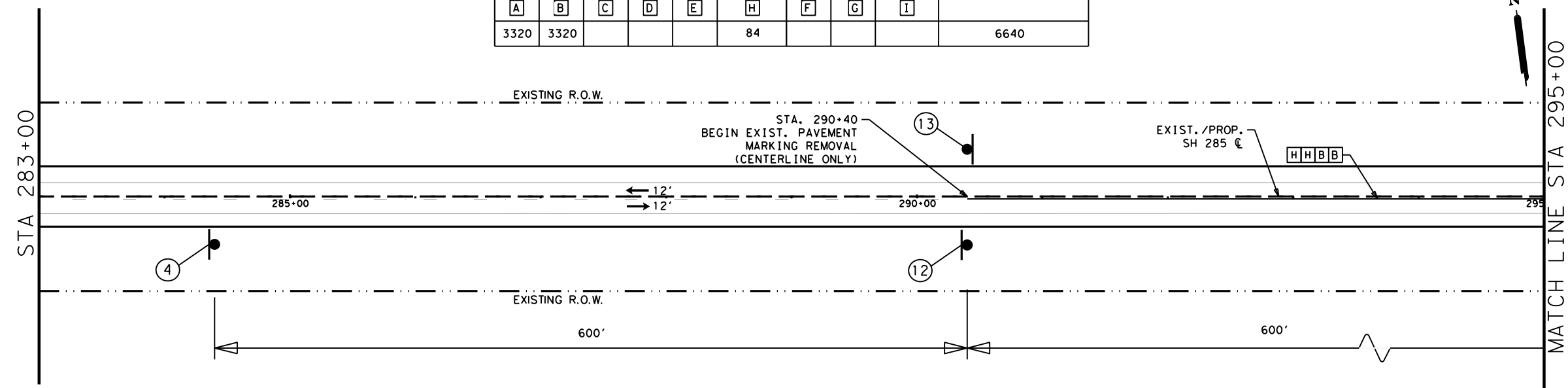
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	115	

DATE: 3/4/2024 5:01:02 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
3320	3320				84				6640

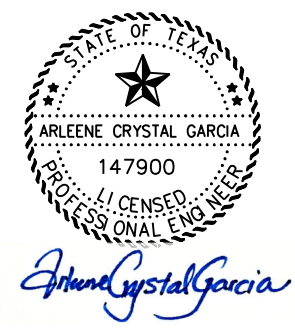
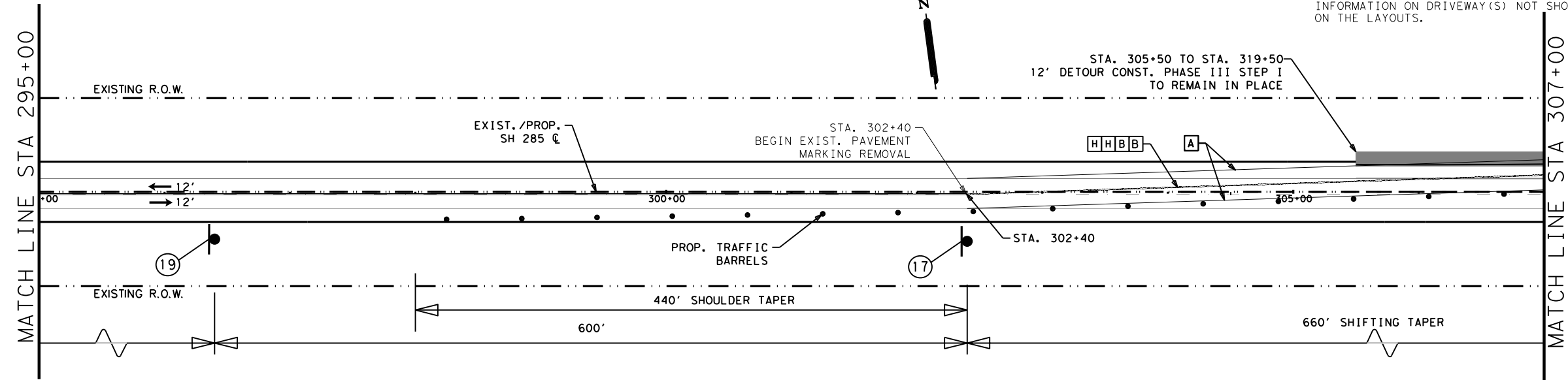
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TCP PHASE IV STEP II

SCALE: 1" = 100' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	116	

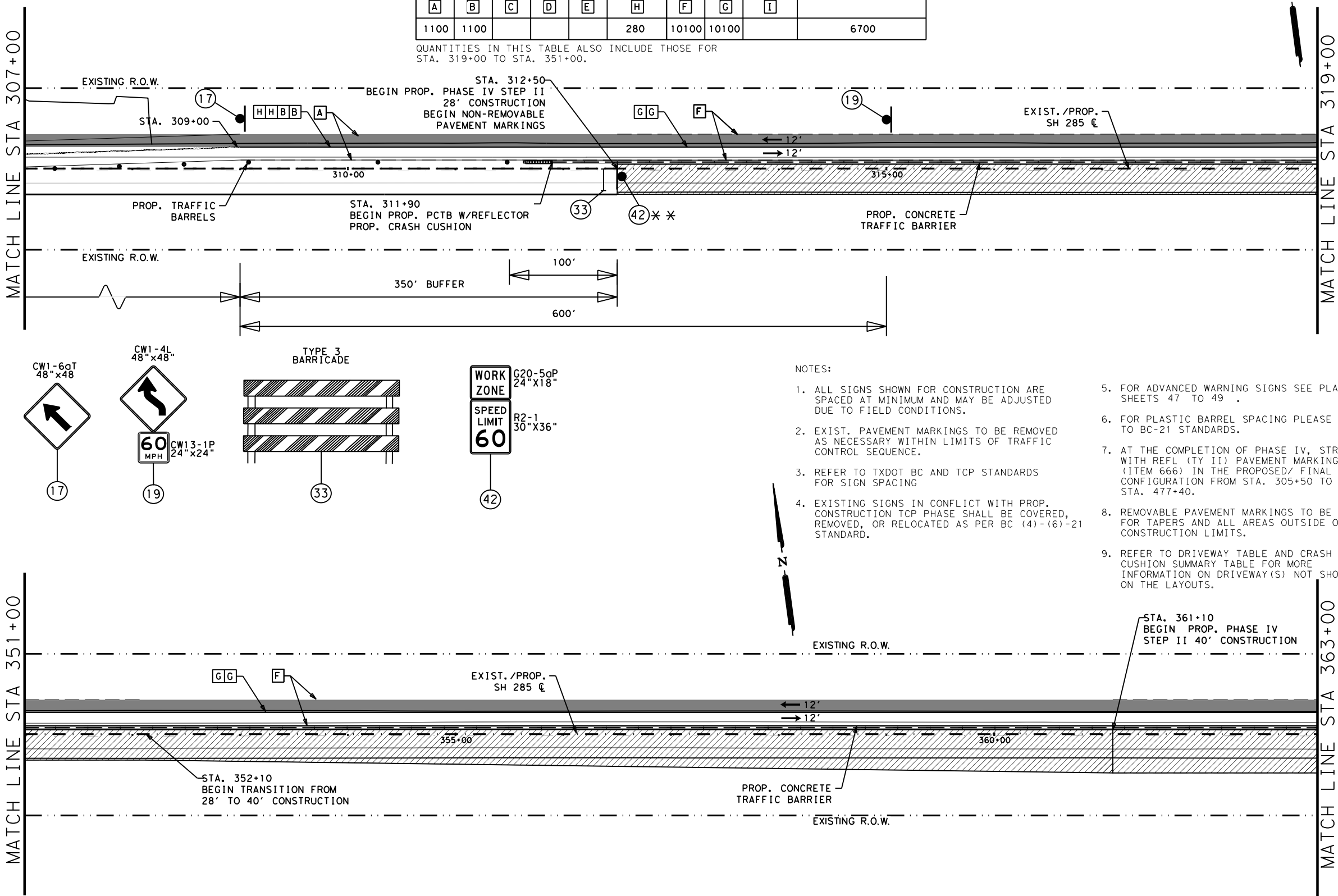
DATE: 3/4/2024 5:01:03 PM
 FILE: P:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1100	1100				280	10100	10100		6700

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 319+00 TO STA. 351+00.

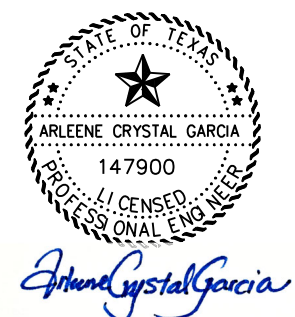
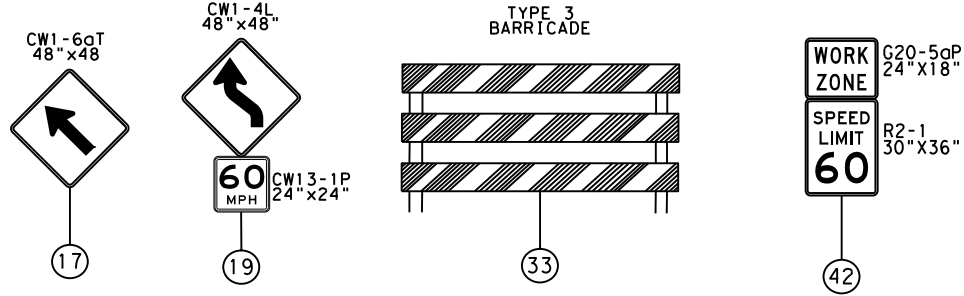
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PAVT MARK (REM) 6" WHITE SOLID
- WORK ZONE PAVT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PAVT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PAVT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PAVT MARK (REM) 24" WHITE SOLID
- WORK ZONE PAVT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PAVT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PAVT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PAVT MARK (REM) (W) (8") (SLD)
- WORK ZONE PAVT MARK (REM) (W) (ARROW)
- WORK ZONE PAVT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE IV STEP II**

SCALE: 1" = 100' SHEET 2 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	117	

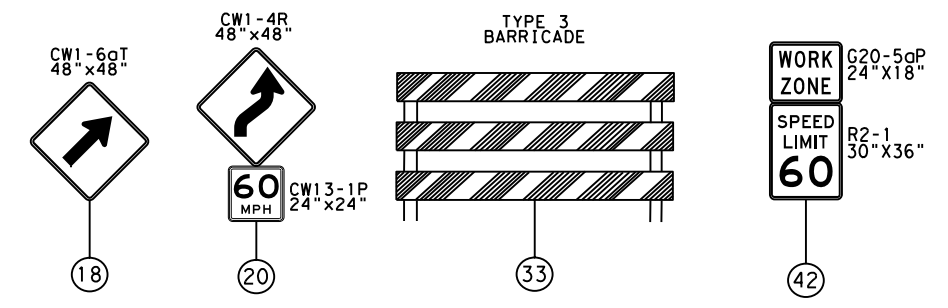
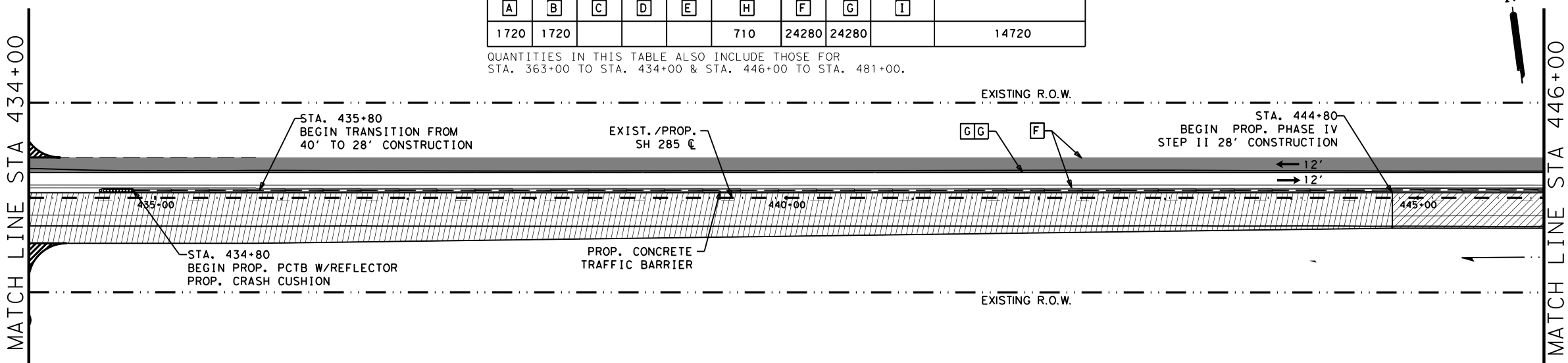
DATE: 3/4/2024 5:01:05 PM
 FILE: P:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step II Sheets.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1720	1720				710	24280	24280		14720

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 363+00 TO STA. 434+00 & STA. 446+00 TO STA. 481+00.

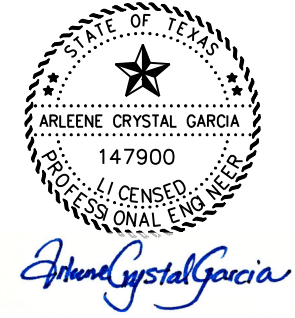
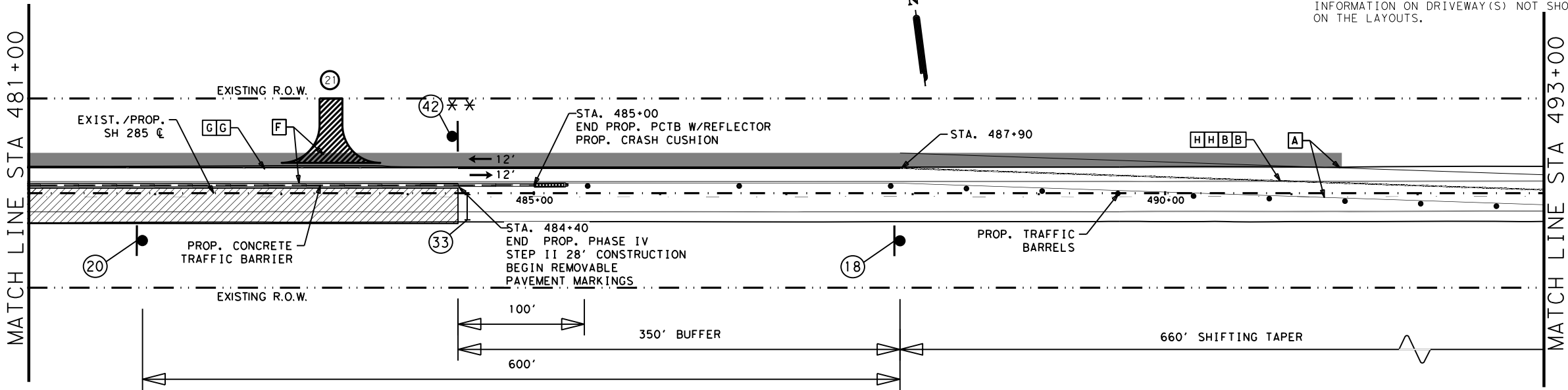
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE IV STEP II**

SCALE: 1" = 100' SHEET 3 OF 4

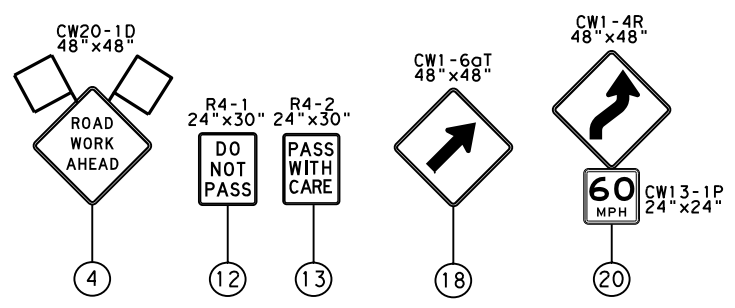
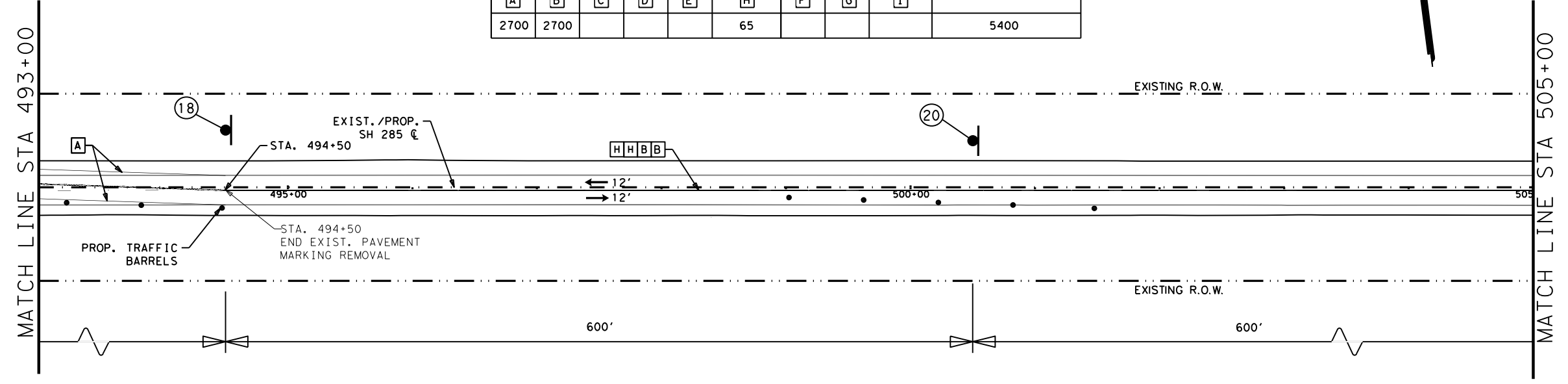
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	118	

DATE: 3/4/2024 5:01:07 PM
 FILE: pw:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE IV STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
2700	2700				65				5400

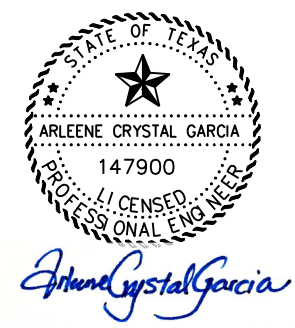
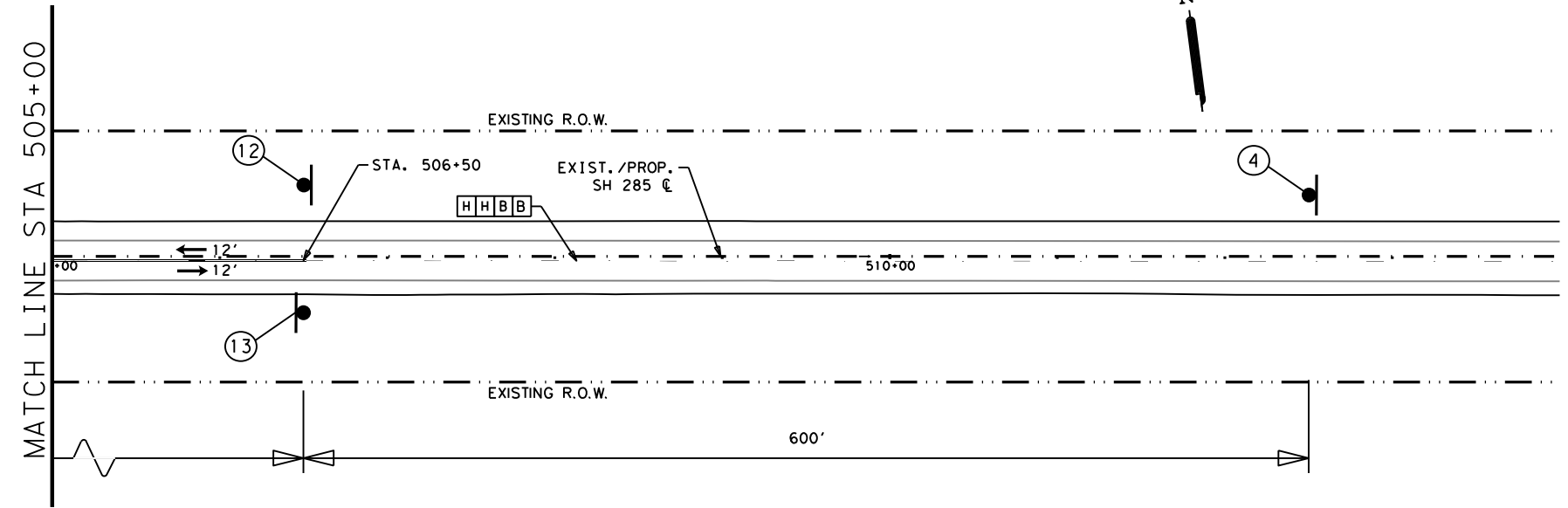
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE IV STEP II**

SCALE: 1" = 100' SHEET 4 OF 4

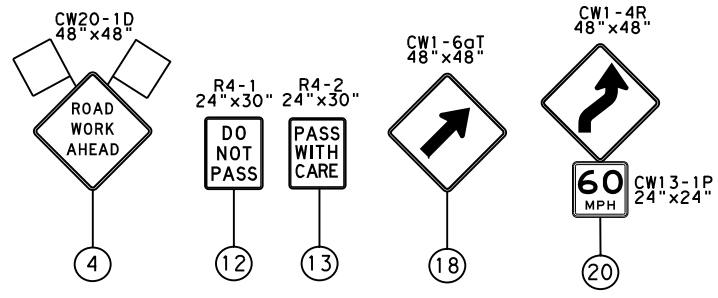
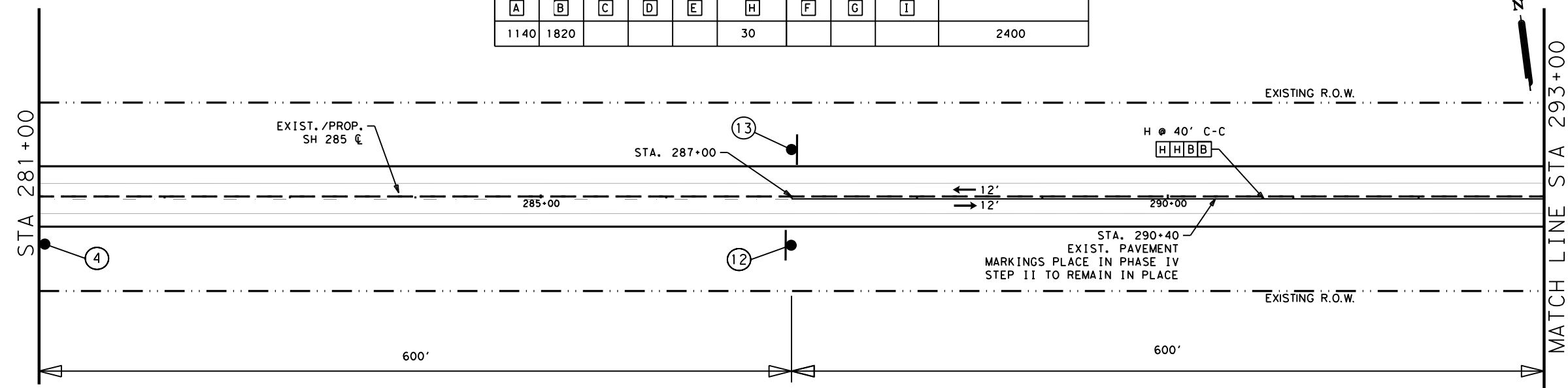
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	119	

DATE: 3/4/2024 5:01:12 PM
 FILE: D:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step III Sheets.dgn

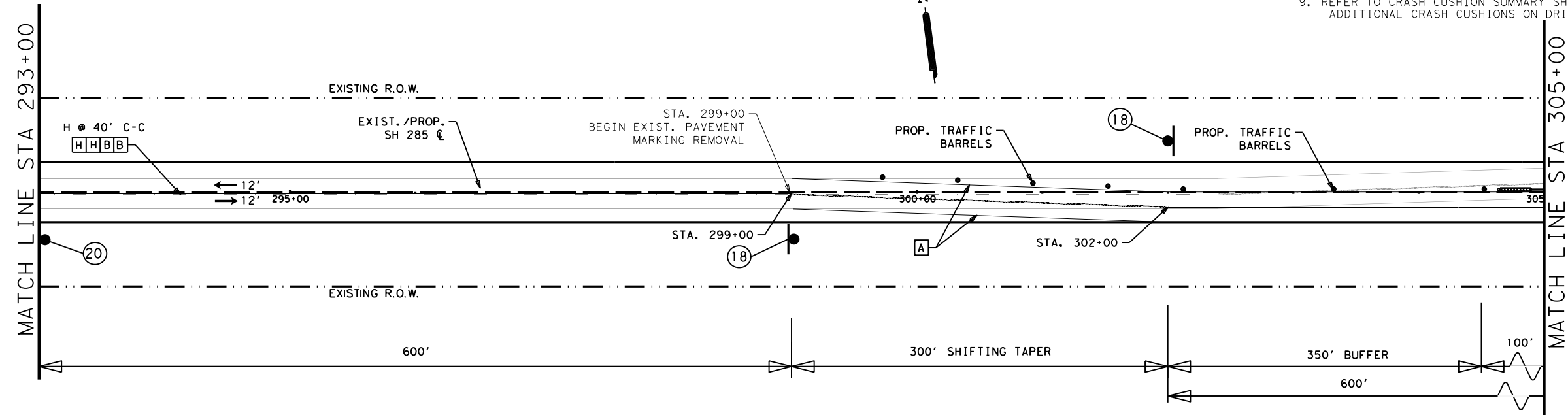
WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1140	1820				30				2400

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



- NOTES:
- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
 - EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
 - REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
 - EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
 - FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
 - FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
 - AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
 - REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
 - REFER TO CRASH CUSHION SUMMARY SHEET FOR ADDITIONAL CRASH CUSHIONS ON DRIVEWAYS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TRAFFIC CONTROL PLAN PHASE IV STEP III

SCALE: 1" = 100' SHEET 1 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	120	

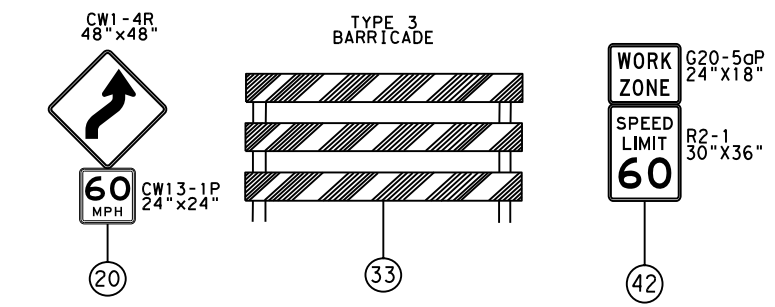
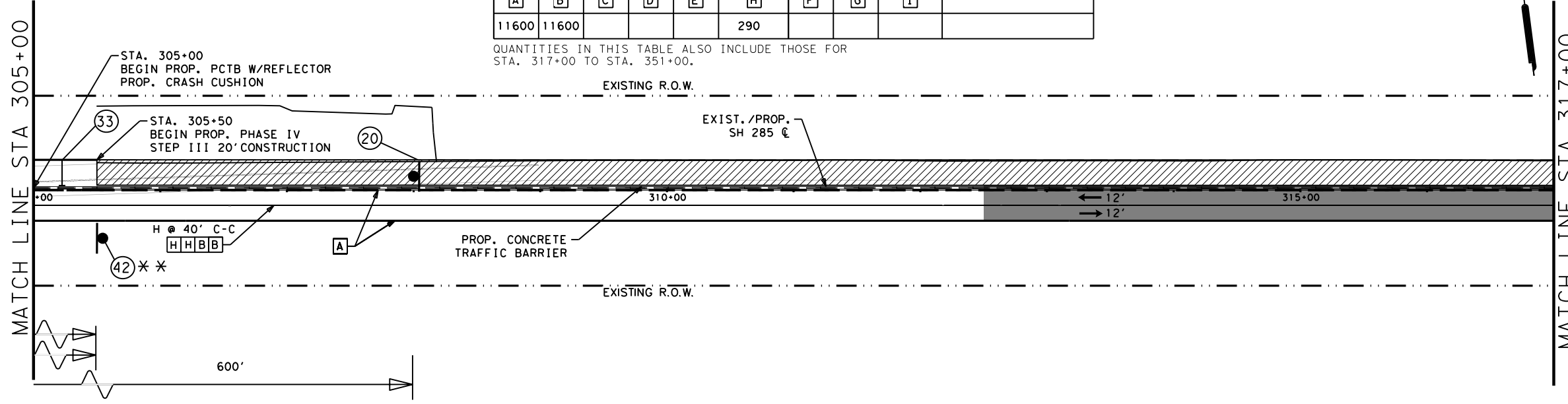
DATE: 3/4/2024 5:01:13 PM
 FILE: D:\xtdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step III Sheets.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
11600	11600				290				

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 317+00 TO STA. 351+00.

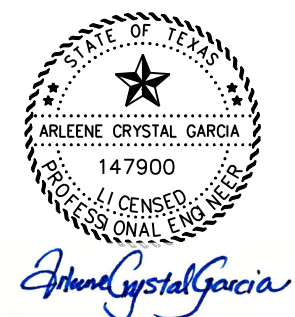
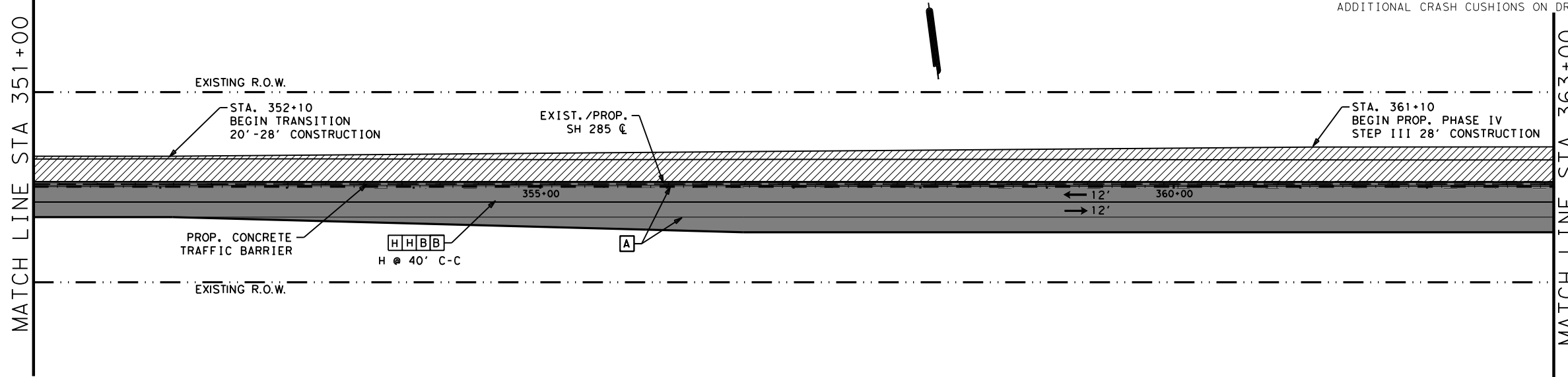
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO CRASH CUSHION SUMMARY SHEET FOR ADDITIONAL CRASH CUSHIONS ON DRIVEWAYS.



03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TRAFFIC CONTROL PLAN PHASE IV STEP III

SCALE: 1" = 100' SHEET 2 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	121	

DATE: 3/4/2024 5:01:15 PM
 FILE: D:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step III Sheets.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)									PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)						(NON-REMOV)			678	677
662	662	662	662	662	662	662	662	662	6008	6001
6067	6098	6064	6096	6075	6050	6008	6037	6002		
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"		ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I		
24600	24600				616					

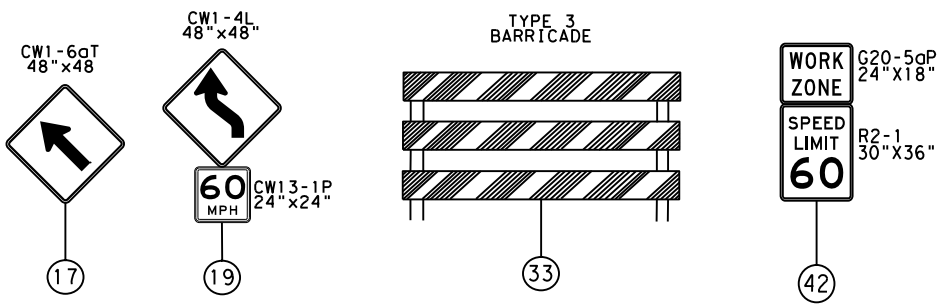
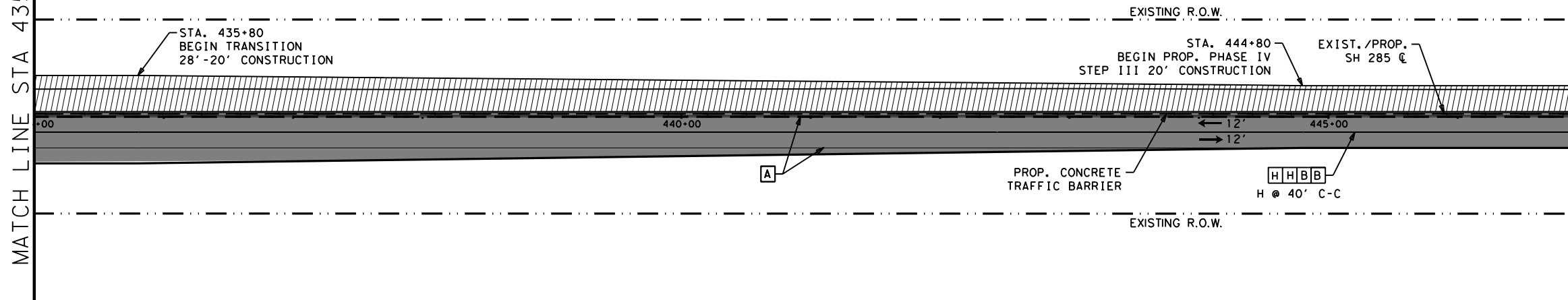
QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 363+00 TO STA. 435+00 & STA. 447+00 TO STA. 474+00.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

MATCH LINE STA 435+00

MATCH LINE STA 447+00

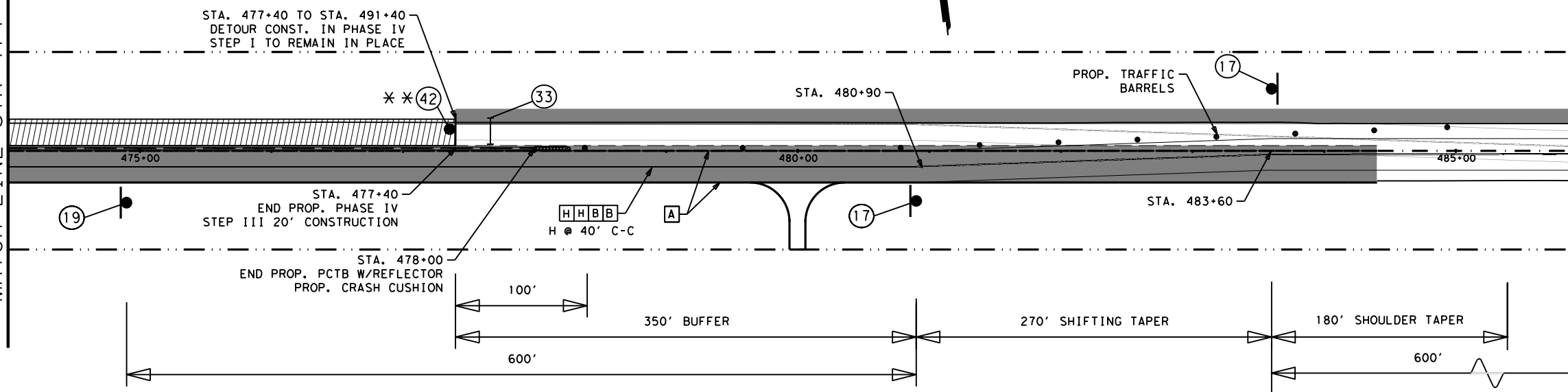


NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO CRASH CUSHION SUMMARY SHEET FOR ADDITIONAL CRASH CUSHIONS ON DRIVEWAYS.

MATCH LINE STA 474+00

MATCH LINE STA 486+00



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

SH 285 TRAFFIC CONTROL PLAN PHASE IV STEP III

SCALE: 1" = 100' SHEET 3 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	122	

DATE: 3/4/2024 5:01:16 PM
 FILE: pw:\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase IV Step III Sheets.dgn

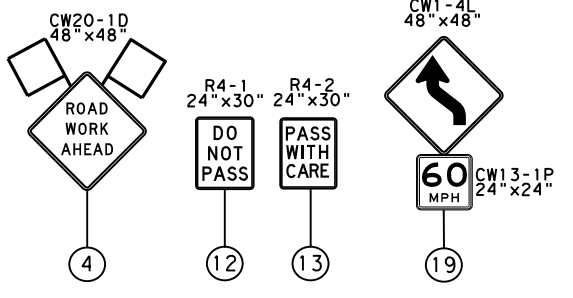
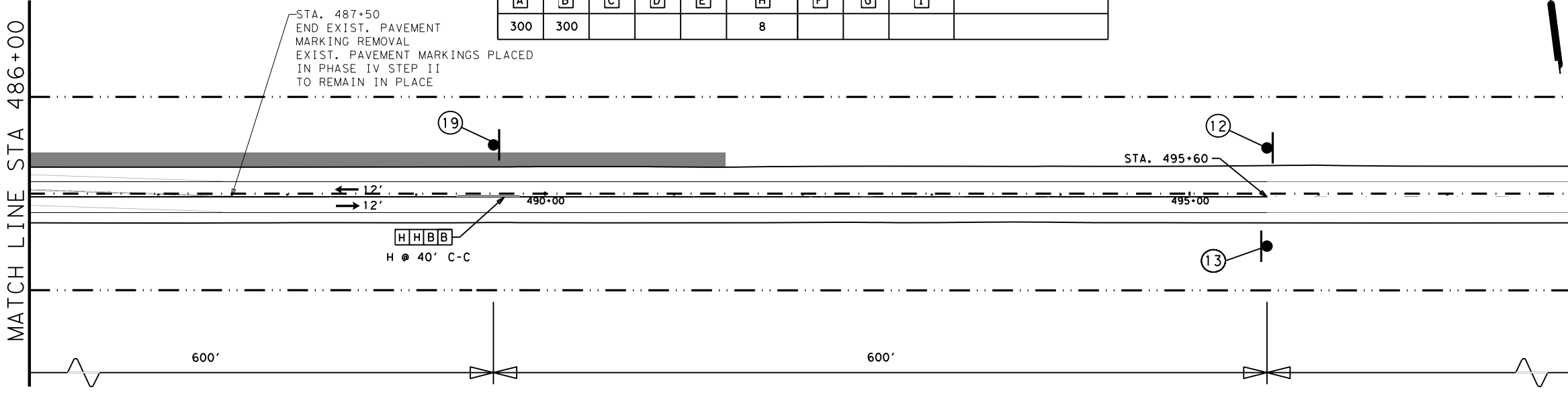
WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
300	300				8				

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG

MATCH LINE STA 486+00

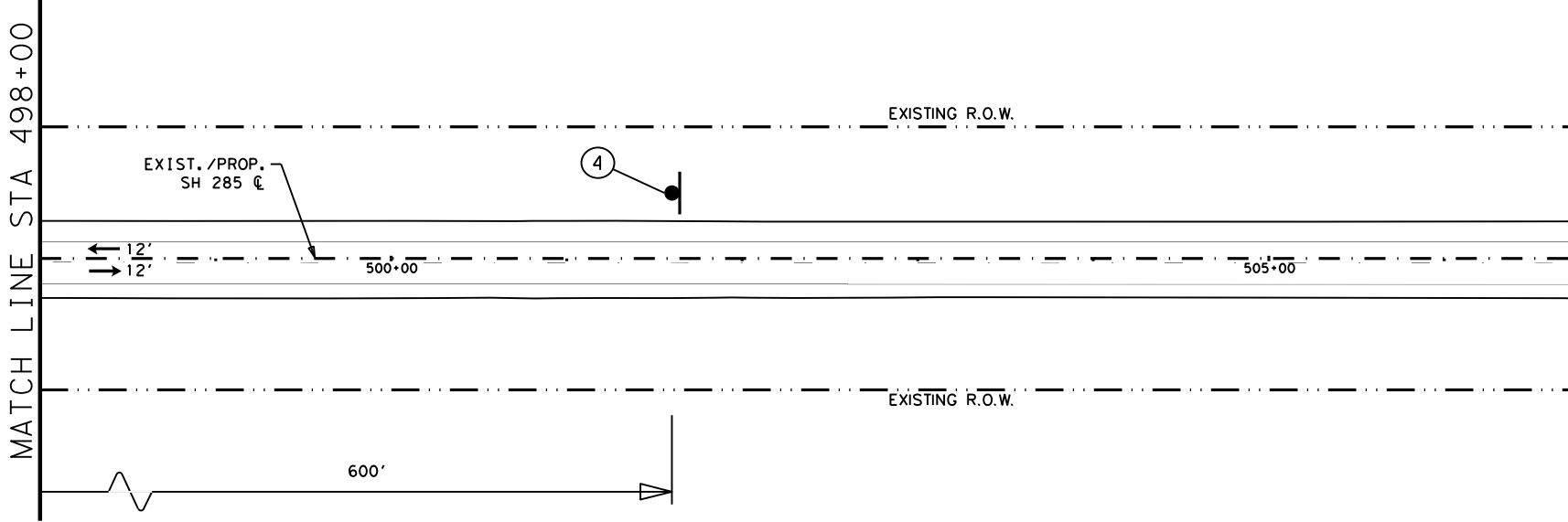
MATCH LINE STA 498+00



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 305+50 TO STA. 477+40.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO CRASH CUSHION SUMMARY SHEET FOR ADDITIONAL CRASH CUSHIONS ON DRIVEWAYS.

MATCH LINE STA 498+00



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

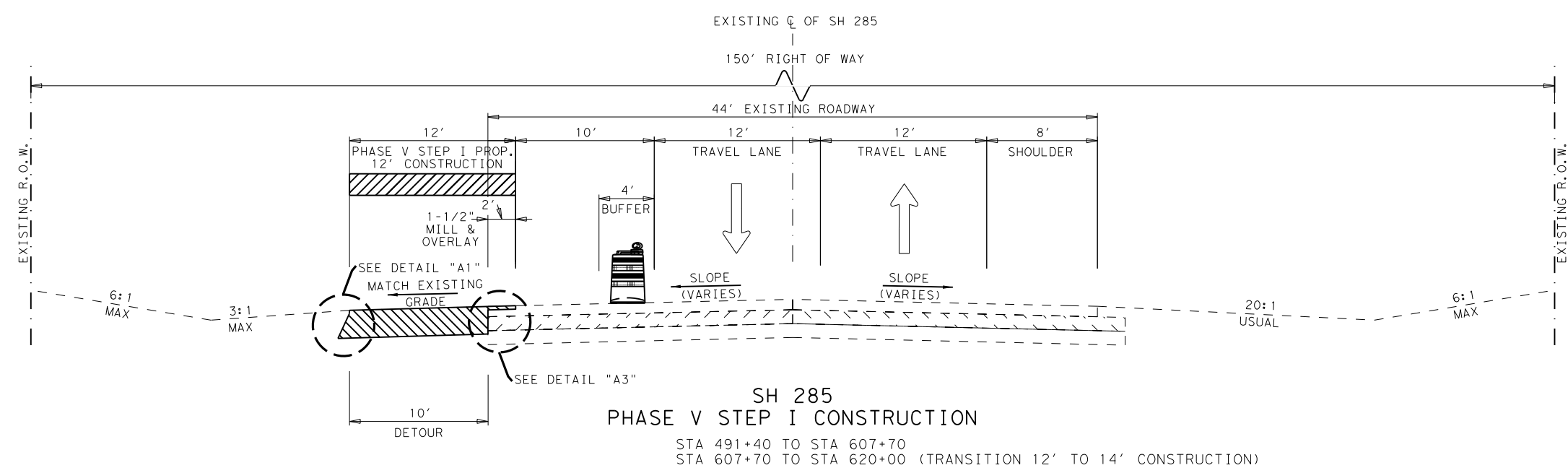
Texas Department of Transportation

SH 285 TCP PHASE IV STEP III

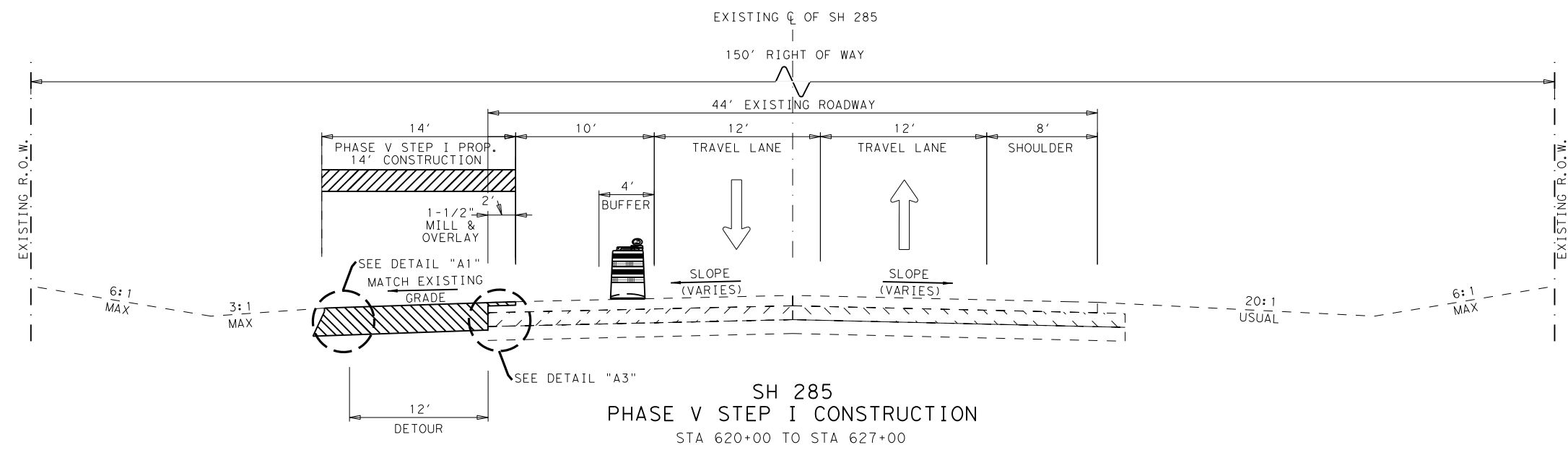
SCALE: 1" = 100' SHEET 4 OF 4

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		123

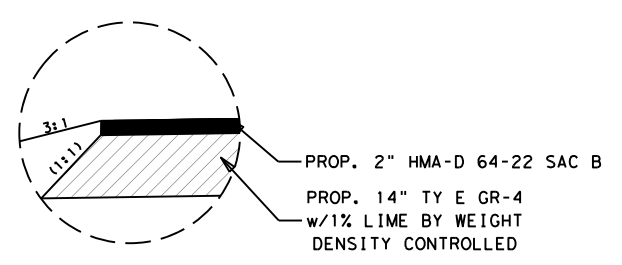
DATE: 3/4/2024 5:01:26 PM
 FILE: \\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP Phase V Typical Sections NEW.dgn



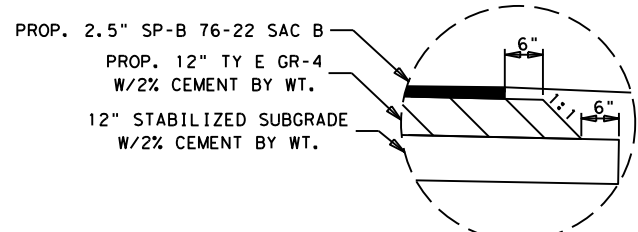
SH 285
 PHASE V STEP I CONSTRUCTION
 STA 491+40 TO STA 607+70
 STA 607+70 TO STA 620+00 (TRANSITION 12' TO 14' CONSTRUCTION)



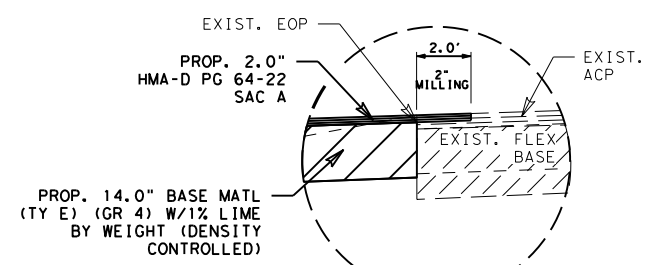
SH 285
 PHASE V STEP I CONSTRUCTION
 STA 620+00 TO STA 627+00



PAVEMENT DETAIL "A1"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.



PAVEMENT DETAIL "A3"
 N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - CENTER LINE
 - PROFILE GRADE LINE
 - PERMISSIBLE CONSTRUCTION JOINT
 - R. O. W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

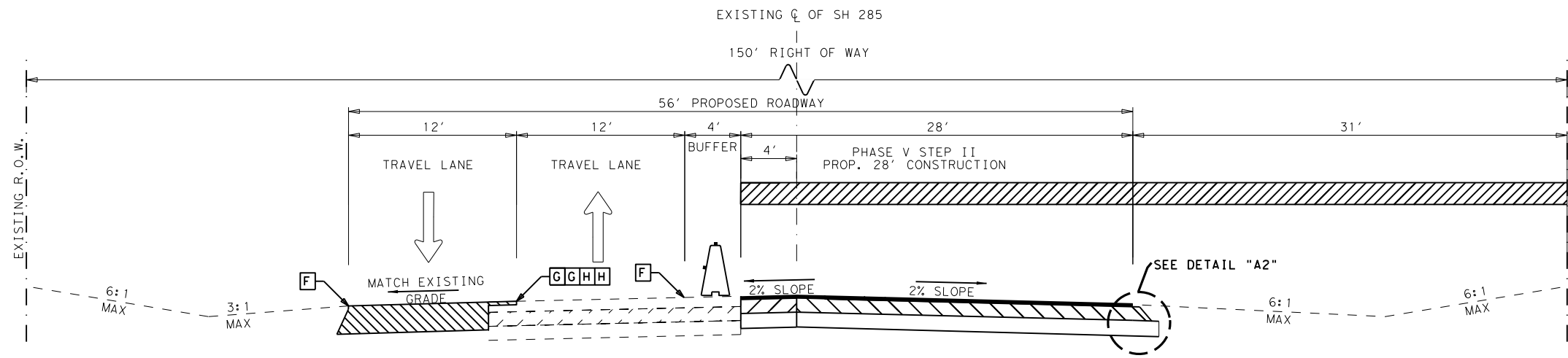
Pharr District Central Design



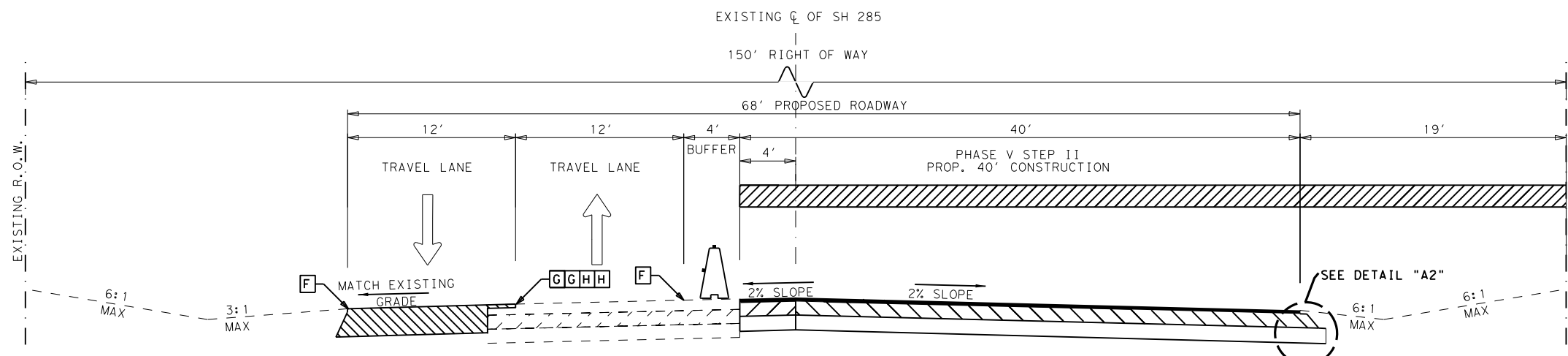
SH 285
 TCP
 PHASE V
 TYPICAL SECTIONS

N. T. S.		SHEET 1 OF 4	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO.
			124

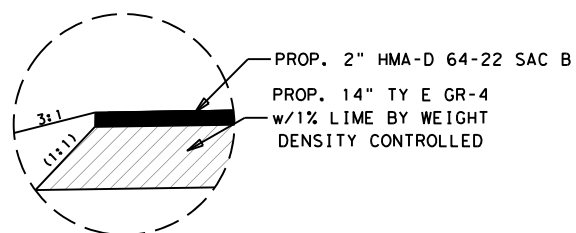
DATE: 3/4/2024 5:01:27 PM
 FILE: \\phr\dot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V\SH285 TCP PHASE V TYPICALS NEW.dgn



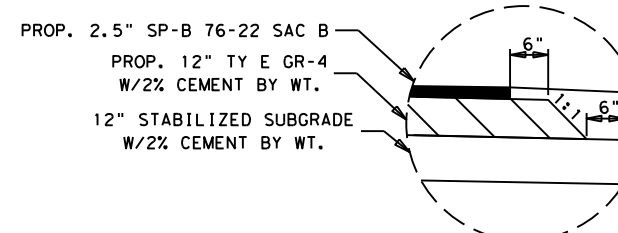
SH 285
 PHASE V STEP II CONSTRUCTION
 STA 484+40 TO STA 524+00
 STA 524+00 TO STA 528+50 (28' TO 40' TRANSITION)



SH 285
 PHASE V STEP II CONSTRUCTION
 STA 528+50 TO STA 607+70
 STA 607+70 TO STA 620+00 (40' TO 28' TRANSITION)

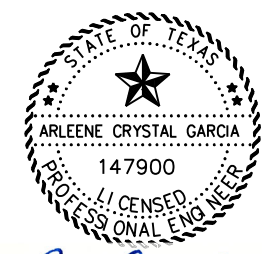


PAVEMENT DETAIL "A1"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - CL - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - PAV SURF PREP FOR MRK (4")
 - WORK ZONE PVMT MARK (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

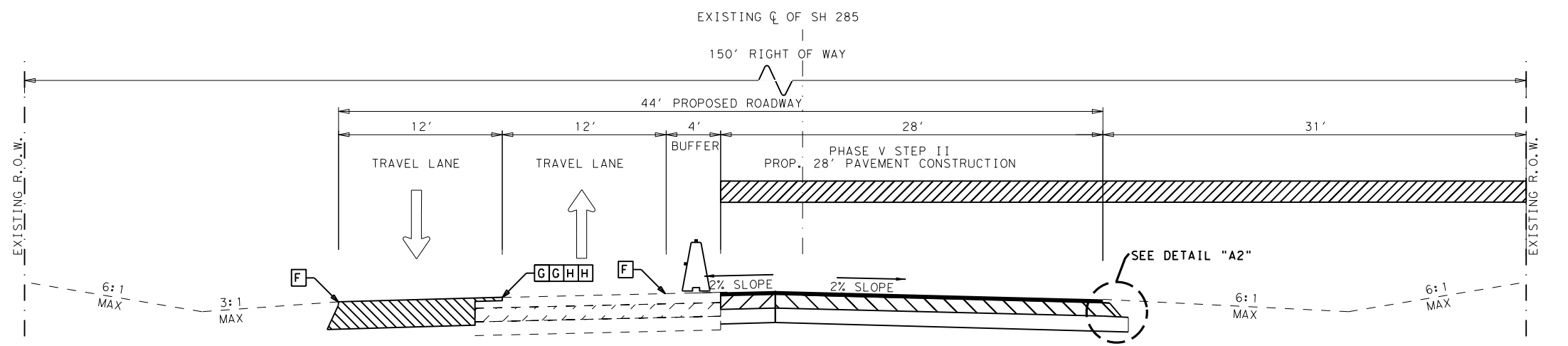
Texas Department of Transportation

**SH 285
 TCP
 PHASE V
 TYPICAL SECTIONS**

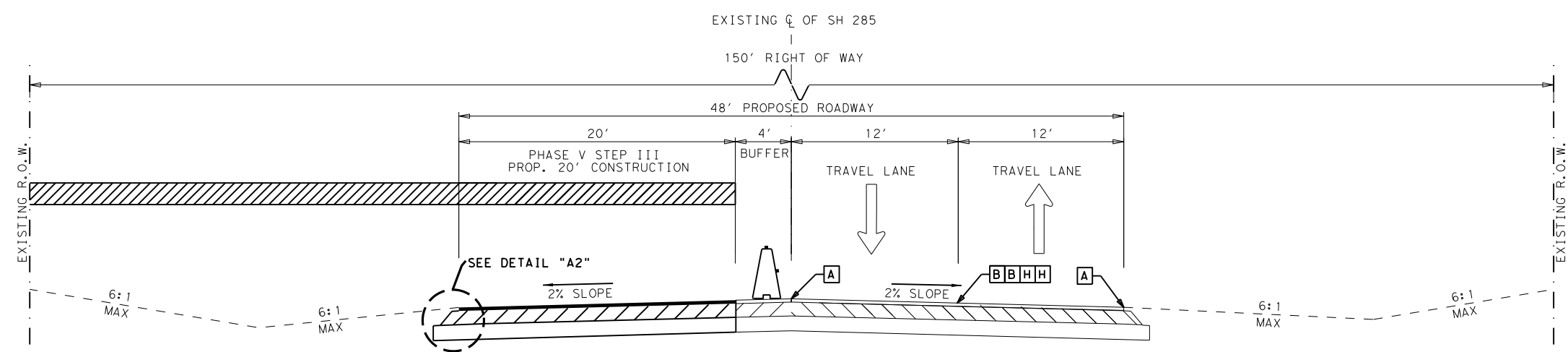
N. T. S. SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	125	

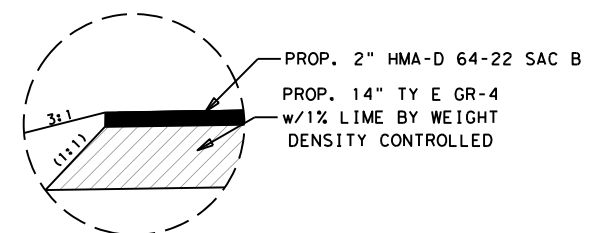
DATE: 3/4/2024 5:01:29 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE V\SH285 TCP PHASE V TYPICALS NEW.dgn



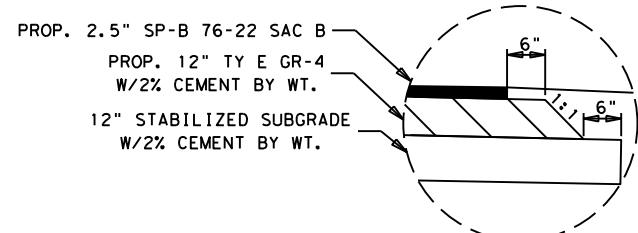
SH 285
 PHASE V STEP II CONSTRUCTION
 STA 620+00





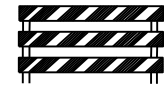

SH 285
 PHASE V STEP III CONSTRUCTION
 STA 477+40 TO STA 524+00
 STA 524+00 TO STA 533+00 (20' TO 28' TRANSITION)



PAVEMENT DETAIL "A1"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.

- LEGEND**
-  PLASTIC BARREL WITH REFLECTOR
 -  CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 -  TYPE 3 BARRICADE
 - ℄ - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REM) (REFL) TYP A-A @ 40 FT C-C
 -  WORK ZONE AREA



Arleene Crystal Garcia

03.07.2024

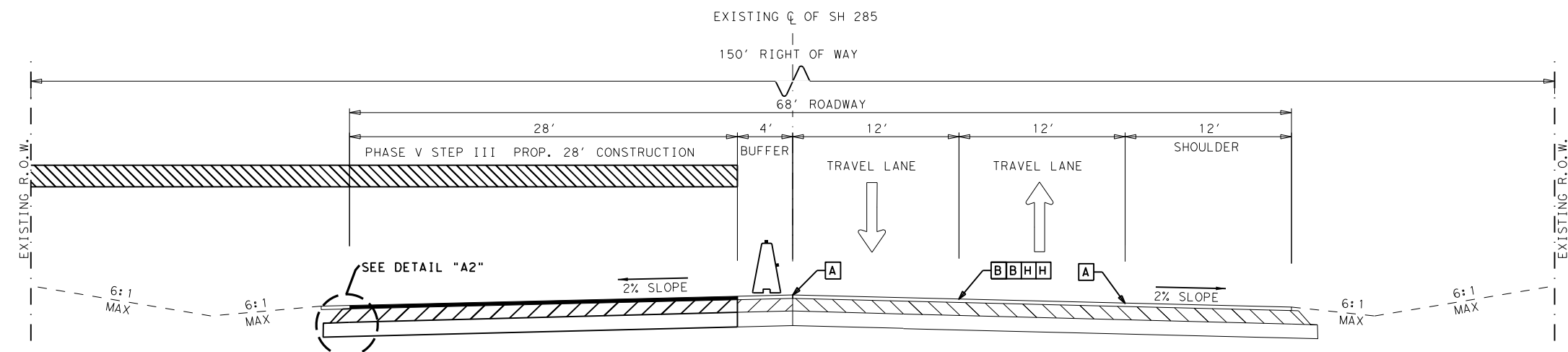
Pharr District Central Design



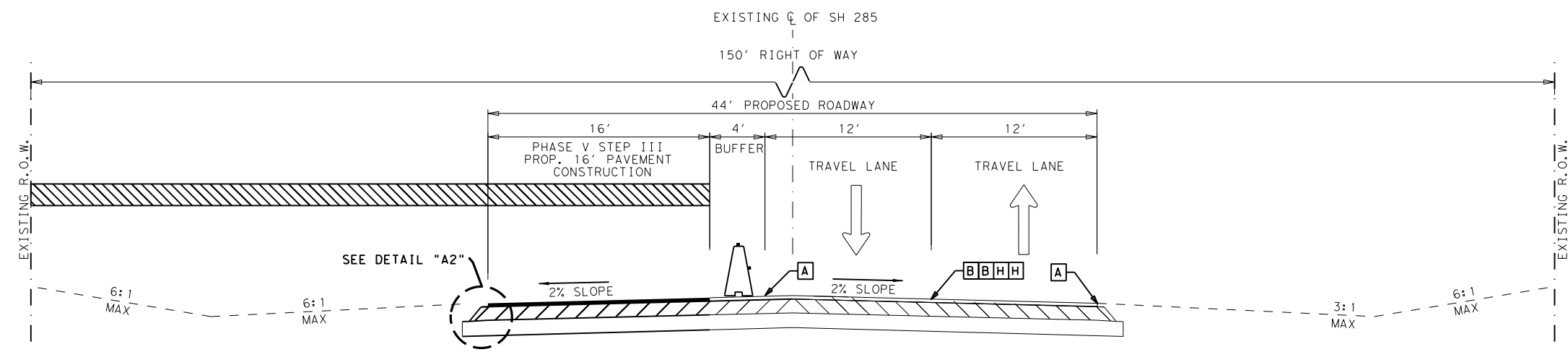
SH 285
 TCP
 PHASE V
 TYPICAL SECTIONS

N. T. S.		SHEET 3 OF 4	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO. 126

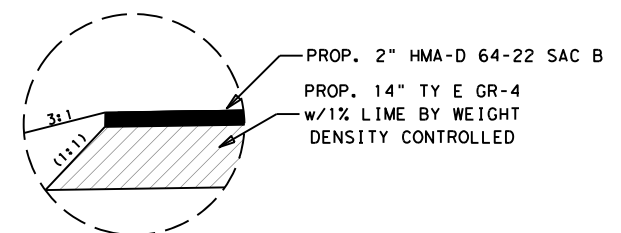
DATE: 3/4/2024 5:01:30 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V\SH285 TCP PHASE V TYPICALS NEW.dgn



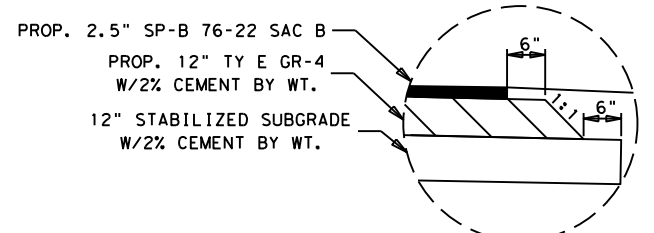
SH 285
 PHASE V STEP III CONSTRUCTION
 STA 533+00 TO STA 607+70
 STA 607+70 TO STA 620+00 (28' TO 16' TRANSITION)



SH 285
 PHASE V STEP III CONSTRUCTION
 STA 620+00



PAVEMENT DETAIL "A1"
 N. T. S.



PAVEMENT DETAIL "A2"
 N. T. S.

- LEGEND**
- PLASTIC BARREL WITH REFLECTOR
 - CONCRETE TRAFFIC BARRIER (CTB) W/ REFLECTORS
 - TYPE 3 BARRICADE
 - ℄ - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER
 - A - WORK ZONE PVMT MARK (REM) 4" WHITE SOLID
 - B - WORK ZONE PVMT MARK (REM) 4" YELLOW SOLID
 - F - WORK ZONE PVMT MARK (NON-REM) 4" WHITE SOLID
 - G - WORK ZONE PVMT MARK (NON-REM) 4" YELLOW SOLID
 - I - PAV SURF PREP FOR MRK (4")
 - H - WORK ZONE PVMT MARK (REFL) TYP A-A @ 40 FT C-C
 - WORK ZONE AREA



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE V
 TYPICAL SECTIONS

























N. T. S.		SHEET 4 OF 4	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO.
			127

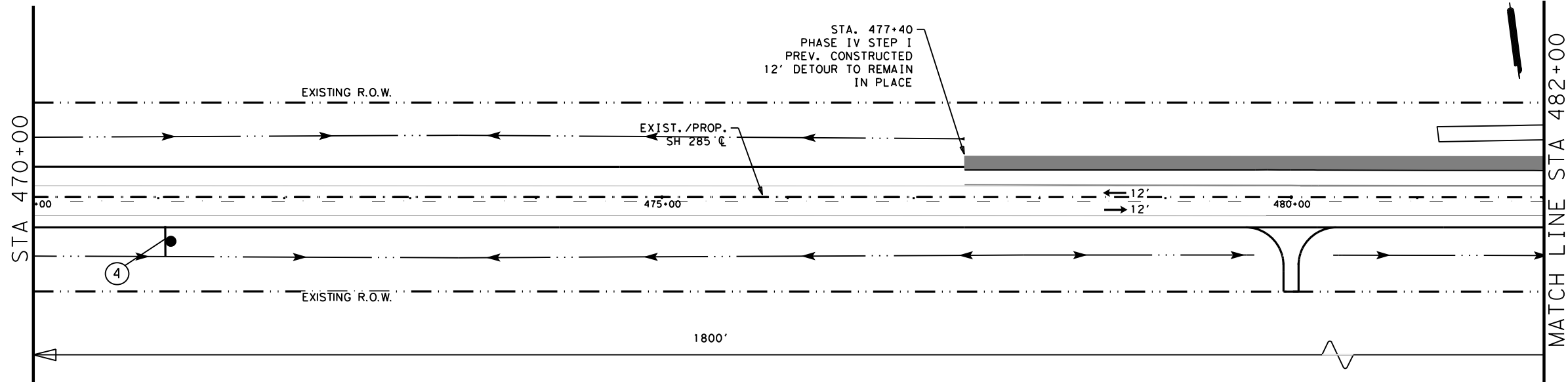
DATE: 3/4/2024 5:01:36 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE V STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 491+99, 509+85, 530+01, 573+99 AND 606+22. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

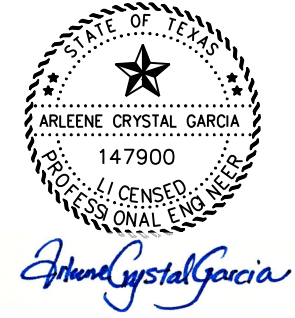
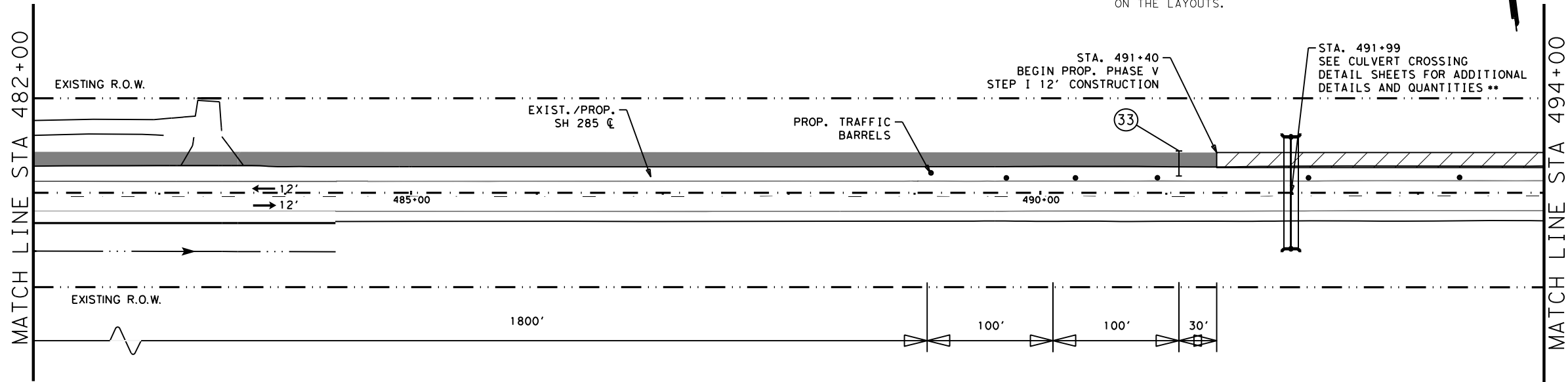
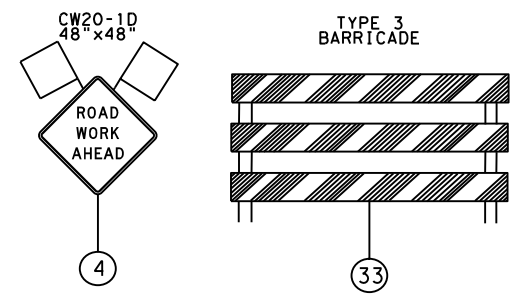
LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



03.07.2024

Pharr District Central Design



**SH 285
 TCP
 PHASE V STEP I**

SCALE: 1" = 100' SHEET 1 OF 4

























© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	128	

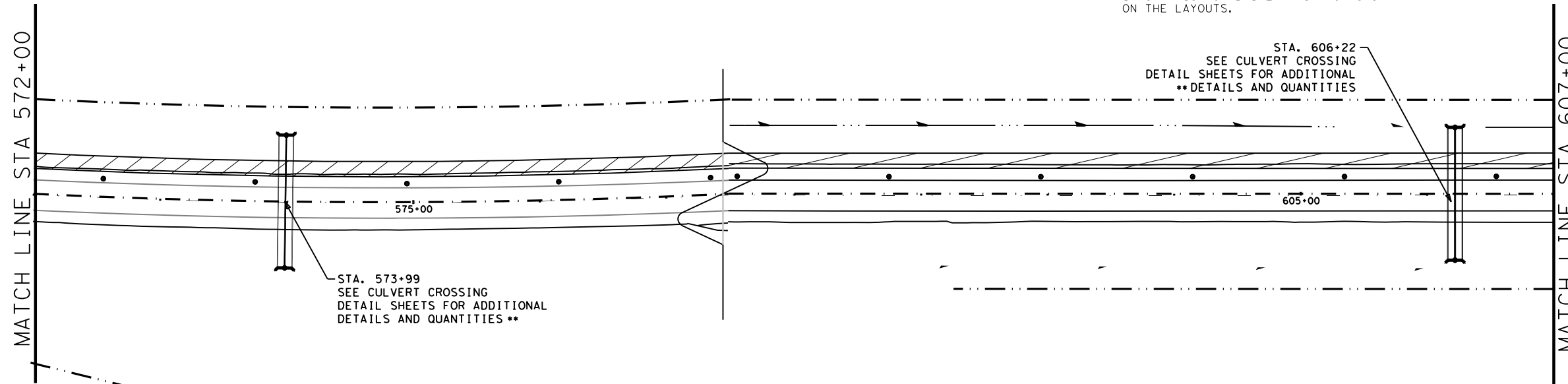
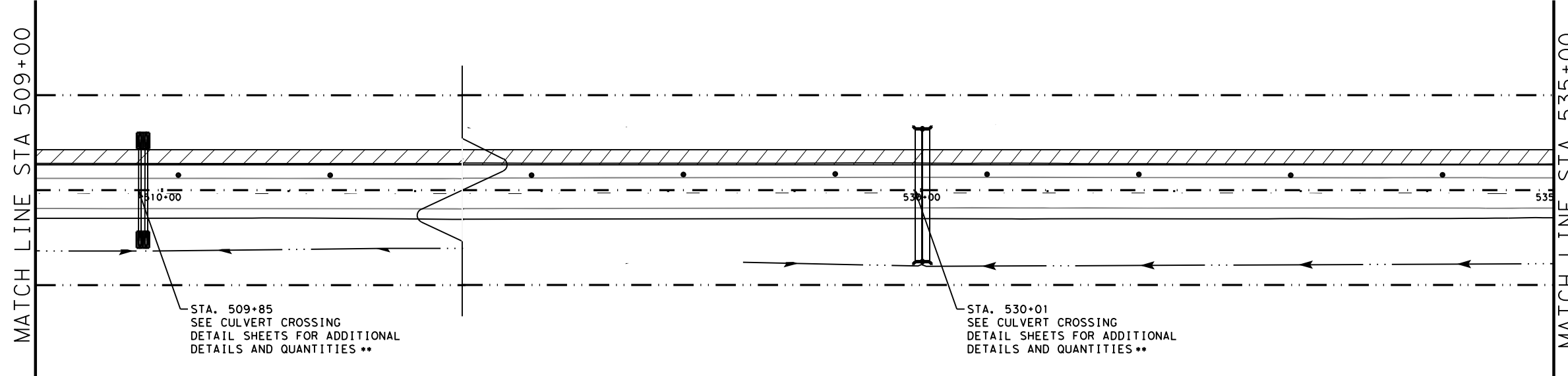
DATE: 3/4/2024 5:01:37 PM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE V STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 491+99, 509+85, 530+01, 573+99 AND 606+22. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLO)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:


1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

 **Texas Department of Transportation**

**SH 285
TCP
PHASE V STEP I**

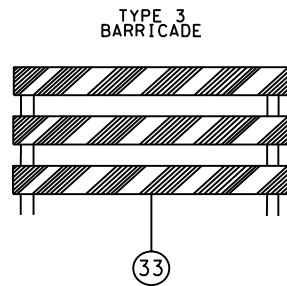
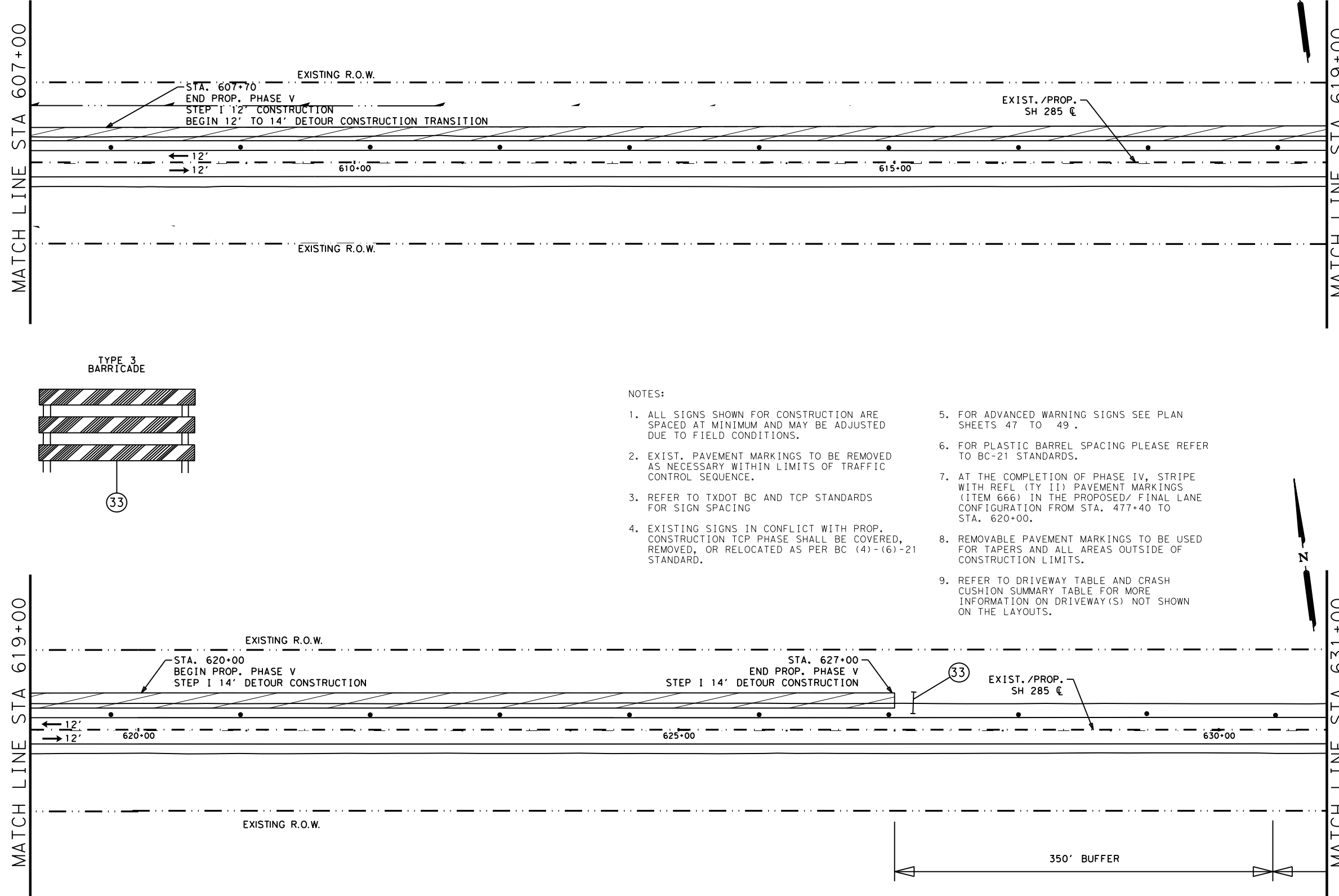
SCALE: 1" = 100' SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		129

DATE: 3/4/2024 5:01:39 PM
 FILE: P:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase V Step I.dgn

****NOTE TO CONTRACTOR:**

PHASE V STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 491+99, 509+85, 530+01, 573+99 AND 606+22. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK_ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK_ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK_ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK_ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK_ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK_ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK_ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK_ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK_ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK_ZONE PVMT MARK (REM) (W) (ARROW)
- WORK_ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE V STEP I**

SCALE: 1" = 100' SHEET 3 OF 4

























© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	130	

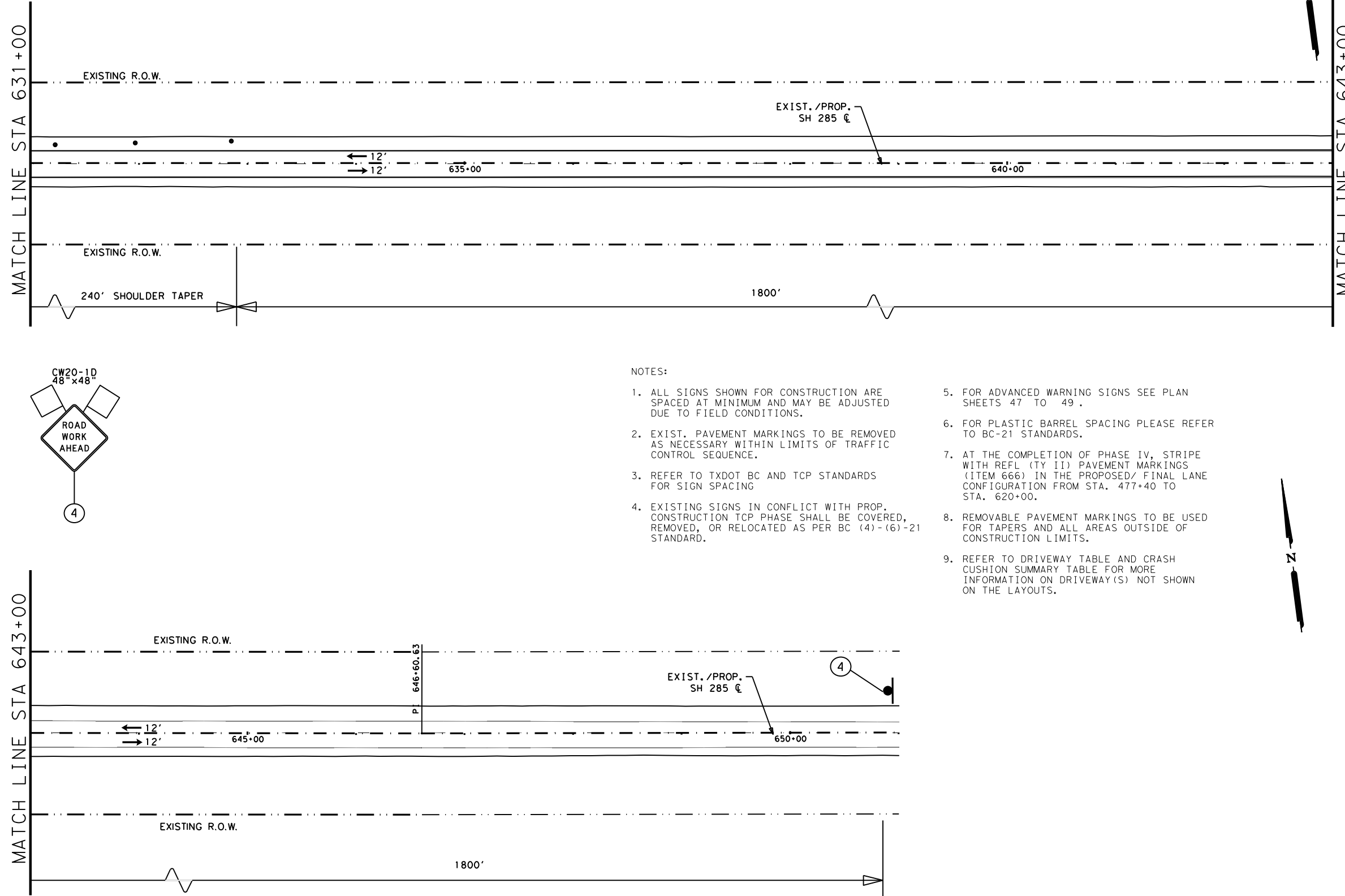
DATE: 3/4/2024 5:01:41 PM
 FILE: \\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP I SHEETS.dgn

****NOTE TO CONTRACTOR:**

PHASE V STEP I INVOLVES REMOVAL AND REPLACEMENT OF EXISTING DRAINAGE CROSSINGS ON STATIONS 491+99, 509+85, 530+01, 573+99 AND 606+22. THESE CROSSINGS SHALL BE COMPLETED ENTIRELY PRIOR TO PROCEEDING WITH CONSTRUCTION OF PROPOSED DETOUR. SEE PROPOSED IRRIGATION LAYOUTS, CUT & RESTORE DETAILS AND TCP TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.

LEGEND

-  CONSTRUCTION AREA
-  TEMPORARY ROADWAY WIDENING
-  PREVIOUSLY CONSTRUCTED
-  CONCRETE TRAFFIC BARRIER (CTB)
-  CRASH CUSHION ATTENUATOR
-  TEMPORARY TRAFFIC SIGNAL
-  WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
-  WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
-  WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
-  WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
-  WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
-  PAV SURF PREP FOR MRK (6")
-  WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
-  WORK ZONE PVMT MARK (REM) (W) (ARROW)
-  WORK ZONE PVMT MARK (REM) (W) (WORD)
-  DIRECTION OF TRAFFIC FLOW
-  TYPE 3 BARRICADE
-  CHANNELIZING DEVICES (PLASTIC DRUMS)
-  REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
-  CONSTRUCTION SIGN
-  FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



**SH 285
TCP
PHASE V STEP I**

SCALE: 1" = 100' SHEET 4 OF 4

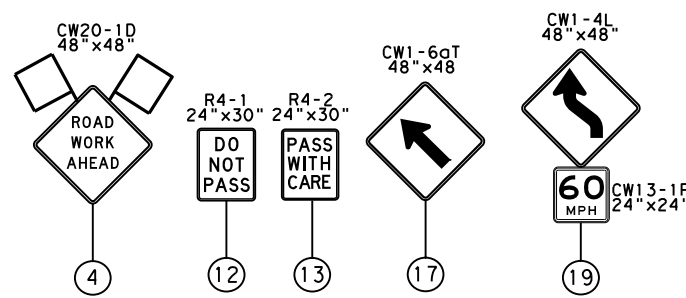
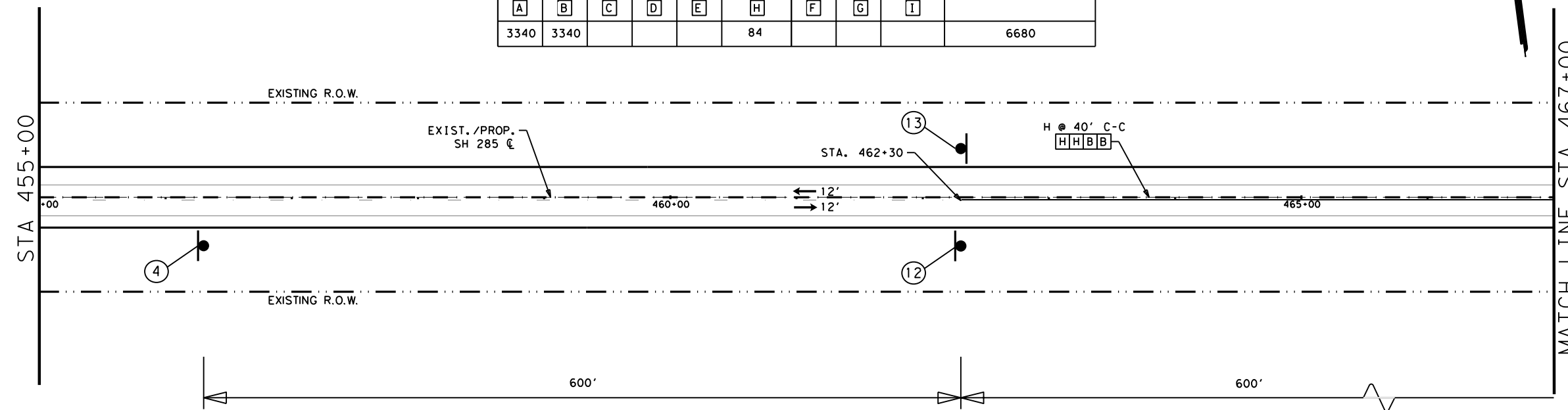
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	131	

DATE: 3/4/2024 5:01:45 PM
 FILE: \\txdot\project\wisonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
3340	3340				84				6680

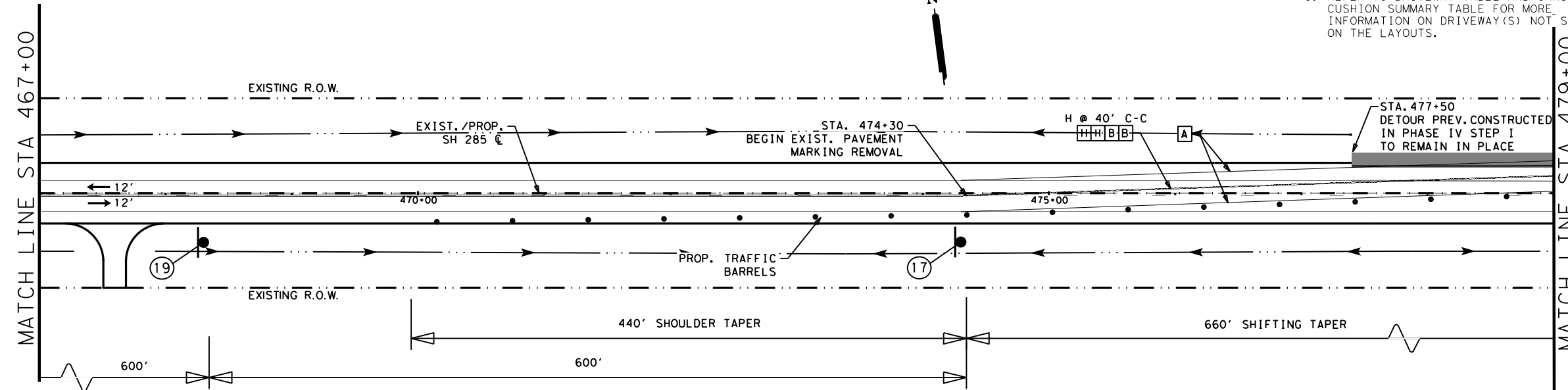
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE V STEP II

SCALE: 1" = 100' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		132

DATE: 3/4/2024 5:01:47 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1080	1080				280	10120	10120		6680

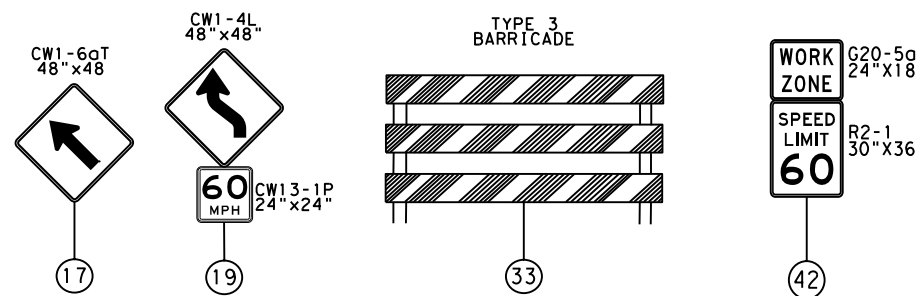
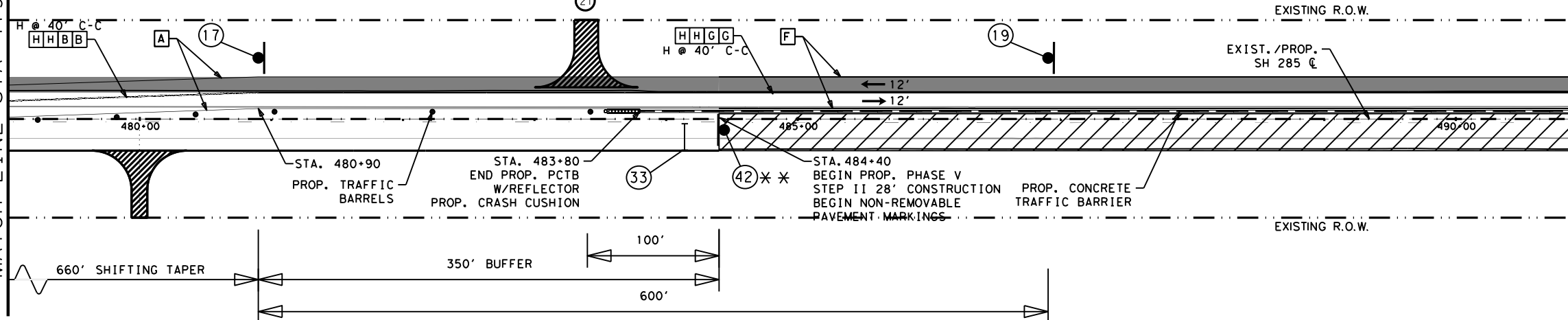
QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 491+00 TO STA. 523+00.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

MATCH LINE STA 479+00

MATCH LINE STA 491+00

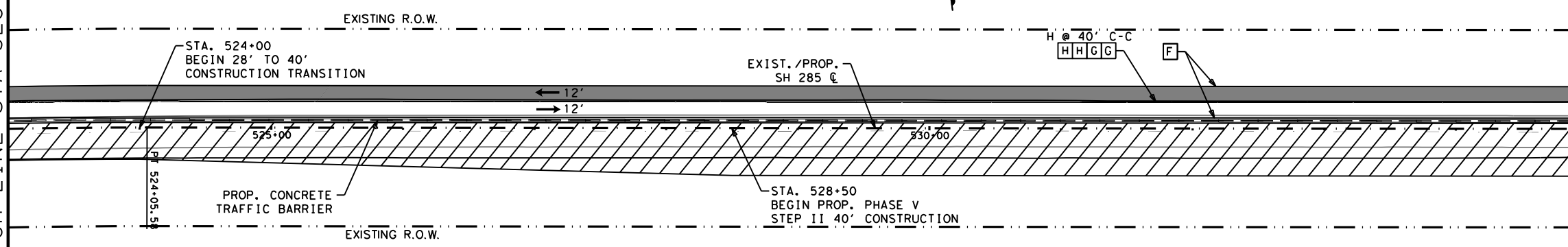


NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.

MATCH LINE STA 523+00

MATCH LINE STA 535+00



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE V STEP II**

SCALE: 1" = 100' SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	133	

DATE: 3/4/2024 5:01:49 PM
 FILE: \\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP Phase V Step II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)									PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)					(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001	
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")	
A	B	C	D	E	H	F	G	I		
2020	2020				480	17000	17000		11820	

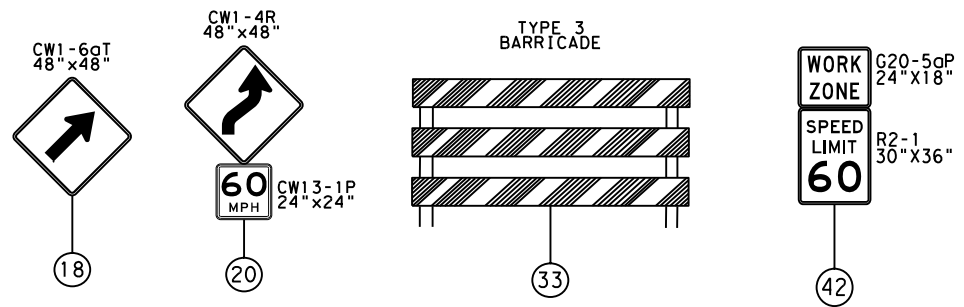
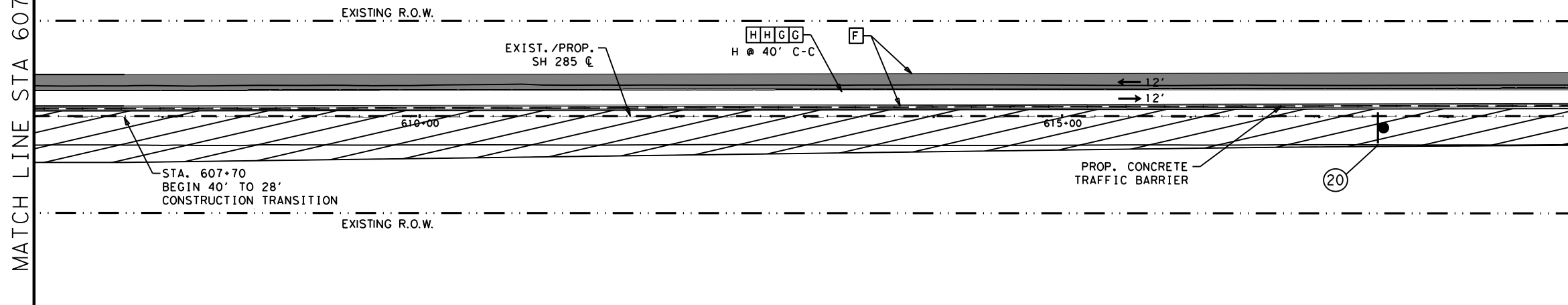
QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 535+00 TO STA. 607+00.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PAVT MARK (REM) 6" WHITE SOLID
- WORK ZONE PAVT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PAVT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PAVT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PAVT MARK (REM) 24" WHITE SOLID
- WORK ZONE PAVT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PAVT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PAVT MARK (REM) (REF) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PAVT MARK (REM) (W) (8") (SLD)
- WORK ZONE PAVT MARK (REM) (W) (ARROW)
- WORK ZONE PAVT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

MATCH LINE STA 607+00

MATCH LINE STA 619+00

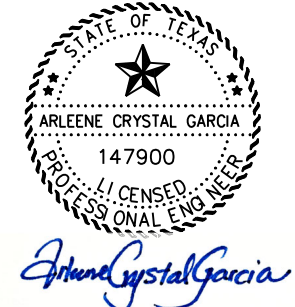
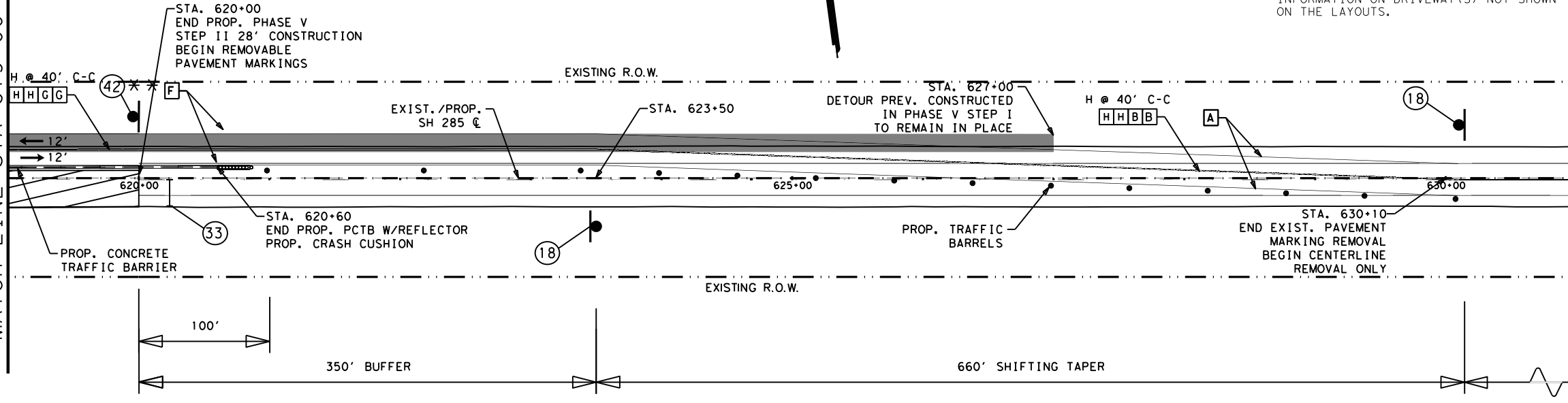


NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.

MATCH LINE STA 619+00

MATCH LINE STA 631+00



03.07.2024

Pharr District Central Design



SH 285
 TCP
 PHASE V STEP II

SCALE: 1" = 100' SHEET 3 OF 4

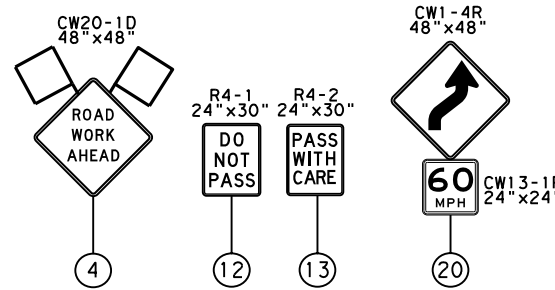
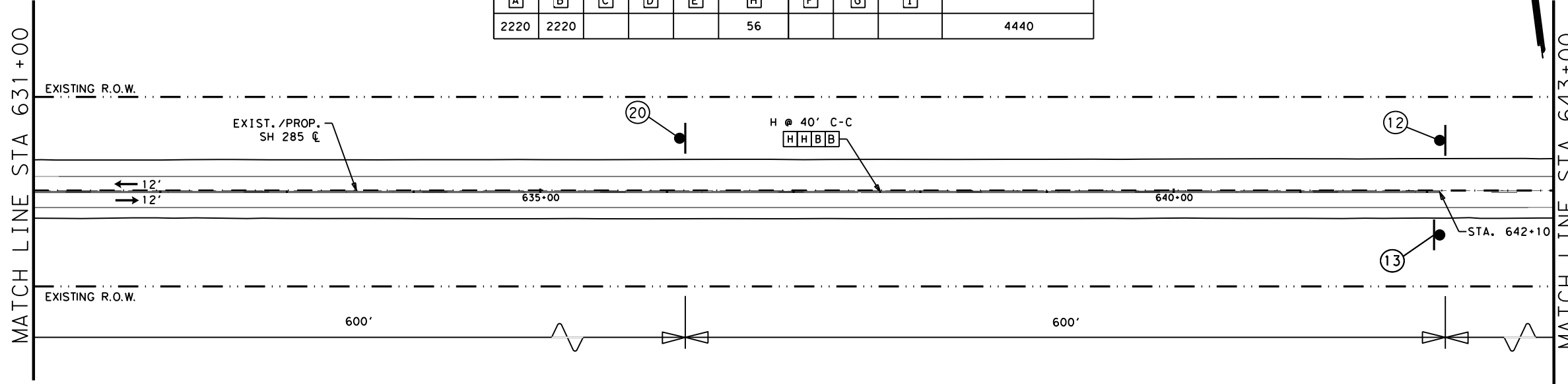
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		134

DATE: 3/4/2024 5:01:50 PM
 FILE: \\txdot\project\wisonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP II SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
2220	2220				56				4440

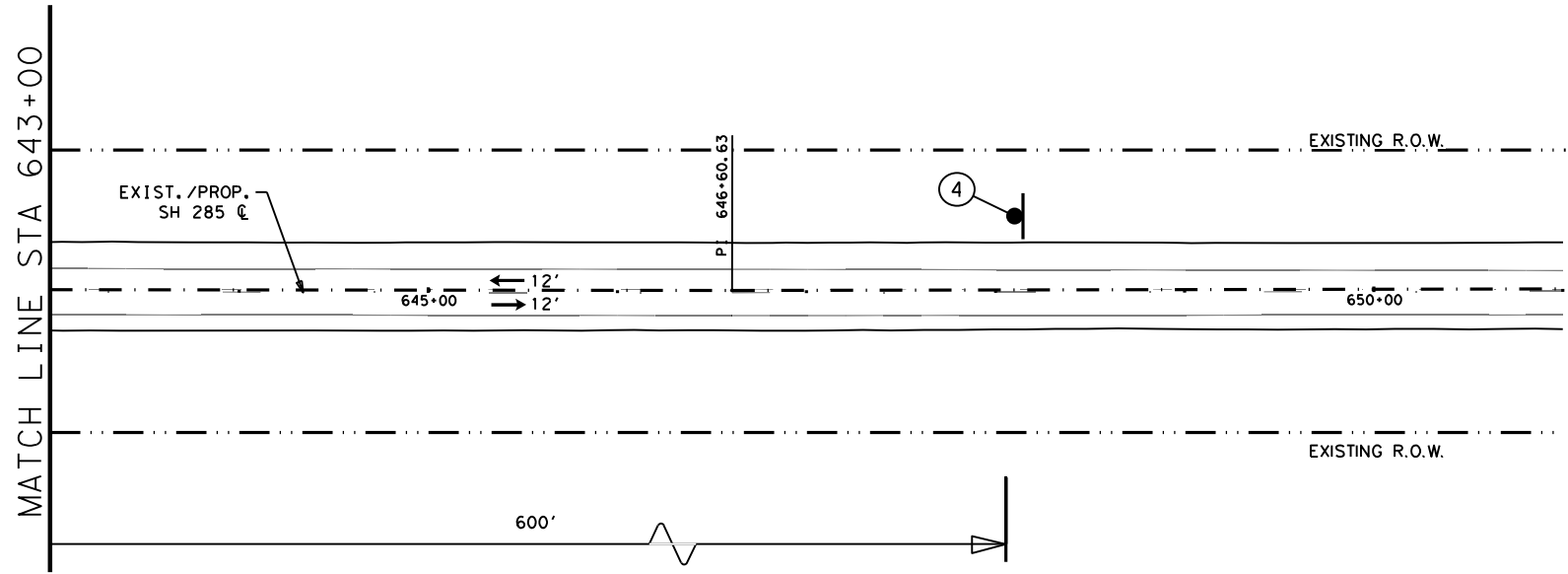
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE V STEP II**

SCALE: 1" = 100' SHEET 4 OF 4

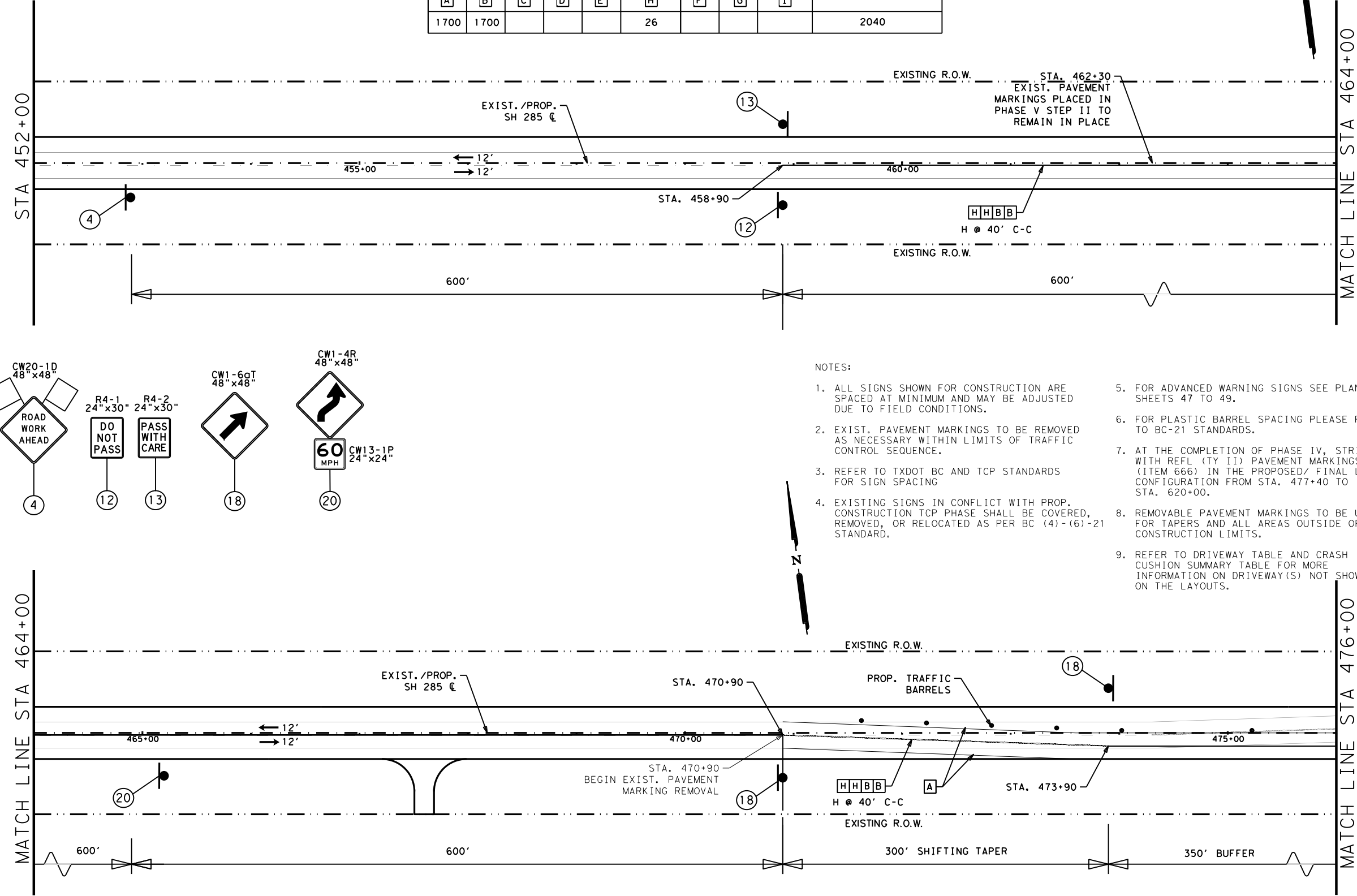
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		135

DATE: 3/4/2024 5:01:55 PM
 FILE: P:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
1700	1700				26				2040

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49.
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE V STEP III**

SCALE: 1" = 100' SHEET 1 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	136	

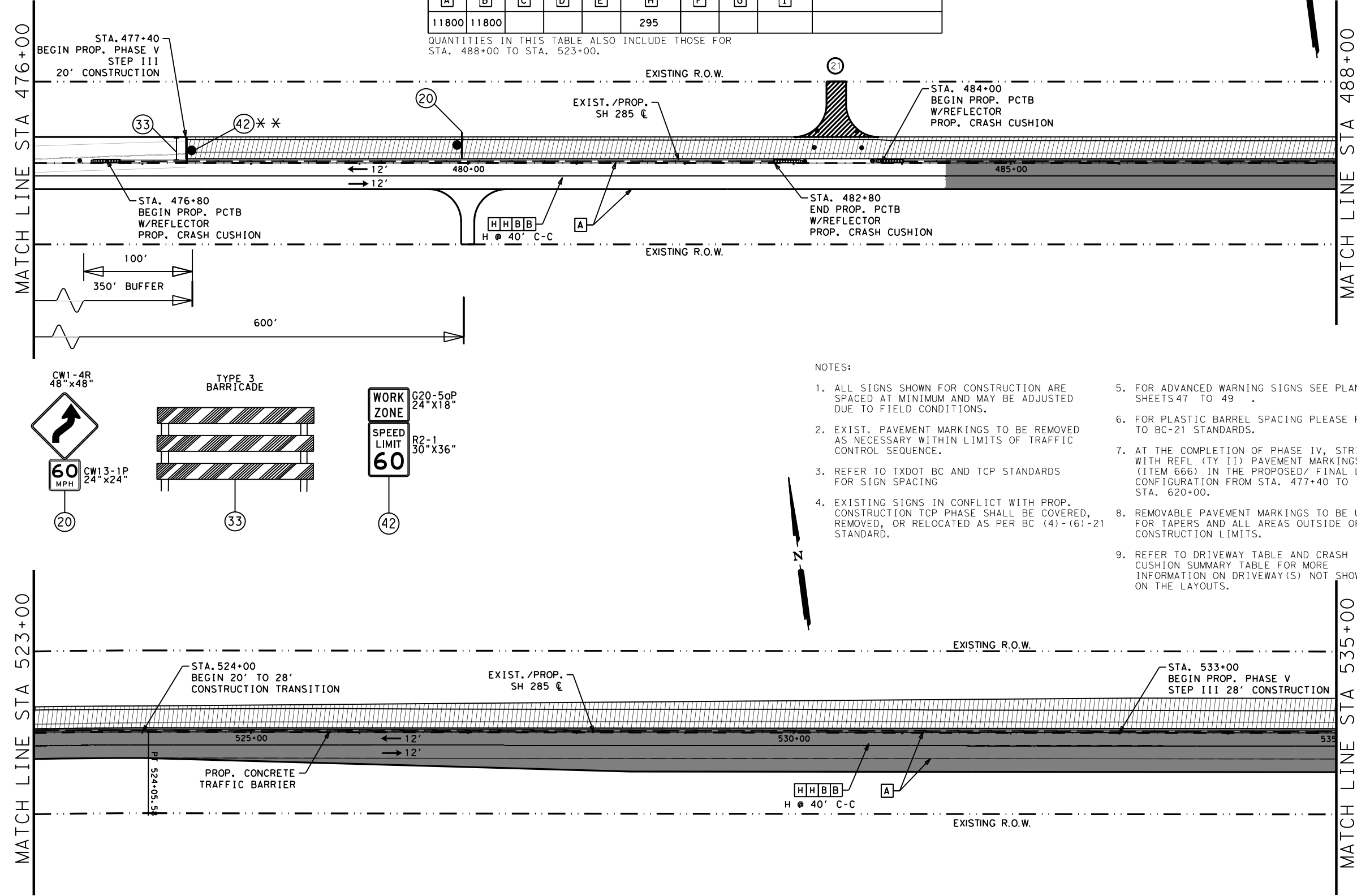
DATE: 3/4/2024 5:01:57 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662	662	662	662	662	662	662	662	678	677
6067	6098	6064	6096	6075	6050	6008	6037	6002	6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
11800	11800				295				

QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 488+00 TO STA. 523+00.

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE V STEP III**

SCALE: 1" = 100' SHEET 2 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	137	

DATE: 3/4/2024 5:01:59 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	
19020	19020				456				

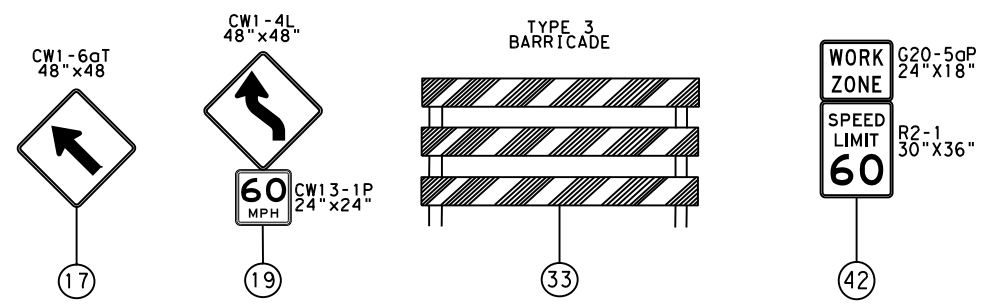
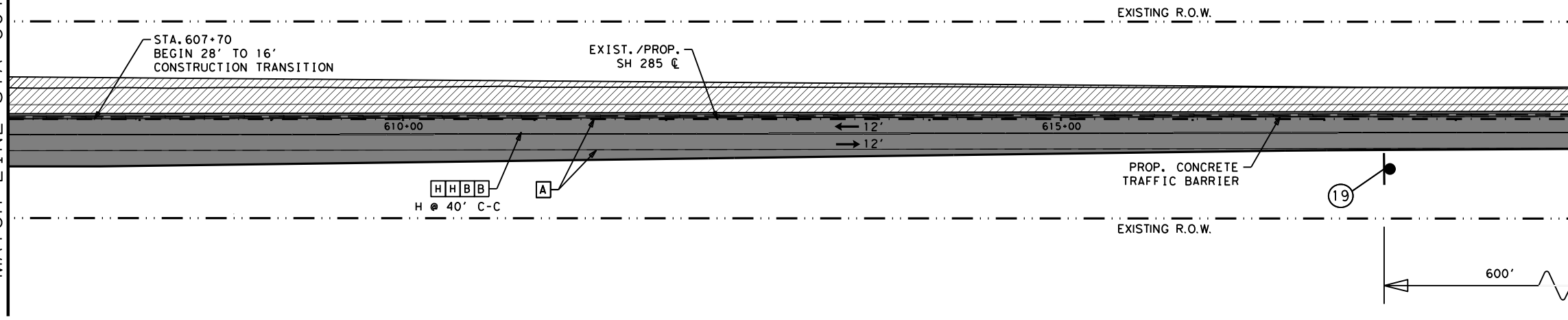
QUANTITIES IN THIS TABLE ALSO INCLUDE THOSE FOR STA. 535+00 TO STA. 607+00

LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG
- REFER TO BC(3)-21 STANDARDS FOR SIGN SPACING

MATCH LINE STA 607+00

MATCH LINE STA 619+00

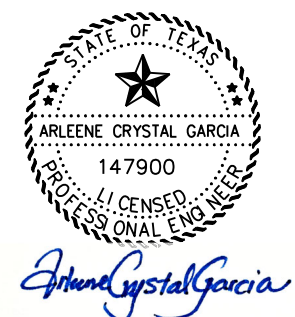
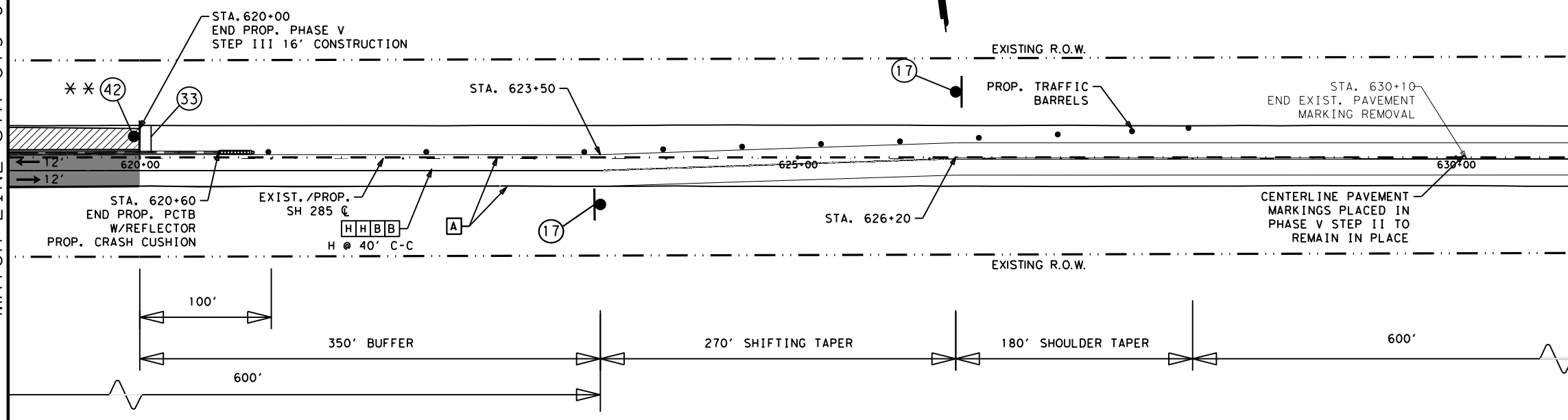


NOTES:

- ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
- EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
- REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
- EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
- FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
- FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
- AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
- REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
- REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.

MATCH LINE STA 619+00

MATCH LINE STA 631+00



03.07.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
TCP
PHASE V STEP III**

SCALE: 1" = 100' SHEET 3 OF 4

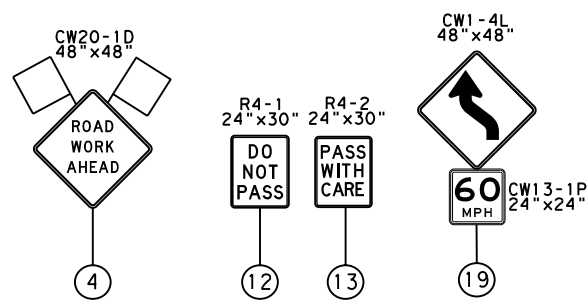
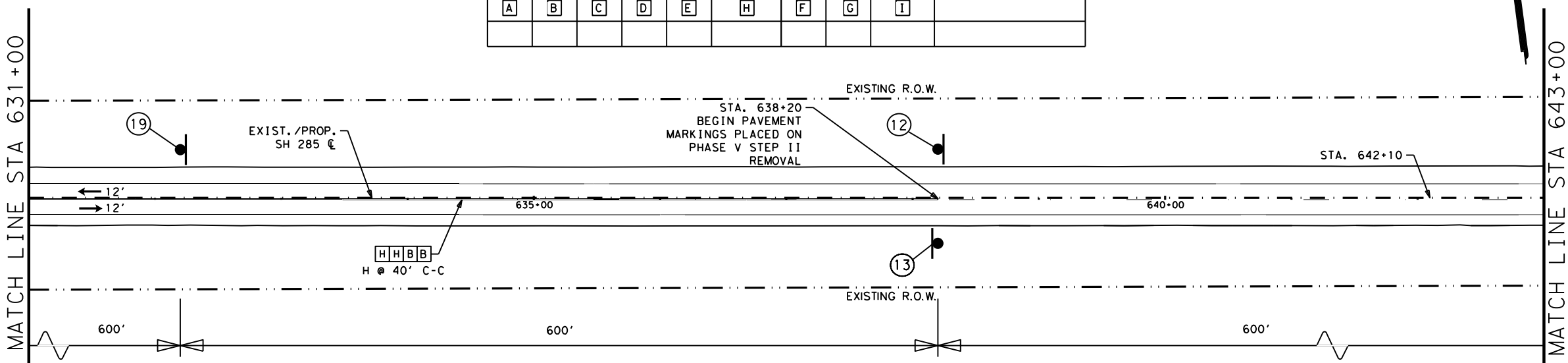
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		138

DATE: 3/4/2024 5:02:00 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE V STEP III SHEETS.dgn

WORK ZONE PAVEMENT MARKINGS (ITEM 662)								PAV SURF PREP	ELIM. EXIST. PAV. MARKINGS (ITEM 677)
(REMOVABLE)				(NON-REMOV)					
662 6067	662 6098	662 6064	662 6096	662 6075	662 6050	662 6008	662 6037	678 6002	677 6001
6" (W) SLD (LF)	6" (Y) SLD (LF)	6" (W) BRK (LF)	6" (Y) BRK (LF)	24" (W) SLD (LF)	PAV MRK TY II-A-A (EA)	6" (W) SLD (LF)	6" (Y) SLD (LF)	PAV SURF PREP FOR MRK 6"	ELIM EXT PAV MRK & MRKS (4")
A	B	C	D	E	H	F	G	I	

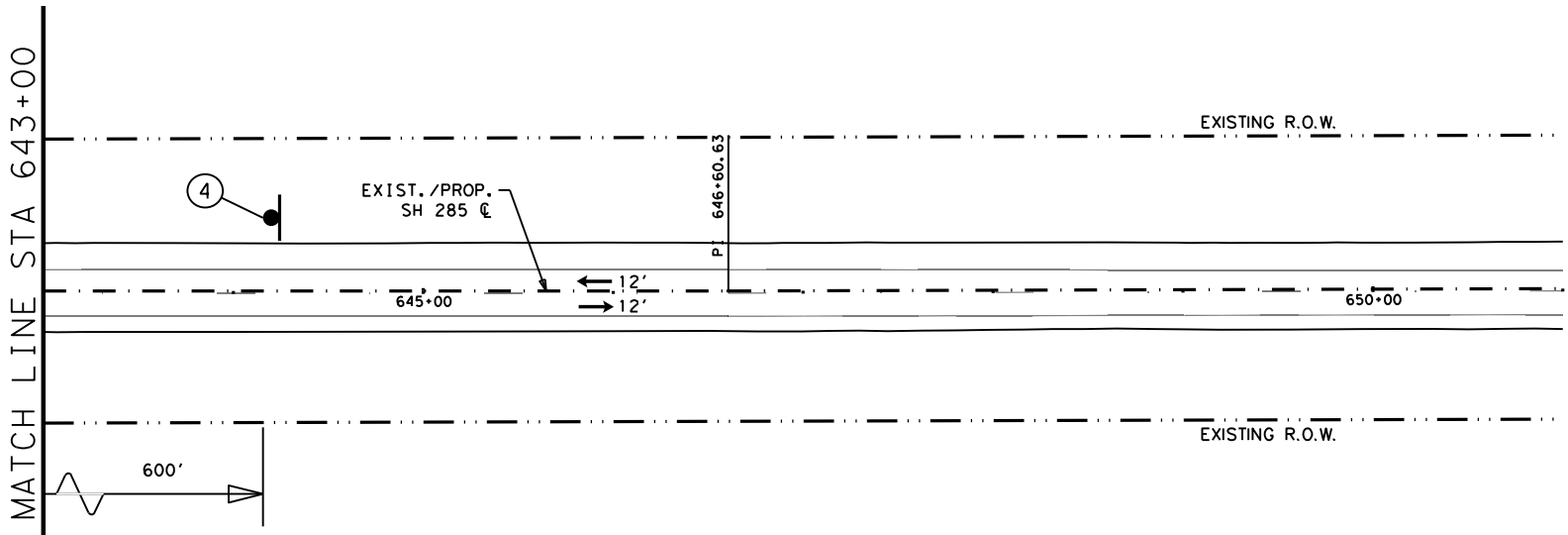
LEGEND

- CONSTRUCTION AREA
- TEMPORARY ROADWAY WIDENING
- PREVIOUSLY CONSTRUCTED
- CONCRETE TRAFFIC BARRIER (CTB)
- CRASH CUSHION ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- WORK ZONE PVMT MARK (REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) 6" WHITE BROKEN
- WORK ZONE PVMT MARK (REM) 6" YELLOW BROKEN
- WORK ZONE PVMT MARK (REM) 24" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" WHITE SOLID
- WORK ZONE PVMT MARK (NON-REM) 6" YELLOW SOLID
- WORK ZONE PVMT MARK (REM) (REFL) TY II-A-A
- PAV SURF PREP FOR MRK (6")
- WORK ZONE PVMT MARK (REM) (W) (8") (SLD)
- WORK ZONE PVMT MARK (REM) (W) (ARROW)
- WORK ZONE PVMT MARK (REM) (W) (WORD)
- DIRECTION OF TRAFFIC FLOW
- TYPE 3 BARRICADE
- CHANNELIZING DEVICES (PLASTIC DRUMS)
- REFER TO BC(2)-21 STANDARDS FOR SIGN SPACING
- CONSTRUCTION SIGN
- FLAG



NOTES:

1. ALL SIGNS SHOWN FOR CONSTRUCTION ARE SPACED AT MINIMUM AND MAY BE ADJUSTED DUE TO FIELD CONDITIONS.
2. EXIST. PAVEMENT MARKINGS TO BE REMOVED AS NECESSARY WITHIN LIMITS OF TRAFFIC CONTROL SEQUENCE.
3. REFER TO TXDOT BC AND TCP STANDARDS FOR SIGN SPACING
4. EXISTING SIGNS IN CONFLICT WITH PROP. CONSTRUCTION TCP PHASE SHALL BE COVERED, REMOVED, OR RELOCATED AS PER BC (4)-(6)-21 STANDARD.
5. FOR ADVANCED WARNING SIGNS SEE PLAN SHEETS 47 TO 49 .
6. FOR PLASTIC BARREL SPACING PLEASE REFER TO BC-21 STANDARDS.
7. AT THE COMPLETION OF PHASE IV, STRIPE WITH REFL (TY II) PAVEMENT MARKINGS (ITEM 666) IN THE PROPOSED/ FINAL LANE CONFIGURATION FROM STA. 477+40 TO STA. 620+00.
8. REMOVABLE PAVEMENT MARKINGS TO BE USED FOR TAPERS AND ALL AREAS OUTSIDE OF CONSTRUCTION LIMITS.
9. REFER TO DRIVEWAY TABLE AND CRASH CUSHION SUMMARY TABLE FOR MORE INFORMATION ON DRIVEWAY(S) NOT SHOWN ON THE LAYOUTS.



Arlene Crystal Garcia

03.07.2024

Pharr District Central Design



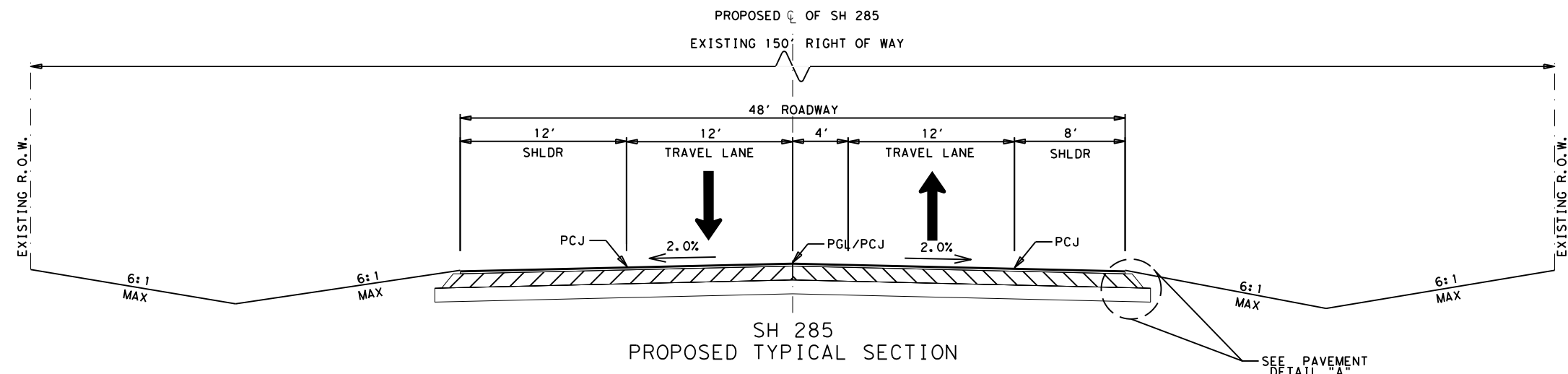
**SH 285
TCP
PHASE V STEP III**

SCALE: 1" = 100' SHEET 4 OF 4

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	139	

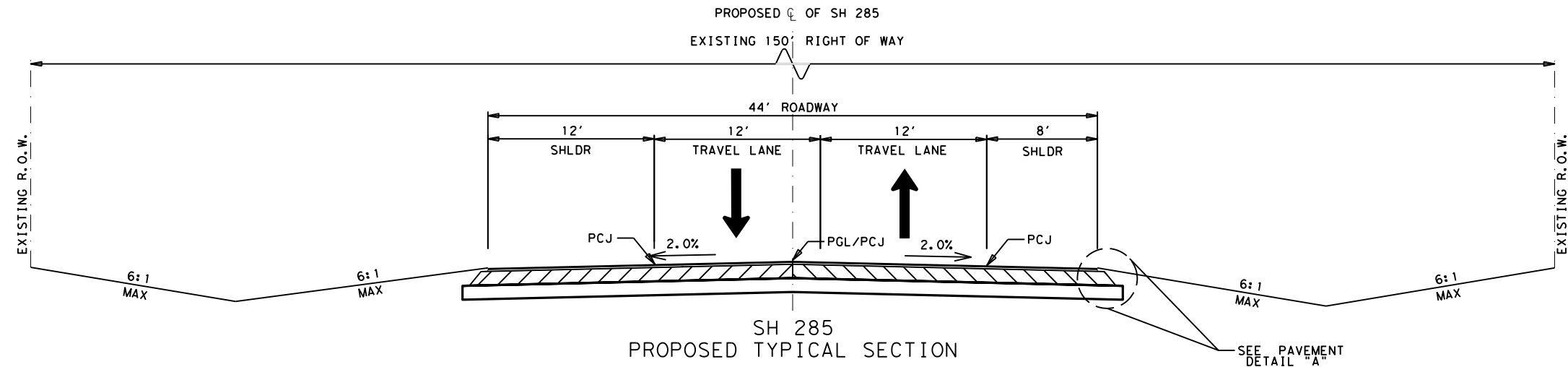
DATE: 3/4/2024 5:02:10 PM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2 - TCP\SH 285 TCP PHASE VI TYPICALS NEW.dgn

- LEGEND**
- ℄ - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER



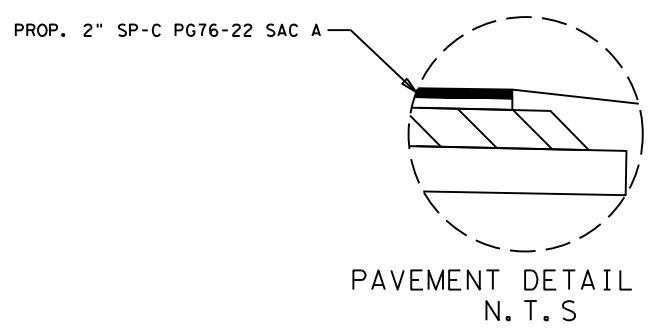
SH 285
 PROPOSED TYPICAL SECTION

STA 96+00 TO STA 196+25
 STA 196+25 TO STA 197+75 (48' TO 44' TRANSITION)
 205+00 TO 206+60
 STA 206+60 TO STA 215+60 (48' TO 68' TRANSITION)
 STA 272+90 TO STA 352+10
 STA 352+10 TO STA 361+10 (48' TO 68' TRANSITION)
 STA 444+80 TO STA 524+00
 STA 524+00 TO STA 533+00 (48' TO 68' TRANSITION)
 STA 616+70 TO STA 620+00 (48' TO 44' TRANSITION)



SH 285
 PROPOSED TYPICAL SECTION

STA 197+75 TO STA 199+95
 STA 201+30 TO STA 203+50
 STA 203+50 TO STA 205+00 (44' TO 48' TRANSITION)



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design

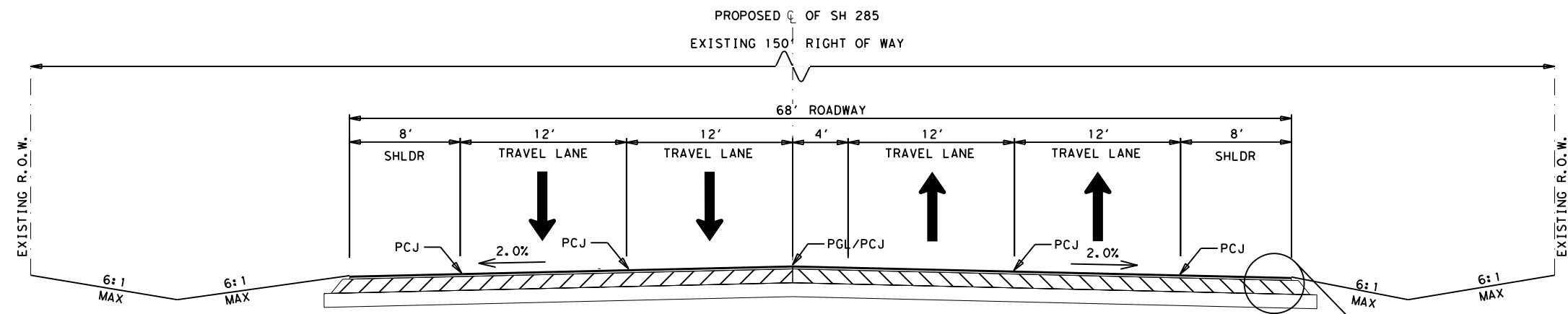


**SH 285
 TCP PHASE VI
 TYPICAL SECTIONS**

SCALE= 10"=1' SHEET 1 OF 2

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	140

DATE: 3/4/2024 5:02:11 PM
 FILE: pw:\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\2. TCP\SH 285 TCP PHASE VI\SH285 TCP PHASE VI TYPICALS NEW.dgn



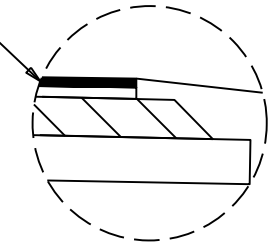
SH 285
 PROPOSED TYPICAL SECTION

STA 215+60 TO STA 263+90
 STA 263+90 TO STA 272+90 (68' TO 48' TRANSITION)
 STA 361+10 TO STA 435+80
 STA 435+80 TO STA 444+80 (68' TO 48' TRANSITION)
 STA 533+00 TO STA 607+70
 STA 607+70 TO STA 616+70 (68' TO 48' TRANSITION)

SEE PAVEMENT
 DETAIL "A"

- LEGEND**
- CL - CENTER LINE
 - PGL - PROFILE GRADE LINE
 - PCJ - PERMISSIBLE CONSTRUCTION JOINT
 - R.O.W. - RIGHT OF WAY
 - SHLDR - SHOULDER

PROP. 2" SP-C PG76-22 SAC A



PAVEMENT DETAIL "A"
 N.T.S



Arleene Crystal Garcia

03.07.2024

Pharr District Central Design



SH 285
 TCP PHASE VI
 TYPICAL SECTIONS

SCALE= 10":1' SHEET 2 OF 2

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	141

DATE: 2/12/2024 7:56:47 AM
 FILE: \\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\14 - Sign\285 Standard or as SHD\BC(1)-21.dgn
 No. warranty of any kind is made by TxDOT or any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this file to any other format or for the use of this file in any project.

BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES:

- The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction pavement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.
- The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.
- The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer.
- Geometric design of lane shifts and detours should, when possible, meet the applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.
- When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.
- The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.
- All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.
- The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.
- Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES, CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.
- Traffic control devices should be in place only while work is actually in progress or a definite need exists.
- The Engineer has the final decision on the location of all traffic control devices.
- Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

WORKER SAFETY NOTES:


- Workers on foot who are exposed to traffic or to construction equipment within the right-of-way shall wear high-visibility safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel," or equivalent revisions, and labeled as ANSI 107-2004 standard performance for Class 2 or 3 risk exposure. Class 3 garments should be considered for high traffic volume work areas or night time work.
- Except in emergency situations, flagger stations shall be illuminated when flagging is used at night.

COMPLIANT WORKZONE TRAFFIC CONTROL DEVICES

- Only pre-qualified products shall be used. The "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products and their sources.
- Work zone traffic control devices shall be compliant with the Manual for Assessing safety Hardware (MASH).

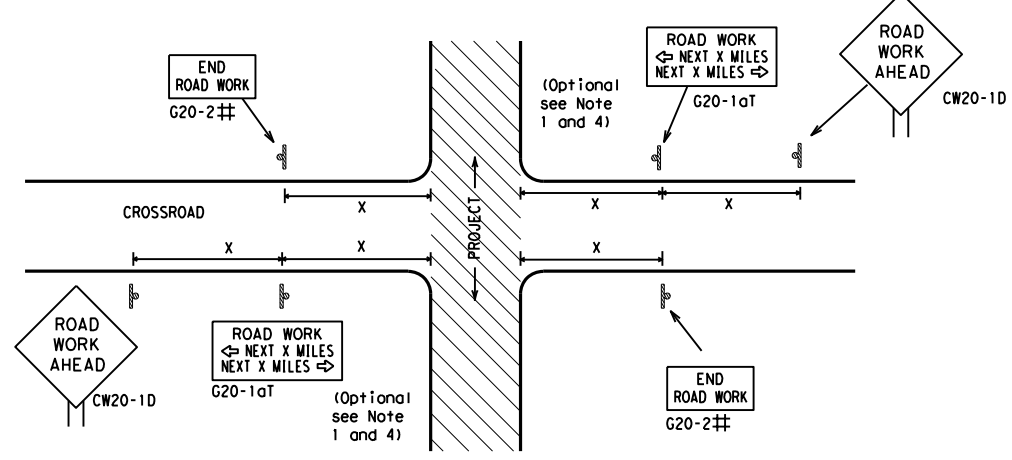
THE DOCUMENTS BELOW CAN BE FOUND ON-LINE AT http://www.txdot.gov
COMPLIANT WORK ZONE TRAFFIC CONTROL DEVICES LIST (CWZTCD)
DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS)
MATERIAL PRODUCER LIST (MPL)
ROADWAY DESIGN MANUAL - SEE "MANUALS (ONLINE MANUALS) "
STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS (SHSD)
TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD)
TRAFFIC ENGINEERING STANDARD SHEETS

SHEET 1 OF 12

 Texas Department of Transportation		Traffic Safety Division Standard	
BARRICADE AND CONSTRUCTION GENERAL NOTES AND REQUIREMENTS			
BC (1) -21			
FILE:	bc-21.dgn	DN:	TxDOT
© TxDOT	November 2002	CK:	TxDOT
		DW:	TxDOT
		CR:	TxDOT
REVISIONS	CONT	SECT	JOB
4-03 7-13	0482	01	036, etc.
9-07 8-14			SH 285
5-10 5-21	DIST	COUNTY	SHEET NO.
	PHR	JIM HOGG	142

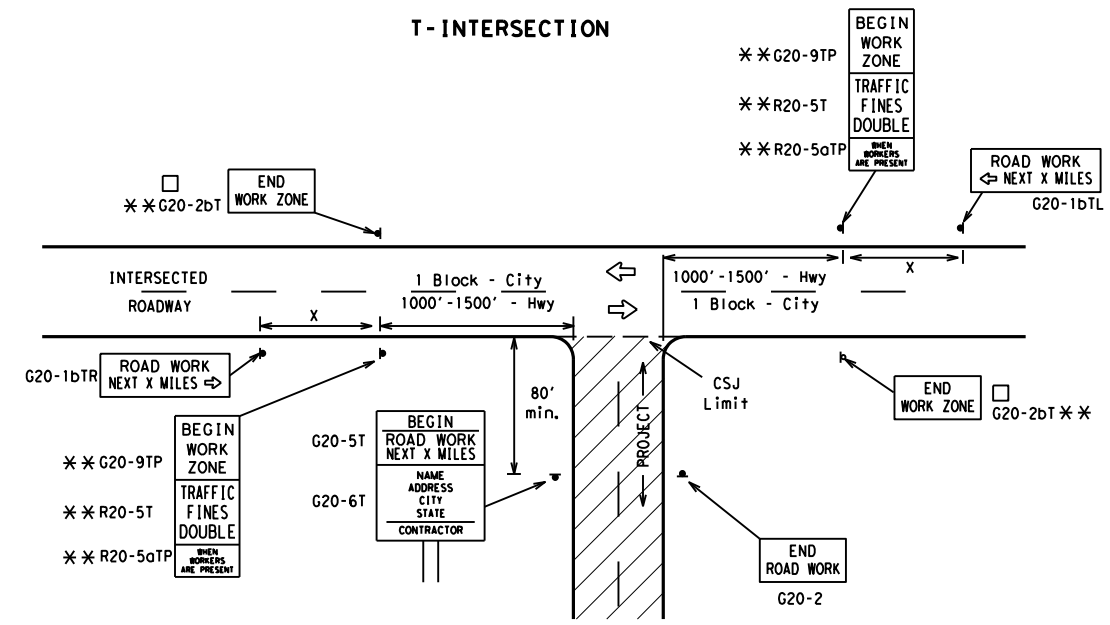
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of any information contained herein. This standard is for informational purposes only.

TYPICAL LOCATION OF CROSSROAD SIGNS



- ## May be mounted on back of "ROAD WORK AHEAD" (CW20-1D) sign with approval of Engineer. (See note 2 below)
- The typical minimum signing on a crossroad approach should be a "ROAD WORK AHEAD" (CW20-1D) sign and a (G20-2) "END ROAD WORK" sign, unless noted otherwise in plans.
 - The Engineer may use the reduced size 36" x 36" ROAD WORK AHEAD (CW20-1D) sign mounted back to back with the reduced size 36" x 18" "END ROAD WORK" (G20-2) sign on low volume crossroads (see Note 4 under "Typical Construction Warning Sign Size and Spacing"). See the "Standard Highway Sign Designs for Texas" manual for sign details. The Engineer may omit the advance warning signs on low volume crossroads. The Engineer will determine whether a road is low volume as per TMUTCD Part 5. This information shall be shown in the plans.
 - Based on existing field conditions, the Engineer/Inspector may require additional signs such as FLAGGER AHEAD, LOOSE GRAVEL, or other appropriate signs. When additional signs are required, these signs will be considered part of the minimum requirements. The Engineer/Inspector will determine the proper location and spacing of any sign not shown on the BC sheets, Traffic Control Plan sheets or the Work Zone Standard Sheets.
 - The "ROAD WORK NEXT X MILES" (G20-1aT) sign shall be required at high volume crossroads to advise motorists of the length of construction in either direction from the intersection. The Engineer will determine whether a roadway is considered high volume.
 - Additional traffic control devices may be shown elsewhere in the plans for higher volume crossroads.
 - When work occurs in the intersection area, appropriate traffic control devices, as shown elsewhere in the plans or as determined by the Engineer/Inspector, shall be in place.

T-INTERSECTION



CSJ LIMITS AT T-INTERSECTION

- The Engineer will determine the types and location of any additional traffic control devices, such as a flagger and accompanying signs, or other signs, that should be used when work is being performed at or near an intersection.
- If construction closes the road at a T-intersection, the Contractor shall place the "CONTRACTOR NAME" (G20-6T) sign behind the Type 3 Barricades for the road closure (see BC(10) also). The "ROAD WORK NEXT X MILES" left arrow (G20-1bTL) and "ROAD WORK NEXT X MILES" right arrow (G20-1bTR) signs shall be replaced by the detour signing called for in the plans.

TYPICAL CONSTRUCTION WARNING SIGN SIZE AND SPACING^{1,5,6}

Sign Number or Series	SIZE		SPACING	
	Conventional Road	Expressway/Freeway	Posted Speed MPH	Sign Δ Spacing "x" Feet (Apprx.)
CW20 ⁴	48" x 48"	48" x 48"	30	120
CW21			35	160
CW22			40	240
CW23			45	320
CW25			50	400
CW1, CW2, CW7, CW8, CW9, CW11, CW14	36" x 36"	48" x 48"	55	500 ²
CW3, CW4, CW5, CW6, CW8-3, CW10, CW12	48" x 48"	48" x 48"	60	600 ²
			65	700 ²
			70	800 ²
			75	900 ²
			80	1000 ²
			*	* ³

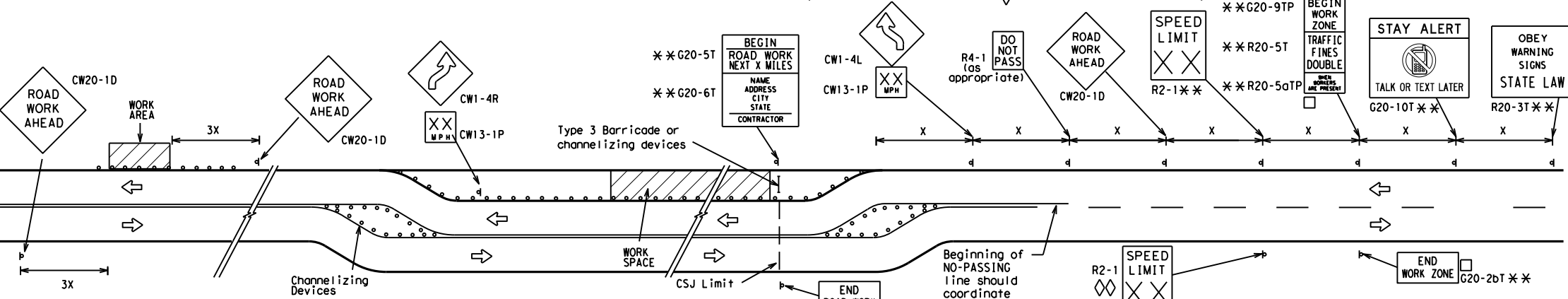
* For typical sign spacings on divided highways, expressways and freeways, see Part 6 of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) typical application diagrams or TCP Standard Sheets.

Δ Minimum distance from work area to first Advance Warning sign nearest the work area and/or distance between each additional sign.

GENERAL NOTES

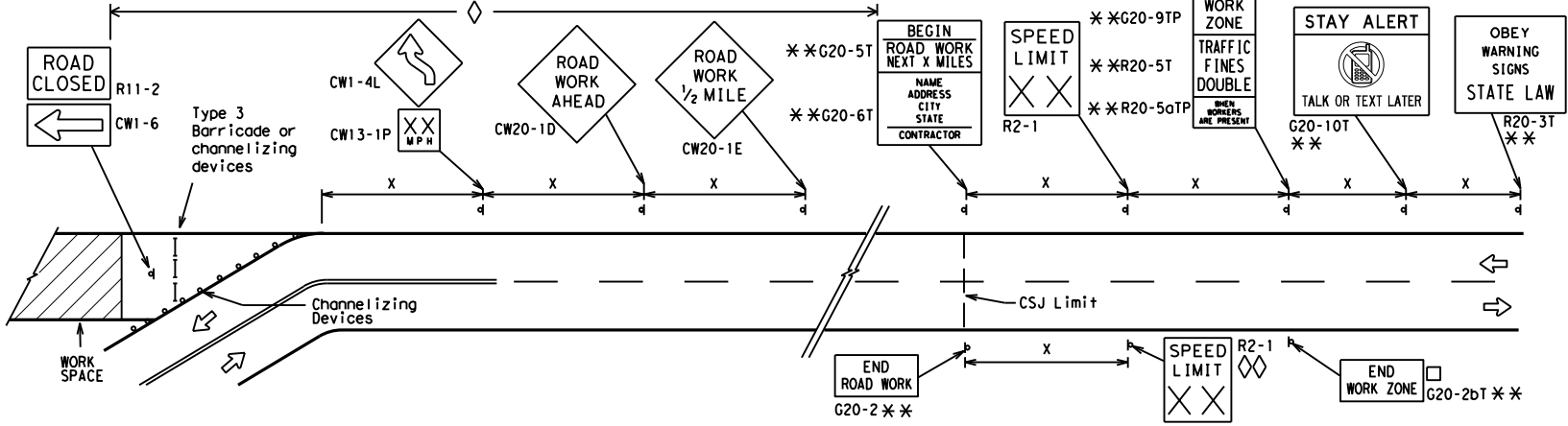
- Special or larger size signs may be used as necessary.
- Distance between signs should be increased as required to have 1500 feet advance warning.
- Distance between signs should be increased as required to have 1/2 mile or more advance warning.
- 36" x 36" "ROAD WORK AHEAD" (CW20-1D) signs may be used on low volume crossroads at the discretion of the Engineer as per TMUTCD Part 5. See Note 2 under "Typical Location of Crossroad Signs".
- Only diamond shaped warning sign sizes are indicated.
- See sign size listing in "TMUTCD", Sign Appendix or the "Standard Highway Sign Designs for Texas" manual for complete list of available sign design sizes.

WORK AREAS IN MULTIPLE LOCATIONS WITHIN CSJ LIMITS

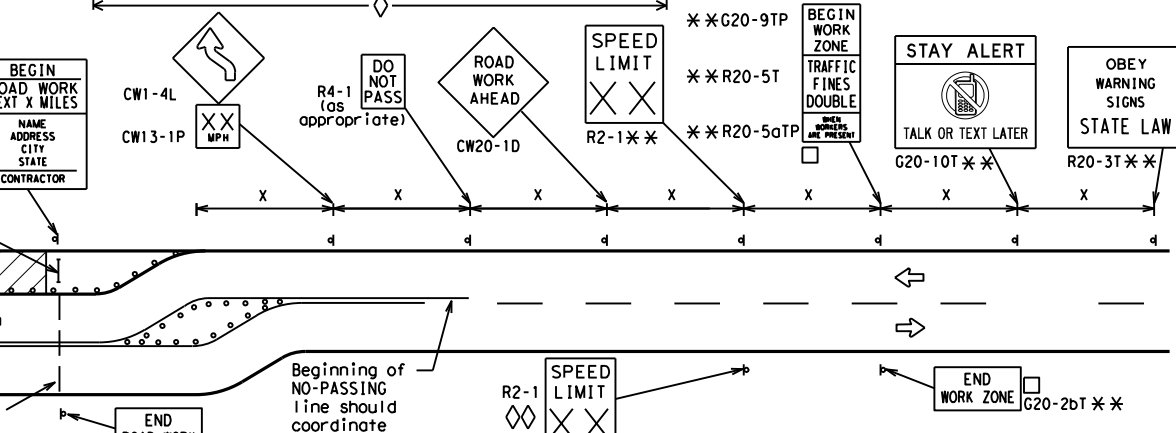


When extended distances occur between minimal work spaces, the Engineer/Inspector should ensure additional "ROAD WORK AHEAD" (CW20-1D) signs are placed in advance of these work areas to remind drivers they are still within the project limits. See the applicable TCP sheets for exact location and spacing of signs and channelizing devices.

SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING DOWNSTREAM OF THE CSJ LIMITS



SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING AT THE CSJ LIMITS



NOTES

- The Contractor shall determine the appropriate distance to be placed on the G20-1 series signs and "BEGIN ROAD WORK NEXT X MILES" (G20-5T) sign for each specific project. This distance shall replace the "x" and shall be rounded to the nearest whole mile with the approval of the Engineer. No decimals shall be used.
- The "BEGIN WORK ZONE" (G20-9TP) and "END WORK ZONE" (G20-2bT) shall be used as shown on the sample layout when advance signs are required outside the CSJ Limits. They inform the motorist of entering or leaving a part of the work zone lying outside the CSJ Limits where traffic fines may double if workers are present.
 - CSJ limit signing is required for highway construction and maintenance work, with the exception of mobile operations.
 - Area for placement of "ROAD WORK AHEAD" (CW20-1D) sign and other signs or devices as called for on the Traffic Control Plan.
 - Contractor will install a regulatory speed limit sign at the end of the work zone.

LEGEND

—	Type 3 Barricade
○ ○ ○	Channelizing Devices
■	Sign
X	See Typical Construction Warning Sign Size and Spacing chart or the TMUTCD for sign spacing requirements.

SHEET 2 OF 12



BARRICADE AND CONSTRUCTION PROJECT LIMIT

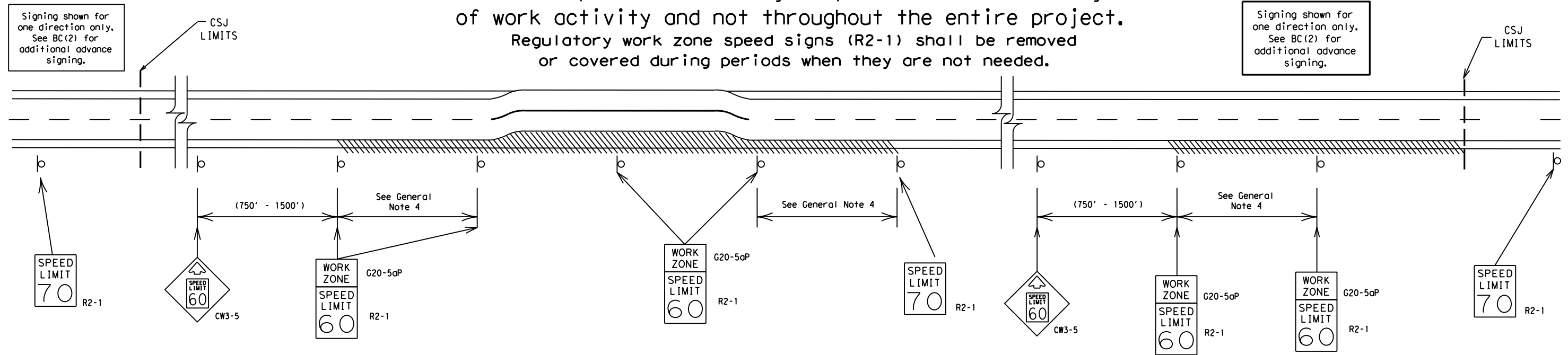
BC(2)-21

FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CR: TxDOT
© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
9-07 8-14	DIST	COUNTY		SHEET NO.
7-13 5-21	PHR	JIM HOGG		143

TYPICAL APPLICATION OF WORK ZONE SPEED LIMIT SIGNS

Work zone speed limits shall be regulatory, established in accordance with the "Procedures for Establishing Speed Zones," and approved by the Texas Transportation Commission, or by City Ordinance when within Incorporated City Limits.

Reduced speeds should only be posted in the vicinity of work activity and not throughout the entire project. Regulatory work zone speed signs (R2-1) shall be removed or covered during periods when they are not needed.



GUIDANCE FOR USE:

LONG/INTERMEDIATE TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit should be included on the design of the traffic control plans when restricted geometrics with a lower design speed are present in the work zone and modification of the geometrics to a higher design speed is not feasible.

Long/Intermediate Term Work Zone Speed Limit signs, when approved as described above, should be posted and visible to the motorist when work activity is present. Work activity may also be defined as a change in the roadway that requires a reduced speed for motorists to safely negotiate the work area, including:

- rough road or damaged pavement surface
- substantial alteration of roadway geometrics (diversions)
- construction detours
- grade
- width
- other conditions readily apparent to the driver

As long as any of these conditions exist, the work zone speed limit signs should remain in place.

SHORT TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit may be included on the design of the traffic control plans when workers or equipment are not behind concrete barrier, when work activity is within 10 feet of the traveled way or actually in the traveled way.

Short Term Work Zone Speed Limit signs should be posted and visible to the motorists only when work activity is present. When work activity is not present, signs shall be removed or covered. (See Removing or Covering on BC(4)).

GENERAL NOTES

- Regulatory work zone speed limits should be used only for sections of construction projects where speed control is of major importance.
- Regulatory work zone speed limit signs shall be placed on supports at a 7 foot minimum mounting height.
- Speed zone signs are illustrated for one direction of travel and are normally posted for each direction of travel.
- Frequency of work zone speed limit signs should be:

40 mph and greater	0.2 to 2 miles
35 mph and less	0.2 to 1 mile
- Regulatory speed limit signs shall have black legend and border on a white reflective background (See "Reflective Sheeting" on BC(4)).
- Fabrication, erection and maintenance of the "ADVANCE SPEED LIMIT" (CW3-5) sign, "WORK ZONE" (G20-5aP) plaque and the "SPEED LIMIT" (R2-1) signs shall not be paid for directly, but shall be considered subsidiary to Item 502.
- Turning signs from view, laying signs over or down will not be allowed, unless as otherwise noted under "REMOVING OR COVERING" on BC(4).
- Techniques that may help reduce traffic speeds include but are not limited to:
 - Law enforcement.
 - Flagger stationed next to sign.
 - Portable changeable message sign (PCMS).
 - Low-power (drone) radar transmitter.
 - Speed monitor trailers or signs.
- Speeds shown on details above are for illustration only. Work Zone Speed Limits should only be posted as approved for each project.
- For more specific guidance concerning the type of work, work zone conditions and factors impacting allowable regulatory construction speed zone reduction see TxDOT form #1204 in the TxDOT e-form system.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein. The user of this standard shall be responsible for the accuracy of the information contained herein.

DATE: 2/12/2024 7:56:49 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT15\Documents\21 - PHR\Design Project\210616\01 - PHR\BARRICADE AND CONSTRUCTION WORK ZONE SPEED LIMIT.dgn

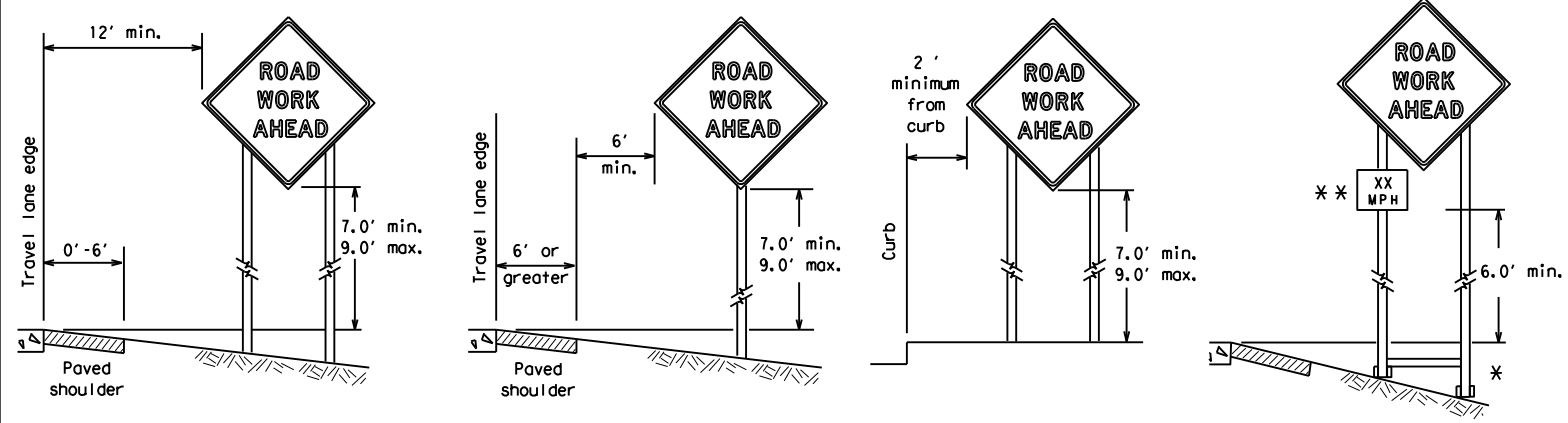
SHEET 3 OF 12

		Traffic Safety Division Standard	
<h2>BARRICADE AND CONSTRUCTION WORK ZONE SPEED LIMIT</h2>			
<h3>BC (3) - 21</h3>			
FILE:	bc-21.dgn	DW:	TxDOT
© TxDOT	November 2002	CONT SECT	JOB HIGHWAY
REVISIONS		0482 01	036, etc. SH 285
9-07	8-14	DIST	COUNTY SHEET NO.
7-13	5-21	PHR	JIM HOGG 144

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 7:56:50 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standards\bc-21.dgn

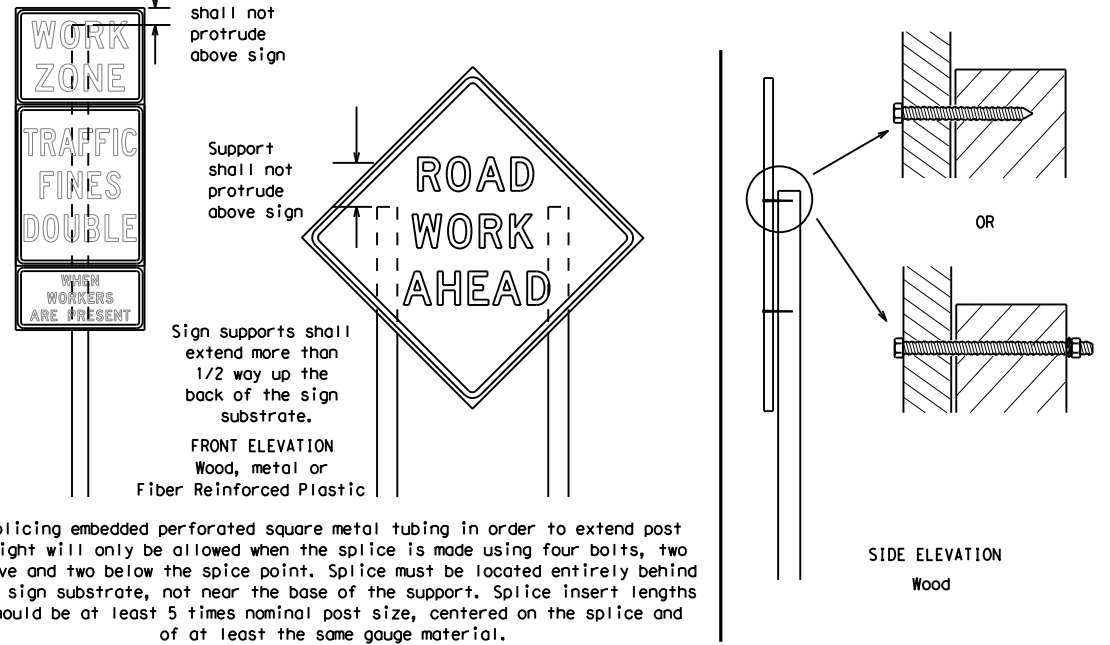
TYPICAL MINIMUM CLEARANCES FOR LONG TERM AND INTERMEDIATE TERM SIGNS



* When placing skid supports on unlevel ground, the leg post lengths must be adjusted so the sign appears straight and plumb. Objects shall NOT be placed under skids as a means of leveling.

** When plaques are placed on dual-leg supports, they should be attached to the upright nearest the travel lane. Supplemental plaques (advisory or distance) should not cover the surface of the parent sign.

ATTACHMENT FOR SIGN SUPPORTS



Splicing embedded perforated square metal tubing in order to extend post height will only be allowed when the splice is made using four bolts, two above and two below the splice point. Splice must be located entirely behind the sign substrate, not near the base of the support. Splice insert lengths should be at least 5 times nominal post size, centered on the splice and of at least the same gauge material.

GENERAL NOTES FOR WORK ZONE SIGNS

- Contractor shall install and maintain signs in a straight and plumb condition and/or as directed by the Engineer.
- Wooden sign posts shall be painted white.
- Barricades shall NOT be used as sign supports.
- All signs shall be installed in accordance with the plans or as directed by the Engineer. Signs shall be used to regulate, warn, and guide the traveling public safely through the work zone.
- The Contractor may furnish either the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD). The Engineer/Inspector may require the Contractor to furnish other work zone signs that are shown in the TMUTCD but may have been omitted from the plans. Any variation in the plans shall be documented by written agreement between the Engineer and the Contractor's Responsible Person. All changes must be documented in writing before being implemented. This can include documenting the changes in the Inspector's TxDOT diary and having both the Inspector and Contractor initial and date the agreed upon changes.
- The Contractor shall furnish sign supports listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCD) for small roadside signs. Supports for temporary large roadside signs shall meet the requirements detailed on the Temporary Large Roadside Signs (TLRS) standard sheets. The Contractor shall install the sign support in accordance with the manufacturer's recommendations. If there is a question regarding installation procedures, the Contractor shall furnish the Engineer a copy of the manufacturer's installation recommendations so the Engineer can verify the correct procedures are being followed.
- The Contractor is responsible for installing signs on approved supports and replacing signs with damaged or cracked substrates and/or damaged or marred reflective sheeting as directed by the Engineer/Inspector.
- Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1 inch.
- The Contractor shall replace damaged wood posts. New or damaged wood sign posts shall not be spliced.

DURATION OF WORK (as defined by the "Texas Manual on Uniform Traffic Control Devices" Part 6)

- The types of sign supports, sign mounting height, the size of signs, and the type of sign substrates can vary based on the type of work being performed. The Engineer is responsible for selecting the appropriate size sign for the type of work being performed. The Contractor is responsible for ensuring the sign support, sign mounting height and substrate meets manufacturer's recommendations in regard to crashworthiness and duration of work requirements.
 - Long-term stationary - work that occupies a location more than 3 days.
 - Intermediate-term stationary - work that occupies a location more than one daylight period up to 3 days, or nighttime work lasting more than one hour.
 - Short-term stationary - daytime work that occupies a location for more than 1 hour in a single daylight period.
 - Short, duration - work that occupies a location up to 1 hour.
 - Mobile - work that moves continuously or intermittently (stopping for up to approximately 15 minutes.)

SIGN MOUNTING HEIGHT

- The bottom of Long-term/Intermediate-term signs shall be at least 7 feet, but not more than 9 feet, above the paved surface, except as shown for supplemental plaques mounted below other signs.
- The bottom of Short-term/Short Duration signs shall be a minimum of 1 foot above the pavement surface but no more than 2 feet above the ground.
- Long-term/Intermediate-term Signs may be used in lieu of Short-term/Short Duration signing.
- Short-term/Short Duration signs shall be used only during daylight and shall be removed at the end of the workday or raised to appropriate Long-term/Intermediate sign height.
- Regulatory signs shall be mounted at least 7 feet, but not more than 9 feet, above the paved surface regardless of work duration.

SIZE OF SIGNS

- The Contractor shall furnish the sign sizes shown on BC (2) unless otherwise shown in the plans or as directed by the Engineer.

SIGN SUBSTRATES

- The Contractor shall ensure the sign substrate is installed in accordance with the manufacturer's recommendations for the type of sign support that is being used. The CWZTCD lists each substrate that can be used on the different types and models of sign supports.
- "Mesh" type materials are NOT an approved sign substrate, regardless of the tightness of the weave.
- All wooden individual sign panels fabricated from 2 or more pieces shall have one or more plywood cleat, 1/2" thick by 6" wide, fastened to the back of the sign and extending fully across the sign. The cleat shall be attached to the back of the sign using wood screws that do not penetrate the face of the sign panel. The screws shall be placed on both sides of the splice and spaced at 6" centers. The Engineer may approve other methods of splicing the sign face.

REFLECTIVE SHEETING

- All signs shall be retroreflective and constructed of sheeting meeting the color and retro-reflectivity requirements of DMS-8300 for rigid signs or DMS-8310 for roll-up signs. The web address for DMS specifications is shown on BC(1).
- White sheeting, meeting the requirements of DMS-8300 Type A, shall be used for signs with a white background.
- Orange sheeting, meeting the requirements of DMS-8300 Type B_{FL} or Type C_{FL}, shall be used for rigid signs with orange backgrounds.

SIGN LETTERS

- All sign letters and numbers shall be clear, and open rounded type uppercase alphabet letters as approved by the Federal Highway Administration (FHWA) and as published in the "Standard Highway Sign Design for Texas" manual. Signs, letters and numbers shall be of first class workmanship in accordance with Department Standards and Specifications.

REMOVING OR COVERING

- When sign messages may be confusing or do not apply, the signs shall be removed or completely covered.
- Long-term stationary or intermediate stationary signs installed on square metal tubing may be turned away from traffic 90 degrees when the sign message is not applicable. This technique may not be used for signs installed in the median of divided highways or near any intersections where the sign may be seen from approaching traffic.
- Signs installed on wooden skids shall not be turned at 90 degree angles to the roadway. These signs should be removed or completely covered when not required.
- When signs are covered, the material used shall be opaque, such as heavy mil black plastic, or other materials which will cover the entire sign face and maintain their opaque properties under automobile headlights at night, without damaging the sign sheeting.
- Burlap shall NOT be used to cover signs.
- Duct tape or other adhesive material shall NOT be affixed to a sign face.
- Signs and anchor stubs shall be removed and holes backfilled upon completion of work.

SIGN SUPPORT WEIGHTS

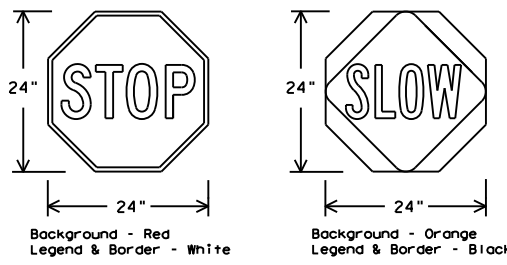
- Where sign supports require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand should be used.
- The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
- Rock, concrete, iron, steel or other solid objects shall not be permitted for use as sign support weights.
- Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
- Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall NOT be used.
- Rubber ballasts designed for channelizing devices should not be used for ballast on portable sign supports. Sign supports designed and manufactured with rubber bases may be used when shown on the CWZTCD list.
- Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign support.
- Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

FLAGS ON SIGNS

- Flags may be used to draw attention to warning signs. When used, the flag shall be 16 inches square or larger and shall be orange or fluorescent red-orange in color. Flags shall not be allowed to cover any portion of the sign face.

STOP/SLOW PADDLES

- STOP/SLOW paddles are the primary method to control traffic by flaggers. The STOP/SLOW paddle size should be 24" x 24".
- STOP/SLOW paddles shall be retroreflectORIZED when used at night.
- STOP/SLOW paddles may be attached to a staff with a minimum length of 6' to the bottom of the sign.
- Any lights incorporated into the STOP or SLOW paddle faces shall only be as specifically described in Section 6E.03 Hand Signaling Devices in the TMUTCD.



SHEETING REQUIREMENTS (WHEN USED AT NIGHT)		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	ORANGE	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND & BORDER	WHITE	TYPE B OR C SHEETING
LEGEND & BORDER	BLACK	ACRYLIC NON-REFLECTIVE FILM

CONTRACTOR REQUIREMENTS FOR MAINTAINING PERMANENT SIGNS WITHIN THE PROJECT LIMITS

- Permanent signs are used to give notice of traffic laws or regulations, call attention to conditions that are potentially hazardous to traffic operations, show route designations, destinations, directions, distances, services, points of interest, and other geographical, recreational, specific service (LOGO), or cultural information. Drivers proceeding through a work zone need the same, if not better route guidance as normally installed on a roadway without construction.
- When permanent regulatory or warning signs conflict with work zone conditions, remove or cover the permanent signs until the permanent sign message matches the roadway condition. For details for covering large guide signs see the TS-CD standard.
- When existing permanent signs are moved and relocated due to construction purposes, they shall be visible to motorists at all times.
- If existing signs are to be relocated on their original supports, they shall be installed on crashworthy bases as shown on the SMD Standard sheets. The signs shall meet the required mounting heights shown on the BC Sheets or the SMD Standards. This work should be paid for under the appropriate pay item for relocating existing signs.
- If permanent signs are to be removed and relocated using temporary supports, the Contractor shall use crashworthy supports as shown on the BC standard sheets, TLRS standard sheets or the CWZTCD list. The signs shall meet the required mounting heights shown on the BC, or the SMD standard sheets during construction. This work should be paid for under the appropriate pay item for relocating existing signs.
- Any sign or traffic control device that is struck or damaged by the Contractor or his/her construction equipment shall be replaced as soon as possible by the Contractor to ensure proper guidance for the motorists. This will be subsidiary to Item 502.

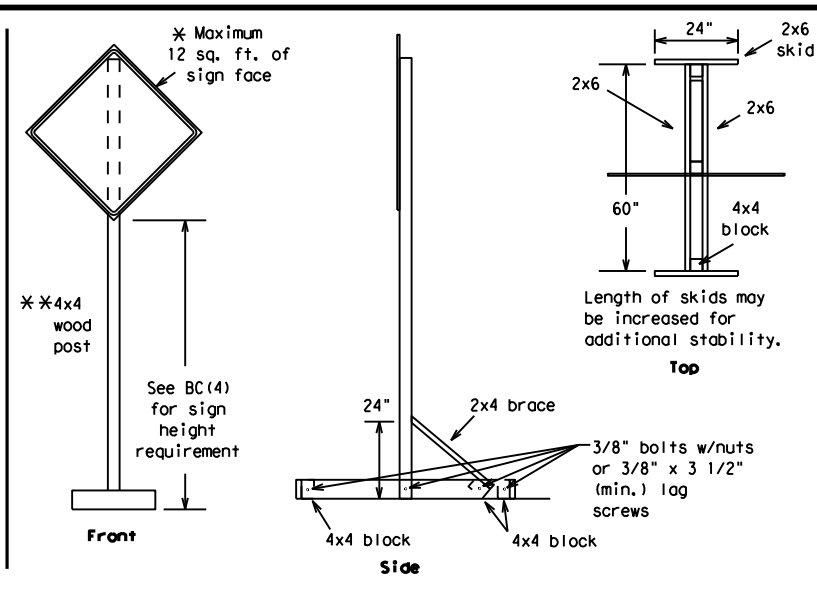
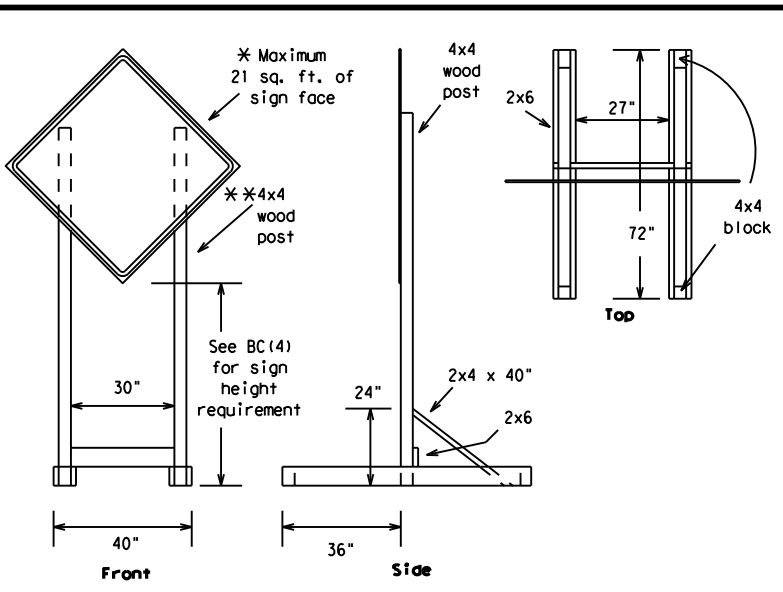


BARRICADE AND CONSTRUCTION TEMPORARY SIGN NOTES

BC (4) - 21

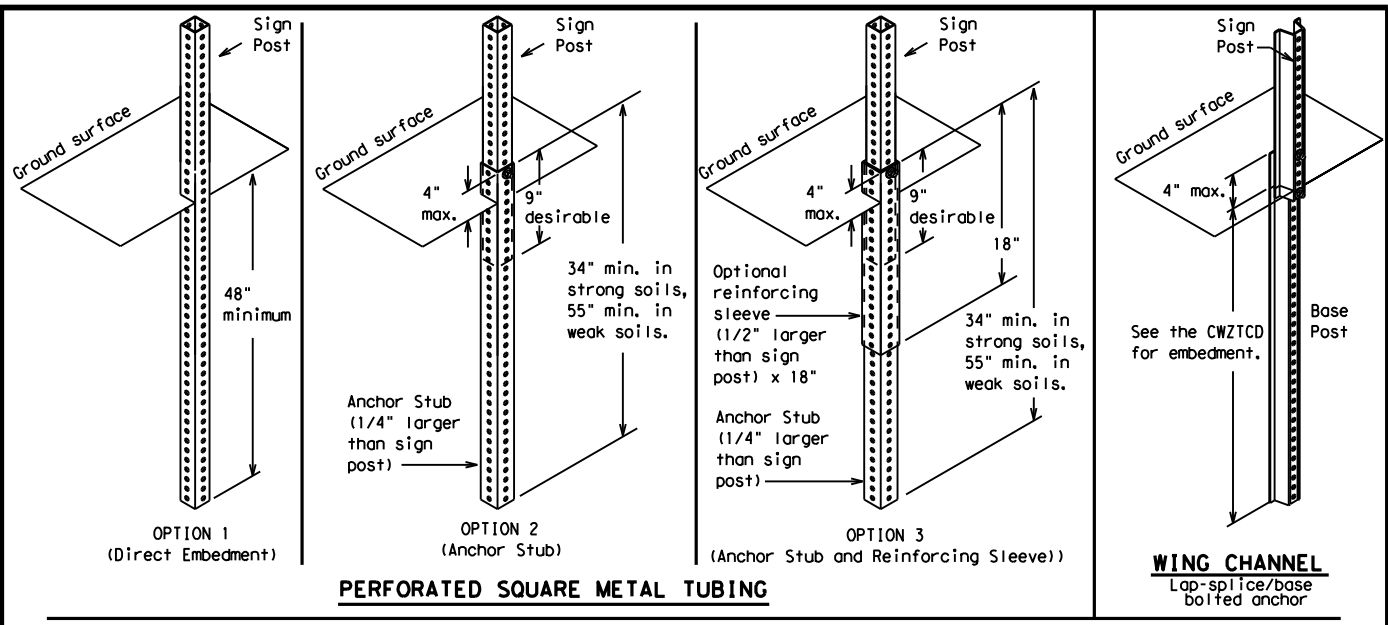
FILE:	bc-21.dgn	DN:	TxDOT	CR:	TxDOT	OW:	TxDOT	CK:	TxDOT
© TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		0482	01	036, etc.	SH 285				
9-07	8-14	DIST	COUNTY	SHEET NO.					
7-13	5-21	PHR	JIM HOGG	145					

DATE: 2/12/2024 7:56:51 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standard\SH 285 Standard.dgn
 No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results resulting from its use.
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results resulting from its use.
 PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standard\SH 285 Standard.dgn



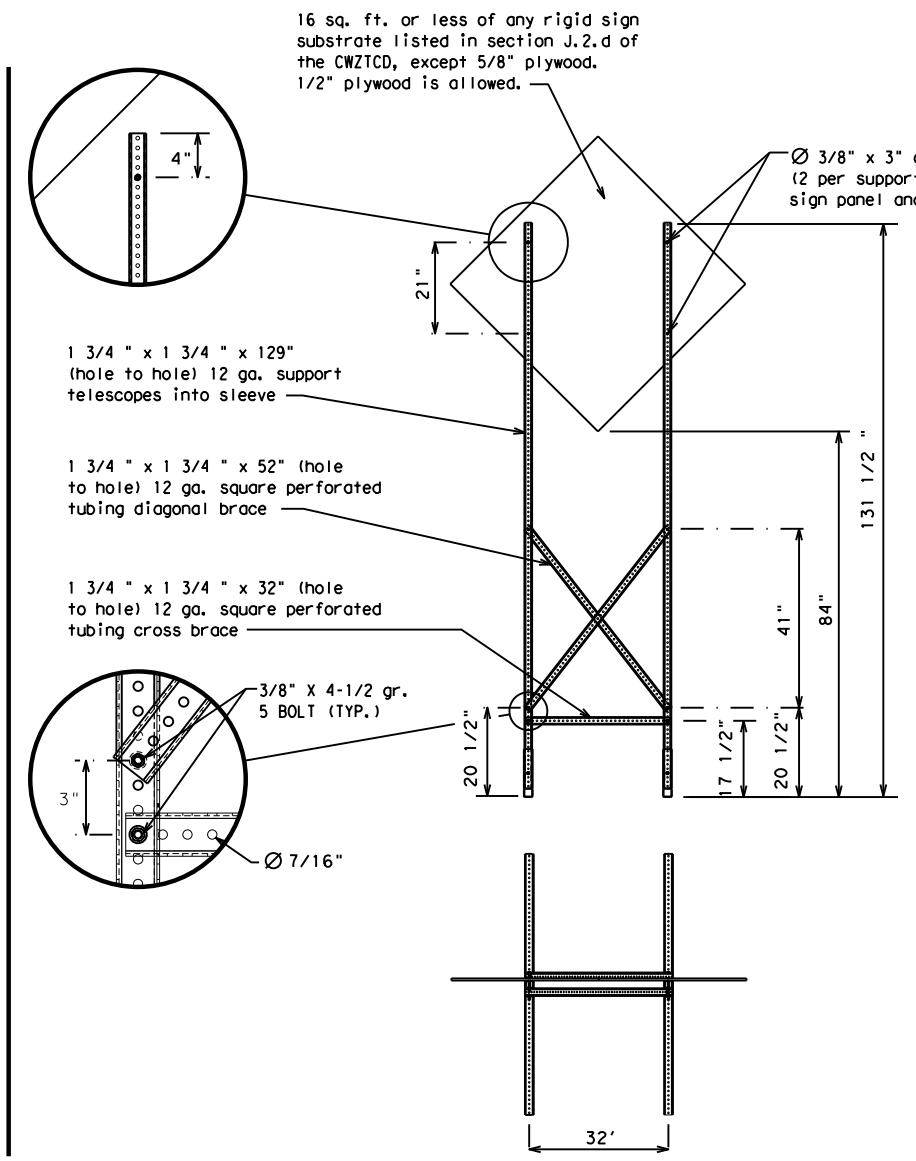
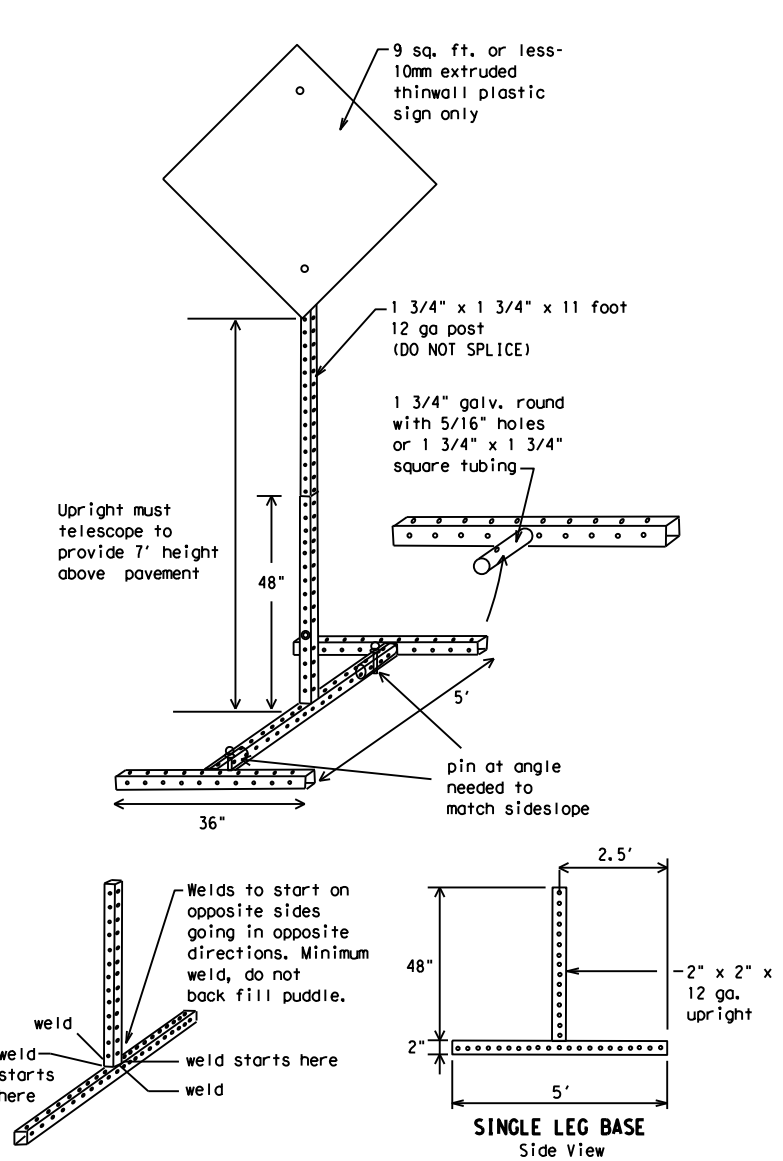
SKID MOUNTED WOOD SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS



GROUND MOUNTED SIGN SUPPORTS

Refer to the CWZTCD and the manufacturer's installation procedure for each type sign support. The maximum sign square footage shall adhere to the manufacturer's recommendation. Two post installations can be used for larger signs.



SKID MOUNTED PERFORATED SQUARE STEEL TUBING SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS

WEDGE ANCHORS
 Both steel and plastic Wedge Anchor Systems as shown on the SMD Standard Sheets may be used as temporary sign supports for signs up to 10 square feet of sign face. They may be set in concrete or in sturdy soils if approved by the Engineer. (See web address for "Traffic Engineering Standard Sheets" on BC(1)).

OTHER DESIGNS
 MORE DETAILS OF APPROVED LONG/INTERMEDIATE AND SHORT TERM SUPPORTS CAN BE FOUND ON THE CWZTCD LIST. SEE BC(1) FOR WEBSITE LOCATION.

- GENERAL NOTES**
- Nails may be used in the assembly of wooden sign supports, but 3/8" bolts with nuts or 3/8" x 3 1/2" lag screws must be used on every joint for final connection.
 - No more than 2 sign posts shall be placed within a 7 ft. circle, except for specific materials noted on the CWZTCD List.
 - When project is completed, all sign supports and foundations shall be removed from the project site. This will be considered subsidiary to Item 502.
- * See BC(4) for definition of "Work Duration."
 ** Wood sign posts MUST be one piece. Splicing will NOT be allowed. Posts shall be painted white.
 See the CWZTCD for the type of sign substrate that can be used for each approved sign support.

BARRICADE AND CONSTRUCTION TYPICAL SIGN SUPPORT
BC(5) - 21

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CK:	TxDOT
© TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS	0482 01	036, etc.		SH 285					
9-07	8-14	DIST	COUNTY	SHEET NO.					
7-13	5-21	PHR	JIM HOGG	146					

WHEN NOT IN USE, REMOVE THE PCMS FROM THE RIGHT-OF-WAY OR PLACE THE PCMS BEHIND BARRIER OR GUARDRAIL WITH SIGN PANEL TURNED PARALLEL TO TRAFFIC

RECOMMENDED PHASES AND FORMATS FOR PCMS MESSAGES DURING ROADWORK ACTIVITIES

(The Engineer may approve other messages not specifically covered here.)

PORTABLE CHANGEABLE MESSAGE SIGNS

- The Engineer/Inspector shall approve all messages used on portable changeable message signs (PCMS).
- Messages on PCMS should contain no more than 8 words (about four to eight characters per word), not including simple words such as "TO," "FOR," "AT," etc.
- Messages should consist of a single phase, or two phases that alternate. Three-phase messages are not allowed. Each phase of the message should convey a single thought, and must be understood by itself.
- Use the word "EXIT" to refer to an exit ramp on a freeway; i.e., "EXIT CLOSED." Do not use the term "RAMP."
- Always use the route or interstate designation (IH, US, SH, FM) along with the number when referring to a roadway.
- When in use, the bottom of a stationary PCMS message panel should be a minimum 7 feet above the roadway, where possible.
- The message term "WEEKEND" should be used only if the work is to start on Saturday morning and end by Sunday evening at midnight. Actual days and hours of work should be displayed on the PCMS if work is to begin on Friday evening and/or continue into Monday morning.
- The Engineer/Inspector may select one of two options which are available for displaying a two-phase message on a PCMS. Each phase may be displayed for either four seconds each or for three seconds each.
- Do not "flash" messages or words included in a message. The message should be steady burn or continuous while displayed.
- Do not present redundant information on a two-phase message; i.e., keeping two lines of the message the same and changing the third line.
- Do not use the word "Danger" in message.
- Do not display the message "LANES SHIFT LEFT" or "LANES SHIFT RIGHT" on a PCMS. Drivers do not understand the message.
- Do not display messages that scroll horizontally or vertically across the face of the sign.
- The following table lists abbreviated words and two-word phrases that are acceptable for use on a PCMS. Both words in a phrase must be displayed together. Words or phrases not on this list should not be abbreviated, unless shown in the TMUTCD.
- PCMS character height should be at least 18 inches for trailer mounted units. They should be visible from at least 1/2 (.5) mile and the text should be legible from at least 600 feet at night and 800 feet in daylight. Truck mounted units must have a character height of 10 inches and must be legible from at least 400 feet.
- Each line of text should be centered on the message board rather than left or right justified.
- If disabled, the PCMS should default to an illegible display that will not alarm motorists and will only be used to alert workers that the PCMS has malfunctioned. A pattern such as a series of horizontal solid bars is appropriate.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act." No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results obtained from its use. STANDARD 500-21.dgn

WORD OR PHRASE	ABBREVIATION	WORD OR PHRASE	ABBREVIATION
Access Road	ACCS RD	Major	MAJ
Alternate	ALT	Miles	MI
Avenue	AVE	Miles Per Hour	MPH
Best Route	BEST RTE	Minor	MNR
Boulevard	BLVD	Monday	MON
Bridge	BRDG	Normal	NORM
Cannot	CANT	North	N
Center	CTR	Northbound	(route) N
Construction Ahead	CONST AHD	Parking	PKING
CROSSING	XING	Road	RD
Detour Route	DETOUR RTE	Right Lane	RT LN
Do Not	DONT	Saturday	SAT
East	E	Service Road	SERV RD
Eastbound	(route) E	Shoulder	SHLDR
Emergency	EMER	Slippery	SLIP
Emergency Vehicle	EMER VEH	South	S
Entrance, Enter	ENT	Southbound	(route) S
Express Lane	EXP LN	Speed	SPD
Expressway	EXPWY	Street	ST
XXXX Feet	XXXX FT	Sunday	SUN
Fog Ahead	FOG AHD	Telephone	PHONE
Freeway	FRWY, FWY	Temporary	TEMP
Freeway Blocked	FWY BLKD	Thursday	THURS
Friday	FRI	To Downtown	TO DWNTN
Hazardous Driving	HAZ DRIVING	Traffic	TRAF
Hazardous Material	HAZMAT	Travelers	TRVLRs
High-Occupancy Vehicle	HOV	Tuesday	TUES
Hour(s)	HR, HRS	Time Minutes	TIME MIN
Information	INFO	Upper Level	UPR LEVEL
It Is	ITS	Vehicles (s)	VEH, VEHS
Junction	JCT	Warning	WARN
Left	LFT	Wednesday	WED
Left Lane	LFT LN	Weight Limit	WT LIMIT
Lane Closed	LN CLOSED	West	W
Lower Level	LWR LEVEL	Westbound	(route) W
Maintenance	MAINT	Wet Pavement	WET PVMT
		Will Not	WONT

Roadway designation # IH-number, US-number, SH-number, FM-number

Phase 1: Condition Lists

Road/Lane/Ramp Closure List

FREEWAY CLOSED X MILE
ROAD CLOSED AT SH XXX
ROAD CLSD AT FM XXXX
RIGHT X LANES CLOSED
CENTER LANE CLOSED
NIGHT LANE CLOSURES
VARIOUS LANES CLOSED
EXIT CLOSED
MALL DRIVEWAY CLOSED
XXXXXXXX BLVD CLOSED

Other Condition List

FRONTAGE ROAD CLOSED
SHOULDER CLOSED XXX FT
RIGHT LN CLOSED XXX FT
RIGHT X LANES OPEN
DAYTIME LANE CLOSURES
I-XX SOUTH EXIT CLOSED
EXIT XXX CLOSED X MILE
RIGHT LN TO BE CLOSED
X LANES CLOSED TUE - FRI

* LANES SHIFT in Phase 1 must be used with STAY IN LANE in Phase 2.

Phase 2: Possible Component Lists

Action to Take/Effect on Travel List

MERGE RIGHT
DETOUR NEXT X EXITS
USE EXIT XXX
STAY ON US XXX SOUTH
TRUCKS USE US XXX N
WATCH FOR TRUCKS
EXPECT DELAYS
REDUCE SPEED XXX FT
USE OTHER ROUTES
STAY IN LANE

Location List

AT FM XXXX
BEFORE RAILROAD CROSSING
NEXT X MILES
PAST US XXX EXIT
XXXXXXXX TO XXXXXXX
US XXX TO FM XXXX

Warning List

SPEED LIMIT XX MPH
MAXIMUM SPEED XX MPH
MINIMUM SPEED XX MPH
ADVISORY SPEED XX MPH
RIGHT LANE EXIT
USE CAUTION
DRIVE SAFELY
DRIVE WITH CARE

** Advance Notice List

TUE-FRI XX AM-X PM
APR XX-XX X PM-X AM
BEGINS MONDAY
BEGINS MAY XX
MAY X-X XX PM - XX AM
NEXT FRI-SUN
XX AM TO XX PM
NEXT TUE AUG XX
TONIGHT XX PM-XX AM

** See Application Guidelines Note 6.

APPLICATION GUIDELINES

- Only 1 or 2 phases are to be used on a PCMS.
- The 1st phase (or both) should be selected from the "Road/Lane/Ramp Closure List" and the "Other Condition List".
- A 2nd phase can be selected from the "Action to Take/Effect on Travel, Location, General Warning, or Advance Notice Phase Lists".
- A Location Phase is necessary only if a distance or location is not included in the first phase selected.
- If two PCMS are used in sequence, they must be separated by a minimum of 1000 ft. Each PCMS shall be limited to two phases, and should be understandable by themselves.
- For advance notice, when the current date is within seven days of the actual work date, calendar days should be replaced with days of the week. Advance notification should typically be for no more than one week prior to the work.

WORDING ALTERNATIVES

- The words RIGHT, LEFT and ALL can be interchanged as appropriate.
- Roadway designations IH, US, SH, FM and LP can be interchanged as appropriate.
- EAST, WEST, NORTH and SOUTH (or abbreviations E, W, N and S) can be interchanged as appropriate.
- Highway names and numbers replaced as appropriate.
- ROAD, HIGHWAY and FREEWAY can be interchanged as needed.
- AHEAD may be used instead of distances if necessary.
- FT and MI, MILE and MILES interchanged as appropriate.
- AT, BEFORE and PAST interchanged as needed.
- Distances or AHEAD can be eliminated from the message if a location phase is used.

PCMS SIGNS WITHIN THE R.O.W. SHALL BE BEHIND GUARDRAIL OR CONCRETE BARRIER OR SHALL HAVE A MINIMUM OF FOUR (4) PLASTIC DRUMS PLACED PERPENDICULAR TO TRAFFIC ON THE UPSTREAM SIDE OF THE PCMS, WHEN EXPOSED TO ONE DIRECTION OF TRAFFIC. WHEN EXPOSED TO TWO WAY TRAFFIC, THE FOUR DRUMS SHOULD BE PLACED WITH ONE DRUM AT EACH OF THE FOUR CORNERS OF THE UNIT.

FULL MATRIX PCMS SIGNS

- When Full Matrix PCMS signs are used, the character height and legibility/visibility requirements shall be maintained as listed in Note 15 under "PORTABLE CHANGEABLE MESSAGE SIGNS" above.
- When symbol signs, such as the "Flagger Symbol" (CW20-7) are represented graphically on the Full Matrix PCMS sign and, with the approval of the Engineer, it shall maintain the legibility/visibility requirement listed above.
- When symbol signs are represented graphically on the Full Matrix PCMS, they shall only supplement the use of the static sign represented, and shall not substitute for, or replace that sign.
- A full matrix PCMS may be used to simulate a flashing arrow board provided it meets the visibility, flash rate and dimming requirements on BC(7), for the same size arrow.

SHEET 6 OF 12



BARRICADE AND CONSTRUCTION PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)

BC (6) - 21

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CR:	TxDOT
© TxDOT	November 2002	CONT:	SECT:	JOB:	HIGHWAY:				
REVISIONS		0482	01	036, etc.		SH 285			
9-07	8-14	DIST:	COUNTY:	SHEET NO.					
7-13	5-21	PHR:	JIM HOGG	147					

DATE: 2/12/2024 7:56:52 AM

FILE: pw:\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PRR\Design Projects\048201036\4 - Design Plan Set\14 - SH 285 Standard\SH 285 PCMS\STANDARD\BC-21.dgn

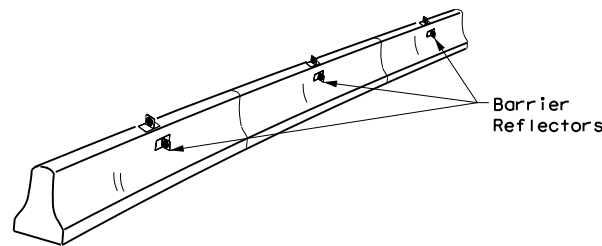
No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for incorrect results or damages resulting from its use. This standard is made available for use on the TxDOT website.

The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for incorrect results or damages resulting from its use. This standard is made available for use on the TxDOT website.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for incorrect results or damages resulting from its use. This standard is made available for use on the TxDOT website.

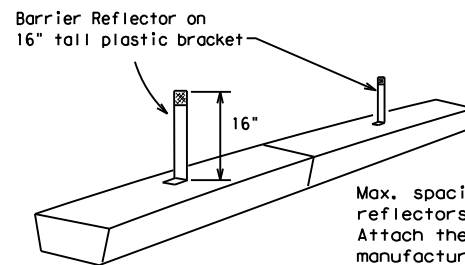
DATE: 2/12/2024 7:56:53 AM
 FILE: \\txdot.projectwiseonline.com:PHR\Design Projects\048201036\4 - Design\Plan Set\14_SH 285 Standards\bc-21.dgn

- Barrier Reflectors shall be pre-qualified, and conform to the color and reflectivity requirements of DMS-8600. A list of prequalified Barrier Reflectors can be found at the Material Producer List web address shown on BC(1).
- Color of Barrier Reflectors shall be as specified in the TMUTCD. The cost of the reflectors shall be considered subsidiary to Item 512.



CONCRETE TRAFFIC BARRIER (CTB)

- Where traffic is on one side of the CTB, two (2) Barrier Reflectors shall be mounted in approximately the midsection of each section of CTB. An alternate mounting location is uniformly spaced at one end of each CTB. This will allow for attachment of a barrier grapple without damaging the reflector. The Barrier Reflector mounted on the side of the CTB shall be located directly below the reflector mounted on top of the barrier, as shown in the detail above.
- Where CTB separates two-way traffic, three barrier reflectors shall be mounted on each section of CTB. The reflector unit on top shall have two yellow reflective faces (Bi-Directional) while the reflectors on each side of the barrier shall have one yellow reflective face, as shown in the detail above.
- When CTB separates traffic traveling in the same direction, no barrier reflectors will be required on top of the CTB.
- Barrier Reflector units shall be yellow or white in color to match the edgeline being supplemented.
- Maximum spacing of Barrier Reflectors is forty (40) feet.
- Pavement markers or temporary flexible-reflective roadway marker tabs shall NOT be used as CTB delineation.
- Attachment of Barrier Reflectors to CTB shall be per manufacturer's recommendations.
- Missing or damaged Barrier Reflectors shall be replaced as directed by the Engineer.
- Single slope barriers shall be delineated as shown on the above detail.

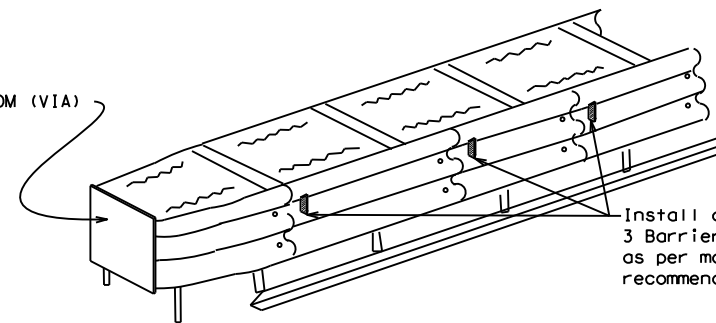


LOW PROFILE CONCRETE BARRIER (LPCB)

LOW PROFILE CONCRETE BARRIER (LPCB) USED IN WORK ZONES

LPCB is approved for use in work zone locations, where the posted speed is 45mph, or less. See Roadway Standard Sheet LPCB.

Max. spacing of barrier reflectors is 20 feet. Attach the delineators as per manufacturer's recommendations.



DELINEATION OF END TREATMENTS

END TREATMENTS FOR CTB'S USED IN WORK ZONES

End treatments used on CTB's in work zones shall meet the appropriate crashworthy standards as defined in the Manual for Assessing Safety Hardware (MASH). Refer to the CWZTCD List for approved end treatments and manufacturers.

BARRIER REFLECTORS FOR CONCRETE TRAFFIC BARRIER AND ATTENUATORS

WARNING LIGHTS

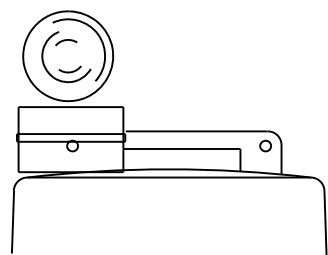
- Warning lights shall meet the requirements of the TMUTCD.
- Warning lights shall NOT be installed on barricades.
- Type A-Low Intensity Flashing Warning Lights are commonly used with drums. They are intended to warn of or mark a potentially hazardous area. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "FL". The Type A Warning Lights shall not be used with signs manufactured with Type B_{FL} or C_{FL} Sheeting meeting the requirements of Departmental Material Specification DMS-8300.
- Type-C and Type D 360 degree Steady Burn Lights are intended to be used in a series for delineation to supplement other traffic control devices. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "SB".
- The Engineer/Inspector or the plans shall specify the location and type of warning lights to be installed on the traffic control devices.
- When required by the Engineer, the Contractor shall furnish a copy of the warning lights certification. The warning light manufacturer will certify the warning lights meet the requirements of the latest ITE Purchase Specifications for Flashing and Steady-Burn Warning Lights.
- When used to delineate curves, Type-C and Type D Steady Burn Lights should only be placed on the outside of the curve, not the inside.
- The location of warning lights and warning reflectors on drums shall be as shown elsewhere in the plans.

WARNING LIGHTS MOUNTED ON PLASTIC DRUMS

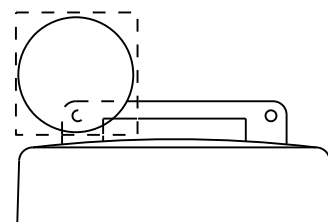
- Type A flashing warning lights are intended to warn drivers that they are approaching or are in a potentially hazardous area.
- Type A random flashing warning lights are not intended for delineation and shall not be used in a series.
- A series of sequential flashing warning lights placed on channelizing devices to form a merging taper may be used for delineation. If used, the successive flashing of the sequential warning lights should occur from the beginning of the taper to the end of the merging taper in order to identify the desired vehicle path. The rate of flashing for each light shall be 65 flashes per minute, plus or minus 10 flashes.
- Type C and D steady-burn warning lights are intended to be used in a series to delineate the edge of the travel lane on detours, on lane changes, on lane closures, and on other similar conditions.
- Type A, Type C and Type D warning lights shall be installed at locations as detailed on other sheets in the plans.
- Warning lights shall not be installed on a drum that has a sign, chevron or vertical panel.
- The maximum spacing for warning lights on drums should be identical to the channelizing device spacing.

WARNING REFLECTORS MOUNTED ON PLASTIC DRUMS AS A SUBSTITUTE FOR TYPE C (STEADY BURN) WARNING LIGHTS

- A warning reflector or approved substitute may be mounted on a plastic drum as a substitute for a Type C, steady burn warning light at the discretion of the Contractor unless otherwise noted in the plans.
- The warning reflector shall be yellow in color and shall be manufactured using a sign substrate approved for use with plastic drums listed on the CWZTCD.
- The warning reflector shall have a minimum retroreflective surface area (one-side) of 30 square inches.
- Round reflectors shall be fully reflectorized, including the area where attached to the drum.
- Square substrates must have a minimum of 30 square inches of reflectorized sheeting. They do not have to be reflectorized where it attaches to the drum.
- The side of the warning reflector facing approaching traffic shall have sheeting meeting the color and retroreflectivity requirements for DMS 8300-Type B or Type C.
- When used near two-way traffic, both sides of the warning reflector shall be reflectorized.
- The warning reflector should be mounted on the side of the handle nearest approaching traffic.
- The maximum spacing for warning reflectors should be identical to the channelizing device spacing requirements.



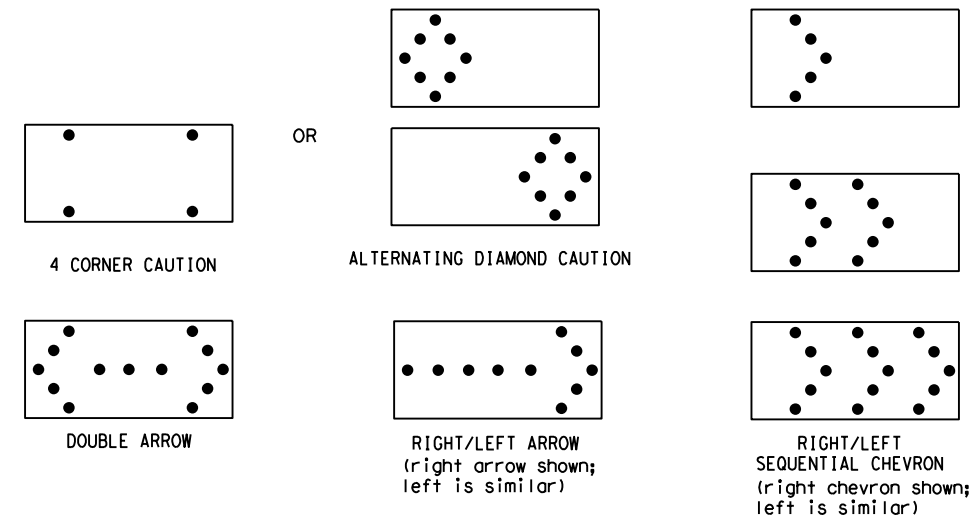
Type C Warning Light or approved substitute mounted on a drum adjacent to the travel way.



Warning reflector may be round or square. Must have a yellow reflective surface area of at least 30 square inches

Arrow Boards may be located behind channelizing devices in place for a shoulder taper or merging taper, otherwise they shall be delineated with four (4) channelizing devices placed perpendicular to traffic on the upstream side of traffic.

- The Flashing Arrow Board should be used for all lane closures on multi-lane roadways, or slow moving maintenance or construction activities on the travel lanes.
- Flashing Arrow Boards should not be used on two-lane, two-way roadways, detours, diversions or work on shoulders unless the "CAUTION" display (see detail below) is used.
- The Engineer/Inspector shall choose all appropriate signs, barricades and/or other traffic control devices that should be used in conjunction with the Flashing Arrow Board.
- The Flashing Arrow Board should be able to display the following symbols:



- The "CAUTION" display consists of four corner lamps flashing simultaneously, or the Alternating Diamond Caution mode as shown.
- The straight line caution display is NOT ALLOWED.
- The Flashing Arrow Board shall be capable of minimum 50 percent dimming from rated lamp voltage. The flashing rate of the lamps shall not be less than 25 nor more than 40 flashes per minute.
- Minimum lamp "on time" shall be approximately 50 percent for the flashing arrow and equal intervals of 25 percent for each sequential phase of the flashing chevron.
- The sequential arrow display is NOT ALLOWED.
- The flashing arrow display is the TxDOT standard; however, the sequential chevron display may be used during daylight operations.
- The Flashing Arrow Board shall be mounted on a vehicle, trailer or other suitable support.
- A Flashing Arrow Board SHALL NOT BE USED to laterally shift traffic.
- A full matrix PCMS may be used to simulate a Flashing Arrow Board provided it meets visibility, flash rate and dimming requirements on this sheet for the same size arrow.
- Minimum mounting height of trailer mounted Arrow Boards should be 7 feet from roadway to bottom of panel.

REQUIREMENTS			
TYPE	MINIMUM SIZE	MINIMUM NUMBER OF PANEL LAMPS	MINIMUM VISIBILITY DISTANCE
B	30 x 60	13	3/4 mile
C	48 x 96	15	1 mile

ATTENTION
 Flashing Arrow Boards shall be equipped with automatic dimming devices.

WHEN NOT IN USE, REMOVE THE ARROW BOARD FROM THE RIGHT-OF-WAY OR PLACE THE ARROW BOARD BEHIND CONCRETE TRAFFIC BARRIER OR GUARDRAIL.

FLASHING ARROW BOARDS

SHEET 7 OF 12

TRUCK-MOUNTED ATTENUATORS

- Truck-mounted attenuators (TMA) used on TxDOT facilities must meet the requirements outlined in the Manual for Assessing Safety Hardware (MASH).
- Refer to the CWZTCD for the requirements of Level 2 or Level 3 TMAs.
- Refer to the CWZTCD for a list of approved TMAs.
- TMAs are required on freeways unless otherwise noted in the plans.
- A TMA should be used anytime that it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the work performance.
- The only reason a TMA should not be required is when a work area is spread down the roadway and the work crew is an extended distance from the TMA.

Texas Department of Transportation

Traffic Safety Division Standard

BARRICADE AND CONSTRUCTION ARROW PANEL, REFLECTORS, WARNING LIGHTS & ATTENUATOR

BC (7) -21

FILE: bc-21.dgn	DN: TxDOT	CR: TxDOT	OW: TxDOT	CK: TxDOT
© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	PHR	JIM HOGG	148	

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.
 DATE: 2/12/2024 7:56:54 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\SH 285 TCP STANDARDS\bc-21.dgn

GENERAL NOTES

- For long term stationary work zones on freeways, drums shall be used as the primary channelizing device.
- For intermediate term stationary work zones on freeways, drums should be used as the primary channelizing device but may be replaced in tangent sections by vertical panels, or 42" two-piece cones. In tangent sections, one-piece cones may be used with the approval of the Engineer but only if personnel are present on the project at all times to maintain the cones in proper position and location.
- For short term stationary work zones on freeways, drums are the preferred channelizing device but may be replaced in tapers, transitions and tangent sections by vertical panels, two-piece cones or one-piece cones as approved by the Engineer.
- Drums and all related items shall comply with the requirements of the current version of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Drums, bases, and related materials shall exhibit good workmanship and shall be free from objectionable marks or defects that would adversely affect their appearance or serviceability.
- The Contractor shall have a maximum of 24 hours to replace any plastic drums identified for replacement by the Engineer/Inspector. The replacement device must be an approved device.

GENERAL DESIGN REQUIREMENTS

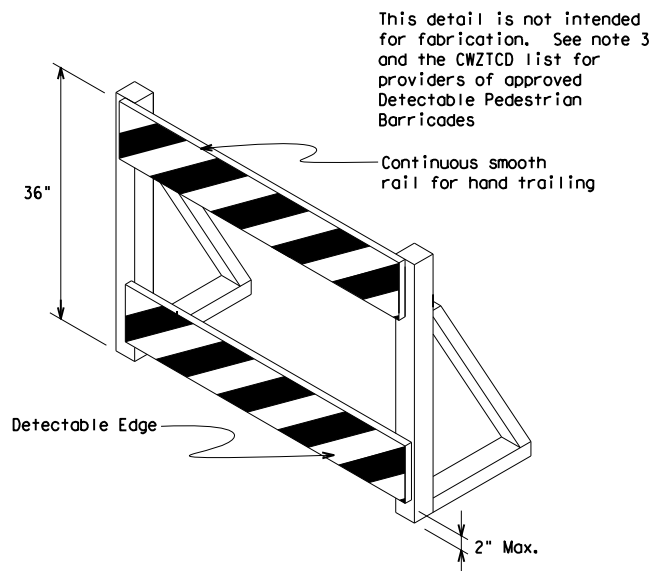
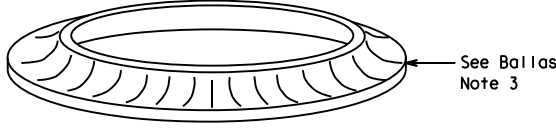
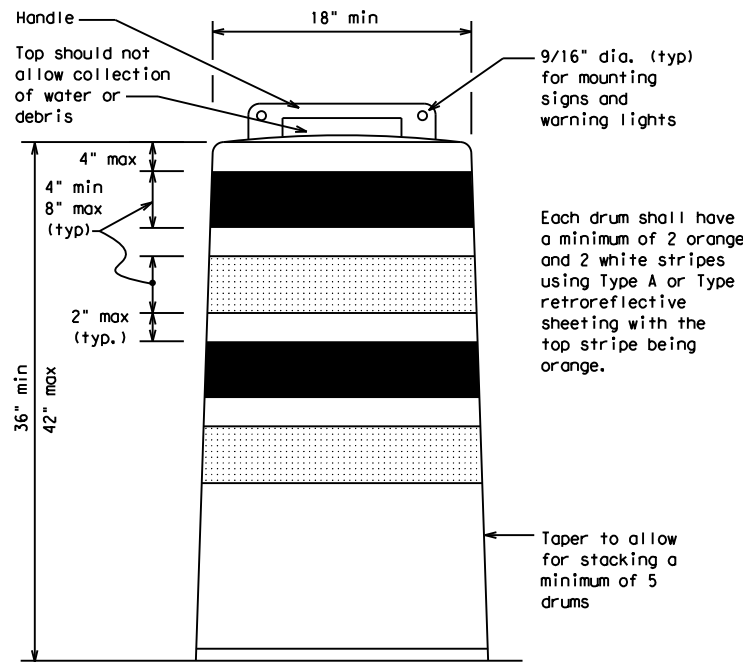
- Pre-qualified plastic drums shall meet the following requirements:
- Plastic drums shall be a two-piece design; the "body" of the drum shall be the top portion and the "base" shall be the bottom.
 - The body and base shall lock together in such a manner that the body separates from the base when impacted by a vehicle traveling at a speed of 20 MPH or greater but prevents accidental separation due to normal handling and/or air turbulence created by passing vehicles.
 - Plastic drums shall be constructed of lightweight flexible, and deformable materials. The Contractor shall NOT use metal drums or single piece plastic drums as channelization devices or sign supports.
 - Drums shall present a profile that is a minimum of 18 inches in width at the 36 inch height when viewed from any direction. The height of drum unit (body installed on base) shall be a minimum of 36 inches and a maximum of 42 inches.
 - The top of the drum shall have a built-in handle for easy pickup and shall be designed to drain water and not collect debris. The handle shall have a minimum of two widely spaced 9/16 inch diameter holes to allow attachment of a warning light, warning reflector unit or approved compliant sign.
 - The exterior of the drum body shall have a minimum of four alternating orange and white retroreflective circumferential stripes not less than 4 inches nor greater than 8 inches in width. Any non-reflectORIZED space between any two adjacent stripes shall not exceed 2 inches in width.
 - Bases shall have a maximum width of 36 inches, a maximum height of 4 inches, and a minimum of two footholds of sufficient size to allow base to be held down while separating the drum body from the base.
 - Plastic drums shall be constructed of ultra-violet stabilized, orange, high-density polyethylene (HDPE) or other approved material.
 - Drum body shall have a maximum unballasted weight of 11 lbs.
 - Drum and base shall be marked with manufacturer's name and model number.

RETROREFLECTIVE SHEETING

- The stripes used on drums shall be constructed of sheeting meeting the color and retroreflectivity requirements of Departmental Materials Specification DMS-8300, "Sign Face Materials." Type A or Type B reflective sheeting shall be supplied unless otherwise specified in the plans.
- The sheeting shall be suitable for use on and shall adhere to the drum surface such that, upon vehicular impact, the sheeting shall remain adhered in-place and exhibit no delaminating, cracking, or loss of retroreflectivity other than that loss due to abrasion of the sheeting surface.

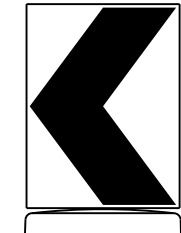
BALLAST

- Unballasted bases shall be large enough to hold up to 50 lbs. of sand. This base, when filled with the ballast material, should weigh between 35 lbs (minimum) and 50 lbs (maximum). The ballast may be sand in one to three sandbags separate from the base, sand in a sand-filled plastic base, or other ballasting devices as approved by the Engineer. Stacking of sandbags will be allowed, however height of sandbags above pavement surface may not exceed 12 inches.
- Bases with built-in ballast shall weigh between 40 lbs. and 50 lbs. Built-in ballast can be constructed of an integral crumb rubber base or a solid rubber base.
- Recycled truck tire sidewalls may be used for ballast on drums approved for this type of ballast on the CWZTCD list.
- The ballast shall not be heavy objects, water, or any material that would become hazardous to motorists, pedestrians, or workers when the drum is struck by a vehicle.
- When used in regions susceptible to freezing, drums shall have drainage holes in the bottoms so that water will not collect and freeze becoming a hazard when struck by a vehicle.
- Ballast shall not be placed on top of drums.
- Adhesives may be used to secure base of drums to pavement.

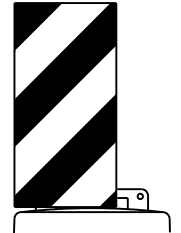


DETECTABLE PEDESTRIAN BARRICADES

- When existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility. Refer to WZ(BTS-2) for Pedestrian Control requirements for Sidewalk Diversions, Sidewalk Detours and Crosswalk Closures.
- Where pedestrians with visual disabilities normally use the closed sidewalk, a Detectable Pedestrian Barricade shall be placed across the full width of the closed sidewalk instead of a Type 3 Barricade.
- Detectable pedestrian barricades similar to the one pictured above, longitudinal channelizing devices, some concrete barriers, and wood or chain link fencing with a continuous detectable edging can satisfactorily delineate a pedestrian path.
- Tape, rope, or plastic chain strung between devices are not detectable, do not comply with the design standards in the "Americans with Disabilities Act Accessibility Guidelines (ADAAG)" and should not be used as a control for pedestrian movements.
- Warning lights shall not be attached to detectable pedestrian barricades.
- Detectable pedestrian barricades should use 8" nominal barricade rails as shown on BC(10) provided that the top rail provides a smooth continuous rail suitable for hand trailing with no splinters, burrs, or sharp edges.



18" x 24" Sign
(Maximum Sign Dimension)
Chevron CW1-8, Opposing Traffic Lane Divider, Driveway sign D70a, Keep Right R4 series or other signs as approved by Engineer



12" x 24" Vertical Panel
mount with diagonals sloping down towards travel way

Plywood, Aluminum or Metal sign substrates shall NOT be used on plastic drums

SIGNS, CHEVRONS, AND VERTICAL PANELS MOUNTED ON PLASTIC DRUMS

- Signs used on plastic drums shall be manufactured using substrates listed on the CWZTCD.
- Chevrons and other work zone signs with an orange background shall be manufactured with Type B_{FL} or Type C_{FL} Orange sheeting meeting the color and retroreflectivity requirements of DMS-8300, "Sign Face Material," unless otherwise specified in the plans.
- Vertical Panels shall be manufactured with orange and white sheeting meeting the requirements of DMS-8300 Type A or Type B. Diagonal stripes on Vertical Panels shall slope down toward the intended traveled lane.
- Other sign messages (text or symbolic) may be used as approved by the Engineer. Sign dimensions shall not exceed 18 inches in width or 24 inches in height, except for the R9 series signs discussed in note 8 below.
- Signs shall be installed using a 1/2 inch bolt (nominal) and nut, two washers, and one locking washer for each connection.
- Mounting bolts and nuts shall be fully engaged and adequately torqued. Bolts should not extend more than 1/2 inch beyond nuts.
- Chevrons may be placed on drums on the outside of curves, on merging tapers or on shifting tapers. When used in these locations, they may be placed on every drum or spaced not more than on every third drum. A minimum of three (3) should be used at each location called for in the plans.
- R9-9, R9-10, R9-11 and R9-11a Sidewalk Closed signs which are 24 inches wide may be mounted on plastic drums, with approval of the Engineer.

SHEET 8 OF 12



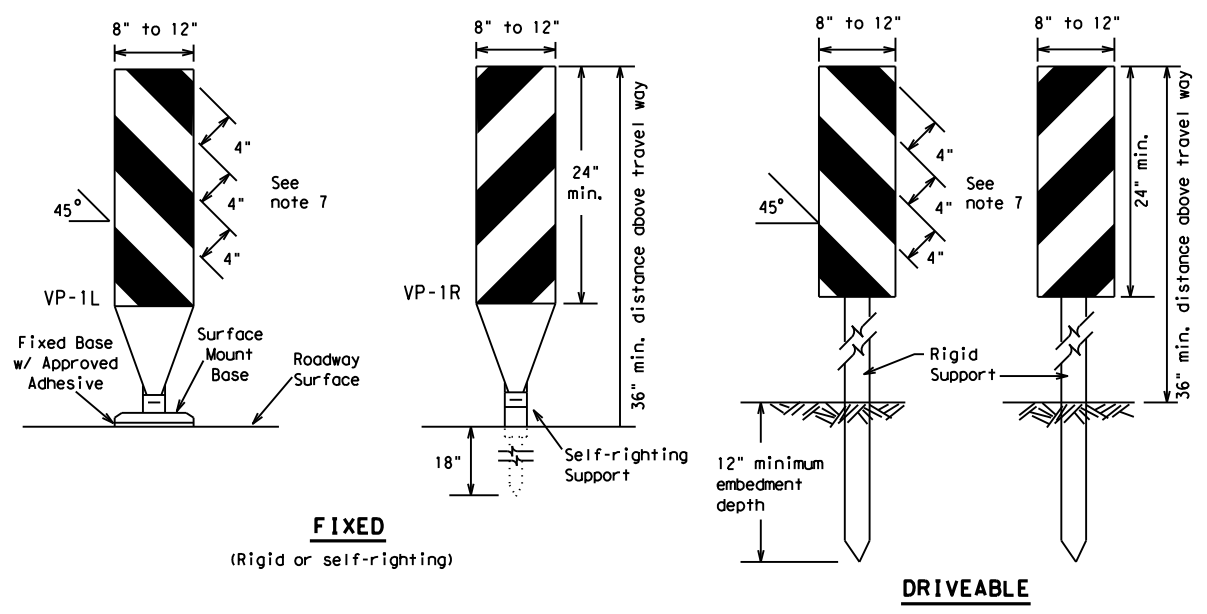
BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (8) - 21

FILE:	bc-21.dgn	DN:	TxDOT	CR:	TxDOT	OW:	TxDOT	CK:	TxDOT
© TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		0482	01	036, etc.		SH 285			
4-03	8-14	DIST	COUNTY		SHEET NO.				
9-07	5-21	PHR	JIM HOGG		149				
7-13									

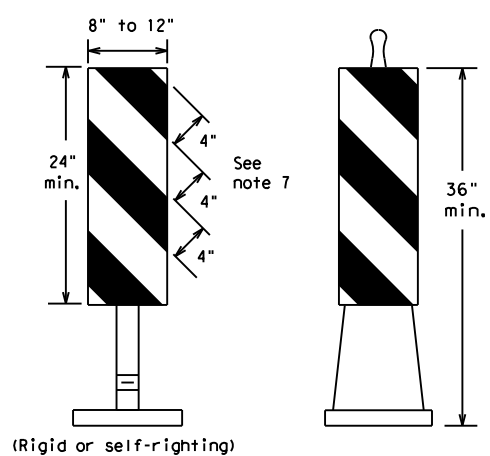
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 7:56:56 AM
 FILE: \\txdot.projectwiseonline.com:PHR\Design Projects\048201036\4 - Design\Plan Set\14_SH_285_Standards\bc-21.dgn



FIXED
(Rigid or self-righting)

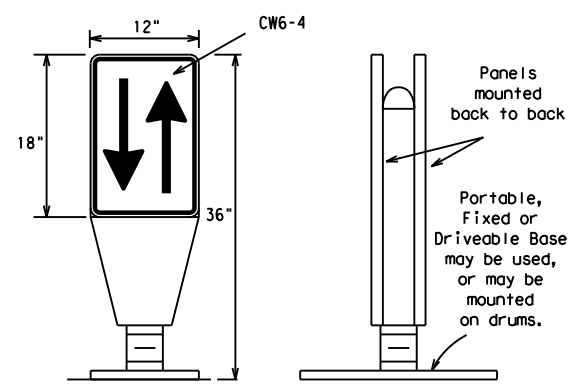
DRIVEABLE



PORTABLE

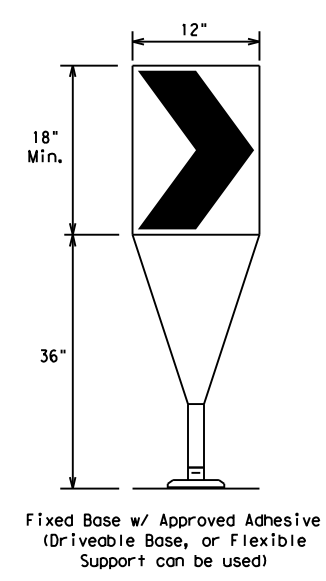
VERTICAL PANELS (VPs)

- Vertical Panels (VP's) are normally used to channelize traffic or divide opposing lanes of traffic.
- VP's may be used in daytime or nighttime situations. They may be used at the edge of shoulder drop-offs and other areas such as lane transitions where positive daytime and nighttime delineation is required. The Engineer/Inspector shall refer to the Roadway Design Manual for additional requirements on the use VP's for drop-offs.
- VP's should be mounted back to back if used at the edge of cuts adjacent to two-way two lane roadways. Stripes are to be reflective orange and reflective white and should always slope downward toward the travel lane.
- VP's used on expressways and freeways or other high speed roadways, may have more than 270 square inches of retroreflective area facing traffic.
- Self-righting supports are available with portable base. See "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Sheeting for the VP's shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300, unless noted otherwise.
- Where the height of reflective material on the vertical panel is 36 inches or greater, a panel stripe of 6 inches shall be used.



OPPOSING TRAFFIC LANE DIVIDERS (OTLD)

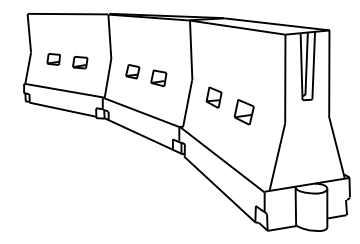
- Opposing Traffic Lane Dividers (OTLD) are delineation devices designed to convert a normal one-way roadway section to two-way operation. OTLD's are used on temporary centerlines. The upward and downward arrows on the sign's face indicate the direction of traffic on either side of the divider. The base is secured to the pavement with an adhesive or rubber weight to minimize movement caused by a vehicle impact or wind gust.
- The OTLD may be used in combination with 42" cones or VPs.
- Spacing between the OTLD shall not exceed 500 feet. 42" cones or VPs placed between the OTLD's should not exceed 100 foot spacing.
- The OTLD shall be orange with a black non-reflective legend. Sheeting for the OTLD shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.



Fixed Base w/ Approved Adhesive (Driveable Base, or Flexible Support can be used)

- The chevron shall be a vertical rectangle with a minimum size of 12 by 18 inches.
- Chevrons are intended to give notice of a sharp change of alignment with the direction of travel and provide additional emphasis and guidance for vehicle operators with regard to changes in horizontal alignment of the roadway.
- Chevrons, when used, shall be erected on the outside of a sharp curve or turn, or on the far side of an intersection. They shall be in line with and at right angles to approaching traffic. Spacing should be such that the motorist always has three in view, until the change in alignment eliminates its need.
- To be effective, the chevron should be visible for at least 500 feet.
- Chevrons shall be orange with a black nonreflective legend. Sheeting for the chevron shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.
- For Long Term Stationary use on tapers or transitions on freeways and divided highways, self-righting chevrons may be used to supplement plastic drums but not to replace plastic drums.

CHEVRONS



LONGITUDINAL CHANNELIZING DEVICES (LCD)

- LCDs are crashworthy, lightweight, deformable devices that are highly visible, have good target value and can be connected together. They are not designed to contain or redirect a vehicle on impact.
- LCDs may be used instead of a line of cones or drums.
- LCDs shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- LCDs should not be used to provide positive protection for obstacles, pedestrians or workers.
- LCDs shall be supplemented with retroreflective delineation as required for temporary barriers on BC(7) when placed roughly parallel to the travel lanes.
- LCDs used as barricades placed perpendicular to traffic should have at least one row of reflective sheeting meeting the requirements for barricade rails as shown on BC(10). Place reflective sheeting near the top of the LCD along the full length of the device.

WATER BALLASTED SYSTEMS USED AS BARRIERS

- Water ballasted systems used as barriers shall not be used solely to channelize road users, but also to protect the work space per the appropriate Manual for Assessing Safety Hardware (MASH) crashworthiness requirements based on roadway speed and barrier application.
- Water ballasted systems used to channelize vehicular traffic shall be supplemented with retroreflective delineation or channelizing devices to improve daytime/nighttime visibility. They may also be supplemented with pavement markings.
- Water ballasted systems used as barriers shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- Water ballasted systems used as barriers should not be used for a merging taper except in low speed (less than 45 MPH) urban areas. When used on a taper in a low speed urban area, the taper shall be delineated and the taper length should be designed to optimize road user operations considering the available geometric conditions.
- When water ballasted systems used as barriers have blunt ends exposed to traffic, they should be attenuated as per manufacturer recommendations or flared to a point outside the clear zone.

If used to channelize pedestrians, longitudinal channelizing devices or water ballasted systems must have a continuous detectable bottom for users of long canes and the top of the unit shall not be less than 32 inches in height.

HOLLOW OR WATER BALLASTED SYSTEMS USED AS LONGITUDINAL CHANNELIZING DEVICES OR BARRIERS

GENERAL NOTES

- Work Zone channelizing devices illustrated on this sheet may be installed in close proximity to traffic and are suitable for use on high or low speed roadways. The Engineer/Inspector shall ensure that spacing and placement is uniform and in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Channelizing devices shown on this sheet may have a driveable, fixed or portable base. The requirement for self-righting channelizing devices must be specified in the General Notes or other plan sheets.
- Channelizing devices on self-righting supports should be used in work zone areas where channelizing devices are frequently impacted by errant vehicles or vehicle related wind gusts making alignment of the channelizing devices difficult to maintain. Locations of these devices shall be detailed elsewhere in the plans. These devices shall conform to the TMUTCD and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- The Contractor shall maintain devices in a clean condition and replace damaged, nonreflective, faded, or broken devices and bases as required by the Engineer/Inspector. The Contractor shall be required to maintain proper device spacing and alignment.
- Portable bases shall be fabricated from virgin and/or recycled rubber. The portable bases shall weigh a minimum of 30 lbs.
- Pavement surfaces shall be prepared in a manner that ensures proper bonding between the adhesives, the fixed mount bases and the pavement surface. Adhesives shall be prepared and applied according to the manufacturer's recommendations.
- The installation and removal of channelizing devices shall not cause detrimental effects to the final pavement surfaces, including pavement surface discoloration or surface integrity. Driveable bases shall not be permitted on final pavement surfaces. The Engineer/Inspector shall approve all application and removal procedures of fixed bases.

Posted Speed	Formula	Minimum Desirable Taper Lengths			Suggested Maximum Spacing of Channelizing Devices	
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent
30	L = WS ² / 60	150'	165'	180'	30'	60'
35		205'	225'	245'	35'	70'
40		265'	295'	320'	40'	80'
45	L = WS	450'	495'	540'	45'	90'
50		500'	550'	600'	50'	100'
55		550'	605'	660'	55'	110'
60		600'	660'	720'	60'	120'
65		650'	715'	780'	65'	130'
70		700'	770'	840'	70'	140'
75		750'	825'	900'	75'	150'
80		800'	880'	960'	80'	160'

**Taper lengths have been rounded off.
 L=Length of Taper (FT.) W=Width of Offset (FT.)
 S=Posted Speed (MPH)

SUGGESTED MAXIMUM SPACING OF CHANNELIZING DEVICES AND MINIMUM DESIRABLE TAPER LENGTHS

SHEET 9 OF 12



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (9) - 21

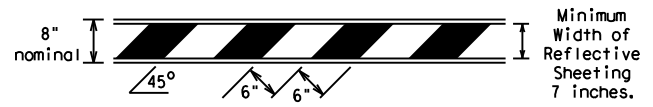
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
9-07 8-14	DIST	COUNTY		SHEET NO.
7-13 5-21	PHR	JIM HOGG		150

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

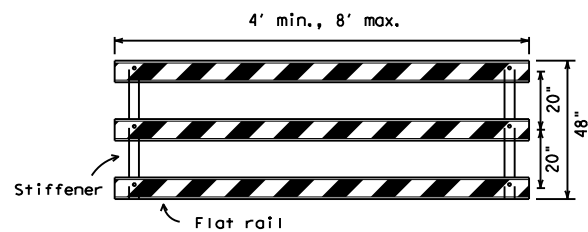
TYPE 3 BARRICADES

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTCD) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road, striping should slope downward in both directions toward the center of roadway.
4. Striping of rails, for the right side of the roadway, should slope downward to the left. For the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rails. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand bags shall not be stacked in a manner that covers any portion of a barricade rails reflective sheeting. Rock, concrete, iron, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed along or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheeting for barricades shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300 unless otherwise noted.

Barricades shall NOT be used as a sign support.

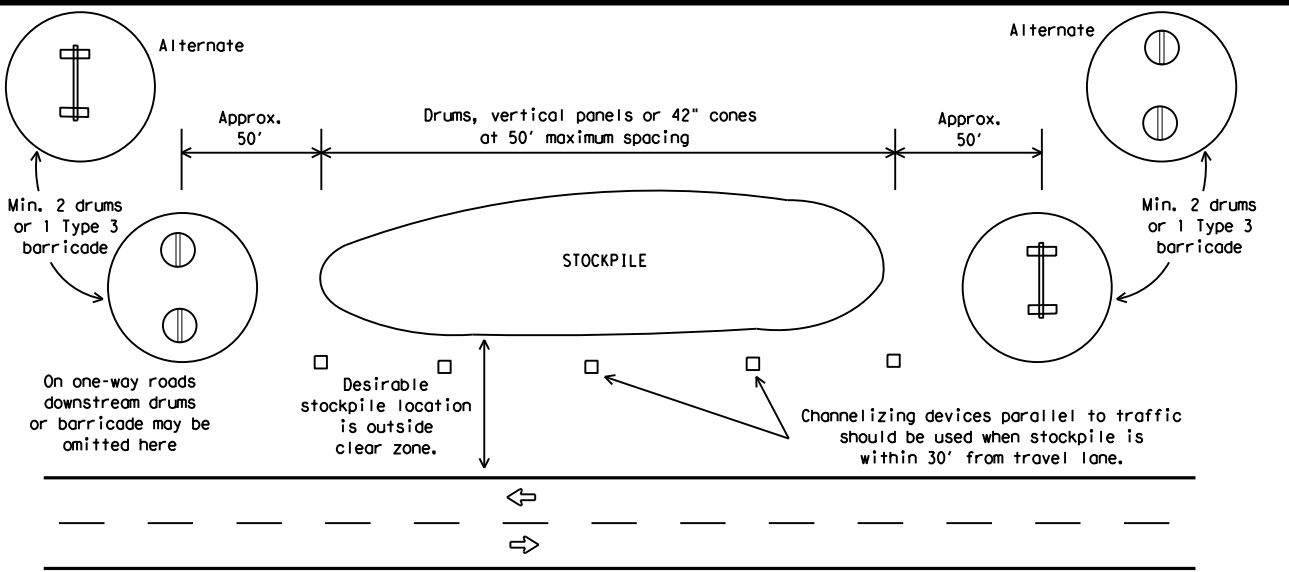


TYPICAL STRIPING DETAIL FOR BARRICADE RAIL



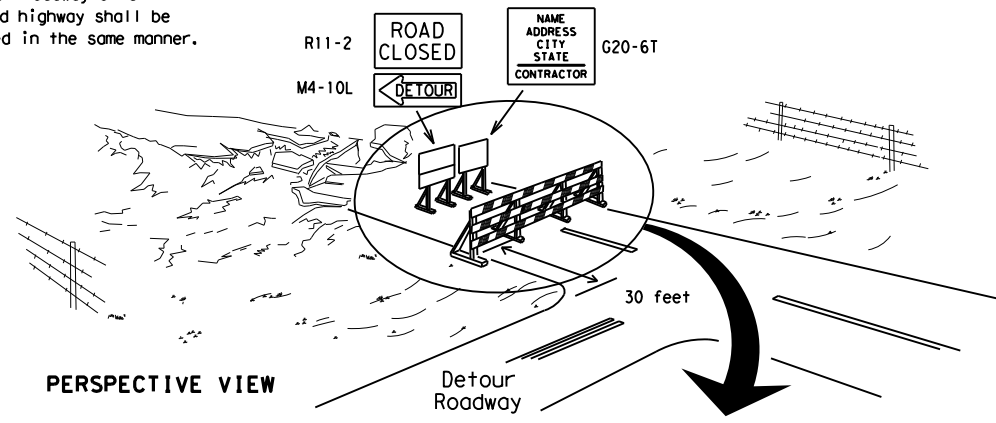
Stiffener may be inside or outside of support, but no more than 2 stiffeners shall be allowed on one barricade.

TYPICAL PANEL DETAIL FOR SKID OR POST TYPE BARRICADES



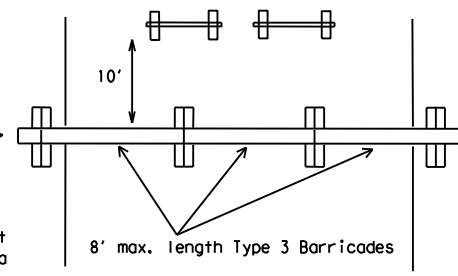
TRAFFIC CONTROL FOR MATERIAL STOCKPILES

Each roadway of a divided highway shall be barricaded in the same manner.



PERSPECTIVE VIEW

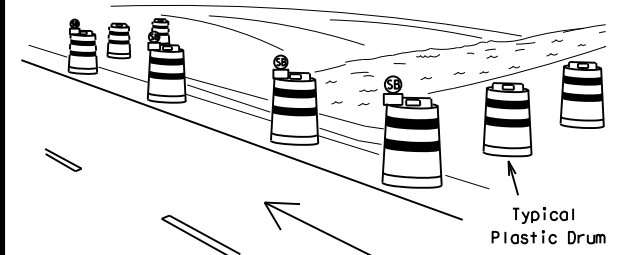
The three rails on Type 3 barricades shall be reflectorized orange and reflective white stripes on one side facing one-way traffic and both sides for two-way traffic. Barricade striping should slant downward in the direction of detour.



PLAN VIEW

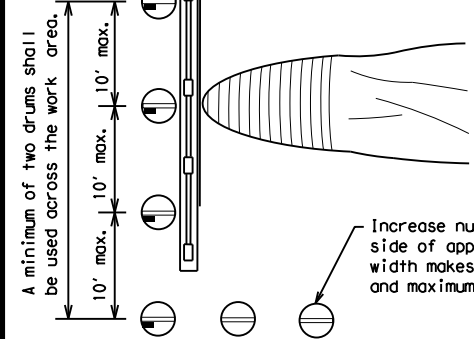
1. Signs should be mounted on independent supports at a 7 foot mounting height in center of roadway. The signs should be a minimum of 10 feet behind Type 3 Barricades.
2. Advance signing shall be as specified elsewhere in the plans.

TYPE 3 BARRICADE (POST AND SKID) TYPICAL APPLICATION



PERSPECTIVE VIEW

These drums are not required on one-way roadway



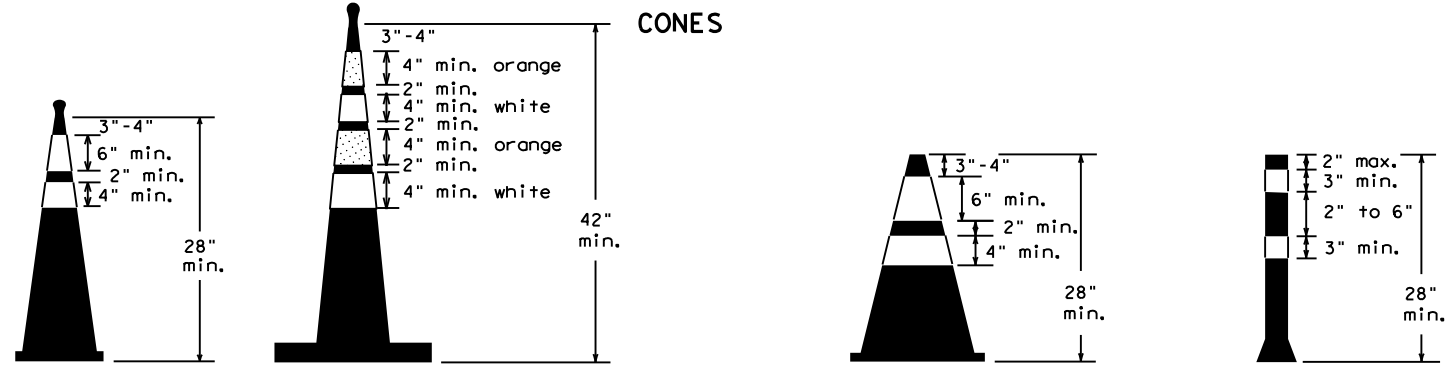
PLAN VIEW

Increase number of plastic drums on the side of approaching traffic if the crown width makes it necessary. (minimum of 2 and maximum of 4 drums)

1. Where positive redirection capability is provided, drums may be omitted.
2. Plastic construction fencing may be used with drums for safety as required in the plans.
3. Vertical Panels on flexible support may be substituted for drums when the shoulder width is less than 4 feet.
4. When the shoulder width is greater than 12 feet, steady-burn lights may be omitted if drums are used.
5. Drums must extend the length of the culvert widening.

LEGEND	
	Plastic drum
	Plastic drum with steady burn light or yellow warning reflector
	Steady burn warning light or yellow warning reflector

CULVERT WIDENING OR OTHER ISOLATED WORK WITHIN THE PROJECT LIMITS



Two-Piece cones

One-Piece cones

Tubular Marker

28" Cones shall have a minimum weight of 9 1/2 lbs.
 42" 2-piece cones shall have a minimum weight of 30 lbs. including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A or Type B.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined on BC(4). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone durations.
7. Cones or tubular markers used on each project should be of the same size and shape.



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (10) - 21

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	OW:	TxDOT	CR:	TxDOT
© TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		0482	01	036, etc.		SH 285			
9-07	8-14	DIST	COUNTY	SHEET NO.					
7-13	5-21	PHR	JIM HOGG	151					

WORK ZONE PAVEMENT MARKINGS

GENERAL

- The Contractor shall be responsible for maintaining work zone and existing pavement markings, in accordance with the standard specifications and special provisions, on all roadways open to traffic within the CSJ limits unless otherwise stated in the plans.
- Color, patterns and dimensions shall be in conformance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Additional supplemental pavement marking details may be found in the plans or specifications.
- Pavement markings shall be installed in accordance with the TMUTCD and as shown on the plans.
- When short term markings are required on the plans, short term markings shall conform with the TMUTCD, the plans and details as shown on the Standard Plan Sheet WZ(STPM).
- When standard pavement markings are not in place and the roadway is opened to traffic, DO NOT PASS signs shall be erected to mark the beginning of the sections where passing is prohibited and PASS WITH CARE signs at the beginning of sections where passing is permitted.
- All work zone pavement markings shall be installed in accordance with Item 662, "Work Zone Pavement Markings."

RAISED PAVEMENT MARKERS

- Raised pavement markers are to be placed according to the patterns on BC(12).
- All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and Departmental Material Specification DMS-4200 or DMS-4300.

PREFABRICATED PAVEMENT MARKINGS

- Removable prefabricated pavement markings shall meet the requirements of DMS-8241.
- Non-removable prefabricated pavement markings (foil back) shall meet the requirements of DMS-8240.

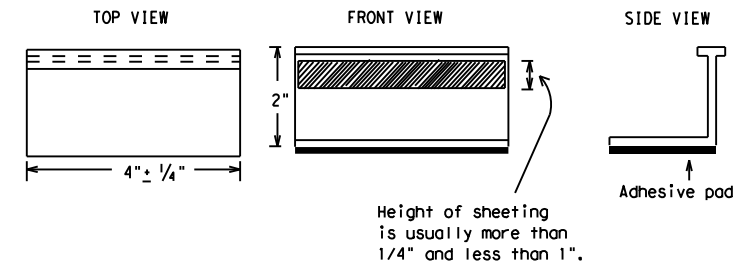
MAINTAINING WORK ZONE PAVEMENT MARKINGS

- The Contractor will be responsible for maintaining work zone pavement markings within the work limits.
- Work zone pavement markings shall be inspected in accordance with the frequency and reporting requirements of work zone traffic control device inspections as required by Form 599.
- The markings should provide a visible reference for a minimum distance of 300 feet during normal daylight hours and 160 feet when illuminated by automobile low-beam headlights at night, unless sight distance is restricted by roadway geometrics.
- Markings failing to meet this criteria within the first 30 days after placement shall be replaced at the expense of the Contractor as per Specification Item 662.

REMOVAL OF PAVEMENT MARKINGS

- Pavement markings that are no longer applicable, could create confusion or direct a motorist toward or into the closed portion of the roadway shall be removed or obliterated before the roadway is opened to traffic.
- The above shall not apply to detours in place for less than three days, where flaggers and/or sufficient channelizing devices are used in lieu of markings to outline the detour route.
- Pavement markings shall be removed to the fullest extent possible, so as not to leave a discernable marking. This shall be by any method approved by TxDOT Specification Item 677 for "Eliminating Existing Pavement Markings and Markers".
- The removal of pavement markings may require resurfacing or seal coating portions of the roadway as described in Item 677.
- Subject to the approval of the Engineer, any method that proves to be successful on a particular type pavement may be used.
- Blast cleaning may be used but will not be required unless specifically shown in the plans.
- Over-painting of the markings SHALL NOT BE permitted.
- Removal of raised pavement markers shall be as directed by the Engineer.
- Removal of existing pavement markings and markers will be paid for directly in accordance with Item 677, "ELIMINATING EXISTING PAVEMENT MARKINGS AND MARKERS," unless otherwise stated in the plans.
- Black-out marking tape may be used to cover conflicting existing markings for periods less than two weeks when approved by the Engineer.

Temporary Flexible-Reflective Roadway Marker Tabs



**STAPLES OR NAILS SHALL NOT BE USED TO SECURE
TEMPORARY FLEXIBLE-REFLECTIVE ROADWAY MARKER
TABS TO THE PAVEMENT SURFACE**

- Temporary flexible-reflective roadway marker tabs used as guidemarks shall meet the requirements of DMS-8242.
- Tabs detailed on this sheet are to be inspected and accepted by the Engineer or designated representative. Sampling and testing is not normally required, however at the option of the Engineer, either "A" or "B" below may be imposed to assure quality before placement on the roadway.
 - Select five (5) or more tabs at random from each lot or shipment and submit to the Construction Division, Materials and Pavement Section to determine specification compliance.
 - Select five (5) tabs and perform the following test. Affix five (5) tabs at 24 inch intervals on an asphaltic pavement in a straight line. Using a medium size passenger vehicle or pickup, run over the markers with the front and rear tires at a speed of 35 to 40 miles per hour, four (4) times in each direction. No more than one (1) out of the five (5) reflective surfaces shall be lost or displaced as a result of this test.
- Small design variances may be noted between tab manufacturers.
- See Standard Sheet WZ(STPM) for tab placement on new pavements. See Standard Sheet TCP(7-1) for tab placement on seal coat work.

RAISED PAVEMENT MARKERS USED AS GUIDEMARKS

- Raised pavement markers used as guidemarks shall be from the approved product list, and meet the requirements of DMS-4200.
- All temporary construction raised pavement markers provided on a project shall be of the same manufacturer.
- Adhesive for guidemarks shall be bituminous material hot applied or butyl rubber pad for all surfaces, or thermoplastic for concrete surfaces.

Guidemarks shall be designated as:
 YELLOW - (two amber reflective surfaces with yellow body).
 WHITE - (one silver reflective surface with white body).

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
TRAFFIC BUTTONS	DMS-4300
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240
TEMPORARY REMOVABLE, PREFABRICATED PAVEMENT MARKINGS	DMS-8241
TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS	DMS-8242

A list of prequalified reflective raised pavement markers, non-reflective traffic buttons, roadway marker tabs and other pavement markings can be found at the Material Producer List web address shown on BC(1).

SHEET 11 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKINGS

BC(11)-21

FILE: bc-21.dgn	DN: TxDOT	CR: TxDOT	DW: TxDOT	CK: TxDOT
© TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS		0482	01	036, etc.
2-98	9-07	5-21		
1-02	7-13			
11-02	8-14			
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	152	

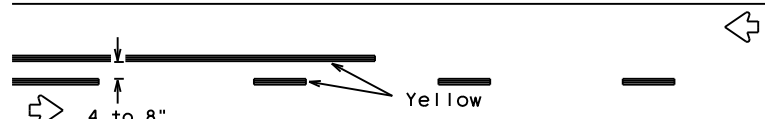
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 7:56:58 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standard\SH 285 TCP STANDARD\bc-21.dgn

PAVEMENT MARKING PATTERNS

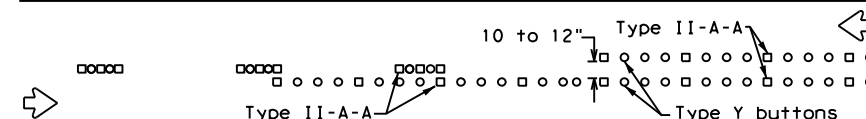


REFLECTORIZED PAVEMENT MARKINGS - PATTERN A

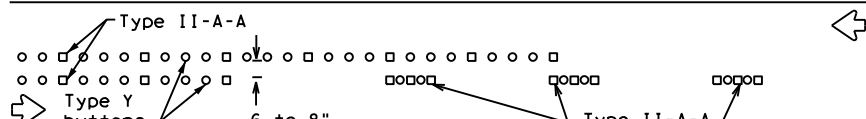


REFLECTORIZED PAVEMENT MARKINGS - PATTERN B

Pattern A is the TXDOT Standard, however Pattern B may be used if approved by the Engineer. Prefabricated markings may be substituted for reflectORIZED pavement markings.

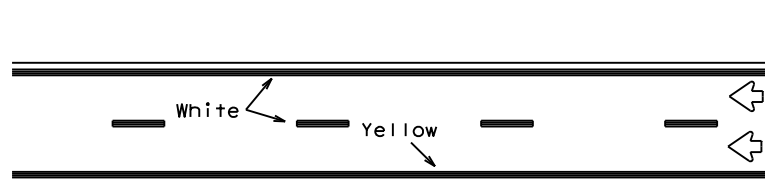


RAISED PAVEMENT MARKERS - PATTERN A



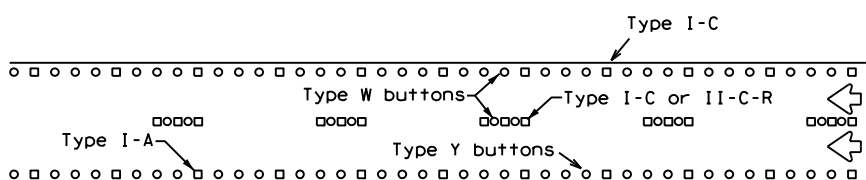
RAISED PAVEMENT MARKERS - PATTERN B

CENTER LINE & NO-PASSING ZONE BARRIER LINES FOR TWO-LANE, TWO-WAY HIGHWAYS



REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectORIZED pavement markings.



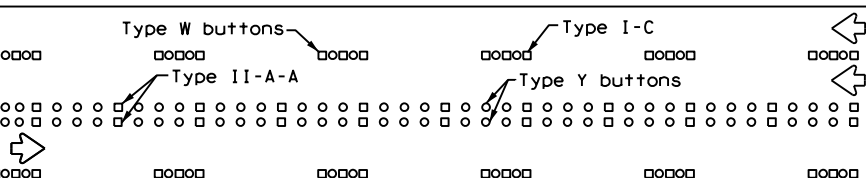
RAISED PAVEMENT MARKERS

EDGE & LANE LINES FOR DIVIDED HIGHWAY



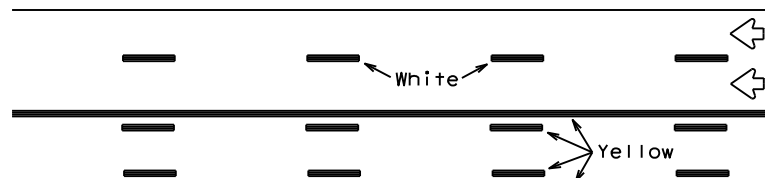
REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectORIZED pavement markings.



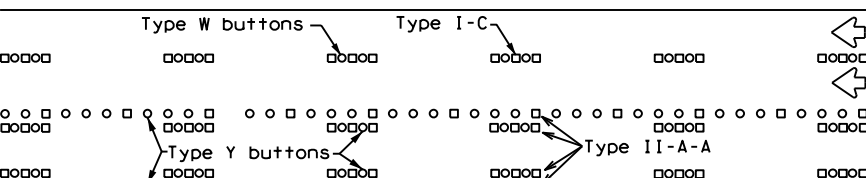
RAISED PAVEMENT MARKERS

LANE & CENTER LINES FOR MULTILANE UNDIVIDED HIGHWAYS



REFLECTORIZED PAVEMENT MARKINGS

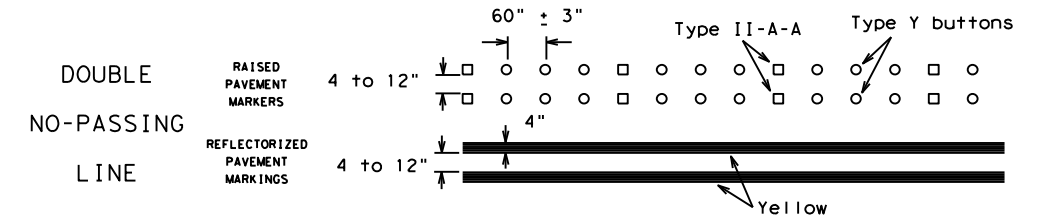
Prefabricated markings may be substituted for reflectORIZED pavement markings.



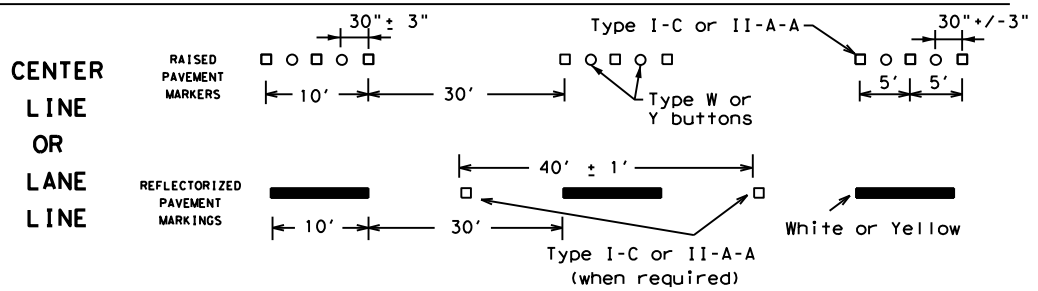
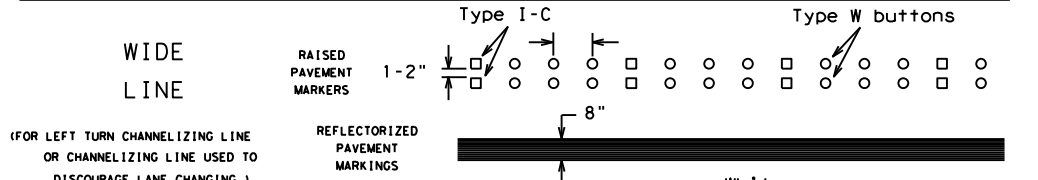
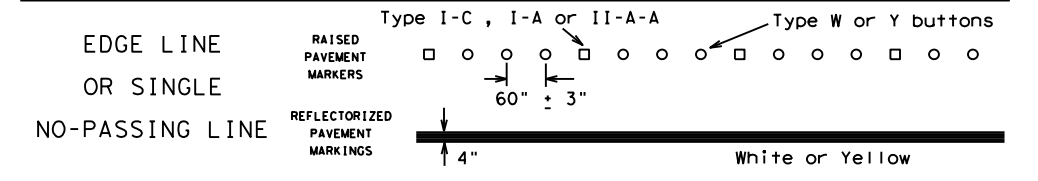
RAISED PAVEMENT MARKERS

TWO-WAY LEFT TURN LANE

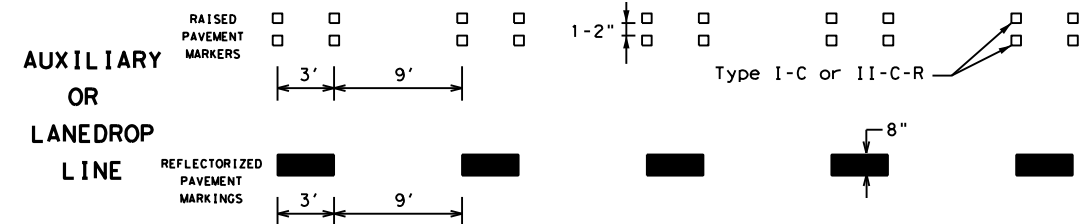
STANDARD WORK ZONE PAVEMENT MARKINGS DETAILS



SOLID LINES

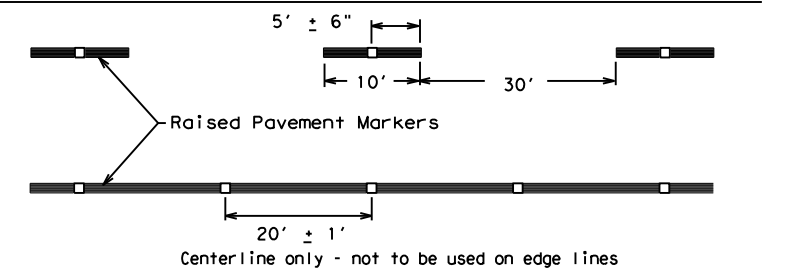


BROKEN LINES



REMOVABLE MARKINGS WITH RAISED PAVEMENT MARKERS

If raised pavement markers are used to supplement REMOVABLE markings, the markers shall be applied to the top of the tape at the approximate mid length of tape used for broken lines or at 20 foot spacing for solid lines. This allows an easier removal of raised pavement markers and tape.



SHEET 12 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKING PATTERNS

BC(12)-21

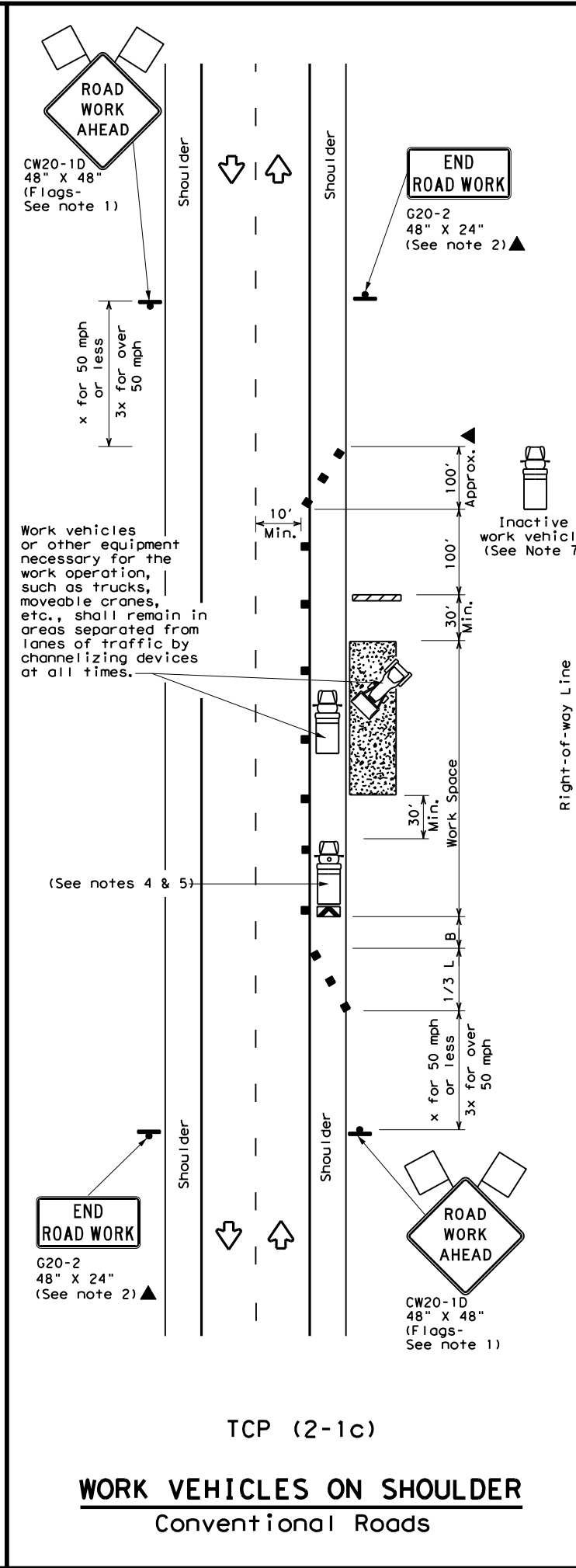
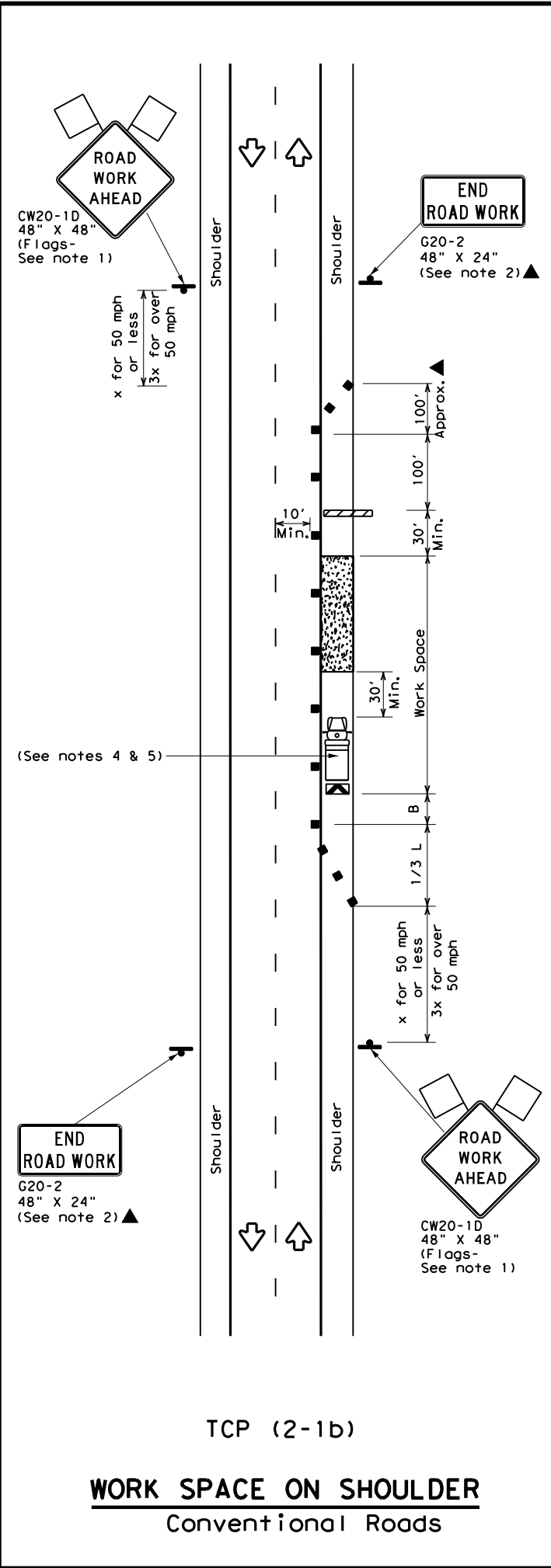
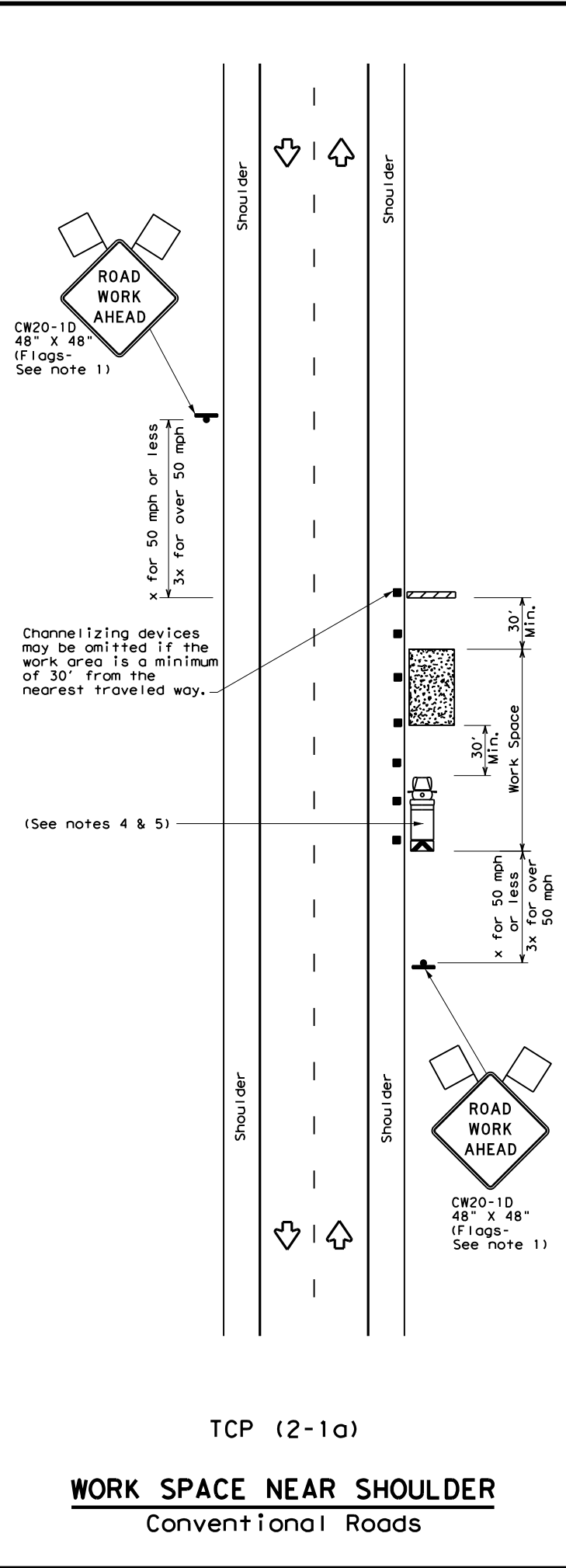
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	OW: TxDOT	CK: TxDOT
©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
1-97 9-07 5-21	DIST	COUNTY	SHEET NO.	
2-98 7-13	PHR	JIM HOGG	153	
11-02 8-14				

Raised pavement markers used as standard pavement markings shall be from the approved products list and meet the requirements of Item 672 "RAISED PAVEMENT MARKERS."

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TXDOT for any purpose whatsoever. TXDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 7:56:59 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14_SH 285 Standards\bc-21.dgn

DATE: 2/12/2024 7:57:03 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\021-18.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to any other format or for any errors or omissions in this standard.



LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70	700'	770'	840'	70'	140'	800'	475'	
75	750'	825'	900'	75'	150'	900'	540'	

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	✓

- GENERAL NOTES**
- Flags attached to signs where shown, are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated in the plans, or for routine maintenance work, when approved by the Engineer.
 - Stockpiled material should be placed a minimum of 30 feet from nearest traveled way.
 - Shadow Vehicle with TMA and high intensity rotating, flashing, oscillating or strobe lights. A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
 - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.
 - See TCP(5-1) for shoulder work on divided highways, expressways and freeways.
 - Additional work vehicles or other equipment should be parked near the right-of-way line and not parked on the paved shoulder.
 - CW21-5 "SHOULDER WORK" signs may be used in place of CW20-1D "ROAD WORK AHEAD" signs for shoulder work on conventional roadways.

Texas Department of Transportation
 Traffic Operations Division Standard

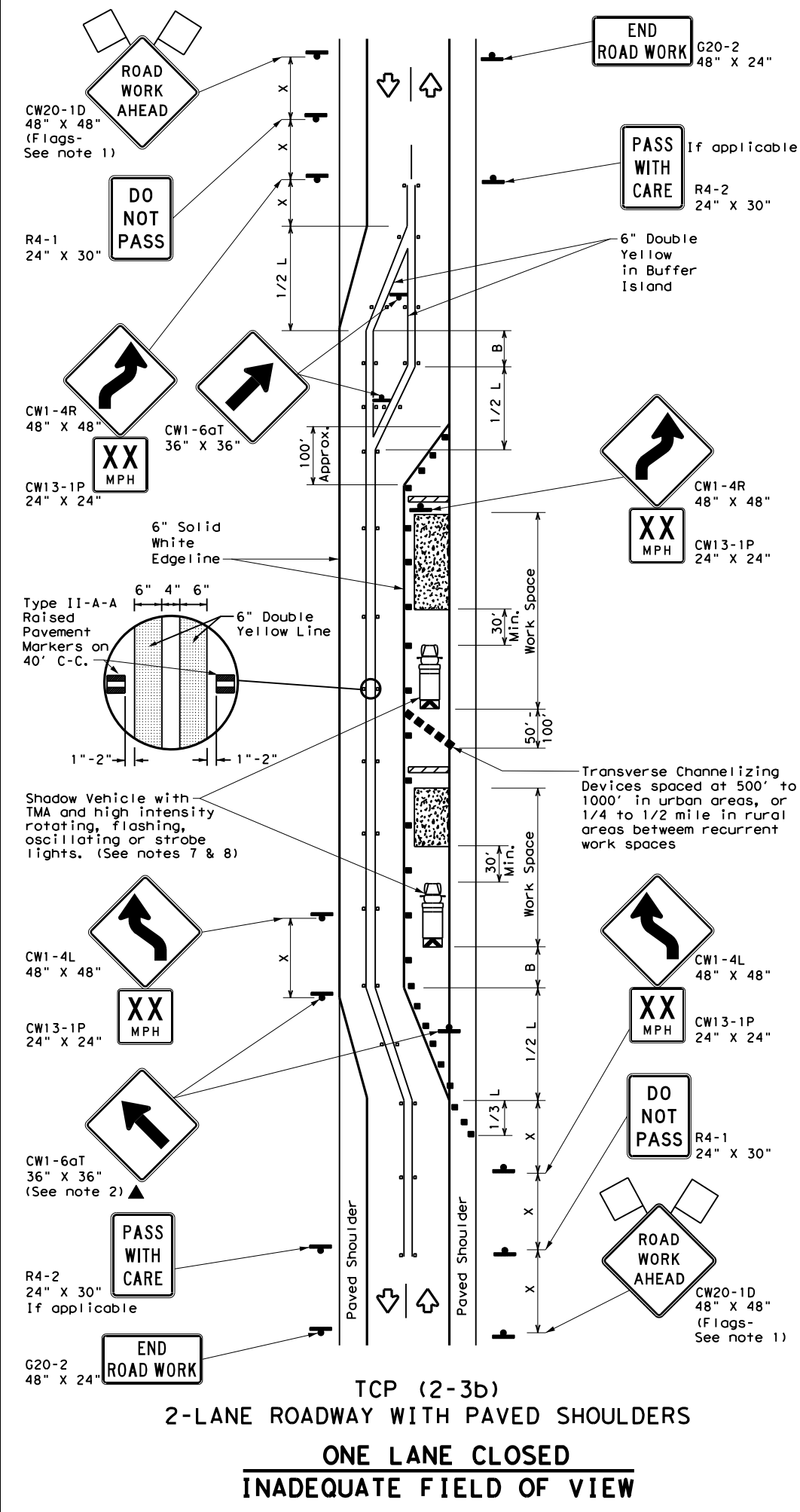
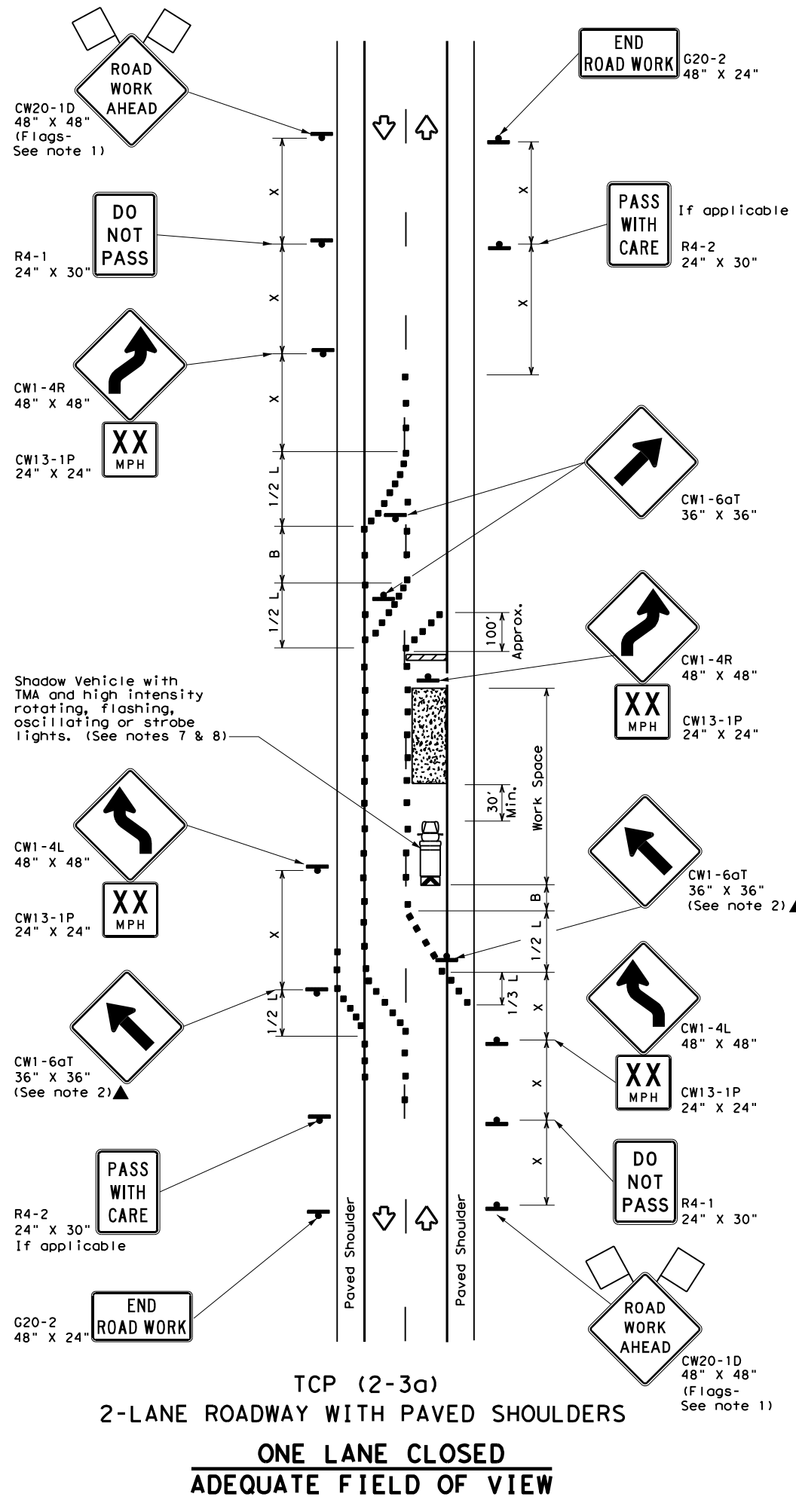
TRAFFIC CONTROL PLAN
CONVENTIONAL ROAD
SHOULDER WORK

TCP (2-1) - 18

FILE: tcp2-1-18.dgn	DN:	CK:	DW:	CK:
© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
2-94 4-98	DIST	COUNTY	SHEET NO.	
8-95 2-12	PHR	JIM HOGG	154	
1-97 2-18				

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: FILE:



LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Raised Pavement Markers Ty II-AA
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	L = WS ² / 60	150'	165'	180'	30'	70'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
			✓	✓
				TCP (2-3b) ONLY

- GENERAL NOTES**
- Flags attached to signs where shown, are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
 - When work space will be in place less than three days existing pavement markings may remain in place. Channelizing devices shall be used to separate traffic.
 - Flagger control should NOT be used unless roadway conditions or heavy traffic volume require additional emphasis to safely control traffic. Flagger should be positioned at end of traffic queue.
 - The R4-1 "DO NOT PASS," R4-2 "PASS WITH CARE" and construction regulatory speed zone signs may be installed within CW20-1D "ROAD WORK AHEAD" signs. Proper spacing of signs shall be maintained.
 - Conflicting pavement marking shall be removed for long term projects.
 - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted.
 - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.
- TCP (2-3a)**
- Conflicting pavement markings shall be removed for long-term projects. For shorter durations where traffic is directed over a yellow centerline, channelizing devices which separate two-way traffic should be spaced on tapers at 20' or 15' if posted speeds are 35 mph or slower, and for tangent sections, at 1/2(S) where S is the speed in mph. This tighter device spacing is intended for the area of the conflicting markings, not the entire work zone.

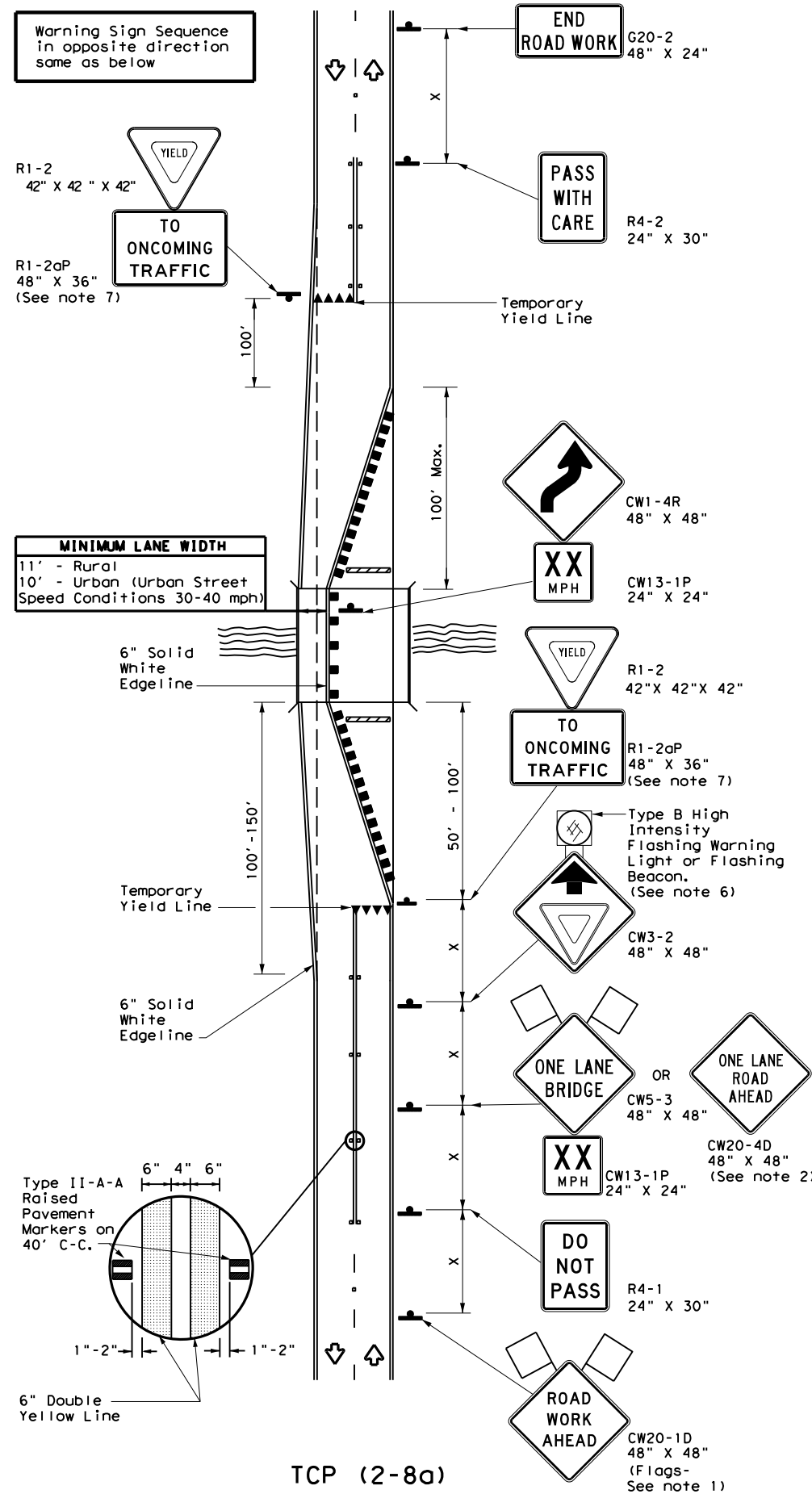


TRAFFIC CONTROL PLAN
TRAFFIC SHIFTS ON
TWO-LANE ROADS

TCP (2-3) - 23

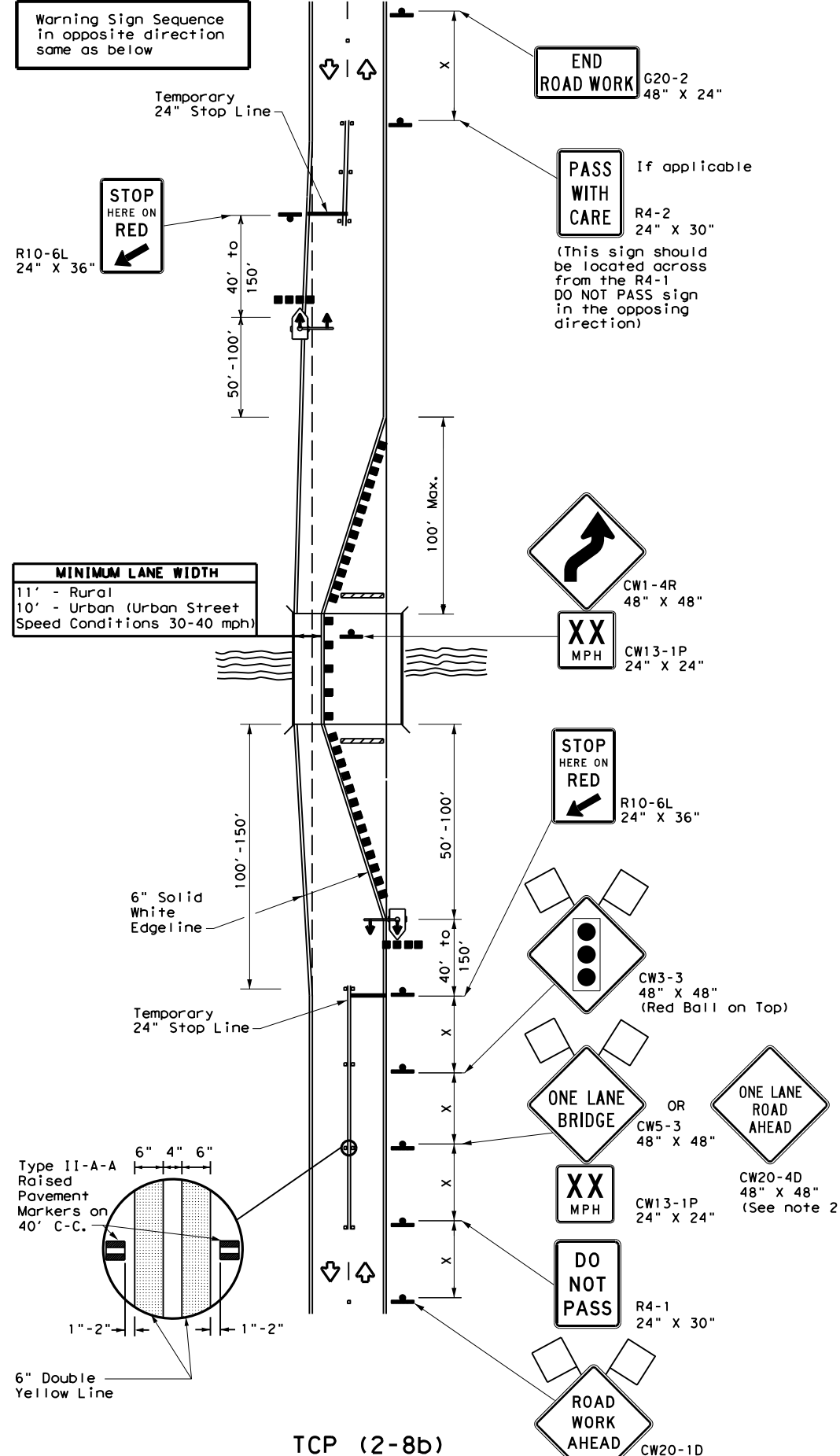
FILE:	tcp(2-3)-23.dgn	DN:	CK:	DW:	CK:
© TxDOT	April 2023	CONT	SECT	JOB	HIGHWAY
12-85	4-98	0482	01	036, etc.	SH 285
8-95	3-03	DIST	COUNTY		SHEET NO.
1-97	2-12	PHR	JIM HOGG		155

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.



TCP (2-8a)

**ONE LANE TWO-WAY
 TRAFFIC CONTROL WITH YIELD SIGNS
 (Less Than 2000 ADT-See Note 5)**



TCP (2-8b)

**ONE LANE TWO-WAY
 TRAFFIC CONTROL WITH TRAFFIC SIGNAL**

LEGEND

	Type 3 Barricade		Channelizing Devices
	Sign		Traffic Flow
	Flag		Flagger
	Raised Pavement Markers Ty II-AA		Temporary or Portable Traffic Signal

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"	Stopping Sight Distance
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent			
30	L = WS ² / 60	150'	165'	180'	30'	60'	120'	90'	200'
35		205'	225'	245'	35'	70'	160'	120'	250'
40		265'	295'	320'	40'	80'	240'	155'	305'
45	L = WS	450'	495'	540'	45'	90'	320'	195'	360'
50		500'	550'	600'	50'	100'	400'	240'	425'
55		550'	605'	660'	55'	110'	500'	295'	495'
60	L = WS	600'	660'	720'	60'	120'	600'	350'	570'
65		650'	715'	780'	65'	130'	700'	410'	645'
70		700'	770'	840'	70'	140'	800'	475'	730'
75	L = WS	750'	825'	900'	75'	150'	900'	540'	820'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE

MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
			✓	✓

GENERAL NOTES

- Flags attached to signs where shown are REQUIRED.
 - When this TCP is used at a location which does not involve a bridge, a 48" x 48" CW20-4D "ONE LANE ROAD AHEAD" signs should be used in lieu of the CW5-3 "ONE LANE BRIDGE" signs. The CW13-1P Advisory Speed Plaque is required with either warning sign.
 - Raised pavement markers shall be placed 40 feet c-c on centerline between DO NOT PASS signs and stop or yield lines.
 - For intermediate term situations, when it is not feasible to remove and restore pavement markings, the channelization must be made dominant by using a very close spacing. This is especially important in locations of conflicting information, such as where traffic is directed over a double yellow centerline. In such locations a maximum channelizing device spacing of 20 feet is recommended. The 20 foot channelizing device spacing recommendation is intended for the area of conflicting information and not the entire work zone.
- TCP (2-8a)**
- Traffic control by CW3-2 "YIELD AHEAD" symbol signs for one lane two-way traffic control operations should be limited to work spaces less than 400 feet long and roadways with less than 2000 ADT. Otherwise, portable traffic signals should be used.
 - If power is available, a flashing beacon should be attached to the CW3-2 "YIELD AHEAD" symbol sign for emphasis.
 - The R1-2 "YIELD" and R1-2aP "TO ONCOMING TRAFFIC" signs and other regulatory signs shall be installed at 7 foot minimum mounting height.
- TCP (2-8b)**
- A list of approved Portable Traffic Signals can be found in the "Compliant Work Zone Traffic Control Devices" list.
 - Portable traffic signals should be located to provide adequate stopping sight distance for approaching motorist (See table above).

Traffic Safety Division Standard

TEXAS DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL PLAN
 LONG TERM ONE-LANE
 TWO-WAY CONTROL**

TCP (2-8) -23

FILE: tcp2-8-23.dgn	DN:	CK:	DW:	CK:
© TxDOT April 2023	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
12-85 4-98 2-18	DIST	COUNTY	SHEET NO.	
8-95 3-03 4-23	PHR	JIM HOGG	156	
1-97 2-12				

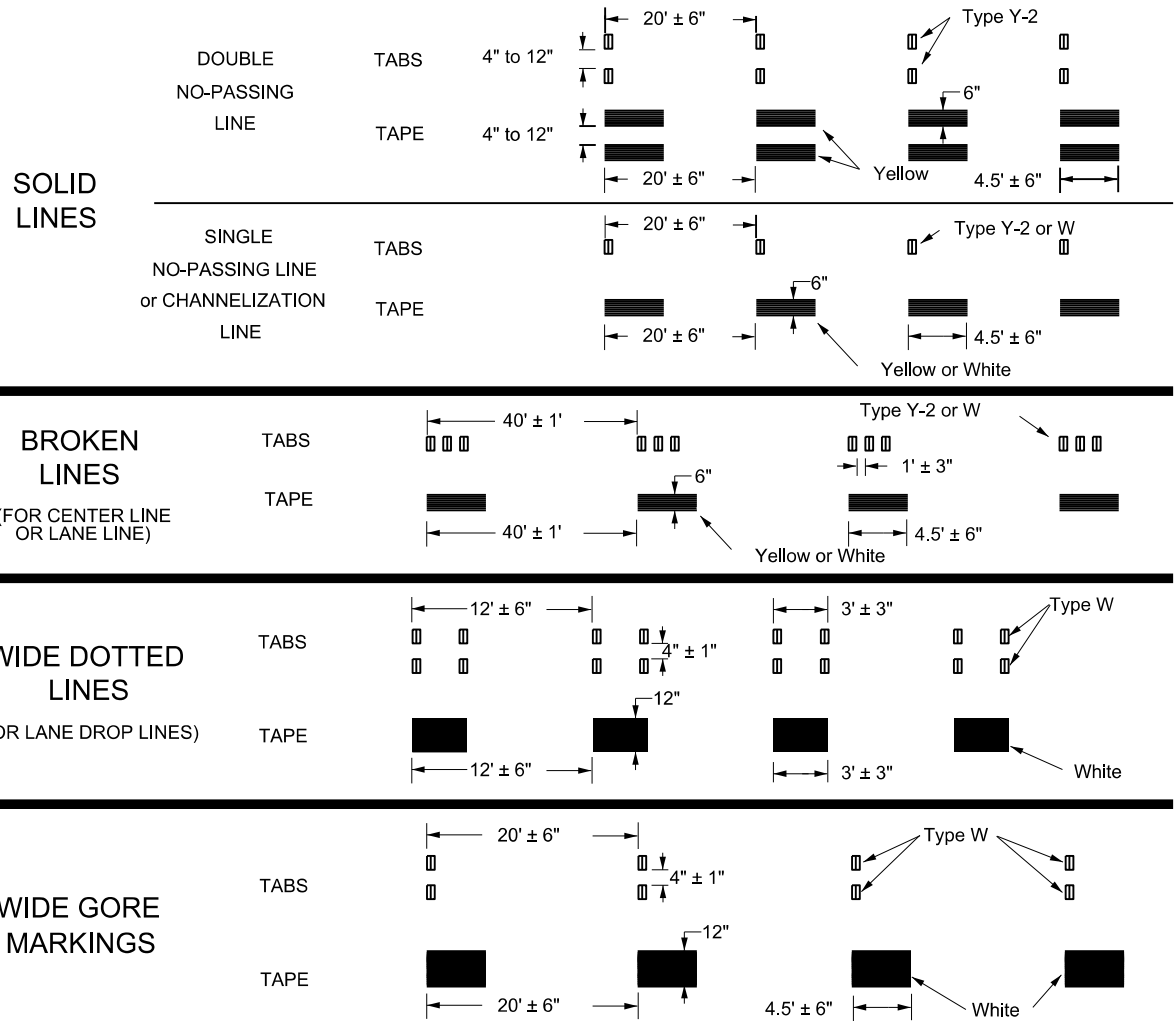
168

DATE:
 FILE:

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or for the accuracy of the information contained herein.

DATE: 2/12/2024 7:57:16 AM
 FILE: p:\w\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan_Sets\14_SH 285 Standard\WZ(STPM)-23.dgn

WORK ZONE SHORT TERM PAVEMENT MARKINGS DETAILS



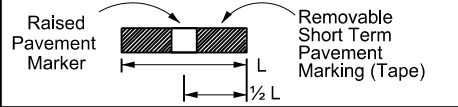
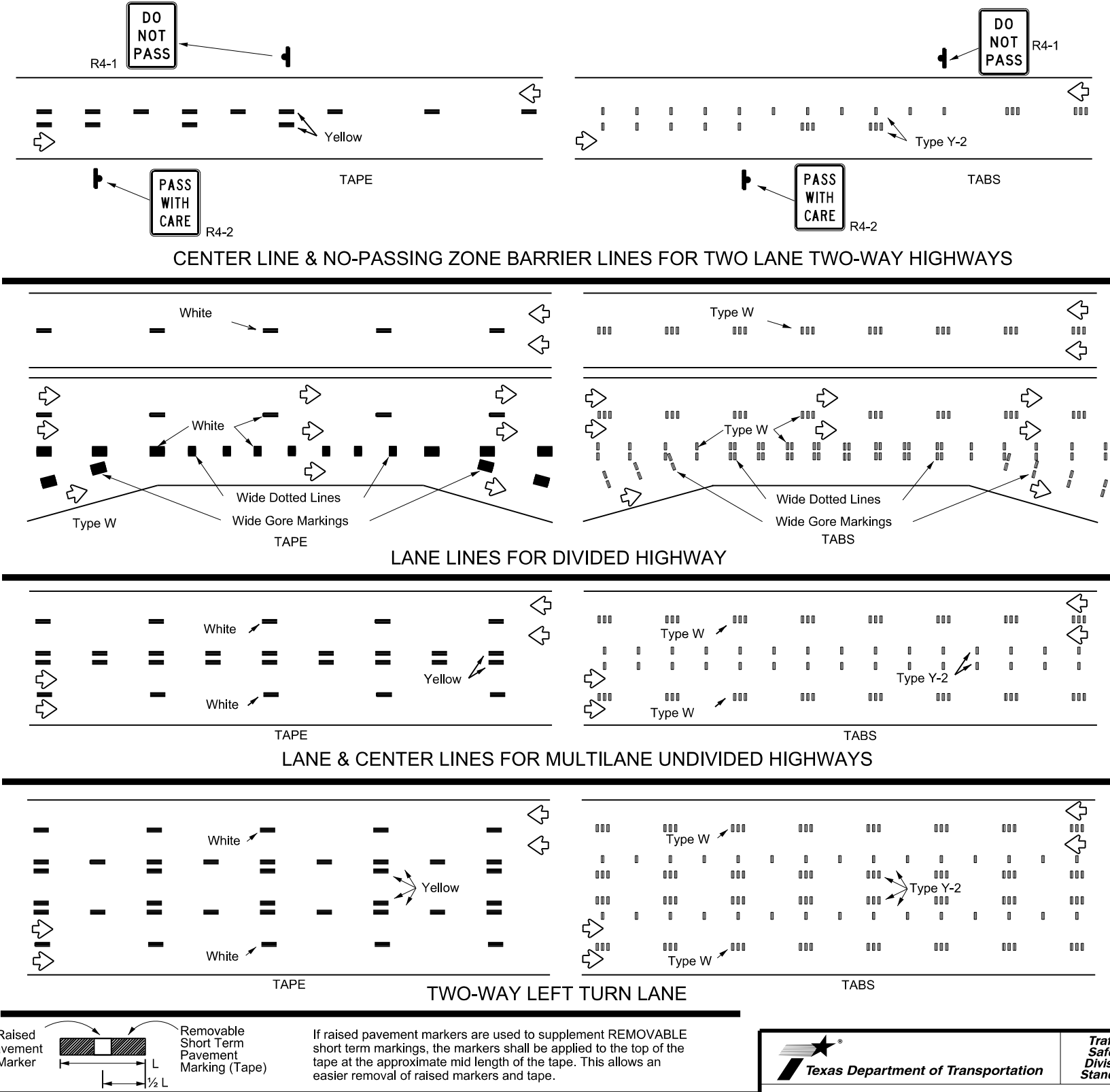
NOTES:

- Short term pavement markings may be prefabricated markings (stick down tape) or temporary flexible reflective roadway marker tabs unless otherwise specified elsewhere in plans.
- Short term pavement markings shall NOT be used to simulate edge lines.
- Dimensions indicated on this sheet are typical and approximate. Variations in size and height may occur between markers or devices made by manufacturers, by as much as 1/4 inch, unless otherwise noted.
- Temporary flexible-reflective roadway marker tabs will require normal maintenance replacement when used on roadways with an ADT per lane of up to 7500 vehicles with no more than 10% truck mix. When roadways exceed these values, additional maintenance replacement of devices should be planned.
- No segment of roadway open to traffic shall remain without permanent pavement markings for a period greater than 14 calendar days. The Contractor will be responsible for maintaining short term pavement markings until permanent pavement markings are in place. When the Contractor is responsible for placement of permanent pavement markings, no segment of roadway shall remain without permanent pavement markings for a period greater than 14 calendar days unless weather conditions prohibit placement. Permanent pavement markings shall be placed as soon as weather permits.
- For two lane, two-way roadways, DO NOT PASS signs shall be erected to mark the beginning of sections where passing is prohibited and PASS WITH CARE signs shall be erected to mark the beginning of sections where passing is permitted. Signs shall be in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and may be used to indicate the limits of no-passing zones for up to 14 calendar days. Permanent pavement markings should then be placed.
- For low volume two lane, two-way roadways of 4000 ADT or less, no-passing lines may be omitted when approved by the Engineer. DO NOT PASS and PASS WITH CARE signs shall be erected (see note 6).
- For exit gores where a lane is being dropped place wide gore markings or retroreflective channelizing devices to guide motorist through the exit. If channelizing devices are to be used it should be noted elsewhere in the plans. One piece cones are not allowed for this purpose.

TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS (TABS)

- Temporary flexible-reflective roadway marker tabs detailed on this sheet will be designated Type Y-2 (two amber reflective surfaces with yellow body); Type Y (one amber reflective surface with yellow body); and Type W (one white or silver reflective surface with white body). Additional details may be found on BC(11).
- Tabs shall meet requirements of Departmental Material Specification DMS-8242.
- When dry, tabs shall be visible for a minimum distance of 200 feet during normal daylight hours and when illuminated by automobile low-beam head light at night, unless sight distance is restricted by roadway geometrics.
- No two consecutive tabs nor four tabs per 1000 feet of line shall be missing or fail to meet the visual performance requirements of Note 3.

WORK ZONE SHORT TERM PAVEMENT MARKINGS PATTERNS



If raised pavement markers are used to supplement REMOVABLE short term markings, the markers shall be applied to the top of the tape at the approximate mid length of the tape. This allows an easier removal of raised markers and tape.

PREFABRICATED PAVEMENT MARKINGS

- Temporary Removable Prefabricated Pavement Markings shall meet the requirements of DMS-8241.
- Non-removable Prefabricated Pavement Markings shall meet the requirements of either DMS-8240 "Permanent Prefabricated Pavement Markings" or DMS-8243 "Temporary Construction-Grade Prefabricated Pavement Markings."

RAISED PAVEMENT MARKERS

- All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and DMS-4200.

DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS) & MATERIAL PRODUCER LISTS (MPL)

- DMSs referenced above can be found along with embedded links to their respective MPLs at the following website:

http://www.txdot.gov/business/contractors_consultants/material_specifications/default.htm



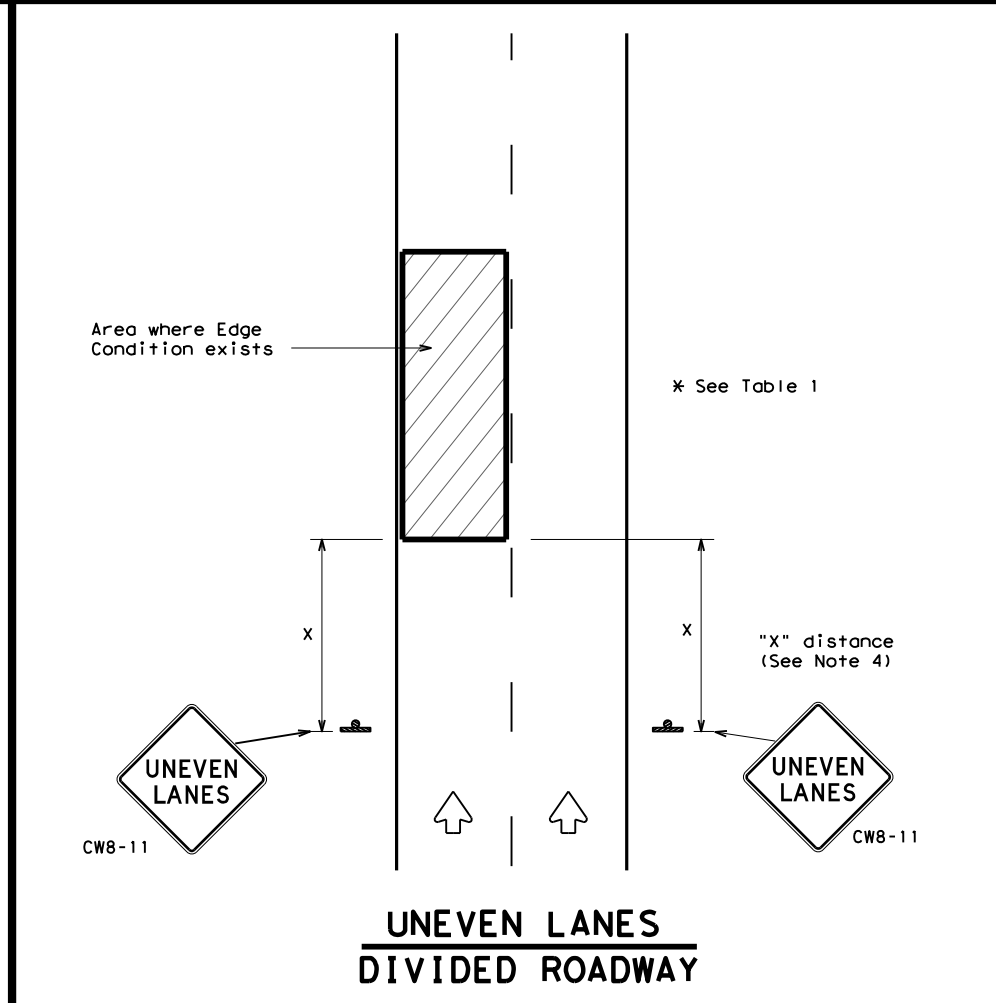
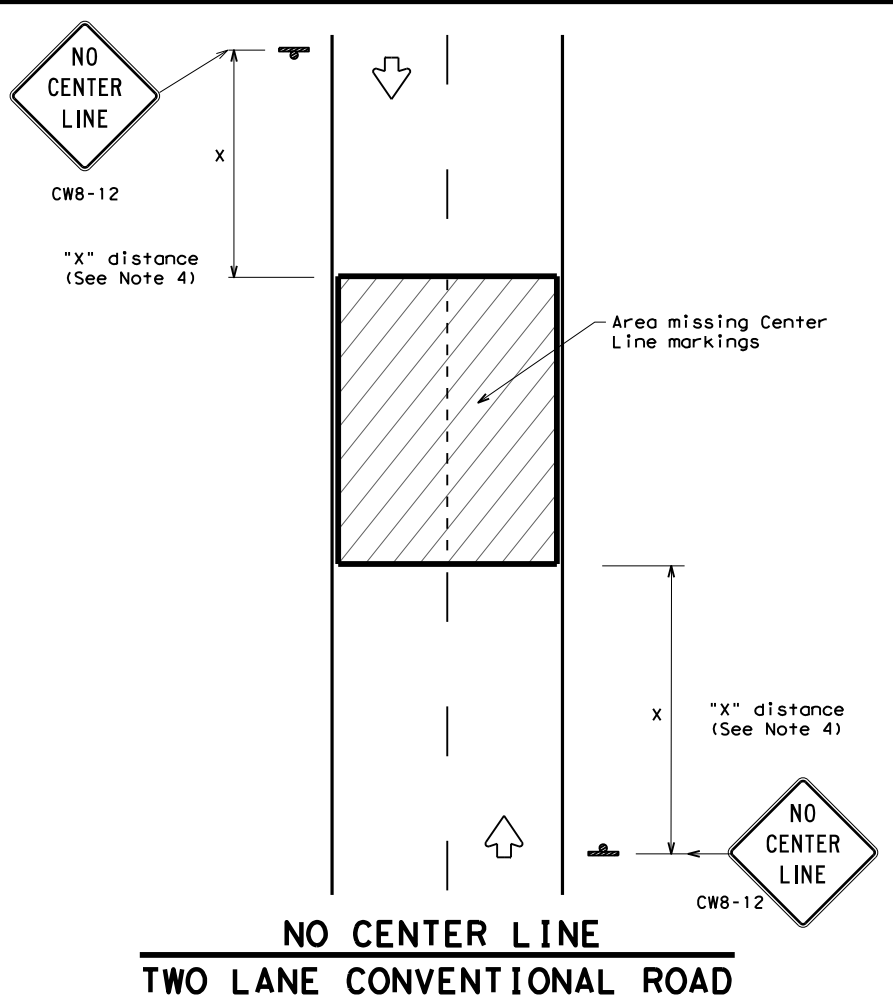
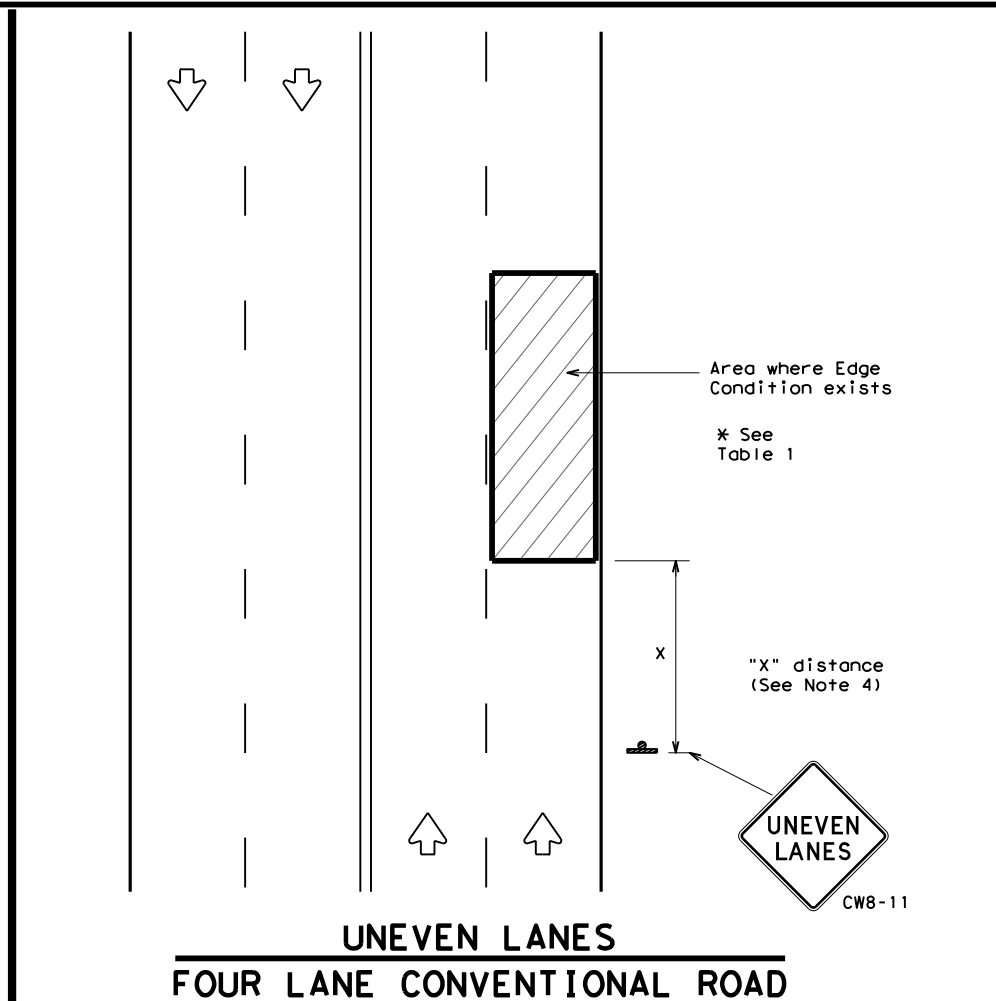
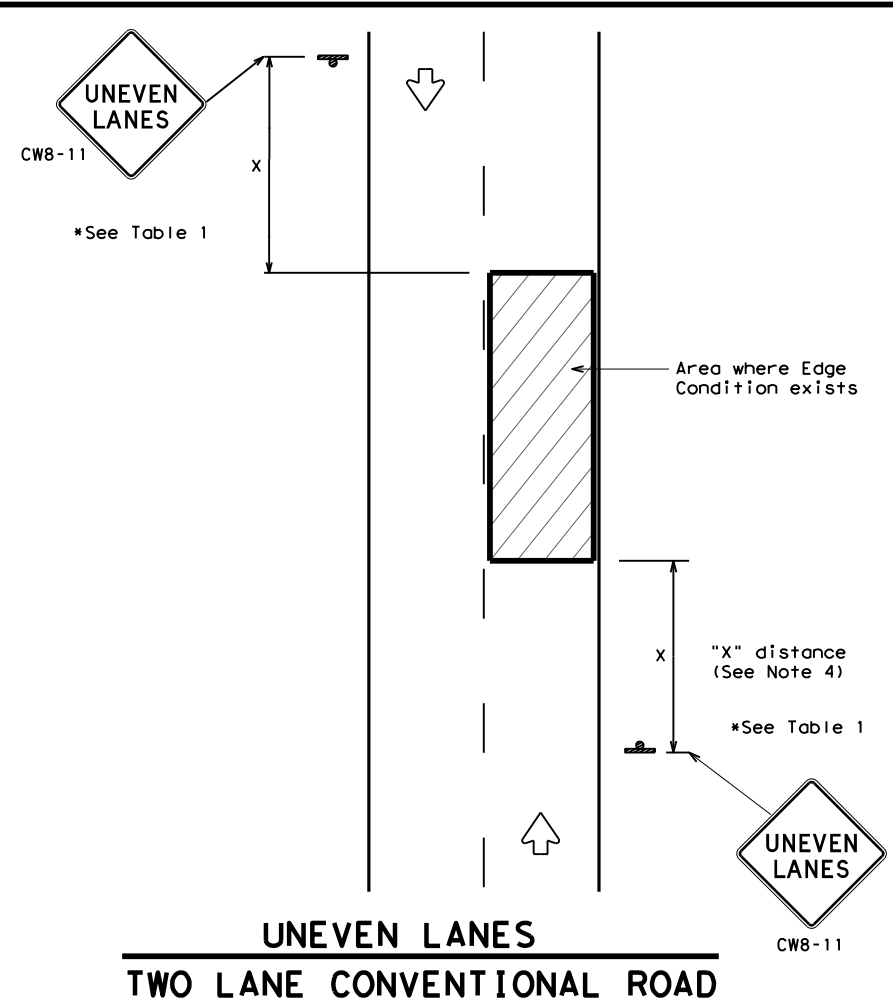
WORK ZONE SHORT TERM PAVEMENT MARKINGS

WZ(STPM)-23

FILE: wzsstpm-23.dgn	DWG: 0482	SECT: 01	JOB: 036, etc.	HIGHWAY: SH 285
© TxDOT February 2023	REVISIONS: 4-92 7-13, 1-97 2-23, 3-03	DIST: PHR	COUNTY: JIM HOGG	SHEET NO.: 157

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of any information derived from this standard. The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of any information derived from this standard.

DATE: 2/12/2024 7:57:20 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\0482\01\036\etc\WZ(UL)-13.dgn



DEPARTMENTAL MATERIAL SPECIFICATIONS	
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240
TEMPORARY (REMOVABLE) PREFABRICATED PAVEMENT MARKINGS	DMS-8241
SIGN FACE MATERIALS	DMS-8300

COLOR	USAGE	SHEETING MATERIAL
ORANGE	BACKGROUND	TYPE B _{FL} OR TYPE C _{FL} SHEETING
BLACK	LEGEND & BORDERS	ACRYLIC NON-REFLECTIVE SHEETING

GENERAL NOTES

- If spalling or holes occur, ROUGH ROAD (CW8-8) signs should be placed in advance of the condition and be repeated every two miles where the condition persists.
- UNEVEN LANES (CW8-11) signs shall be installed in advance of the condition and repeated every mile. Signs installed along the uneven lane condition may be supplemented with the NEXT XX MILES (CW7-3aP) plaque or Advisory Speed (CW13-1P) plaque.
- NO CENTER LINE (CW8-12) signs and temporary pavement markings as per the WZ(STPM) standard shall be installed if yellow centerlines separating two way traffic are obscured or obliterated. Repeat NO CENTER LINE signs every two miles where the center line markings are not in place. The signs and markings shall remain in place until permanent pavement markings are installed.
- Signs shall be spaced at the distances recommended as per BC standards.
- Additional signs may be required as directed by the Engineer. Signs shall remain in place until final surface is applied. Signs shall be considered subsidiary to Item 502 "BARRICADES, SIGNS AND TRAFFIC HANDLING."
- Signs shall be fabricated and mounted on supports as shown on the BC standards and/or listed on the "Compliant Work Zone Traffic Control Devices" list.
- Short term markings shall not be used to simulate edge lines.
- All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition.

TABLE 1		
Edge Condition	Edge Height (D)	* Warning Devices
①	Less than or equal to: 1/4" (maximum-planing) 1/2" (typical-overlay)	Sign: CW8-11
②	Less than or equal to 3"	Sign: CW8-11
③	Distance "D" may be a maximum of 3" if uneven lanes with edge condition 2 or 3 are open to traffic after work operations cease. Uneven lanes should not be open to traffic when "D" is greater than 3".	

TRAFFIC CONTROL DURING PLANING, OVERLAY AND LEVELING OPERATIONS ARE SHOWN ELSEWHERE IN THE PLANS.

MINIMUM WARNING SIGN SIZE	
Conventional roads	36" x 36"
Freeways/expressways, divided roadways	48" x 48"



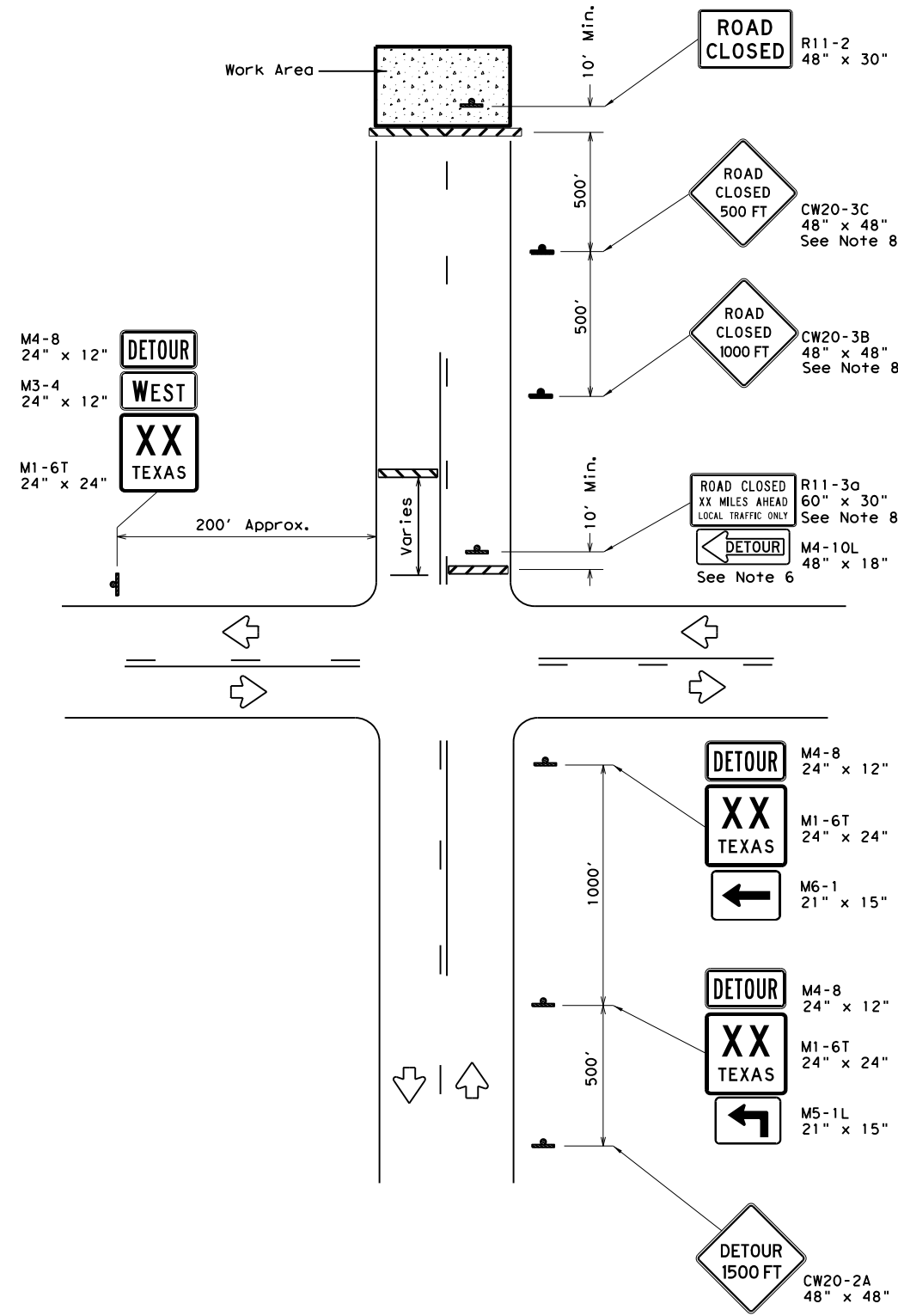
SIGNING FOR UNEVEN LANES

WZ(UL) - 13

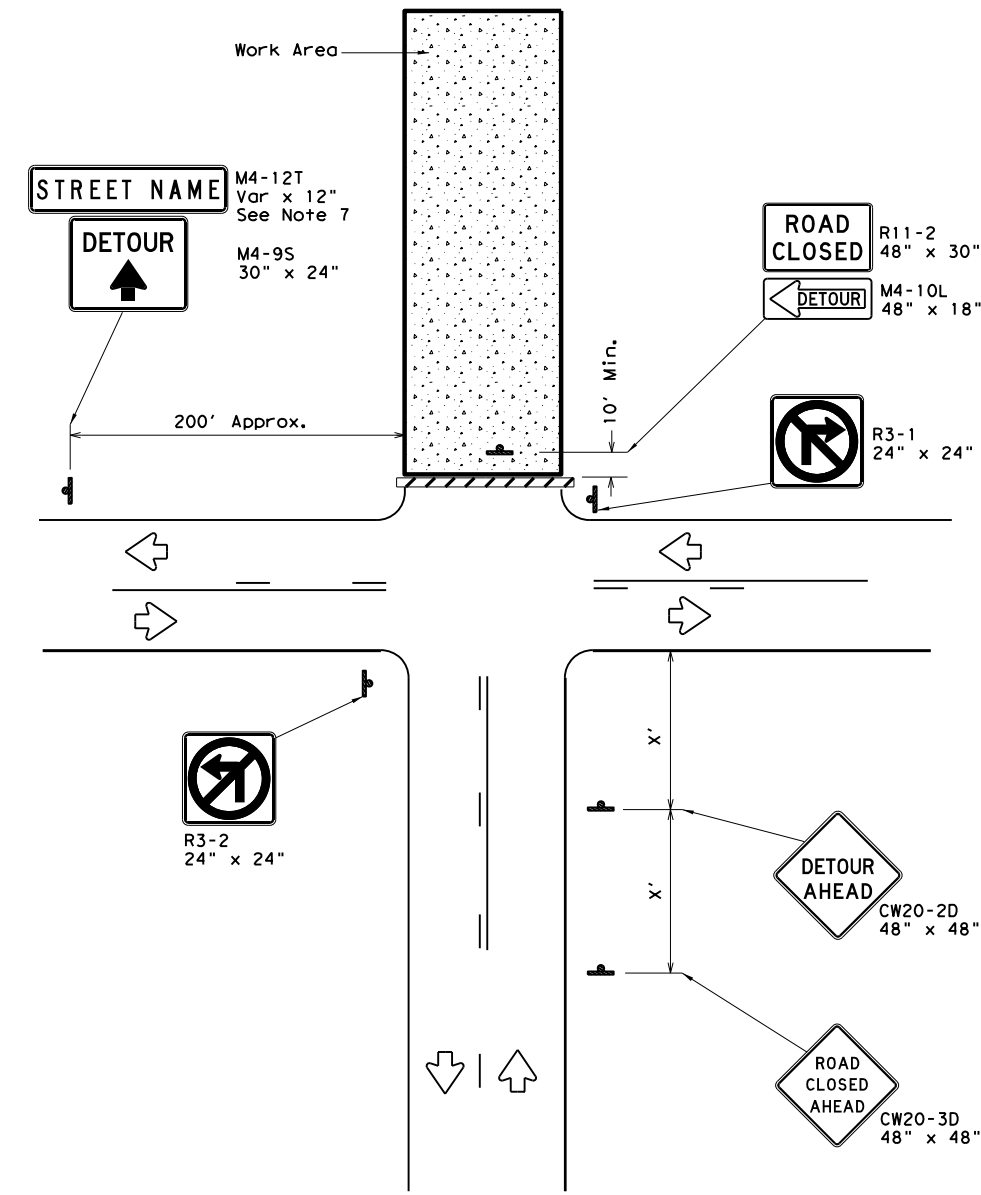
FILE: wzu1-13.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CR: TxDOT
© TxDOT	APRIL 1992	CONT	SECT	JOB
REVISIONS	0482	01	036, etc.	SH 285
8-95	2-98	7-13	DIST	COUNTY
1-97	3-03	PHR	JIM HOGG	SHEET NO. 158

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any units of measurement or for the accuracy of any information contained herein. The user of this standard is advised to consult the current edition of the Texas Engineering Practice Act and the Texas Department of Transportation Standard Specifications for Highway Construction, Section 201.02, for the most current information.

DATE: 2/12/2024 7:57:24 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\010610\010610.dgn



ROAD CLOSURE BEYOND THE INTERSECTION
 Signing for a Numbered Route with an Off-Site Detour



ROAD CLOSURE AT THE INTERSECTION
 Signing for an Un-numbered Route with an Off-Site Detour

LEGEND	
	Type 3 Barricade
	Sign

Posted Speed *	Minimum Sign Spacing "x" Distance
30	120'
35	160'
40	240'
45	320'
50	400'
55	500'
60	600'
65	700'
70	800'
75	900'

* Conventional Roads Only

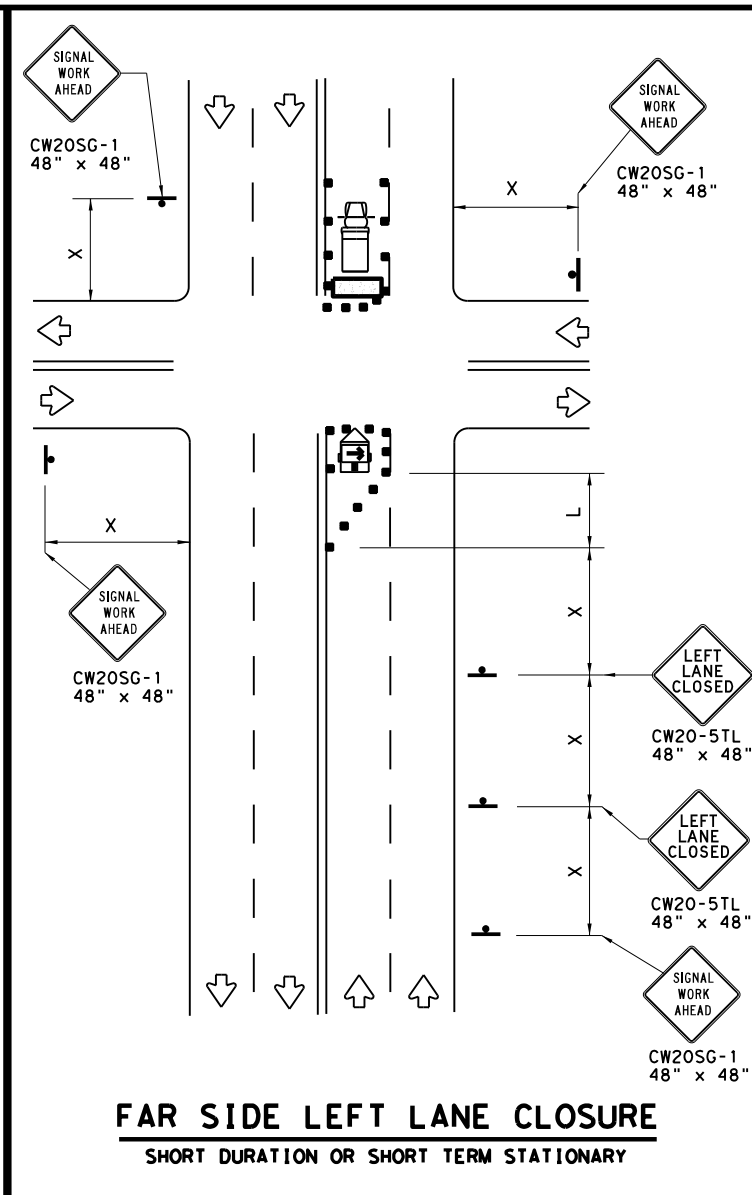
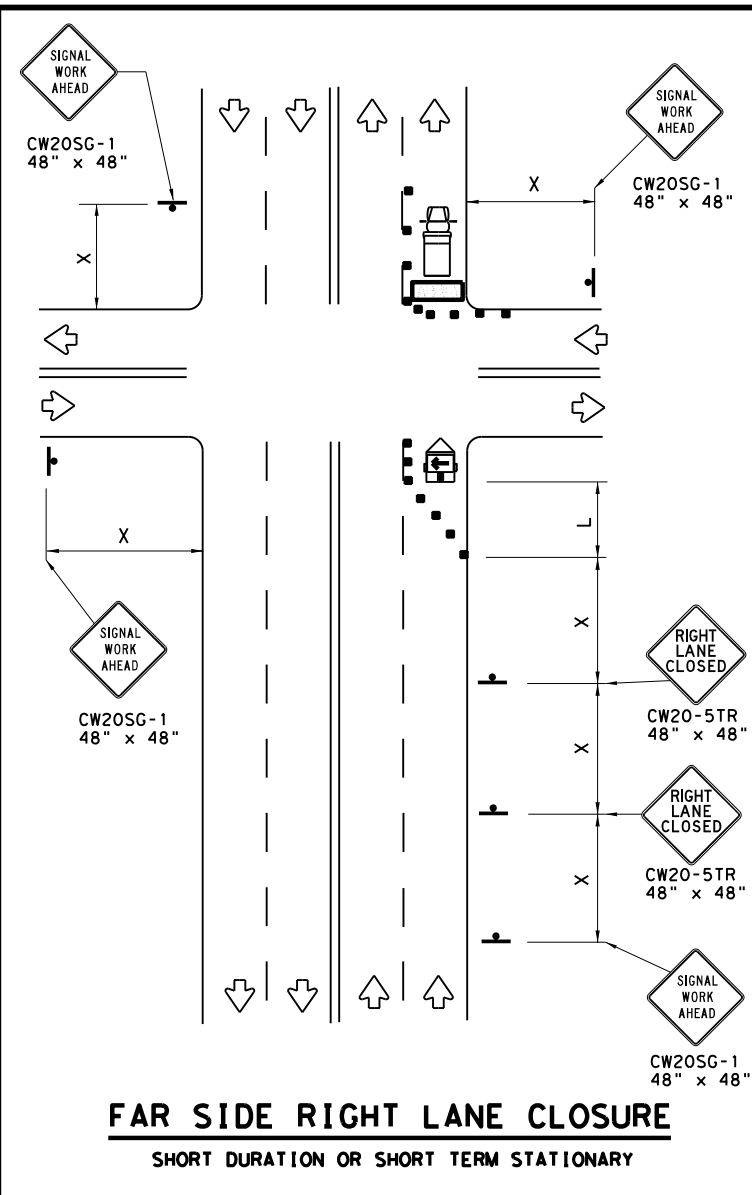
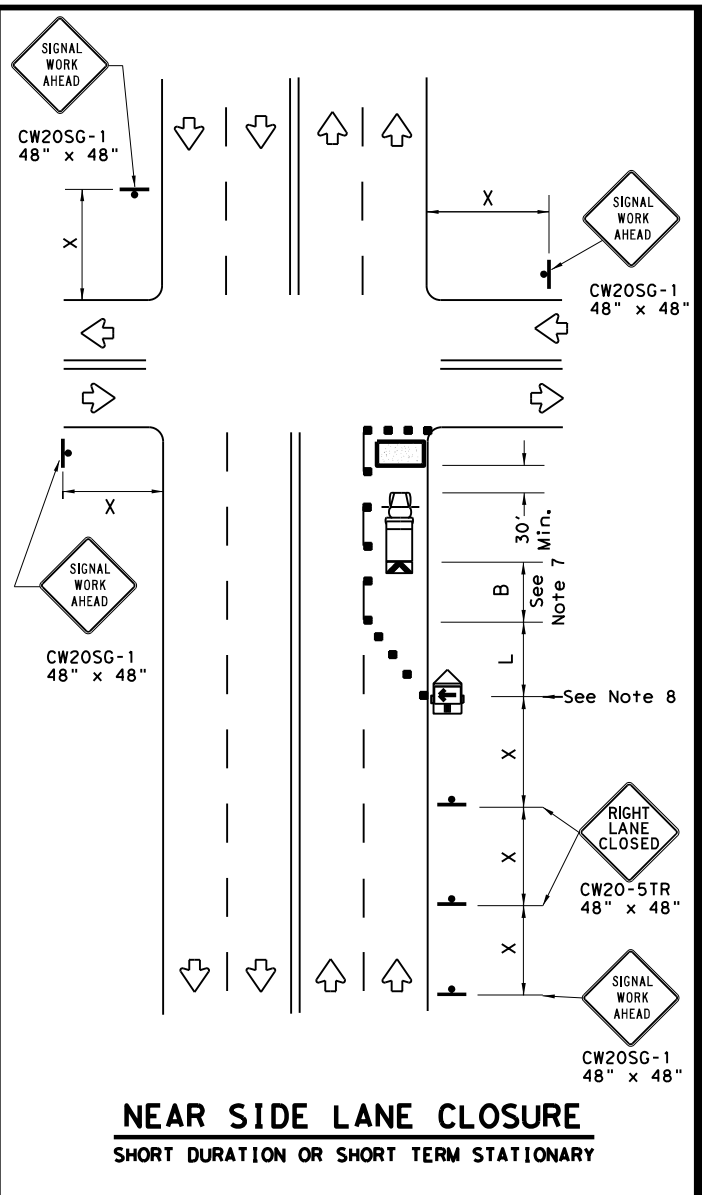
GENERAL NOTES

- This sheet is intended to provide details for temporary work zone road closures. For permanent road closure details see the D&OM standards.
- Barricades used shall meet the requirements shown on Barricade and Construction Standard BC(10) and listed on the Compliant Work Zone Traffic Control Devices list (CWZTCD).
- Stockpiled materials shall not be placed on the traffic side of barricades.
- Barricades at the road closure should extend from pavement edge to pavement edge.
- Detour signing shown is intended to illustrate the type of signing that is appropriate for numbered routes or un-numbered routes as labeled. It does not indicate the full extent of detour signing required. Detour routes should be signed as shown elsewhere in the plans.
- If the road is open for a significant distance beyond the intersection or there are significant origin/destination points beyond the intersection, the signs and barricades at this location should be located at the edge of the traveled way.
- The Street Name (M4-12T) sign is to be placed above the DETOUR (M4-9S) sign.
- For urban areas where there is a shorter distance between the intersection and the actual closure location, the ROAD CLOSED XX MILES AHEAD (R11-3a) sign may be replaced with a ROAD CLOSED TO THRU TRAFFIC (R11-4) sign. If adequate space does not exist between the intersection and the closure a single ROAD CLOSED AHEAD (CW20-3D) sign spaced as per the table above may replace the ROAD CLOSED 1000 FT (CW20-3B) and ROAD CLOSED 500 FT (CW20-3C) signs.
- Signs and barricades shown shall be subsidiary to Item 502. Locations where these details will be required shall be as shown elsewhere in the plans.

		Traffic Operations Division Standard	
WORK ZONE ROAD CLOSURE DETAILS			
WZ (RCD) - 13			
FILE: w2rcd-13.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
© TxDOT August 1995	CONT	SECT	JOB
REVISIONS	0482	01	036, etc.
1-97 4-98 7-13	DIST	COUNTY	SHEET NO.
2-98 3-03	PHR	JIM HOGG	159

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein. The user of this standard is advised to verify the accuracy of the information contained herein. TxDOT is not responsible for any errors or omissions in this standard. TxDOT is not responsible for any damages, including consequential damages, resulting from the use of this standard. TxDOT is not responsible for any claims, damages, losses, or expenses, including reasonable attorneys' fees, incurred by any person using this standard. TxDOT is not responsible for any claims, damages, losses, or expenses, including reasonable attorneys' fees, incurred by any person using this standard. TxDOT is not responsible for any claims, damages, losses, or expenses, including reasonable attorneys' fees, incurred by any person using this standard.

DATE: 2/12/2024 7:57:28 AM
 FILE: \\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201\036\036-13.dgn

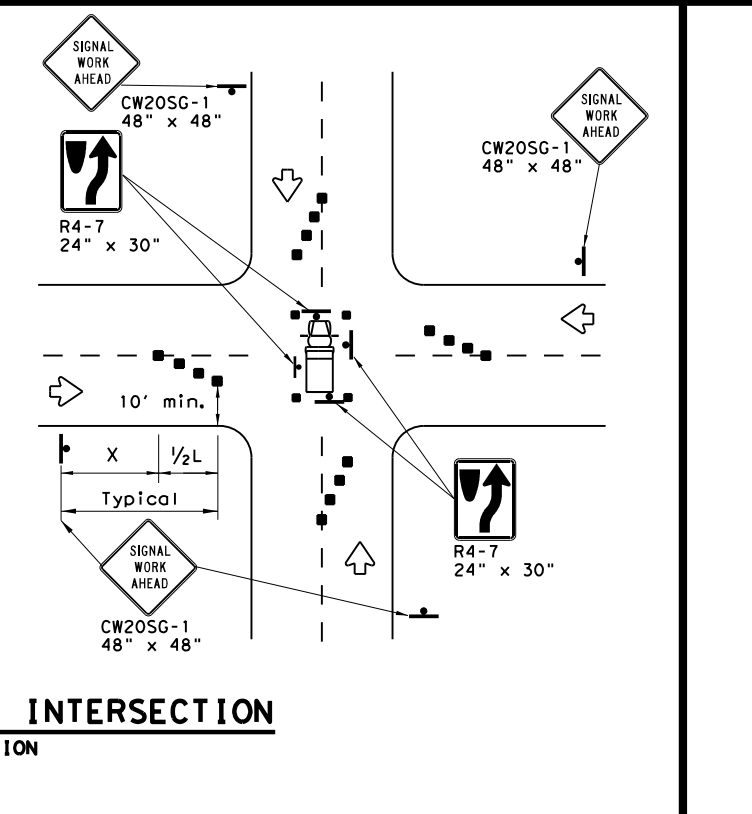
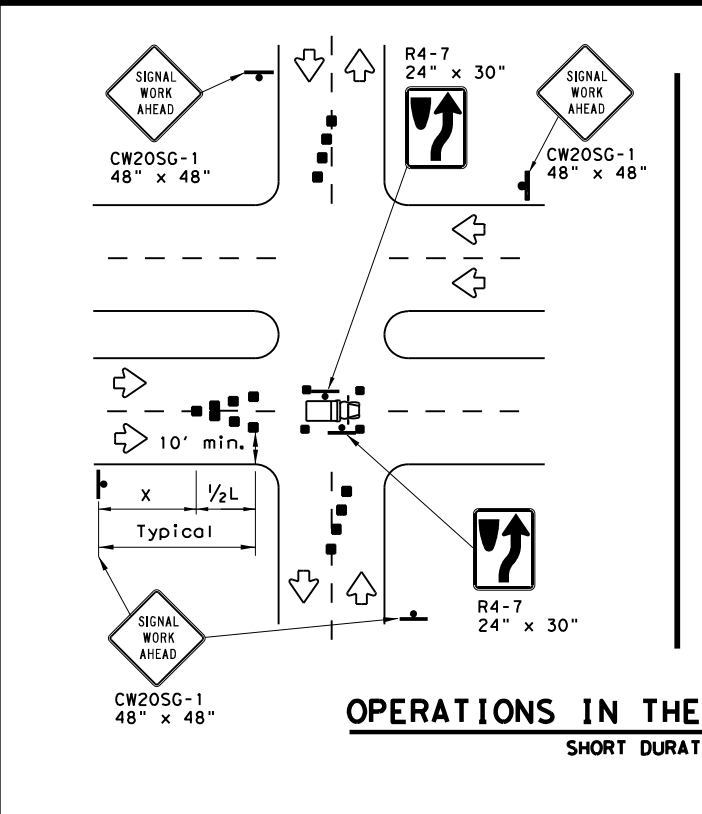


LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

WORKERS IN BUCKET TRUCKS SHALL NOT WORK ABOVE OPEN LANES OF TRAFFIC.



GENERAL NOTES

- The minimum size channelizing device is the 28" cone. 42" Two-piece cones, drums, vertical panels or barricades will be required when the device must be left unattended at night.
- Obstructions or hazards at the work area shall be clearly marked and delineated at all times.
- Flaggers and Flagger Symbol (CW20-7) signs may be required according to field conditions.
- Vehicles parked in roadway shall be equipped with at least two high intensity rotating, flashing, oscillating or strobe type lights.
- High level warning devices (flag trees) may be used at corners of the vehicle.
- When work operations are performed on existing signals, the signals may be placed in flashing red mode when approved by the engineer. If existing signals do not have power, All-Way Stop (R1-1 and R1-3P) signs may be implemented when approved by the engineer.
- For Short-Term Stationary work the buffer space "B" from the above table should be used if field conditions permit. For Short Duration (less than 1 hour) any buffer space provided will enhance the safety of the setup.
- The arrow board at this location may be omitted for Short Duration work if the work vehicle has an arrow board in operation. As an option, the arrow board may be placed at the end of the taper in the closed lane if space is not available at the beginning of the taper.
- Signs and devices for the NEAR SIDE LANE CLOSURE may be altered for a left lane closure by using a LEFT LANE CLOSED (CW20-5TL) and adding channelizing devices on the centerline to protect the work space from opposing traffic.

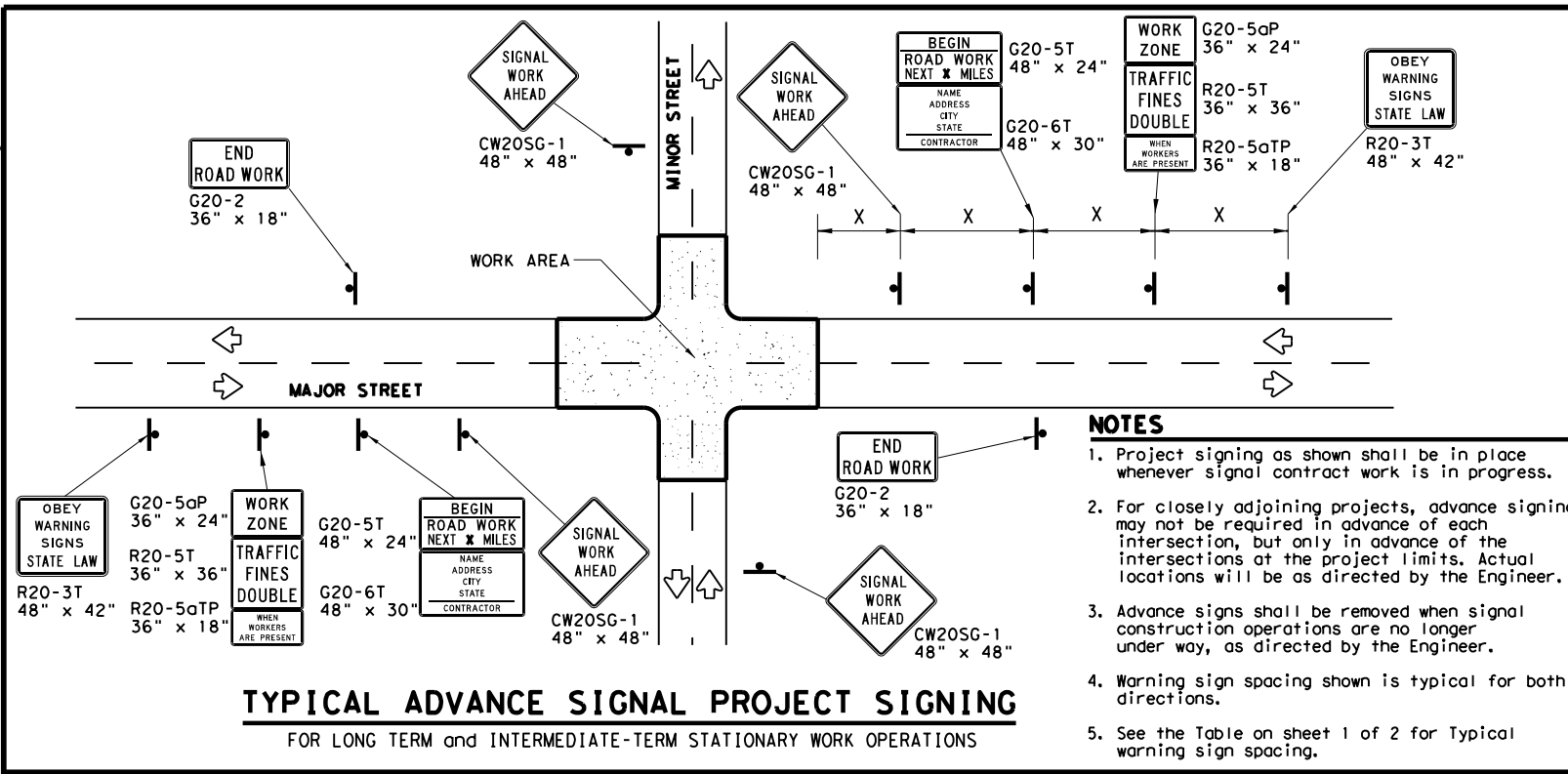
Texas Department of Transportation
 Traffic Operations Division Standard

TRAFFIC SIGNAL WORK TYPICAL DETAILS

WZ (BTS-1) - 13

FILE: wzbts-13.dgn	DN: TxDOT	CR: TxDOT	DW: TxDOT	CK: TxDOT
© TxDOT April 1992	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
2-98 10-99 7-13	DIST	COUNTY	SHEET NO.	
4-98 3-03	PHR	JIM HOGG	160	

No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to metric units. The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to metric units.



TYPICAL ADVANCE SIGNAL PROJECT SIGNING
FOR LONG TERM and INTERMEDIATE-TERM STATIONARY WORK OPERATIONS

- NOTES**
1. Project signing as shown shall be in place whenever signal contract work is in progress.
 2. For closely adjoining projects, advance signing may not be required in advance of each intersection, but only in advance of the intersections at the project limits. Actual locations will be as directed by the Engineer.
 3. Advance signs shall be removed when signal construction operations are no longer under way, as directed by the Engineer.
 4. Warning sign spacing shown is typical for both directions.
 5. See the Table on sheet 1 of 2 for Typical warning sign spacing.

GENERAL NOTES FOR WORK ZONE SIGNS

1. Signs shall be installed and maintained in a straight and plumb condition.
2. Wooden sign posts shall be painted white.
3. Barricades shall NOT be used as sign supports.
4. Nails shall NOT be used to attach signs to any support.
5. All signs shall be installed in accordance with the plans or as directed by the Engineer.
6. The Contractor shall furnish the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD).
7. The Contractor shall furnish sign supports and substrates listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCD), installed as per the manufacturer's recommendations.
8. Temporary signs that have damaged or cracked substrates and/or damaged or marred reflective sheeting shall be replaced as directed by the Engineer.
9. Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1".
10. Damaged wood posts shall be replaced. Splicing wood posts will not be allowed.

DURATION OF WORK

1. Work zone durations are defined in Part 6, Section 60.02 of the Texas Manual on Uniform Traffic Control Devices (TMUTCD).

SIGN MOUNTING HEIGHT

1. Sign height of Long-term/Intermediate-term warning signs shall be as shown on Figure 6F-1 of the TMUTCD.
2. Sign height of Short-term/Short Duration warning signs shall be as shown on Figure 6F-2 of the TMUTCD.
3. Regulatory signs shall be mounted at least 7 feet, but not more than 9 feet, above the paved surface regardless of work duration.

REMOVING OR COVERING

1. When sign messages may be confusing or do not apply, the signs shall be removed or completely covered, unless otherwise approved by the Engineer.
2. When signs are covered, the material used shall be opaque, such as heavy mil black plastic, or other materials which will cover the entire sign face and maintain their opaque properties under automobile headlights at night without damaging the sign sheeting. Burlap, or heavy materials such as plywood or aluminum shall not be used to cover signs.
3. Duct tape or other adhesive material shall NOT be affixed to a sign face.
4. Signs and anchor stubs shall be removed and holes back filled upon completion of the work.

REFLECTIVE SHEETING

1. All signs shall be retroreflective and constructed of sheeting meeting the requirements of the DMS and color usage table shown on this sheet.

SIGN SUPPORT WEIGHTS

1. Weights used to keep signs from turning over should be sandbags filled with dry, cohesionless material.
2. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
3. Rock, concrete, iron, steel or other solid objects will not be permitted for use as sign support weights.
4. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
5. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber, such as fire inner tubes, shall not be used.
6. Rubber ballasts designed for channelizing devices should not be used for ballast on portable sign supports. Sign supports designed and manufactured with rubber bases may be used when shown on the CWZTCD list.
7. Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign support.
8. Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

LEGEND

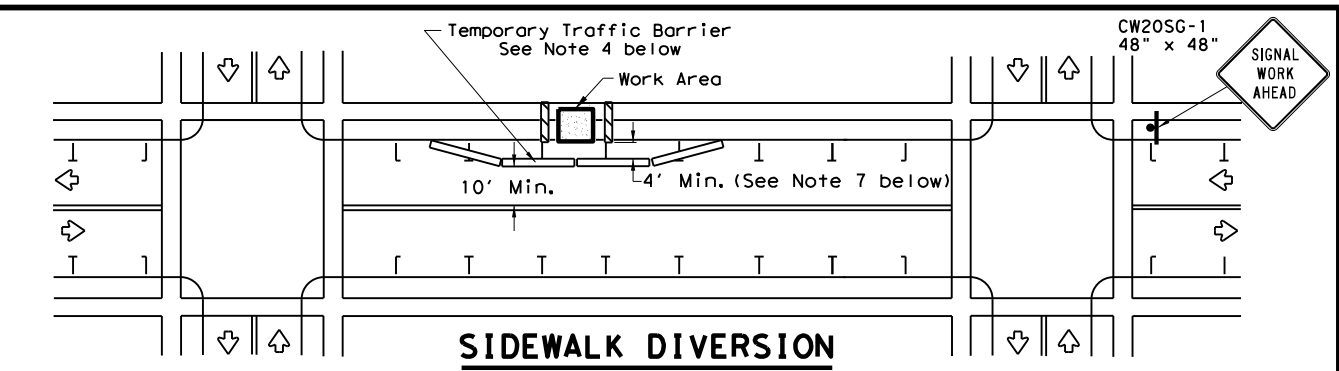
	Sign
	Channelizing Devices
	Type 3 Barricade

DEPARTMENTAL MATERIAL SPECIFICATIONS

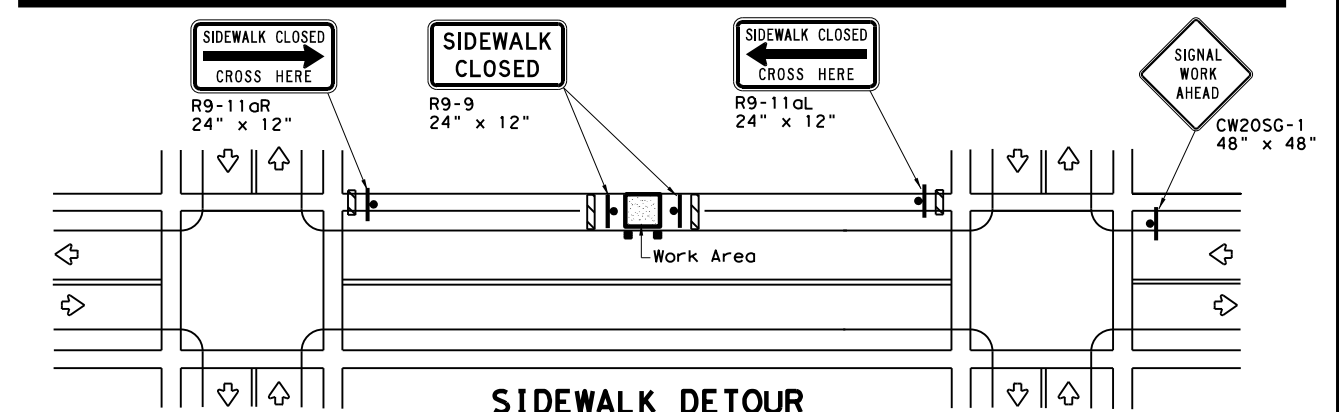
SIGN FACE MATERIALS	DMS-8300
FLEXIBLE ROLL-UP REFLECTIVE SIGNS	DMS-8310

COLOR	USAGE	SHEETING MATERIAL
ORANGE	BACKGROUND	TYPE B _{FL} OR TYPE C _{FL} SHEETING
WHITE	BACKGROUND	TYPE A SHEETING
BLACK	LEGEND & BORDERS	ACRYLIC NON-REFLECTIVE SHEETING

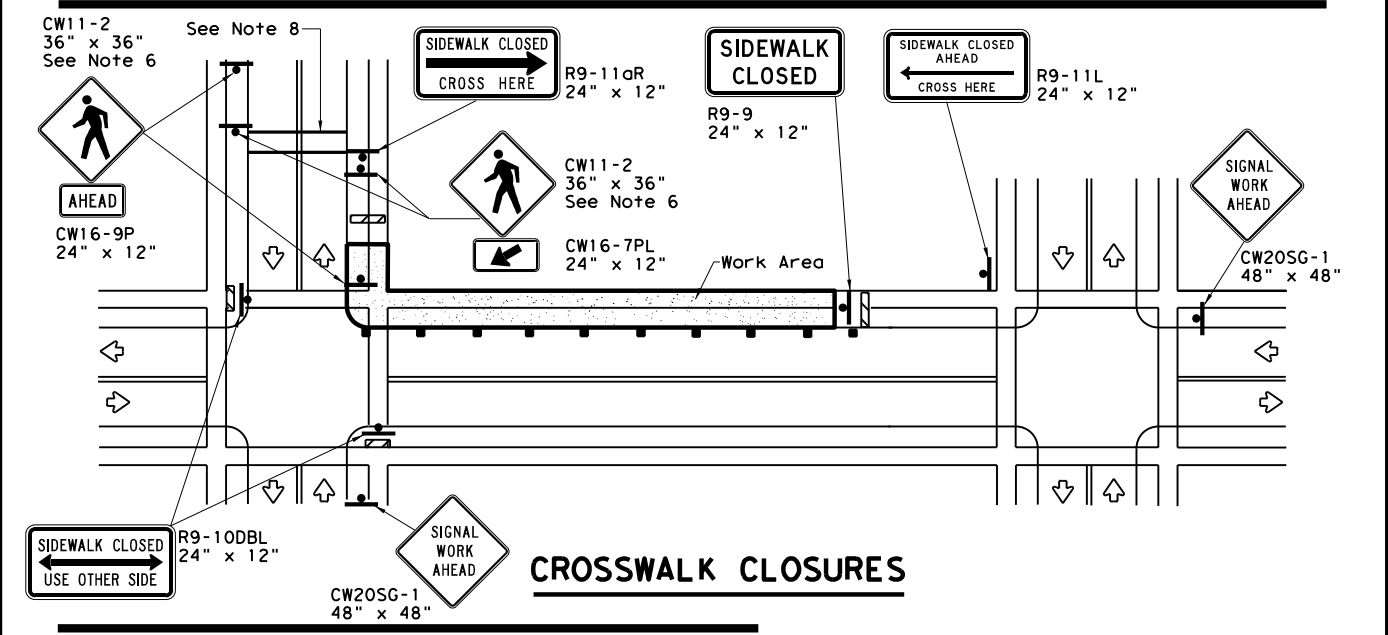
Only pre-qualified products shall be used. A copy of the "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products and their sources and may be found at the following web address:
http://www.txdot.gov/txdot_library/publications/construction.htm



SIDEWALK DIVERSION



SIDEWALK DETOUR



CROSSWALK CLOSURES

PEDESTRIAN CONTROL

1. Holes, trenches or other hazards shall be adequately protected by covering, delineating or surrounding the hazard with orange plastic pedestrian fencing or longitudinal channelizing devices, or as directed by the Engineer.
2. "CROSSWALK CLOSURES" as detailed above will require the Engineer's approval prior to installation.
3. R9 series signs shown may be placed on supports detailed on the BC standards or CWZTCD list, or when fabricated from approved lightweight plastic substrates, they may be mounted on top of a plastic drum at or near the location shown.
4. For speeds less than 45 mph longitudinal channelizing devices may be used instead of traffic barriers when approved by the Engineer. Attenuation of blunt ends and installation of water filled devices shall be as per BC(9) and manufacturer's recommendations.
5. Location of devices are for general guidance. Actual device spacing and location must be field adjusted to meet actual conditions.
6. Where pedestrians with visual disabilities normally use the closed sidewalk Detectable Pedestrian Barricades should be used instead of the Type 3 Barricades shown.
7. The width of existing sidewalk should be maintained if practical.
8. Pavement markings for mid-block crosswalks shall be paid for under the appropriate bid items.
9. When crosswalks or other pedestrian facilities are closed or relocated, temporary facilities shall be detectable and shall include accessibility features consistent with the features present in the existing pedestrian facility.

SHEET 2 OF 2

Texas Department of Transportation
 Traffic Operations Division Standard

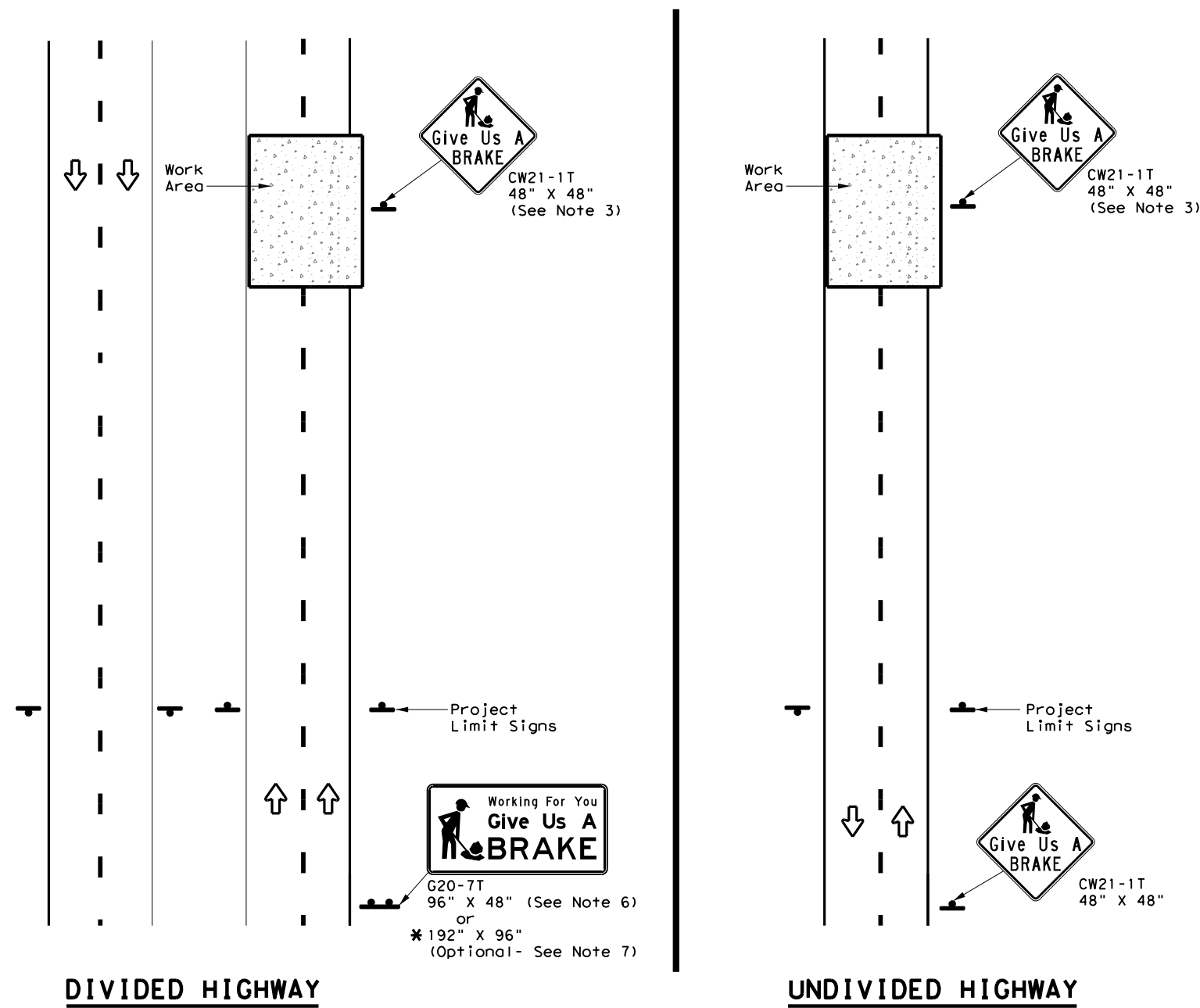
TRAFFIC SIGNAL WORK BARRICADES AND SIGNS

WZ (BTS-2) - 13

FILE: wzbts-13.dgn	DN: TxDOT	CR: TxDOT	OW: TxDOT	CK: TxDOT
© TxDOT April 1992	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
2-98 10-99 7-13	DIST	COUNTY	SHEET NO.	
4-98 3-03	PHR	JIM HOGG	161	

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information from a digital file to a printed format. The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information from a digital file to a printed format.

DATE: 2/12/2024 7:57:33 AM
 FILE: \\txdot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\21-036\21-036.dgn



SIGNS ARE SHOWN FOR ONE DIRECTION OF TRAVEL

* When the optional larger WORKING FOR YOU GIVE US A BRAKE (G20-7T) 192" x 96" sign is required, the locations shall be noted elsewhere in the plans.

SUMMARY OF LARGE SIGNS									
BACKGROUND COLOR	SIGN DESIGNATION	SIGN	SIGN DIMENSIONS	REFLECTIVE SHEETING	SQ FT	GALVANIZED STRUCTURAL STEEL		DRILLED SHAFT	
						Size	(LF)		24" DIA. (LF)
Orange	G20-7T		96" X 48"	Type B _{FL} or C _{FL}	32	▲	▲	▲	▲
Orange	G20-7T		192" X 96"	Type B _{FL} or C _{FL}	128	W8x18	16	17	12

▲ See Note 6 Below

LEGEND	
	Sign
	Large Sign
	Traffic Flow

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PLYWOOD SIGN BLANKS	DMS-7100
ALUMINUM SIGN BLANKS	DMS-7110
SIGN FACE MATERIALS	DMS-8300

COLOR	USAGE	SHEETING MATERIAL
ORANGE	BACKGROUND	TYPE B _{FL} OR TYPE C _{FL}
BLACK	LEGEND & BORDERS	NON-REFLECTIVE ACRYLIC FILM

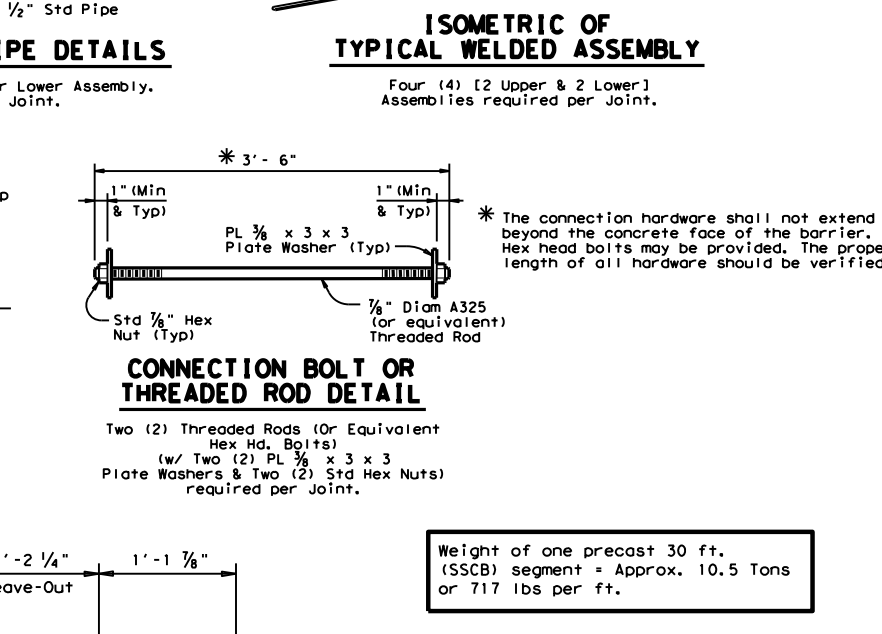
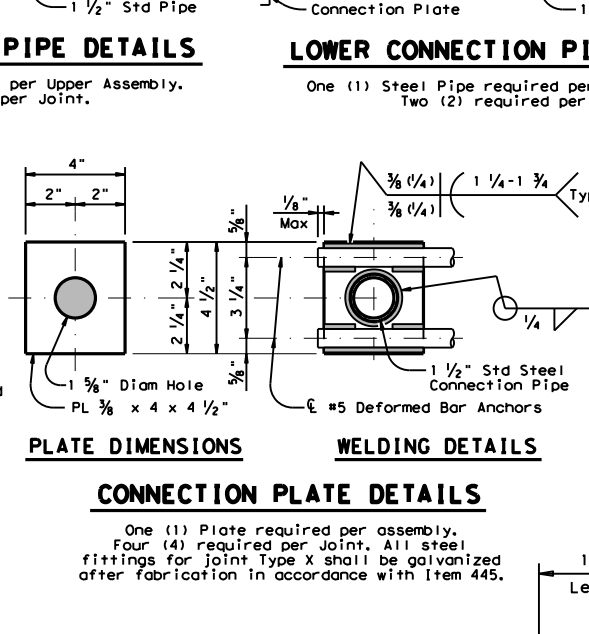
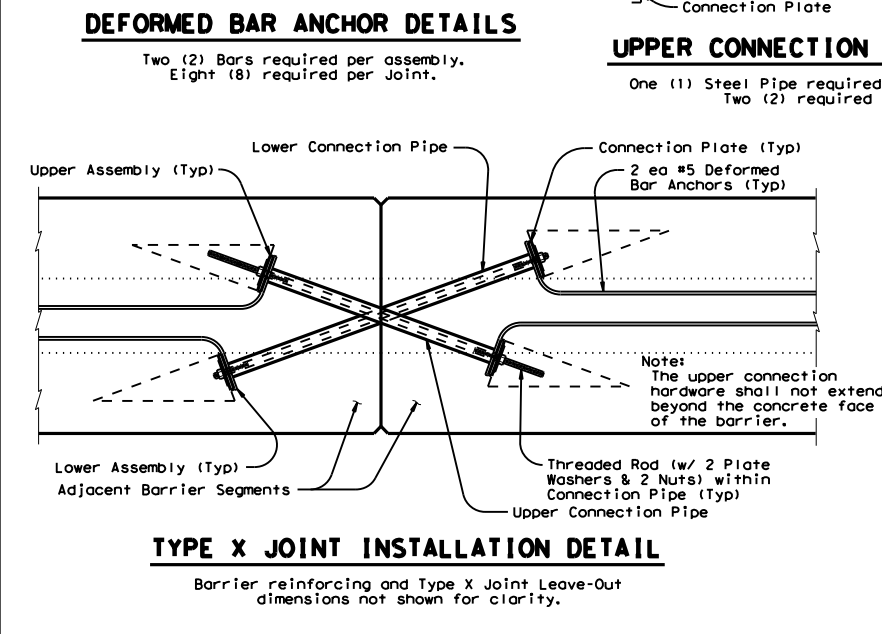
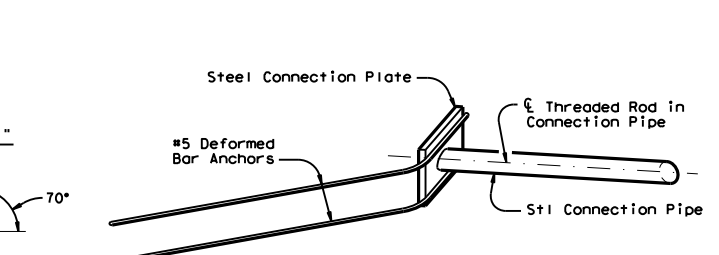
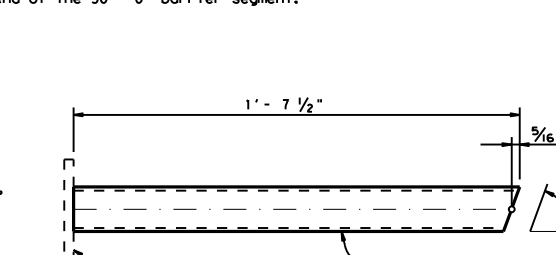
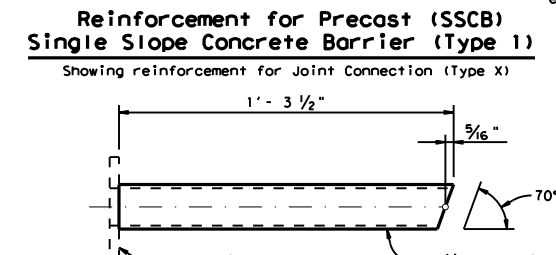
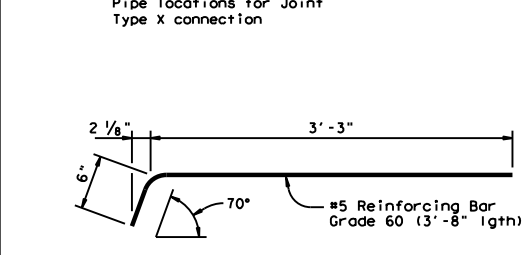
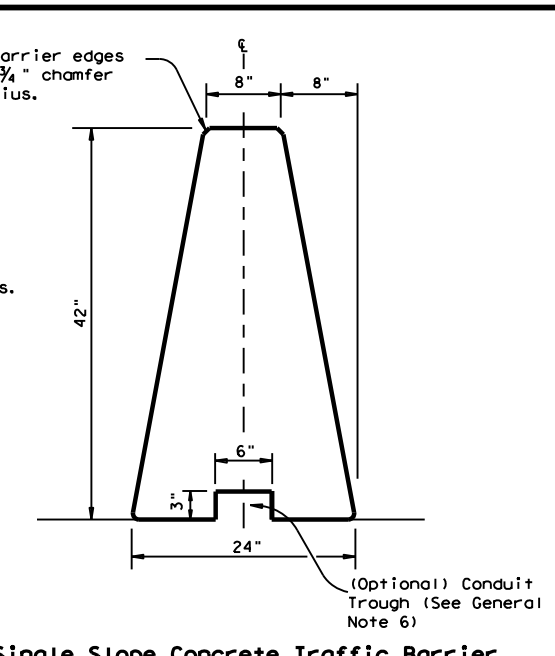
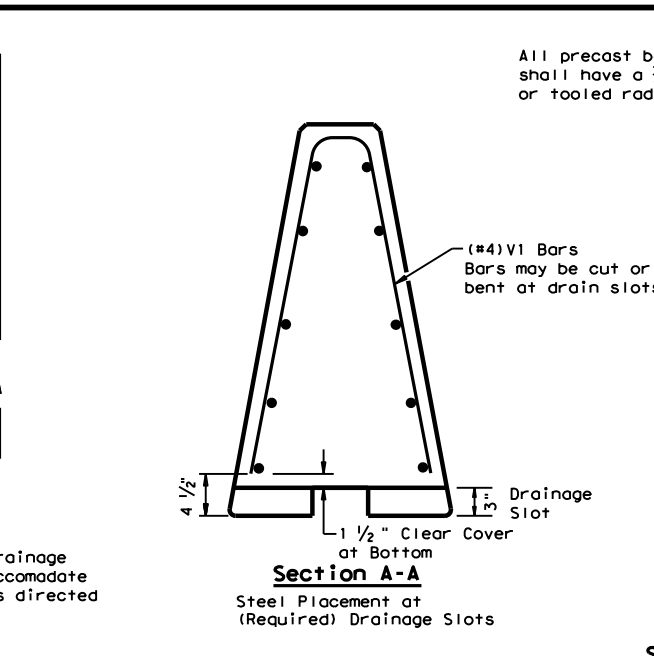
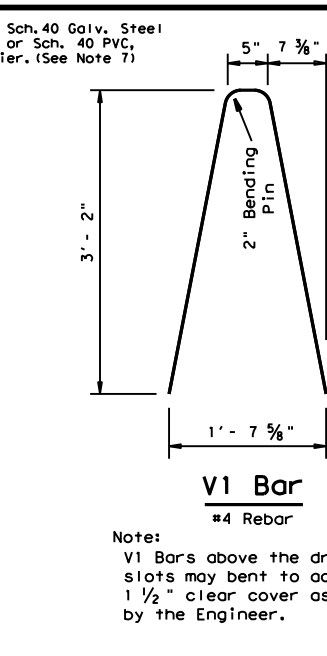
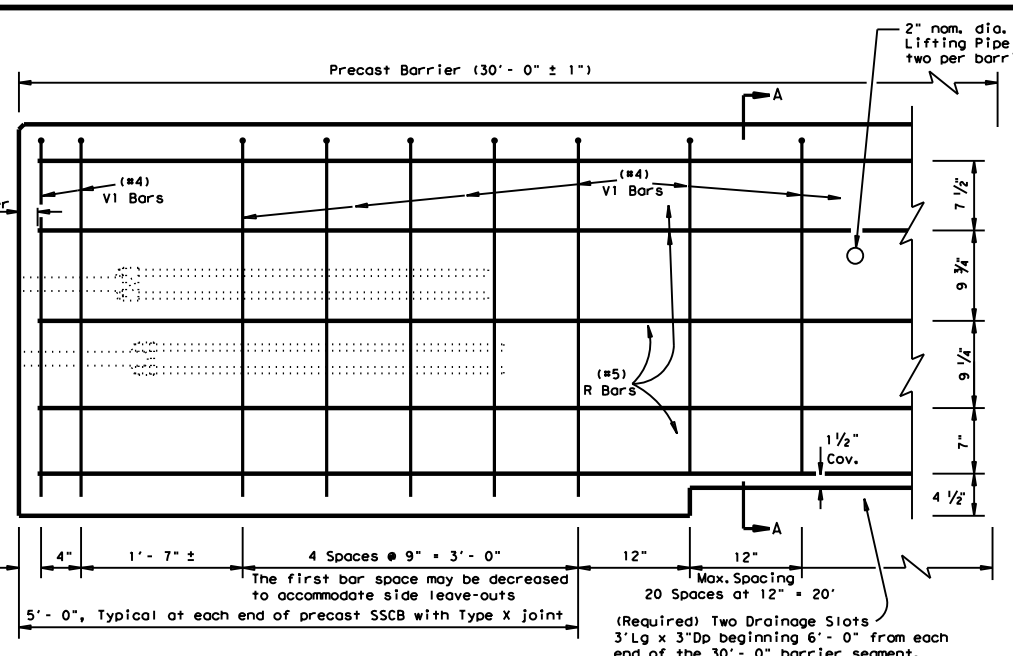
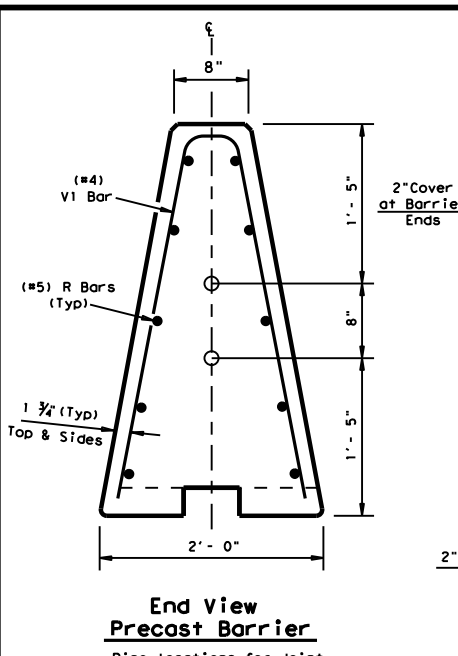
GENERAL NOTES

- See BC and SMD sheets for additional sign support details.
- Sign locations shall be approved by the Engineer.
- For projects more than two miles in length, Give Us a BRAKE signs should be repeated halfway through the project. The Give Us a Brake (CW21-1T) may be used for this purpose.
- Work zone speed limits are sometimes used in conjunction with GIVE US A BRAKE signing. See BC(3) for location and spacing of construction speed zone signing when required.
- Give Us a Brake (CW21-1T) signs and supports shall be considered subsidiary to Item 502, "Barricades, Signs and Traffic Handling."
- The 96" X 48" Working For You Give Us A BRAKE (G20-7T) may use a 1/2" or 5/8" plywood substrate or 0.125" aluminum sheeting substrate and may be supported by two 4" x 6" wood posts with drilled holes for breakaway as per BC(5) and will be subsidiary to Item 502.
- The Working For You Give Us A BRAKE (G20-7T) 192" X 96" sign shall be paid for under the following specification items:
 Item 636 - Aluminum Signs
 Item 647 - Large Roadside Sign Supports and Assemblies.
 Item 416 - Drilled Shaft Foundations
- All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.

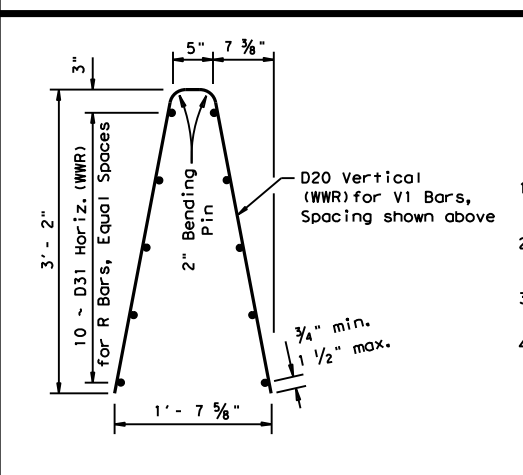
				Traffic Operations Division Standard	
WORK ZONE "GIVE US A BRAKE" SIGNS					
WZ (BRK) - 13					
FILE:	wzbrk-13.dgn	DN:	TxDOT	CK:	TxDOT
©TxDOT	August 1995	CONT	SECT	JOB	HIGHWAY
REVISIONS		0482	01	036, etc.	SH 285
6-96	5-98	7-13	DIST	COUNTY	SHEET NO.
8-96	3-03		PHR	JIM HOGG	162

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

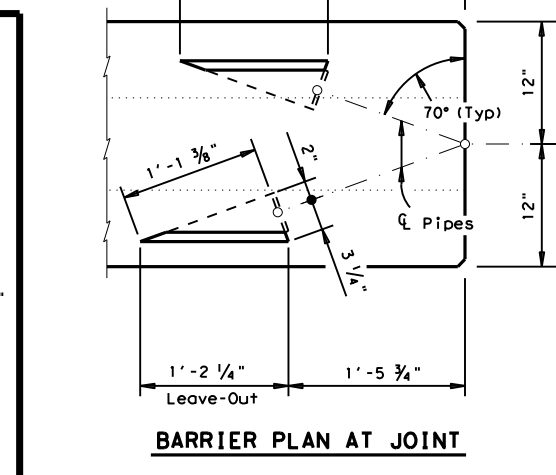
DATE: 2/12/2024
 FILE: \\twdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standards\SSCB210.dgn



- General Notes**
- Concrete shall be Class H with a minimum compressive strength of 3,600 psi.
 - Where used, rebar reinforcement shall be Grade 60 and conform to ASTM A615.
 - Precast barrier length shall be 30 ft. unless otherwise specified on the plans.
 - All precast barrier edges shall have a 3/4 inch chamfer or a tooling radius.
 - All concrete, reinforcement, joint connection systems, grout etc. as shown, are considered as part of the barrier payment.
 - Conduit trough when required shall be shown elsewhere on the plans, or as directed by the Engineer.
 - Regardless of the method of handling, barrier lifting points shall be approx. 7.5 feet from the ends of the barrier. Lifting devices and attachments to barrier sections shall be approved by the Engineer.
 - Surface finishing and grouting (where required) shall be two parts sand and one part cement with enough water to make the mixture plastic. Grouting shall be done in a manner that will assure a smooth surface. Surface finishing shall be considered subsidiary to the various bid items.
 - All steel assemblies shall be galvanized after fabrication in accordance with Item 445, "Galvanizing."



- (WWR) General Notes**
- Deformed Welded Wire Reinforcement (WWR) shall conform to ASTM A497.
 - Welded wire cage may be cut or bent to accommodate the Type X joint connection and drainage slots, as directed by the Engineer.
 - All reinforcement shall comply with Item 440, "Reinforcing Steel."
 - Combinations of reinforcing steel and WWR will be permitted, as directed by the Engineer. The dimension from the end of the barrier section to the first wire shall not exceed 3 inches.



Weight of one precast 30 ft. (SSCB) segment = Approx. 10.5 Tons or 717 lbs per ft.

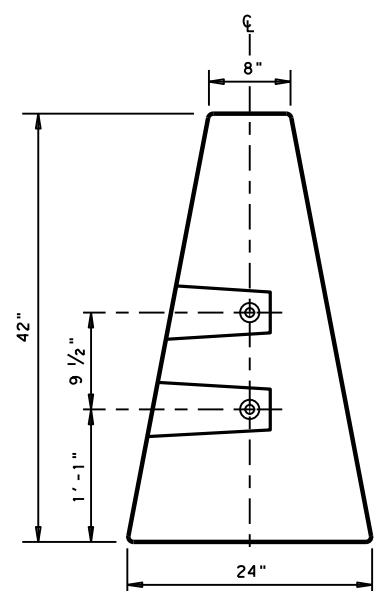
SHEET 1 OF 2

Texas Department of Transportation
 Design Division Standard

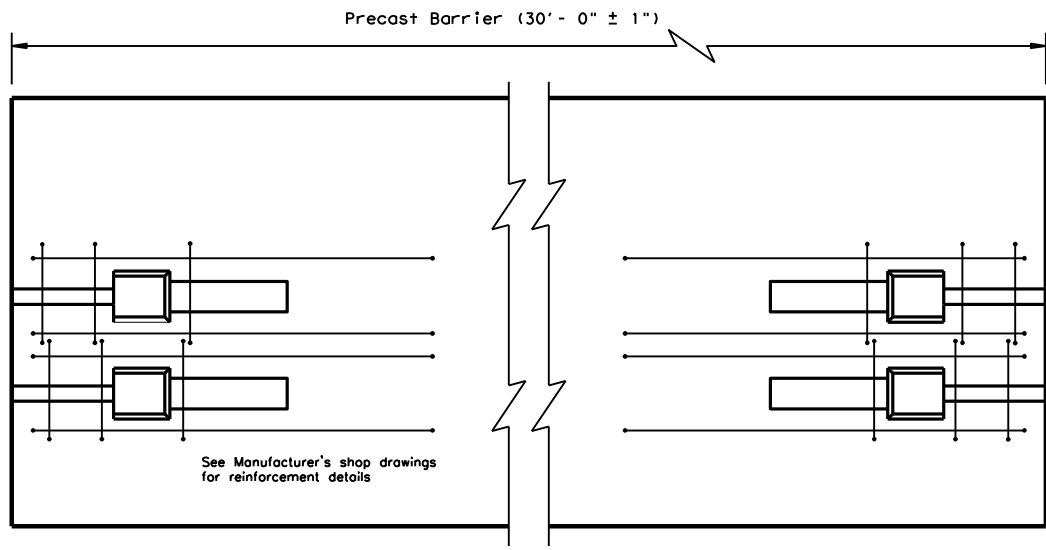
SINGLE SLOPE CONCRETE BARRIER
 PRECAST BARRIER (TYPE 1)
 SSCB (2) - 10

FILE: sscb210.dgn	DN: TxDOT	CR: AM	DW: BD	CK:
© TxDOT December 2010	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.		
PHR	JIM HOGG	163		

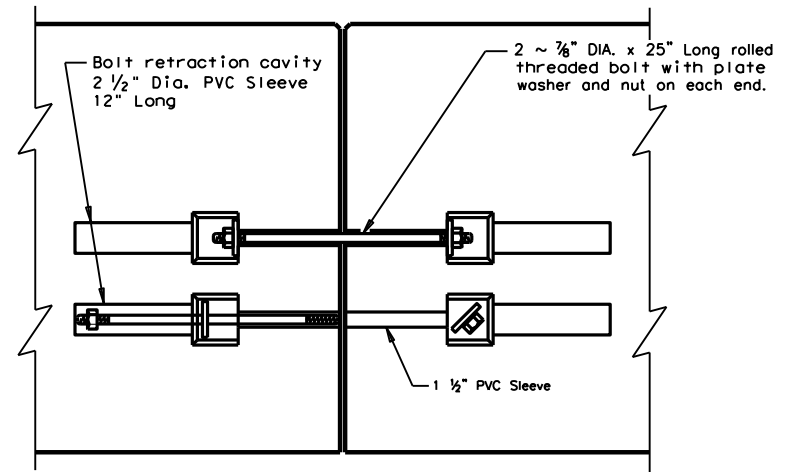
DATE: 2/12/2024
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\SSCB210.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.



END VIEW
 "QUICK-BOLT" POCKET LOCATIONS

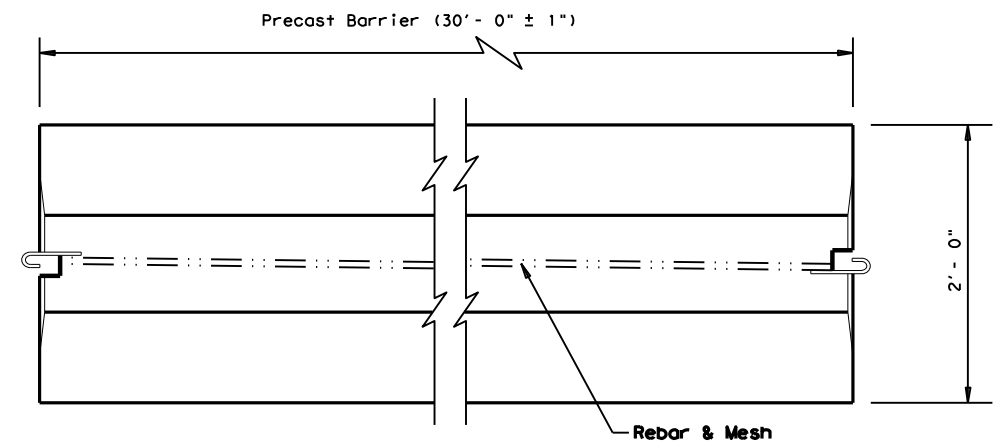


ELEVATION VIEW
 "QUICK-BOLT" (SSCB)
 See Manufacturer's shop drawing for additional details

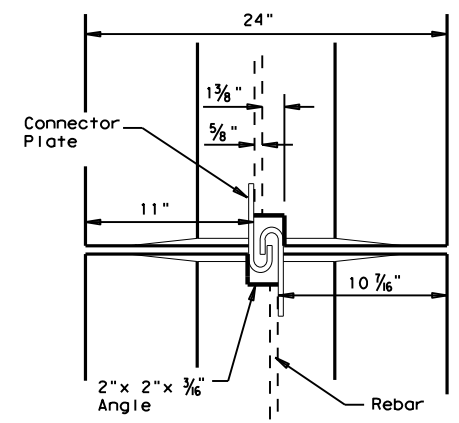


ELEVATION VIEW SHOWING JOINT CONNECTION
 "QUICK-BOLT"

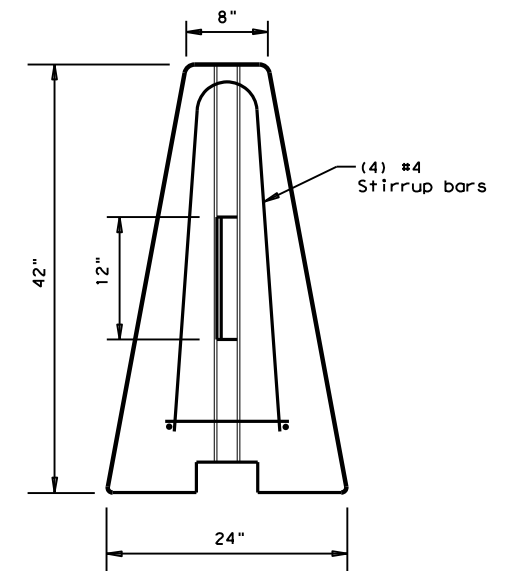
Joint Connection (Type Q)



TOP VIEW
 PRECAST (SSCB) WITH J-J HOOKS
 See Manufacturer's shop drawing for additional details



VIEW FROM ABOVE
 J-J HOOK CONNECTION



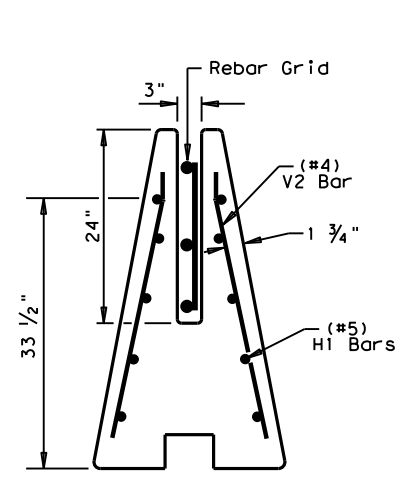
END VIEW

Proprietary Joint Connections (SSCB)

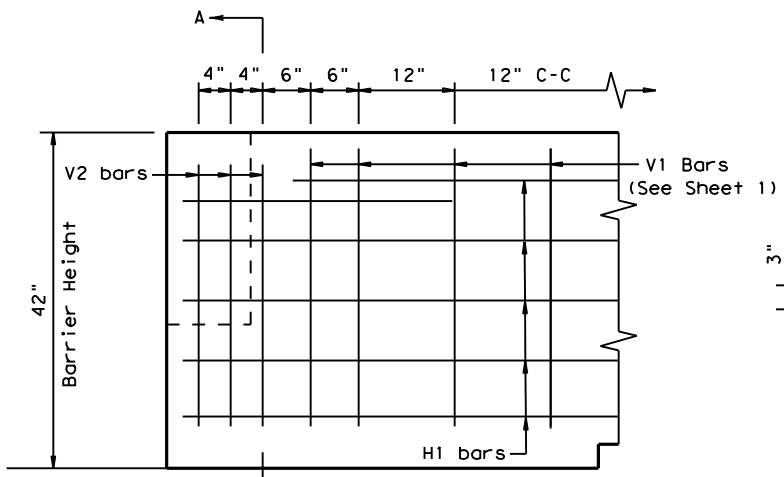
Two proprietary joint connections are acceptable as alternates to the (Type X) connection shown, here on. These joint connections types are:

J-J Hooks by Easi-Set Industries, (800)547-4045
 Quick-Bolt by Bexar Concrete, (210)497-3773

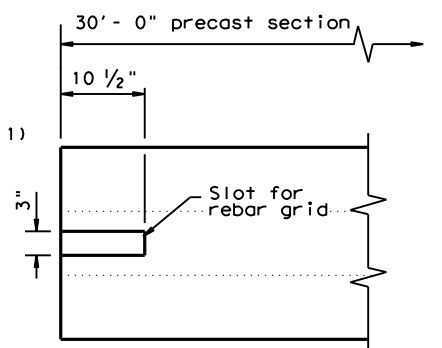
If one of these connection systems are exclusively specified in the plans, prior approval for sole source use must be obtained. Details of the connection components and barrier reinforcement for these systems, will be shown on the manufacturer's shop drawing(s) furnished to the Engineer.



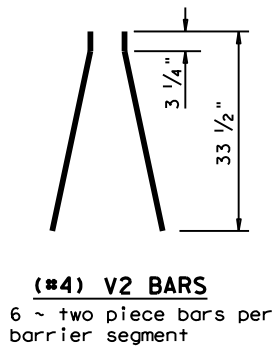
SECTION A-A
 Showing (Type R)
 Rebar Grid



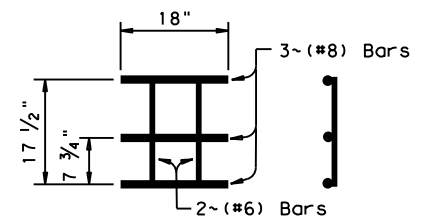
ELEVATION
 V1 Bars (See Sheet 1)



TOP VIEW
 JOINT CONNECTION
 Typical at both ends of barrier segment



(#4) V2 BARS
 6 ~ two piece bars per barrier segment



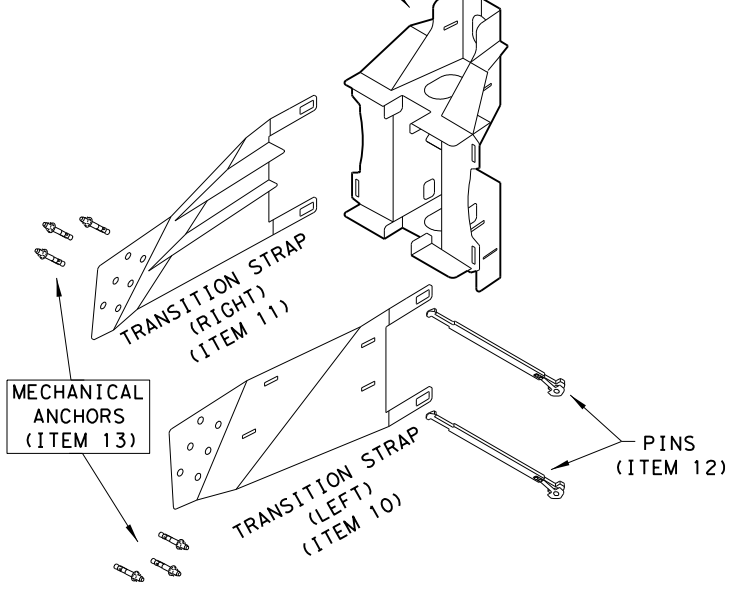
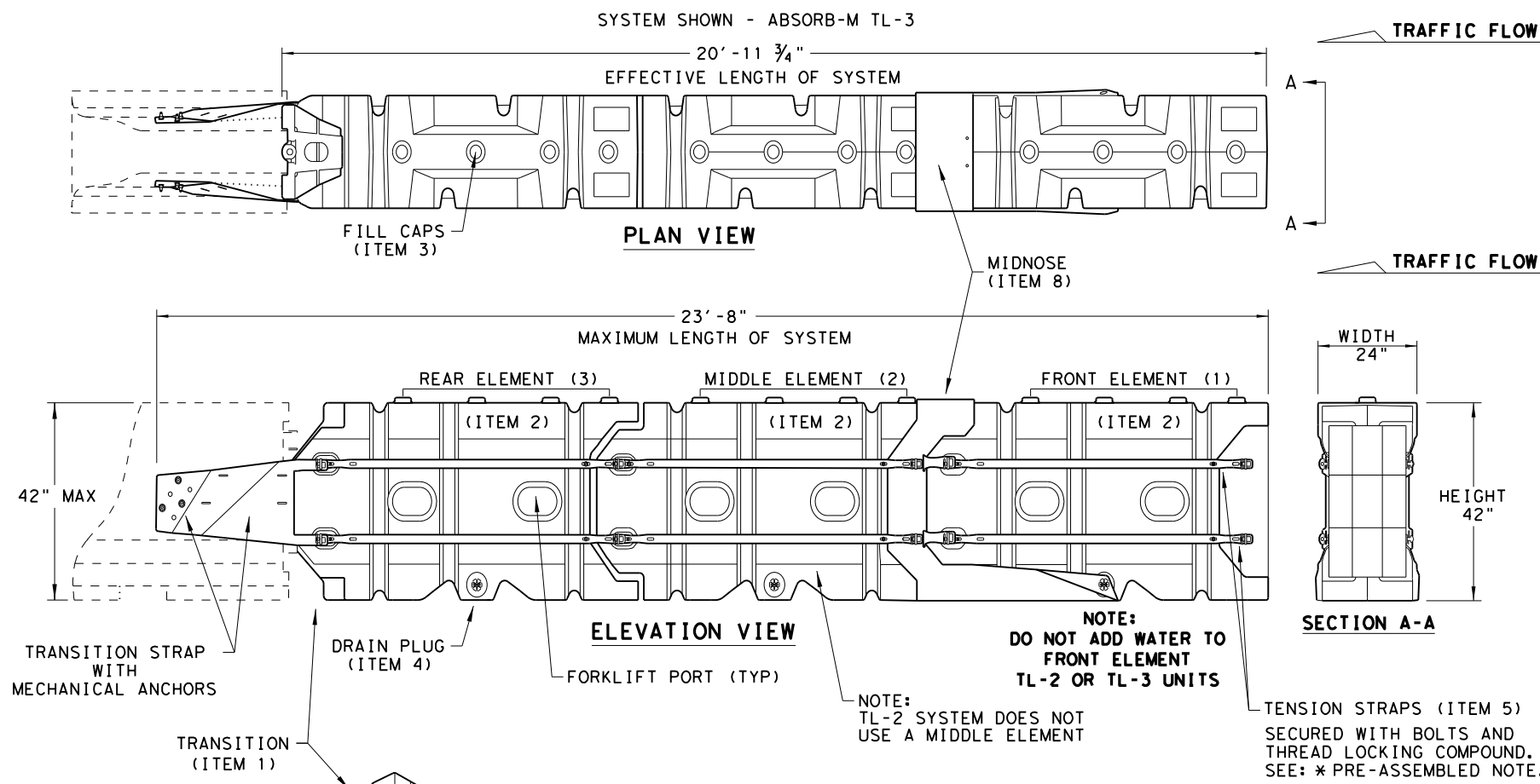
WELDED REBAR GRID

Joint Connection (Type R)

SINGLE SLOPE CONCRETE BARRIER
 PRECAST BARRIER (TYPE 1)
SSCB(2) - 10

FILE: sscb210.dgn	DN: TxDOT	CK: AM	DW: VP	CK:
© TxDOT December 2010	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.		
PHR	JIM HOGG	164		

DATE: 2/12/2024
 FILE: pw:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standard\SH 285 TCP Standards\absorbm19.dgn
 DISCLAIMER: THE USE OF THIS STANDARD IS COVERED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.

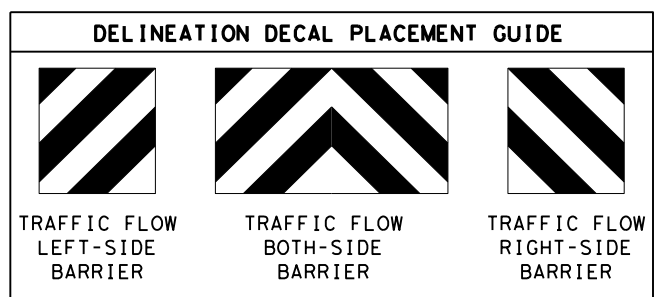
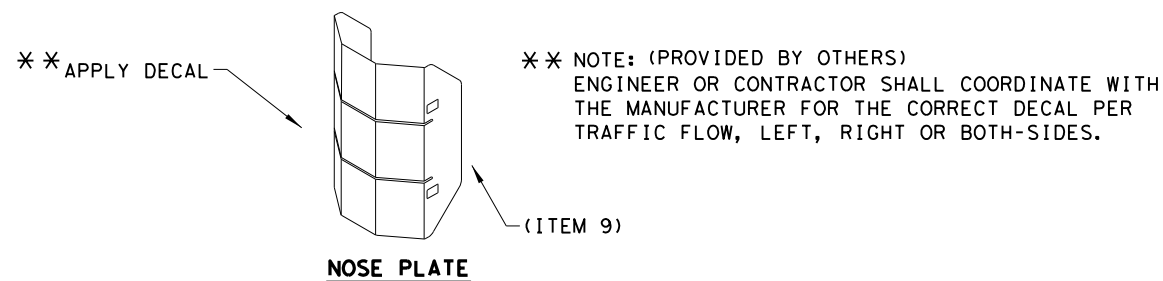


THE ABSORB-M IS A NON-REDIRECTIVE, GATING, CRASH CUSHION DESIGNED TO MEET THE LATEST TL-3 & TL-2 MASH REQUIREMENTS.

THE SYSTEM IS DESIGNED TO ACCOMMODATE A VARIETY OF F-SHAPE AND SINGLE SLOPE CONCRETE BARRIERS. CONTACT THE MANUFACTURER FOR GUIDANCE REGARDING OTHER ALLOWABLE SHAPES.

TEST LEVEL	NUMBER OF ELEMENTS	EFFECTIVE LENGTH	MAXIMUM LENGTH
TL-2	2	14' - 7 3/4"	17' - 4"
TL-3	3	20' - 11 3/4"	23' - 8"

NOTE: CROSS SLOPES OF UP TO 8% (OR 1:12 SLOPE) CAN BE ACCOMMODATED WITH STANDARD HARDWARE SHOWN WITHIN THE INSTRUCTIONS MANUAL. FOR SLOPES WITH EXCESS OF 8% (OR 1:12) CONTACT, LINDSAY TRANSPORTATION SOLUTIONS.



NOTE: APPLY A HIGH REFLECTIVE DECAL TO THE NOSE PLATE. DELINEATION DECAL ORIENTATION IS SHOWN ON THE CONSTRUCTION PLAN SET AND SHALL BE IN ACCORDANCE WITH THE TEXAS MUTCD FOR (TRAFFIC CONTROL DEVICES). DECALS ARE AVAILABLE FOR TRAFFIC FLOW ON THE LEFT-SIDE, BOTH -SIDES AND RIGHT-SIDE.

- ### GENERAL NOTES
- FOR SPECIFIC INFORMATION REGARDING THE INSTALLATION AND TECHNICAL GUIDANCE, CONTACT: LINDSAY TRANSPORTATION SOLUTIONS (LTS) - BARRIER SYSTEMS, INC. AT (707) 374-6800. 180 RIVER ROAD, RIO VISTA, CA 94571
 - THE ABSORB-M SYSTEM IS ONLY APPROVED FOR USE IN (TEMPORARY WORK ZONE) LOCATIONS.
 - THE ABSORB-M IS A WATER FILLED NON-REDIRECTIVE, GATING CRASH CUSHION THAT DOES NOT NEED TO BE ATTACHED TO A FOUNDATION AND CAN BE INSTALLED ON TOP OF CONCRETE, ASPHALT, OR ANY SURFACE CAPABLE OF BEARING THE WEIGHT OF THE SYSTEM.
 - MAXIMUM PERMISSIBLE CROSS-SLOPE IS 8%.
 - THE INSTALLATION AREA SHOULD BE FREE FROM CURBS, ELEVATED OBJECTS, OR DEPRESSIONS.
 - THE ABSORB-M SHOULD BE LOCATED APPROXIMATELY PARALLEL WITH THE BARRIER.
 - THE USE OF THE ABSORB-M IS RESTRICTED TO A BARRIER HEIGHT OF UP TO 42 INCHES.
 - DO NOT ADD WATER TO FRONT ELEMENT (TL-2 OR TL-3 UNIT).

BILL OF MATERIALS (BOM) ABSORB-M TL-3 & TL-2 SYSTEMS			QTY	QTY
ITEM #	PART NUMBER	PART DESCRIPTION	TL-2 SYSTEM	TL-3 SYSTEM
1	BSI-1809036-00	TRANSITION - (GALV)	1	1
2	BSI-1808002-00	PRE-ASSEMBLED ABSORBING (ELEMENTS)	2	3
3	BSI-4004598	FILL CAPS	8	12
4	BSI-4004599	DRAIN PLUGS	2	3
5	BSI-1809053-00	TENSION STRAP - (GALV)	8	12
6	BSI-2001998	C-SCR FH 3/8-16 X 1 1/2 GR5 PLT	8	12
7	BSI-2001999	C-SCR FH 3/8-16 X 1 GR5 PLT	8	12
8	BSI-1809035-00	MIDNOSE - (GALV)	1	1
9	BSI-1808014-00	NOSE PLATE	1	1
10	BSI-1809037-00	TRANSITION STRAP (LEFT-HAND) - (GALV)	1	1
11	BSI-1809038-00	TRANSITION STRAP (RIGHT-HAND) - (GALV)	1	1
12	BSI-1808005-00	PIN ASSEMBLY	8	10
13	BSI-2002001	ANC MECH 5/8-11X5 (GALV)	6	6
14	ABSORB-M	INSTALLATION AND INSTRUCTIONS MANUAL	1	1

* COMPONENTS PRE-ASSEMBLED WITH ELEMENT ASSEMBLY

NOTE: THIS STANDARD IS A BASIC REPRESENTATION OF THE ABSORB-M, IT IS NOT INTENDED TO REPLACE THE INSTALLATION INSTRUCTIONS MANUAL.

SACRIFICIAL

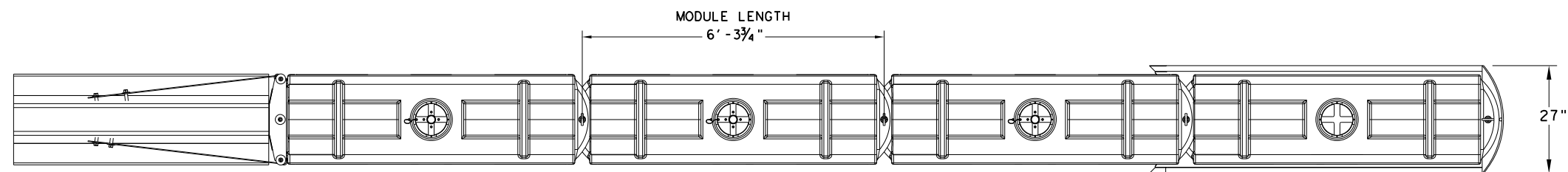
Design Division Standard

LINDSAY TRANSPORTATION SOLUTIONS CRASH CUSHION (MASH TL-3 & TL-2) TEMPORARY - WORK ZONE ABSORB (M) - 19

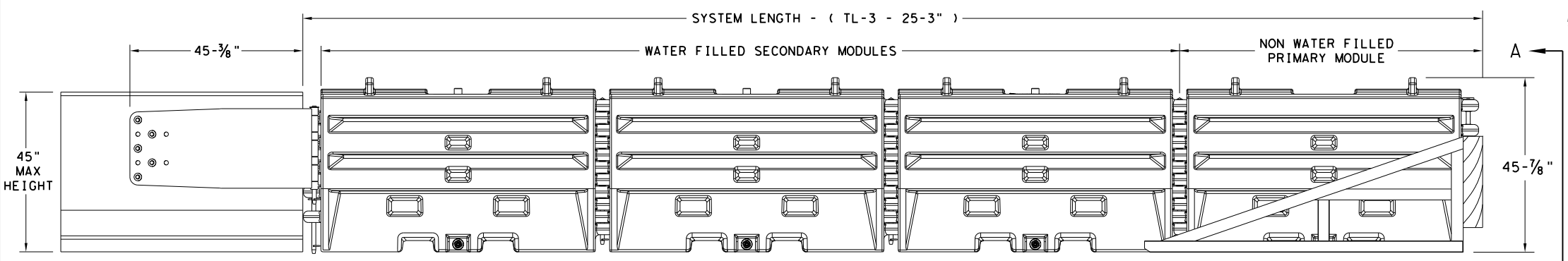
FILE: absorbm19	DN: TxDOT	CK: KM	DW: VP	CK:
© TxDOT: JULY 2019	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.		
PHR	JIM HOGG	165		

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024
 FILE: \\txdotprojectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\SH 285 TCP Standards\sled19.dgn



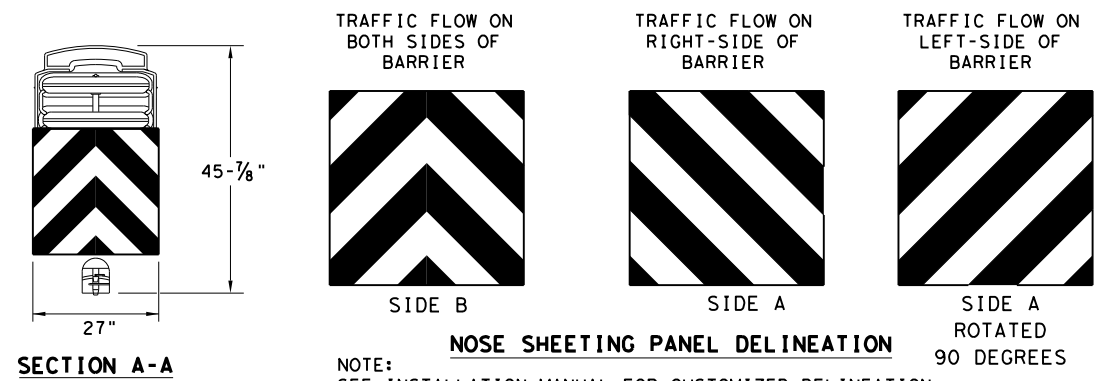
PLAN VIEW



ELEVATION VIEW

GENERAL NOTES

- REFER TO THE INSTALLATION MANUAL FOR SPECIFIC SYSTEM ASSEMBLY AND MODULE ORIENTATION. FOR ADDITIONAL INFORMATION, CONTACT TRAFFIX, INC. AT (949) 361-5663.
- THE SLED SYSTEM IS A MASH APPROVED TEST LEVEL 3 (TL-3) CRASH CUSHION APPROVED FOR USE IN TEMPORARY WORK ZONES. THE SLED SYSTEM IS A NON-REDIRECTIVE, GATING CRASH CUSHION THAT DOES NOT NEED TO BE ATTACHED TO THE GROUND AND CAN BE INSTALLED ON CONCRETE, ASPHALT, GRAVEL OR COMPACTED SOIL.
- MAXIMUM PERMISSIBLE CROSS SLOPE IS 8° (DEGREES) (14%).
- THE INSTALLATION AREA SHOULD BE FREE FROM CURBS, ELEVATED OBJECTS, OR DEPRESSIONS.
- THE SLED SYSTEM CAN BE ATTACHED TO:
 - CONCRETE BARRIER, TEMPORARY OR PERMANENT, 45" MAXIMUM HEIGHT
 - STEEL BARRIER
 - PLASTIC BARRIER
 - CONCRETE BRIDGE ABUTMENTS
 - W-BEAM GUARD RAIL
 - THRIE BEAM GUARD RAIL



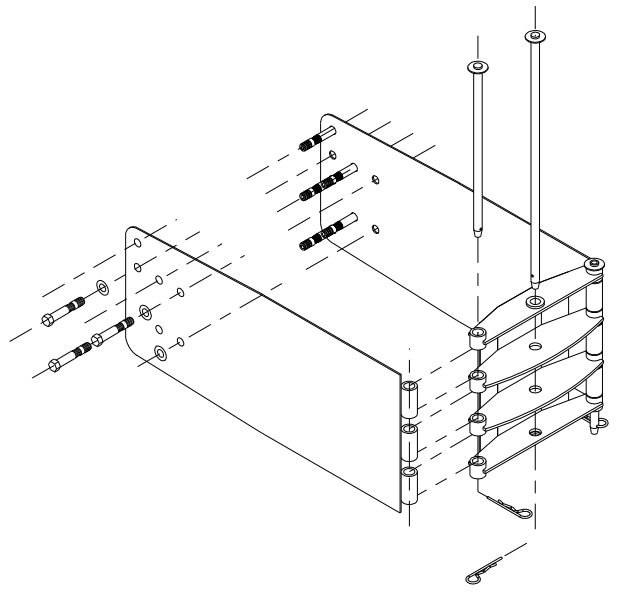
SECTION A-A

NOTE: SEE INSTALLATION MANUAL FOR CUSTOMIZED DELINEATION NOSE SHEETING FOR DECAL PLACEMENT.

TEST LEVEL	NUMBER OF SECONDARY MODULES	SYSTEM LENGTH
TL-3	3	25' 3"

BILL OF MATERIAL		
PART NUMBER	DESCRIPTION	QTY: TL-3
45131	TRANSITION FRAME, GALVANIZED	1
45150	TRANSITION PANEL, GALVANIZED	2
45147-CP	TRANSITION SHORT DROP PIN W/ KEEPER PIN, GALVANIZED	2
45148-CP	TRANSITION LONG DROP PIN W/ KEEPER PIN, GALVANIZED	1
45050	ANCHOR BOLTS	9
12060	WASHER, 3/4" ID X 2" OD	9
45044-Y	SLED YELLOW WATER FILLED MODULE	3
45044-YH	SLED YELLOW "NO FILL" MODULE	1
45044-S	CIS (CONTAINMENT IMPACT SLED), GALVANIZED	1
45043-CP	T-PIN W/ KEEPER PIN	4
18009-B-I	FILL CAP W/ "DRIVE BY" FLOAT INDICATOR	3
45033-RC-B	DRAIN PLUG	3
45032-DPT	DRAIN PLUG REMOVAL TOOL	1

TRANSITION OPTIONS
SLED TRANSITION TO CONCRETE TRAFFIC BARRIER (TEMPORARY OR PERMANENT)
SLED TRANSITION TO STEEL TRAFFIC BARRIER (CONTACT MFGR FOR PROPER TRANSITION)
SLED TRANSITION TO PLASTIC TRAFFIC BARRIER (CONTACT MFGR FOR PROPER TRANSITION)
SLED TRANSITION TO W-BEAM OR THRIE BEAM GUARD RAIL (CONTACT MFGR FOR PROPER TRANSITION)
SLED TRANSITION TO CONCRETE BRIDGE ABUTMENT



SLED TRANSITION COMPONENTS FOR ATTACHMENT TO CMB

NOTE: SEE MANUFACTURER'S INSTALLATION MANUAL FOR FURTHER DETAILS.

NOTE: THIS STANDARD IS A BASIC REPRESENTATION OF THE SLED, IT IS NOT INTENDED TO REPLACE THE INSTALLATION INSTRUCTIONS MANUAL.

SACRIFICIAL

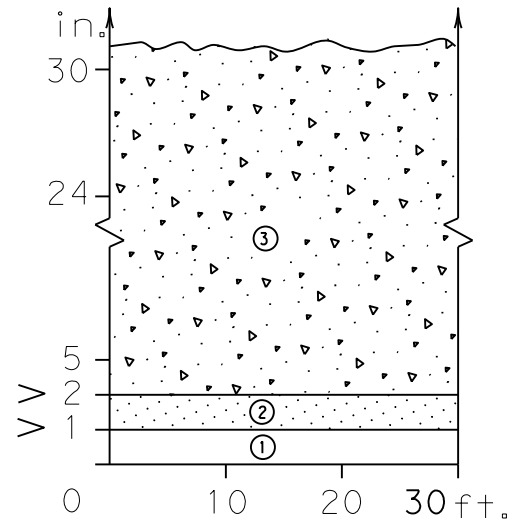
Texas Department of Transportation
 Design Division Standard

SLED CRASH CUSHION TL-3 MASH COMPLIANT (TEMPORARY, WORK ZONE) SLED-19

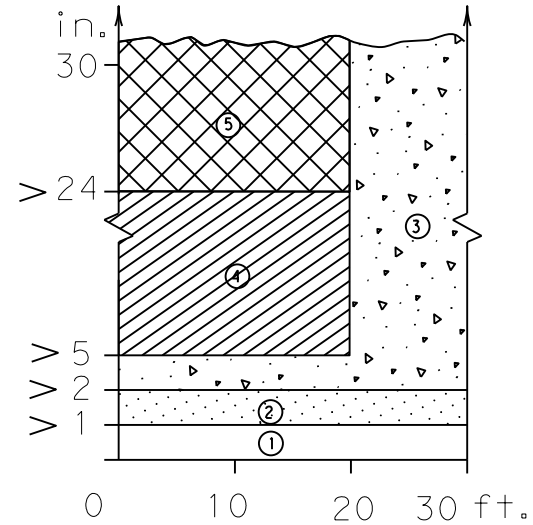
FILE: sled19.dgn	DN: TxDOT	CK: KM	DW: VP	CK:
© TxDOT: DECEMBER 2019	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.		
PHR	JIM HOGG	166		

DEFINITION OF TREATMENT ZONES FOR VARIOUS EDGE CONDITIONS

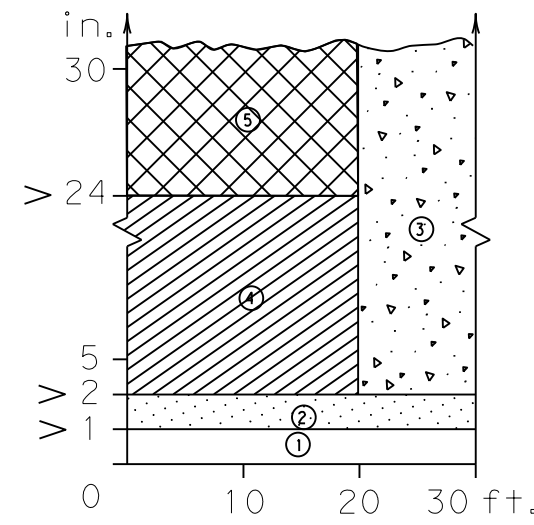
Edge Height (D) in Inches versus Lateral Clearance (Y) in Feet



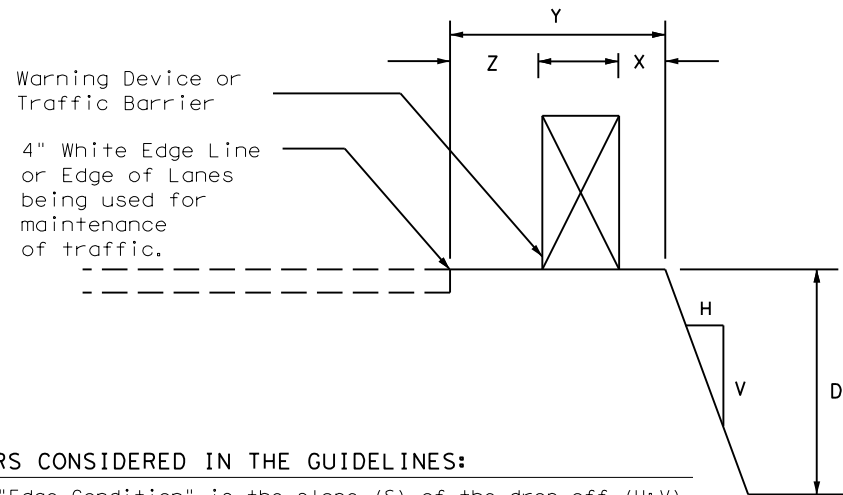
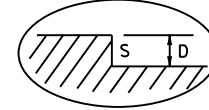
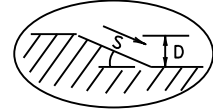
Edge Condition I
S = (3:1) (or flatter)



Edge Condition II
S = ((2.99):1) to (1:1)



Edge Condition III
S is steeper than (1:1)



Zone	Treatment Types Guidelines:
①	No treatment
②	CW 8-11 "Uneven Lanes" signs.
③	CW 8-9a Shoulder Drop-Off" or CW 8-11 signs plus vertical panels.
④	CW8-9a or CW 8-11, signs plus drums. Where restricted space precludes the use of drums, use vertical panels. An edge slope to that of the profered Edge Condition I.
⑤	Check indications (Figure-1) for possitive barrier. Where positive barrier is not indicated, the treatment shown above for Zone-4 may be used after consideration of other applicable factors.

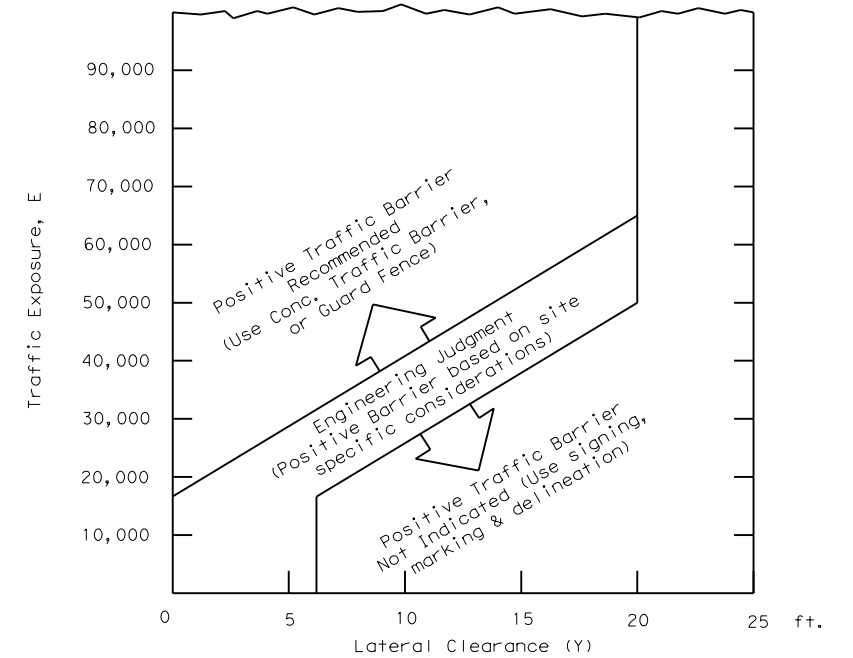
FACTORS CONSIDERED IN THE GUIDELINES:

- The "Edge Condition" is the slope (S) of the drop-off (H:V). The "Edge Height" is the depth of the drop-off "D".
- Distance "X" is to be the maximum practical under job conditions. Two feet minimum for high speed conditions. Distance "Y" is the lateral clearance from edge of travel lane to edge of dropoff. Distance "Z" does not have a minimum.
- In addition to the factors considered in the guidelines, each construction zone drop-off situation should be analyzed individually, taking into account other variables, such as: traffic mix, posted speed in the construction zone, horizontal curvature, and the practicality of the treatment options.
- The conditions for indicating the use of positive or protective barriers are given by Zone-5 and Figure-1. Traffic barriers are primarily applicable for high speed conditions. Urban areas with speeds of 30 mph or less may have a lesser need for signing, delineation, and barriers. Right-angled edges, however, with "D" greater than 2 inches and located within a lateral offset of 6 feet, may indicate a higher level of treatment.
- If the distance "Y" must be less than 3 feet, the use of a positive barrier may not be feasible. In such a case, consider either: 1) narrowing the lanes to a desired 11 to 12 feet or 10 foot minimum (see CW20-8 sign), or 2) provide an edge slope such as Edge Condition I.

Edge Condition Notes:

- Edge Condition I: Most vehicles are able to traverse an edge condition with a slope rate of (3 to 1) or flatter. The slope must be constructed with a compacted material capable of supporting vehicles.
- Edge Condition II: Most vehicles are able to traverse an edge condition with a slope between (2.99 to 1) and (1 to 1) so long as "D" does not exceed 5 inches. Under-carriage drag on most automobiles will occur when "D" exceeds 6 inches. As "D" exceeds 24 inches, the possibility for rollover is greater in most vehicles.
- Edge Condition III: When slopes are greater than (1 to 1) and where "D" is greater than 2 inches, a more difficult control factor may exist for some vehicles, if not properly treated. For example, where "D" is greater than 2 inches and up to 24 inches different types of vehicles may experience different steering control at different edge heights. Automobiles might experience more steering control differential when "D" is greater than 2 inches and up to 5 inches. Trucks, particularly those with high loads, have more steering control differential when "D" is greater than 5 inches and up to 24 inches. When "D" exceeds 24 inches, the possibility of rollover is greater for most vehicles.
- Milling or overlay operations that result in Edge Condition III should not be in place without appropriate warning treatments, and these conditions should not be left in place for extended periods of time.

FIGURE-1: CONDITIONS INDICATING USE OF POSITIVE BARRIER FOR ZONE 5 ([Cross-hatch])



- $E = ADT \times T$
Where ADT is that portion of the average daily traffic volume traveling within 20 feet (generally two adjacent lanes) of the edge dropoff condition; and, T is the duration time in years of the dropoff condition.
- Figure-1 provides a practical approach to the use of positive barriers for the protection of vehicles from pavement drop-offs. Other factors, such as the presence of heavy machinery, construction workers, or the mix and volume of traffic may make the use of positive barriers appropriate, even when the edge condition alone may not justify the use of a barrier.
- An approved end treatment should be provided for any positive barrier end located within the clear zone.

These guidelines apply to temporary traffic control areas or work zones where continuous pavement edges or drop-offs exists parallel and adjacent to a lane used by traffic. The edge conditions may be present between shoulders and travel lanes, between adjacent or opposing travel lanes, or at intermediate points across the width of the paved surface. Due to the variability in construction operations, tolerances in the variables may be allowed by the engineer. These guidelines do not apply to short term operations. These guidelines do not constitute a rigid standard or policy; rather, they are guidance to be used in conjunction with engineering judgement. These guidelines may be updated on the Design Division's on-line manuals.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE:
FILE:

Engineer's Seal

Date 02/29/2024

Texas Department of Transportation

Traffic Safety Division Standard

TREATMENT FOR VARIOUS EDGE CONDITIONS

FILE: edgecon.dgn	DN:	CK:	DW:	CK:
© TxDOT August 2000	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
03-01	DIST	COUNTY	SHEET NO.	
08-01	PHR	JIM HOGG	167	
9-21				

DATE: 2/12/2024 8:00:12 AM
FILE: pw:\\ttdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\SH 285 ROADWAY COVER.dgn

ROADWAY COVER SHEET

Pharr District Central Design



**SH 285
ROADWAY
COVER SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		168

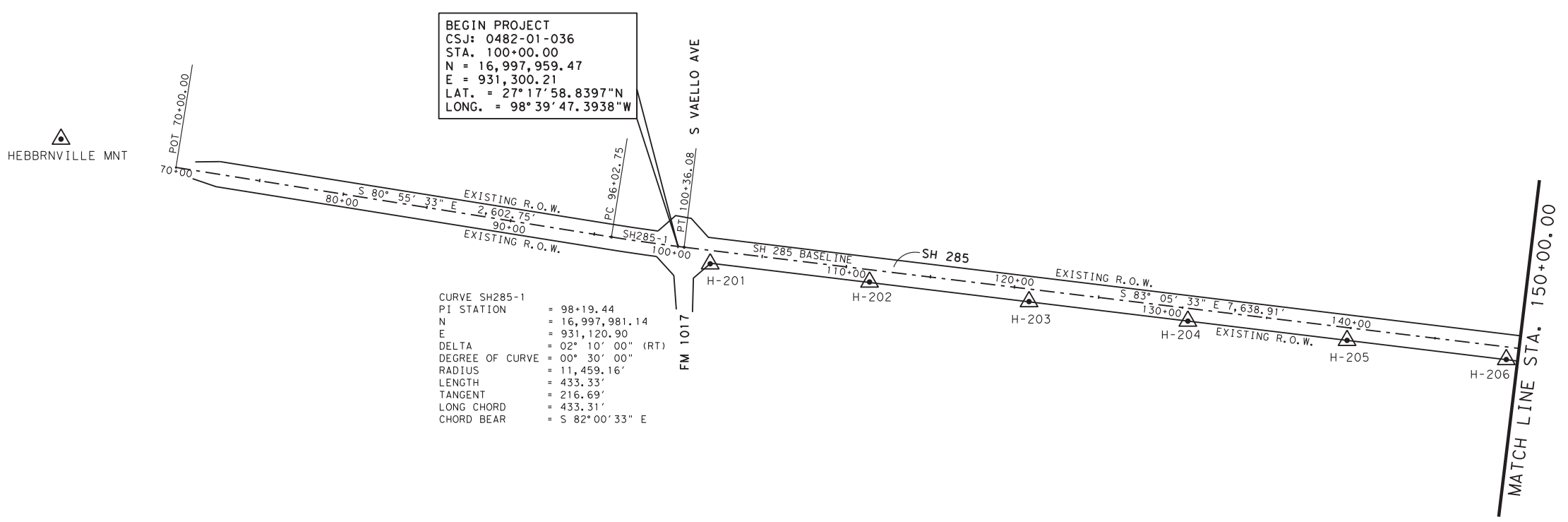
1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. EXISTING RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.



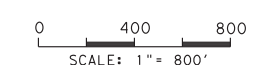
BEGIN PROJECT
CSJ: 0482-01-036
STA. 100+00.00
N = 16,997,959.47
E = 931,300.21
LAT. = 27° 17' 58.8397"N
LONG. = 98° 39' 47.3938"W

CURVE SH285-1
PI STATION = 98+19.44
N = 16,997,981.14
E = 931,120.90
DELTA = 02° 10' 00" (RT)
DEGREE OF CURVE = 00° 30' 00"
RADIUS = 11,459.16'
LENGTH = 433.33'
TANGENT = 216.69'
LONG CHORD = 433.31'
CHORD BEAR = S 82° 00' 33" E

TIE TO EXISTING NATIONAL GEODETIC SURVEY (NGS) MONUMENTS					
POINT NO.	CALLED NORTHING (Y)	CALLED EASTING (X)	NAVD88 CALLED ELEVATION	NAVD88 FOUND ELEVATION	DESCRIPTION
PID AJ0288	16,972,544.14	1,090,441.72	136.47'	135.53'	BRASS DISC STAMPED "Y 203 1934" IN TOP OF A CONCRETE MONUMENT AT THE NORTHWEST CORNER OF SH 285 AND FM 754
PID AJ0394	16,981,059.33	1,020,628.14	315.01'	314.79' (RTK ELEV)	BRASS DISC IN TOP OF A CONCRETE MONUMENT (LEANING) ON THE WEST SIDE OF CR 107, 0.7 MILES SOUTH OF SH 285
PID AJ0286	N/A	N/A	123.90'	122.77'	BRASS DISC STAMPED "V 203 1934" IN TOP CONCRETE STEP ON THE WEST ENTRANCE TO THE COUNTY COURT HOUSE IN FALFURRIAS, TX

SURVEY CONTROL MONUMENTATION TABLE						
POINT	STATION	OFFSET	NORTHING (Y)	EASTING (X)	ELEVATION	DESCRIPTION
HEBBRONVILLE MNT	N/A	N/A	16,998,592.90	927,658.16	541.22'	FND. 9/16" STEEL DATUM ROD w/ SLEEVE
H-201	102+00.50	78.63' (RT)	16,997,857.24	931,489.80	532.07'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "201"
H-202	111+47.09	72.09' (RT)	16,997,749.89	932,430.30	526.70'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "202"
H-203	120+95.41	73.64' (RT)	16,997,634.29	933,371.55	520.94'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "203"
H-204	130+39.97	75.06' (RT)	16,997,519.29	934,309.09	518.14'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "204"
H-205	139+86.45	74.97' (RT)	16,997,405.55	935,248.70	507.82'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "205"
H-206	149+32.96	74.10' (RT)	16,997,292.57	936,188.44	510.26'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "206"

SURVEY CONTROL MONUMENT INVERSE TABLE			
FROM POINT	BEARING	DISTANCE	TO POINT
HEBBRONVILLE MNT	S 79° 07' 54" E	3,901.62'	H-201
H-201	S 83° 29' 17" E	946.61'	H-202
H-202	S 82° 59' 55" E	948.32'	H-203
H-203	S 83° 00' 24" E	944.57'	H-204
H-204	S 83° 05' 52" E	946.47'	H-204
H-205	S 83° 08' 42" E	946.51'	H-206
H-206	S 83° 04' 56" E	946.03'	H-207



UNIT OF MEASURE: U. S. SURVEY FEET

© 2021

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

SH 285
SURVEY CONTROL INDEX SHEET

SHEET 1 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			169

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*SCM*01.dgn



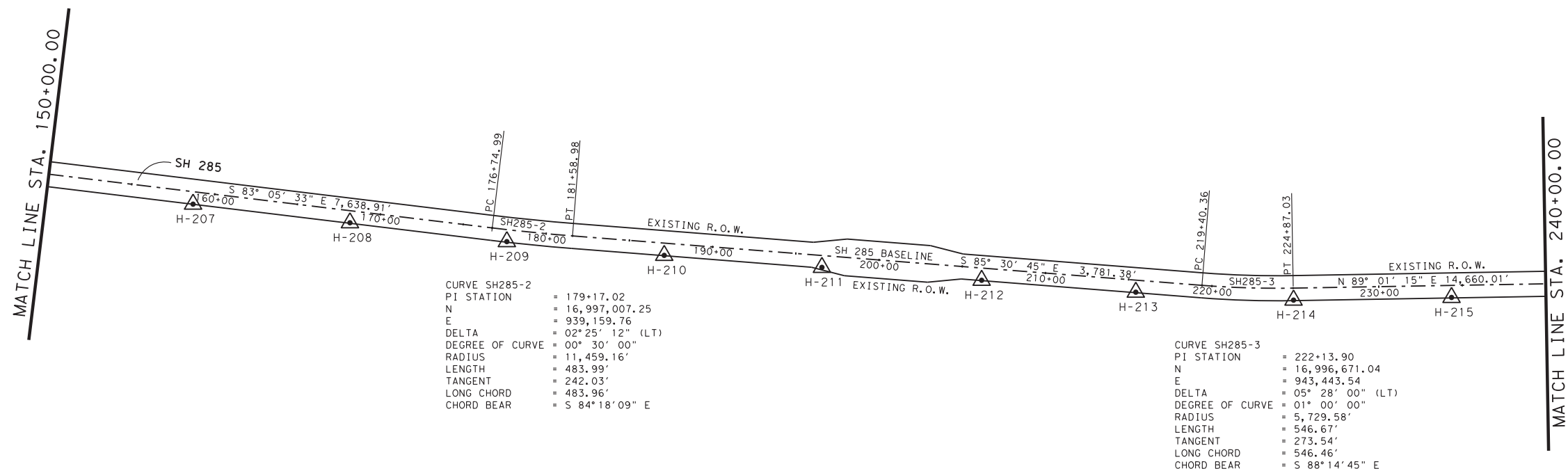
1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. EXISTING RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.

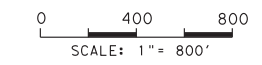


CURVE SH285-2
 PI STATION = 179+17.02
 N = 16,997,007.25
 E = 939,159.76
 DELTA = 02° 25' 12" (LT)
 DEGREE OF CURVE = 00° 30' 00"
 RADIUS = 11,459.16'
 LENGTH = 483.99'
 TANGENT = 242.03'
 LONG CHORD = 483.96'
 CHORD BEAR = S 84° 18' 09" E

CURVE SH285-3
 PI STATION = 222+13.90
 N = 16,996,671.04
 E = 943,443.54
 DELTA = 05° 28' 00" (LT)
 DEGREE OF CURVE = 01° 00' 00"
 RADIUS = 5,729.58'
 LENGTH = 546.67'
 TANGENT = 273.54'
 LONG CHORD = 546.46'
 CHORD BEAR = S 88° 14' 45" E

SURVEY CONTROL MONUMENTATION TABLE						
POINT	STATION	OFFSET	NORTHING (Y)	EASTING (X)	ELEVATION	DESCRIPTION
H-207	158+78.99	74.27' (RT)	16,997,178.63	937,127.59	505.00'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "207"
H-208	168+25.27	72.03' (RT)	16,997,067.04	938,067.27	506.61'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "208"
H-209	177+71.12	71.47' (RT)	16,996,954.18	939,006.96	506.12'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "209"
H-210	187+15.05	73.15' (RT)	16,996,871.87	939,949.69	502.82'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "210"
H-211	196+62.35	70.94' (RT)	16,996,799.96	940,894.26	497.87'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "211"
H-212	206+22.08	73.11' (RT)	16,996,722.70	941,850.88	497.53'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "212"
H-213	215+48.39	71.93' (RT)	16,996,651.40	942,774.44	494.59'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "213"
H-214	224+88.03	71.87' (RT)	16,996,603.87	943,719.27	491.55'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "214"
H-215	234+34.47	72.03' (RT)	16,996,619.88	944,665.58	488.18'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "215"

SURVEY CONTROL MONUMENT INVERSE TABLE			
FROM POINT	BEARING	DISTANCE	TO POINT
H-206	S 83° 04' 56" E	946.03'	H-207
H-207	S 83° 13' 41" E	946.29'	H-208
H-208	S 83° 09' 05" E	946.44'	H-209
H-209	S 85° 00' 37" E	946.32'	H-210
H-210	S 85° 38' 47" E	947.30'	H-211
H-211	S 85° 22' 56" E	959.73'	H-212
H-212	S 85° 35' 09" E	926.31'	H-213
H-213	S 87° 07' 12" E	946.03'	H-214
H-214	N 89° 01' 51" E	946.45'	H-215
H-215	N 88° 54' 58" E	946.32'	H-216



UNIT OF MEASURE: U. S. SURVEY FEET

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*SCM*02.dgn

© 2021

LANDTECH
 2525 North Loop West, Suite 300,
 Houston, Texas 77008
 T: 713-861-7068 F: 713-861-4131
 TBPELS Registration No. 10019100

SH 285
 SURVEY CONTROL INDEX SHEET

SHEET 2 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			170

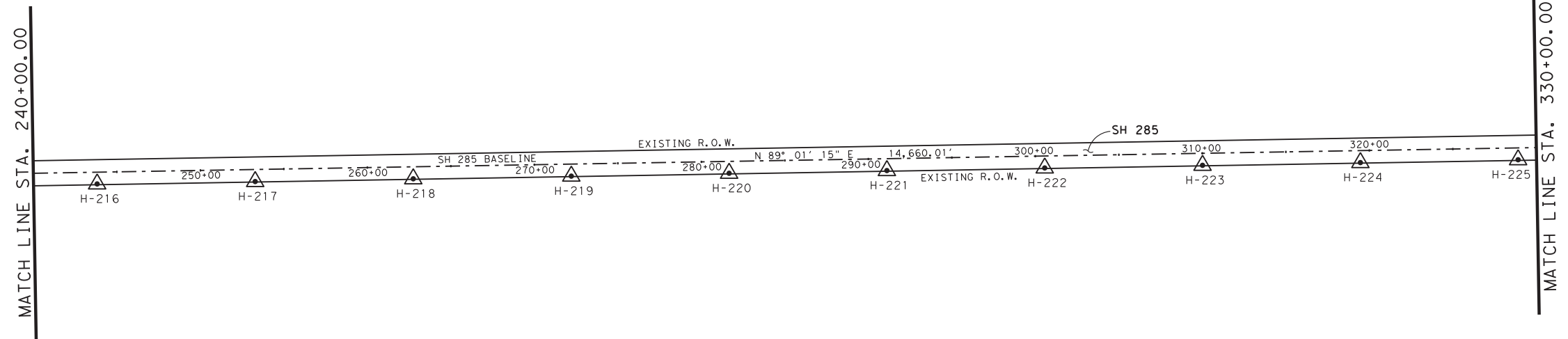
1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. EXISTING RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



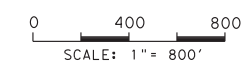
W. R. Wolfram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.



SURVEY CONTROL MONUMENTATION TABLE						
POINT	STATION	OFFSET	NORTHING (Y)	EASTING (X)	ELEVATION	DESCRIPTION
H-216	243+80.80	70.30' (RT)	16,996,637.78	945,611.73	487.42'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "216"
H-217	253+27.22	72.06' (RT)	16,996,652.19	946,558.05	484.51'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "217"
H-218	262+73.73	71.89' (RT)	16,996,668.53	947,504.42	480.87'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "218"
H-219	272+20.06	72.02' (RT)	16,996,684.58	948,450.61	477.99'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "219"
H-220	281+65.08	71.04' (RT)	16,996,701.70	949,395.47	480.57'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "220"
H-221	291+09.86	71.01' (RT)	16,996,717.88	950,340.12	488.09'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "221"
H-222	300+54.98	71.57' (RT)	16,996,733.46	951,285.11	484.60'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "222"
H-223	309+99.98	71.88' (RT)	16,996,749.30	952,229.97	474.85'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "223"
H-224	319+45.25	71.72' (RT)	16,996,765.62	953,175.10	478.79'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "224"
H-225	328+89.71	70.83' (RT)	16,996,782.64	954,119.41	474.43'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "225"

SURVEY CONTROL MONUMENT INVERSE TABLE			
FROM POINT	BEARING	DISTANCE	TO POINT
H-215	N 88° 54' 58" E	946.32'	H-216
H-216	N 89° 07' 39" E	946.42'	H-217
H-217	N 89° 00' 39" E	946.52'	H-218
H-218	N 89° 01' 42" E	946.32'	H-219
H-219	N 88° 57' 43" E	945.02'	H-220
H-220	N 89° 01' 08" E	944.78'	H-221
H-221	N 89° 03' 19" E	945.13'	H-222
H-222	N 89° 02' 23" E	944.99'	H-223
H-223	N 89° 00' 39" E	945.27'	H-224
H-224	N 88° 58' 01" E	944.46'	H-225
H-225	N 89° 04' 45" E	945.43'	H-226



UNIT OF MEASURE: U.S. SURVEY FEET

© 2021

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

SH 285
SURVEY CONTROL INDEX SHEET

SHEET 3 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			171

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*SCM*03.dgn

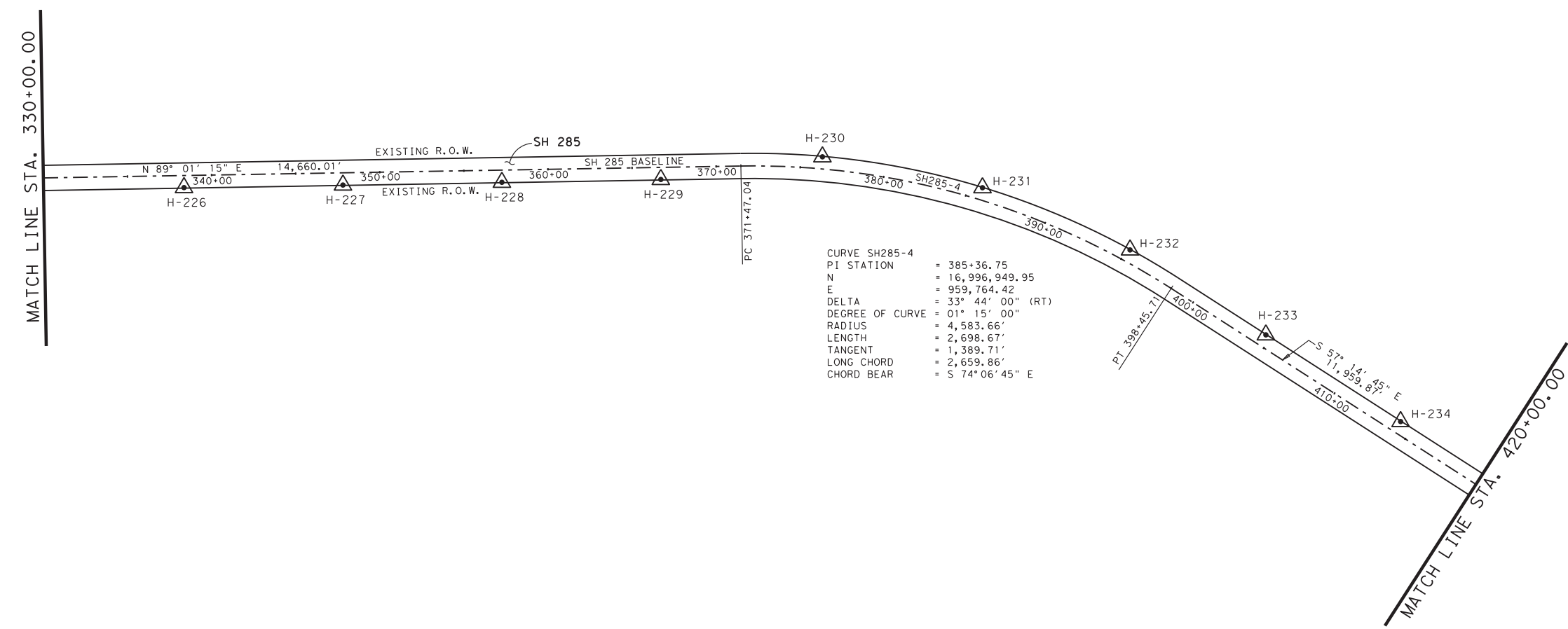
1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. EXISTING RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfram, III
08-26-2021

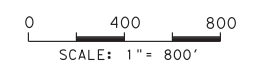
THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.



CURVE SH285-4
 PI STATION = 385+36.75
 N = 16,996,949.95
 E = 959,764.42
 DELTA = 33° 44' 00" (RT)
 DEGREE OF CURVE = 01° 15' 00"
 RADIUS = 4,583.66'
 LENGTH = 2,698.67'
 TANGENT = 1,389.71'
 LONG CHORD = 2,659.86'
 CHORD BEAR = S 74° 06' 45" E

SURVEY CONTROL MONUMENTATION TABLE						
POINT	STATION	OFFSET	NORTHING (Y)	EASTING (X)	ELEVATION	DESCRIPTION
H-226	338+35.14	71.79' (RT)	16,996,797.84	955,064.72	471.88'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "226"
H-227	347+80.01	72.64' (RT)	16,996,813.13	956,009.47	467.83'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "227"
H-228	357+25.05	72.50' (RT)	16,996,829.42	956,954.36	458.54'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "228"
H-229	366+70.00	71.97' (RT)	16,996,846.10	957,899.17	457.79'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "229"
H-230	376+26.25	71.89' (LT)	16,996,980.98	958,859.89	462.25'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "230"
H-231	385+81.85	72.69' (LT)	16,996,797.15	959,811.20	456.68'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "231"
H-232	395+20.83	71.84' (LT)	16,996,427.04	960,688.44	454.34'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "232"
H-233	404+68.54	72.17' (LT)	16,995,921.79	961,496.01	454.21'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "233"
H-234	414+21.20	71.89' (LT)	16,995,406.13	962,297.05	447.96'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "234"

SURVEY CONTROL MONUMENT INVERSE TABLE			
FROM POINT	BEARING	DISTANCE	TO POINT
H-225	N 89° 04' 45" E	945.43'	H-226
H-226	N 89° 04' 22" E	944.87'	H-227
H-227	N 89° 00' 43" E	945.04'	H-228
H-228	N 88° 59' 20" E	944.96'	H-229
H-229	N 82° 00' 30" E	970.14'	H-230
H-230	S 79° 03' 48" E	968.91'	H-231
H-231	S 67° 07' 29" E	952.12'	H-232
H-232	S 57° 58' 07" E	952.60'	H-233
H-233	S 57° 13' 44" E	952.66'	H-234
H-234	S 57° 15' 15" E	952.91'	H-235



UNIT OF MEASURE: U. S. SURVEY FEET

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*SCM*04.dgn

© 2021

LANDTECH
 2525 North Loop West, Suite 300,
 Houston, Texas 77008
 T: 713-861-7068 F: 713-861-4131
 TBPELS Registration No. 10019100

SH 285
 SURVEY CONTROL INDEX SHEET

SHEET 4 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO. JOB NO. SHEET NO.
PHARR	JIM HOGG	0482	01 036 172



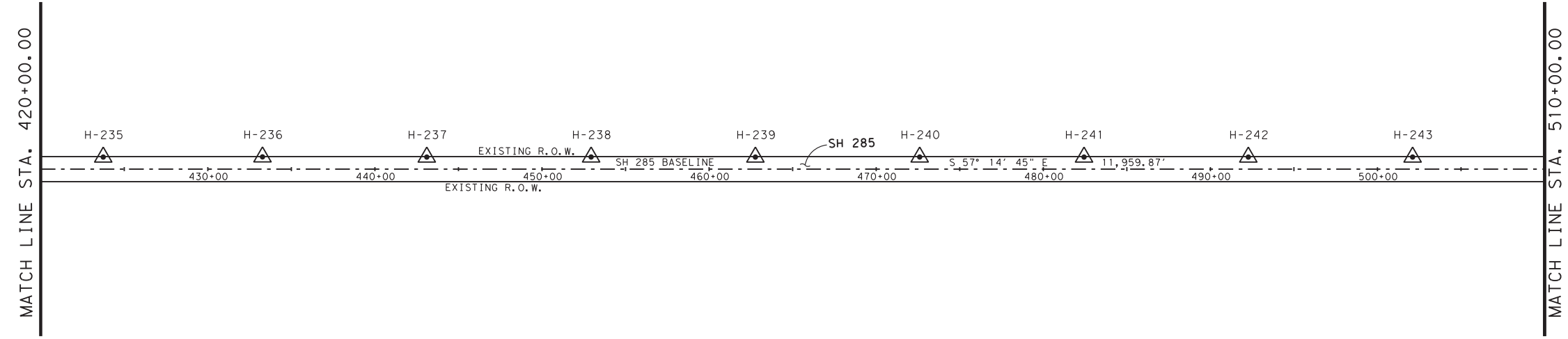
1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. EXISTING RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



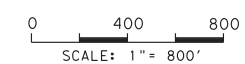
W. R. Wolfram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.



FROM POINT	BEARING	DISTANCE	TO POINT
H-234	S 57° 15' 15" E	952.91'	H-235
H-235	S 57° 18' 34" E	953.15'	H-236
H-236	S 57° 11' 03" E	983.26'	H-237
H-237	S 57° 16' 10" E	983.39'	H-238
H-238	S 57° 13' 19" E	983.14'	H-239
H-239	S 57° 16' 05" E	983.22'	H-240
H-240	S 57° 13' 14" E	983.63'	H-241
H-241	S 57° 14' 50" E	982.98'	H-243
H-243	S 57° 01' 27" E	986.60'	H-244

POINT	STATION	OFFSET	NORTHING (Y)	EASTING (X)	ELEVATION	DESCRIPTION
H-235	423+74.11	72.03' (LT)	16,994,890.69	963,098.52	451.97'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "235"
H-236	433+27.27	73.09' (LT)	16,994,375.88	963,900.70	442.93'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "236"
H-237	443+10.53	72.03' (LT)	16,993,843.01	964,727.05	440.17'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "237"
H-238	452+93.93	72.44' (LT)	16,993,311.30	965,554.30	439.83'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "238"
H-239	462+77.06	72.03' (LT)	16,992,779.05	966,380.90	445.11'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "239"
H-240	472+60.29	72.41' (LT)	16,992,247.41	967,207.99	441.56'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "240"
H-241	482+43.92	71.98' (LT)	16,991,714.86	968,034.99	440.07'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "241"
H-242	492+26.90	72.01' (LT)	16,991,183.06	968,861.69	430.72'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "242"
H-243	502+10.23	72.12' (LT)	16,990,651.14	969,688.74	427.88'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "243"



UNIT OF MEASURE: U.S. SURVEY FEET

© 2021

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

SH 285
SURVEY CONTROL INDEX SHEET

SHEET 5 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO. JOB NO. SHEET NO.
PHARR	JIM HOGG	0482	01 036 173

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*SCM*05.dgn



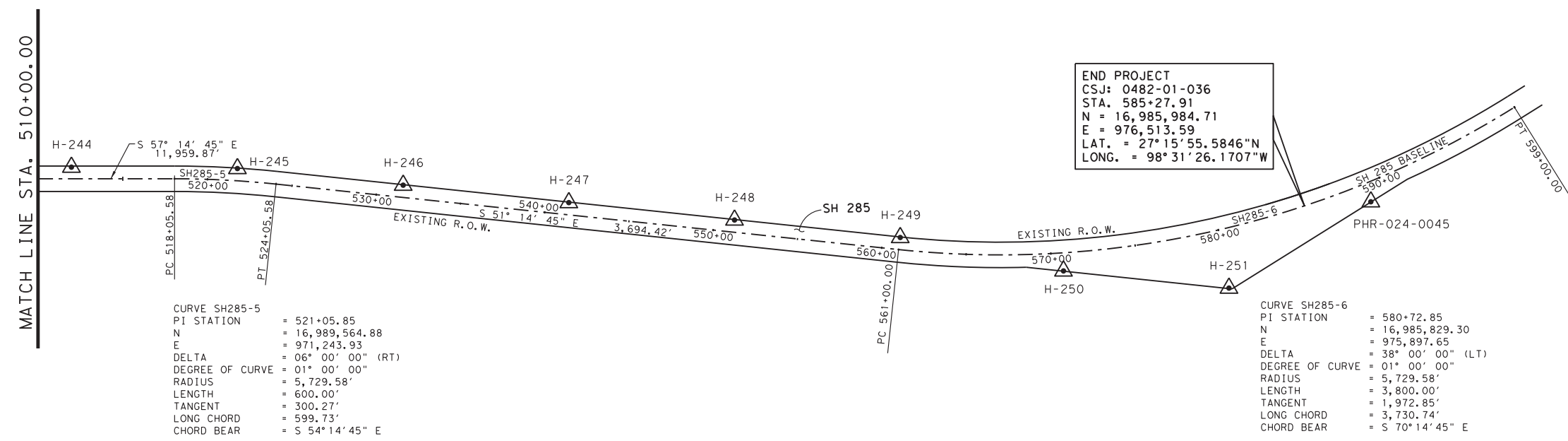
1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. EXISTING RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.

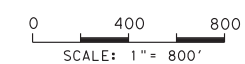


CURVE SH285-5
 PI STATION = 521+05.85
 N = 16,989,564.88
 E = 971,243.93
 DELTA = 06° 00' 00" (RT)
 DEGREE OF CURVE = 01° 00' 00"
 RADIUS = 5,729.58'
 LENGTH = 600.00'
 TANGENT = 300.27'
 LONG CHORD = 599.73'
 CHORD BEAR = S 54° 14' 45" E

CURVE SH285-6
 PI STATION = 580+72.85
 N = 16,985,829.30
 E = 975,897.65
 DELTA = 38° 00' 00" (LT)
 DEGREE OF CURVE = 01° 00' 00"
 RADIUS = 5,729.58'
 LENGTH = 3,800.00'
 TANGENT = 1,972.85'
 LONG CHORD = 3,730.74'
 CHORD BEAR = S 70° 14' 45" E

FROM POINT	BEARING	DISTANCE	TO POINT
H-243	S 57° 01' 27" E	986.60'	H-244
H-244	S 56° 46' 11" E	979.75'	H-245
H-245	S 51° 31' 22" E	983.29'	H-246
H-246	S 51° 14' 42" E	983.08'	H-247
H-247	S 51° 15' 28" E	983.26'	H-248
H-248	S 51° 13' 39" E	983.52'	H-249
H-249	S 45° 40' 49" E	983.59'	H-250
H-250	S 51° 19' 52" E	981.75'	H-251
H-251	S 88° 31' 51" E	979.91'	PHR-024-0045

POINT	STATION	OFFSET	NORTHING (Y)	EASTING (X)	ELEVATION	DESCRIPTION
H-244	511+96.82	68.31' (LT)	16,990,114.14	970,516.40	426.27'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "244"
H-245	521+72.18	72.04' (LT)	16,989,577.24	971,335.93	430.51'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "245"
H-246	531+52.59	71.98' (LT)	16,988,965.43	972,105.71	428.30'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "246"
H-247	541+35.67	71.97' (LT)	16,988,350.03	972,872.34	433.34'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "247"
H-248	551+18.93	72.18' (LT)	16,987,734.69	973,639.25	431.33'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "248"
H-249	561+02.48	71.87' (LT)	16,987,118.78	974,406.04	428.80'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "249"
H-250	570+68.07	106.63' (RT)	16,986,431.58	975,109.76	424.20'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "250"
H-251	579+84.58	347.87' (RT)	16,985,818.17	975,876.28	423.91'	SET TXDOT ALUMINUM CAP IN CONCRETE STAMPED "251"
PHR-024-0045	589+00.20	112.36' (RT)	16,985,793.04	976,855.87	424.66'	FND. 9/16" STEEL DATUM ROD/SLEEVE



UNIT OF MEASURE: U.S. SURVEY FEET

© 2021

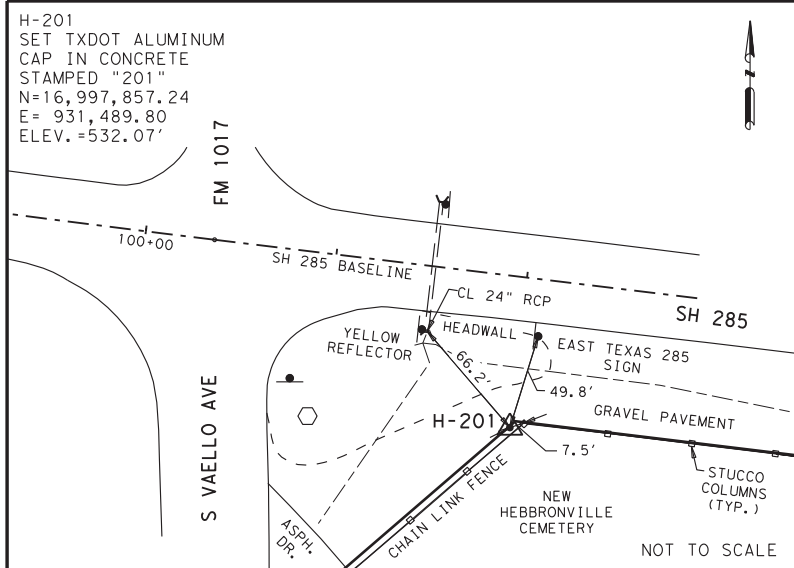
LANDTECH
 2525 North Loop West, Suite 300,
 Houston, Texas 77008
 T: 713-861-7068 F: 713-861-4131
 TBPELS Registration No. 10019100

SH 285
 SURVEY CONTROL INDEX SHEET

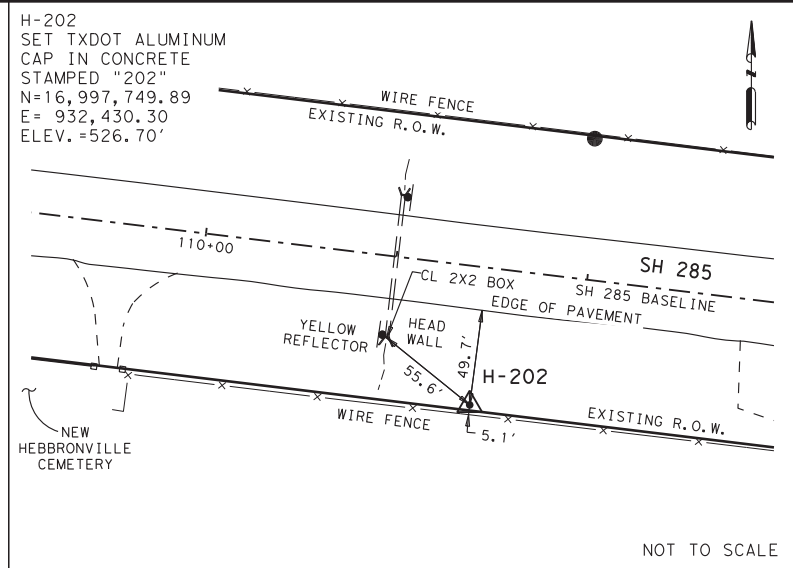
SHEET 6 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO. JOB NO. SHEET NO.
PHARR	JIM HOGG	0482	01 036 174

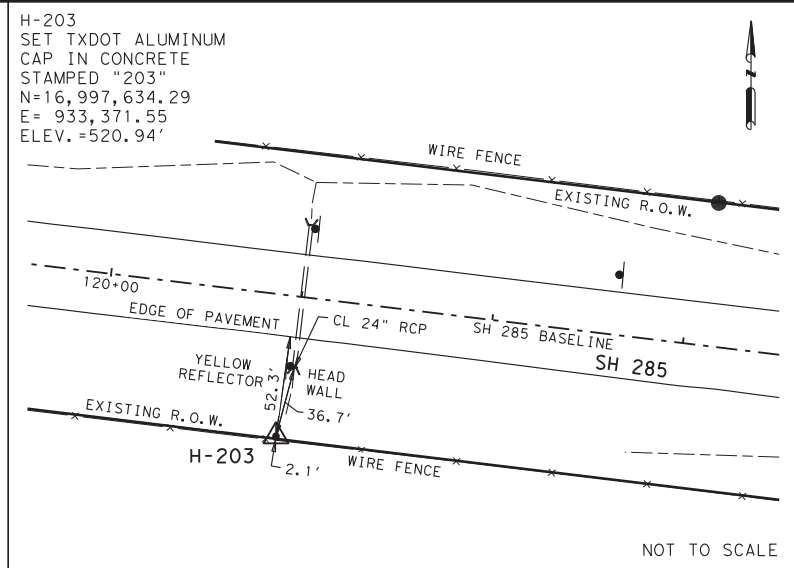
S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*SCM*06.dgn



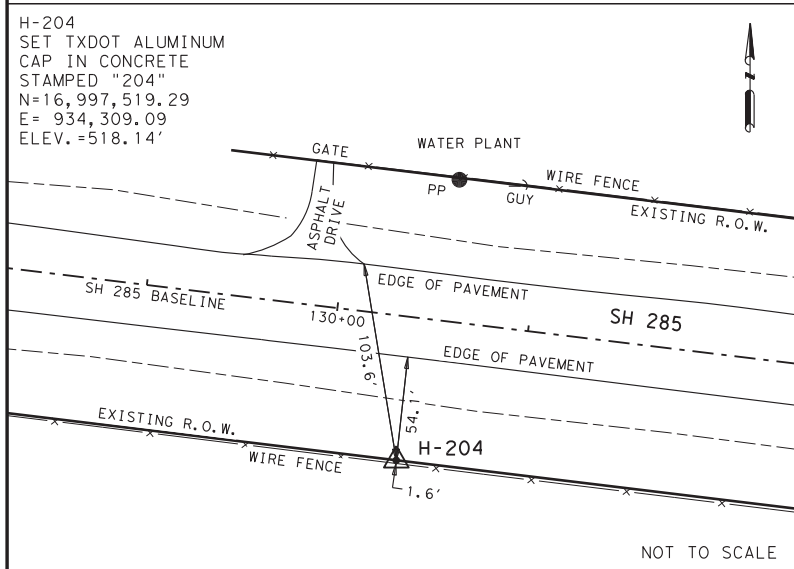
H-201: LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE AND THE NORTHWEST CORNER OF NEW HEBBRONVILLE CEMETERY.



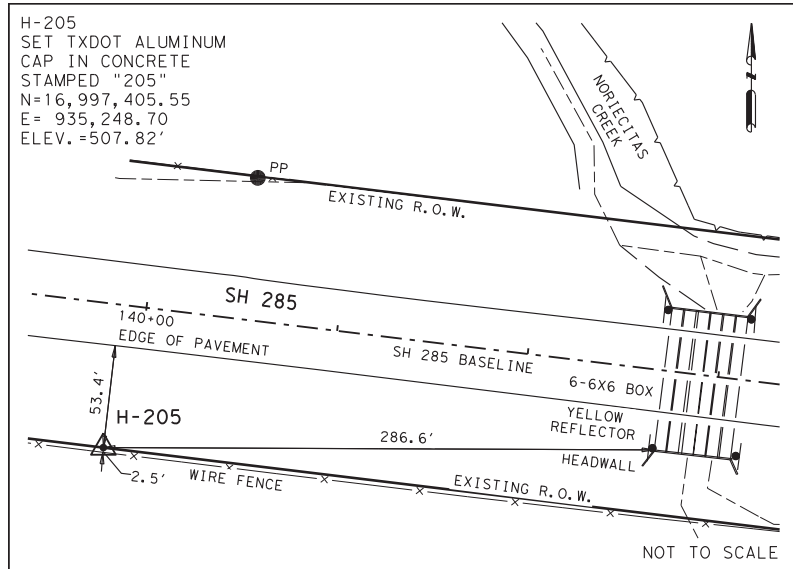
H-202: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE TRAVEL EAST ALONG SH 285 1,100 FEET, LOCATED ON THE SOUTH SIDE OF SH 285 180 FEET EAST OF NEW HEBBRONVILLE CEMETERY.



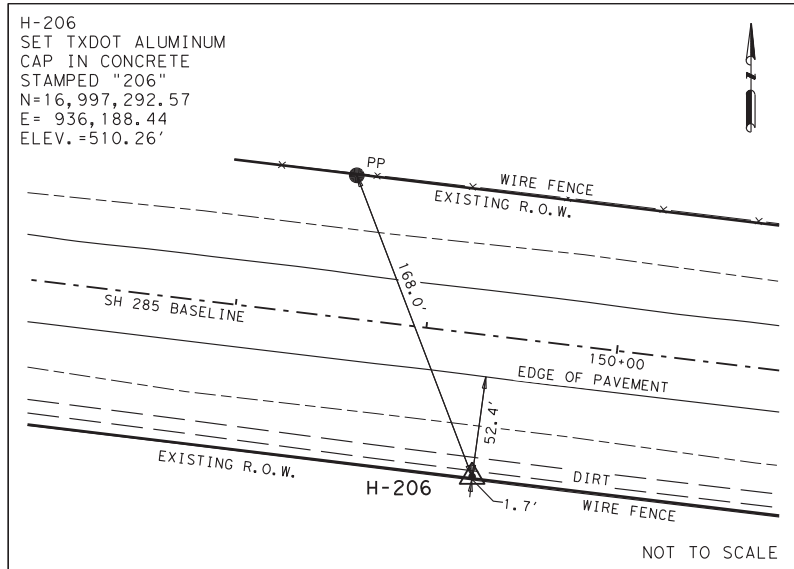
H-203: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE TRAVEL EAST ALONG SH 285 2,050 FEET, LOCATED ON THE SOUTH SIDE OF SH 285, ACROSS FROM AND 890 FEET WEST OF THE ENTRANCE TO A WATER TREATMENT PLANT.



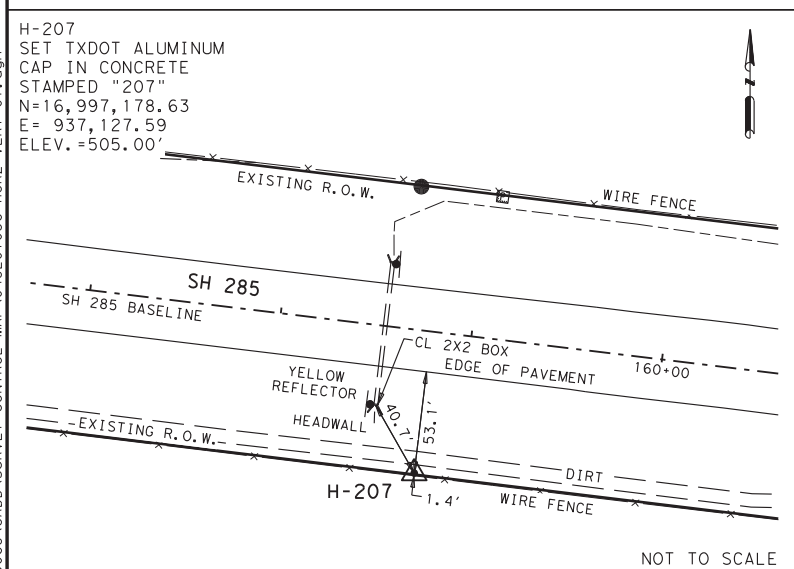
H-204: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE TRAVEL EAST ALONG SH 285 0.57 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, ACROSS FROM AND 60 FEET EAST OF THE ENTRANCE TO A WATER TREATMENT PLANT.



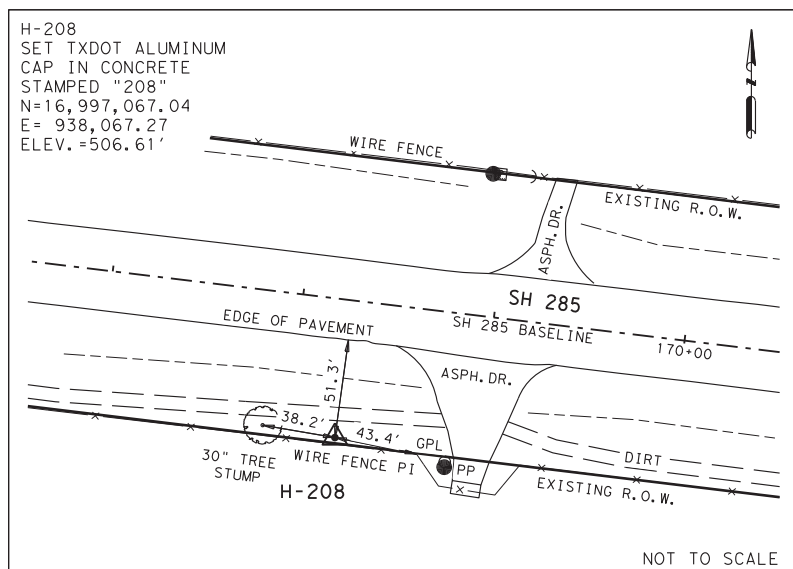
H-205: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 0.75 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 300 FEET WEST OF NORIECITAS CREEK.



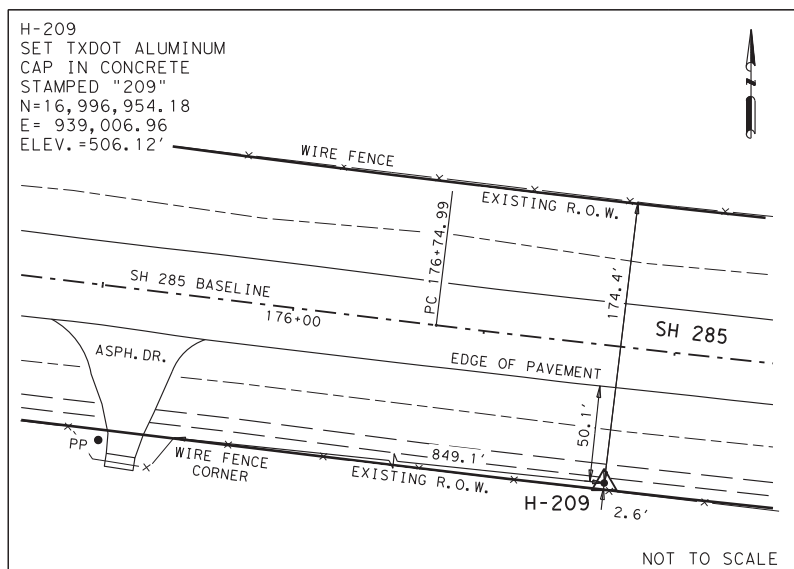
H-206: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 0.90 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 650 FEET EAST OF NORIECITAS CREEK.



H-207: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 1.1 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1,600 FEET EAST OF NORIECITAS CREEK.



H-208: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 1.3 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2,500 FEET EAST OF NORIECITAS CREEK AND 70 FEET WEST OF A DRIVEWAY.



H-209: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 1.5 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2,300 FEET WEST OF MESQUITE CREEK.

1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. UNIT OF MEASURE: U.S. SURVEY FEET

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS8E.



LANDTECH

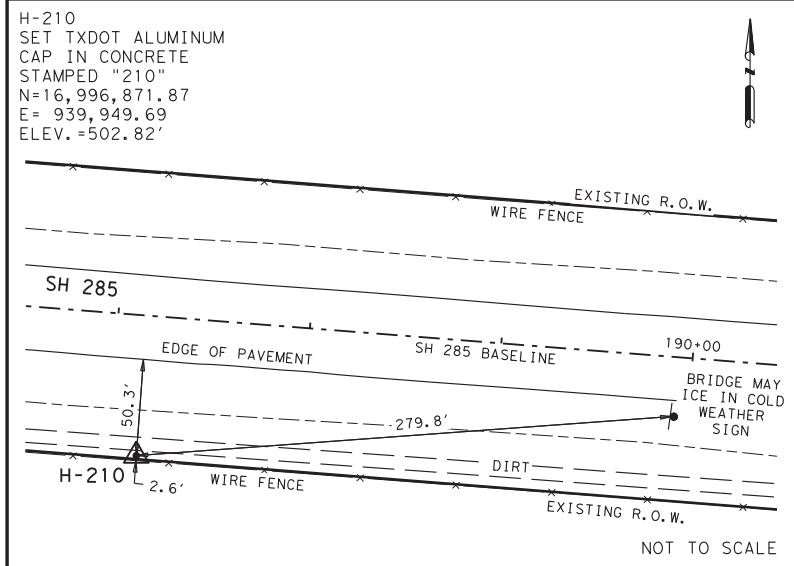
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

**SH 285
HORIZONTAL & VERTICAL
CONTROL**

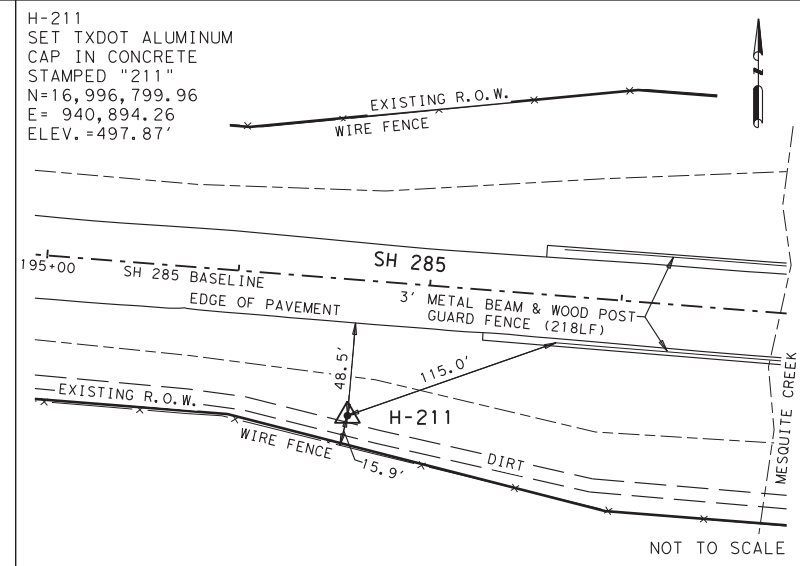
SHEET 1 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.		
6	TX		SH 285		
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.	JOB NO.	SHEET NO.
PHARR	JIM HOGG	0482	01	036	175

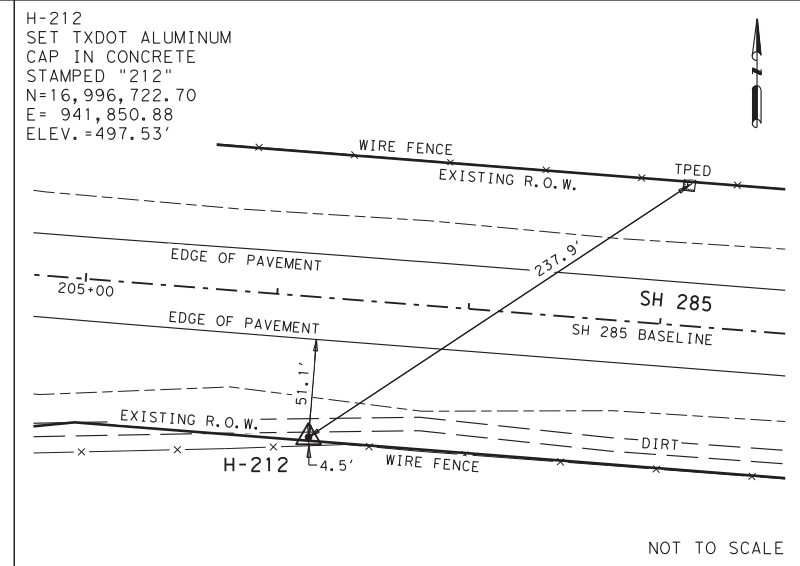
S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036+H02Z+VERT-01.dgn



H-210: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 1.65 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1,350 FEET WEST OF MESQUITE CREEK.



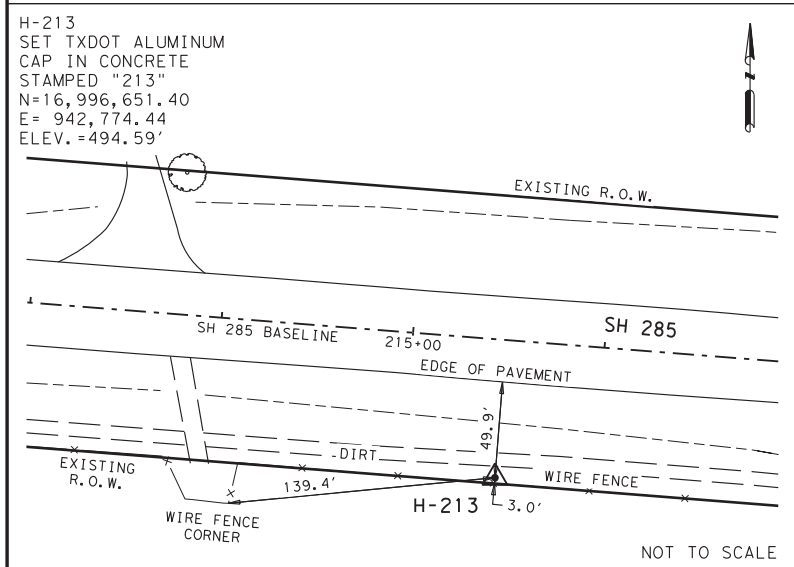
H-211: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 1.8 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 400 FEET WEST OF MESQUITE CREEK.



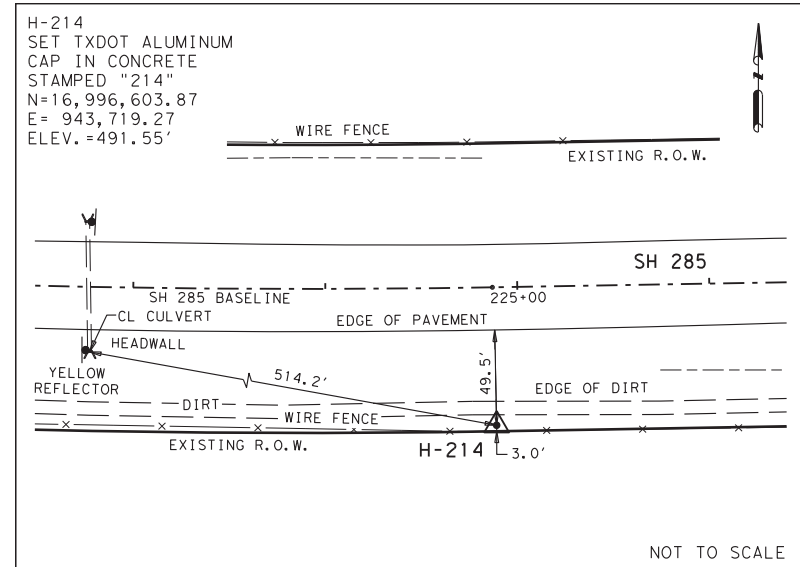
H-212: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 2.0 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 560 FEET EAST OF MESQUITE CREEK.

1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. UNIT OF MEASURE: U.S. SURVEY FEET

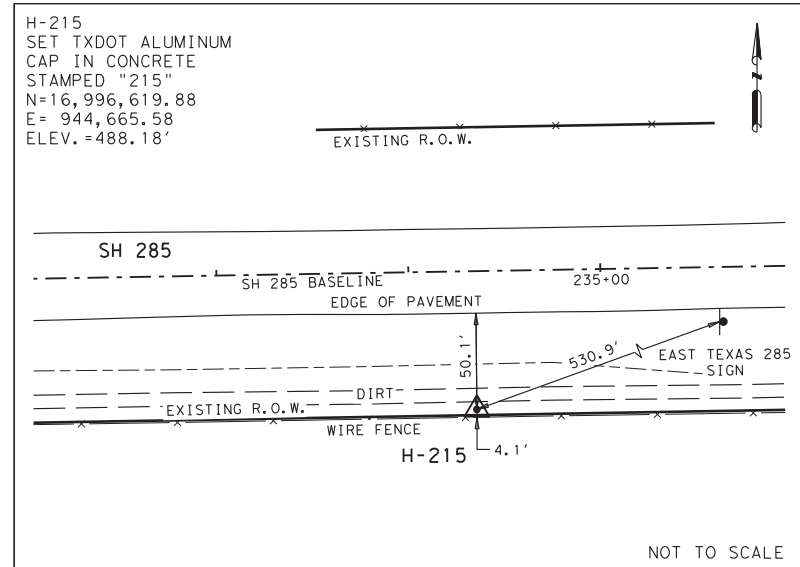
THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



H-213: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 2.2 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1,500 FEET EAST OF MESQUITE CREEK.



H-214: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 2.4 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2,400 FEET EAST OF MESQUITE CREEK.

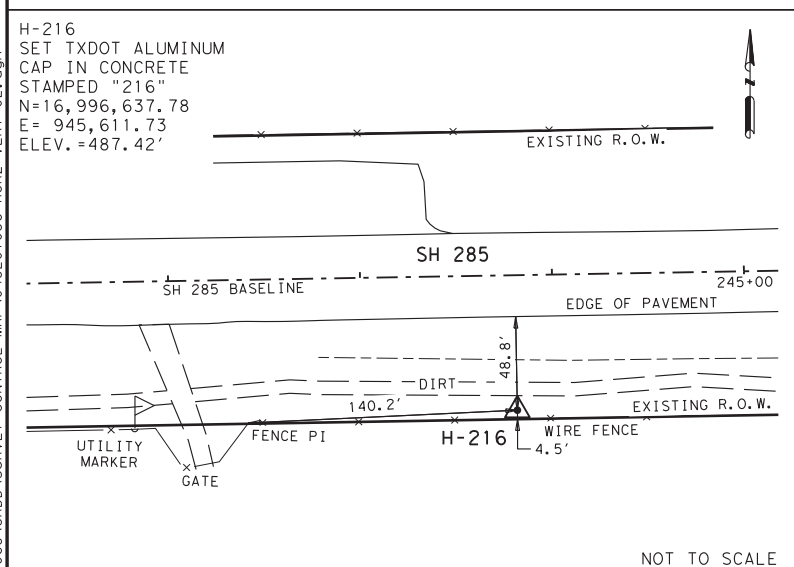


H-215: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 2.5 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 0.6 MILES EAST OF MESQUITE CREEK.

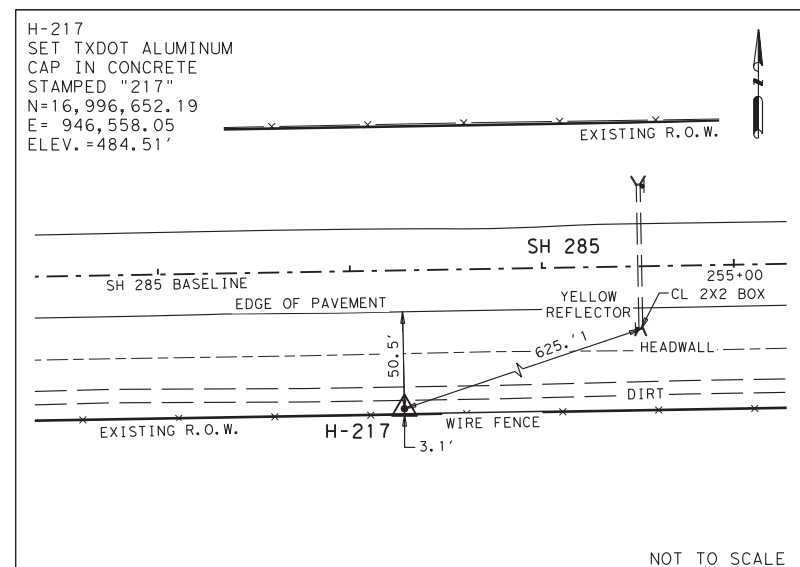


W. R. Wolfman, III
08-26-2021

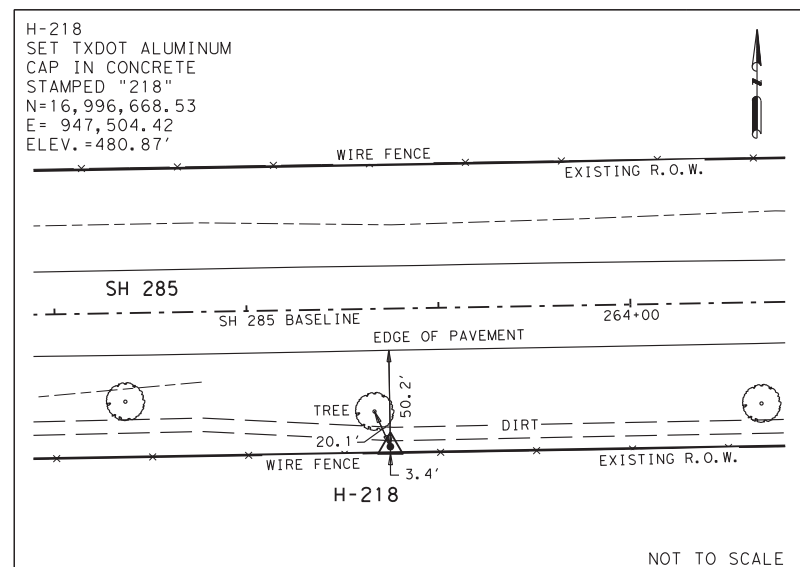
THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.



H-216: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 2.7 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 0.8 MILES EAST OF MESQUITE CREEK AND 170 FEET EAST OF A DRIVEWAY.



H-217: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 2.9 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1.0 MILES EAST OF MESQUITE CREEK.



H-218: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 3.1 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1.2 MILES EAST OF MESQUITE CREEK.

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*H0RZ*VERT-02.dgn

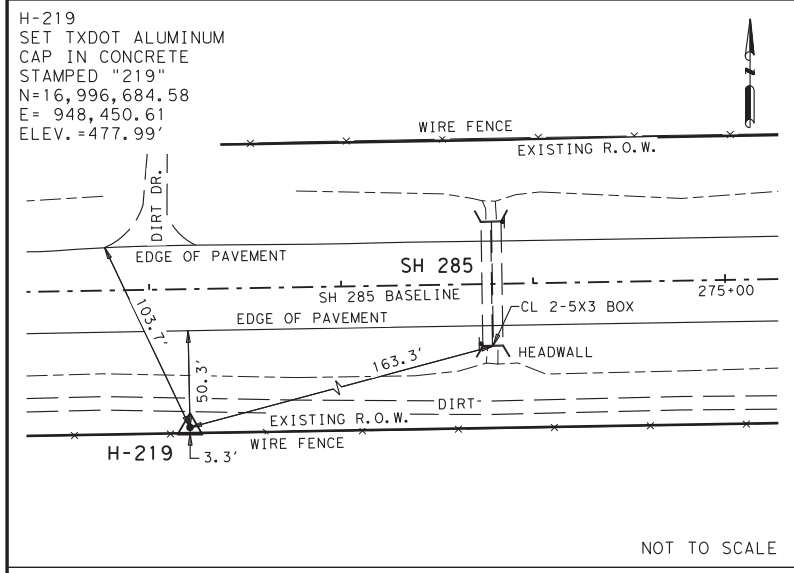
© 2021 Texas Department of Transportation

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

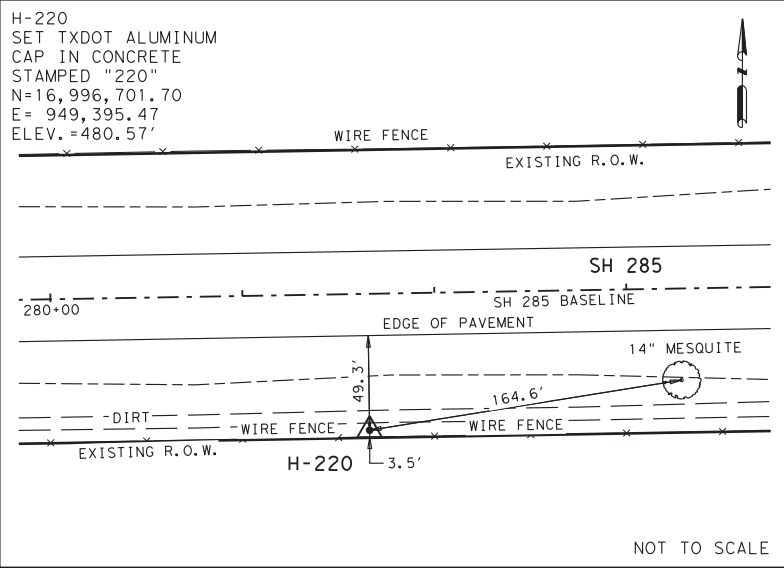
**SH 285
HORIZONTAL & VERTICAL
CONTROL**

SHEET 2 OF 6

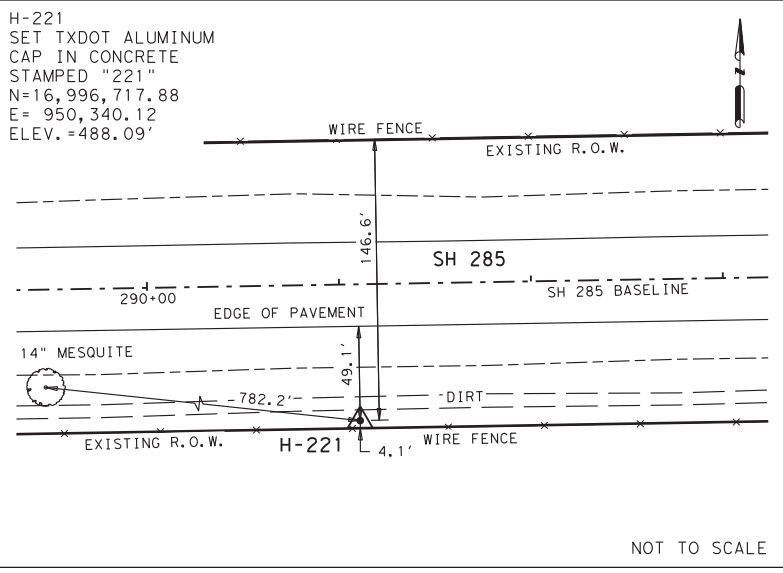
FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			176



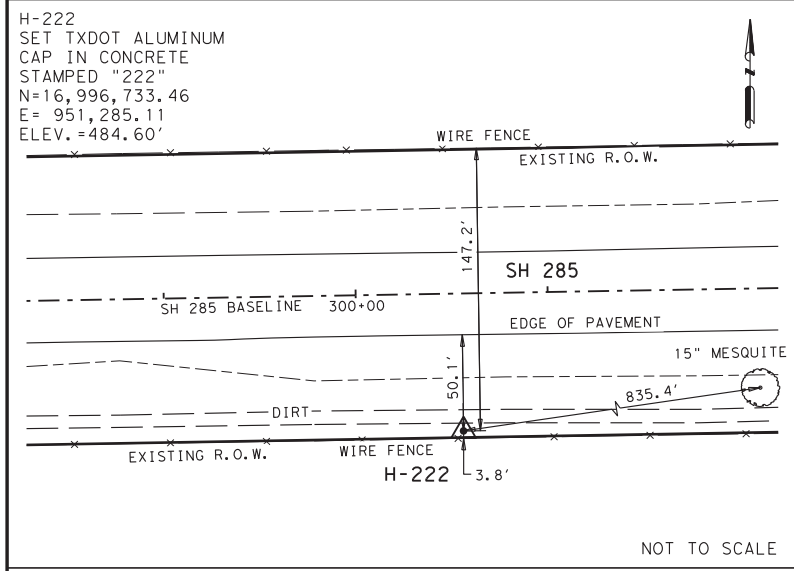
H-219: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 3.25 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1.4 MILES EAST OF MESQUITE CREEK AND ACROSS FROM A DRIVEWAY.



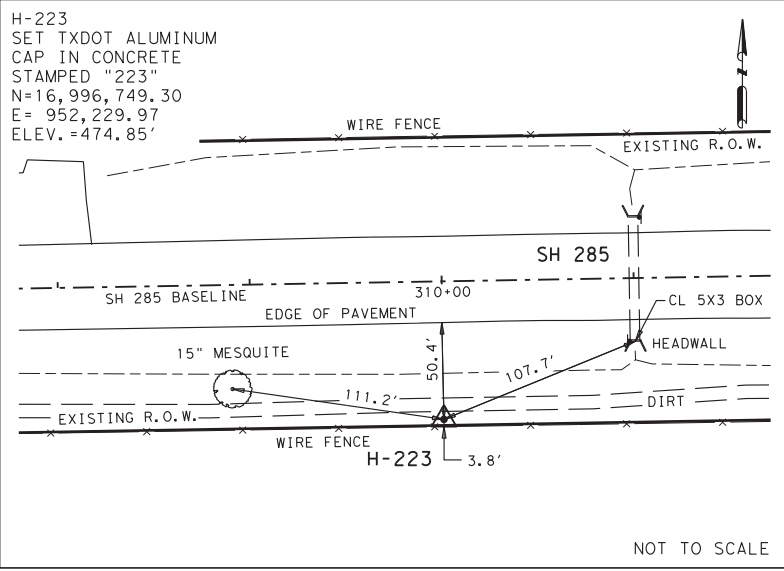
H-220: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 3.45 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1.5 MILES EAST OF MESQUITE CREEK.



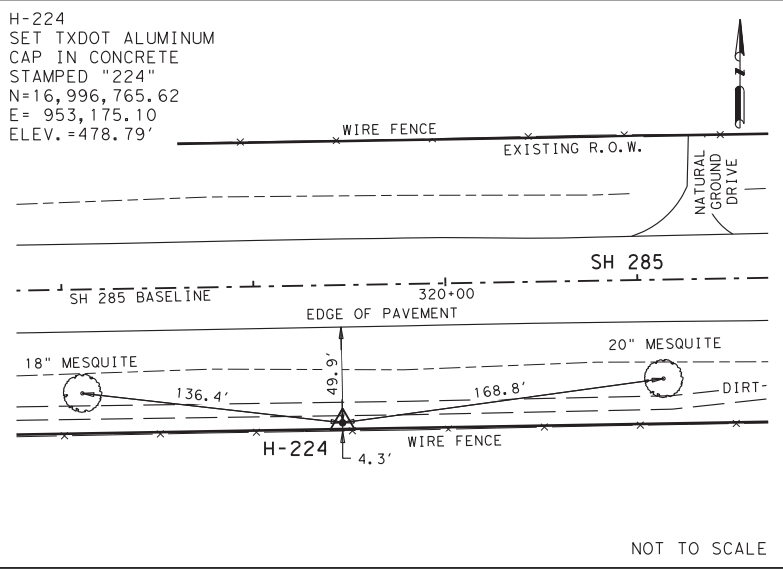
H-221: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 3.6 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1.7 MILES EAST OF MESQUITE CREEK.



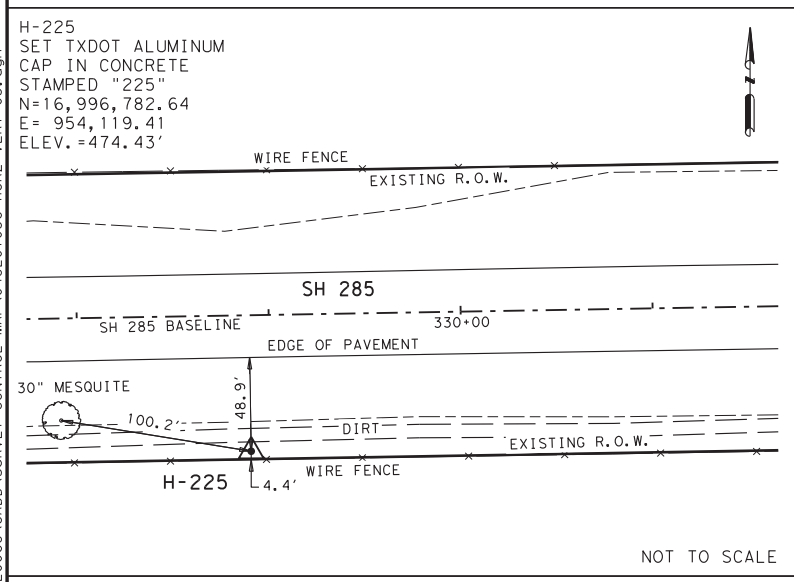
H-222: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 3.8 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 1.9 MILES EAST OF MESQUITE CREEK.



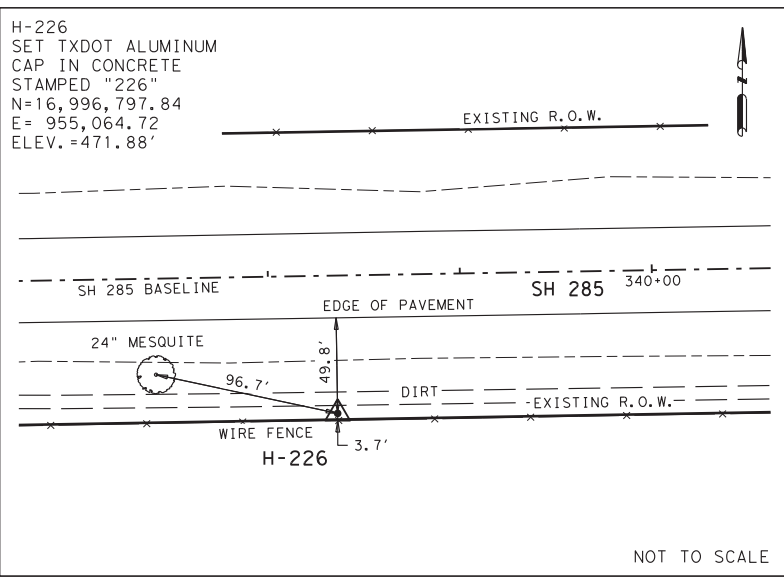
H-223: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 4.0 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2.1 MILES EAST OF MESQUITE CREEK.



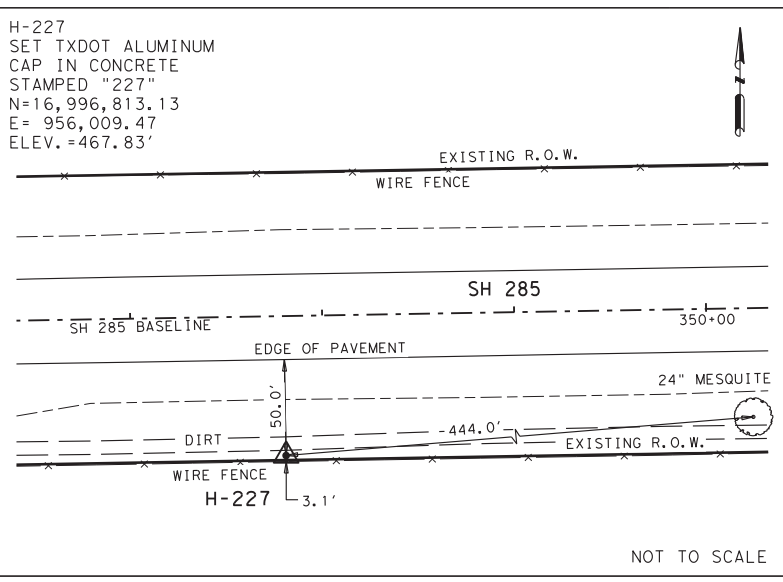
H-224: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 4.2 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2.25 MILES EAST OF MESQUITE CREEK AND 625 FEET WEST OF A DRIVEWAY.



H-225: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 4.35 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2.4 MILES EAST OF MESQUITE CREEK AND 300 FEET EAST OF A DRIVEWAY.



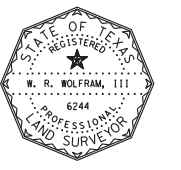
H-226: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 4.5 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2.6 MILES EAST OF MESQUITE CREEK.



H-227: FROM THE INTERSECTION OF SH 285 AND FM 1017/S Vaello Ave, TRAVEL EAST ALONG SH 285 4.7 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 2.8 MILES EAST OF MESQUITE CREEK.

1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. UNIT OF MEASURE: U.S. SURVEY FEET

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfgram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036*HORIZ*VERT-03.dgn

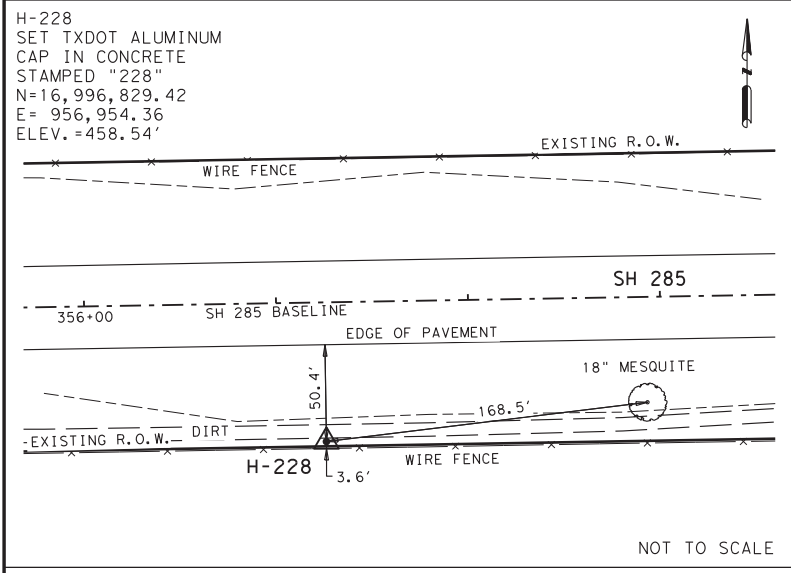
© 2021 Texas Department of Transportation

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

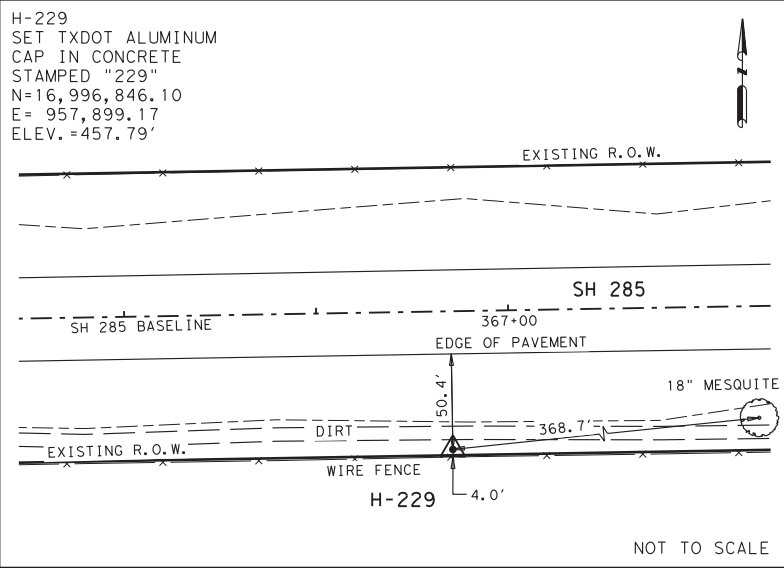
**SH 285
HORIZONTAL & VERTICAL
CONTROL**

SHEET 3 OF 6

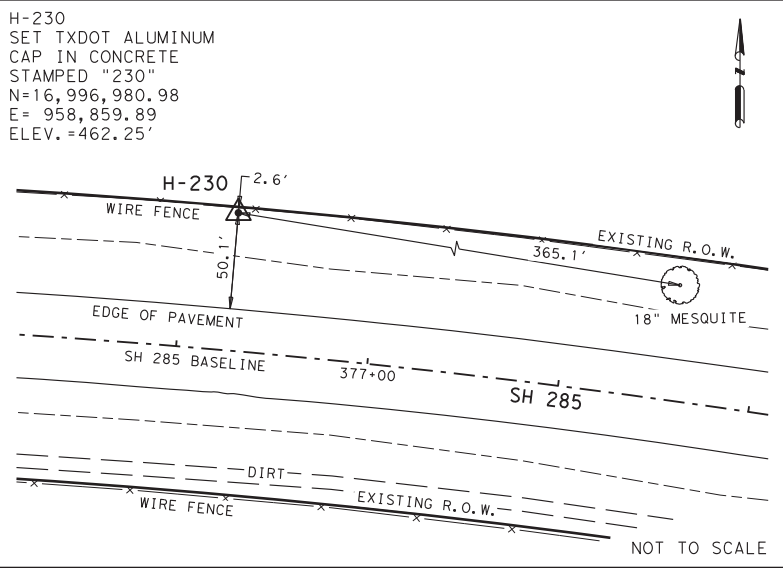
FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			177



H-228: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 4.9 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 3.0 MILES EAST OF MESQUITE CREEK.



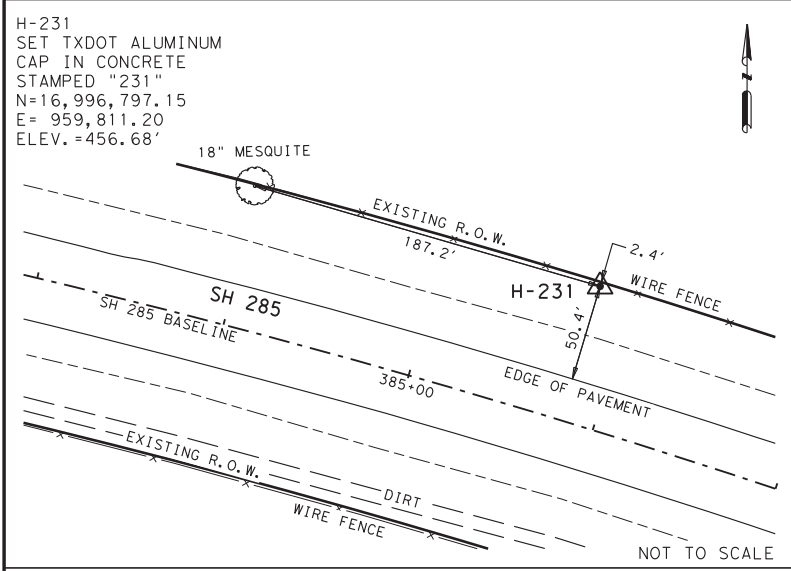
H-229: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 5.05 MILES, LOCATED ON THE SOUTH SIDE OF SH 285, 3.15 MILES EAST OF MESQUITE CREEK.



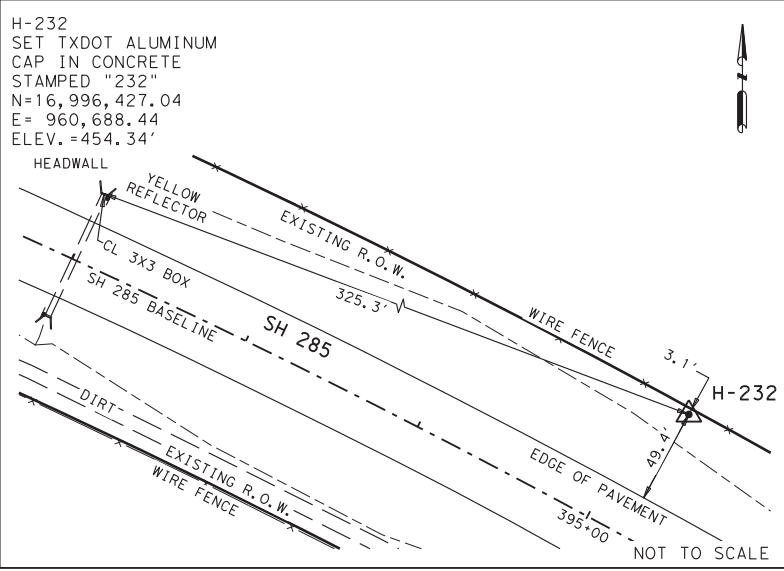
H-230: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 5.2 MILES, LOCATED ON THE NORTH SIDE OF SH 285, 3.3 MILES EAST OF MESQUITE CREEK.

1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. UNIT OF MEASURE: U.S. SURVEY FEET

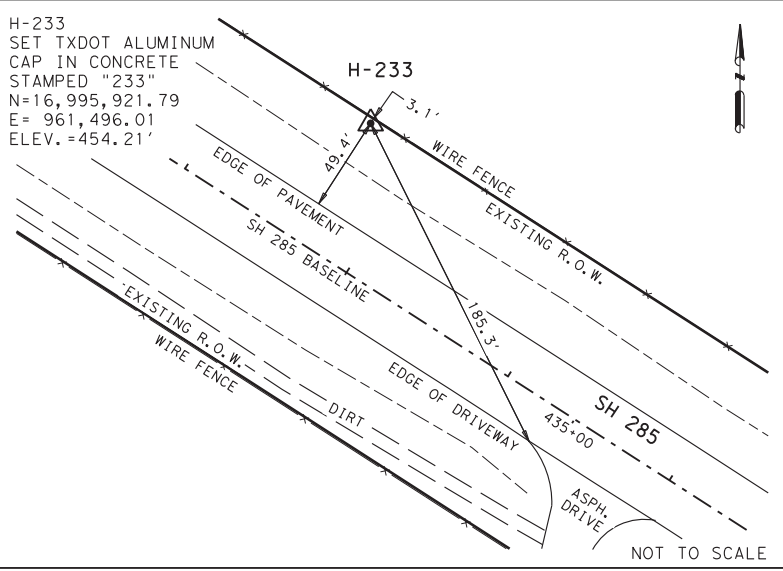
THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



H-231: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 5.4 MILES, LOCATED ON THE NORTH SIDE OF SH 285, 3.5 MILES EAST OF MESQUITE CREEK AND 3.8 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-232: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 5.6 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 3.7 MILES EAST OF MESQUITE CREEK AND 3.6 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.

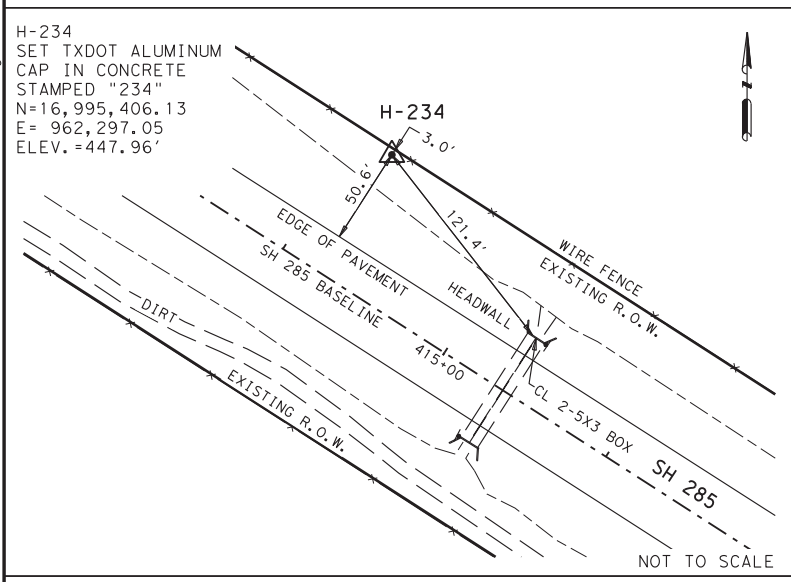


H-233: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 5.8 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 3.9 MILES EAST OF MESQUITE CREEK, ACROSS FROM AND 200 FEET WEST OF A DRIVEWAY AND 3.4 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.

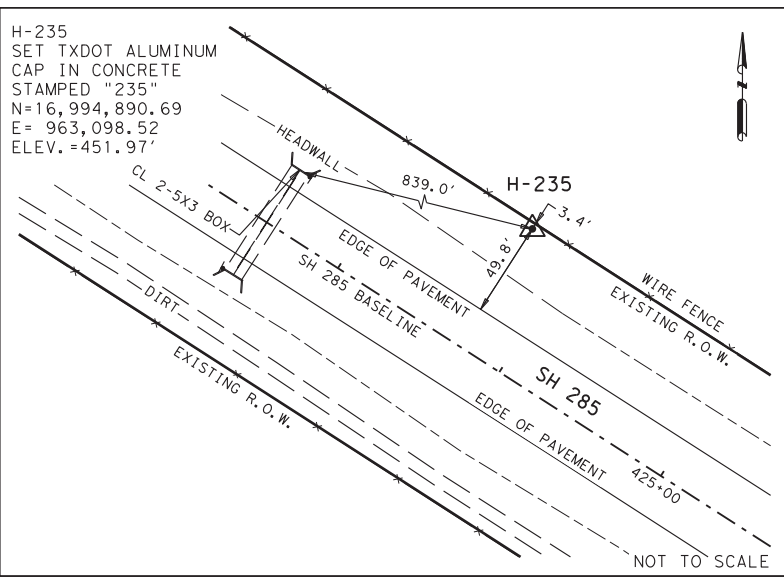


W. R. Wolfgram, III
08-26-2021

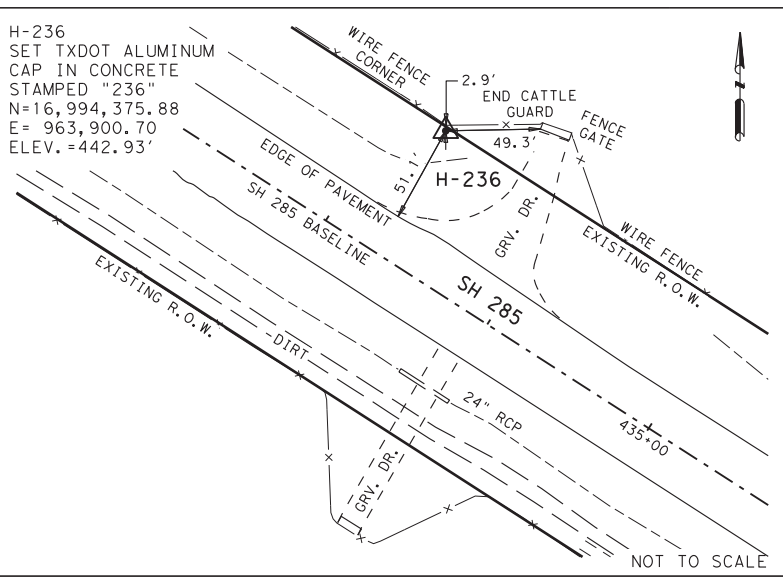
THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.



H-234: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 5.95 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 3.2 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-235: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 6.1 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 3.05 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-236: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 6.3 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 55 FEET WEST OF A DRIVEWAY AND 2.9 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.

© 2021 Texas Department of Transportation

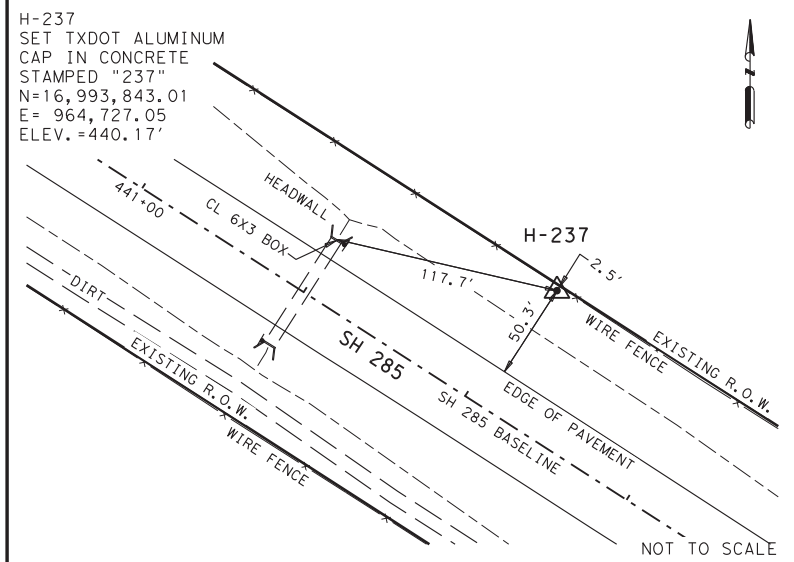
LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBP&S Registration No. 10019100

**SH 285
HORIZONTAL & VERTICAL
CONTROL**

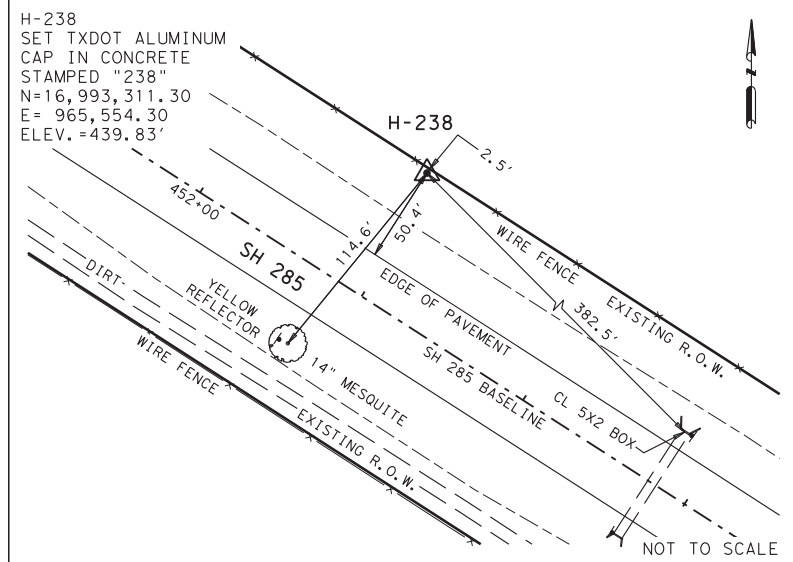
SHEET 4 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			178

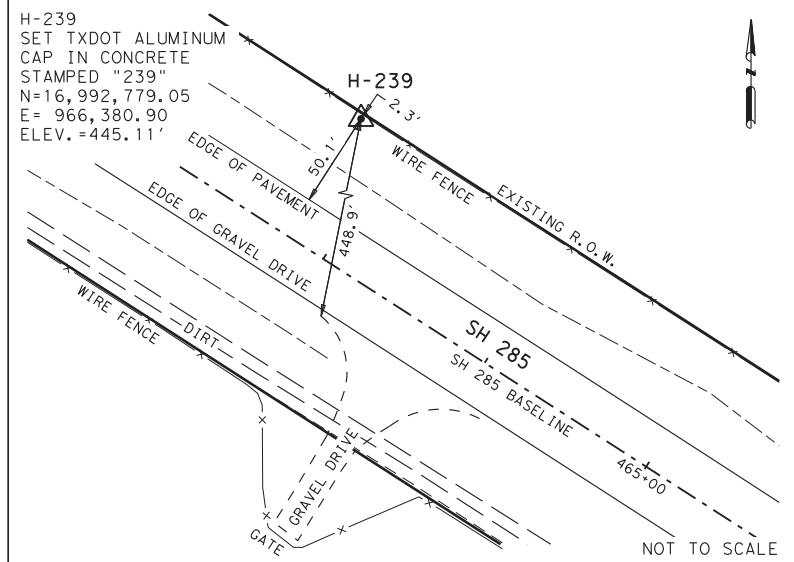
S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036+H0RZ+VERT-04.dgn



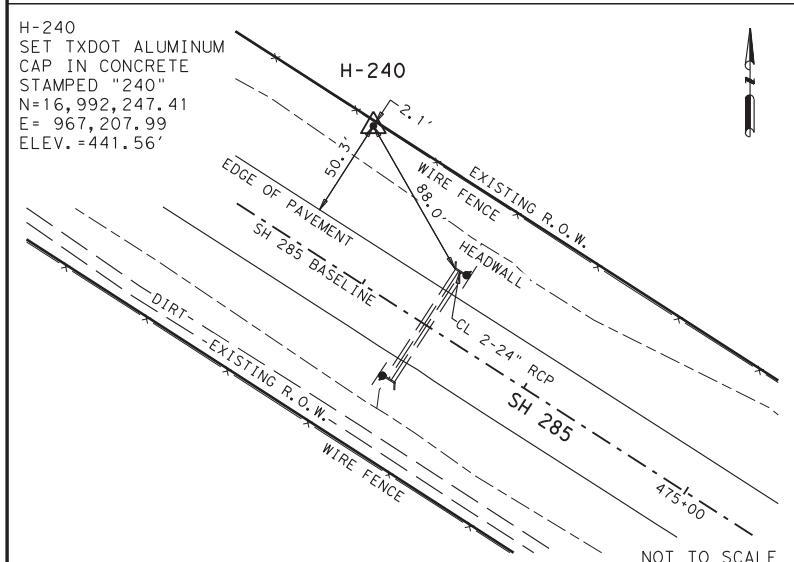
H-237: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 6.5 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 2.7 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



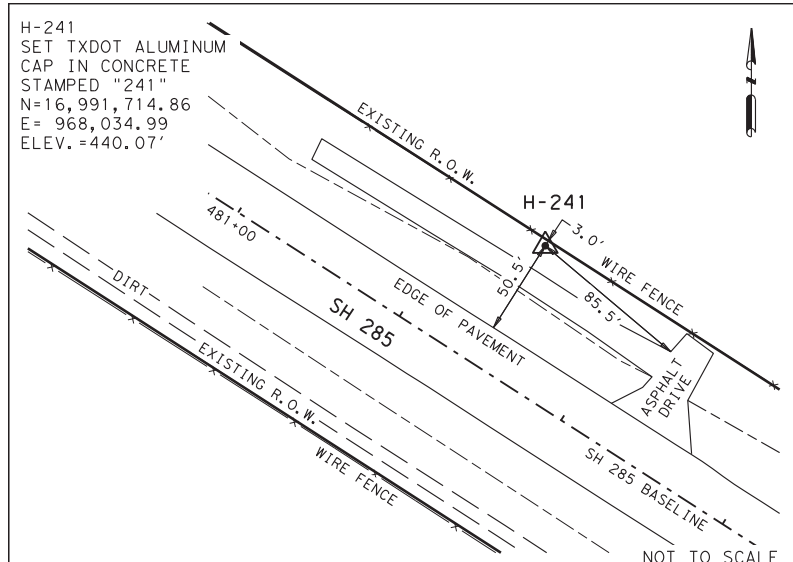
H-238: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 6.7 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 2.5 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



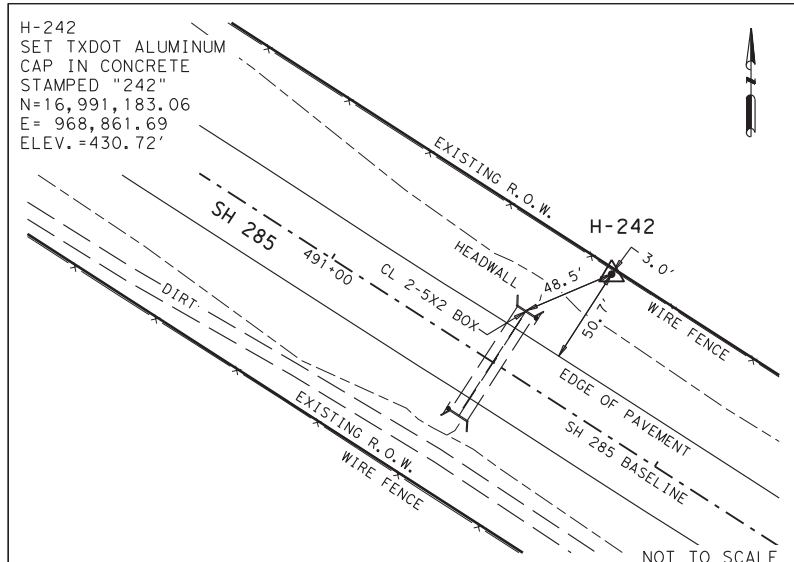
H-239: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 6.9 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, ACROSS FROM AND 500 FEET WEST OF A DRIVEWAY AND 2.3 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



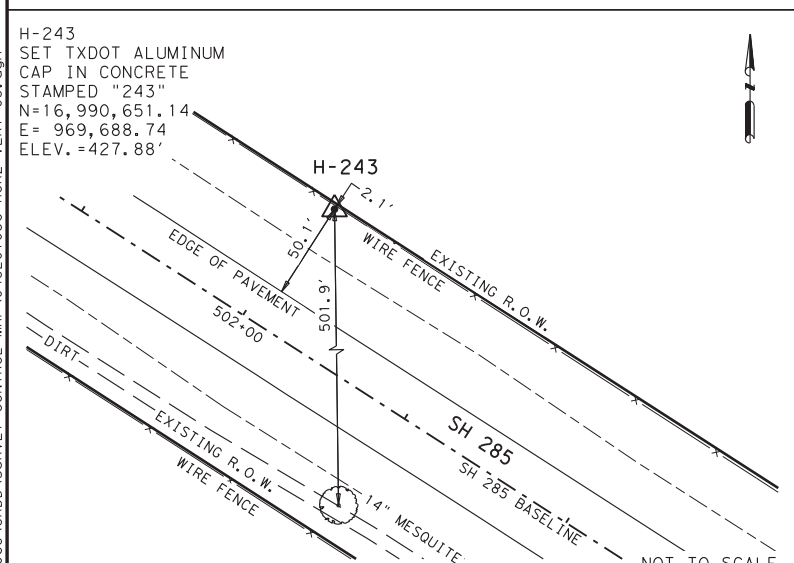
H-240: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 7.1 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, ACROSS FROM AND 500 FEET EAST OF A DRIVEWAY AND 2.1 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



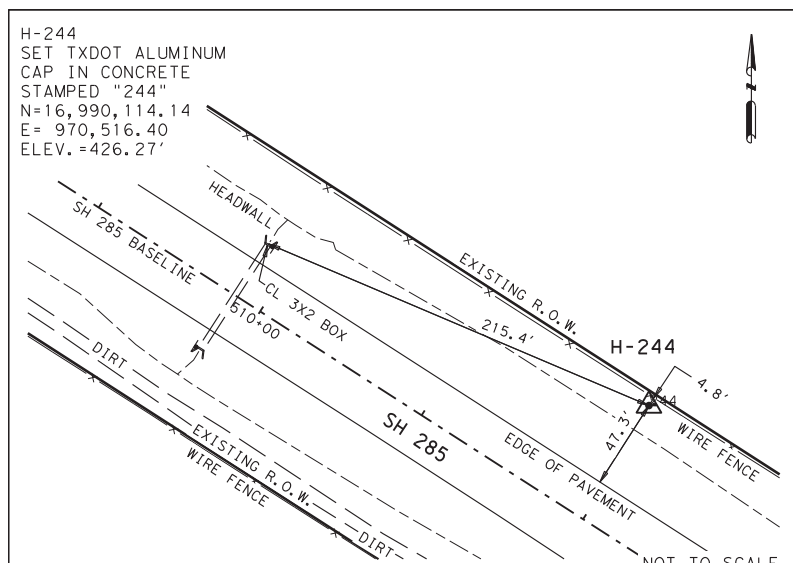
H-241: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 7.2 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 1.95 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



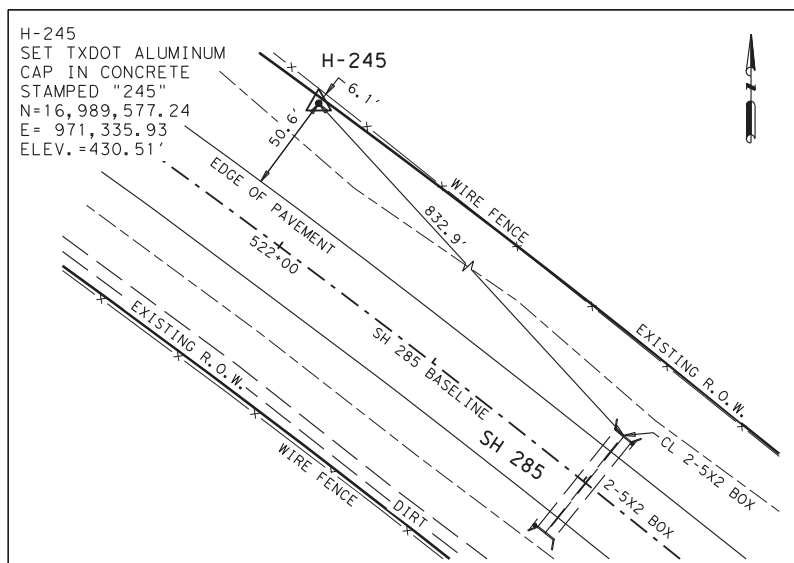
H-242: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 7.4 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 1.8 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-243: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 7.6 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 1.6 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-244: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 7.8 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 1.4 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-245: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 8.0 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 1.2 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.

1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. UNIT OF MEASURE: U.S. SURVEY FEET

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfgram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036+H0BZ+VERT-05.dgn

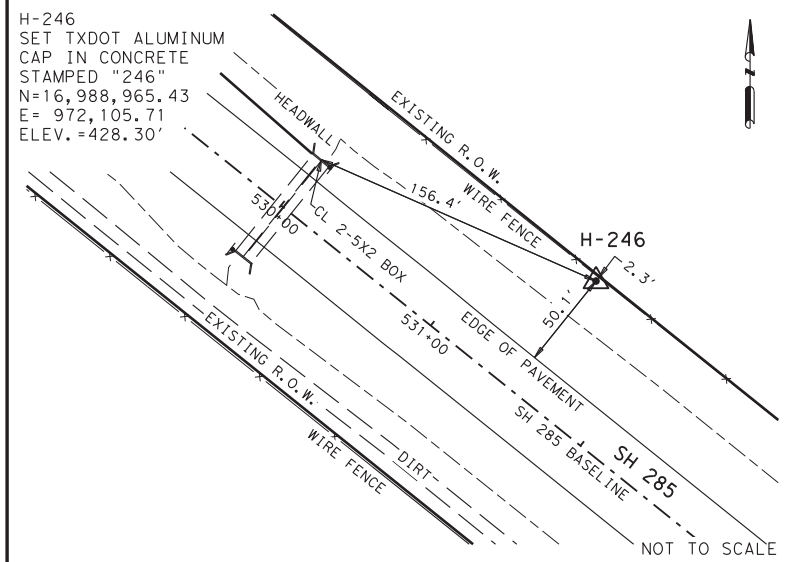
© 2021 Texas Department of Transportation

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

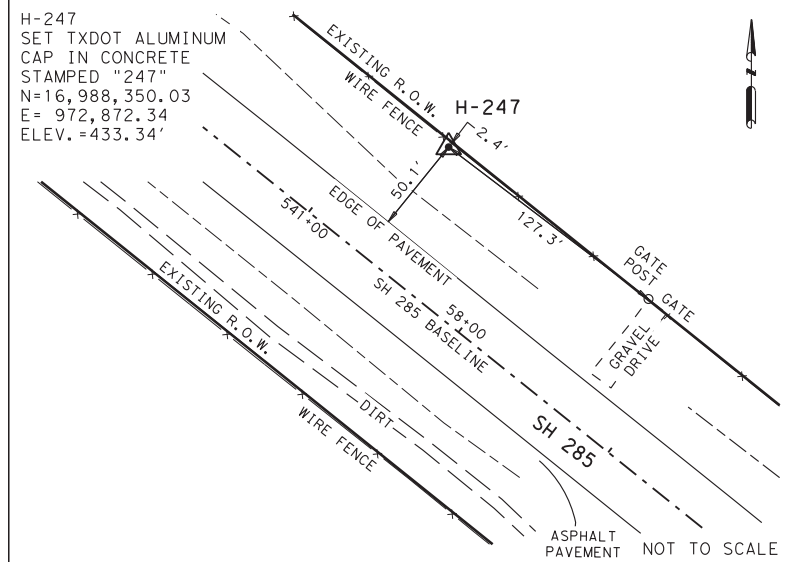
**SH 285
HORIZONTAL & VERTICAL
CONTROL**

SHEET 5 OF 6

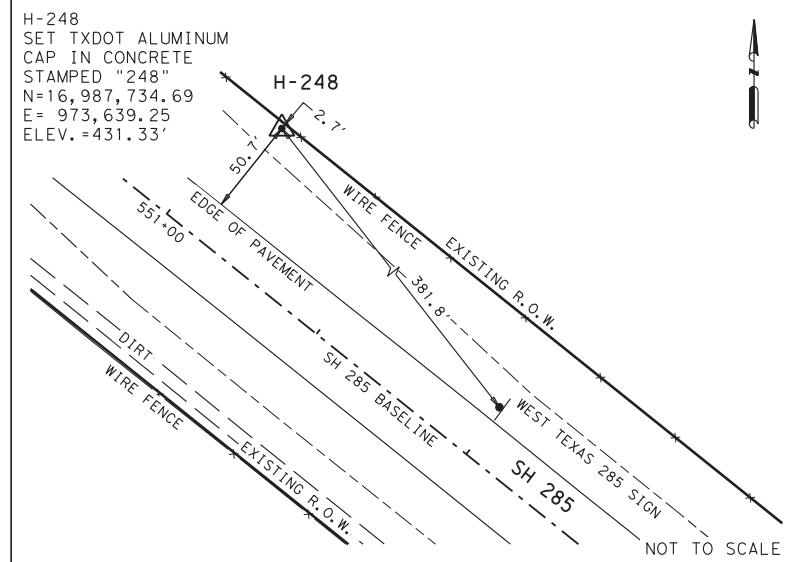
FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
PHARR	JIM HOGG	0482	01
			JOB NO.
			036
			SHEET NO.
			179



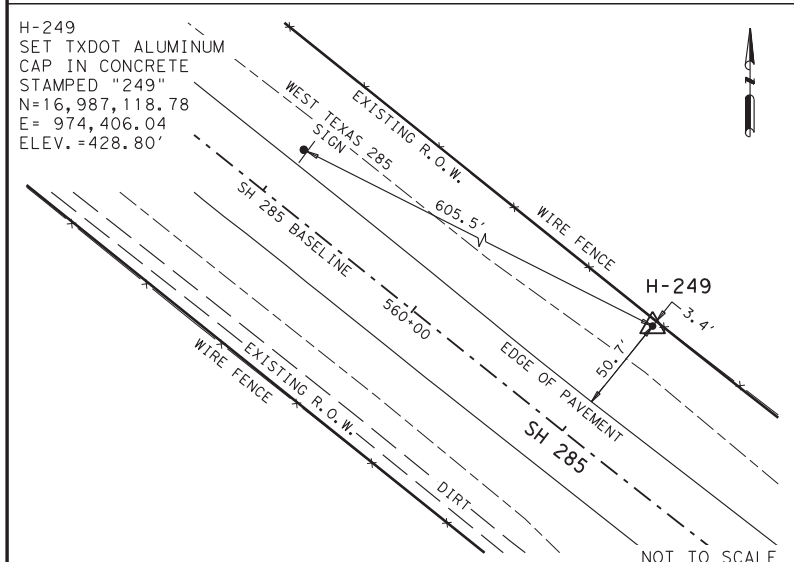
H-246: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 8.2 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 1.0 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



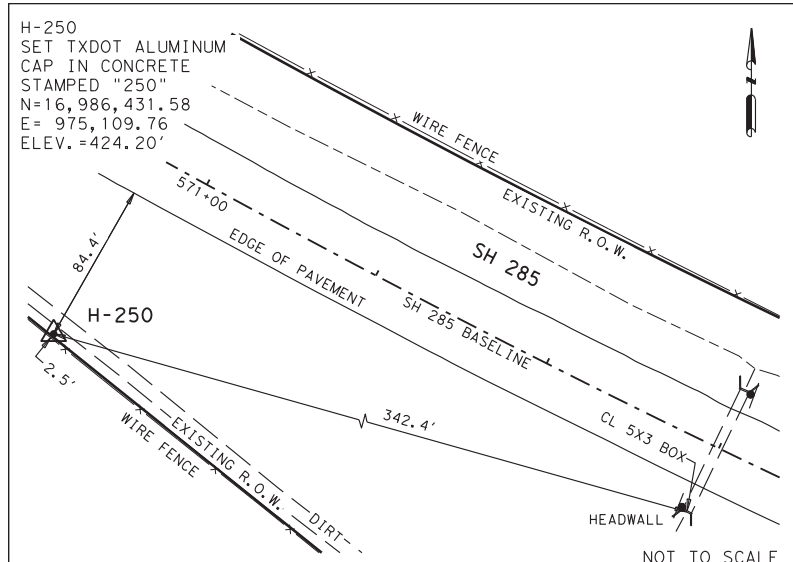
H-247: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 8.35 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, ACROSS FROM AND 200 FEET WEST OF A DRIVEWAY AND 0.80 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



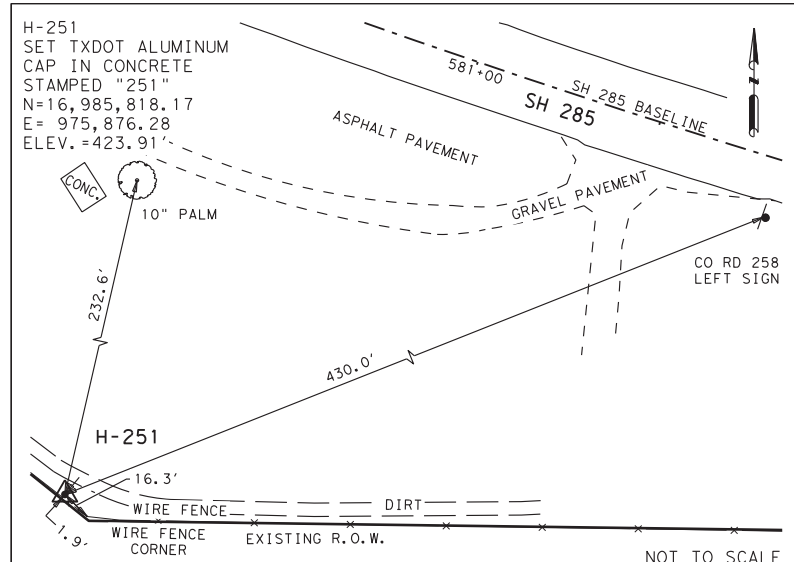
H-248: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 8.55 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 0.65 MILES WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-249: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 8.7 MILES, LOCATED ON THE NORTHEAST SIDE OF SH 285, 2,425 FEET WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-250: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 8.9 MILES, LOCATED ON THE SOUTHWEST SIDE OF SH 285, 1,450 FEET WEST OF BROOKS/JIM HOGG COUNTY LINE.



H-251: FROM THE INTERSECTION OF SH 285 AND FM 1017/S VAELO AVE, TRAVEL EAST ALONG SH 285 9.1 MILES, LOCATED ON THE SOUTHWEST SIDE OF SH 285, 600 FEET WEST OF BROOKS/JIM HOGG COUNTY LINE AND ACROSS FROM AND 850 FEET WEST OF CO RD 258.

1. ALL BEARINGS AND COORDINATES ARE BASED ON THE TEXAS COORDINATE SYSTEM, SOUTH ZONE, (4205), NORTH AMERICAN DATUM OF 1983 (NAD 83), 2011 ADJUSTMENT, EPOCH 2010.00. ALL DISTANCES AND COORDINATES SHOWN ARE SURFACE VALUES AND MAY BE CONVERTED TO GRID BY DIVIDING BY THE COMBINED ADJUSTMENT FACTOR OF 1.00004.
2. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/N=16,997,912.987 E=927,621.055; TXRS/N=13,750,195.46 E=2,984,381.03; AND PHR-024-0045/N=16,985,113.637 E=976,816.795 PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR HORIZONTAL CONTROL. HORIZONTAL SURVEY METHOD: BASE STATION (RTK) AND ROVER TXDOT RTN (VRS)
3. ALL ELEVATIONS HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
4. TXDOT PRIMARY CONTROL POINTS HEBBRONVILLE MNT/541.219'; PHR-024-0045/424.660' PREPARED BY COBB, FENDLEY & ASSOCIATES DATED NOVEMBER 4, 2015, WERE HELD FOR VERTICAL CONTROL. VERTICAL SURVEY METHOD: DIGITAL LEVELING
5. FIELD SURVEYS WERE PERFORMED BETWEEN JUNE, 2019 AND MARCH, 2021.
6. UNIT OF MEASURE: U.S. SURVEY FEET

THE CONTROL POINTS SHOWN HEREIN WERE DETERMINED BY A SURVEY MADE ON THE GROUND UNDER MY SUPERVISION. SURVEY DATE: MARCH, 2021



W. R. Wolfgram, III
08-26-2021

THIS SURVEY INFORMATION HAS BEEN ACCEPTED INTO THIS PS&E.

S:\2019\1920068\CADD\SURVEY CONTROL MAP\048201036+H0RZ+VERT-06.dgn

© 2021 Texas Department of Transportation

LANDTECH
2525 North Loop West, Suite 300,
Houston, Texas 77008
T: 713-861-7068 F: 713-861-4131
TBPELS Registration No. 10019100

**SH 285
HORIZONTAL & VERTICAL
CONTROL**
SHEET 6 OF 6

FED. RD. DIV. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
6	TX		SH 285
STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO. JOB NO. SHEET NO.
PHARR	JIM HOGG	0482	01 036 180

DATE: 2/12/2024 8:00:18 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\SH 285 Roadway Data Sheet.dgn

Beginning chain PROP_SH285 description
 Feature: Geom_Centerline

Point 4 N 16,998,015.7436 E 930,904.2091 Sta 96+00.00

Course from 4 to PC PROP_SH285_3 S 80° 55' 32.73" E Dist 2.7450

Curve Data

Curve PROP_SH285_3
 P.I. Station 98+19.44 N 16,997,981.1352 E 931,120.9003
 Delta = 2° 10' 00.00" (LT)
 Degree = 0° 30' 00.00"
 Tangent = 216.6925
 Length = 433.3333
 Radius = 11,459.1558
 External = 2.0486
 Long Chord = 433.3075
 Mid. Ord. = 2.0483
 P.C. Station 96+02.75 N 16,998,015.3107 E 930,906.9198
 P.T. Station 100+36.08 N 16,997,955.0740 E 931,336.0199
 C.C. N 17,009,331.0530 E 932,714.1909
 Back = S 80° 55' 32.73" E
 Ahead = S 83° 05' 32.72" E
 Chord Bear = S 82° 00' 32.73" E

Point 5 N 16,997,955.0740 E 931,336.0199 Sta 100+36.08

Course from 5 to PC PROP_SH285_8 S 83° 05' 32.73" E Dist 7,638.9103

Curve Data

Curve PROP_SH285_8
 P.I. Station 179+17.02 N 16,997,007.2481 E 939,159.7570
 Delta = 2° 25' 11.82" (LT)
 Degree = 0° 30' 00.00"
 Tangent = 242.0310
 Length = 483.9900
 Radius = 11,459.1600
 External = 2.5557
 Long Chord = 483.9540
 Mid. Ord. = 2.5551
 P.C. Station 176+74.99 N 16,997,036.3567 E 938,919.4828
 P.T. Station 181+58.98 N 16,996,988.3107 E 939,401.0459
 C.C. N 17,008,412.3398 E 940,297.6543
 Back = S 83° 05' 32.73" E
 Ahead = S 85° 30' 44.54" E
 Chord Bear = S 84° 18' 08.63" E

Course from PT PROP_SH285_8 to PC PROP_SH285_11 S 85° 30' 44.62" E Dist 3,781.3850

Curve Data

Curve PROP_SH285_11
 P.I. Station 222+13.91 N 16,996,671.0391 E 943,443.5423
 Delta = 5° 28' 00.11" (LT)
 Degree = 1° 00' 00.00"
 Tangent = 273.5425
 Length = 546.6700
 Radius = 5,729.5800
 External = 6.5260
 Long Chord = 546.4627
 Mid. Ord. = 6.5186
 P.C. Station 219+40.36 N 16,996,692.4420 E 943,170.8383
 P.T. Station 224+87.03 N 16,996,675.7133 E 943,717.0449
 C.C. N 17,002,404.4568 E 943,619.1405
 Back = S 85° 30' 44.62" E
 Ahead = N 89° 01' 15.27" E
 Chord Bear = S 88° 14' 44.67" E

Course from PT PROP_SH285_11 to PC PROP_SH285_14 N 89° 01' 15.38" E Dist 14,660.0100

Curve Data

Curve PROP_SH285_14
 P.I. Station 385+36.76 N 16,996,949.9547 E 959,764.4243
 Delta = 33° 44' 00.21" (RT)
 Degree = 1° 15' 00.00"
 Tangent = 1,389.7127
 Length = 2,698.6700
 Radius = 4,583.6600
 External = 206.0415
 Long Chord = 2,659.8611
 Mid. Ord. = 197.1781
 P.C. Station 371+47.04 N 16,996,926.2087 E 958,374.9146
 P.T. Station 398+45.71 N 16,996,198.0672 E 960,933.1700
 C.C. N 16,992,343.2179 E 958,453.2355
 Back = N 89° 01' 15.38" E
 Ahead = S 57° 14' 44.40" E
 Chord Bear = S 74° 06' 44.51" E

Course from PT PROP_SH285_14 to PC PROP_SH285_17 S 57° 14' 44.62" E Dist 11,959.8666

Curve Data

Curve PROP_SH285_17
 P.I. Station 521+05.85 N 16,989,564.8752 E 971,243.9325
 Delta = 5° 59' 59.99" (RT)
 Degree = 1° 00' 00.00"
 Tangent = 300.2745
 Length = 600.0000
 Radius = 5,729.5800
 External = 7.8630
 Long Chord = 599.7259
 Mid. Ord. = 7.8522
 P.C. Station 518+05.58 N 16,989,727.3348 E 970,991.4021
 P.T. Station 524+05.58 N 16,989,376.9089 E 971,478.0978
 C.C. N 16,984,908.7657 E 967,891.4861
 Back = S 57° 14' 44.62" E
 Ahead = S 51° 14' 44.62" E
 Chord Bear = S 54° 14' 44.62" E

Course from PT PROP_SH285_17 to PC PROP_SH285_20 S 51° 14' 44.62" E Dist 3,694.4196

Curve Data


Curve PROP_SH285_20
 P.I. Station 580+72.85 N 16,985,829.3010 E 975,897.6503
 Delta = 37° 59' 59.95" (LT)
 Degree = 1° 00' 00.00"
 Tangent = 1,972.8518
 Length = 3,800.0000
 Radius = 5,729.5800
 External = 330.1421
 Long Chord = 3,730.7363
 Mid. Ord. = 312.1555
 P.C. Station 561+00.00 N 16,987,064.2700 E 974,359.1459
 P.T. Station 599+00.00 N 16,985,803.3295 E 977,870.3312
 C.C. N 16,991,532.4130 E 977,945.7578
 Back = S 51° 14' 44.62" E
 Ahead = S 89° 14' 44.57" E
 Chord Bear = S 70° 14' 44.59" E

Course from PT PROP_SH285_20 to 6 S 89° 14' 44.66" E Dist 2,100.0001

Point 6 N 16,985,775.6852 E 979,970.1494 Sta 620+00.00

Ending chain PROP_SH285 description

Pharr District Central Design

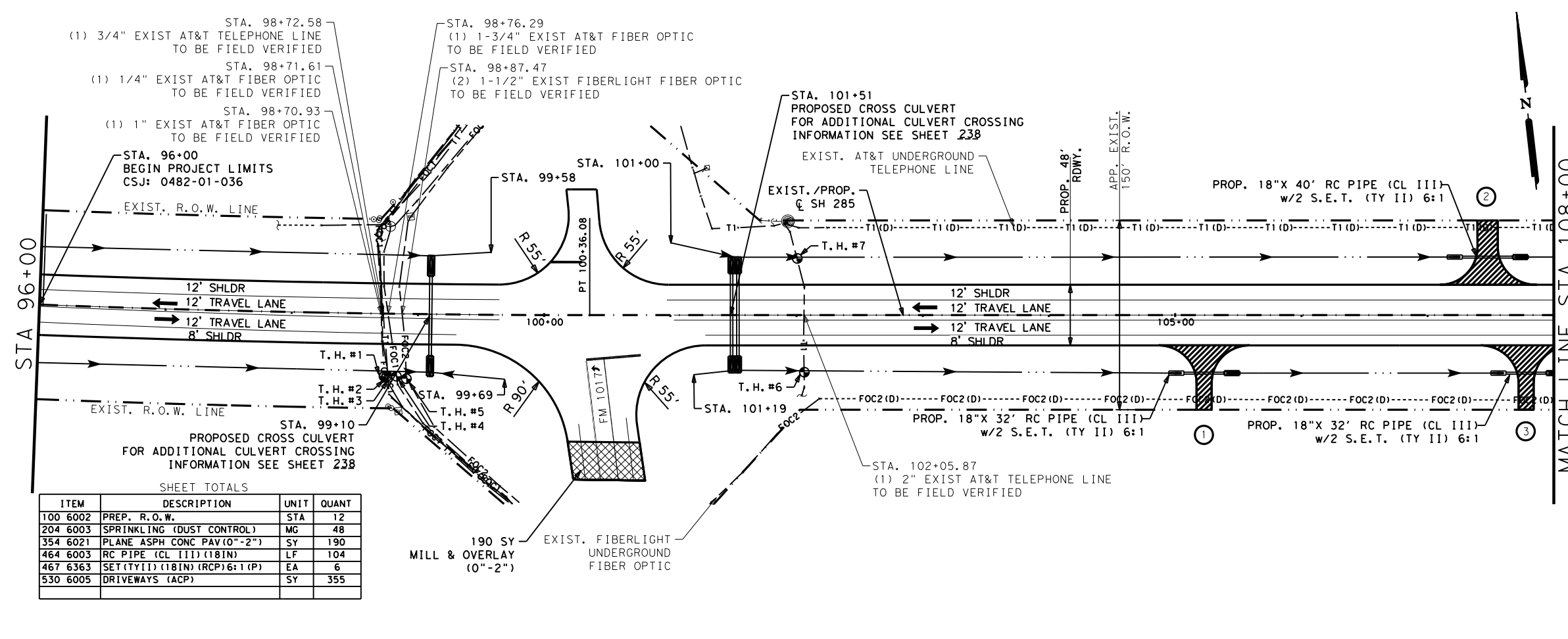


**SH 285
ROADWAY
DATA SHEET**

N. T. S. SHEET 1 OF 1

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	181

DATE: 2/7/2024 10:53:23 AM
 FILE: D:\p\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 1

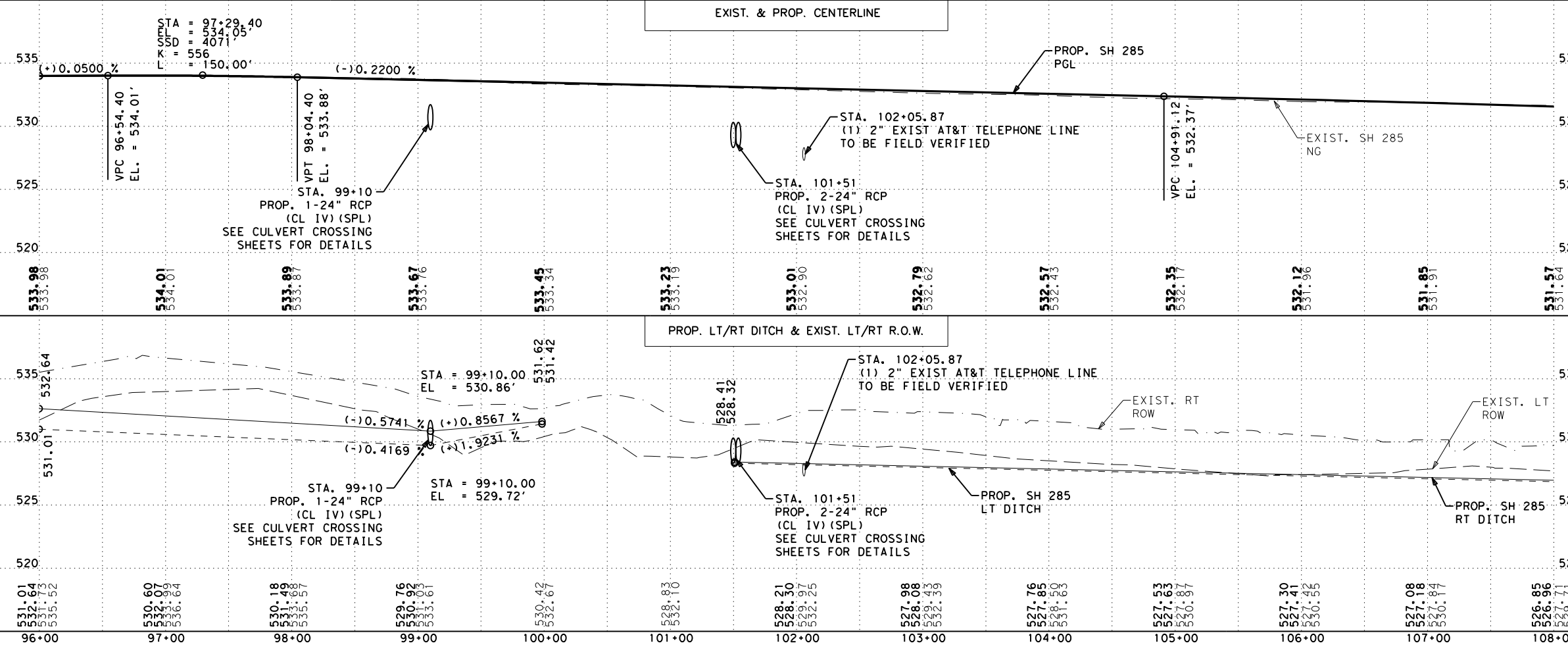


- ROADWAY LEGEND:**
- (*) DRIVEWAY NUMBER
 - [Hatched Box] PROP. ASPHALT DRIVEWAY
 - [Cross-hatched Box] PROP. MILL & OVERLAY (0"-2")
 - [Arrow] DIRECTION OF TRAFFIC FLOW
 - [Circle with X] PROP. SAFETY END TREATMENT
 - [Dashed Line] R.O.W. LINE
 - [Line with Arrow] PROP. DITCH FLOW LINE
 - [Circle with Center] TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

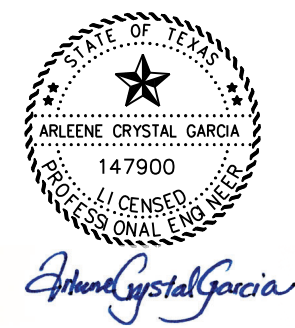
- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
354 6021	PLANE ASPH CONC PAV(0"-2")	SY	190
464 6003	RC PIPE (CL III) (18IN)	LF	104
467 6363	SET(TY I) (18IN) (RCP) 6:1 (P)	EA	6
530 6005	DRIVEWAYS (ACP)	SY	355



- LEGEND:**
- PROP. PGL ELEVATION ---
 - EXIST. PGL ELEVATION ---
 - PROP. LT DITCH ---
 - PROP. RT DITCH ---
 - EXIST. ELEV @ LT ROW ---
 - EXIST. ELEV @ RT ROW ---



02.13.2024

Pharr District Central Design



SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10' SHEET 1 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	182

DATE: 2/7/2024 10:53:26 AM
 FILE: D:\work\pharr\dot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 1

ROADWAY LEGEND:

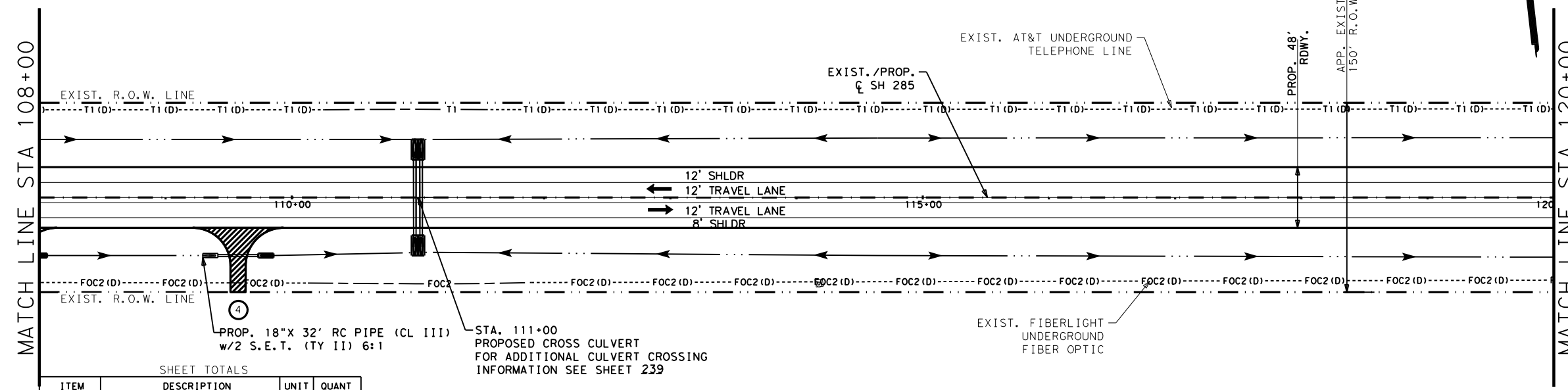
- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

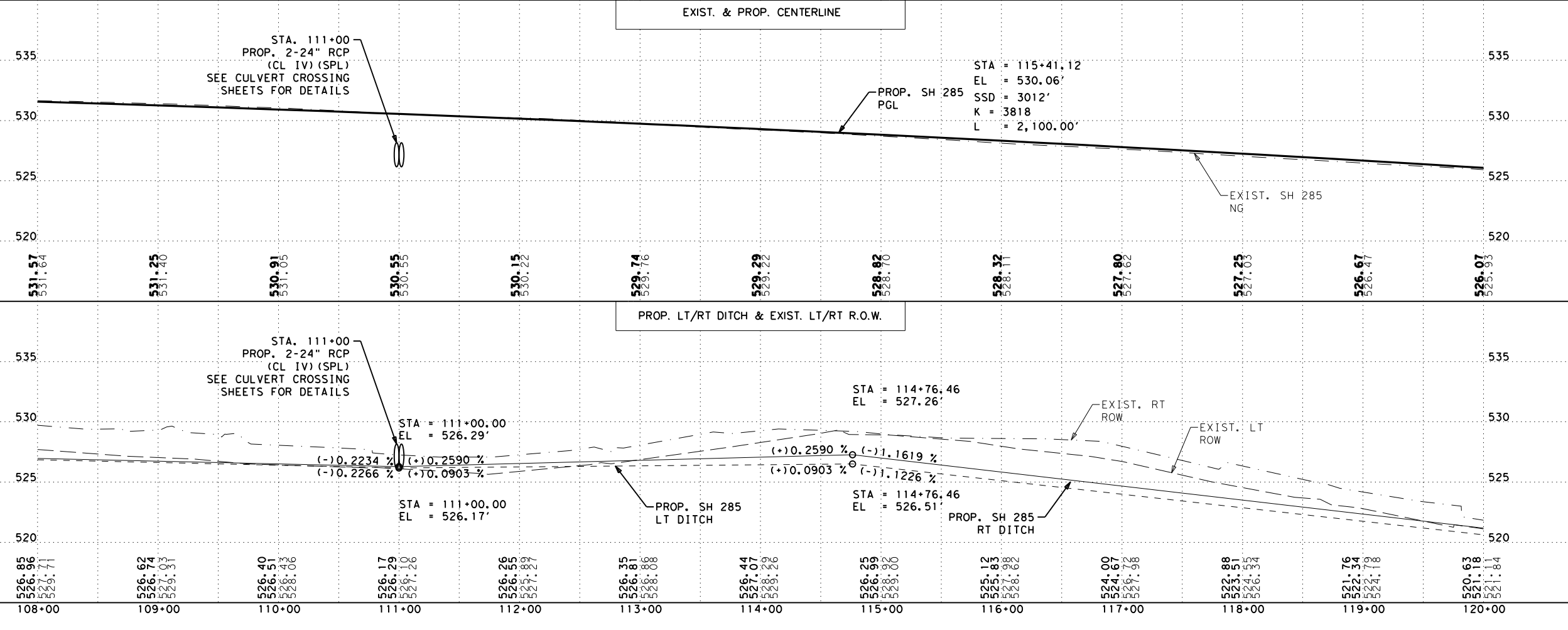
NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
464 6003	RC PIPE (CL III) (18IN)	LF	32
467 6363	SET (TYII) (18IN) (RCP) 6:1 (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	111



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

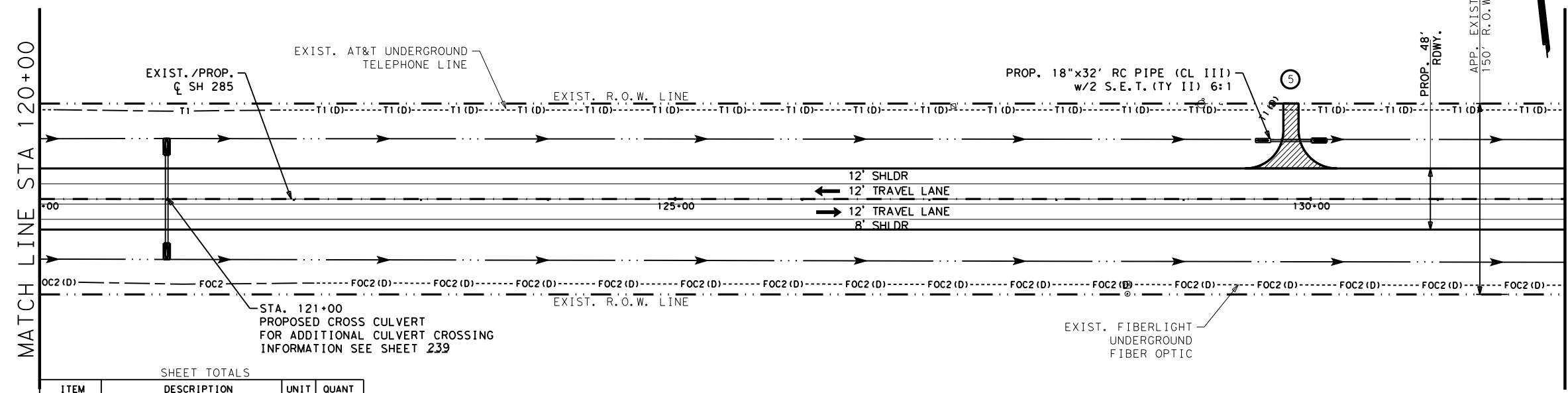


SH 285
PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 2 OF 44

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	183	

DATE: 2/7/2024 10:53:29 AM
 FILE: D:\work\pharr\proj\sh285\sh285_p&p_sheets_1.dwg

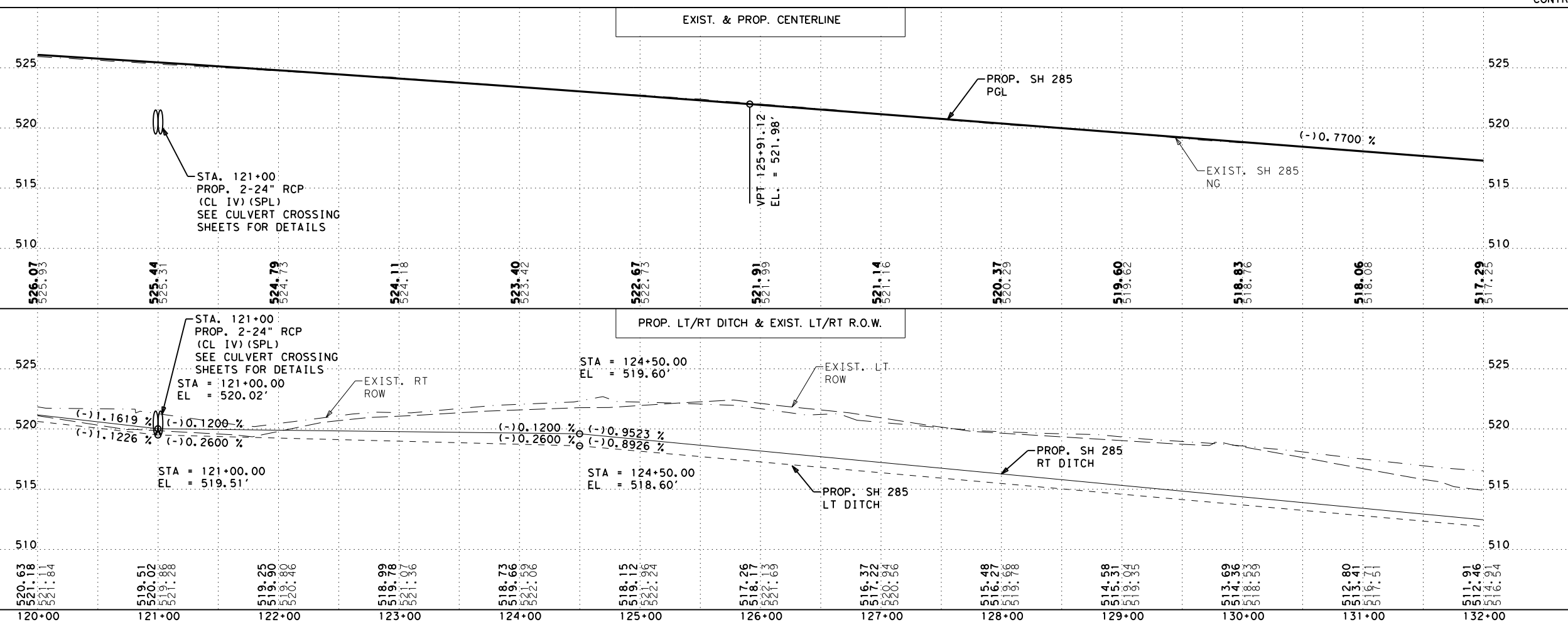


- ROADWAY LEGEND:**
- DRIVWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
464 6003	RC PIPE (CL III) (18IN)	LF	32
467 6363	SET (TY II) (18IN) (RCP) 6:1 (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	111

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

ARLENE CRYSTAL GARCIA
 147900
 PROFESSIONAL ENGINEER

Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

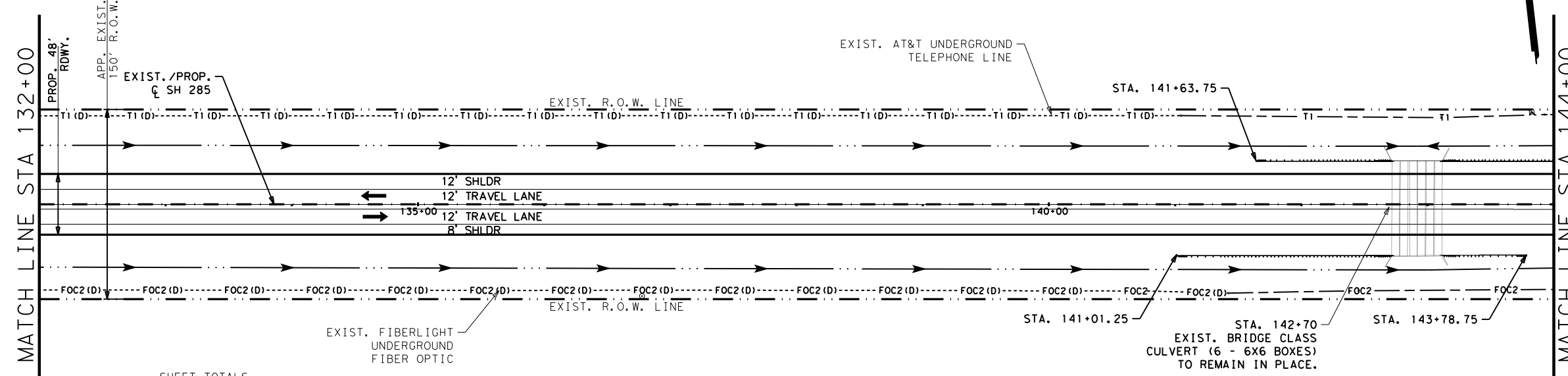
Texas Department of Transportation

SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 3 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		184

DATE: 2/7/2024 10:53:31 AM
 FILE: D:\xtdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 1



ROADWAY LEGEND:

- (*) DRIVEWAY NUMBER
- [Hatched Box] PROP. ASPHALT DRIVEWAY
- [Cross-hatched Box] PROP. MILL & OVERLAY (0"-2")
- [Arrow] DIRECTION OF TRAFFIC FLOW
- [Circle with X] PROP. SAFETY END TREATMENT
- [Dashed Line] R.O.W. LINE
- [Arrow with T] PROP. DITCH FLOW LINE
- [Circle with T] TEST HOLE

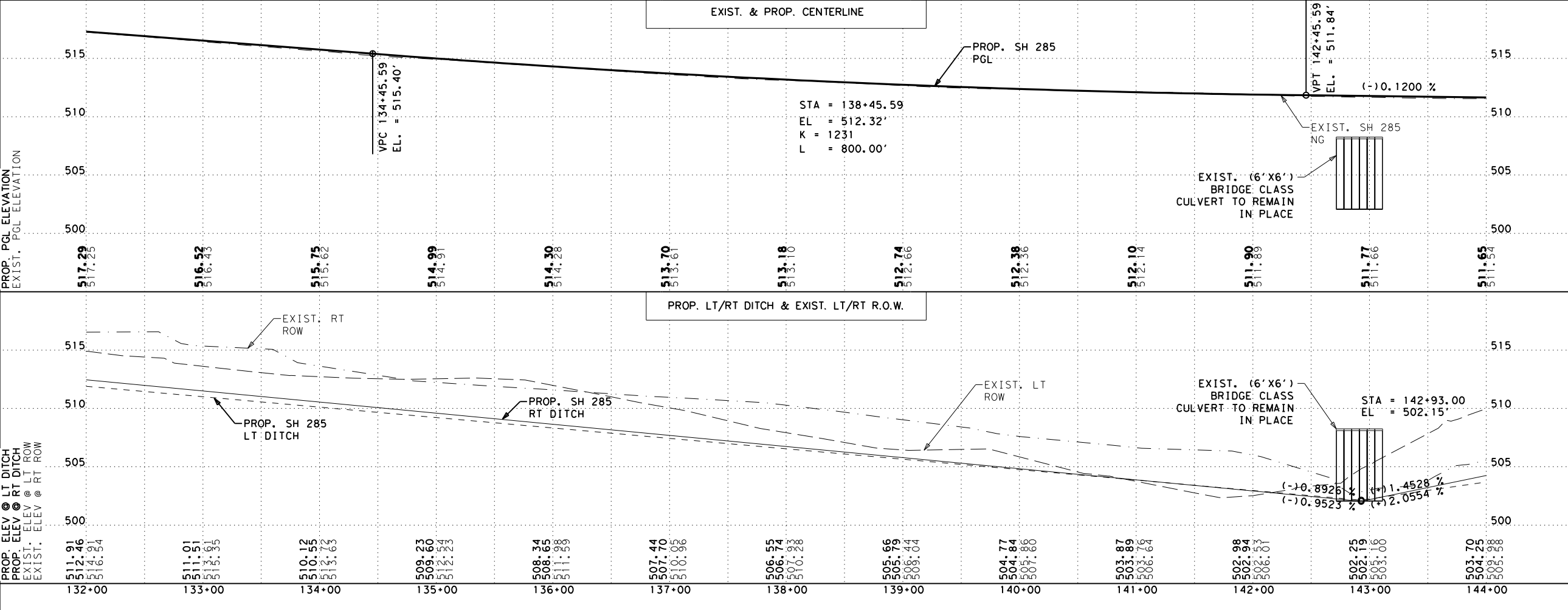
UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

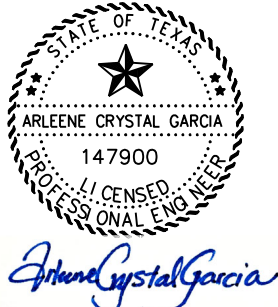
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



LEGEND:

- PROP. PGL ELEVATION ---
- EXIST. PGL ELEVATION ---
- PROP. LT DITCH ---
- PROP. RT DITCH ---
- EXIST. ELEV @ LT ROW ---
- EXIST. ELEV @ RT ROW ---



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

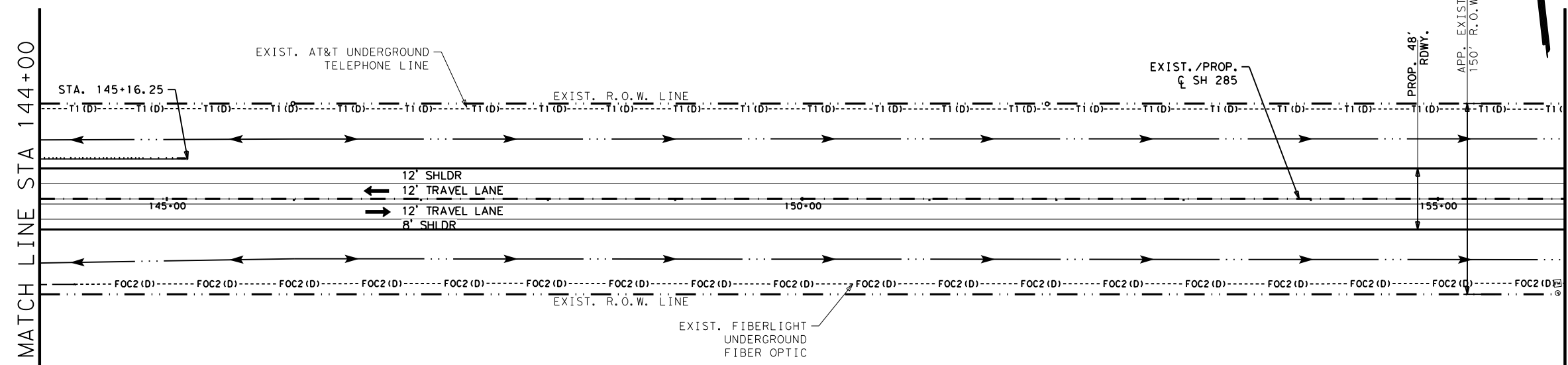


SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 4 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		185

DATE: 2/7/2024 10:53:34 AM
 FILE: D:\work\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 1

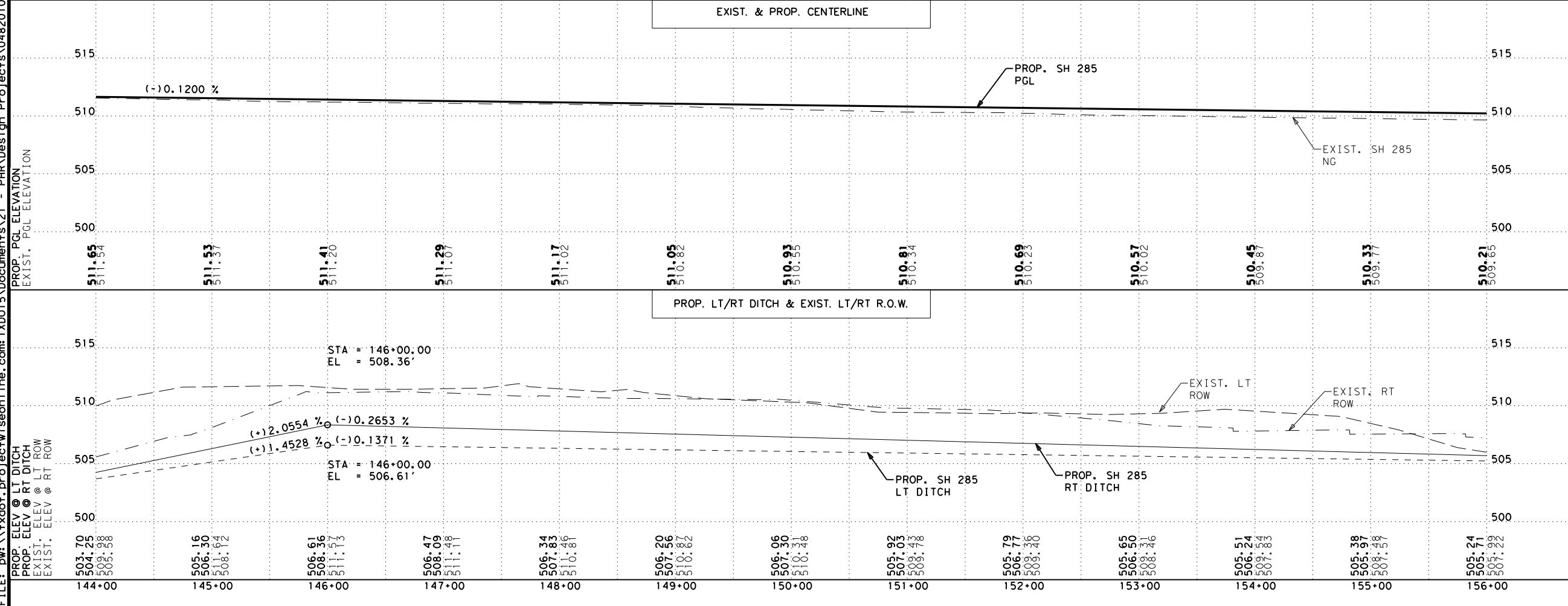


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

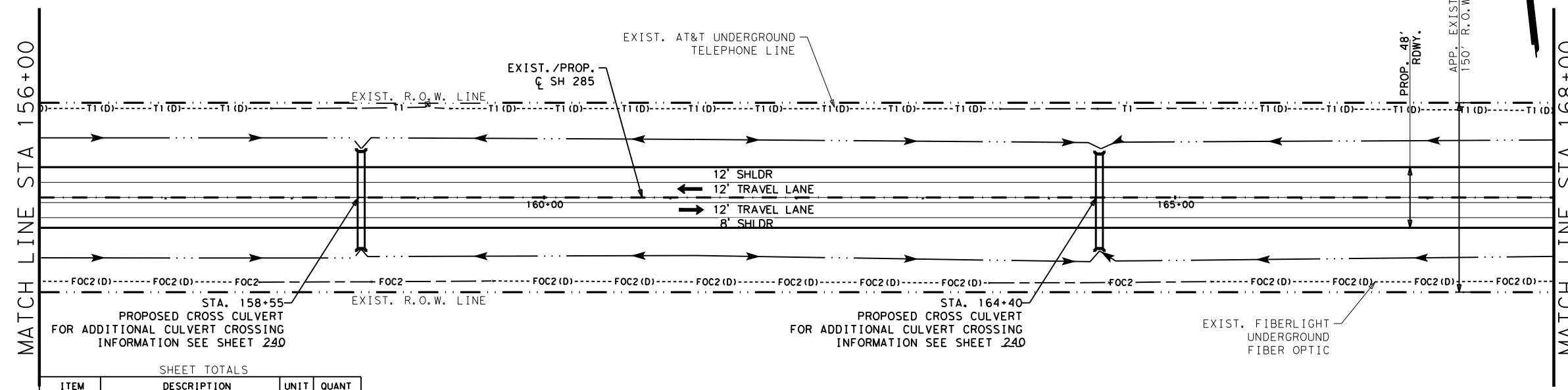
Texas Department of Transportation

**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 5 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	186

DATE: 2/7/2024 10:53:36 AM
 FILE: D:\work\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 1



- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

PROPOSED CROSS CULVERT FOR ADDITIONAL CULVERT CROSSING INFORMATION SEE SHEET 240

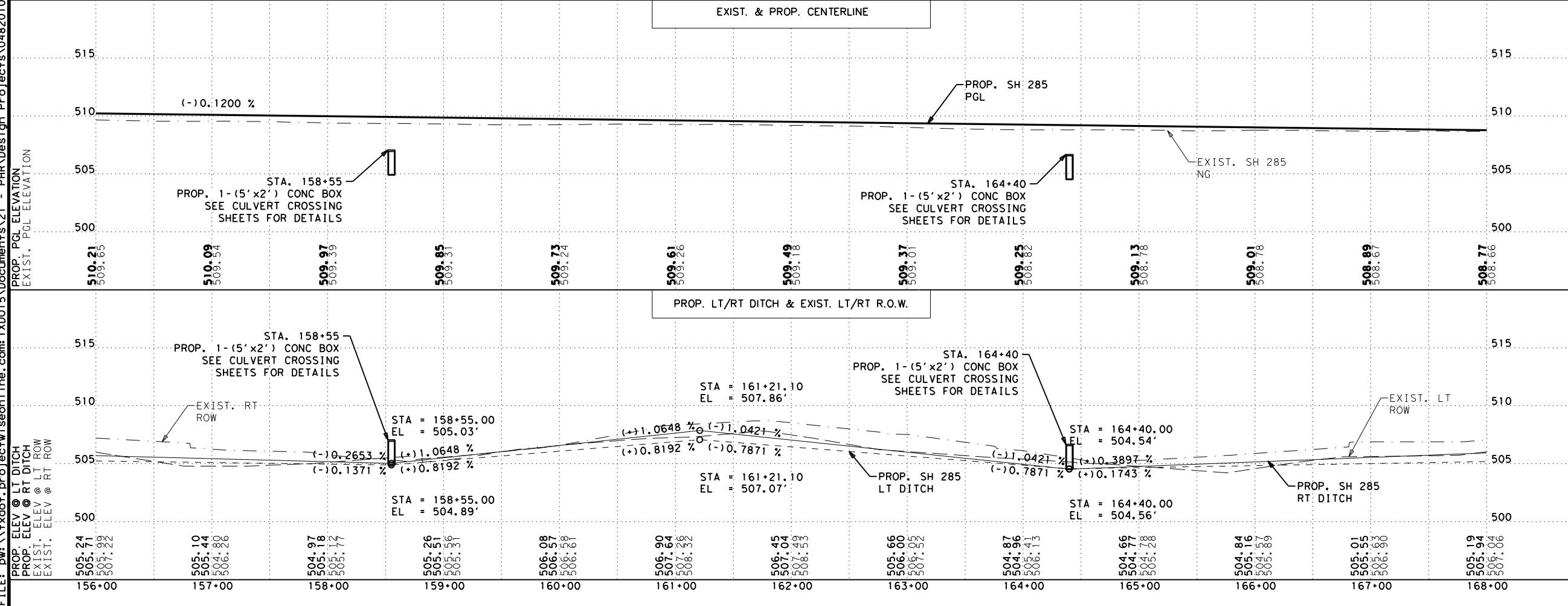
PROPOSED CROSS CULVERT FOR ADDITIONAL CULVERT CROSSING INFORMATION SEE SHEET 240

EXIST. FIBERLIGHT UNDERGROUND FIBER OPTIC

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

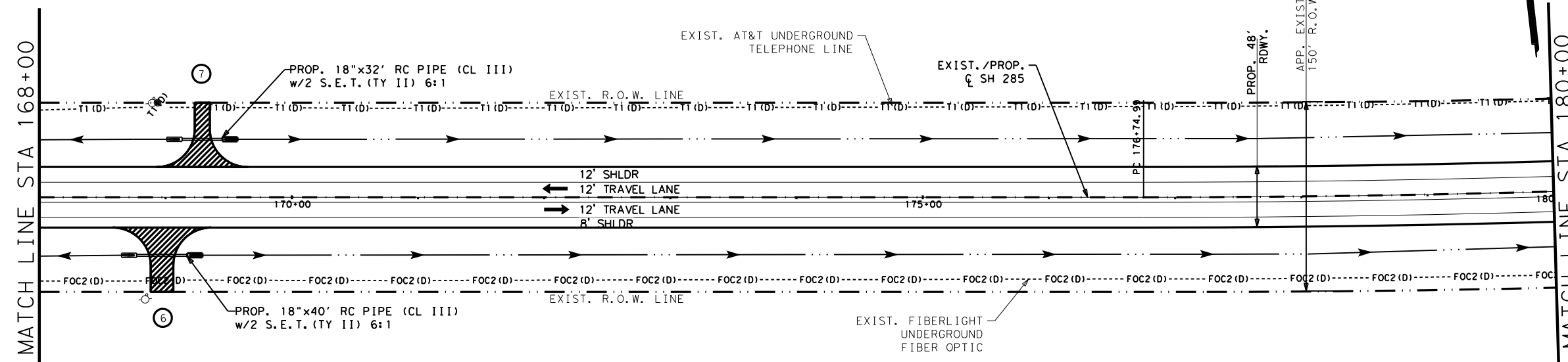
Texas Department of Transportation

SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 6 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG		187	

DATE: 2/7/2024 10:53:39 AM
 FILE: D:\projects\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 1

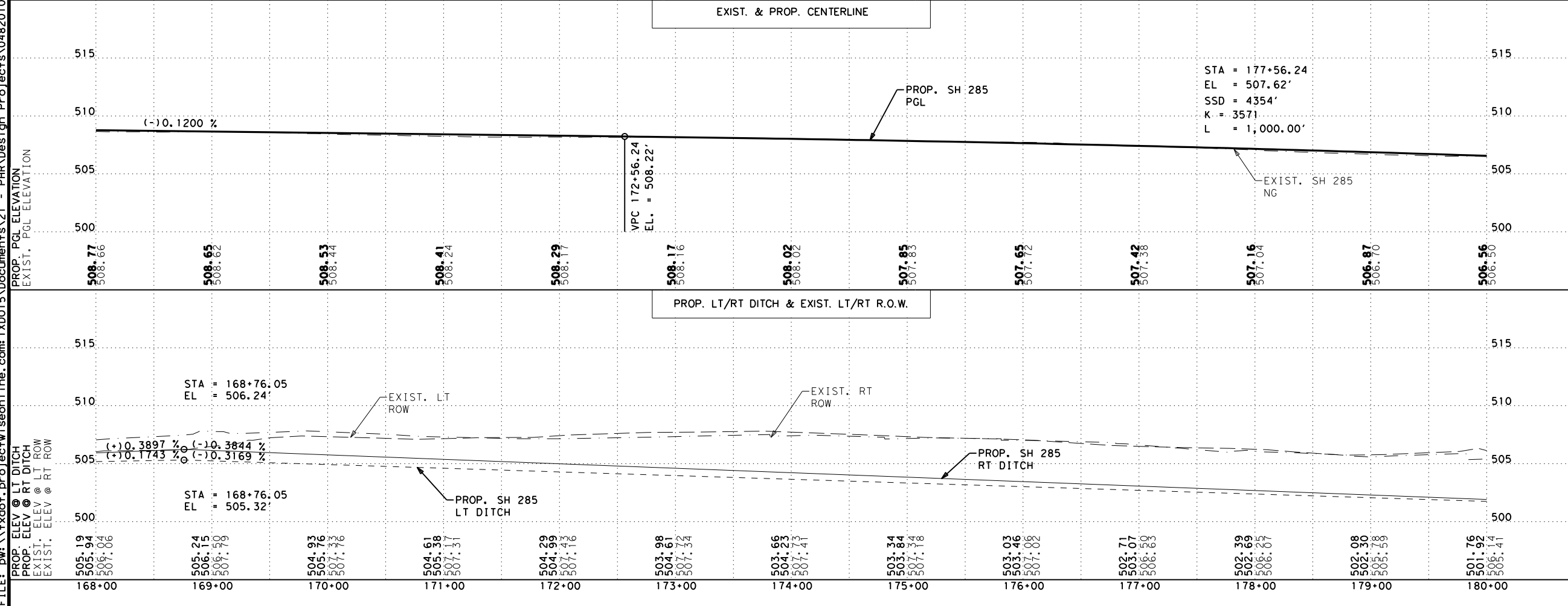


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

- NOTES:**
1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
464 6003	RC PIPE (CL III) (18IN)	LF	72
467 6363	SET (TY II) (18IN) (RCP) 6:1 (P)	EA	4
530 6005	DRIVEWAYS (ACP)	SY	256



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024
Pharr District Central Design



**SH 285
 PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 7 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	188	

DATE: 2/7/2024 10:53:41 AM
 FILE: D:\w:\fxdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 1

ROADWAY LEGEND:

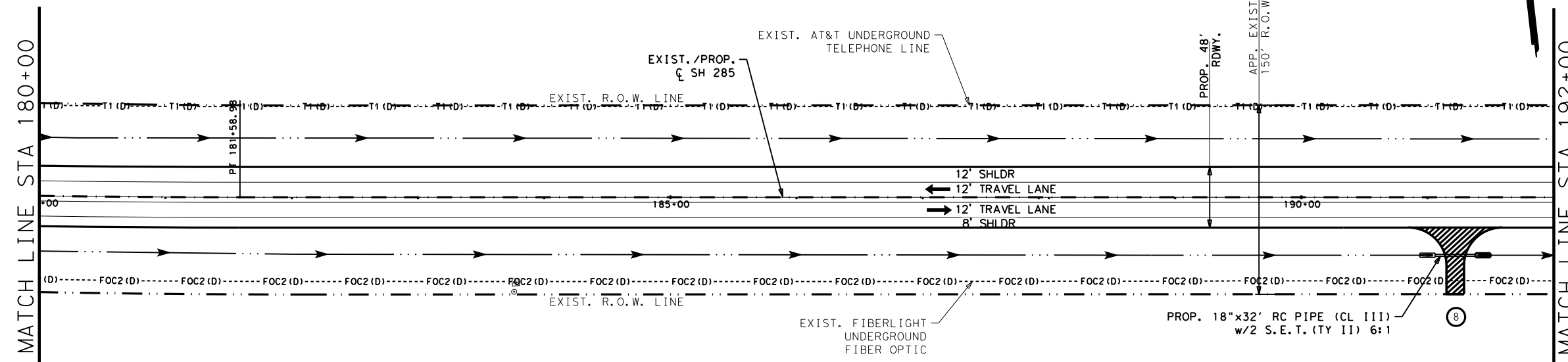
- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

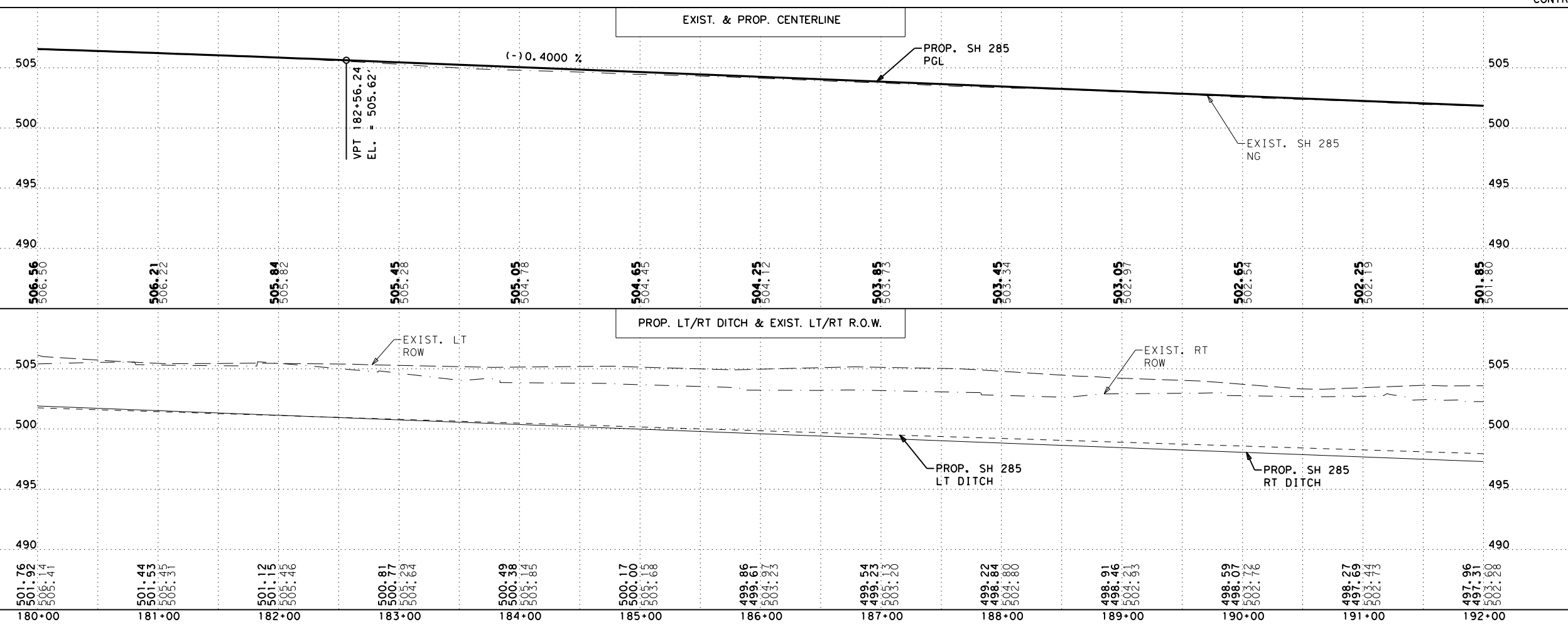
NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
464 6003	RC PIPE (CL III) (18IN)	LF	32
467 6363	SET (TY II) (18IN) (RCP) 6:1 (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	126



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285
PLAN AND PROFILE

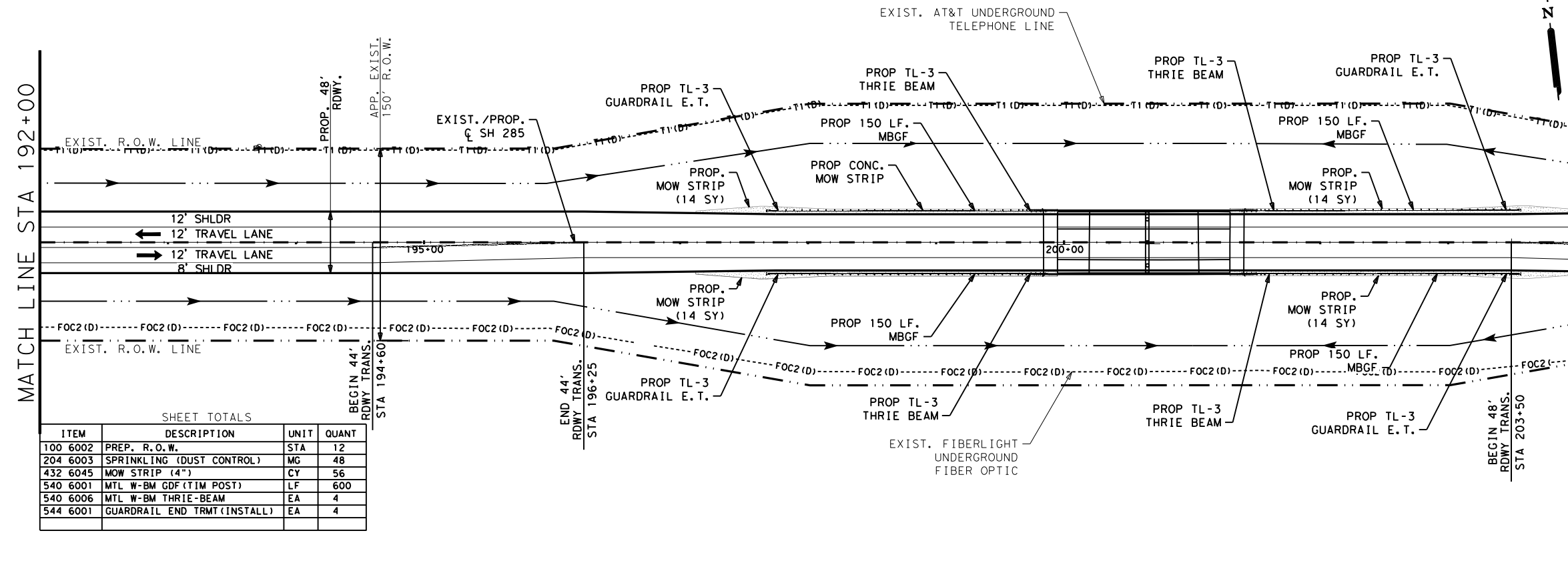
HORIZ: 1"=100'
 VERT: 1"=50' SHEET 8 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	189	

DATE: 2/7/2024 10:53:44 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 1

MATCH LINE STA 192+00

MATCH LINE STA 204+00

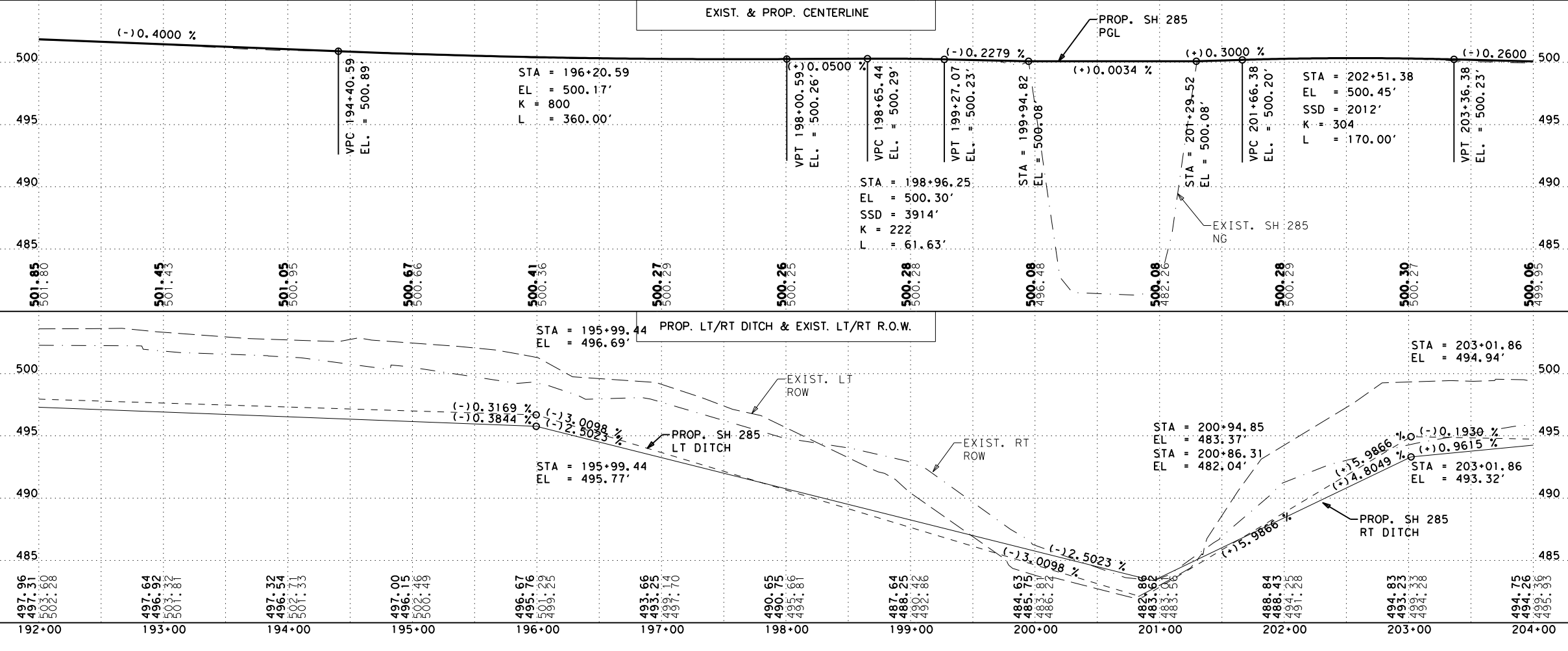


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

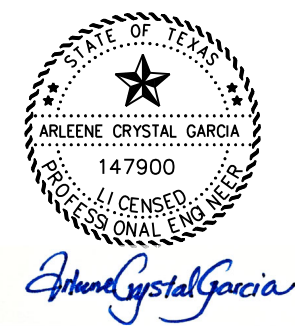
- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
432 6045	MOW STRIP (4")	CY	56
540 6001	MTL W-BM GDF (TIM POST)	LF	600
540 6006	MTL W-BM THRIE-BEAM	EA	4
544 6001	GUARDRAIL END TRMT (INSTALL)	EA	4



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

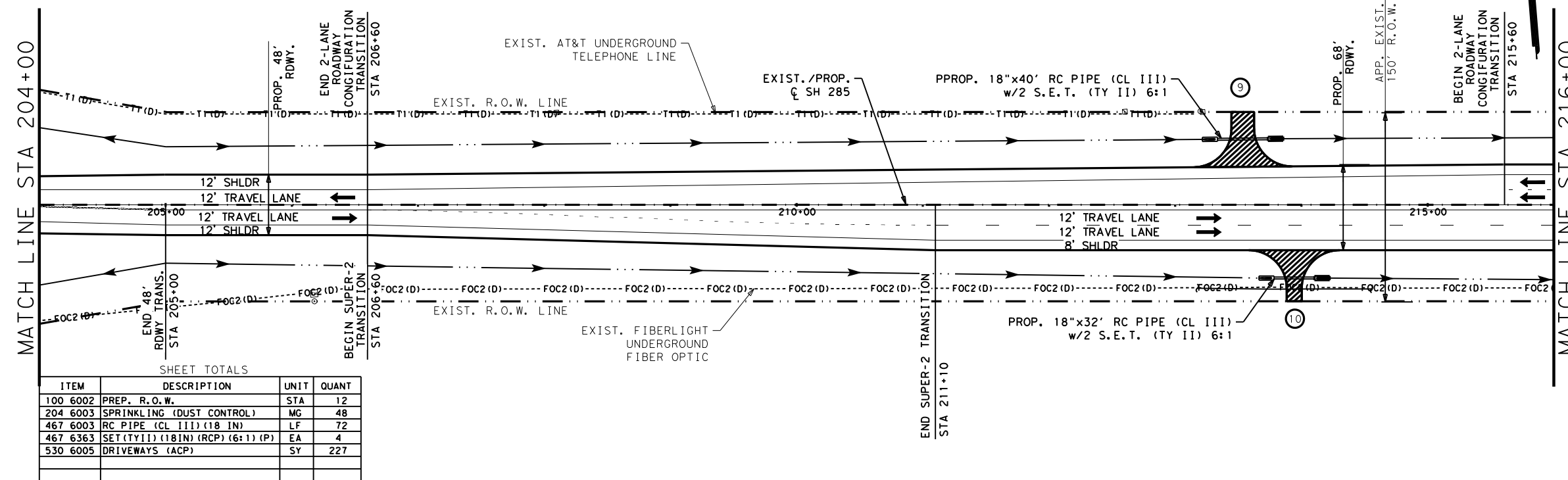


**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=50' SHEET 9 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	190

DATE: 2/7/2024 10:53:52 AM
 FILE: D:\p\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 2

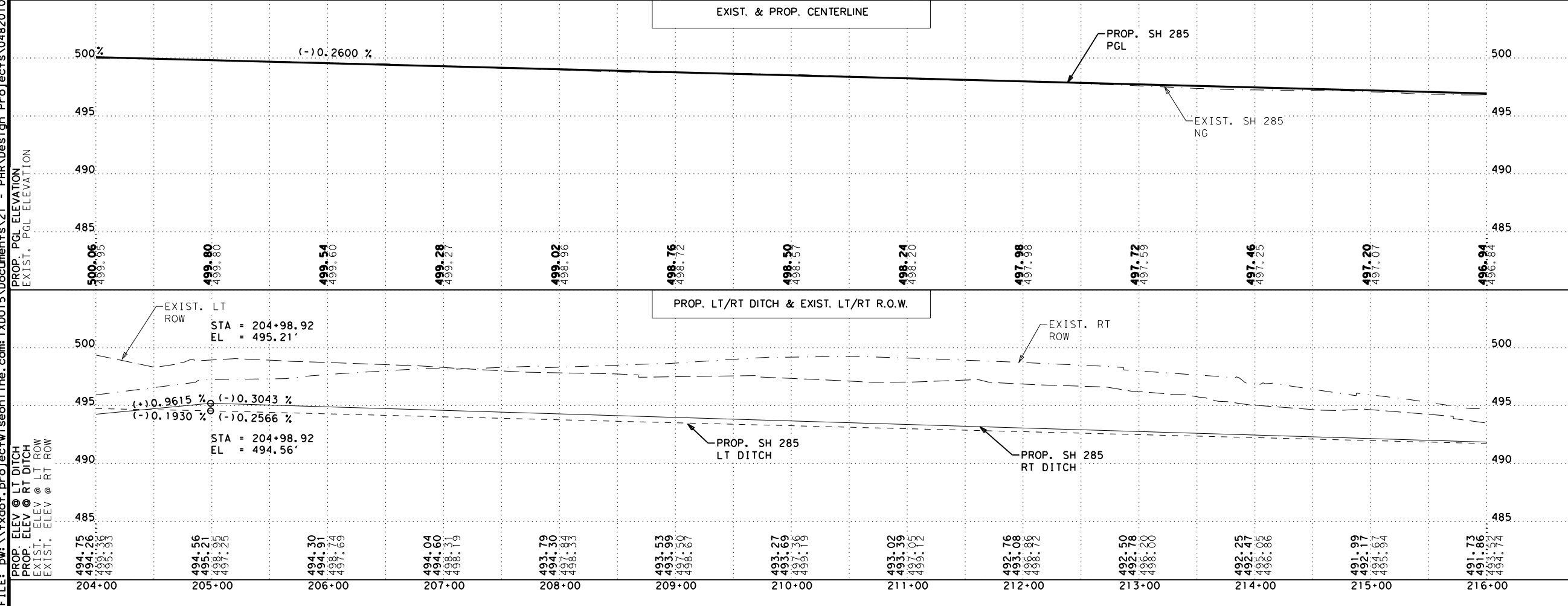


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	72
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	4
530 6005	DRIVEWAYS (ACP)	SY	227

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

Texas Department of Transportation

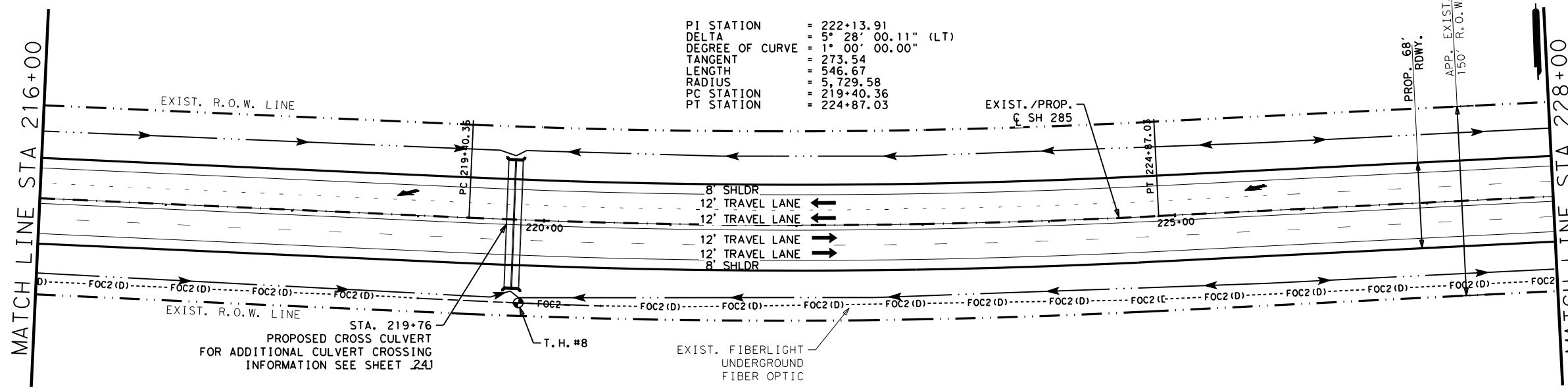
**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 10 OF 44

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	191	

DATE: 2/7/2024 10:53:56 AM
 FILE: D:\work\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 2

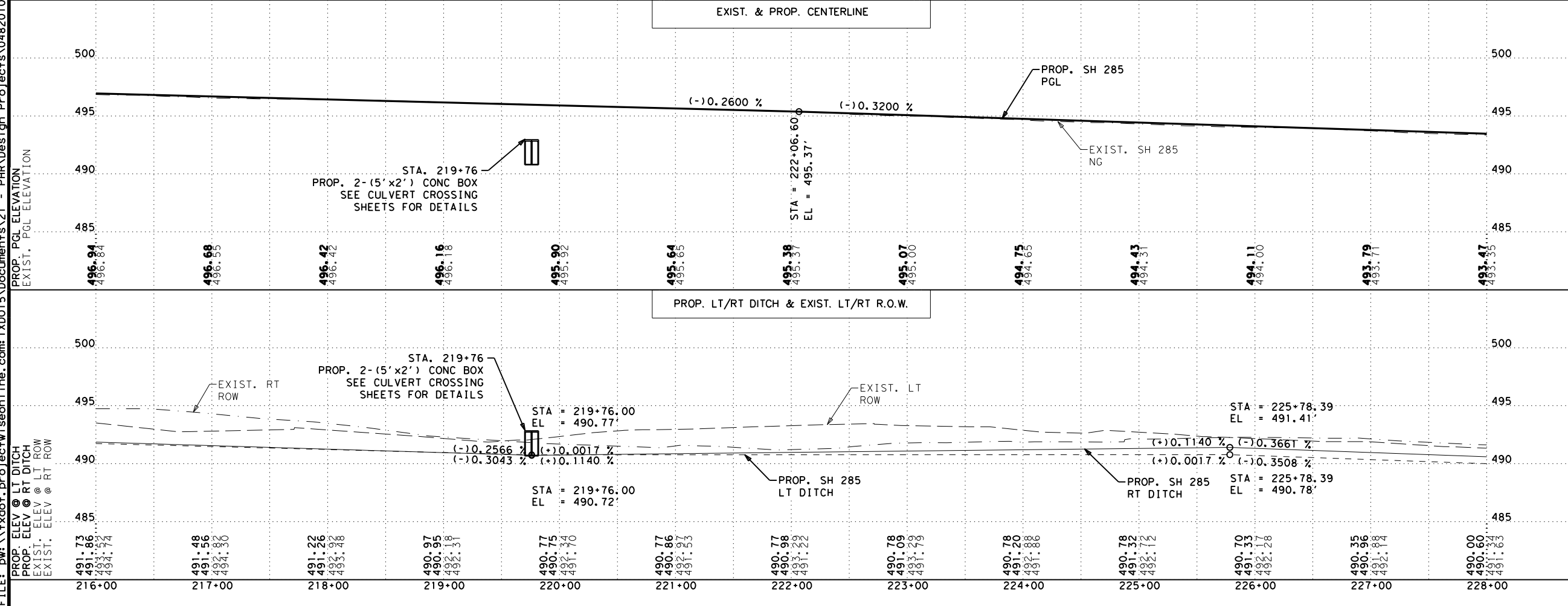


- ROADWAY LEGEND:
- (*) DRIVEWAY NUMBER
 - [Hatched Box] PROP. ASPHALT DRIVEWAY
 - [Cross-hatched Box] PROP. MILL & OVERLAY (0"-2")
 - [Arrow] DIRECTION OF TRAFFIC FLOW
 - [Circle with X] PROP. SAFETY END TREATMENT
 - [Dashed Line] R.O.W. LINE
 - [Line with Arrow] PROP. DITCH FLOW LINE
 - [Circle with Center] TEST HOLE
- UTILITY LEGEND:
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

- NOTES:
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

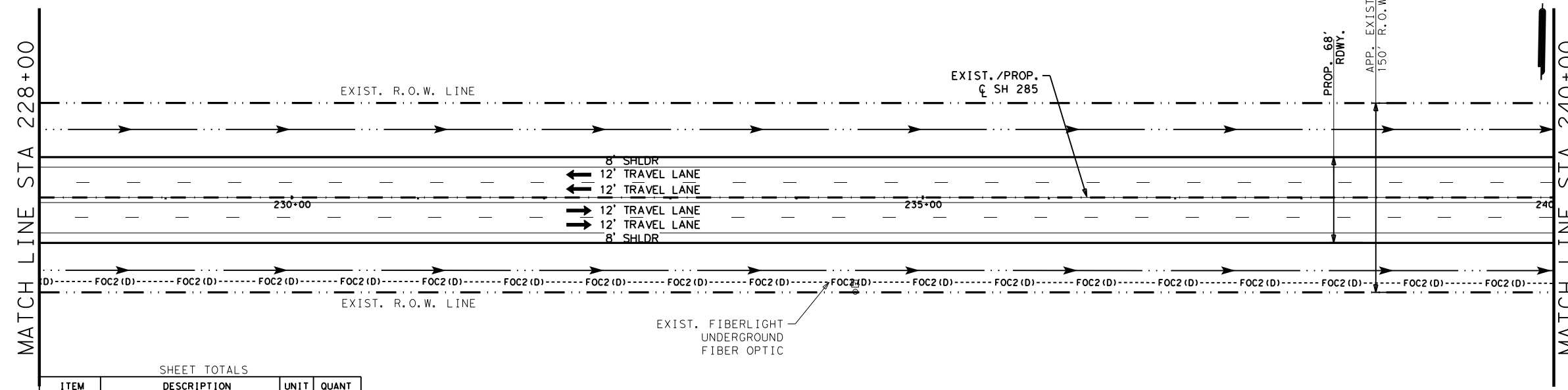


LEGEND:

STATE OF TEXAS
 ARLEENE CRYSTAL GARCIA
 147900
 LICENSED PROFESSIONAL ENGINEER
 Arlene Crystal Garcia
 02.13.2024
 Pharr District Central Design
 Texas Department of Transportation
 SH 285
 PLAN AND PROFILE
 HORIZ: 1"=100'
 VERT: 1"=10'
 SHEET 11 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG		192	

DATE: 2/7/2024 10:53:59 AM
 FILE: D:\w\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 2

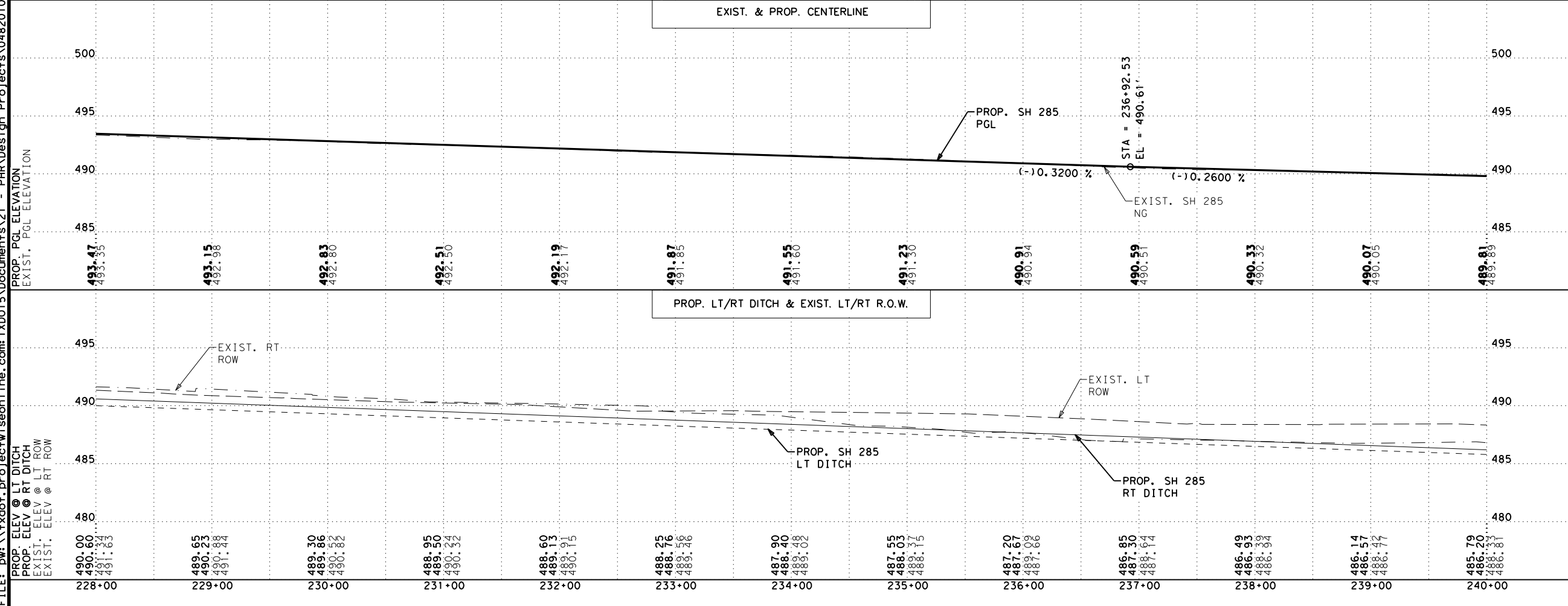


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

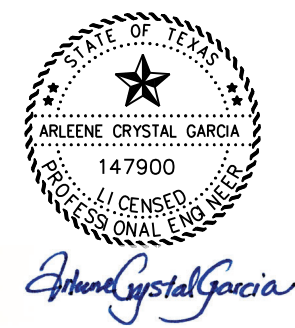
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	193	

DATE: 2/7/2024 10:54:03 AM
 FILE: D:\work\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 2

ROADWAY LEGEND:

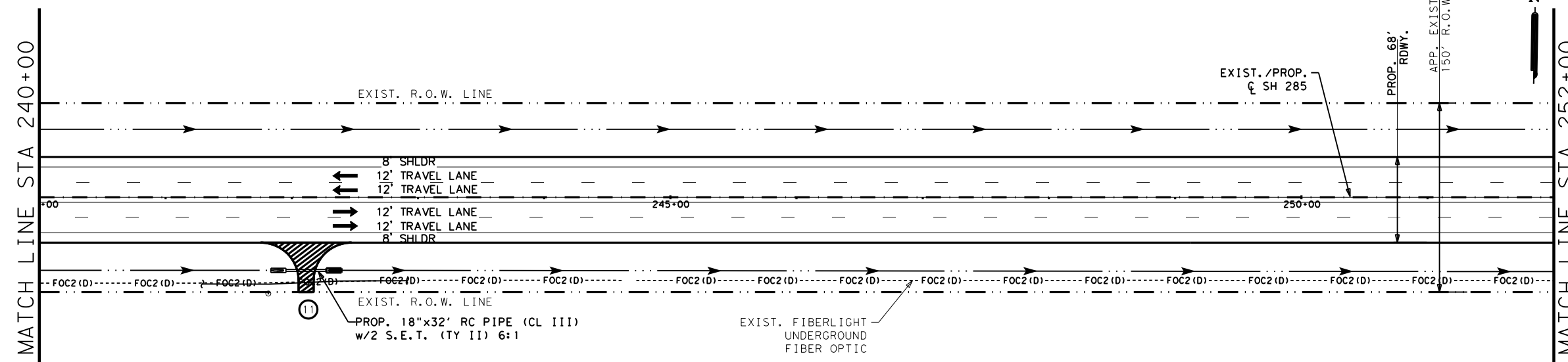
- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

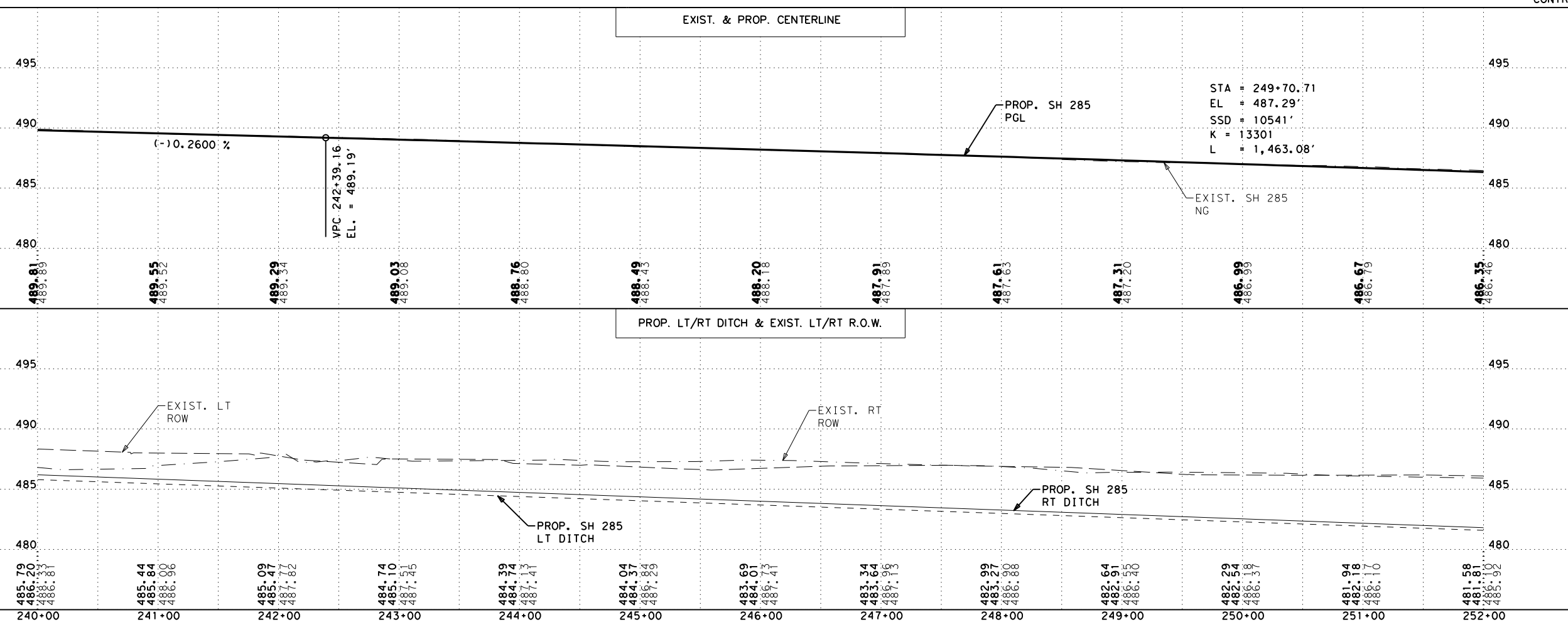
NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	32
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	96



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

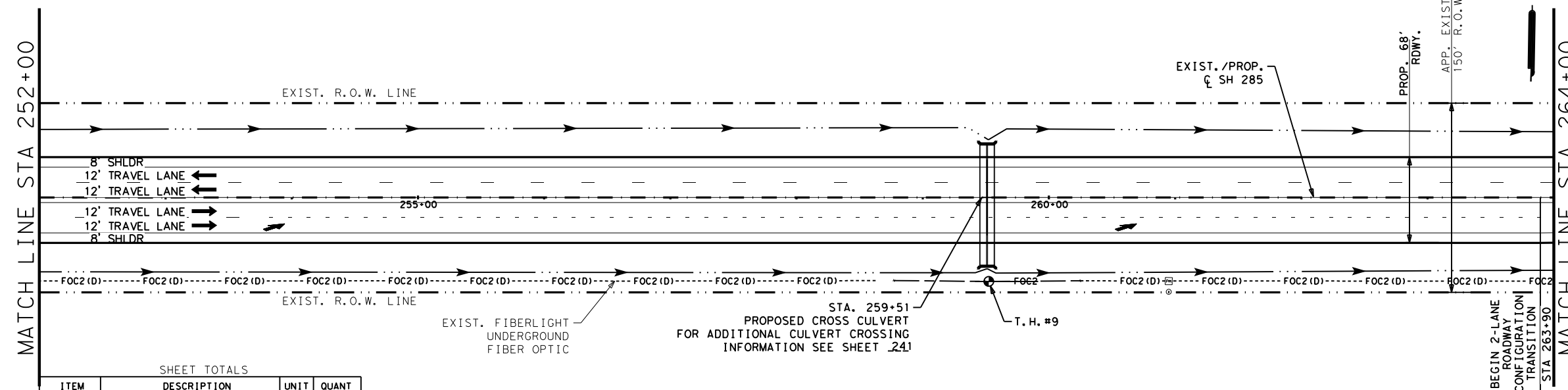


SH 285
PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		194

DATE: 2/7/2024 10:54:06 AM
 FILE: D:\p\dot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 2

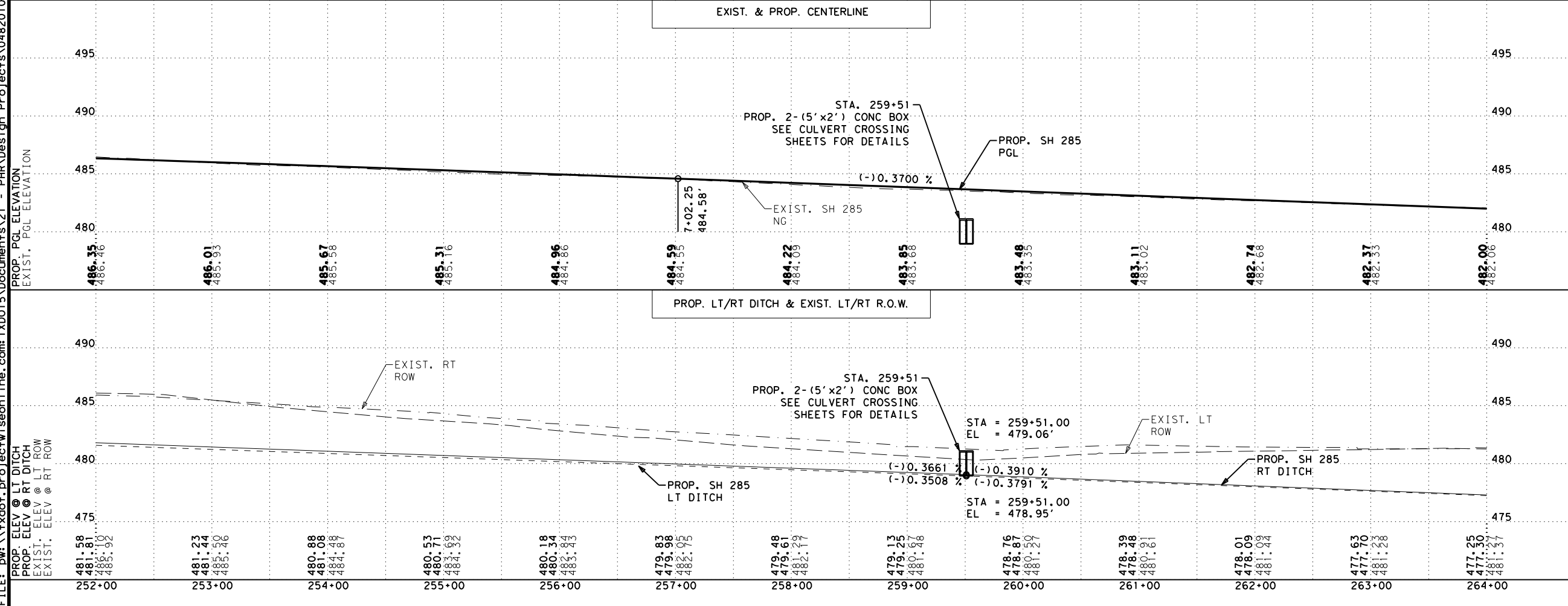


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

ARLENE CRYSTAL GARCIA
 147900
 PROFESSIONAL ENGINEER

Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

**SH 285
 PLAN AND PROFILE**

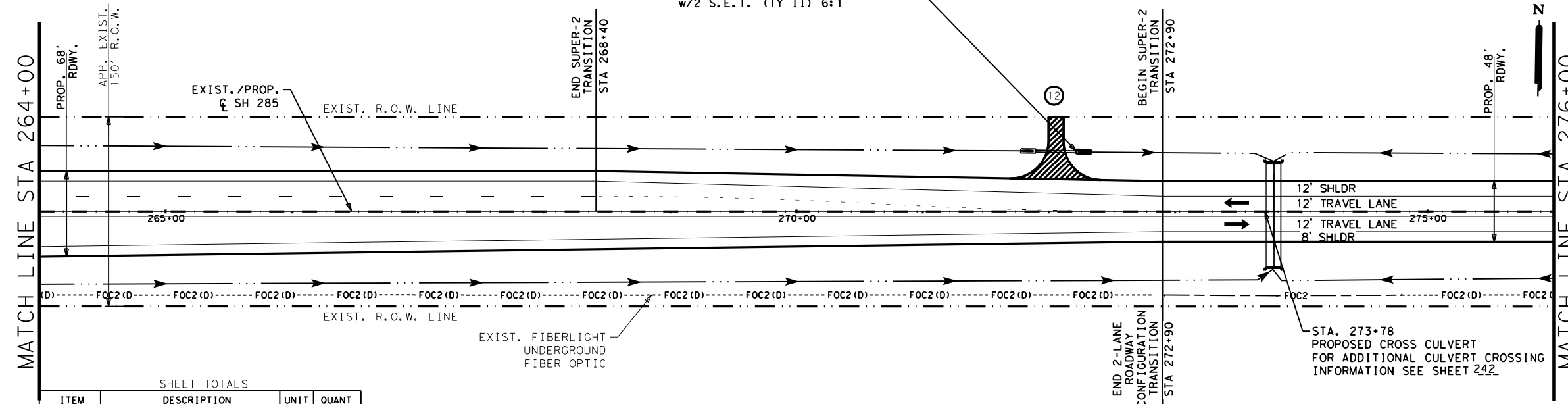
HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 14 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	195

DATE: 2/7/2024 10:54:10 AM
 FILE: D:\Projects\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 2

MATCH LINE STA 264+00

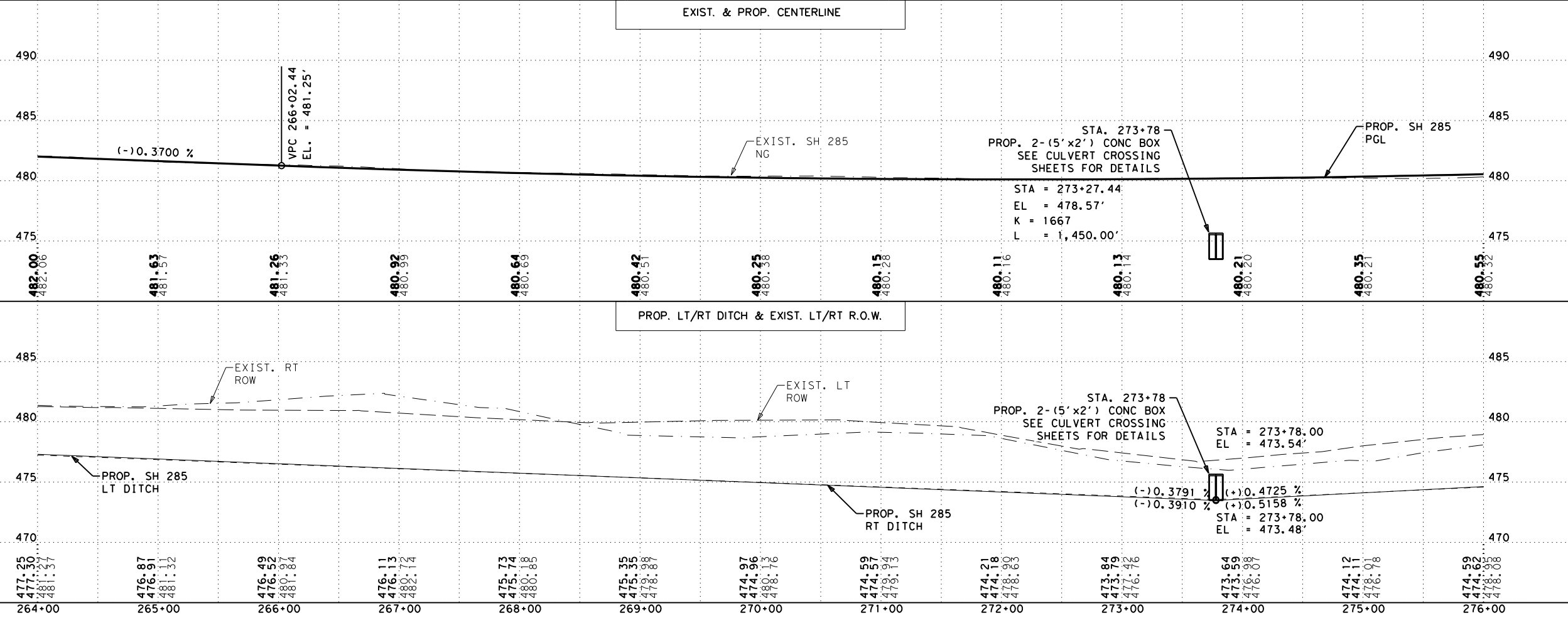


- ROADWAY LEGEND:**
- (*) DRIVEWAY NUMBER
 - [Hatched Box] PROP. ASPHALT DRIVEWAY
 - [Cross-hatched Box] PROP. MILL & OVERLAY (0"-2")
 - [Arrow] DIRECTION OF TRAFFIC FLOW
 - [Circle with X] PROP. SAFETY END TREATMENT
 - [Dashed Line] R.O.W. LINE
 - [Line with Arrow] PROP. DITCH FLOW LINE
 - [Circle with Center] TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

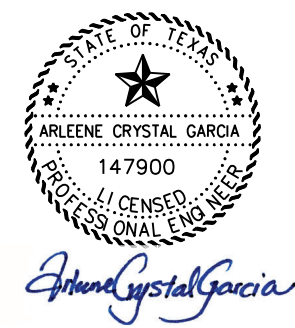
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	32
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	109

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

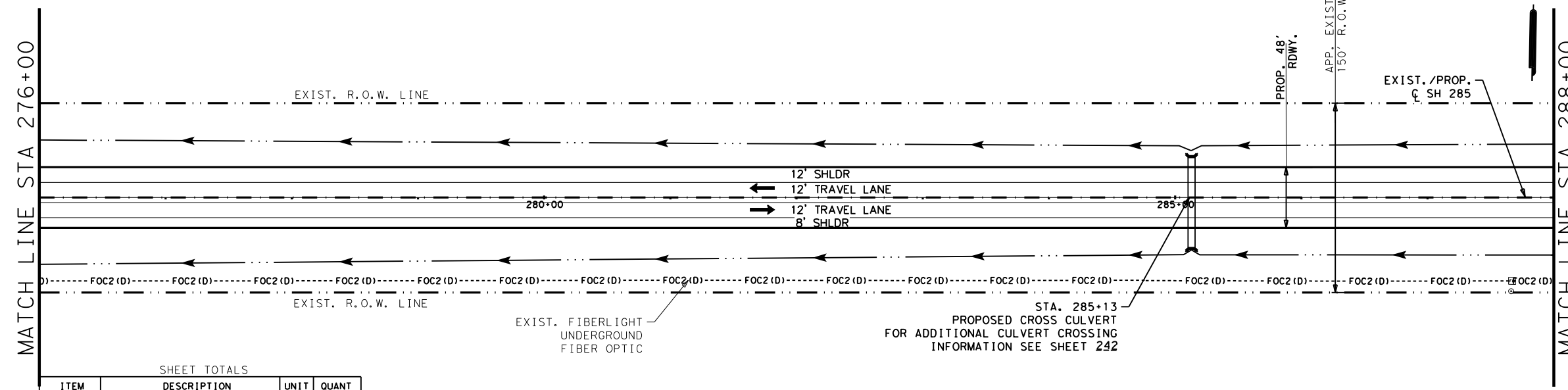


SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	196

DATE: 2/7/2024 10:54:13 AM
 FILE: D:\xtdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 2

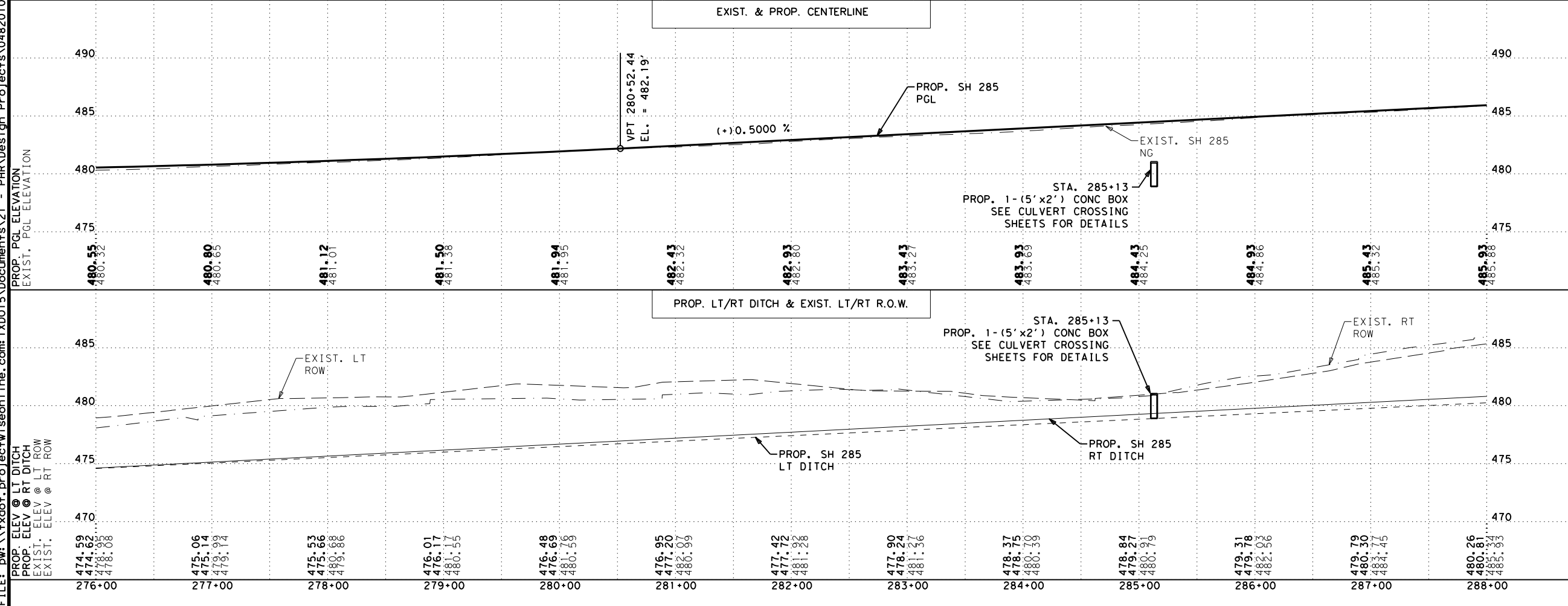


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 16 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	197	

DATE: 2/7/2024 10:54:17 AM
 FILE: D:\work\pdxdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 2

ROADWAY LEGEND:

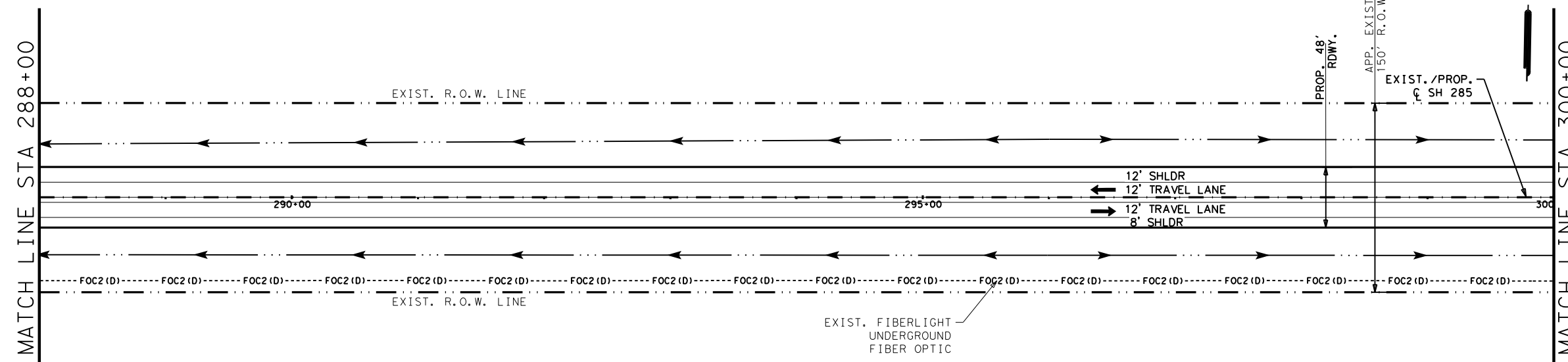
- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

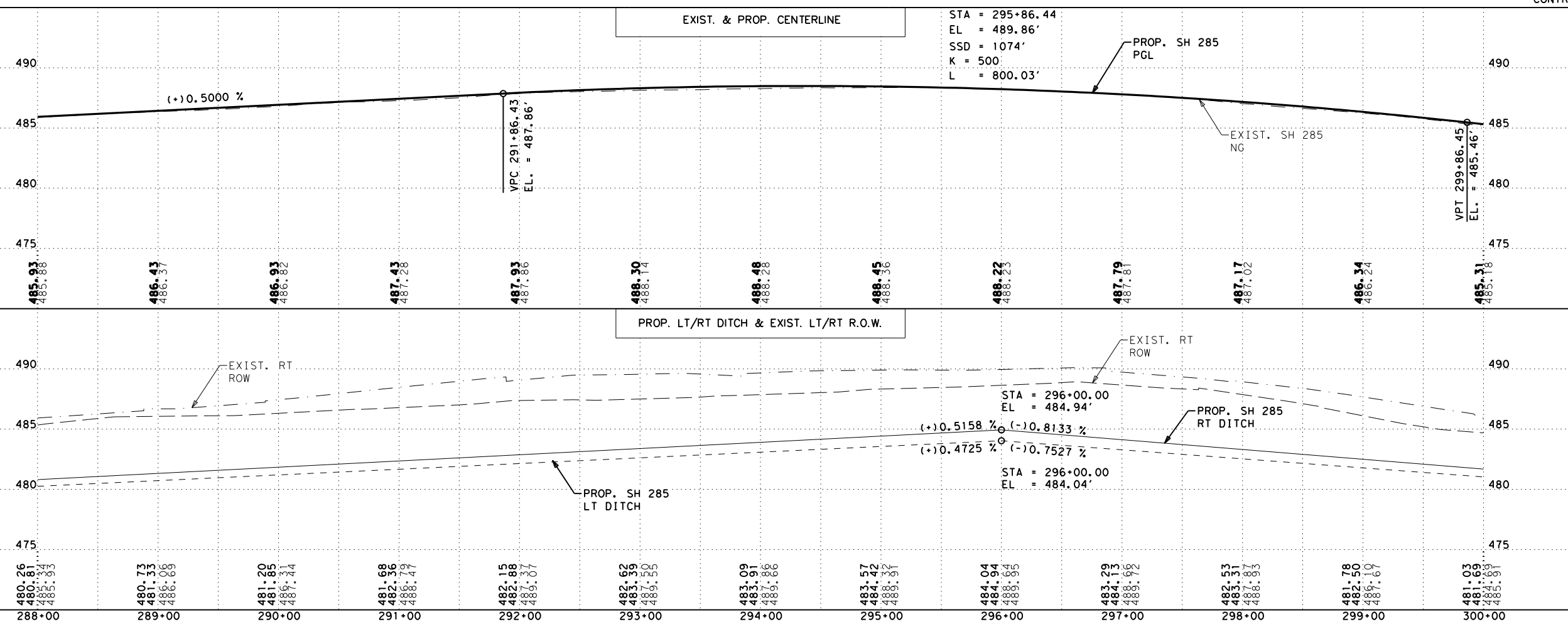
NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



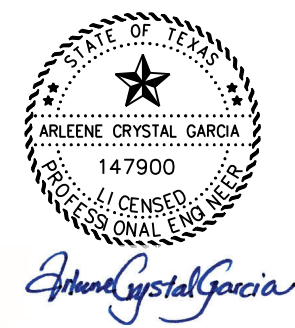
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



LEGEND:

PROP. PGL ELEVATION	---
EXIST. PGL ELEVATION	---
PROP. LT DITCH	---
PROP. RT DITCH	---
EXIST. ELEV @ LT ROW	---
EXIST. ELEV @ RT ROW	---



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285
PLAN AND PROFILE

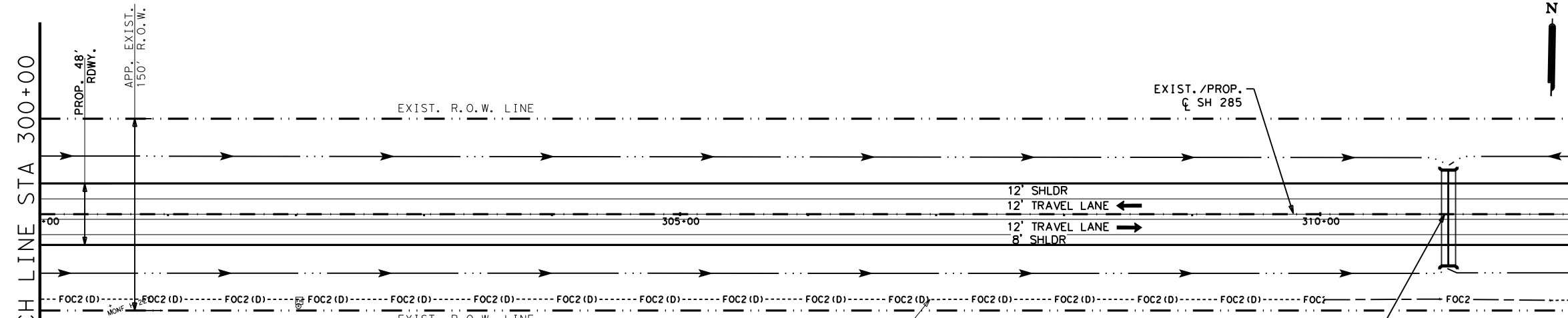
HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	198

DATE: 2/7/2024 10:54:21 AM
 FILE: D:\projects\phr\design\projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 2

MATCH LINE STA 300+00

MATCH LINE STA 312+00

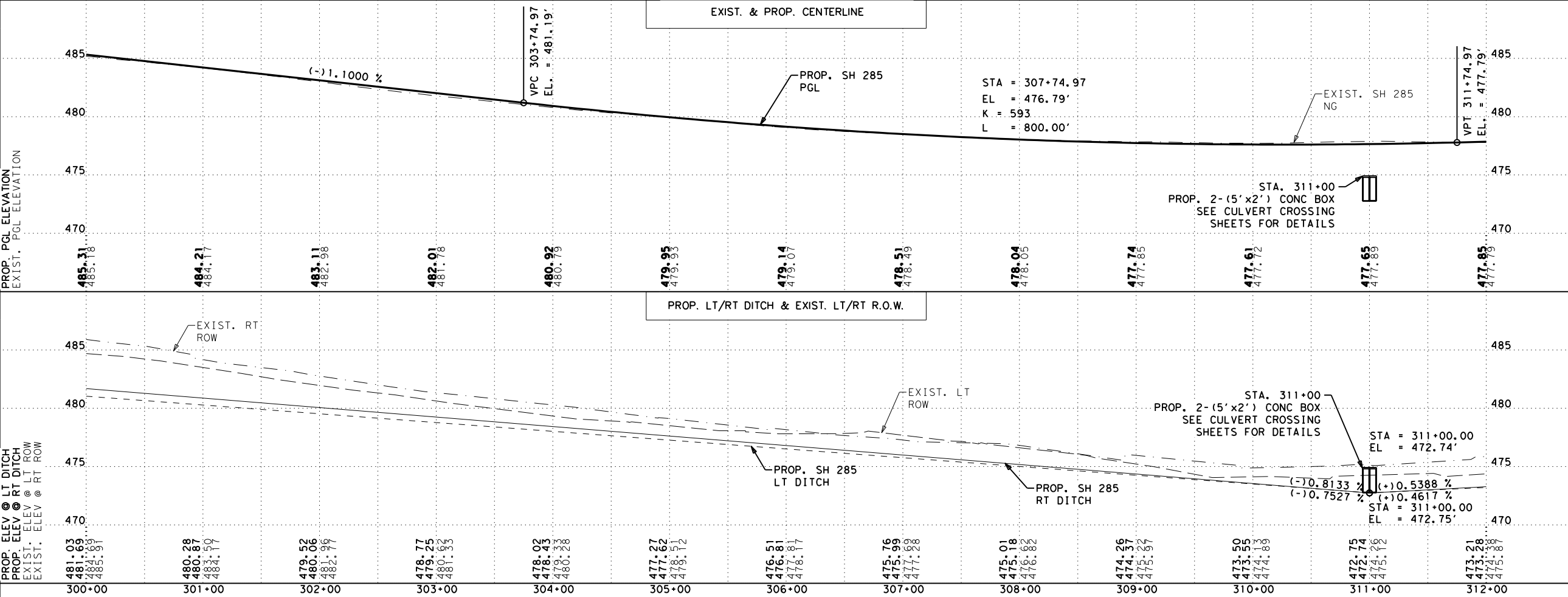


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

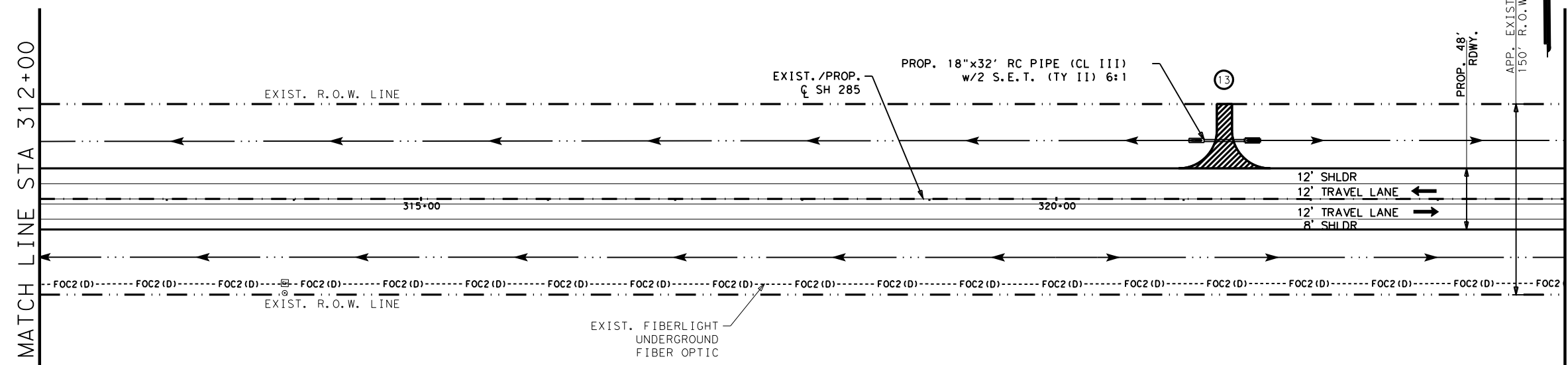


**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	DESIGNER	SHEET NO.
	PHR	JIM HOGG		199

DATE: 2/7/2024 10:54:30 AM
 FILE: D:\work\dot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets_3

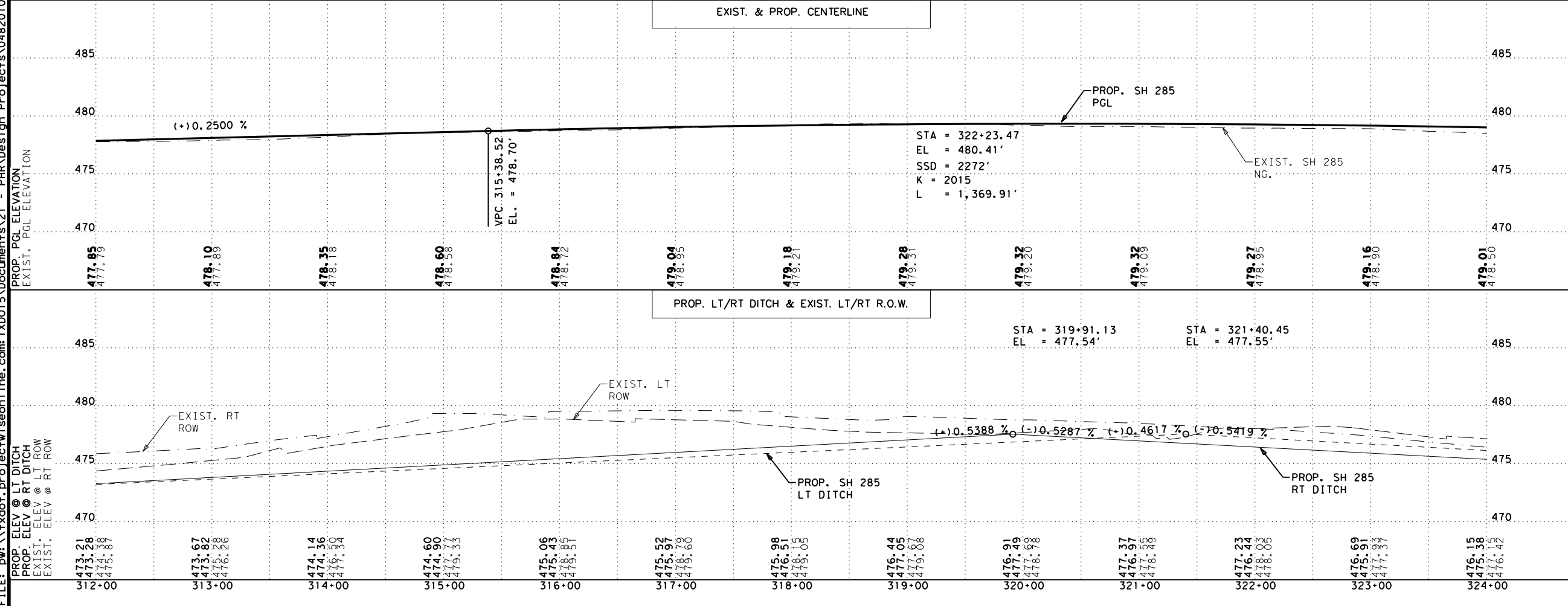


- ROADWAY LEGEND:**
- DRIVWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

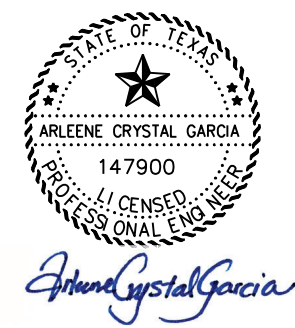
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	32
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	111

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

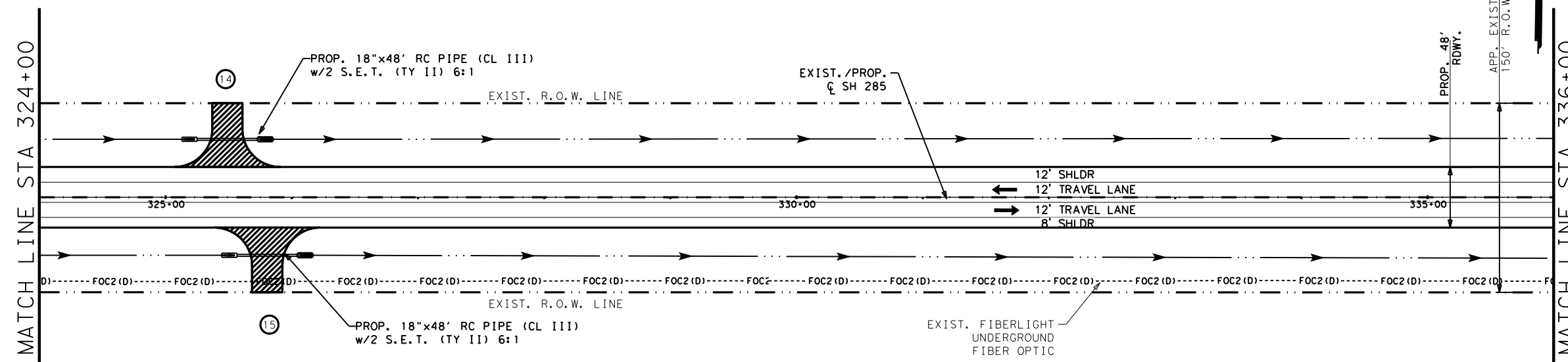


**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10' SHEET 19 OF 44

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST COUNTY			SHEET NO.
PHR JIM HOGG			200

DATE: 2/7/2024 10:54:34 AM
 FILE: D:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets_3

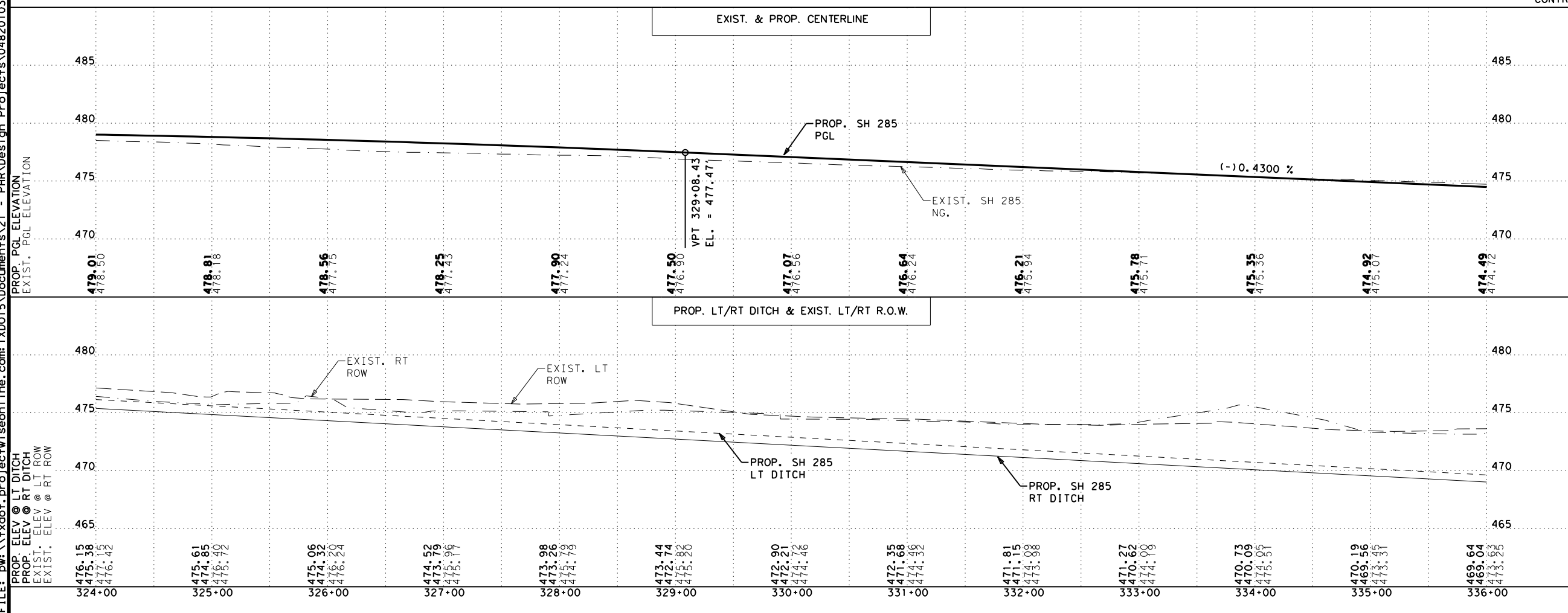


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

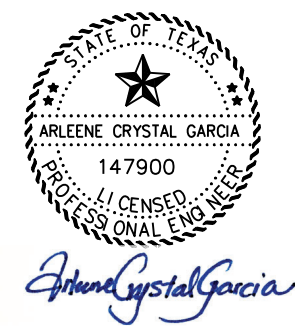
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	96
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	4
530 6005	DRIVEWAYS (ACP)	SY	358

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



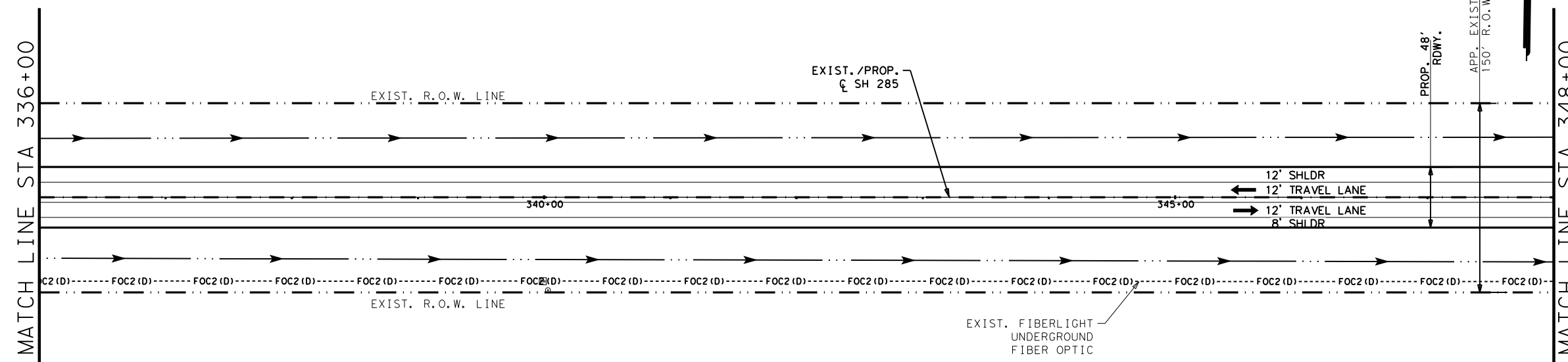
**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 20 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	201

DATE: 2/7/2024 10:54:37 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets_3

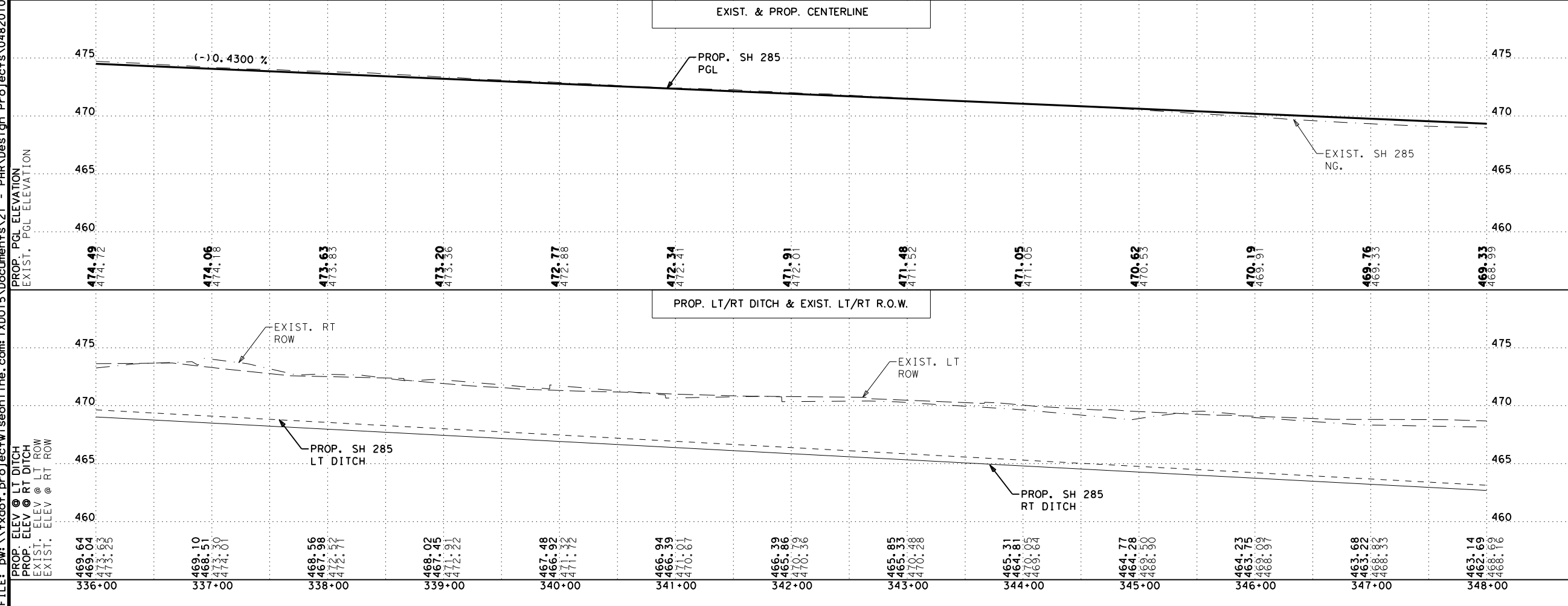


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

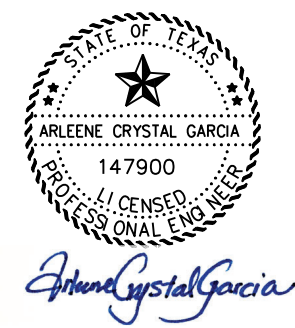
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



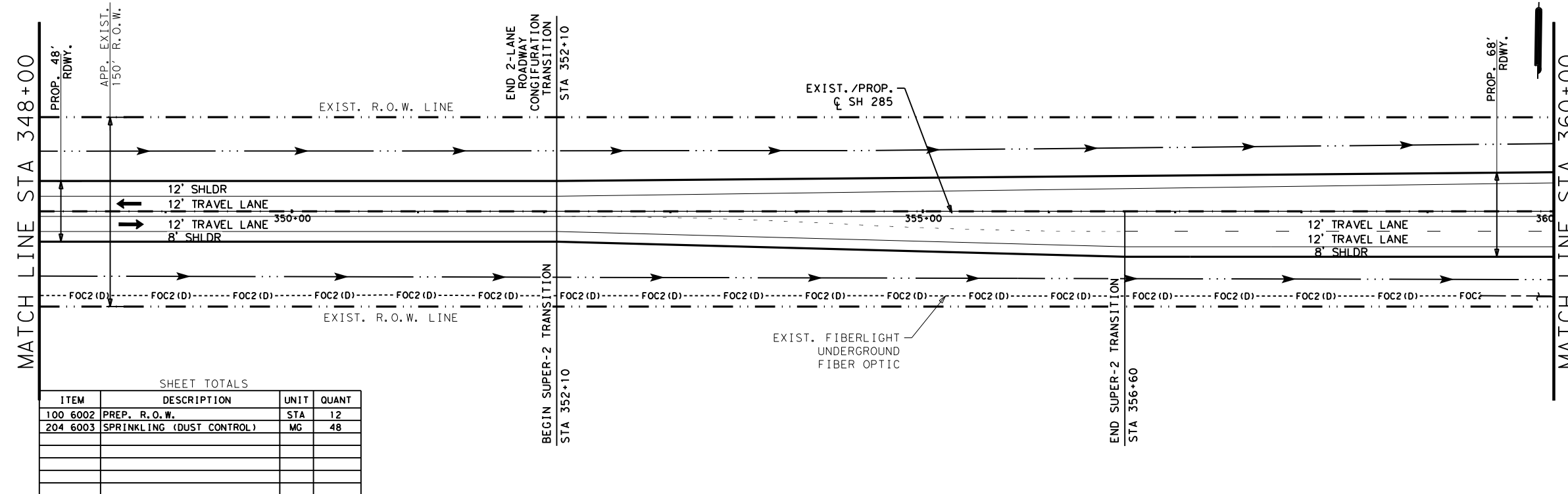
**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	202	

DATE: 2/7/2024 10:54:41 AM
 FILE: D:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets_3

MATCH LINE STA 348+00

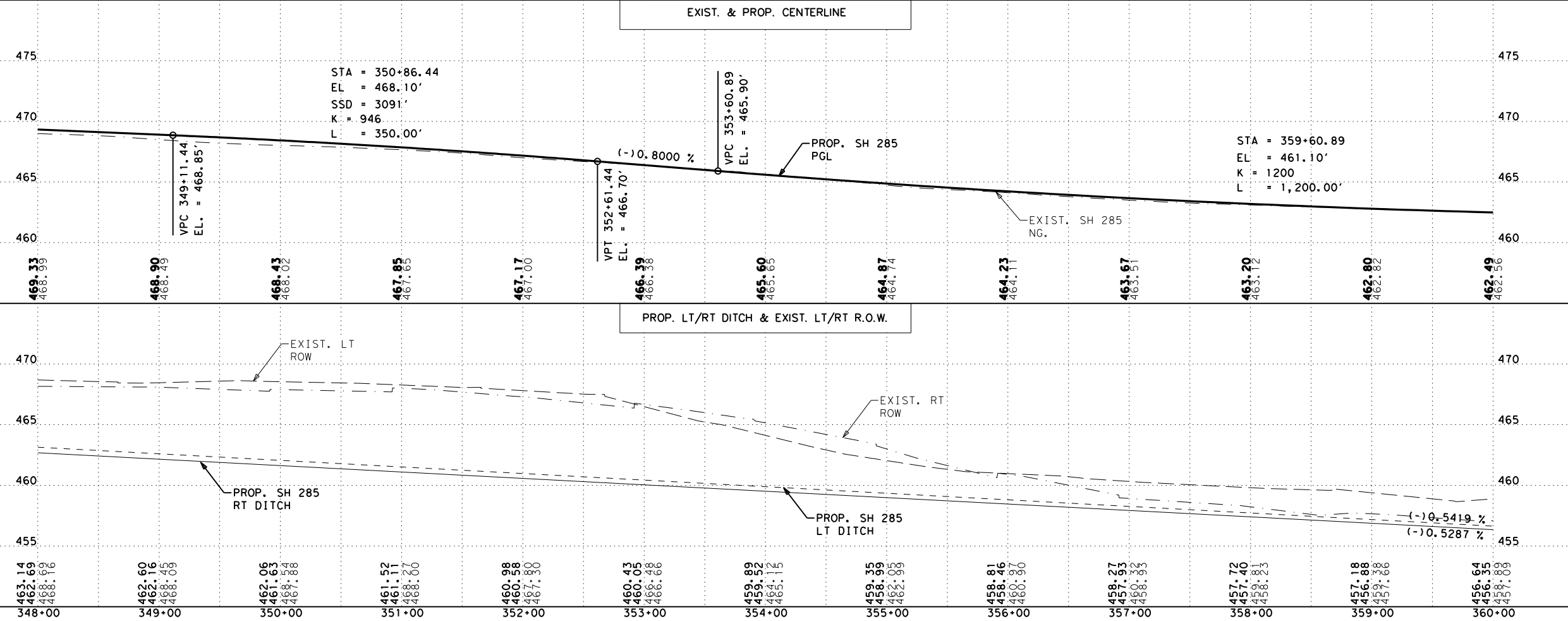


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

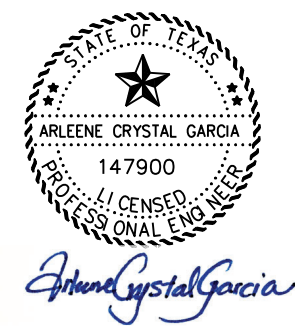
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



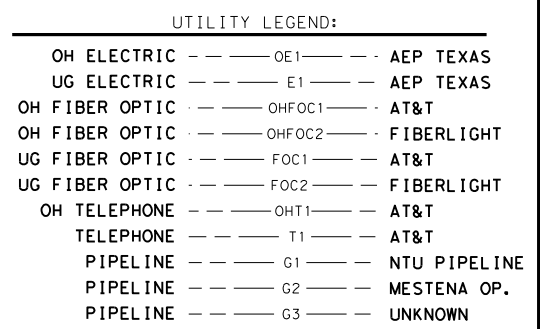
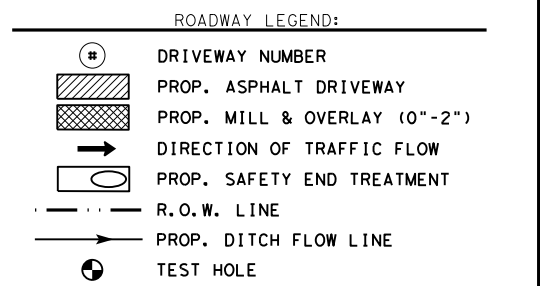
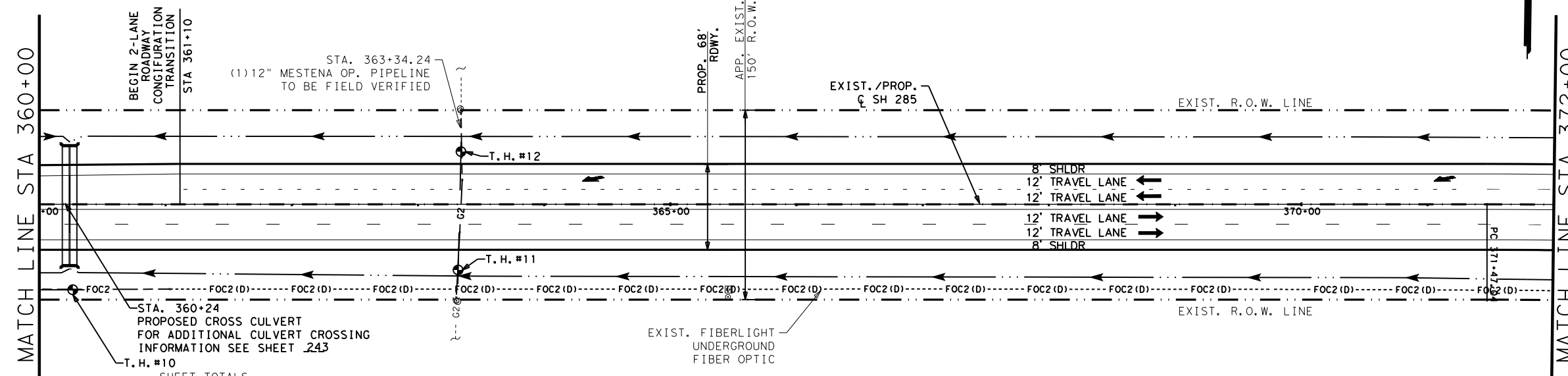
**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 22 OF 44

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	203	

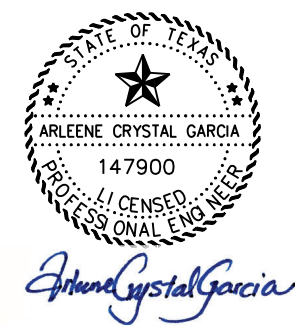
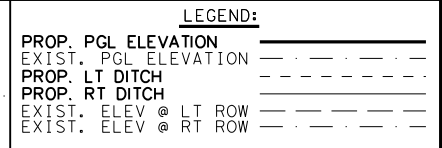
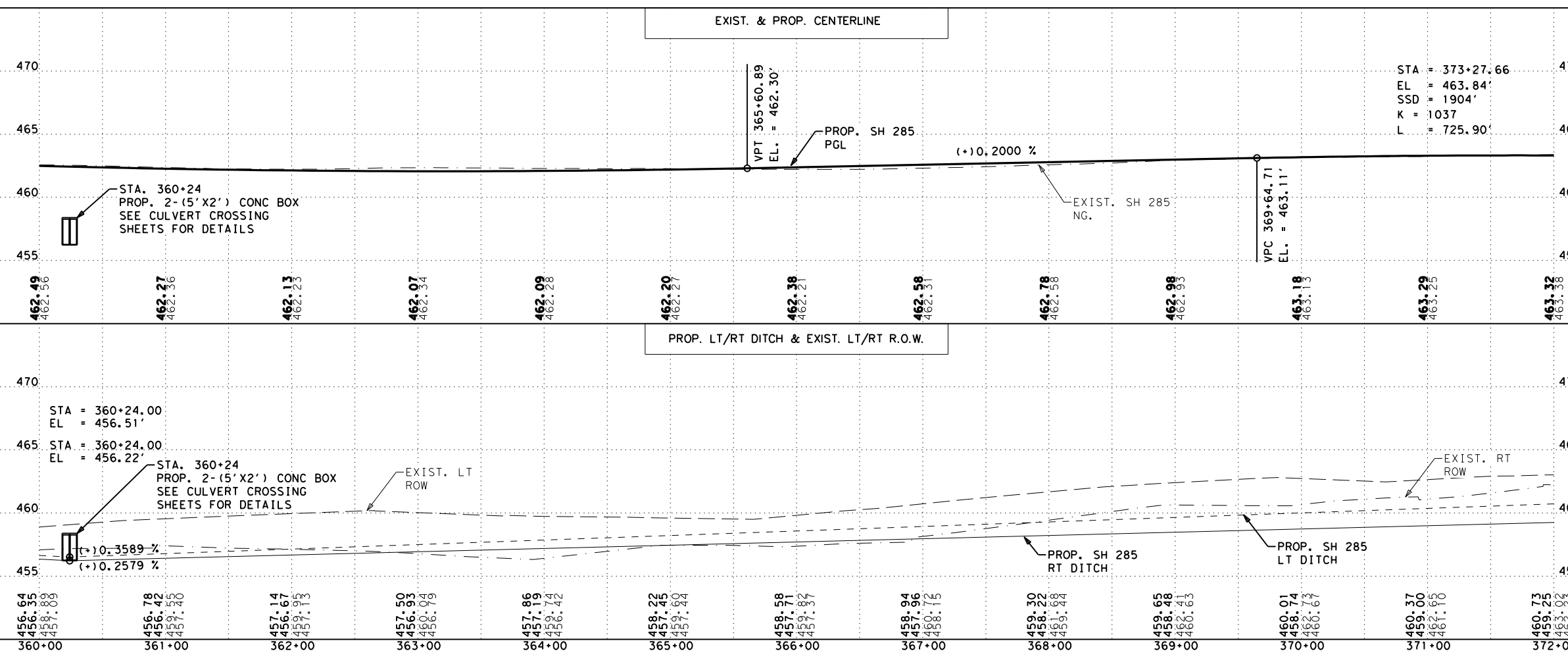
DATE: 2/7/2024 10:54:44 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 3



- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



02.13.2024

Pharr District Central Design
 Texas Department of Transportation

**SH 285
 PLAN AND PROFILE**

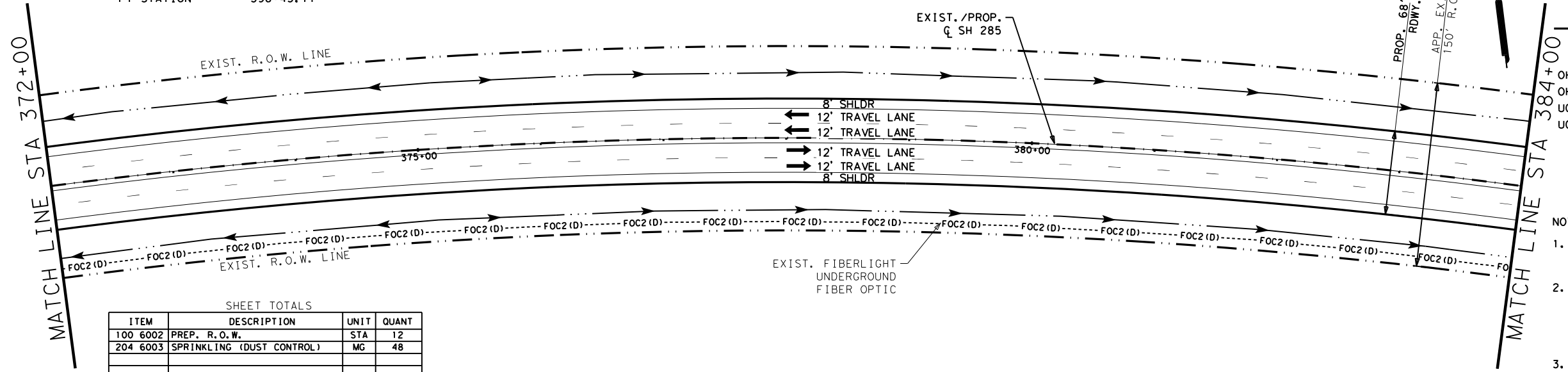
HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 23 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	204

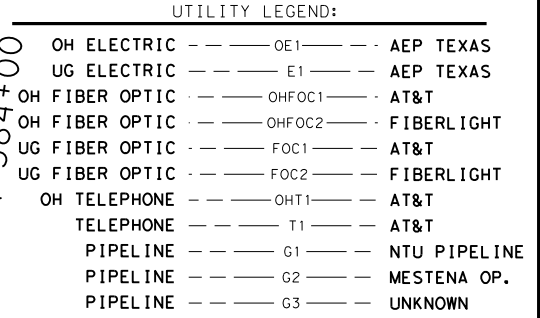
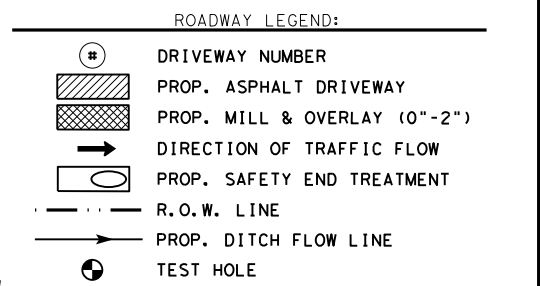
DATE: 2/7/2024 10:54:48 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 3

PI STATION = 385+36.76
 DELTA = 33° 44' 00.21" (RT)
 DEGREE OF CURVE = 1° 15' 00.00"
 TANGENT = 1,389.71
 LENGTH = 2,698.67
 RADIUS = 4,583.66
 PC STATION = 371+47.04
 PT STATION = 398+45.71

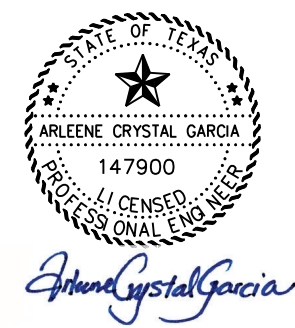
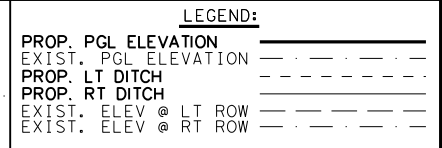
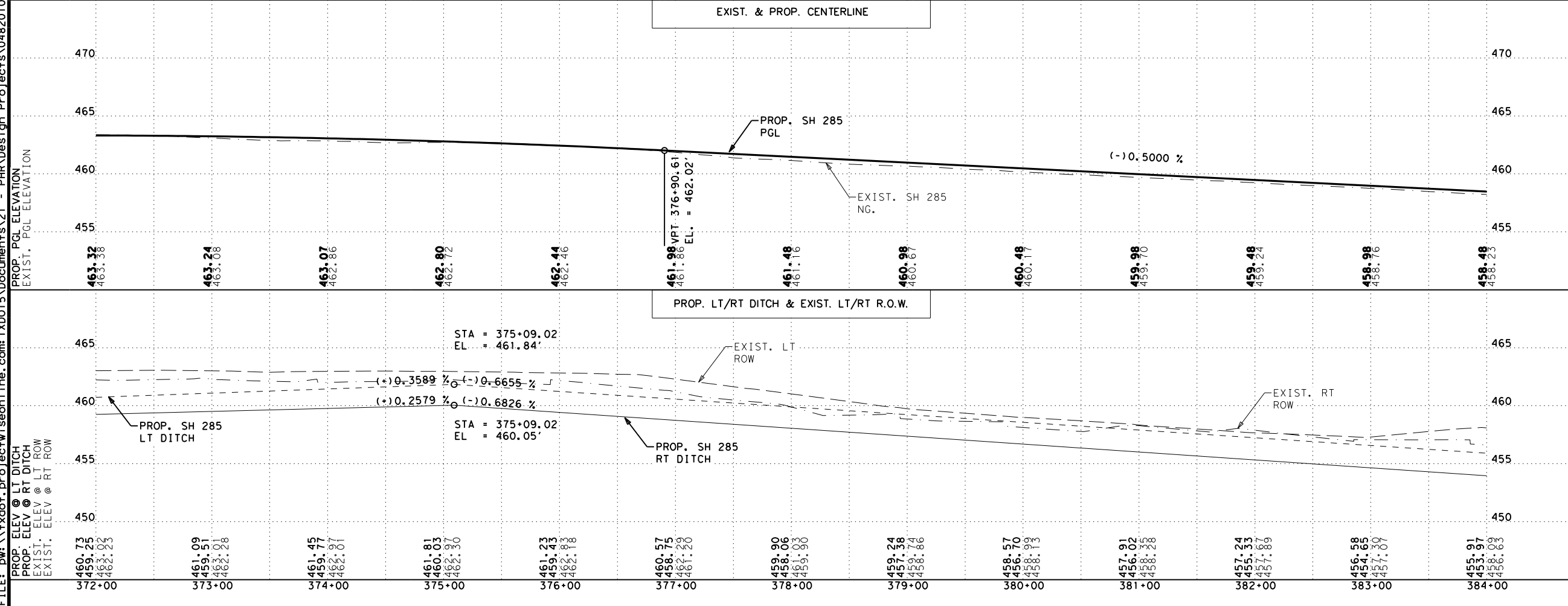


SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



- NOTES:
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



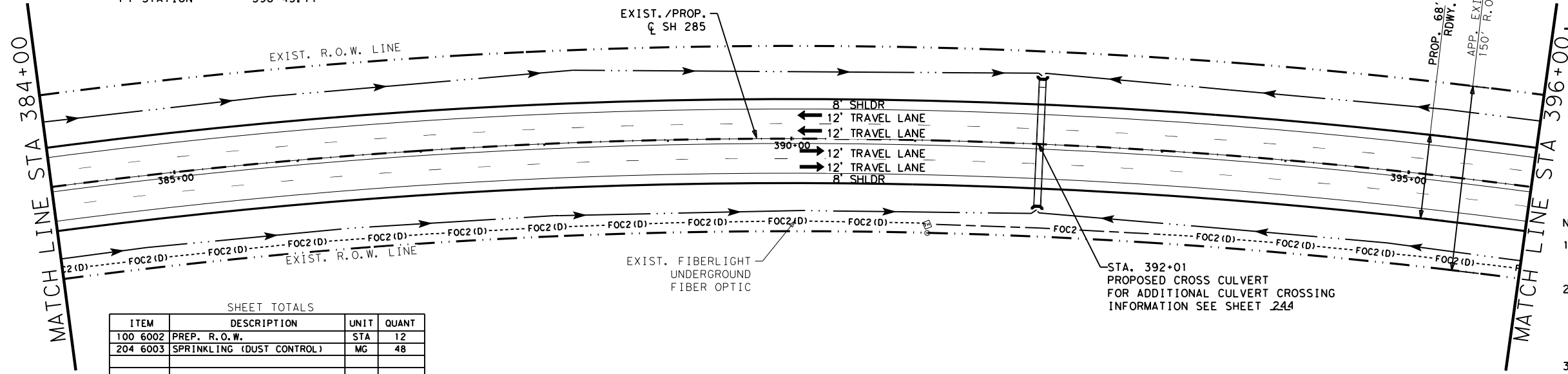
SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG		205	

DATE: 2/7/2024 10:54:51 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 3

PI STATION = 385+36.76
 DELTA = 33° 44' 00.21" (RT)
 DEGREE OF CURVE = 1° 15' 00.00"
 TANGENT = 1,389.71
 LENGTH = 2,698.67
 RADIUS = 4,583.66
 PC STATION = 371+47.04
 PT STATION = 398+45.71

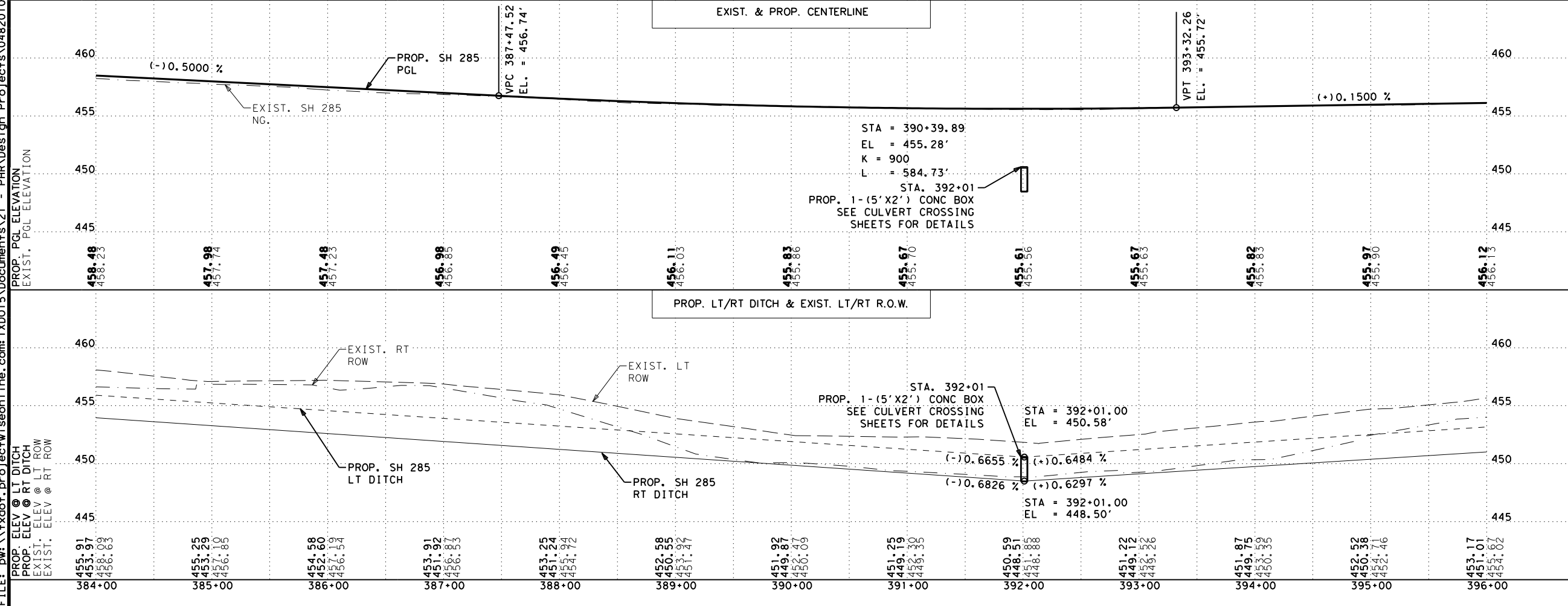


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

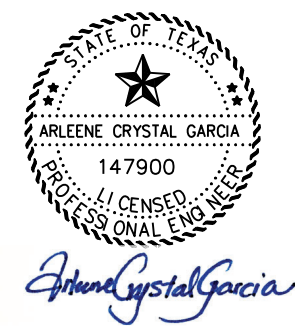
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



02.13.2024

Pharr District Central Design

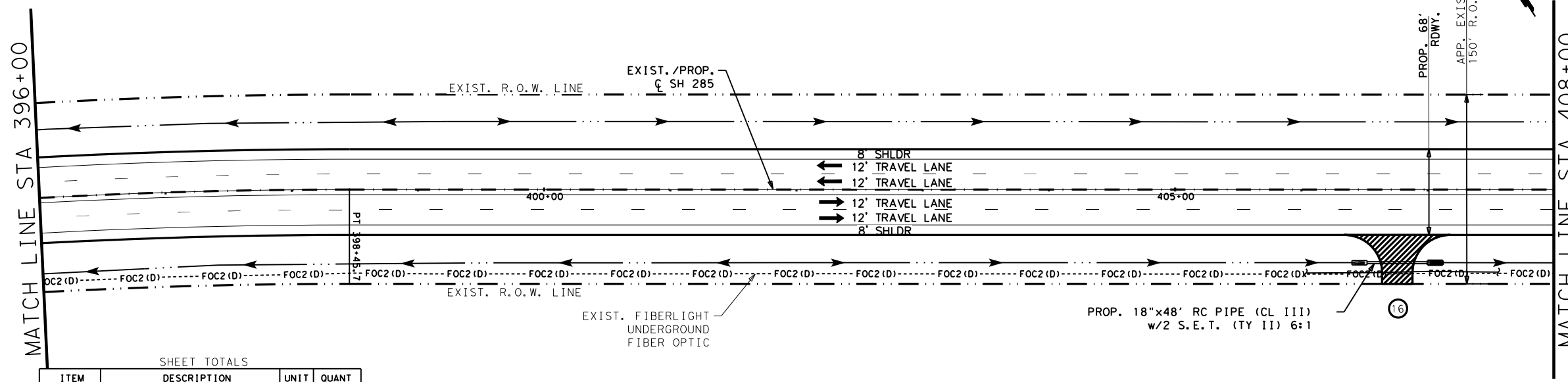


**SH 285
 PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
PHR		JIM HOGG	SHEET NO. 206

DATE: 2/7/2024 10:54:55 AM
 FILE: D:\work\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets\3

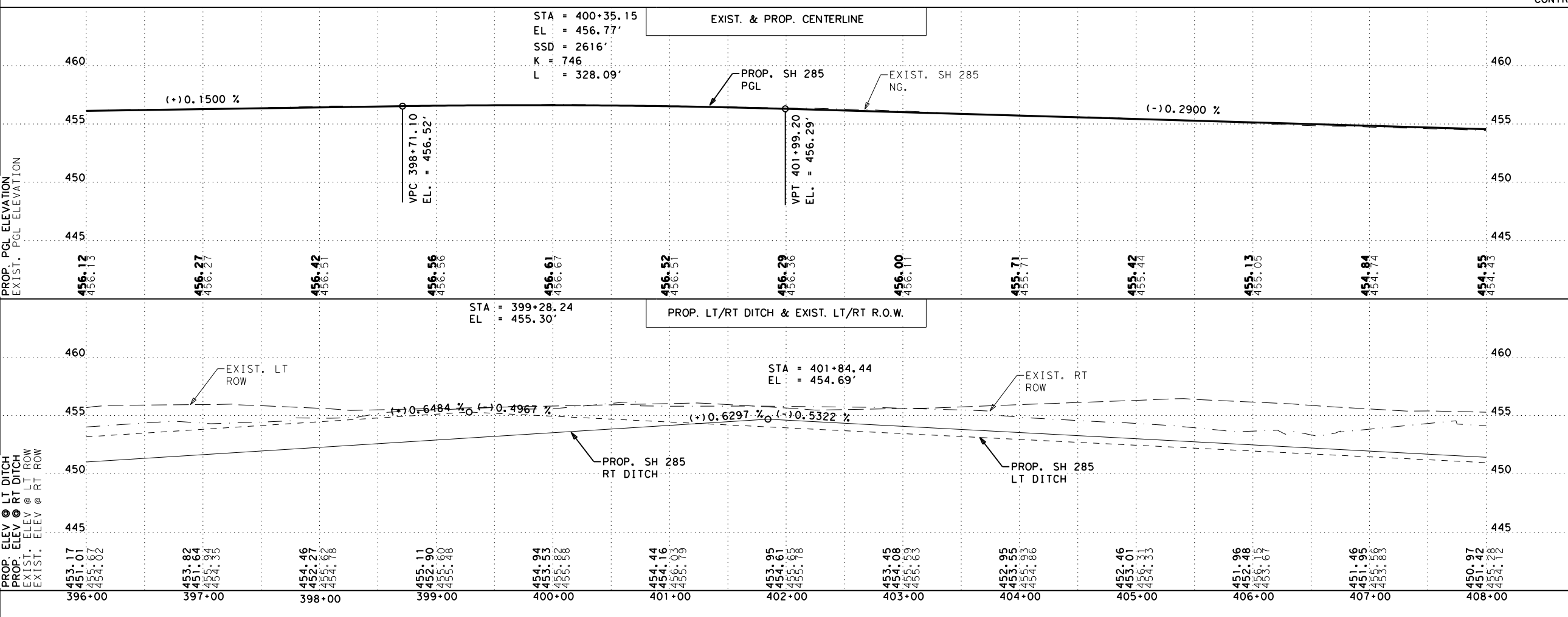


- ROADWAY LEGEND:**
- DRIVWAY NUMBER
 - PROP. ASPHALT DRIVWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

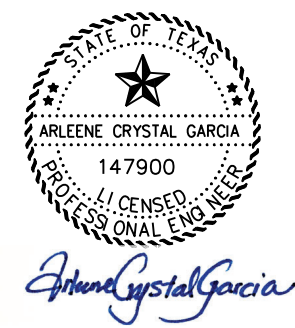
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	32
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	146

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
 PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	207	

DATE: 2/7/2024 10:54:58 AM
 FILE: D:\p\dot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets\3

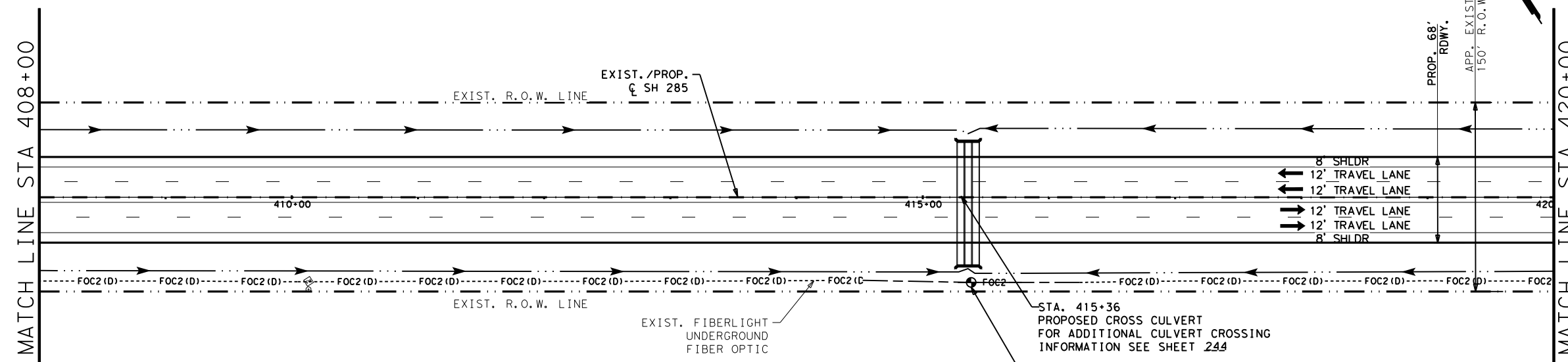
ROADWAY LEGEND:

- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

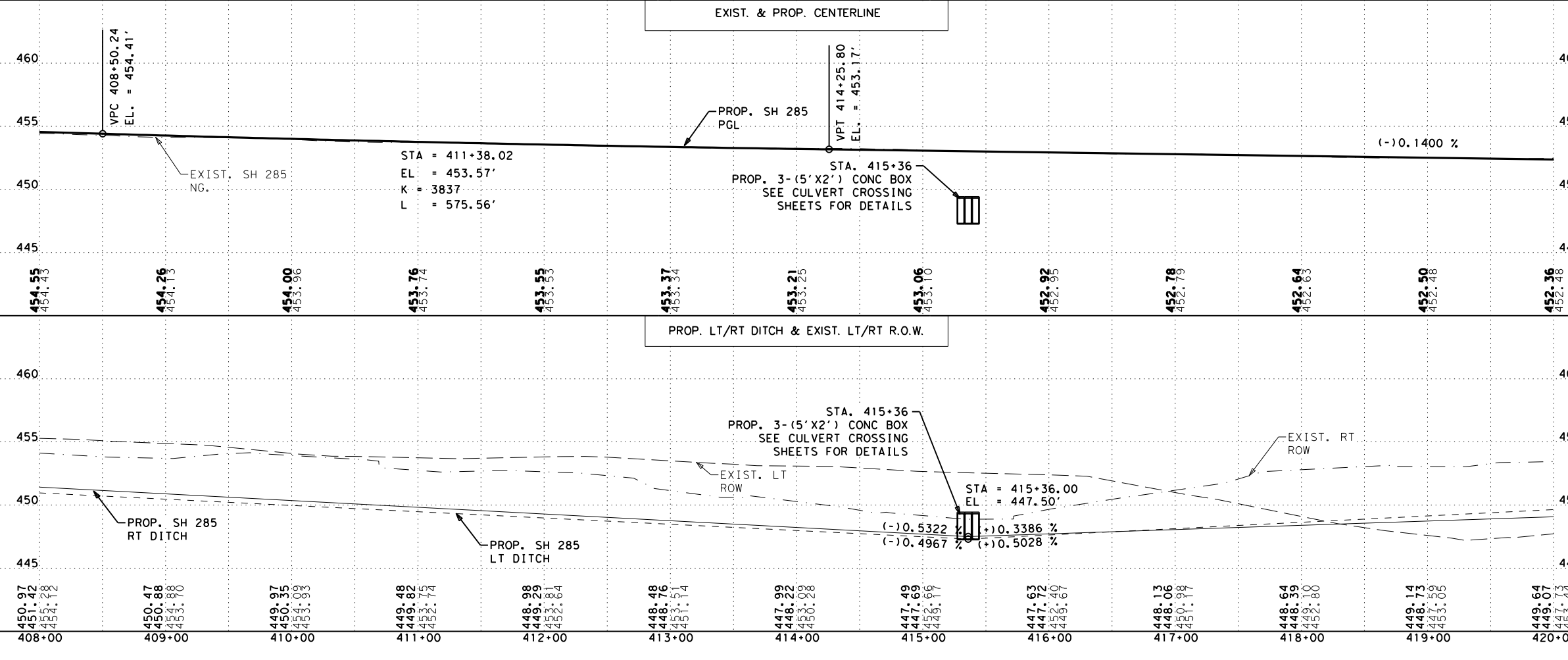
- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

- NOTES:
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

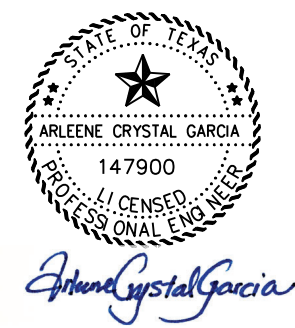


SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

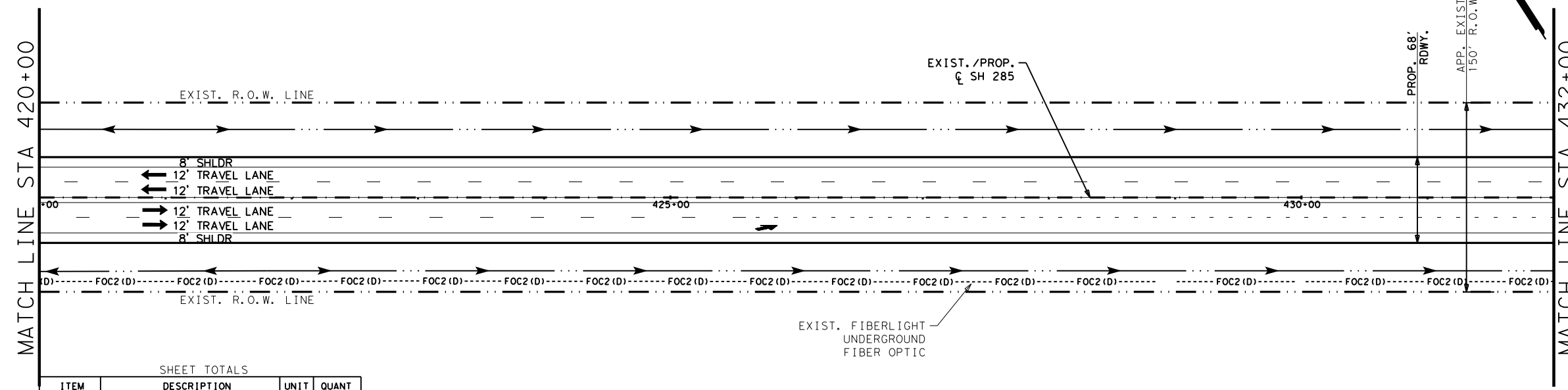


SH 285
PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	208	

DATE: 2/7/2024 10:55:07 AM
 FILE: D:\work\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 4

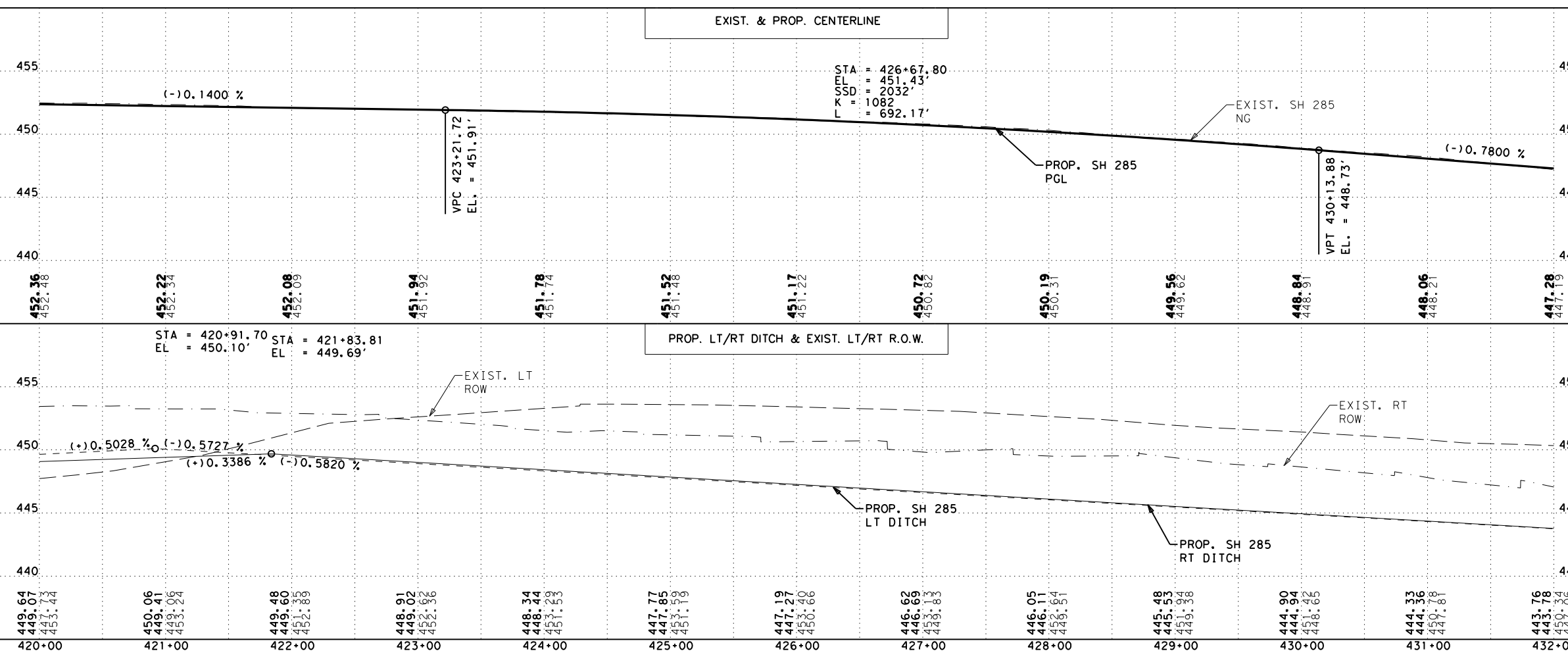


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

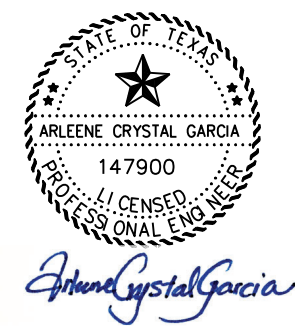
- NOTES:**
1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285 PLAN AND PROFILE

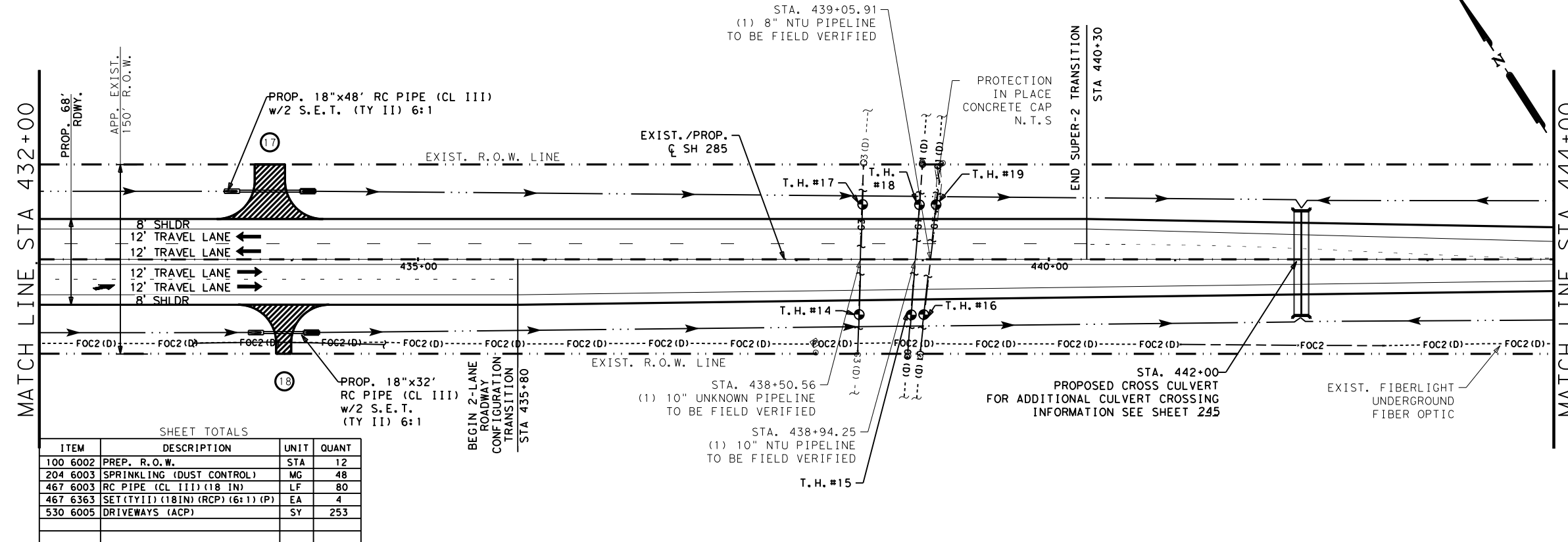
HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	209	

DATE: 2/16/2024 4:14:27 PM
 FILE: D:\work\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 4

MATCH LINE STA 432+00

MATCH LINE STA 444+00

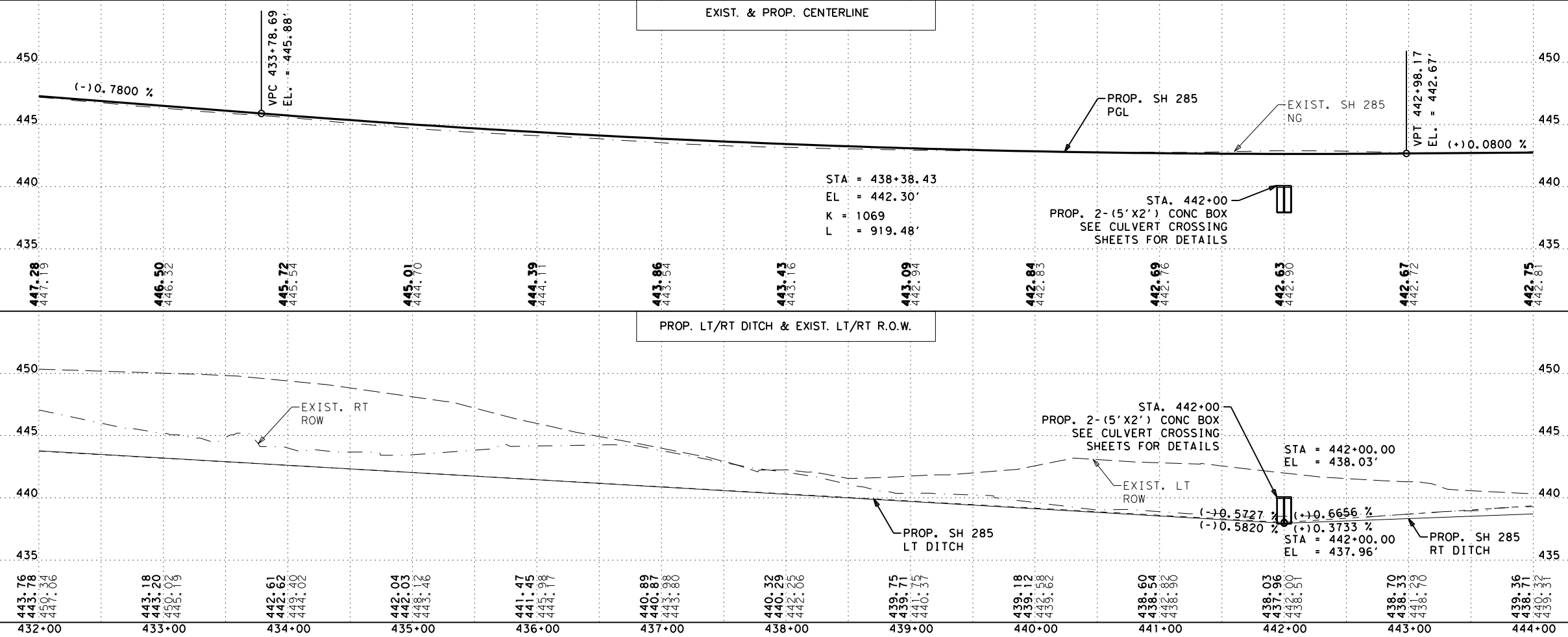


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	80
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	4
530 6005	DRIVEWAYS (ACP)	SY	253



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.16.2024

Pharr District Central Design



**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

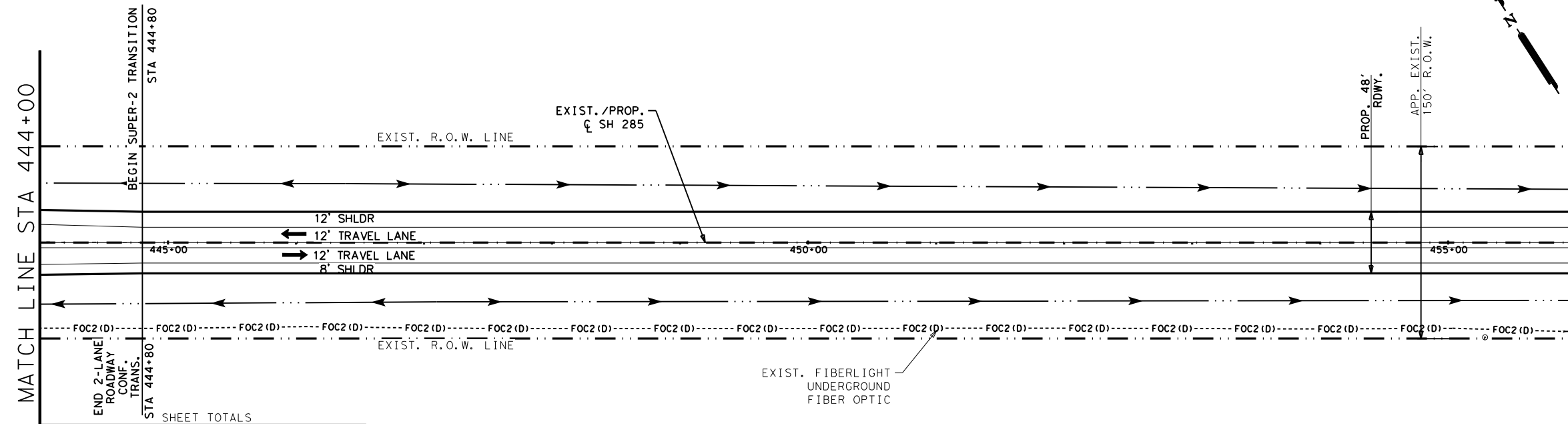
SHEET 29 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	210

DATE: 2/7/2024 10:55:14 AM
 FILE: D:\Projects\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 4

MATCH LINE STA 444+00

MATCH LINE STA 456+00

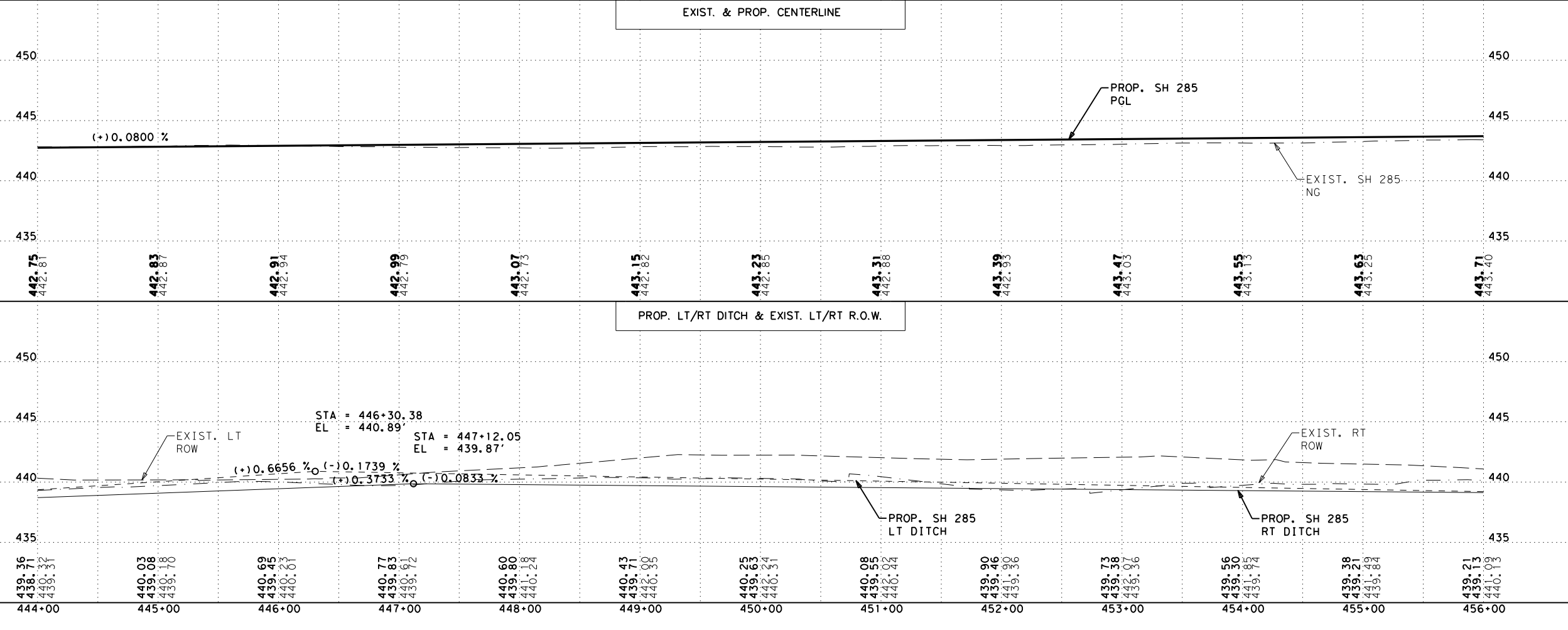


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

Texas Department of Transportation

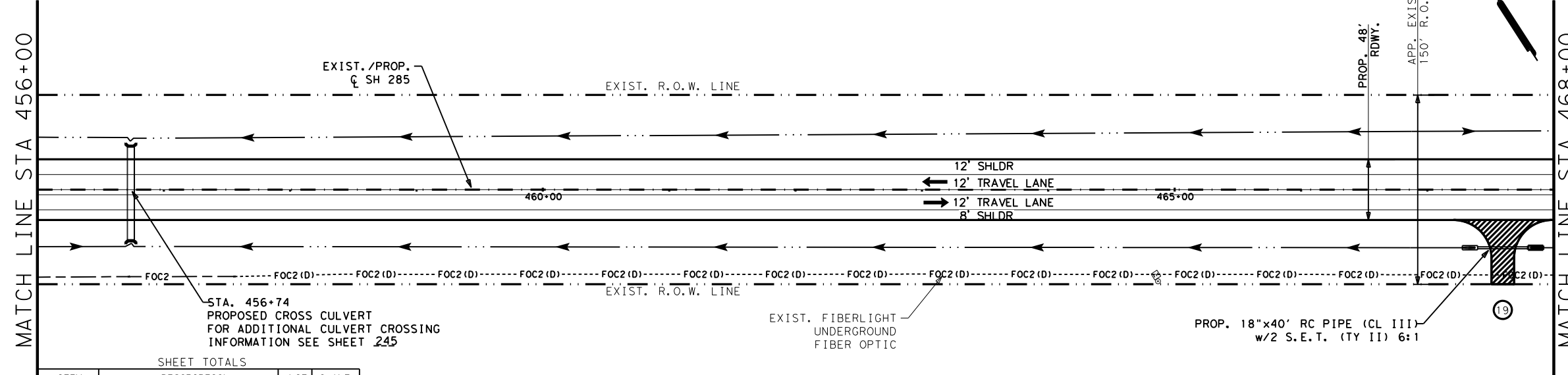
SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 30 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG		211	

DATE: 2/7/2024 10:55:17 AM
 FILE: D:\work\dot\project\wiseon\line.com\TXDOT\5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 4



ROADWAY LEGEND:

- (*) DRIVEWAY NUMBER
- [Hatched] PROP. ASPHALT DRIVEWAY
- [Cross-hatched] PROP. MILL & OVERLAY (0"-2")
- [Arrow] DIRECTION OF TRAFFIC FLOW
- [Square with X] PROP. SAFETY END TREATMENT
- [Dashed line] R.O.W. LINE
- [Solid line with arrow] PROP. DITCH FLOW LINE
- [Circle with cross] TEST HOLE

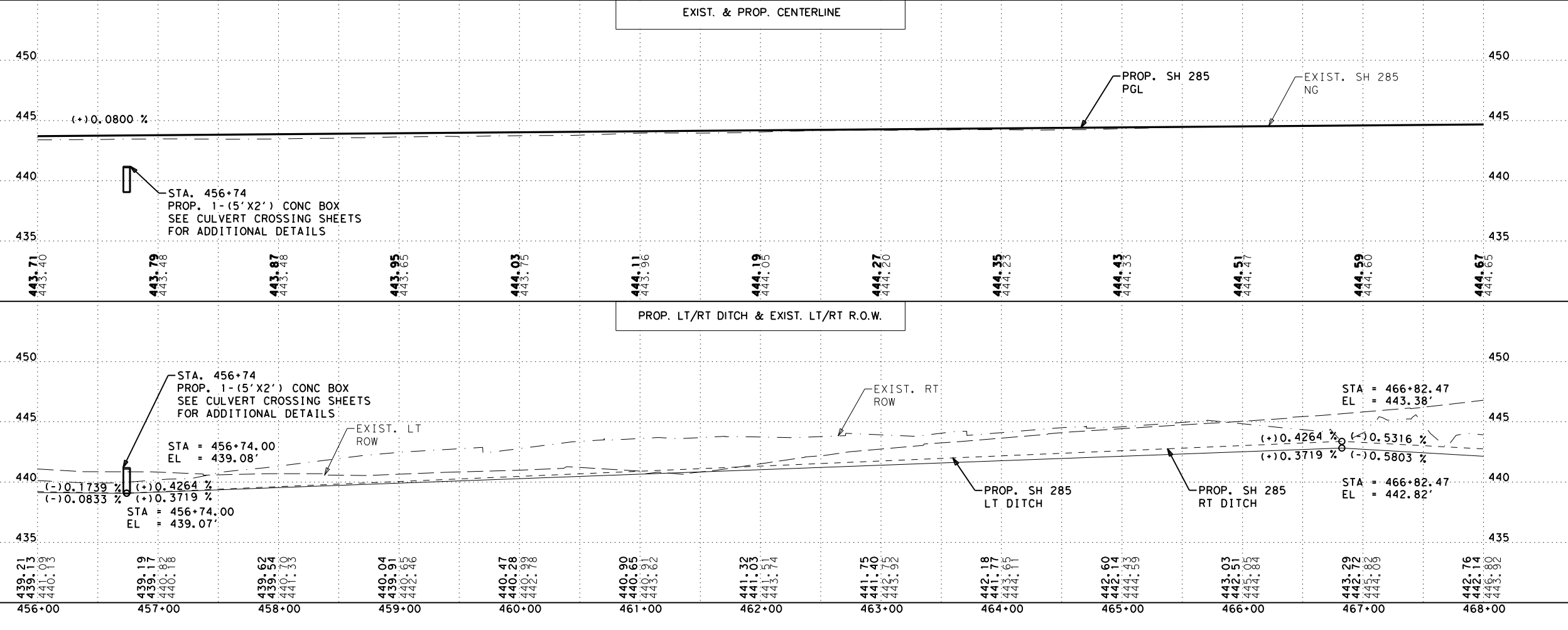
UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

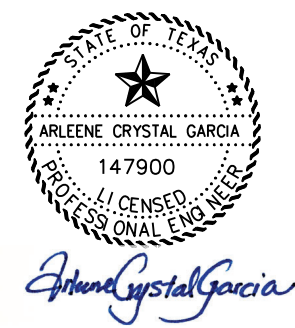
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	40
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	145



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
		COUNTY	SHEET NO.
PHR		JIM HOGG	212

DATE: 2/7/2024 10:55:21 AM
 FILE: D:\work\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 4

ROADWAY LEGEND:

- DRIVWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

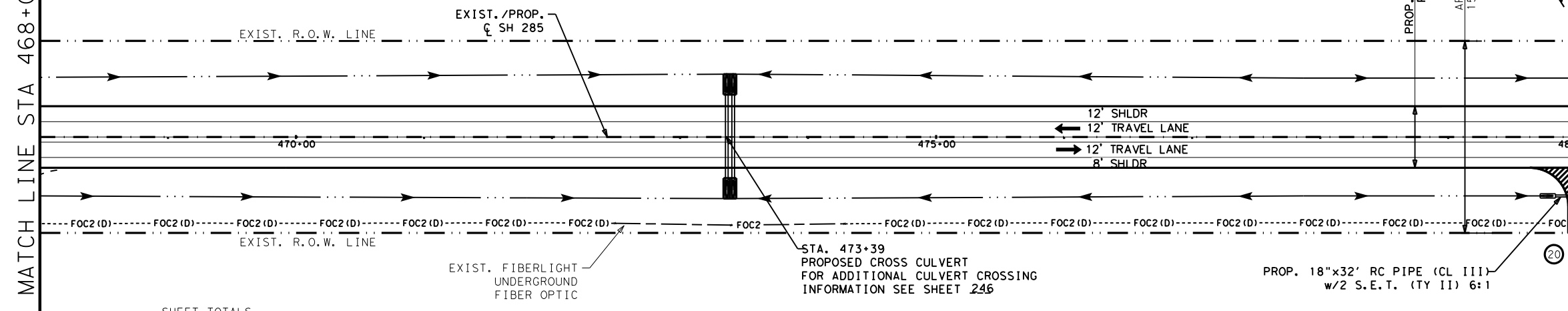
- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

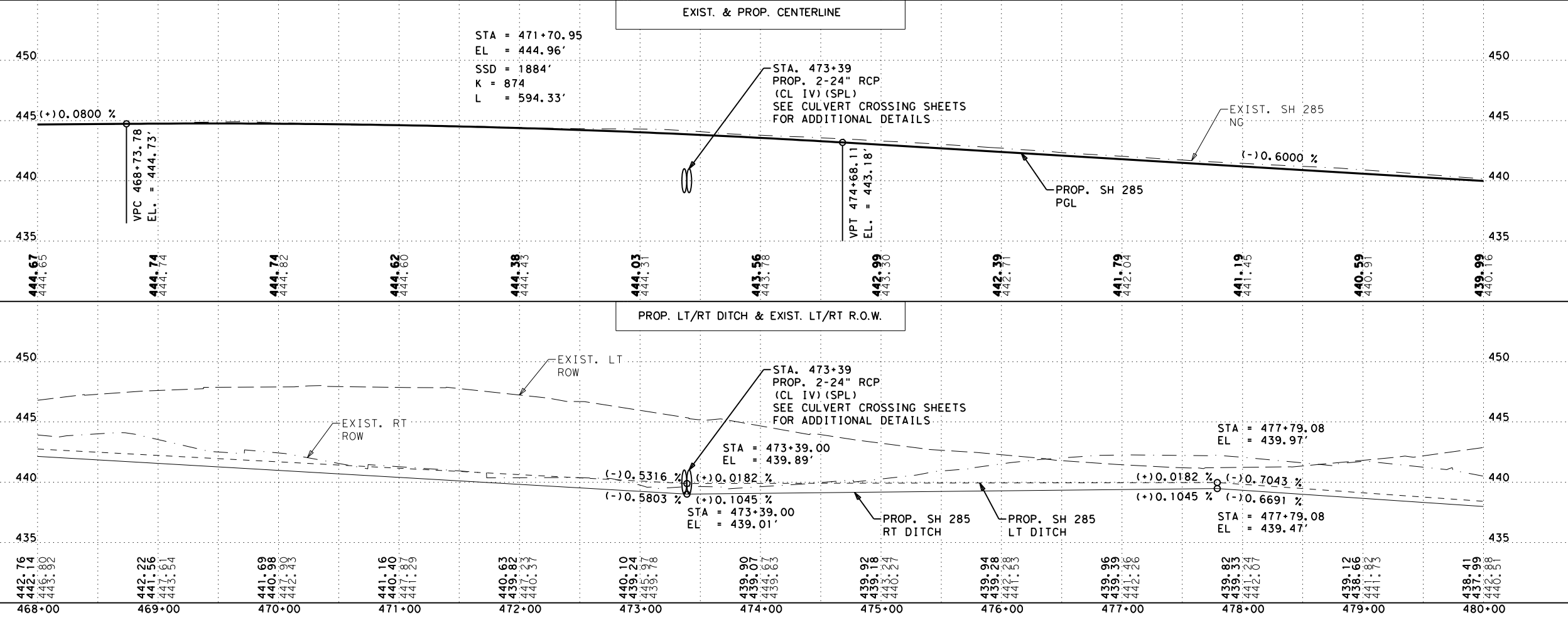
MATCH LINE STA 468+00

MATCH LINE STA 480+00

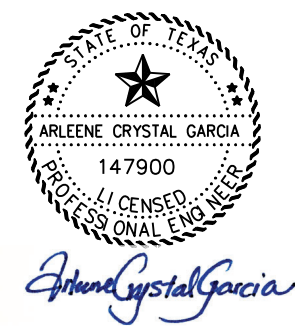


SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	32
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAY (ACP)	SY	111



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	213

DATE: 2/7/2024 10:55:24 AM
 FILE: D:\Projects\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 4

ROADWAY LEGEND:

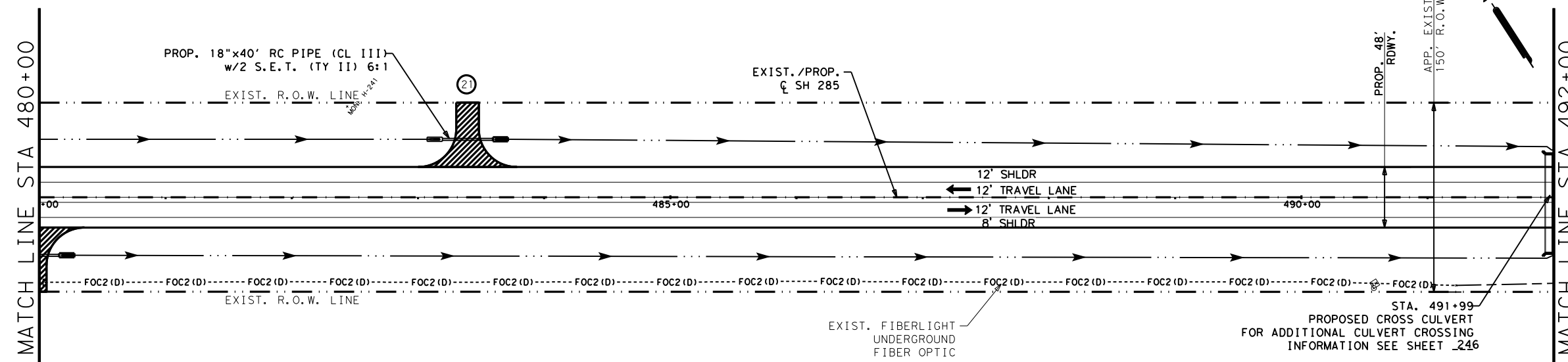
- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

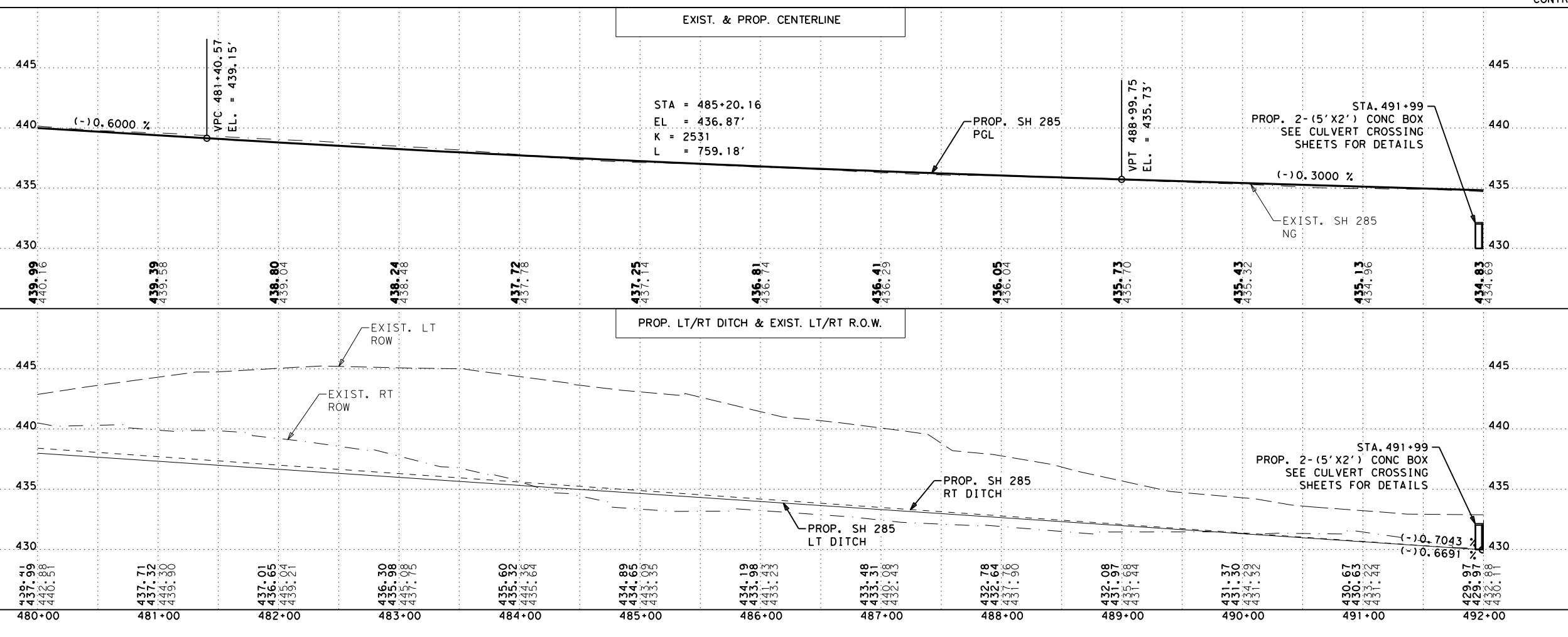
NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	40
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	2
530 6005	DRIVEWAYS (ACP)	SY	145



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

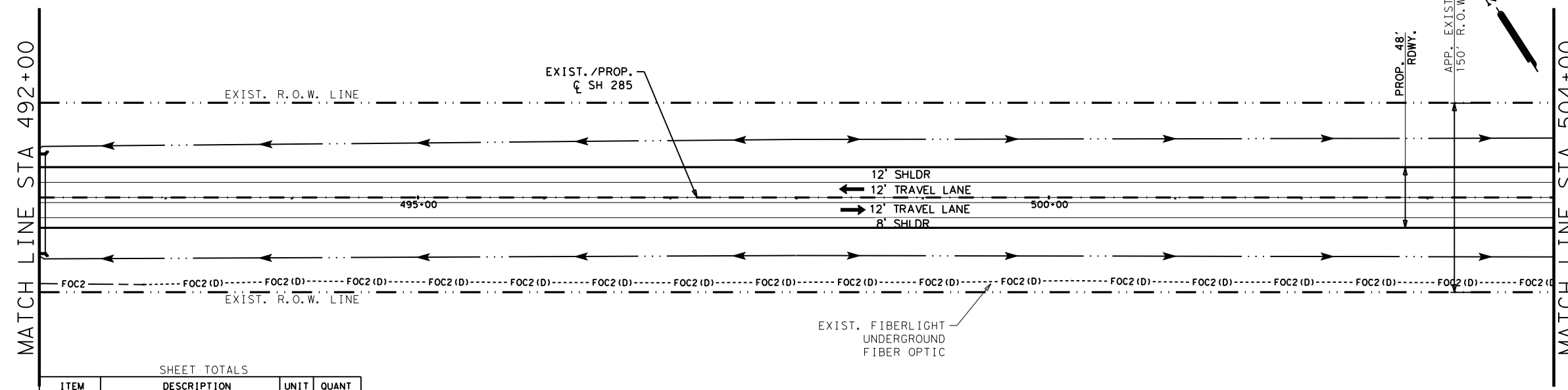


SH 285
 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		214

DATE: 2/7/2024 10:55:28 AM
 FILE: D:\Projects\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\SH 285 P&P Sheets 4

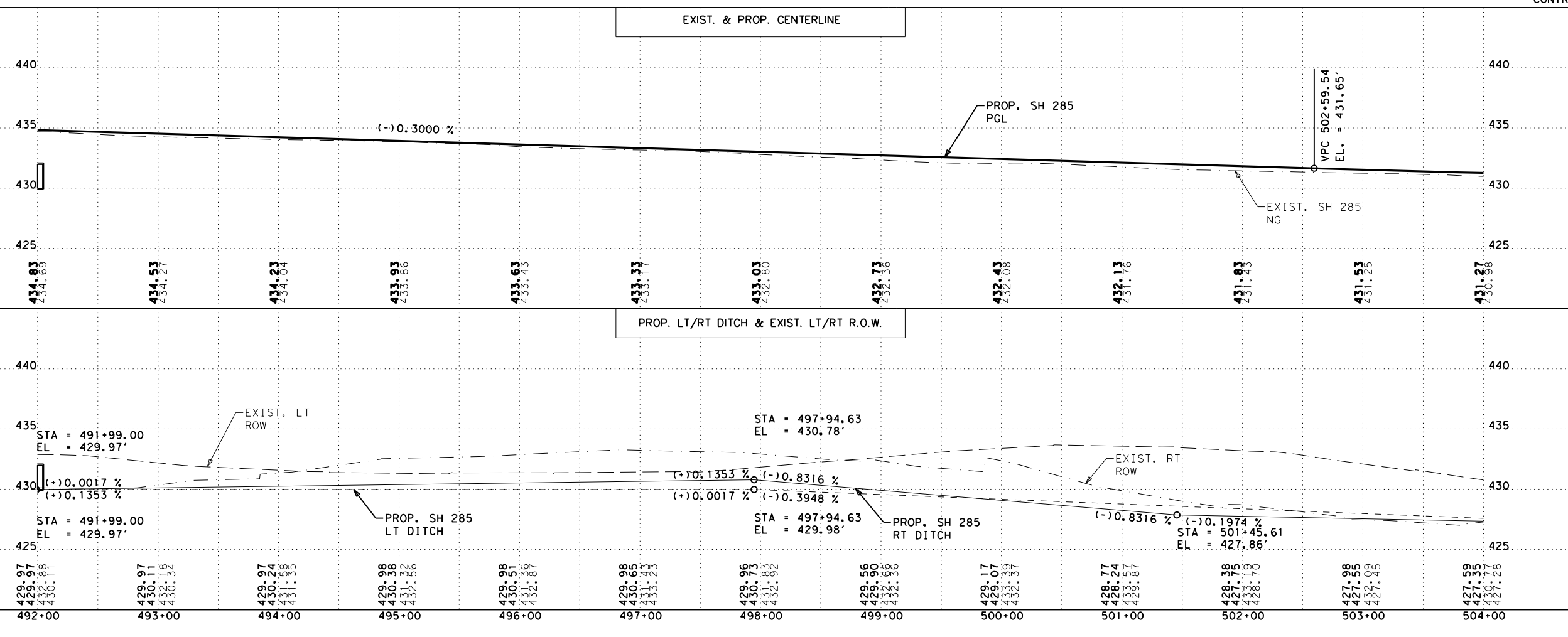


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

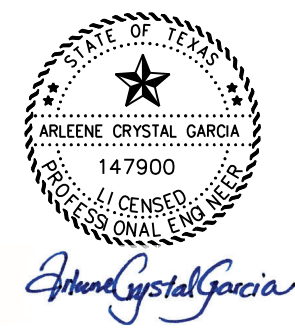
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	215

DATE: 2/7/2024 10:55:31 AM
 FILE: D:\work\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 4

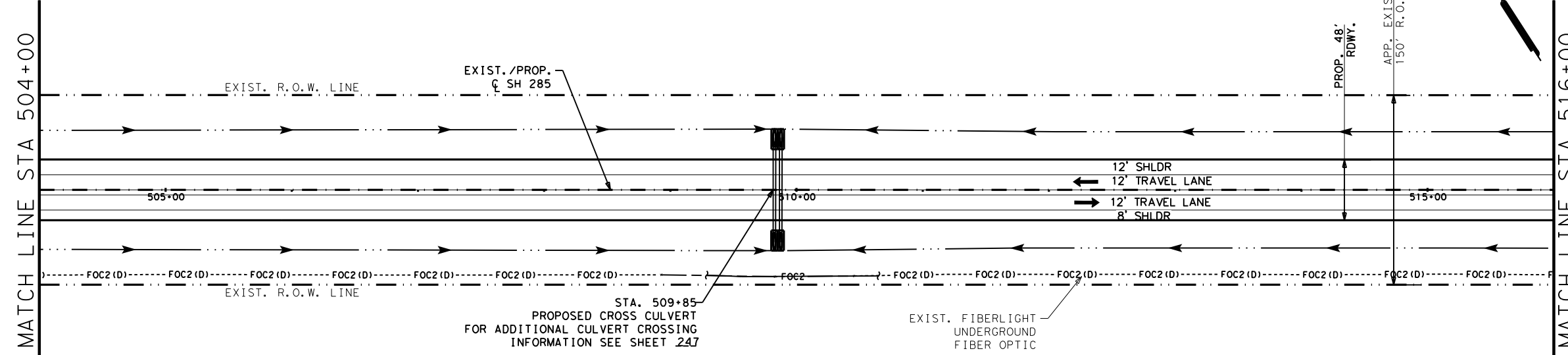
ROADWAY LEGEND:

- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

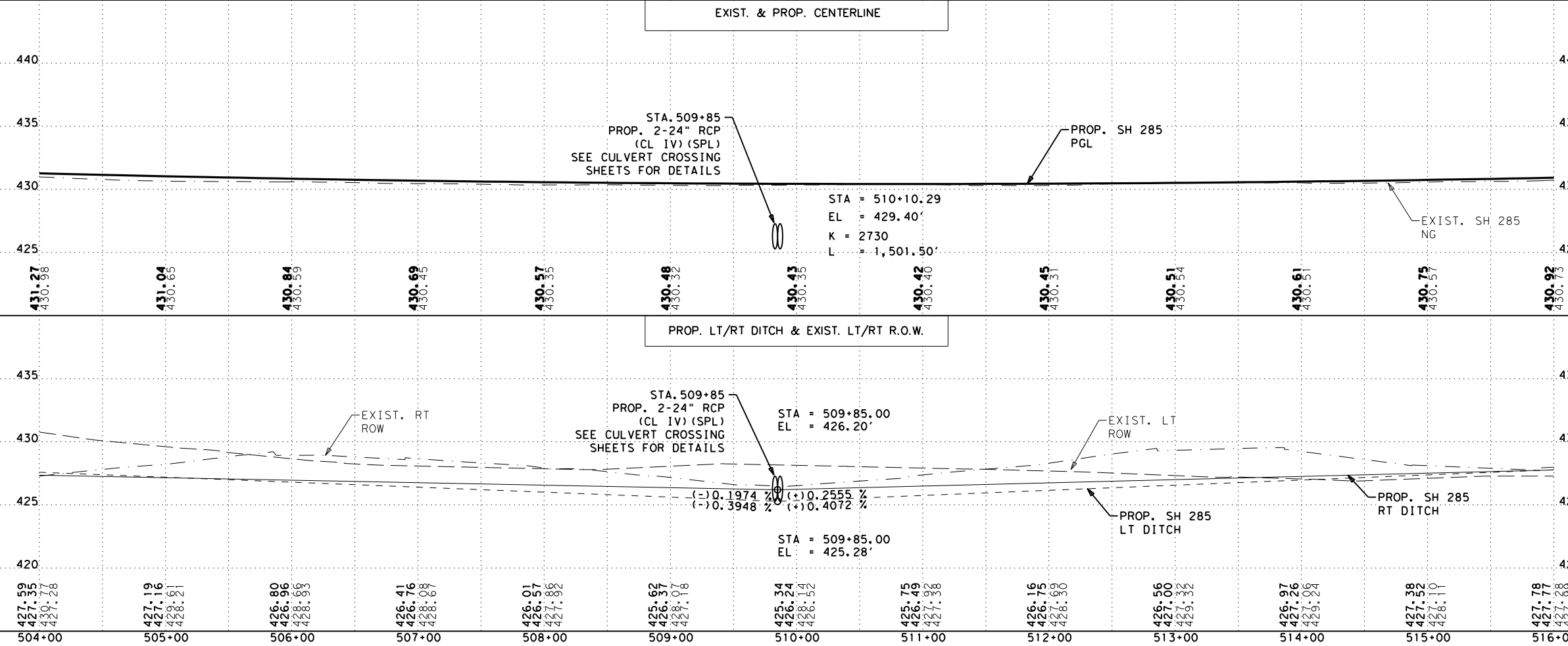
- OH ELECTRIC ----- OE1 ----- AEP TEXAS
- UG ELECTRIC ----- E1 ----- AEP TEXAS
- OH FIBER OPTIC ----- OHFOC1 ----- AT&T
- OH FIBER OPTIC ----- OHFOC2 ----- FIBERLIGHT
- UG FIBER OPTIC ----- FOC1 ----- AT&T
- UG FIBER OPTIC ----- FOC2 ----- FIBERLIGHT
- OH TELEPHONE ----- OHT1 ----- AT&T
- TELEPHONE ----- T1 ----- AT&T
- PIPELINE ----- G1 ----- NTU PIPELINE
- PIPELINE ----- G2 ----- MESTENA OP.
- PIPELINE ----- G3 ----- UNKNOWN

- NOTES:
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



- LEGEND:
- PROP. PGL ELEVATION -----
 - EXIST. PGL ELEVATION -----
 - PROP. LT DITCH -----
 - PROP. RT DITCH -----
 - EXIST. ELEV @ LT ROW -----
 - EXIST. ELEV @ RT ROW -----



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

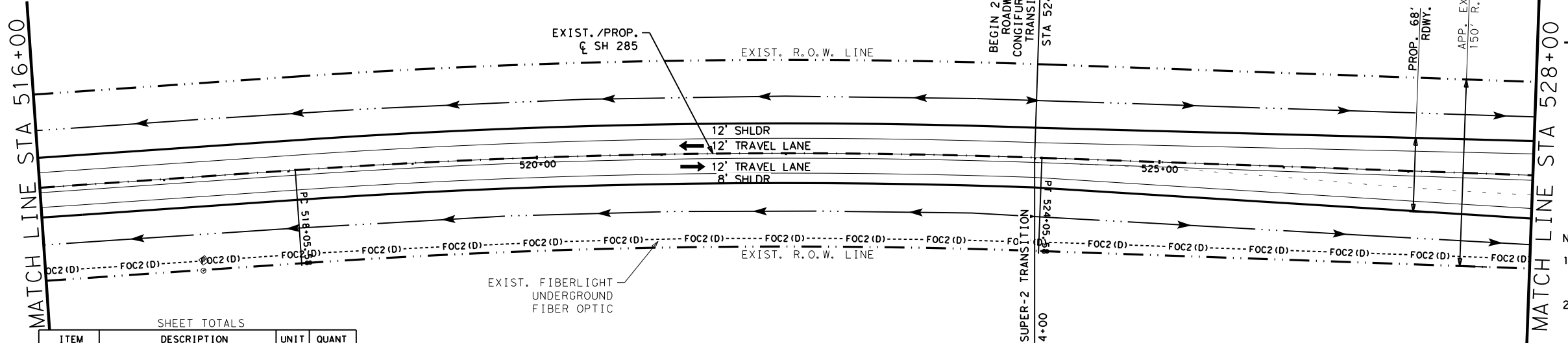


SH 285
 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	216

DATE: 2/7/2024 10:55:35 AM
 FILE: D:\work\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 4

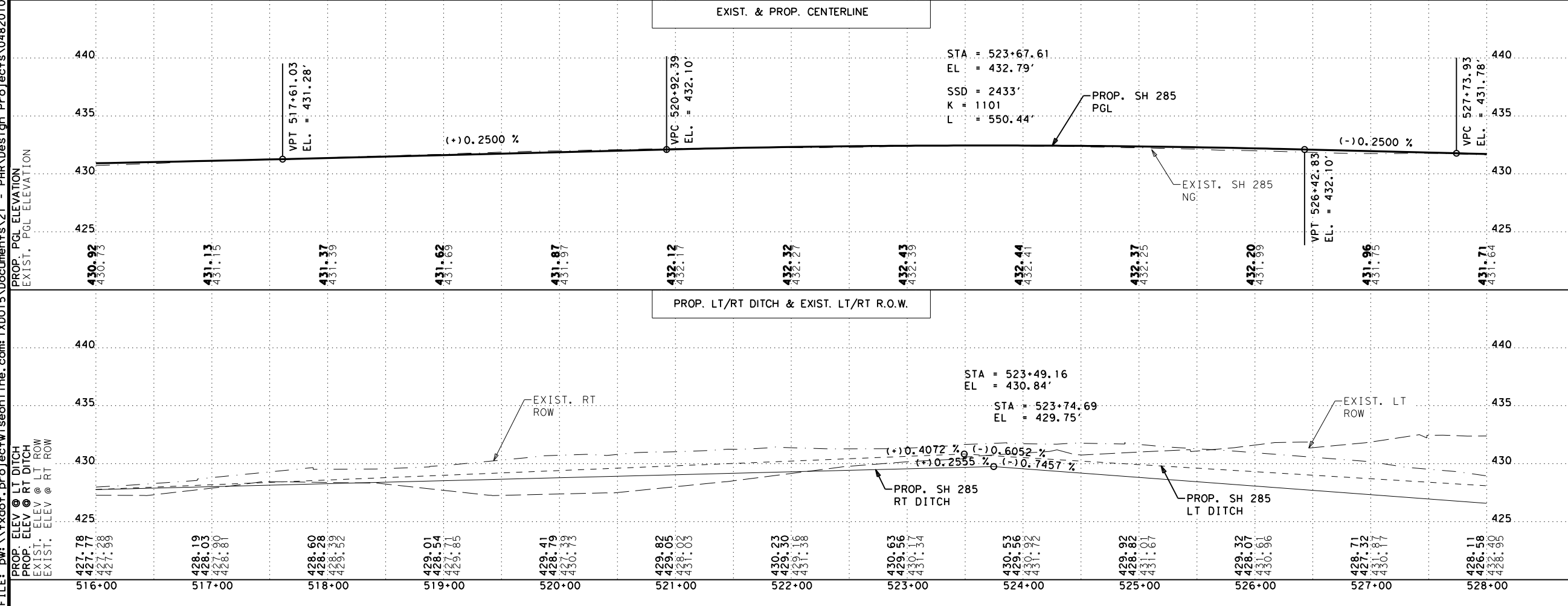


- ROADWAY LEGEND:**
- (*) DRIVEWAY NUMBER
 - [Hatched Box] PROP. ASPHALT DRIVEWAY
 - [Cross-hatched Box] PROP. MILL & OVERLAY (0"-2")
 - [Arrow] DIRECTION OF TRAFFIC FLOW
 - [Circle with X] PROP. SAFETY END TREATMENT
 - [Dashed Line] R.O.W. LINE
 - [Line with Arrow] PROP. DITCH FLOW LINE
 - [Circle with Center] TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

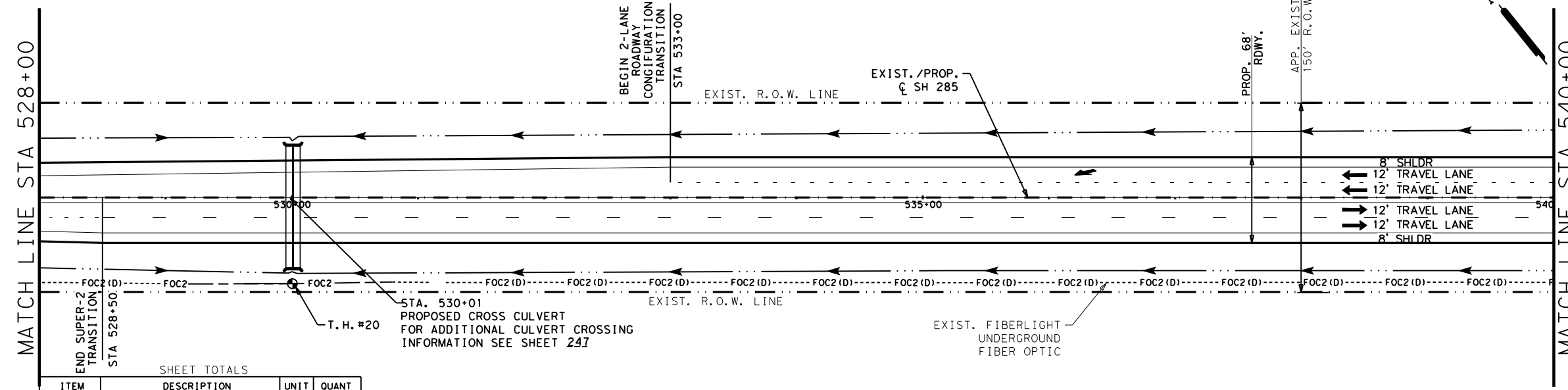
Texas Department of Transportation

**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	217	

DATE: 2/7/2024 10:55:42 AM
 FILE: D:\p\dot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 5

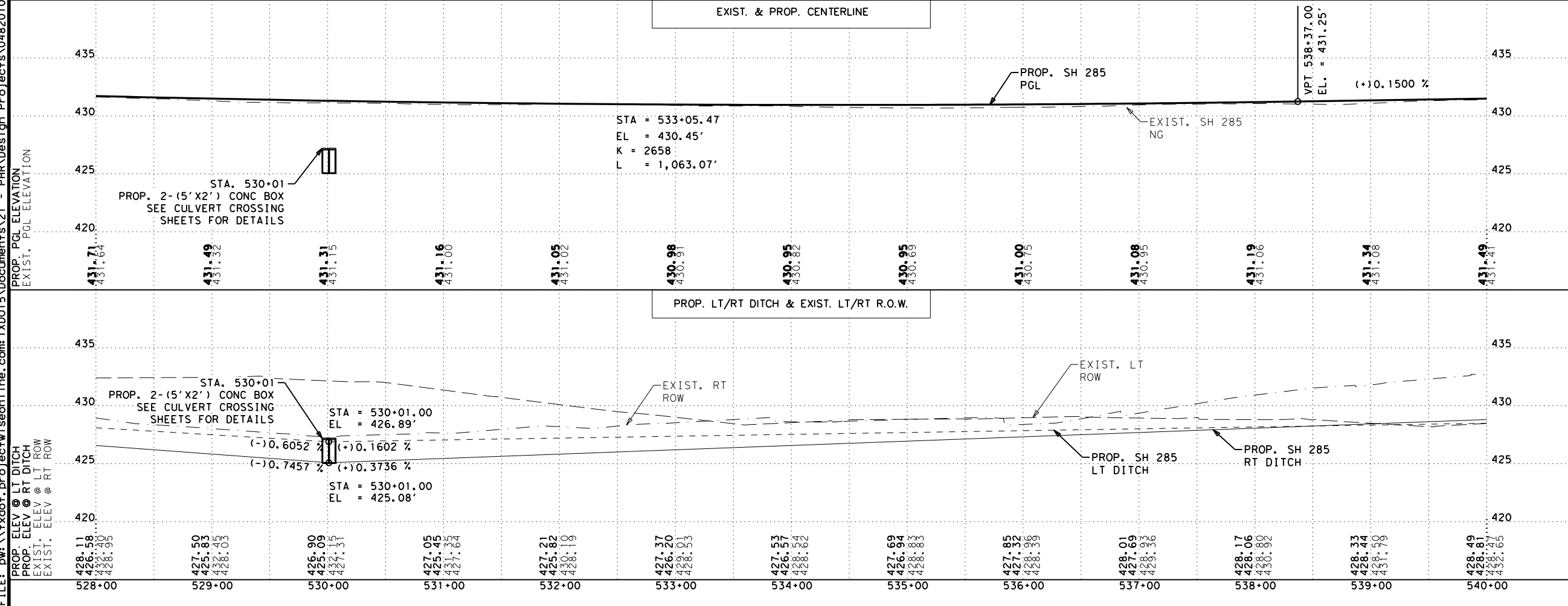


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	218

DATE: 2/7/2024 10:55:45 AM
 FILE: D:\work\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 5

ROADWAY LEGEND:

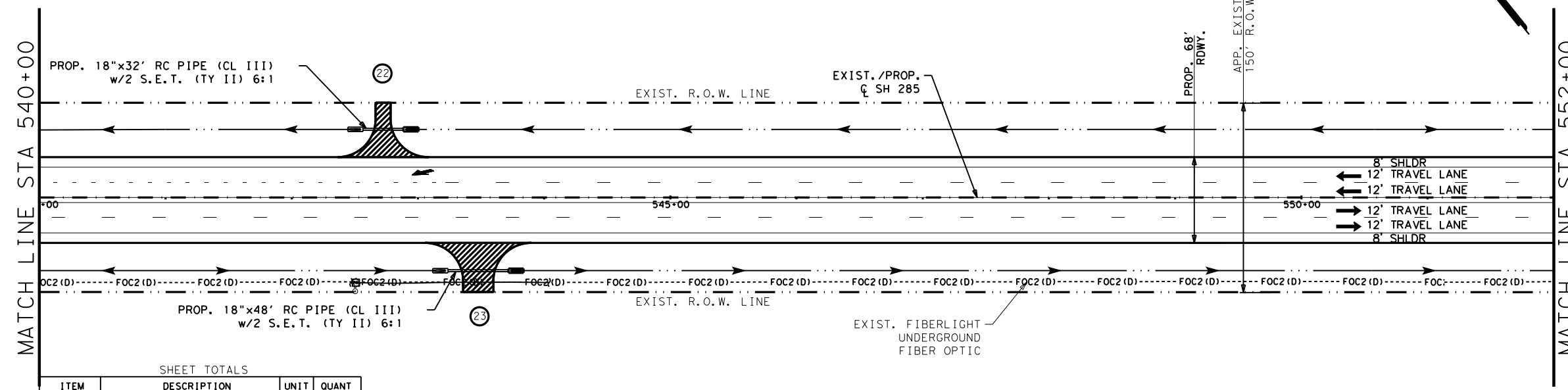
- DRIVEWAY NUMBER
- PROP. ASPHALT DRIVEWAY
- PROP. MILL & OVERLAY (0"-2")
- DIRECTION OF TRAFFIC FLOW
- PROP. SAFETY END TREATMENT
- R.O.W. LINE
- PROP. DITCH FLOW LINE
- TEST HOLE

UTILITY LEGEND:

- OH ELECTRIC --- OE1 --- AEP TEXAS
- UG ELECTRIC --- E1 --- AEP TEXAS
- OH FIBER OPTIC --- OHFOC1 --- AT&T
- OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
- UG FIBER OPTIC --- FOC1 --- AT&T
- UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
- OH TELEPHONE --- OHT1 --- AT&T
- TELEPHONE --- T1 --- AT&T
- PIPELINE --- G1 --- NTU PIPELINE
- PIPELINE --- G2 --- MESTENA OP.
- PIPELINE --- G3 --- UNKNOWN

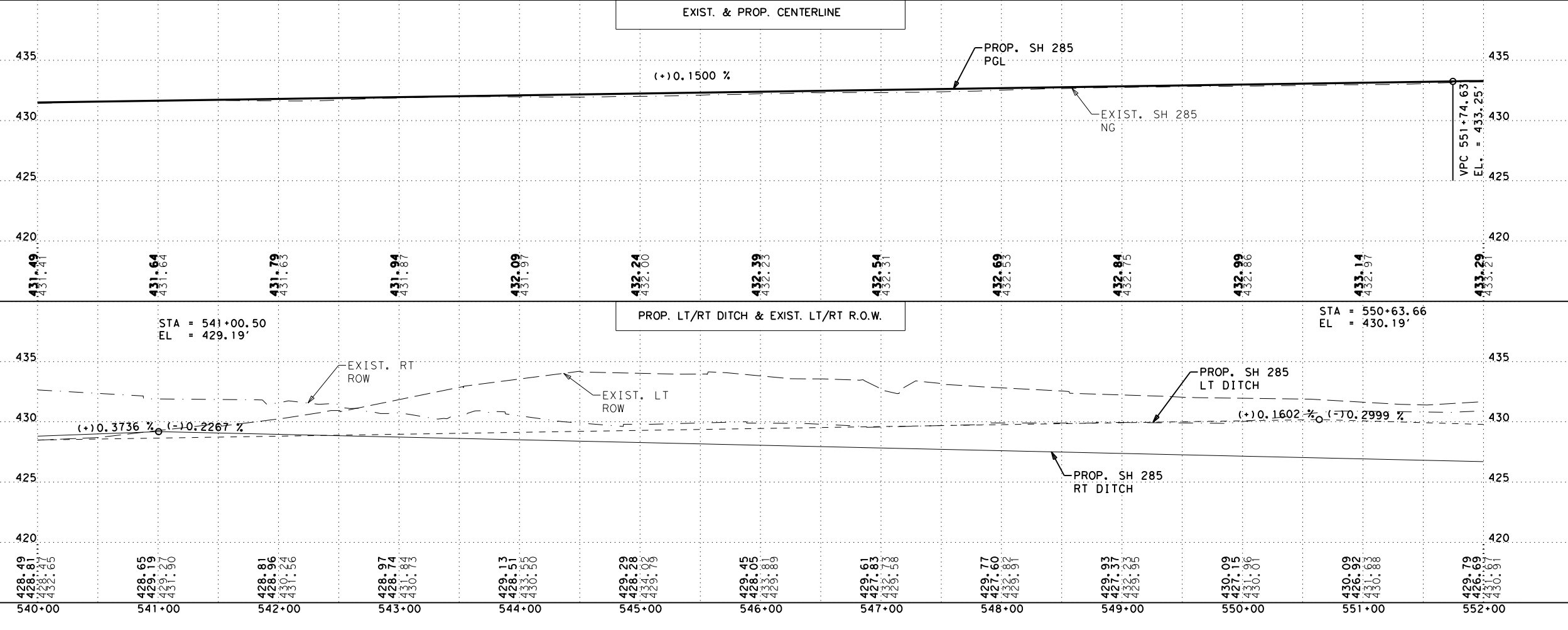
NOTES:

1. ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
2. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
3. ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	80
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	4
530 6005	DRIVEWAYS (ACP)	SY	248



- LEGEND:
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

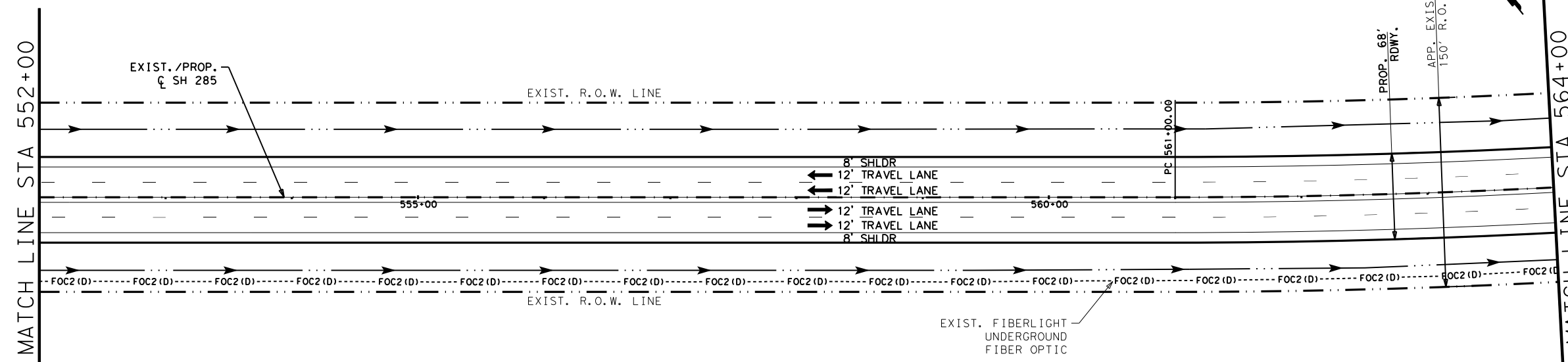
**SH 285
 PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 38 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	219

DATE: 2/7/2024 10:55:49 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 5

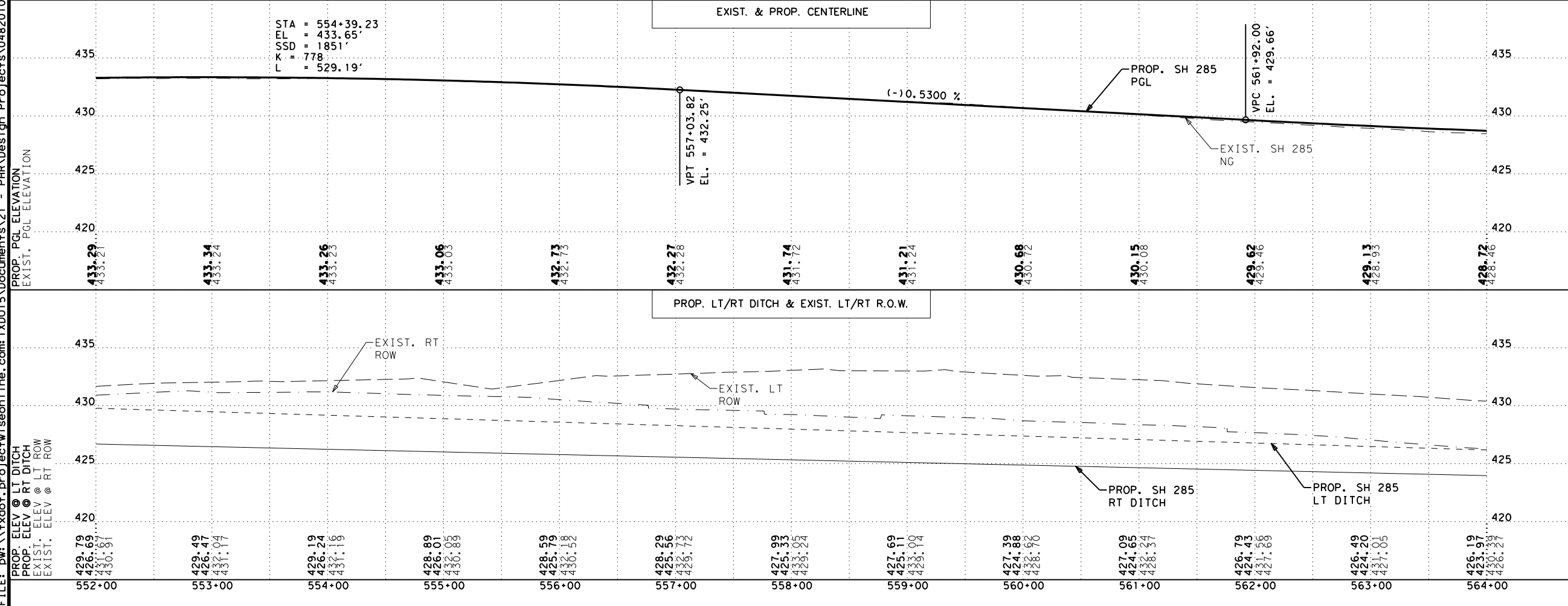


- ROADWAY LEGEND:**
- DRIVWAY NUMBER
 - PROP. ASPHALT DRIVWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

ARLENE CRYSTAL GARCIA
 147900
 PROFESSIONAL ENGINEER

Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

Texas Department of Transportation

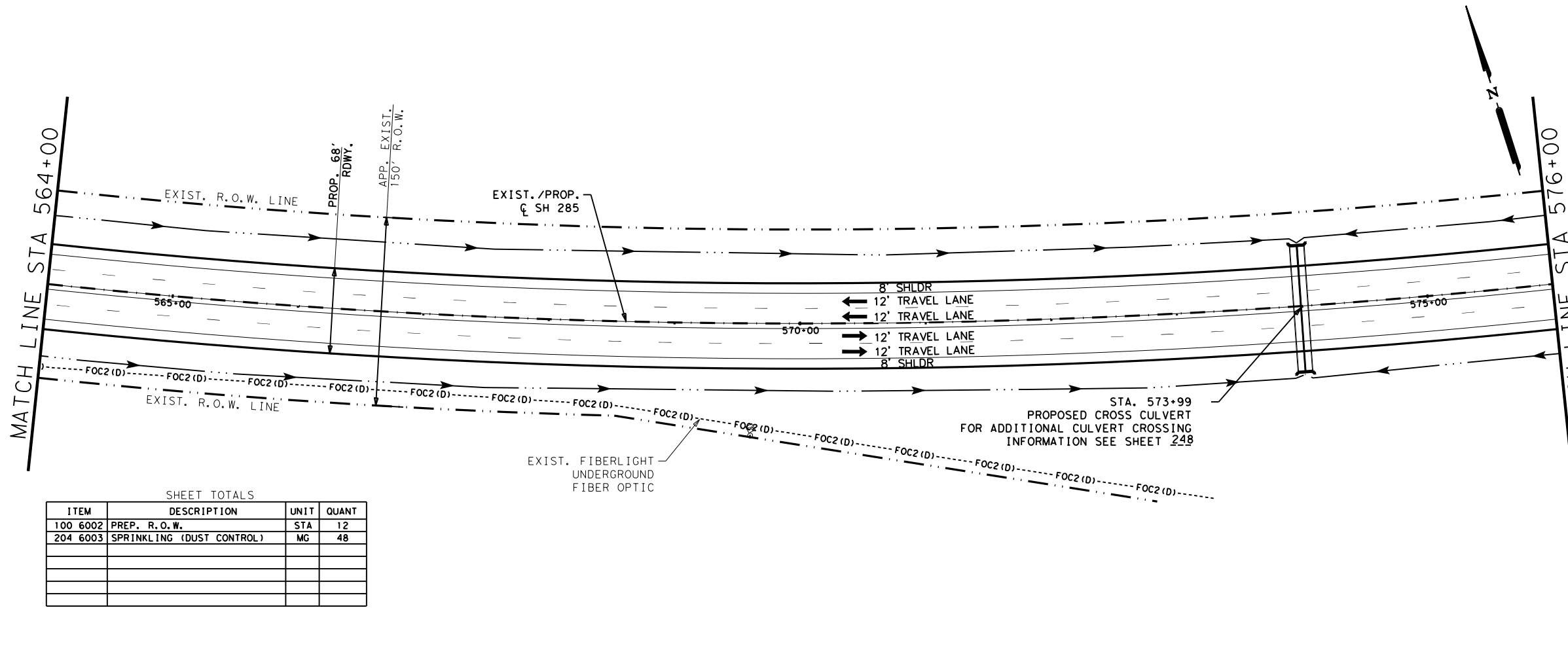
**SH 285
 PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 39 OF 44

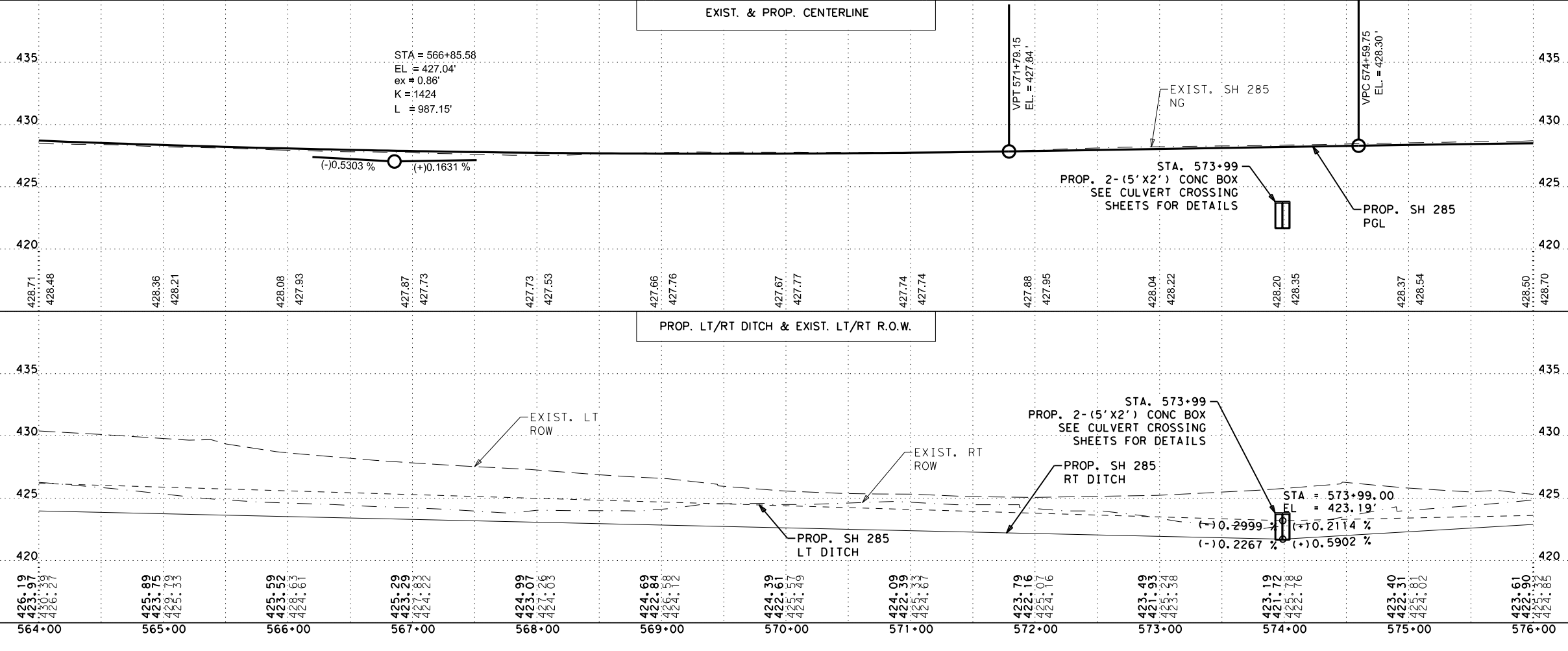
© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG		220	

DATE: 3/13/2024 4:57:46 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 5



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48



03.14.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PLAN AND PROFILE**

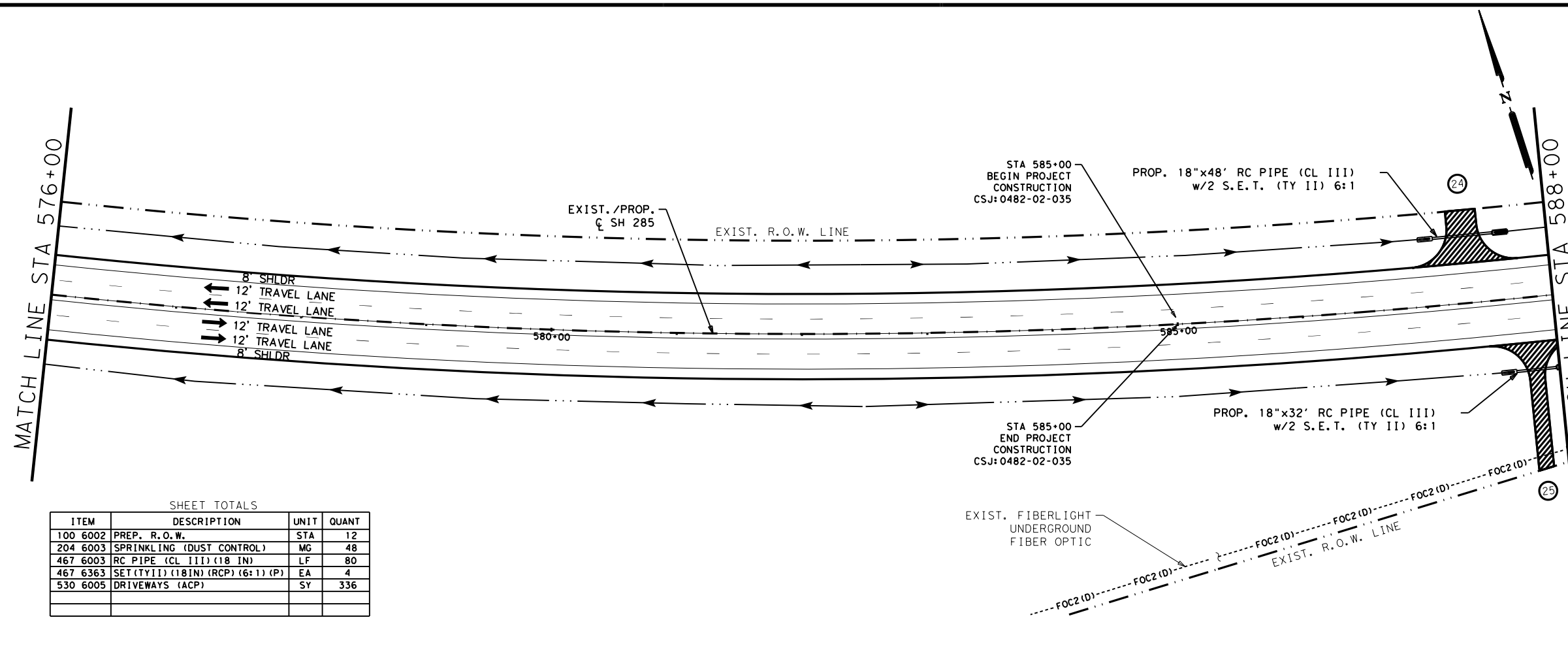
HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 40 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG		221	



DATE: 3/13/2024 4:57:49 PM
 FILE: D:\work\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 5

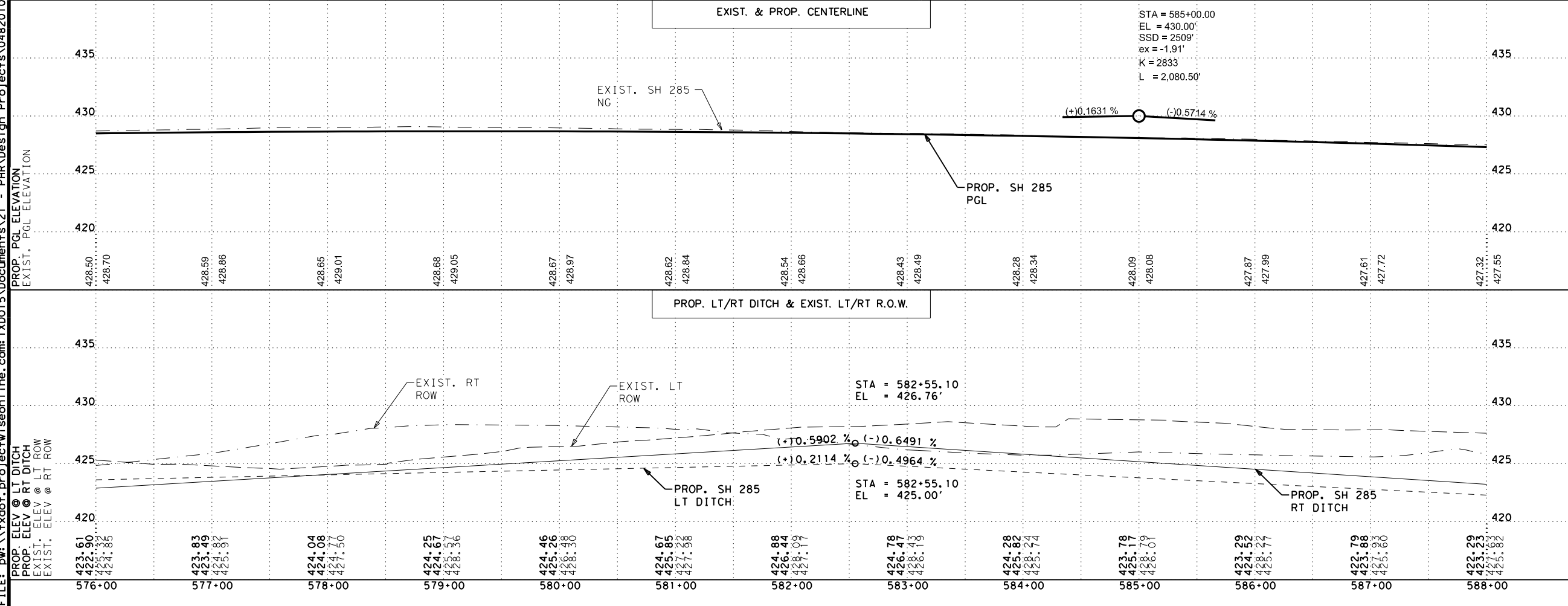


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48
467 6003	RC PIPE (CL III) (18 IN)	LF	80
467 6363	SET (TY II) (18 IN) (RCP) (6:1) (P)	EA	4
530 6005	DRIVEWAYS (ACP)	SY	336

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

Arlene Crystal Garcia

03.14.2024

Pharr District Central Design

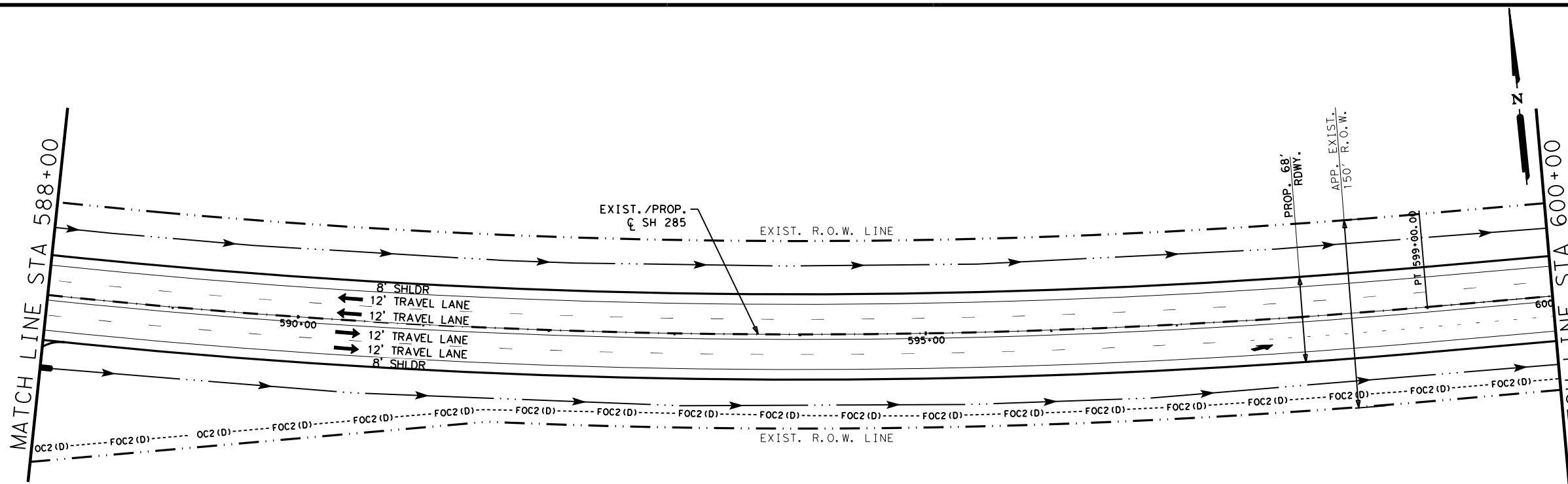
**SH 285
PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 41 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	222

DATE: 3/13/2024 4:57:53 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\Plan & Profile\SH 285 P&P Sheets 5

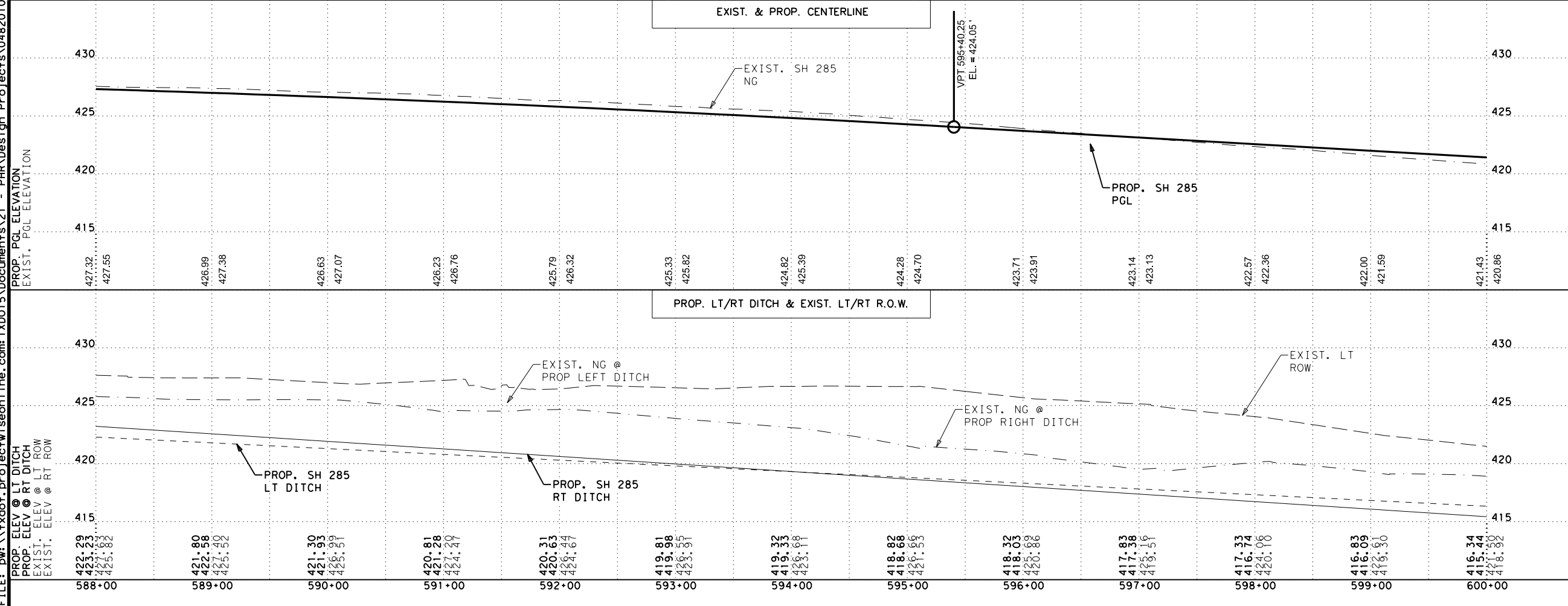


- ROADWAY LEGEND:**
- DRIVEWAY NUMBER
 - PROP. ASPHALT DRIVEWAY
 - PROP. MILL & OVERLAY (0"-2")
 - DIRECTION OF TRAFFIC FLOW
 - PROP. SAFETY END TREATMENT
 - R.O.W. LINE
 - PROP. DITCH FLOW LINE
 - TEST HOLE
- UTILITY LEGEND:**
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



- LEGEND:**
- PROP. PGL ELEVATION
 - EXIST. PGL ELEVATION
 - PROP. LT DITCH
 - PROP. RT DITCH
 - EXIST. ELEV @ LT ROW
 - EXIST. ELEV @ RT ROW



Arlene Crystal Garcia

03.14.2024
Pharr District Central Design



**SH 285
 PLAN AND PROFILE**

HORIZ: 1"=100'
 VERT: 1"=10'

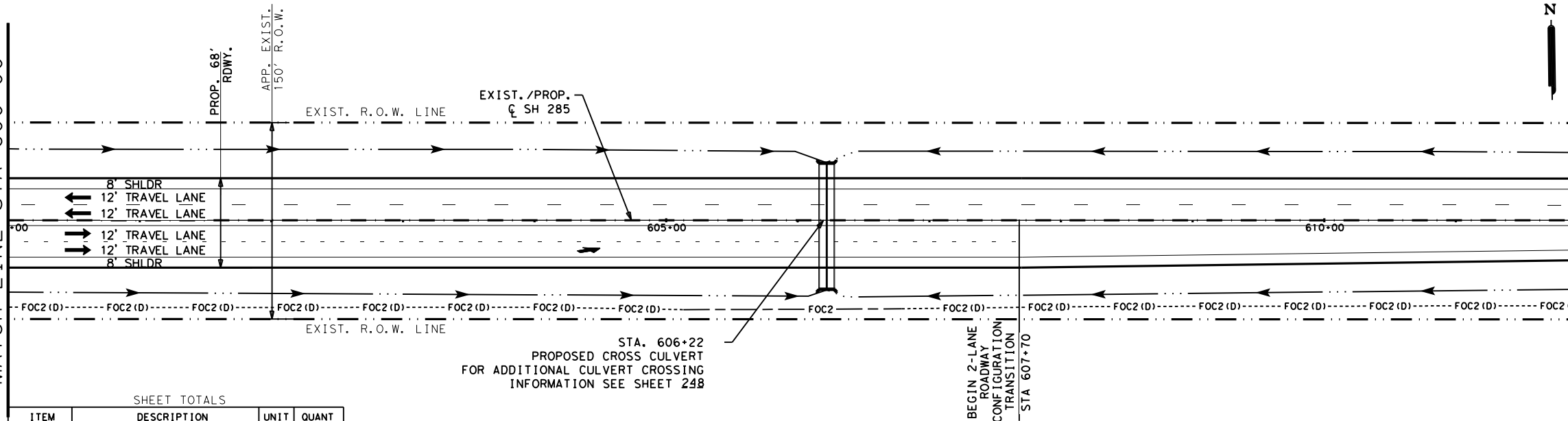
SHEET 42 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	223	

DATE: 3/13/2024 4:57:56 PM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 5

MATCH LINE STA 600+00

MATCH LINE STA 612+00

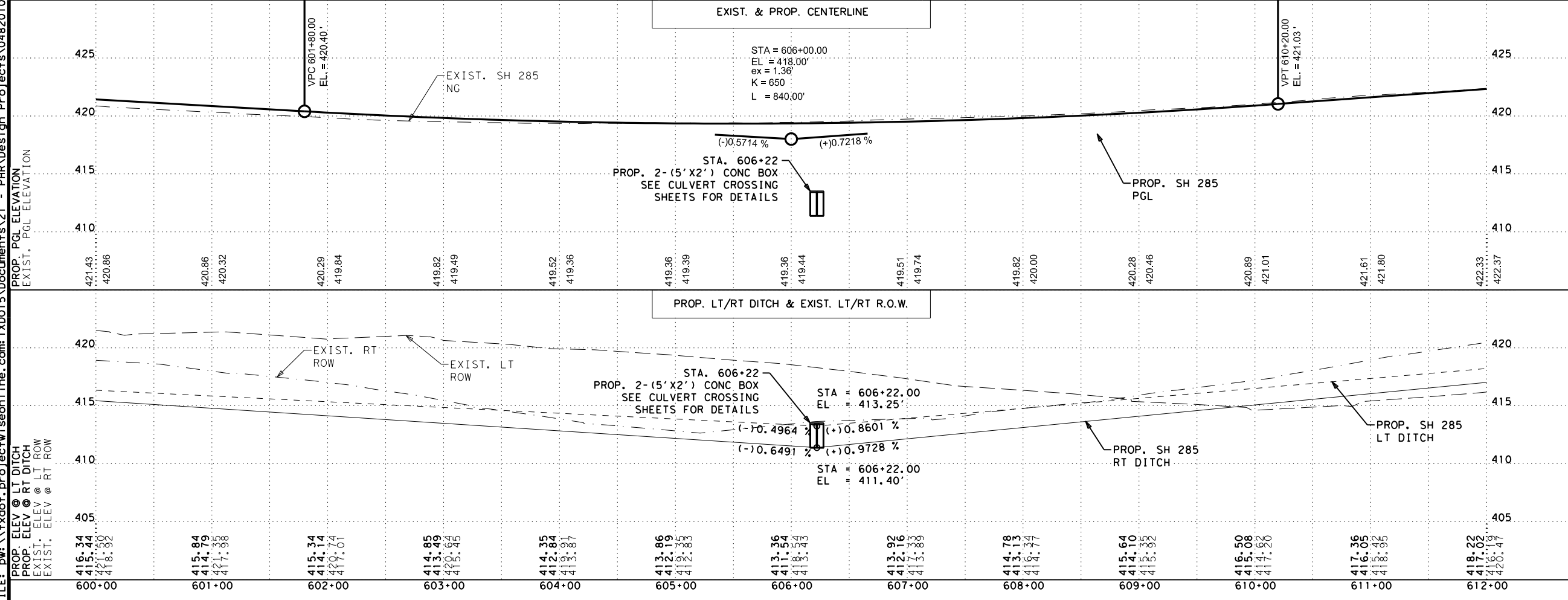


SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	12
204 6003	SPRINKLING (DUST CONTROL)	MG	48

- ROADWAY LEGEND:
- (*) DRIVEWAY NUMBER
 - [Hatched Box] PROP. ASPHALT DRIVEWAY
 - [Cross-hatched Box] PROP. MILL & OVERLAY (0"-2")
 - [Arrow] DIRECTION OF TRAFFIC FLOW
 - [Circle with X] PROP. SAFETY END TREATMENT
 - [Dashed Line] R.O.W. LINE
 - [Line with Arrow] PROP. DITCH FLOW LINE
 - [Circle with Center] TEST HOLE
- UTILITY LEGEND:
- OH ELECTRIC --- OE1 --- AEP TEXAS
 - UG ELECTRIC --- E1 --- AEP TEXAS
 - OH FIBER OPTIC --- OHFOC1 --- AT&T
 - OH FIBER OPTIC --- OHFOC2 --- FIBERLIGHT
 - UG FIBER OPTIC --- FOC1 --- AT&T
 - UG FIBER OPTIC --- FOC2 --- FIBERLIGHT
 - OH TELEPHONE --- OHT1 --- AT&T
 - TELEPHONE --- T1 --- AT&T
 - PIPELINE --- G1 --- NTU PIPELINE
 - PIPELINE --- G2 --- MESTENA OP.
 - PIPELINE --- G3 --- UNKNOWN

- NOTES:
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.



LEGEND:

- PROP. PGL ELEVATION
- EXIST. PGL ELEVATION
- PROP. LT DITCH
- PROP. RT DITCH
- EXIST. ELEV @ LT ROW
- EXIST. ELEV @ RT ROW

ARLENE CRYSTAL GARCIA
 147900
 PROFESSIONAL ENGINEER

Arlene Crystal Garcia

03.14.2024

Pharr District Central Design

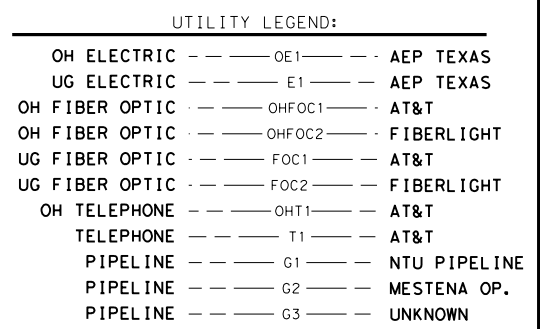
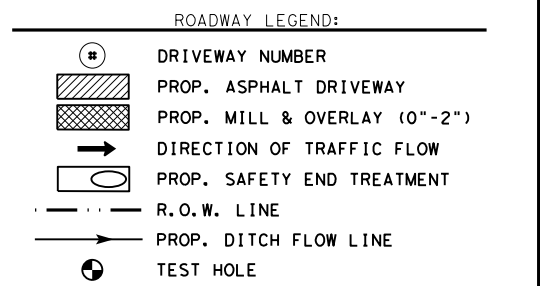
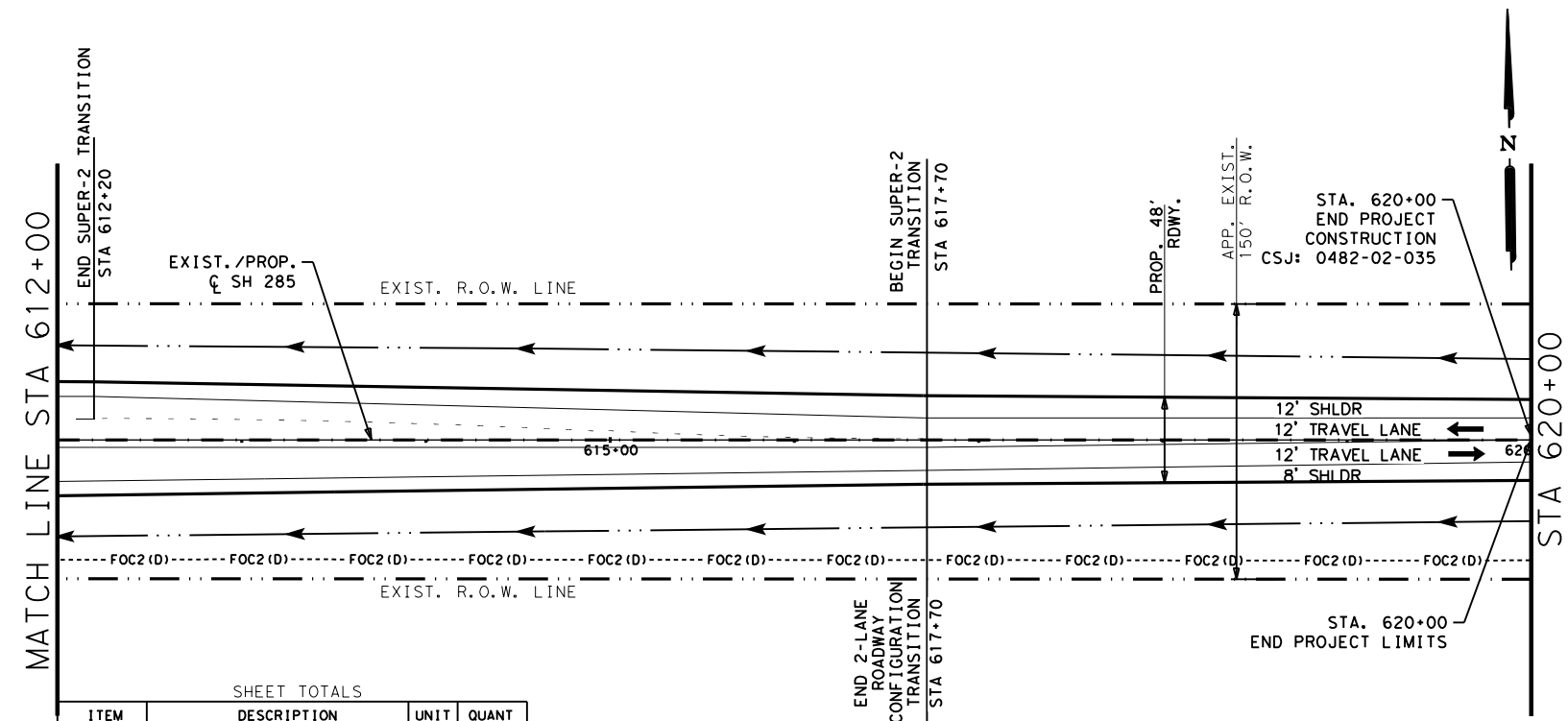
SH 285
 PLAN AND PROFILE

HORIZ: 1"=100'
 VERT: 1"=10'

SHEET 43 OF 44

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		224

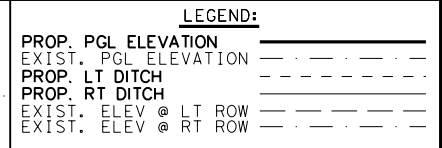
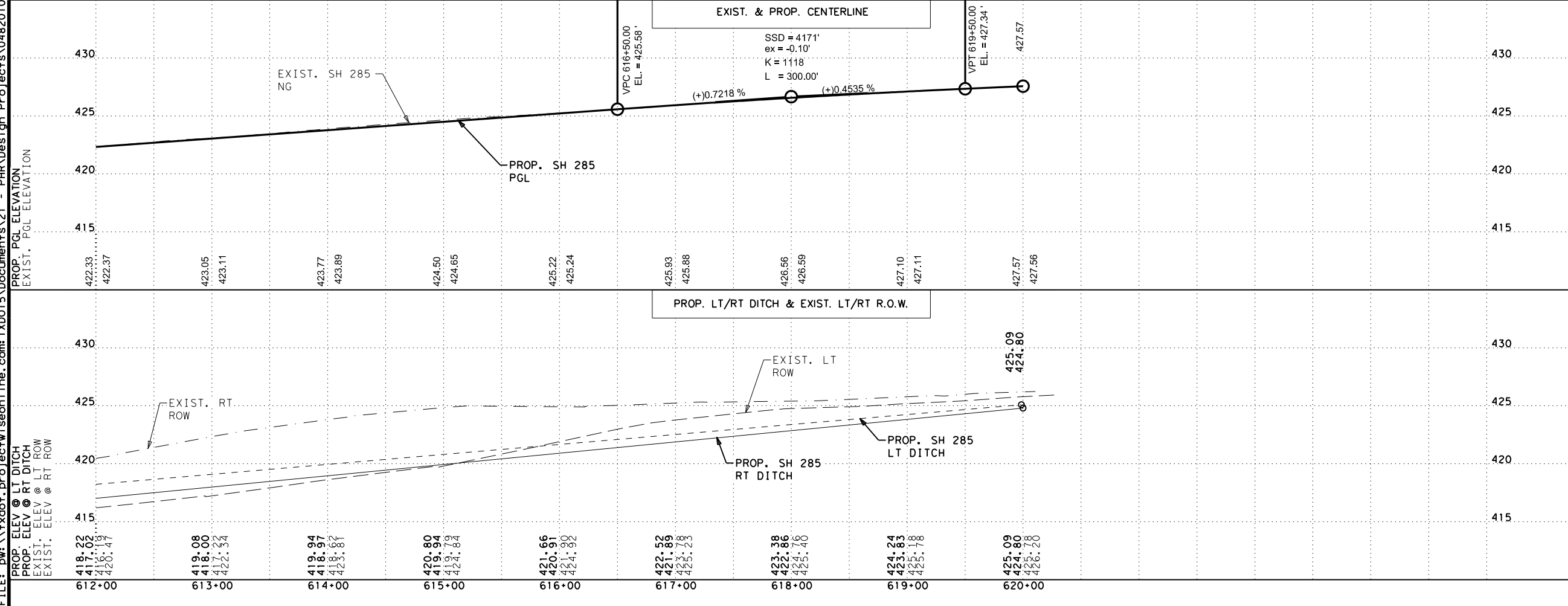
DATE: 3/13/2024 4:57:59 PM
 FILE: D:\work\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan & Profile\Plan & Profile\SH 285 P&P Sheets 5



- NOTES:**
- ALL STATIONS ARE BASED ON PROP. SH 285 ALIGNMENT. SEE "ROADWAY DATA SHEETS" FOR PROPOSED ALIGNMENT DATA.
 - CONTRACTOR SHALL NOTIFY PROPERTY OWNERS PRIOR TO RECONSTRUCTION OF EXISTING DRIVEWAYS. ACCESS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. REFER TO DRIVEWAY SUMMARY SHEET FOR DRIVEWAY QUANTITIES.
 - ALL UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
100 6002	PREP. R.O.W.	STA	8
204 6003	SPRINKLING (DUST CONTROL)	MG	32



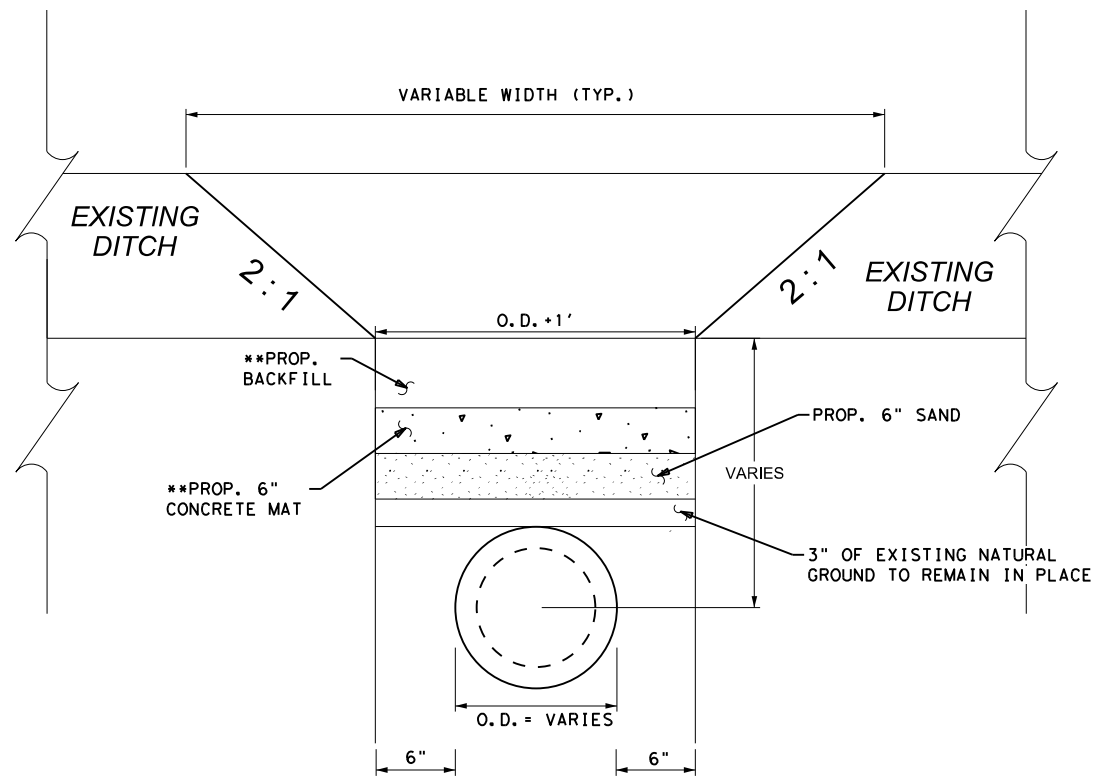
03.14.2024
Pharr District Central Design
 Texas Department of Transportation

**SH 285
 PLAN AND PROFILE**

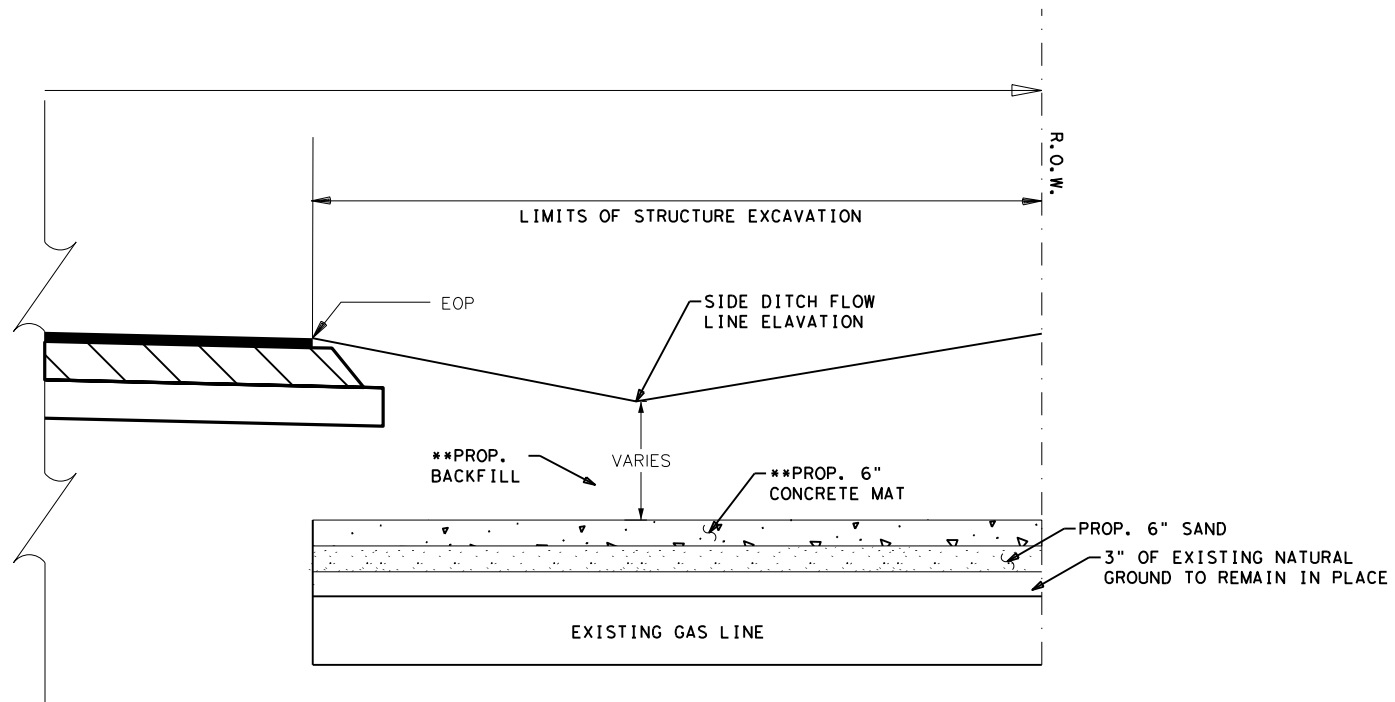
HORIZ: 1"=100'
 VERT: 1"=10'

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	225	

DATE: 2/16/2024 4:16:43 PM
 FILE: P:\txdot\projectwiseonline.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\6. Utilities\SH_285\UtilityDetail.dgn



CONCRETE MAT & BACKFILL DETAIL
 N. T. S.



CONCRETE MAT & BACKFILL DETAIL - LIMITS
 N. T. S.

NOTES
 CONCRETE TO BE PAID BY ITEM 432

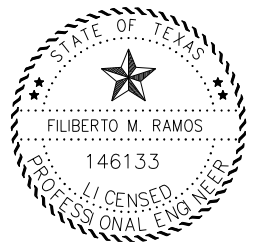
IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY OWNER TO VERIFY HORIZONTAL AND VERTICAL INFORMATION. ANY DAMAGES CAUSED BY THE CONTRACTOR SHALL BE REPAID AND/OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

TXDOT AND UTILITY OWNER PERSONNEL SHALL BE PRESENT DURING OPERATION.

BACKFILL TO BE TAMPED AND JETTED

** PROP. BACKFILL TO BE PAID SUBSIDIARY TO PERTINENT BID ITEMS PER ITEM 400 SPECIFICATIONS.

REINFORCEMENT NO. 4 BARS @ 12" CENTERS EACH WAY OR EQUIVALENT WIRE MESH.



Filiberto M. Ramos
 02/16/2024

Pharr District Central Design



**SH 285
 CONCRETE MAT FOR
 UTILITY CROSSING**

SHEET 1 OF 1


© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	226	

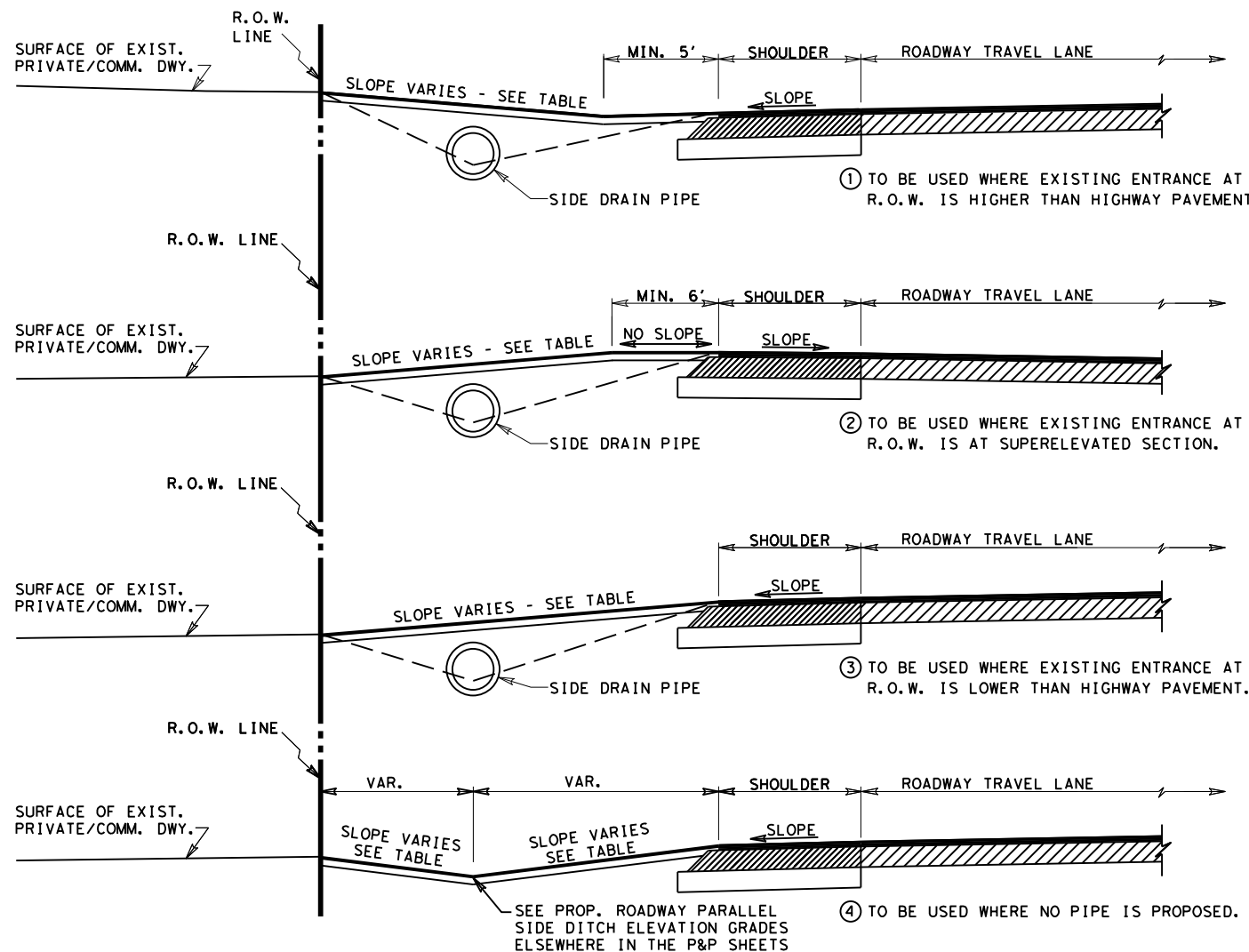
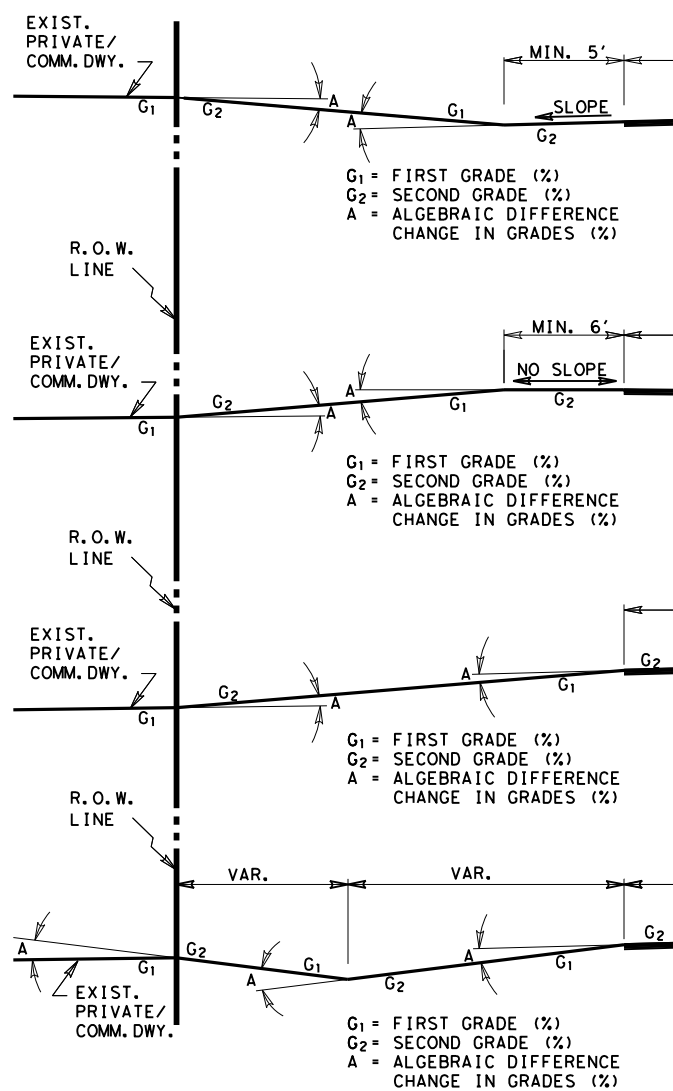
SUMMARY OF PROP. DRIVEWAY, PRIVATE
& PUBLIC DRIVEWAYS (ITEM 530)

DRIVEWAY ID NUMBER	(ALIGNMENT) STATION		EXISTING DRIVEWAY WIDTH	PROP. WIDTH @ EDGE OF PAVEMENT	PROP. WIDTH @ ROW LINE	PRIVATE & PUBLIC DRIVEWAY AREA (SY)		PROP. RADIUS		* ITEM REMOVE ASPHALT DRVWY (SY)	* ITEM REMOVE GRAVEL CALICHE (DRWY) (SY)
	CSJ: 0482-01-036	O/S				FT.	FT.	FT.	PB-1 (ITEM 530-6005)	CONCRETE (ITEM 530-6004)	LEFT
1	105+22.94	RT	45	72	12	111		30	30		144
2	107+47.51	LT	14	72	12	133		30	30		82
3	107+78.11	RT	36	72	12	111		30	30		153
4	109+57.51	RT	55	72	12	111		30	30		128
5	129+84.53	LT	68	72	12	111		30	30	122	
6	168+97.20	RT	85	78	18	145		30	30	270	
7	169+29.10	LT	55	72	12	111		30	30	113	
8	191+21.93	RT	53	74	14	126		30	30		126
9	213+53.50	LT	77	78	18	130		30	30	207	
10	213+94.31	RT	11	72	12	97		30	30		
11	242+11.40	RT	16	72	12	96		30	30		
12	272+05.62	LT	48	76	12	109		30	30		
13	321+32.13	LT	53	72	12	111		30	30	97	
14	325+49	LT	82	84	24	178		30	30		283
15	325+80.57	RT	93	84	24	180		30	30		278
16	406+76	RT	78	84	24	146		30	30	202	
17	433+82.7	LT	113	84	24	158		30	30		238
18	433+93.63	RT	13	72	12	95		30	30		69
19	467+59.67	RT	98	78	18	145		30	30		209
20	479+99.77	RT	46	72	12	111		30	30	88	
21	483+39.33	LT	50	78	18	145		30	30	151	
22	542+72.42	LT	14	72	12	101		30	30		66
23	543+47.78	RT	115	84	24	147		30	30	263	
24	587+32.60	LT		84	24	157		30	30		
25	587+81.47	RT		72	12	179		30	30		
TOTALS						3244				1513	1632

*FOR CONTRACTORS INFORMATION ONLY. NO PAY ITEM.

DATE: 2/16/2024 4:57:38 PM
FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\3. Roadway\SH_285 DRIVEWAY TABLE.dgn

Pharr District Central Design				
				
SH 285 DRIVEWAY TABLE				
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		227



TYPICAL ENTRANCE PROFILE FOR DRIVEWAYS W/OUT C&G

PROPOSED DRIVEWAY SLOPE TABLE
COMMERCIAL DRIVEWAYS @ 12:1 MAX.
RESIDENTIAL DRIVEWAYS @ 8:1 MAX.

PROP. DWY ALGEBRAIC DIFFERENCE TABLE
COMMERCIAL DRIVEWAYS @ $A = 6\%$ DESIRABLE
RESIDENTIAL DRIVEWAYS @ $A = 8\%$ DESIRABLE
FORMULA, $A = G_2 - G_1$

NOTES:

ALL ENTRANCES CONSTRUCTED ON THIS PROJECT ARE SUBJECT TO CONCURRENCE WITH EXISTING GOVERNING REGULATIONS AS SET OUT BY THE STATE - TEXAS TRANSPORTATION COMMISSION.

ENTRANCE'S BASE AND SURFACING MAY BE EXTENDED BEYOND R.O.W. LINE AS REQUIRED TO MEET EXISTING DRIVEWAY GRADE IN A SATISFACTORY MANNER OF WHICH NO STEEPER THAN 12:1 FOR COMMERCIAL DRIVEWAY AND 8:1 FOR RESIDENTIAL DRIVEWAY SLOPE WILL BE CONSTRUCTED.

ALL FLEXIBLE BASE USED FOR PRIVATE DRIVES & COMMERCIAL DRIVES WILL NOT REQUIRE LIME TREATMENT.

EXACT LOCATIONS, DIMENSIONS, AND TYPE TO BE ESTABLISHED DURING CONSTRUCTION BY THE ENGINEER.

PROP. WIDTH OF DRIVEWAYS TO MATCH EXISTING WIDTH AT R.O.W. LINE.

114 #/SY ACP (COMPACTED) IS EQUAL TO 1 IN. DEPTH, 171 #/SY ACP (COMPACTED) IS EQUAL TO 1 1/2 IN. DEPTH.

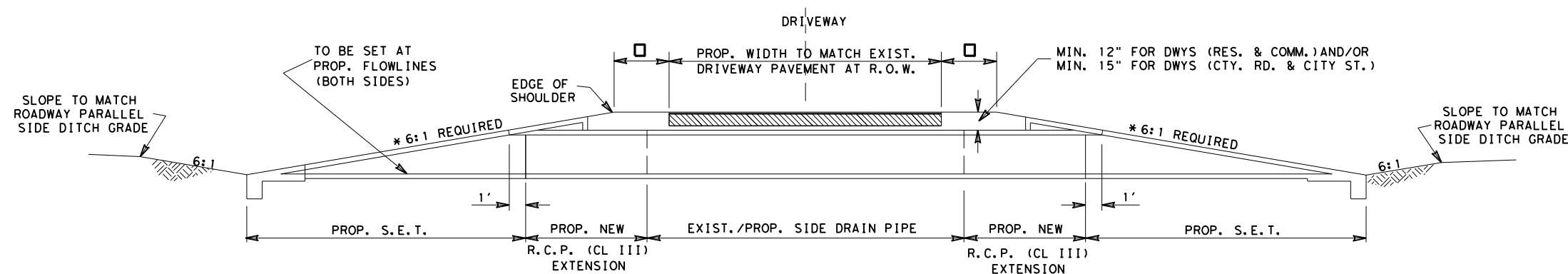
SIDE DRAIN PIPES TO BE INSTALLED WHERE ROADWAY DITCH DRAINAGE IS NECESSARY, AS INDICATED ON PLANS AND/OR AS DIRECTED BY THE ENGINEER.

SIDE DRAIN PIPES TO BE INSTALLED WITH A MINIMUM OF 12" COVER WITH PROPOSED RESIDENTIAL & COMMERCIAL DRIVEWAY MATERIAL OR 15" COVER WITH PROPOSED COUNTY ROAD & CITY STREET ROADWAY MATERIAL.

AVERAGE DRIVEWAY DIMENSIONS SHOWN ON TABLE OF DRIVEWAYS (ELSEWHERE IN PLANS) ARE FOR ESTIMATING PURPOSES ONLY. ACTUAL DRIVEWAY DIMENSIONS MAY BE CHANGED BY THE ENGINEER BASED ON EXISTING FIELD CONDITIONS.

THE RATE OF PRIME COAT SHALL BE 0.10 GAL/SY FOR PRIVATE AND/OR COMMERCIAL DRIVEWAYS AND 0.20 GAL/SY FOR PUBLIC DRIVEWAYS (COUNTY ROADS AND/OR CITY STREETS).

TYPICALLY A CHANGE IN GRADE OF THREE PERCENT (3%) OR LESS AND A DISTANCE BETWEEN CHANGES IN GRADE OF AT LEAST ELEVEN FEET (11') ACCOMMODATES MOST VEHICLES. HOWEVER, LITERATURE SUGGESTS THAT A SIX PERCENT (6%) TO EIGHT PERCENT (8%) CHANGE IN GRADE MAY OPERATE EFFECTIVELY. INDIVIDUAL SITE CONDITIONS SHOULD BE EVALUATED TO ACCOMMODATE THE VEHICLE FLEET USING THE DRIVEWAY.



- - 1' MIN. ON DRIVEWAYS (RES. & COMM.)
- 2' MIN. ON DRIVEWAYS (COUNTY RD. & CITY ST.)
- * - 6:1 SLOPE REQUIRED

© TxDOT 2020 PHARR DISTRICT STANDARD

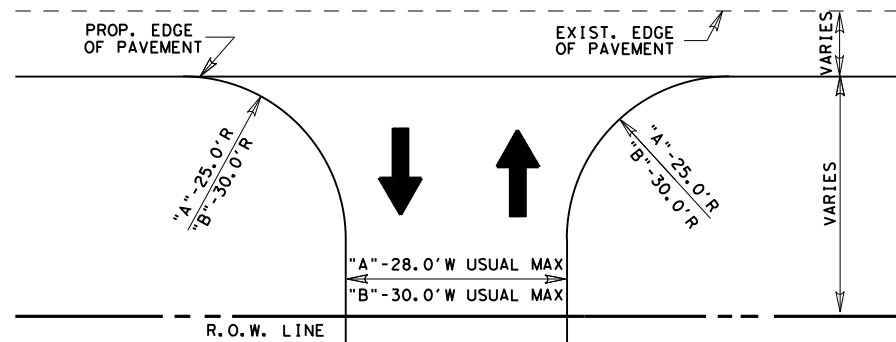


DRIVEWAY PROFILE DETAILS

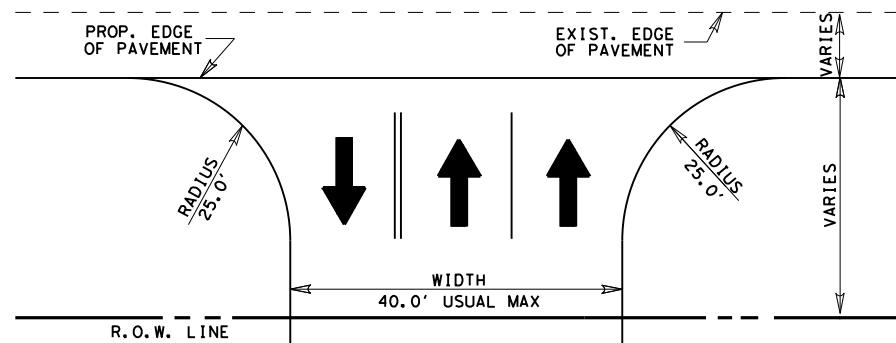
REV. 3/2020 DRIVEWAY1.DGN

FED. RD. DIV. NO.	STATE AID PROJECT NO.	FILE NO.	SHEET NO.
6			228
STATE	STATE DIST. NO.	COUNTY	CONT. SECT. JOB HIGHWAY NO.
TEXAS	21	JIM HOGG	0482 01 036, etc. SH 285

DESIGNS FOR TWO-WAY COMMERCIAL DRIVEWAYS

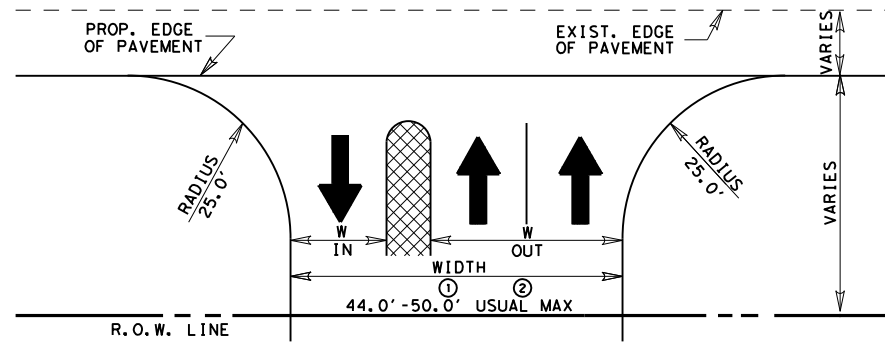


"A"- ONE ENTRY LANE AND ONE EXIT LANE, FEWER THAN 4 LARGE VEHICLES PER HOUR
 "B"- ONE ENTRY LANE AND ONE EXIT LANE, 4 OR MORE SINGLE UNIT VEHICLES^① PER HOUR
 ① - DRIVEWAY DESIGNS FOR LARGER VEHICLES WILL BE CONSIDERED ON A CASE BY CASE BASIS

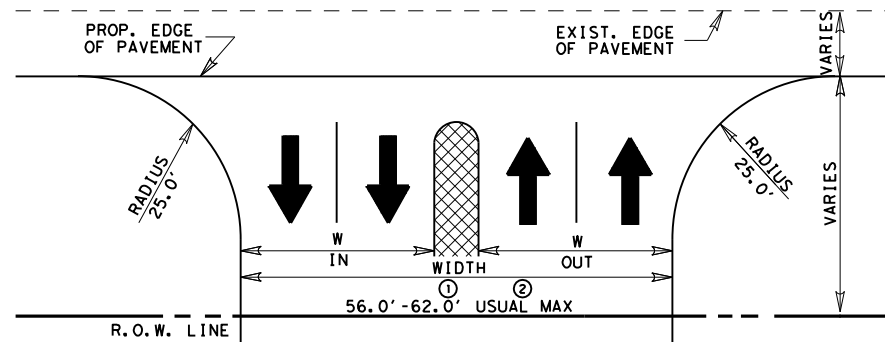


ONE ENTRY LANE AND TWO EXIT LANES (WITHOUT DIVIDERS)

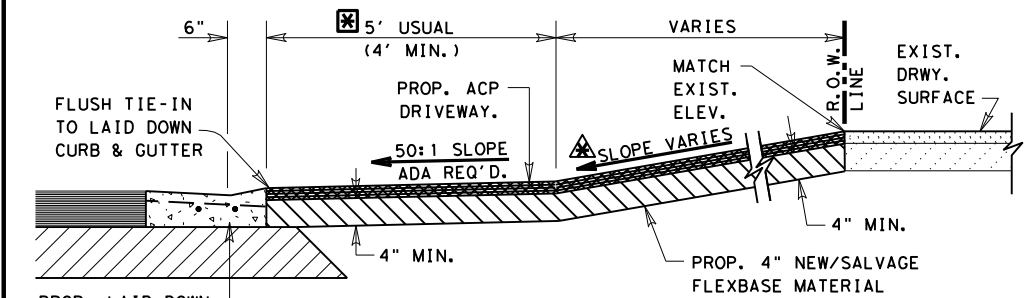
DESIGNS FOR TWO-WAY COMMERCIAL DRIVEWAYS



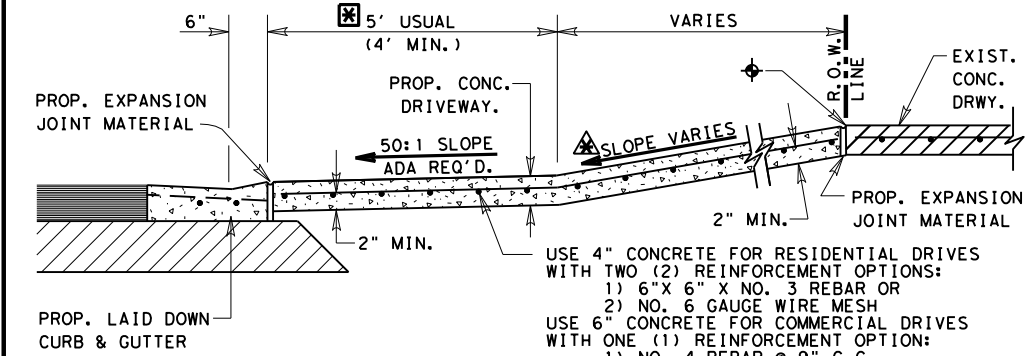
① - 4.0' WIDE DIVIDER, FACE-TO-FACE CURBS
 ② - 10.0' WIDE DIVIDER, FACE-TO-FACE CURBS
 ONE ENTRY LANE AND TWO EXIT LANES (WITH A DIVIDER)



① - 4.0' WIDE DIVIDER, FACE-TO-FACE CURBS
 ② - 10.0' WIDE DIVIDER, FACE-TO-FACE CURBS
 TWO ENTRY LANES AND TWO EXIT LANES (WITH A DIVIDER)



TYPICAL ASPH. CONC. PVM'T DRIVEWAY SECTION
 N.T.S.



TYPICAL CONCRETE DRIVEWAY SECTION
 N.T.S.

CONCRETE SHALL BE SAW CUT TO THE LIMITS OF REMOVAL WHERE APPLICABLE.

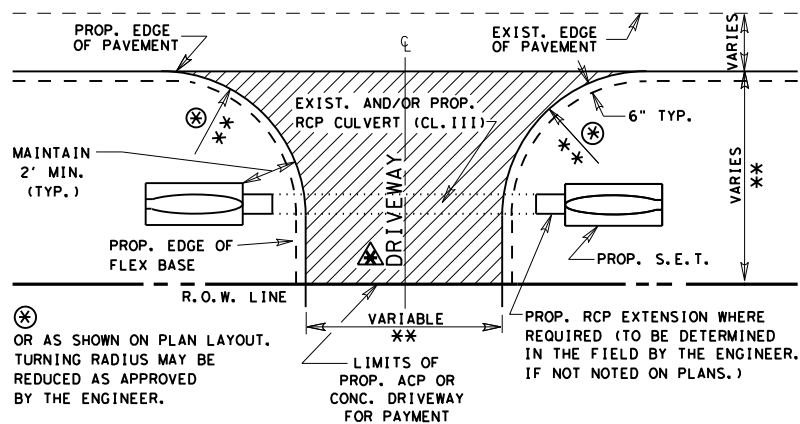
PROF./FUTURE SIDEWALK CROSSING LOCATION UNLESS SHOWN ELSEWHERE ON P&P SHEETS. SEE P&P SHEETS FOR PROF. SIDEWALK LOCATION IF SIDEWALKS ARE INCLUDED AS PART OF PROJECT. REFER TO STATE STANDARDS - PEDESTRIAN FACILITIES - FOR ADDITIONAL REQUIREMENTS.

ENTRANCE'S BASE AND SURFACING MAY BE EXTENDED BEYOND R.O.W. LINE AS REQUIRED TO MEET EXISTING GRADE IN A SATISFACTORY MANNER OF WHICH NO STEEPER THAN 12:1 FOR COMMERCIAL DRIVEWAY AND 8:1 FOR RESIDENTIAL DRIVEWAY SLOPE WILL BE CONSTRUCTED.

PROP. DWY ALGEBRAIC DIFFERENCE TABLE	
COMMERCIAL DRIVEWAYS @ A = 6% MAX.	
RESIDENTIAL DRIVEWAYS @ A = 8% MAX.	

PROPOSED DRIVEWAY SLOPE TABLE	
COMMERCIAL DRIVEWAYS @ 12:1 MAX.	
RESIDENTIAL DRIVEWAYS @ 8:1 MAX.	

PRIVATE AND COMMERCIAL DRIVES WITHOUT CURB & GUTTER

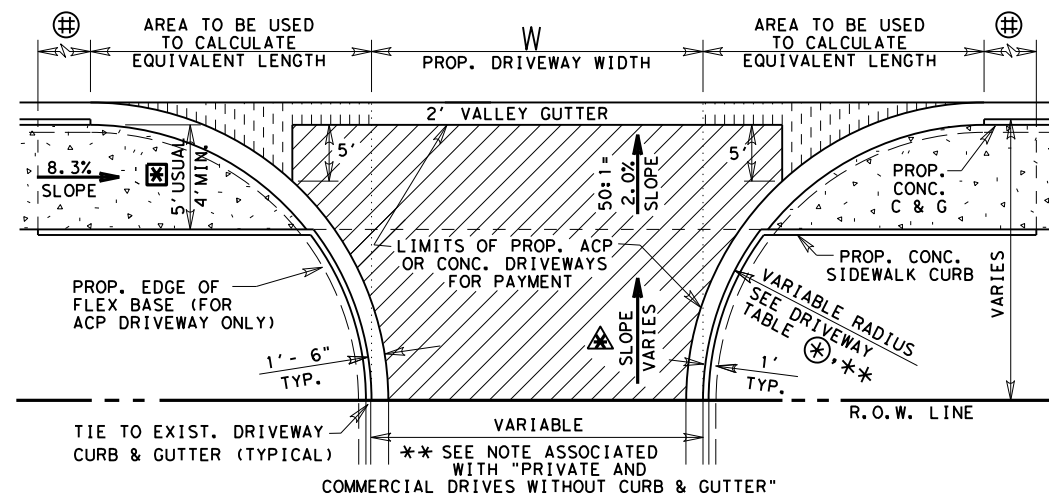


PLAN OF PRIVATE AND COMMERCIAL DRIVES

** FOR PRIVATE RESIDENTIAL DRIVES, TRY TO MATCH EXISTING WITH A MINIMUM WIDTH OF 12 FT. AND A MAXIMUM WIDTH OF 24 FT. WITH 15 FT. USUAL RADIUS. FOR COMMERCIAL DRIVES, USE ABOVE COMMERCIAL DRIVEWAY DETAILS.

SEE TYPICAL DRIVEWAY SECTIONS NOTES FOR DRIVEWAY SLOPE CRITERIA.

PRIVATE AND COMMERCIAL DRIVES WITH CURB & GUTTER



PLAN OF PRIVATE AND COMMERCIAL DRIVES

SEE P&P SHEETS FOR LOCATIONS OF DRIVES
 N.T.S.

PROF./FUTURE CONC. SIDEWALK LOCATION UNLESS SHOWN ELSEWHERE ON P&P SHEETS. REFER TO STATE STANDARDS - PEDESTRIAN FACILITIES - FOR ADDITIONAL REQUIREMENTS.

LIMITS OF SLOPE FOR PROP. CONC. CURB BASED ON 8.3% SLOPE FOR SIDEWALK.

SEE TYPICAL DRIVEWAY SECTIONS NOTES FOR DRIVEWAY SLOPE CRITERIA.

LF EQUIVALENT TABLE FOR PAYMENT LIMITS OF 2' VALLEY GUTTER

LF OF VALLEY GUTTER = W + X1 + X2		
WHERE X1 AND X2 MAY VARY DEPENDING ON RADIUS		
Prop. Driveway Radius	X1 or X2 (Sq Ft Area / 2')	Equivalent LF Length
5'	1	
8'	2	
10'	4	
12'	6	
15'	9	
18'	12	
20'	15	
22'	18	
25'	24	
28'	30	
30'	34	

SEE DRIVEWAY TABLE FOR LIMITS OF LAID DOWN CURB TO BE PAID FOR AS CURB AND GUTTER

DRIVEWAY TYPES

TY PB-1
 EXIST. PRIVATE OR COMMERCIAL DRIVEWAYS TO BE CONSTRUCTED AS SHOWN WITH 4" NEW AND/OR SALVAGE FLEX. BASE, PRIMED AND SURFACED WITH 171#/SY ACP. (HMA-D PG 64-22 SAC B MEETING ITEM 340)

CONCRETE (RESIDENTIAL)
 EXIST. PRIVATE DRIVEWAYS TO BE CONSTRUCTED AS SHOWN WITH 4" CONCRETE. TO BE PAID FOR BY THE SQ. YD.

CONCRETE (COMMERCIAL)
 EXIST. BUSINESS DRIVEWAYS TO BE CONSTRUCTED AS SHOWN WITH 6" CONCRETE. TO BE PAID FOR BY THE SQ. YD.

© TxDOT 2021

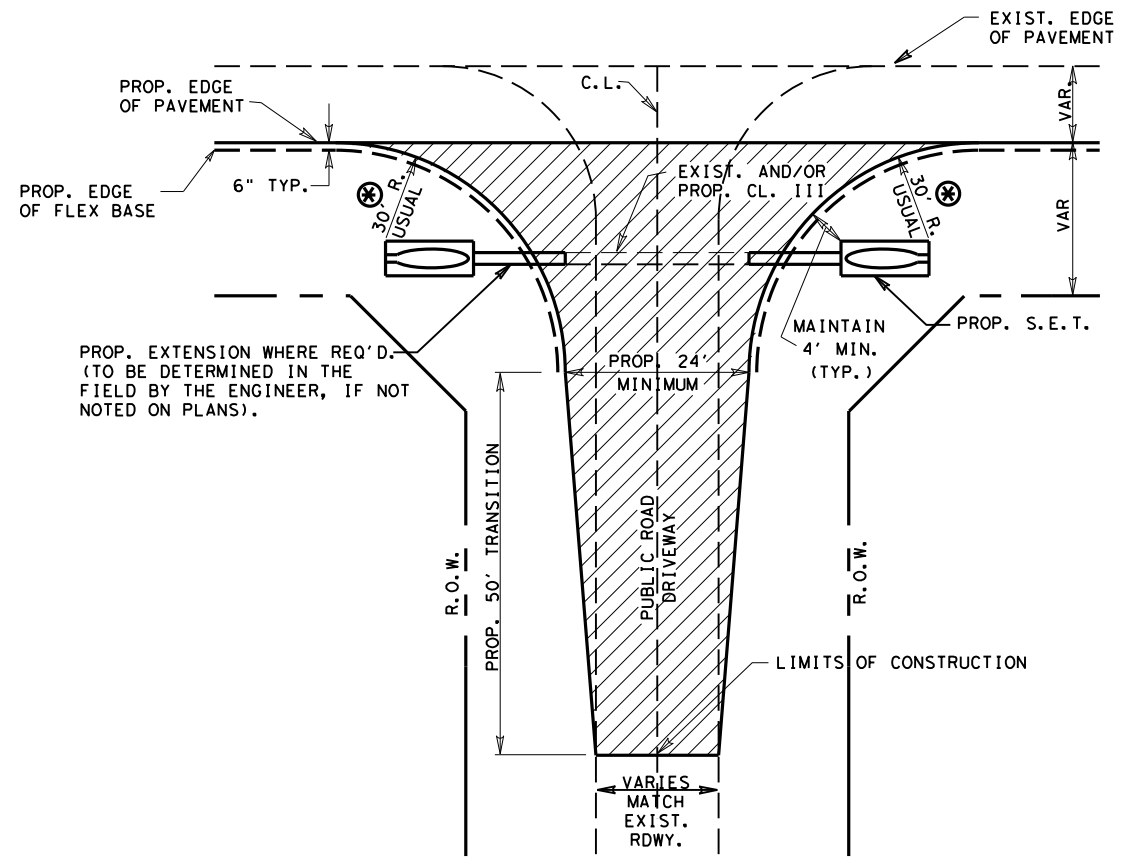
PHARR DISTRICT STANDARD

TEXAS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
 PRIVATE
 (RESIDENTIAL-COMMERCIAL)

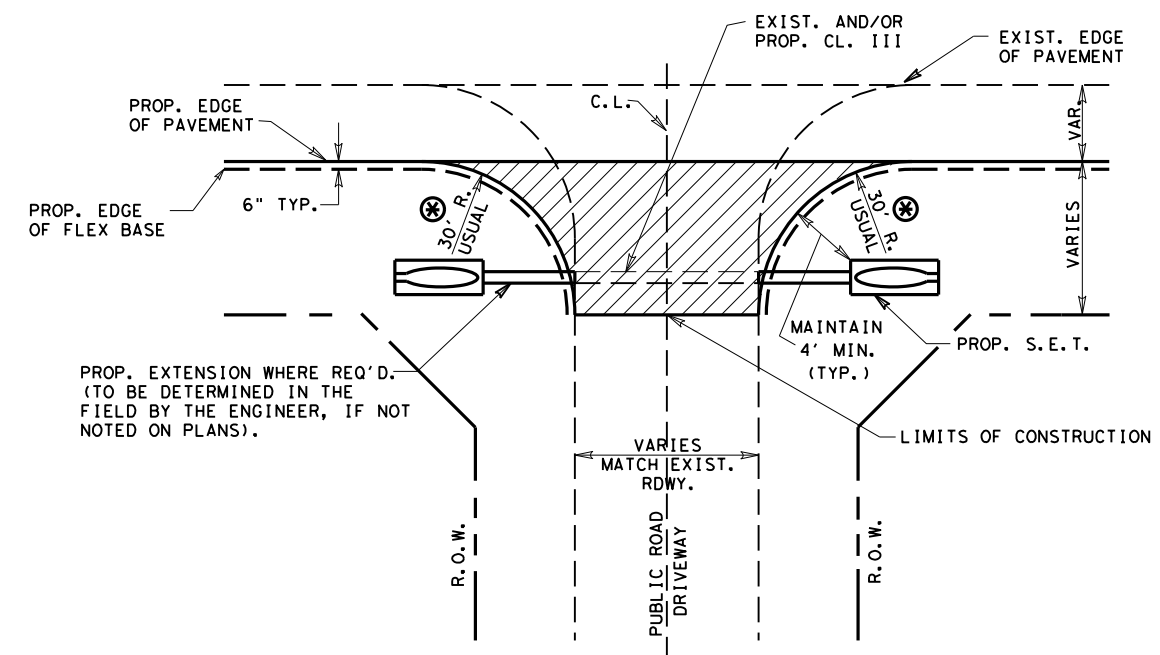
REV. 08/22

DRIVEWAY2.DGN

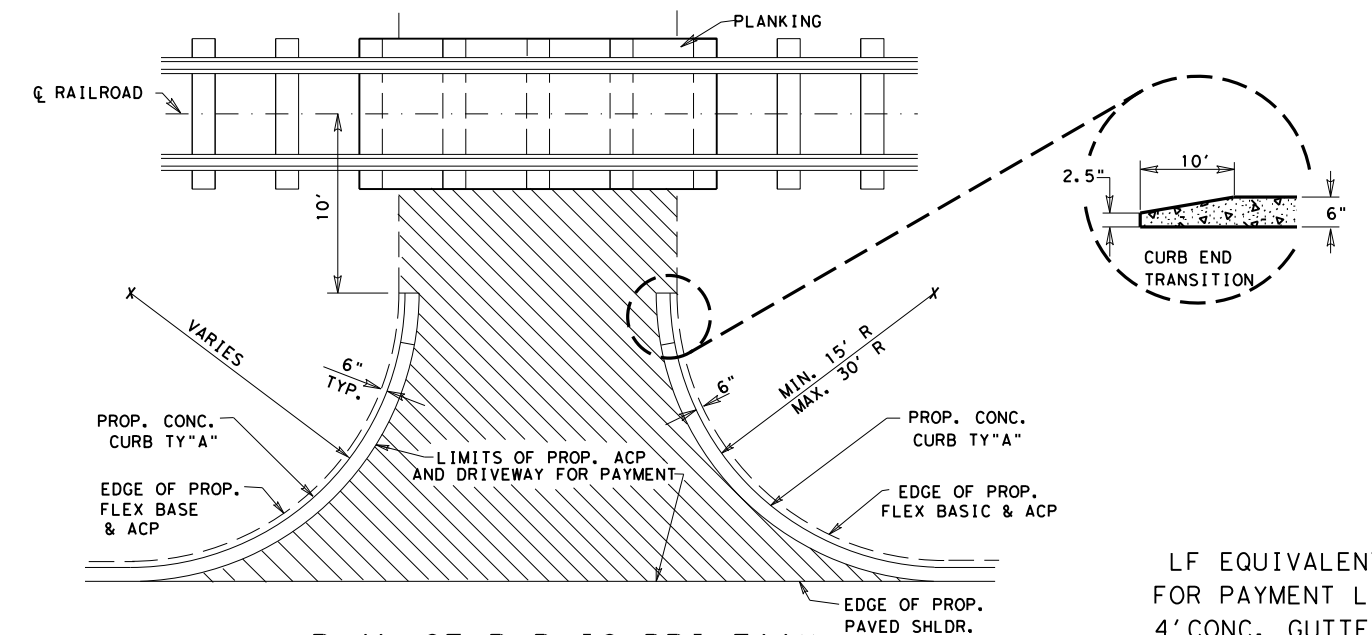
FED. RD. DIV. NO.	PROJECT NO.	FILE NO.	SHEET NO.			
6			229			
STATE	STATE DIST. NO.	COUNTY	CONT.	SECT.	JOB	HIGHWAY NO.
TEXAS	21	JIM HOGG	0482	01	036, etc.	SH 285



TYPICAL DETAIL
(WHEN EXIST. ROADWAY WIDTH LESS THAN 24'.)

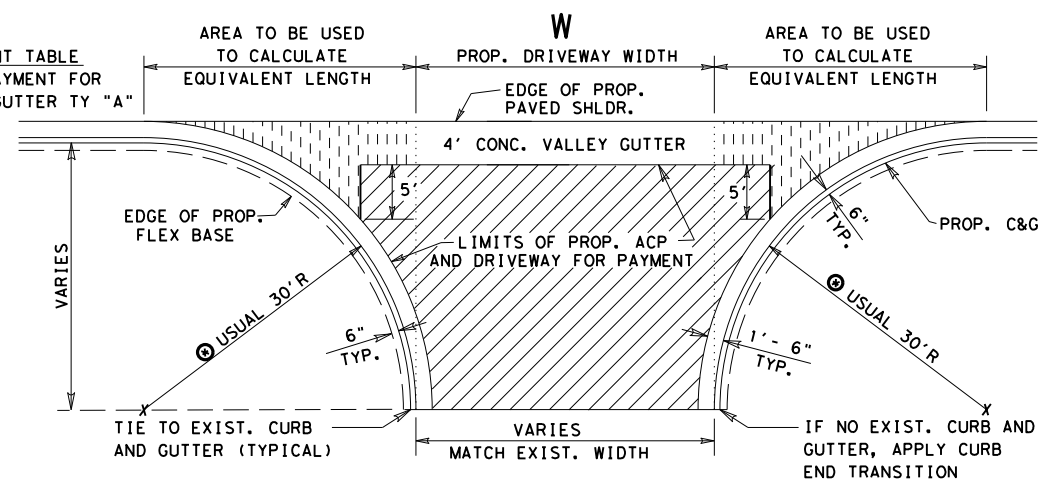


TYPICAL DETAIL
(WHEN EXIST. ROADWAY WIDTH EQUAL TO OR GREATER THAN 24'.)



PLAN OF PUBLIC DRIVEWAY ADJACENT TO R.R. CROSSING

SEE LF EQUIVALENT TABLE FOR LIMITS OF PAYMENT FOR PROP. 4' CONC. GUTTER TY "A" WHERE REQUIRED



PLAN OF PUBLIC DRIVEWAY

LF EQUIVALENT TABLE FOR PAYMENT LIMITS OF 4' CONC. GUTTER TY. "A"

LF OF VALLEY GUTTER= W + X1 + X2

WHERE X1 AND X2 MAY VARY DEPENDING ON RADIUS

Prop. Driveway Radius	X1 or X2 (Sq Ft Area / 4')
10	3
15	7
20	12
25	19
30	27
35	37
40	48
45	61
50	75
55	91
60	109
65	127
70	148
75	170

GENERAL NOTES:

- AVERAGE DIMENSIONS SHOWN ON TABLE OF DRIVEWAYS ARE FOR ESTIMATING PURPOSES ONLY.
- LOCATIONS LISTED ON THE TABLE ARE APPROXIMATE, EXACT LOCATIONS, DIMENSIONS, AND TYPE TO BE ESTABLISHED DURING CONSTRUCTION BY THE ENGINEER AS REQUIRED.
- ⊗ SEE DRIVEWAY TABLE, TURNING RADIUS MAY BE REDUCED AS APPROVED BY THE ENGINEER.
- SEE TABLE OF DRIVEWAYS FOR TOTAL LENGTH OF PROP. 4' CONC. VALLEY GUTTER FOR EACH LOCATION.

TY PBS1

EXIST. UNPAVED PUBLIC DRIVEWAYS TO BE CONSTRUCTED AS SHOWN WITH 12" LIME TREAT. SUBGRADE, 8" FLEX. BASE 1% LIME, THEN PRIMED AND SURFACED WITH 171#/SY ACP. (HMA-D PG 64-22 SAC B MEETING ITEM 340)

TY PBS2

EXIST. DRIVEWAY TO BE CONSTRUCTED SAME AS PROPOSED ROADWAY.

© TxDOT 2019 PHARR DISTRICT STANDARD

TEXAS DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS PUBLIC (COUNTY ROAD-CITY STREET)

REV. 8/22 DRIVEWAY3.DGN

FED. RD. DIV. NO.	STATE AID PROJECT NO.	FILE NO.	SHEET NO.
6			230
STATE	COUNTY	CONT.	SECT.
TEXAS	JIM HOGG	0482	01
JOB			HIGHWAY NO.
036, etc.			SH 285

DATE: 2/12/2024 8:08:38 AM
FILE: P:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 DRAINAGE COVER SHEET.dgn

DRAINAGE COVER SHEET

Pharr District Central Design



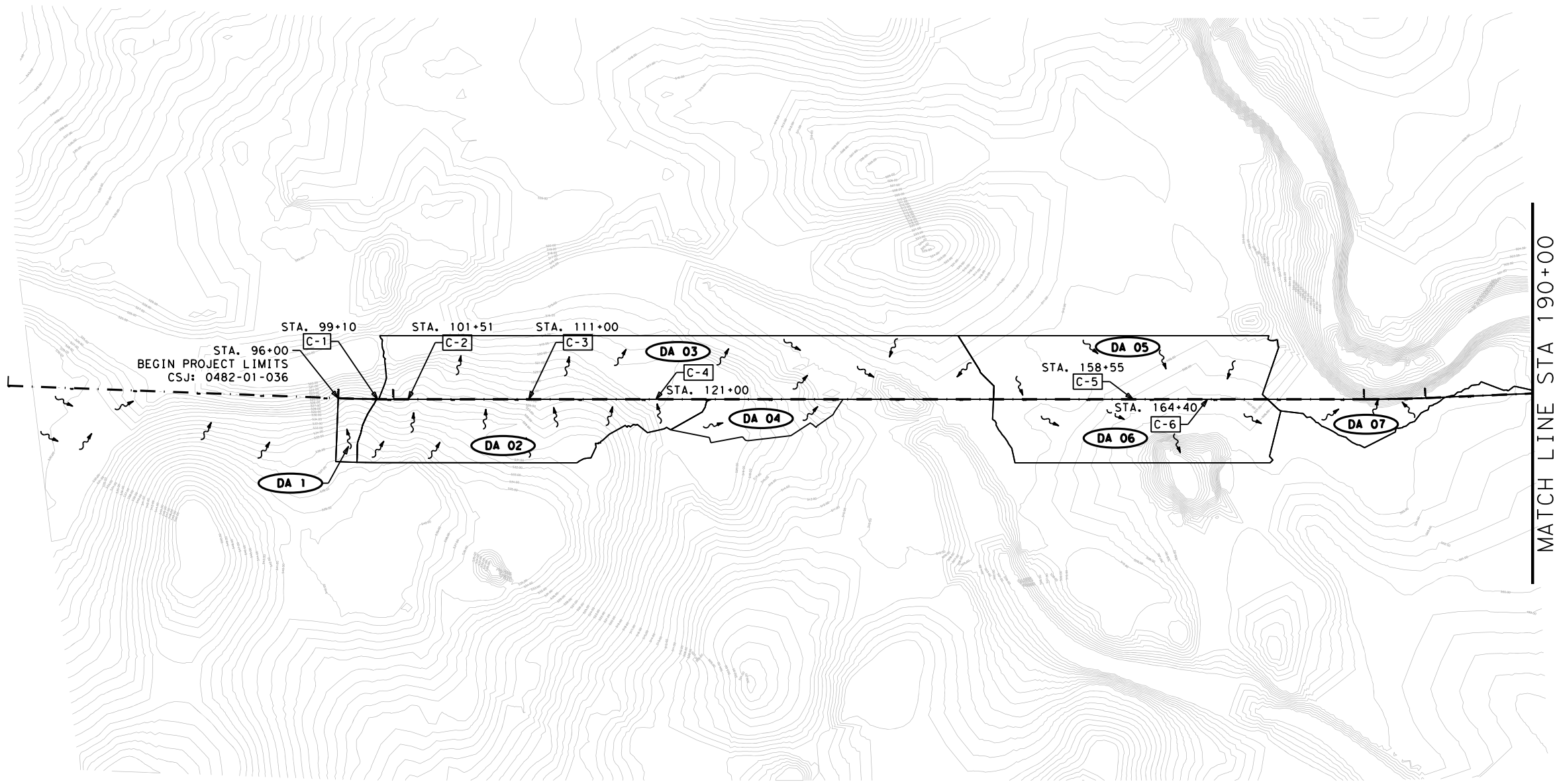
**SH 285
DRAINAGE
COVER SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		231

DATE: 2/7/2024 11:00:25 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Drainage Area Sheets

- LEGEND**
- EXIST ROW LINE
 - - - PROP ROW LINE
 - - - DRAINAGE AREA BOUNDARIES
 - DA XX DRAINAGE AREA IDENTIFICATION
 - C-# CULVERT ID
 - E-# EQUALIZER ID
 - ~ DIRECTION OF FLOW RUNOFF

- NOTES:**
1. DRAINAGE AREAS ARE BASED ON TOPOGRAPHY'S EXISTING CONDITIONS.
 2. SEE HYDRAULIC DATA SHEETS FOR MORE INFORMATION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

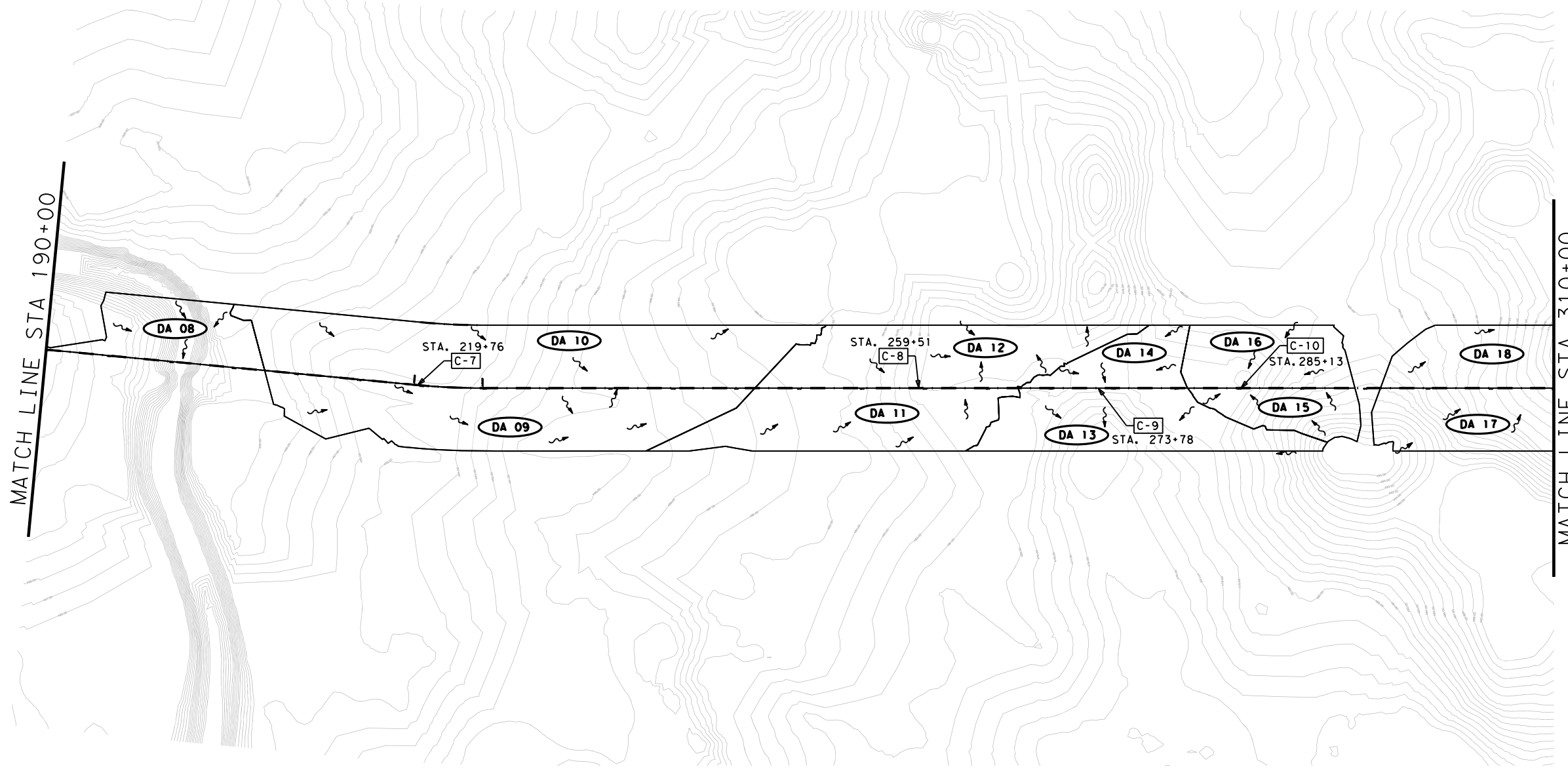


**SH 285
 DRAINAGE AREA
 MAPS**

1" = 1000' SHEET 1 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		232

DATE: 2/7/2024 11:00:26 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Drainage Area Sheets



- LEGEND**
- EXIST ROW LINE
 - - - PROP ROW LINE
 - - - DRAINAGE AREA BOUNDARIES
 - DA XX DRAINAGE AREA IDENTIFICATION
 - C-# CULVERT ID
 - E-# EQUALIZER ID
 - ~ DIRECTION OF FLOW RUNOFF

- NOTES:**
1. DRAINAGE AREAS ARE BASED ON TOPOGRAPHY'S EXISTING CONDITIONS.
 2. SEE HYDRAULIC DATA SHEETS FOR MORE INFORMATION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
 DRAINAGE AREA
 MAPS**

1" = 1000' SHEET 2 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		233

DATE: 2/7/2024 11:00:28 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Drainage Area Sheets



LEGEND

- EXIST ROW LINE
- - - PROP ROW LINE
- - - DRAINAGE AREA BOUNDARIES
- DA XX DRAINAGE AREA IDENTIFICATION
- C-# CULVERT ID
- E-# EQUALIZER ID
- ~ DIRECTION OF FLOW RUNOFF

- NOTES:**
1. DRAINAGE AREAS ARE BASED ON TOPOGRAPHY'S EXISTING CONDITIONS.
 2. SEE HYDRAULIC DATA SHEETS FOR MORE INFORMATION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

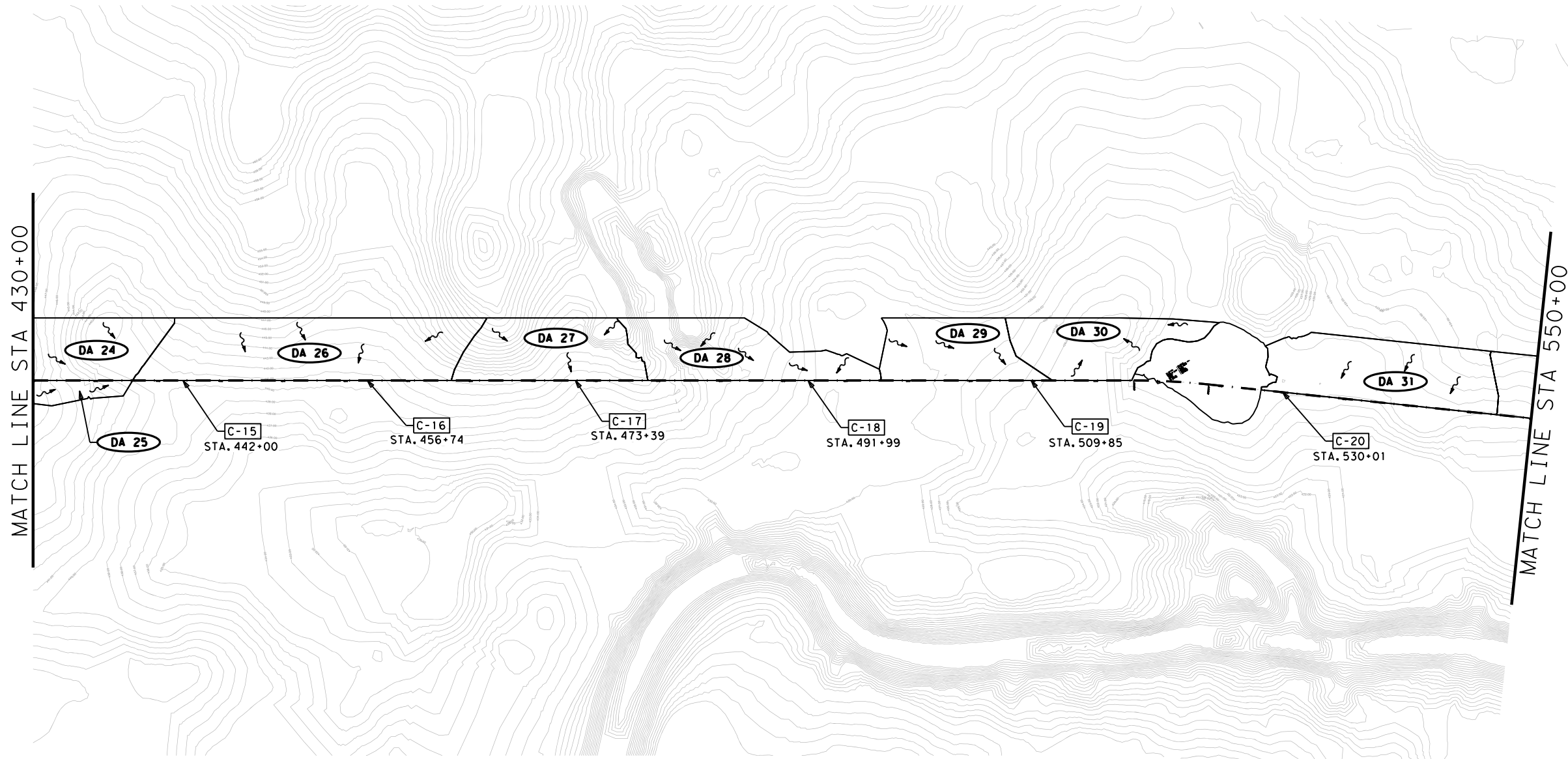


**SH 285
DRAINAGE AREA
MAPS**

1" = 1000' SHEET 3 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		234

DATE: 2/7/2024 11:00:30 AM
 FILE: P:\txdot\project\wiseon\ine.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Drainage Area Sheets



LEGEND

- EXIST ROW LINE
- - - PROP ROW LINE
- - - DRAINAGE AREA BOUNDARIES
- DA XX DRAINAGE AREA IDENTIFICATION
- C-# CULVERT ID
- E-# EQUALIZER ID
- ~ DIRECTION OF FLOW RUNOFF

NOTES:

1. DRAINAGE AREAS ARE BASED ON TOPOGRAPHY'S EXISTING CONDITIONS.
2. SEE HYDRAULIC DATA SHEETS FOR MORE INFORMATION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

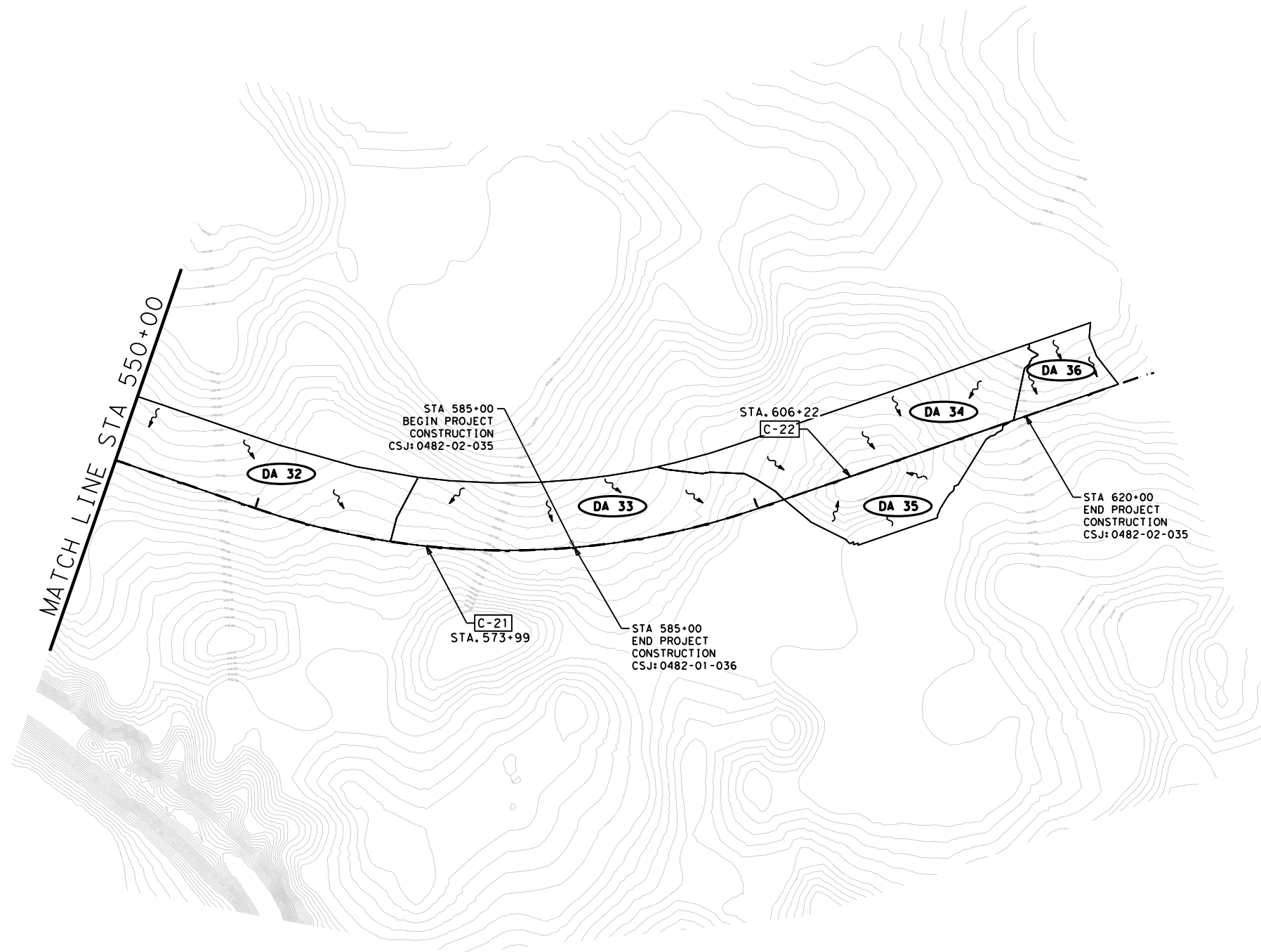


**SH 285
 DRAINAGE AREA
 MAPS**

1" = 1000' SHEET 4 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		235

DATE: 2/7/2024 11:00:31 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Drainage Area Sheets

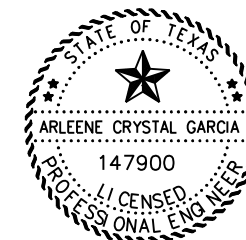


LEGEND

- EXIST ROW LINE
- - - PROP ROW LINE
- - - DRAINAGE AREA BOUNDARIES
- DA XX DRAINAGE AREA IDENTIFICATION
- C-# CULVERT ID
- E-# EQUALIZER ID
- ~ DIRECTION OF FLOW RUNOFF

NOTES:

1. DRAINAGE AREAS ARE BASED ON TOPOGRAPHY'S EXISTING CONDITIONS.
2. SEE HYDRAULIC DATA SHEETS FOR MORE INFORMATION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
 DRAINAGE AREA
 MAPS**

1" = 1000' SHEET 5 OF 5

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		236

DATE: 2/7/2024 11:00:38 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5 - Drainage\SH 285 Hydraulic Data.dgn

RATIONAL METHOD EQUATIONS:

$$Q = CIA$$

$$I = \frac{b}{(T_c + d)^2}$$

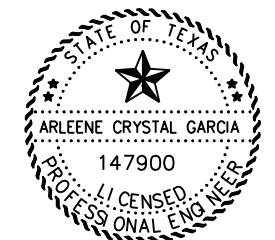
WHERE:

- Q = MAXIMUM RUNOFF RATE (CFS)
- A = DRAINAGE AREA (ACRE)
- C = RUNOFF COEFFICIENT
- I = AVERAGE RAINFALL INTENSITY (IN/HR)
- T_c = TIME OF CONCENTRATION (MIN)
- e, b, d = RAINFALL INTENSITY FACTORS (PER COUNTY)

NOTES:

1. EQUATIONS AND "C" VALUES DERIVED FROM TXDOT 2019 HYDRAULIC DESIGN MANUAL CHAPTER 4 SECTION 12 "RATIONAL METHOD".
2. RAINFALL DEPTH AND INTENSITY ARE BASED ON VALUES FOUND IN THE "NOAA ATLAS OF DEPTH-FREQUENCY PRECIPITATION MANUAL MAXIMA FOR TEXAS".

SH 285 HYDRAULIC DATA (RATIONAL METHOD)								
DRAINAGE AREA ID	DRAINAGE AREA (AC)	RUNOFF COEFFICIENT "C" VALUE	TIME OF CONCENTRATION (MIN)	10 YEAR STORM INTENSITY (IN/HR)	100 YEAR STORM INTENSITY (IN/HR)	PEAK DISCHARGE 10 YEAR STORM Q _p (CFS)	PEAK DISCHARGE 25 YEAR STORM Q _p (CFS)	PEAK DISCHARGE 100 YEAR STORM Q _p (CFS)
DA 01	2.3	0.25	16	5.71	9.16	3.3	3.9	5.3
DA 02	25.3	0.25	17	5.56	8.95	35.2	42.3	56.6
DA 03	53.8	0.25	15	5.87	9.38	79.0	94.7	126.2
DA 04	7.1	0.25	18	5.42	8.75	9.6	11.6	15.5
DA 05	26.4	0.25	10	6.81	10.67	44.9	53.5	70.4
DA 06	24.6	0.25	15	5.87	9.38	36.1	43.3	57.7
DA 07	6.3	0.25	19	5.29	8.56	8.3	10.0	13.5
DA 08	15.1	0.25	14	6.03	9.61	22.8	27.3	36.3
DA 09	38.1	0.25	17	5.56	8.95	53.0	63.7	85.2
DA 10	49.0	0.25	15	5.87	9.38	71.9	86.2	114.9
DA 11	25.8	0.25	18	5.42	8.75	35.0	42.1	56.4
DA 12	26.8	0.25	15	5.87	9.38	39.3	47.2	62.8
DA 13	23.4	0.25	18	5.42	8.75	31.7	38.2	51.2
DA 14	9.4	0.25	21	5.05	8.21	11.9	14.3	19.3
DA 15	9.4	0.25	17	5.56	8.95	13.1	15.7	21.0
DA 16	14.7	0.25	10	6.81	10.67	25.0	29.8	39.2
DA 17	30.4	0.25	16	5.71	9.16	43.4	52.1	69.6
DA 18	34.9	0.25	19	5.29	8.56	46.2	55.6	74.7
DA 19	17.1	0.25	16	5.71	9.16	24.4	29.3	39.2
DA 20	12.2	0.25	19	5.29	8.56	16.1	0.0	26.1
DA 21	28.8	0.25	17	5.56	8.95	40.0	48.2	64.4
DA 22	11.7	0.25	21	5.05	8.21	14.8	17.8	24.0
DA 23	12.1	0.25	52	3.01	5.09	9.1	11.1	15.4
DA 24	19.9	0.25	14	6.03	9.61	30.0	36.0	47.8
DA 25	4.9	0.25	17	5.56	8.95	6.8	8.2	11.0
DA 26	14.6	0.25	15	5.87	9.38	21.4	25.7	34.2
DA 27	15.4	0.25	15	5.87	9.38	22.6	27.1	36.1
DA 28	16.6	0.25	13	6.21	9.85	25.8	30.8	40.9
DA 29	12.5	0.25	17	5.56	8.95	17.4	20.9	28.0
DA 30	13.6	0.25	14	6.03	9.61	20.5	24.6	32.7
DA 31	20.4	0.25	16	5.71	9.16	29.1	35.0	46.7
DA 32	28.3	0.25	15	5.87	9.38	41.5	49.8	66.4
DA 33	28.9	0.25	16	5.71	9.16	41.3	49.6	66.2
DA 34	25.9	0.25	14	6.03	9.61	39.0	46.8	62.2
DA 35	13.3	0.25	10	6.81	10.67	22.6	27.0	35.5
DA 36	6.9	0.25	22	4.93	8.04	8.5	10.3	13.9



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

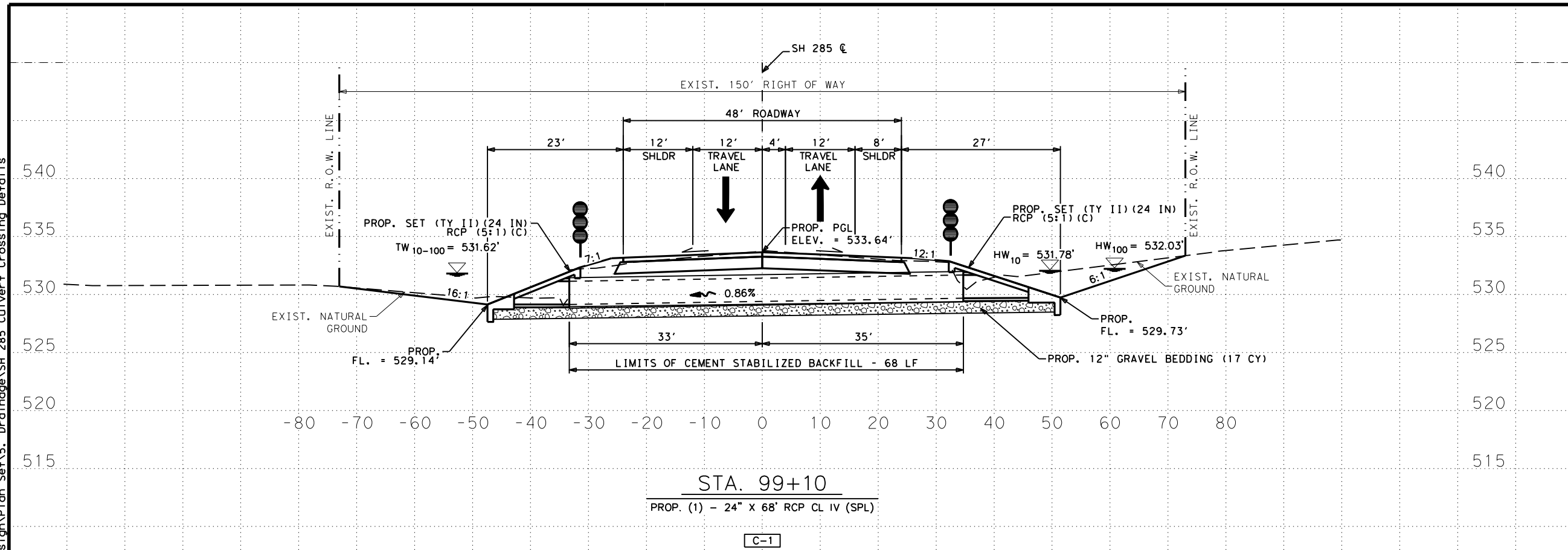


SH 285
HYDRAULIC
DATA

N. T. S.		SHEET 1 OF 1	
© 2024	CONT	SECT	JOB
	0482	01	036, etc.
	DIST	COUNTY	SH 285
	PHR	JIM HOGG	SHEET NO.
			237

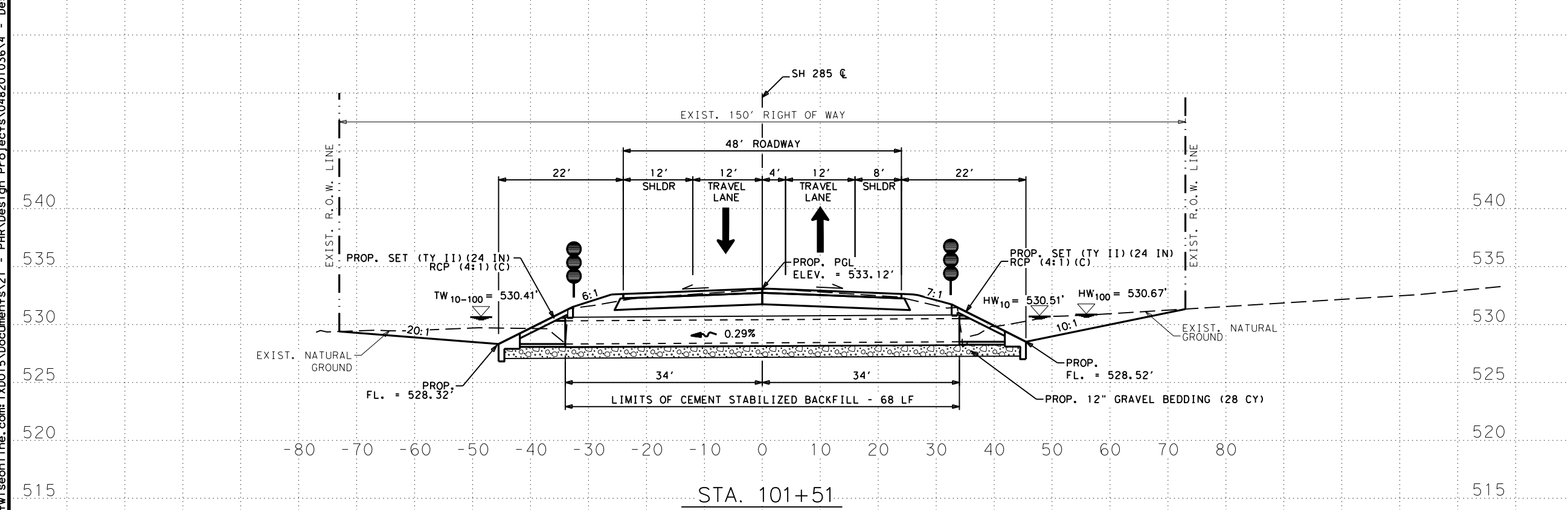
FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:43 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



STA. 99+10
 PROP. (1) - 24" X 68' RCP CL IV (SPL)

C-1



STA. 101+51
 PROP. (2) - 24" X 68' RCP CL IV (SPL)

C-2

STATION	ITEMS									
	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
99+10	45	29	17	-	-	-	68	-	-	2
101+51	82	49	28	-	-	-	136	-	2	-

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z) (FLX) GND (BI)
 - NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - REFER TO SHEET "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z) (FLX) GND (BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

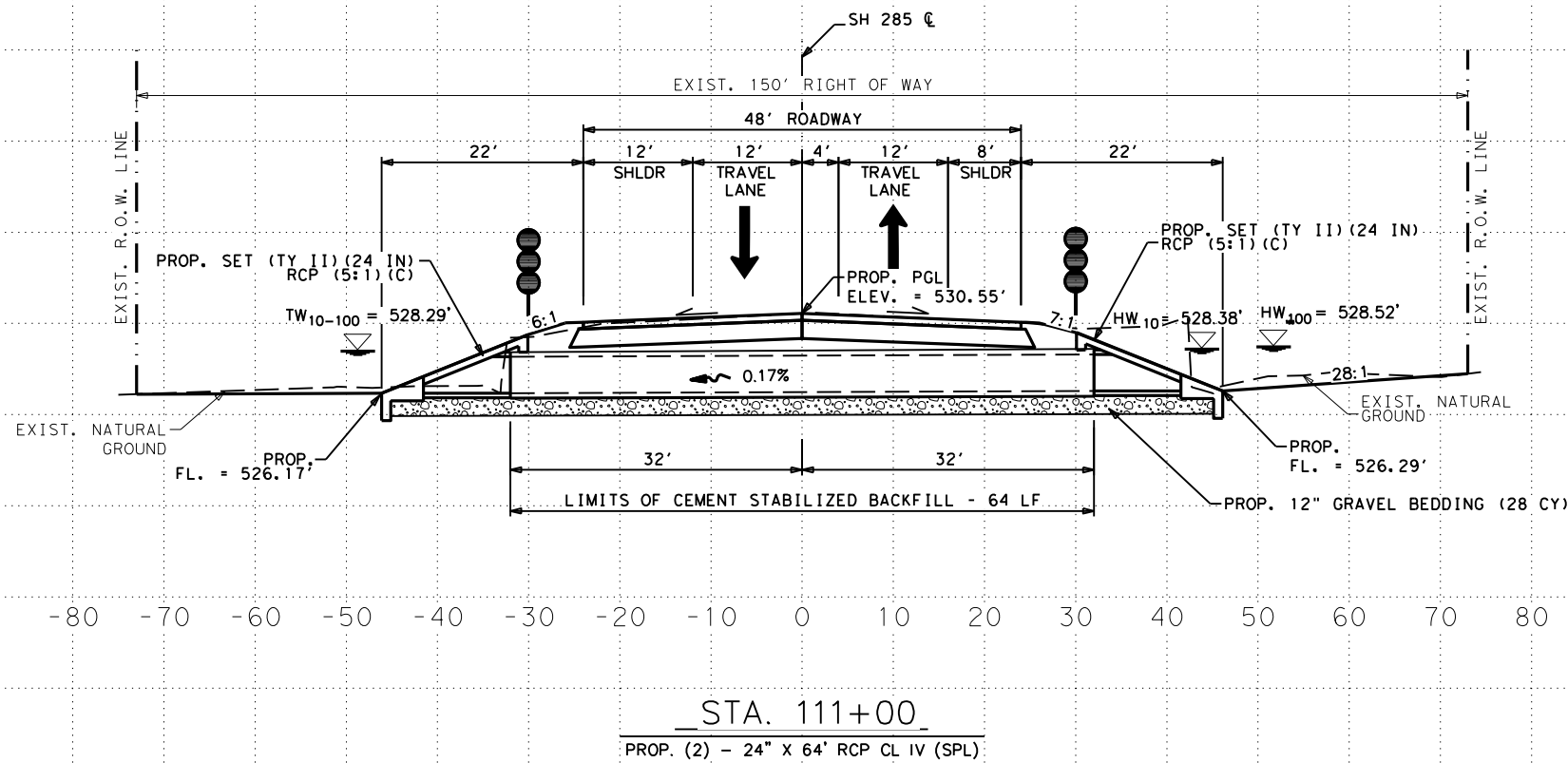
SH 285 CULVERT CROSSING STA. 99+10 & STA. 101+51

SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 1 OF 11

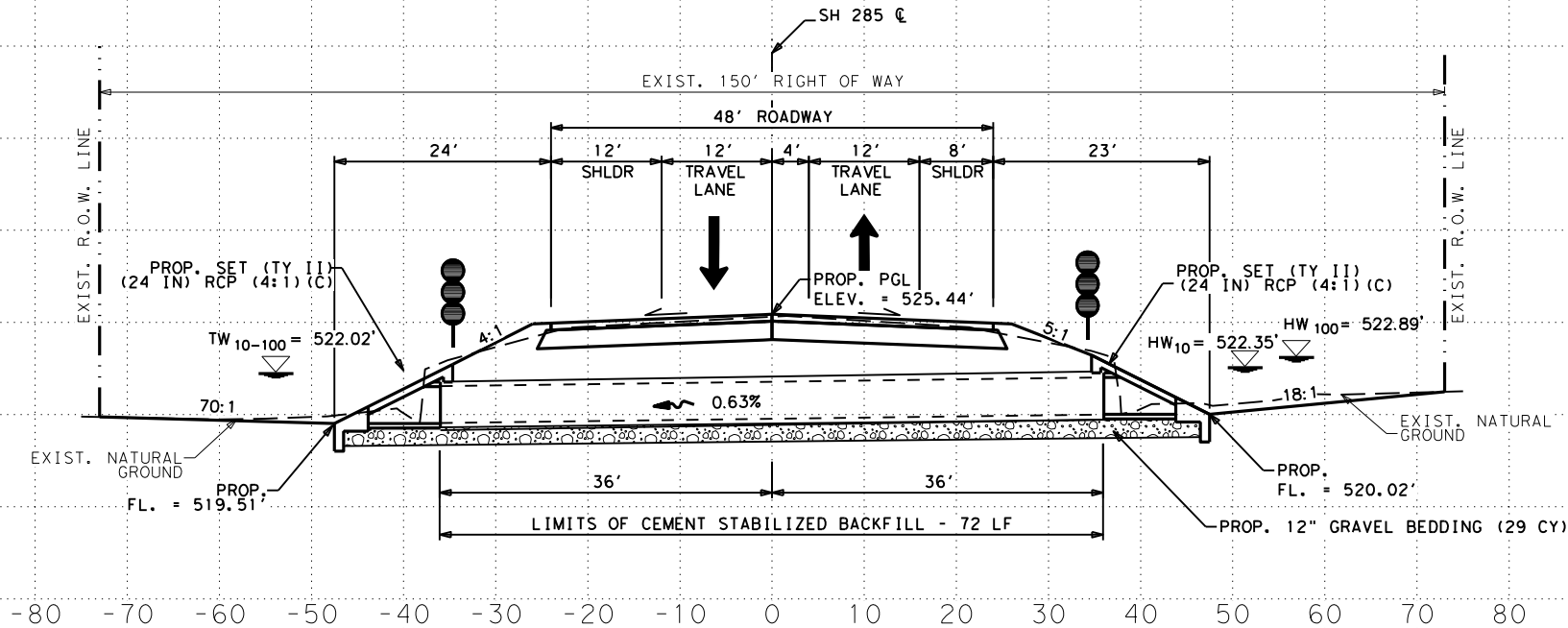
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		238

FILE: pw:\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:44 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



STA. 111+00
 PROP. (2) - 24" X 64' RCP CL IV (SPL)
 C-3



STA. 121+00
 PROP. (2) - 24" X 72' RCP CL IV (SPL)
 C-4

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z)(FLX)GND(BI)
 - NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - REFER TO SHEET "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

SH 285 CULVERT CROSSING STA. 111+00 & STA. 121+00

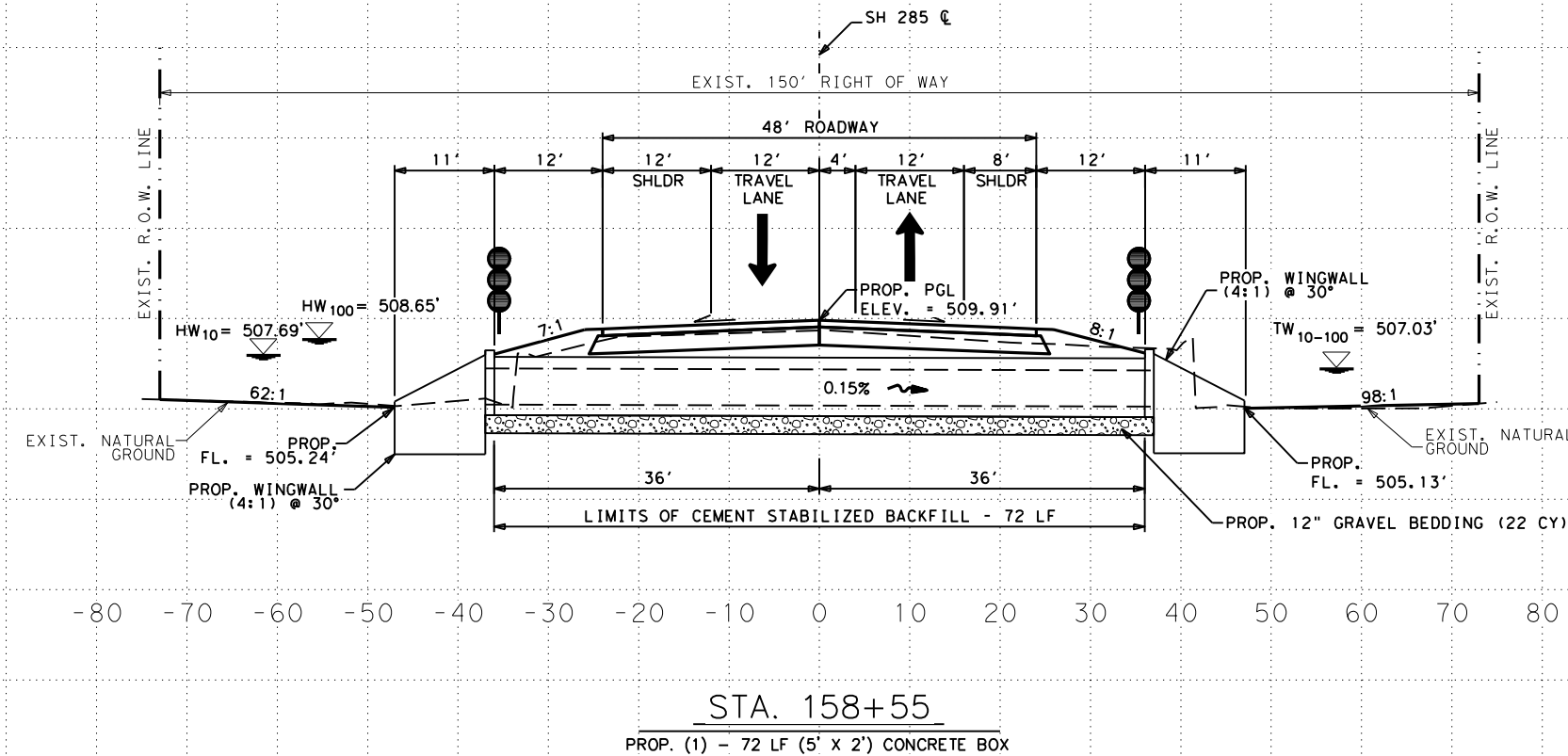
SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 2 OF 11

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	239

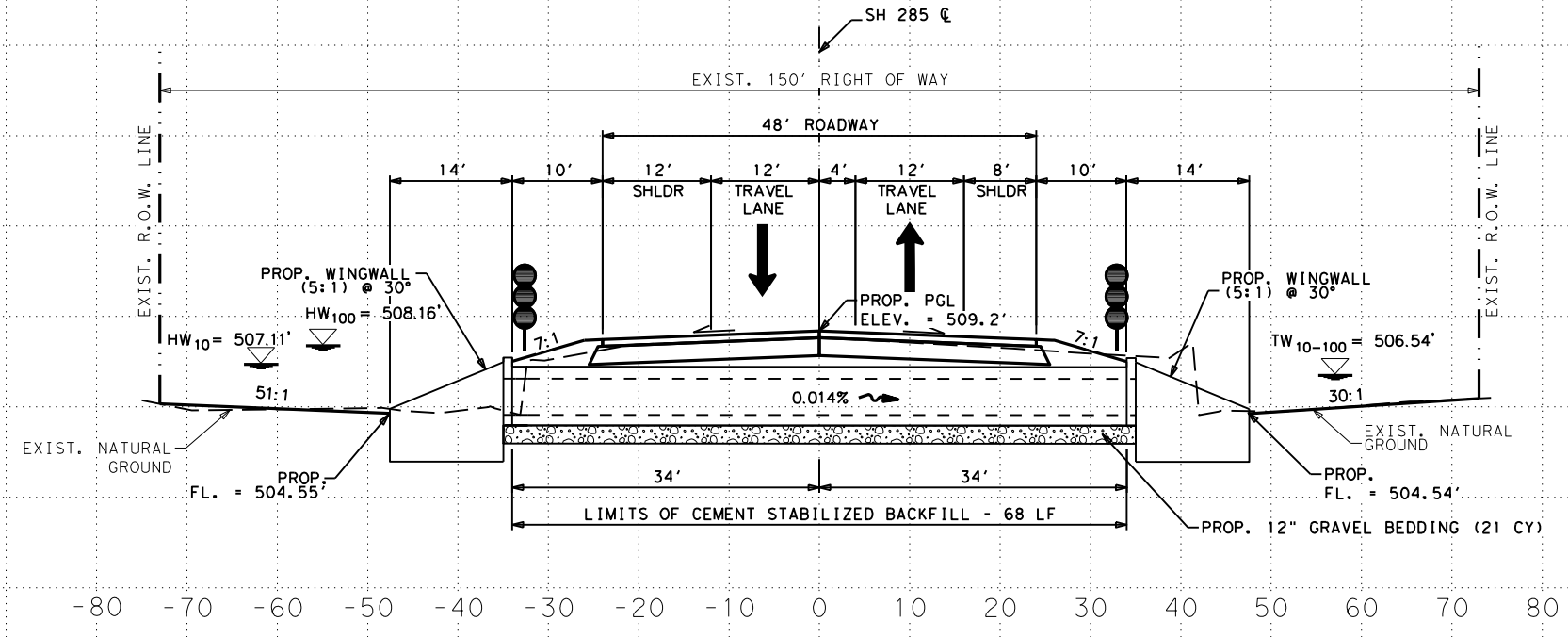
STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
111+00	81	46	28	-	-	-	128	-	-	2
121+00	107	51	29	-	-	-	144	-	2	-

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:46 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



STA. 158+55
 PROP. (1) - 72 LF (5' X 2') CONCRETE BOX
 C-5



STA. 164+40
 PROP. (1) - 68 LF (5' X 2') CONCRETE BOX
 C-6

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z)(FLX)GND(BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

SH 285 CULVERT CROSSING STA. 158+55 & STA. 164+40

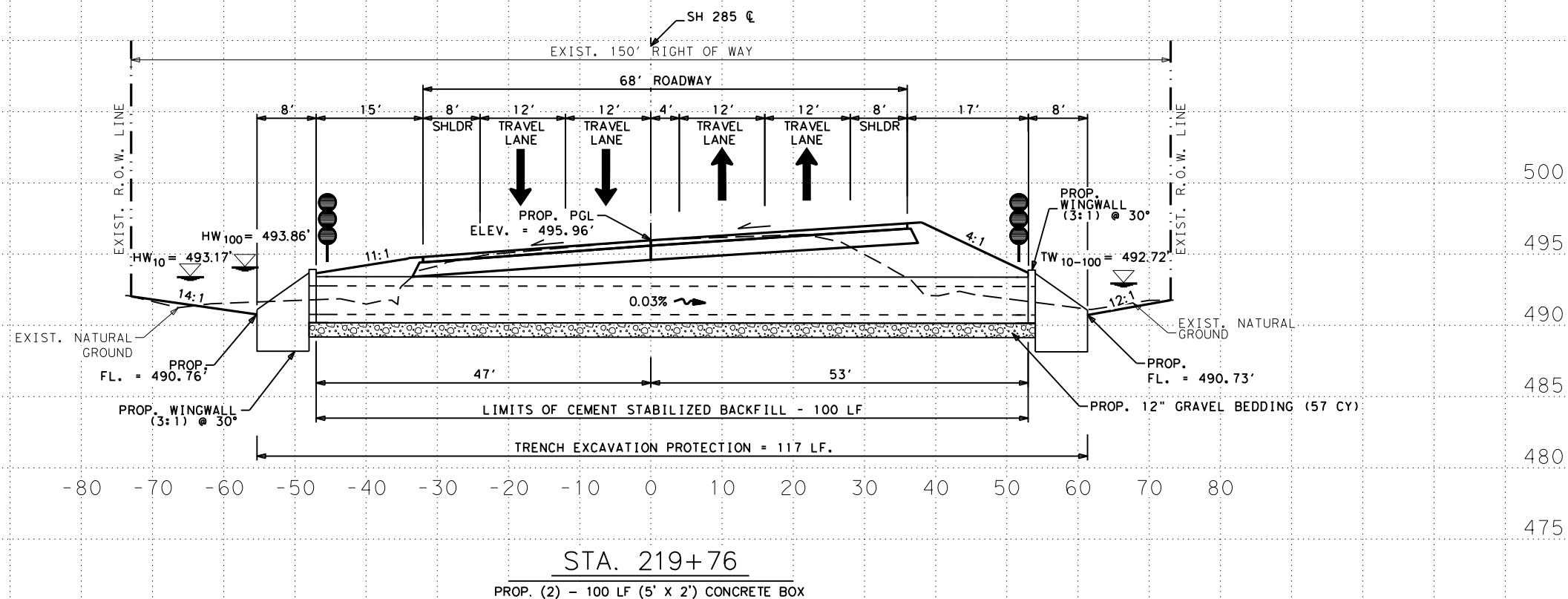
SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 3 OF 11

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	240

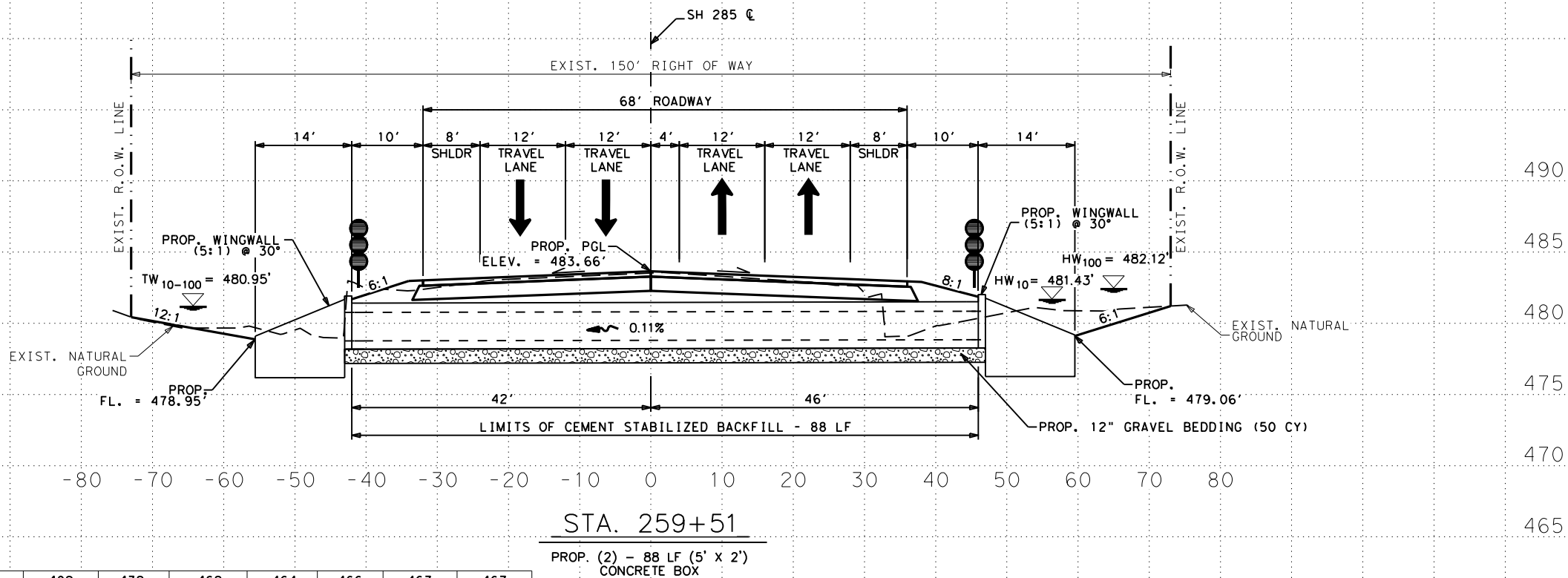
STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
158+55	104	39	22	-	51	72	-	2	-	-
164+40	122	37	21	-	71	68	-	2	-	-

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:48 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



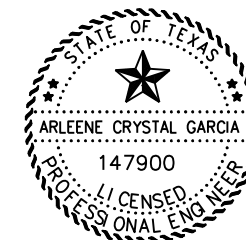
STA. 219+76
 PROP. (2) - 100 LF (5' X 2') CONCRETE BOX
 C-7



STA. 259+51
 PROP. (2) - 88 LF (5' X 2') CONCRETE BOX
 C-8

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z)(FLX)GND(BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
 CULVERT CROSSING
 STA. 219+76 &
 STA. 259+51**

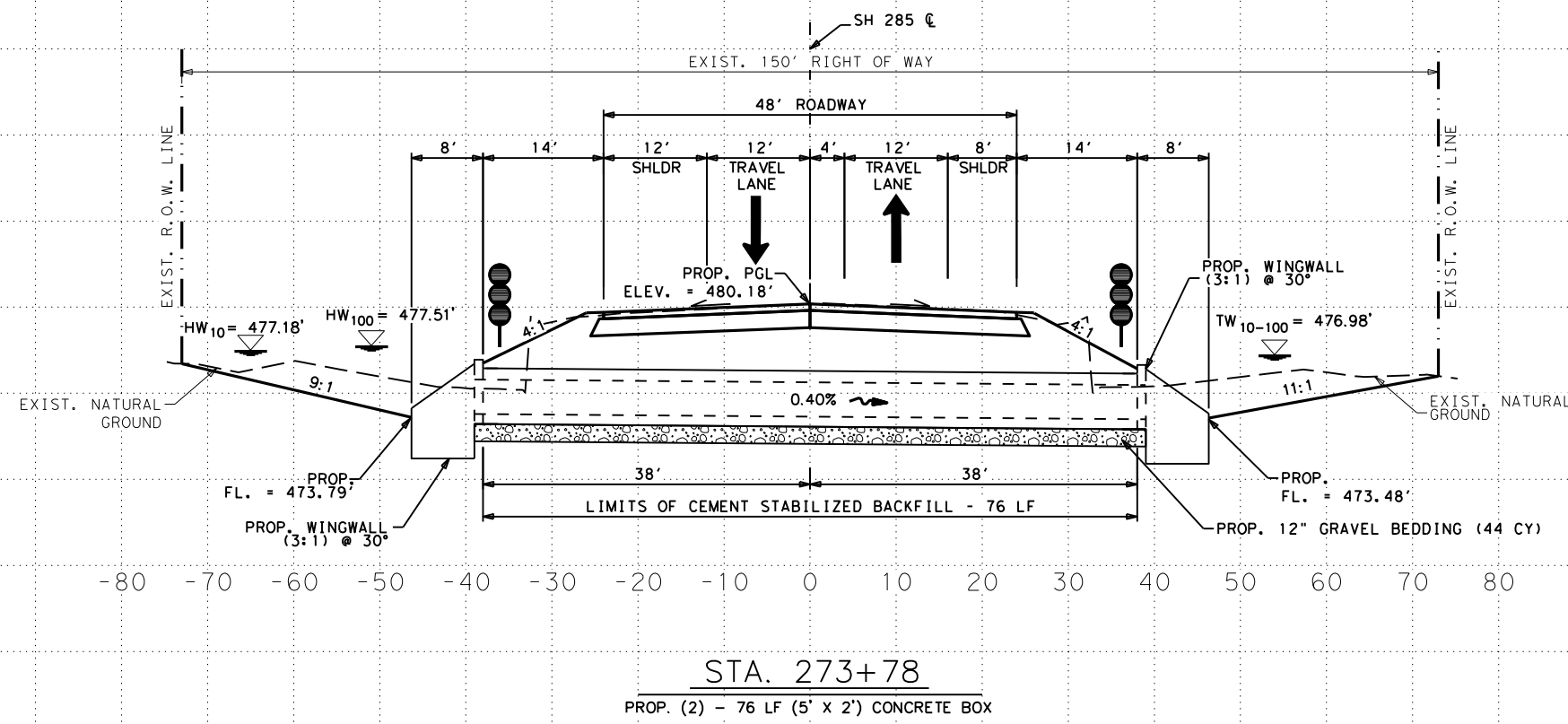
SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 4 OF 11

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	241

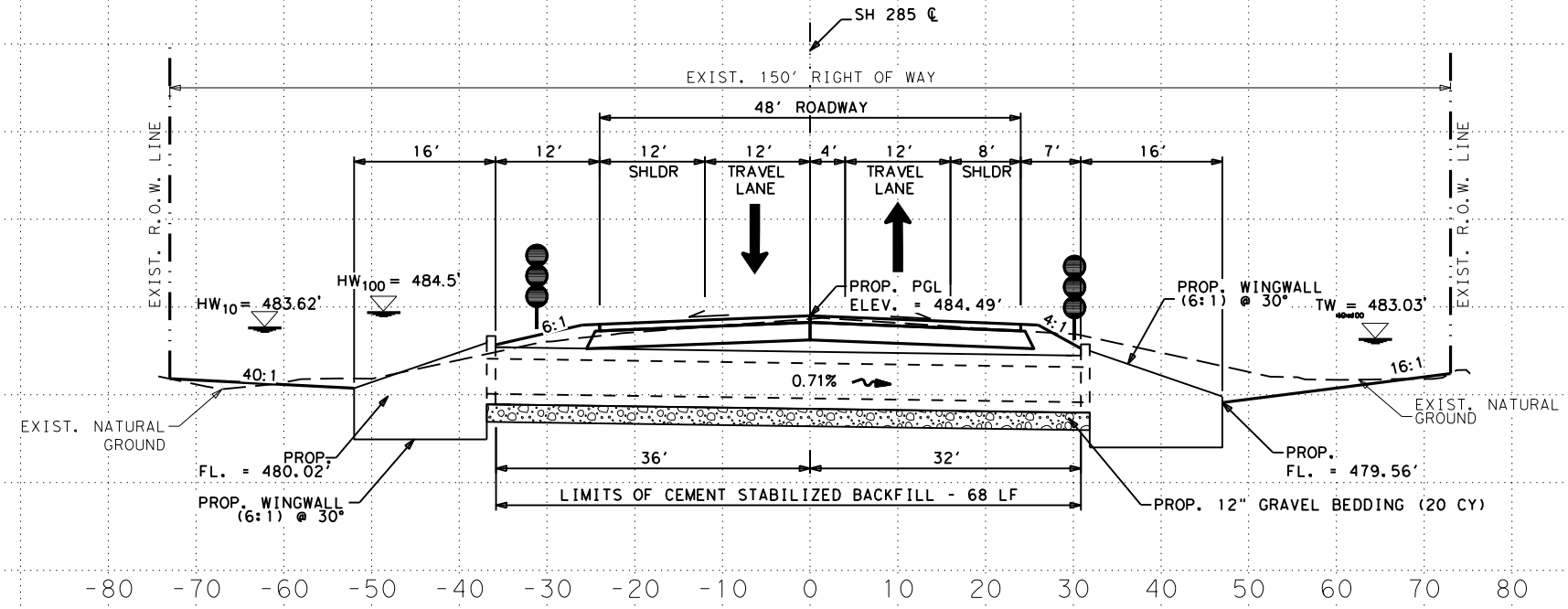
STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
219+76	234	92	57	117	55	200	-	2	-	-
259+51	228	81	50	-	109	176	-	2	-	-

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5 - Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:50 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5 - Drainage\SH 285 Culvert Crossing Details



STA. 273+78
 PROP. (2) - 76 LF (5' X 2') CONCRETE BOX
 C-9



STA. 285+13
 PROP. (1) - 68 LF (5' X 2') CONCRETE BOX
 C-10

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-22)(FLX)GND(BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-22)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Pharr District Central Design
 Texas Department of Transportation

**SH 285
 CULVERT CROSSING
 STA. 273+78 &
 STA. 285+13**

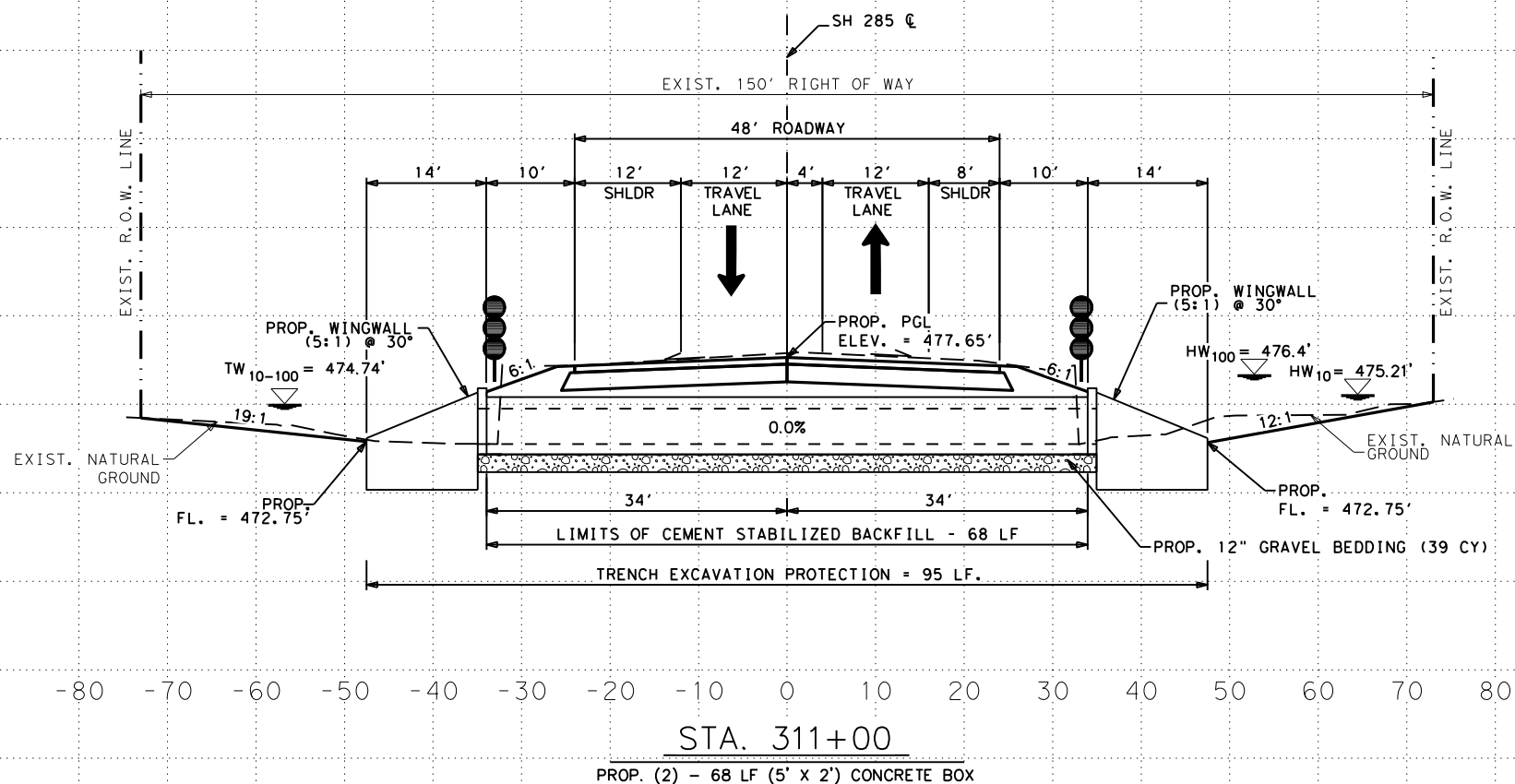
SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 5 OF 11

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	242

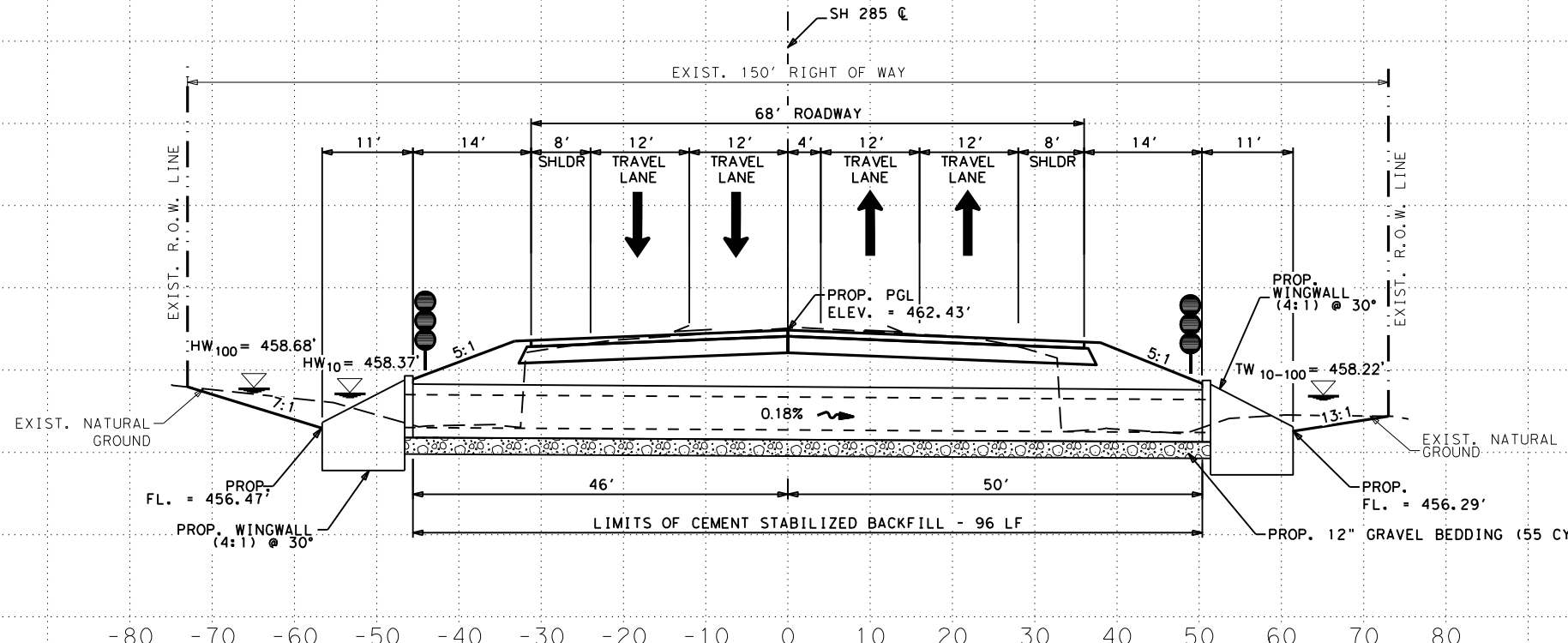
STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
273+78	266	70	44	-	55	152	-	2	-	-
285+13	248	37	21	-	95	68	-	2	-	-

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:52 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



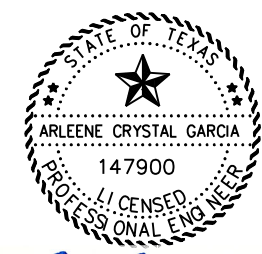
STA. 311+00
 PROP. (2) - 68 LF (5' X 2') CONCRETE BOX
 C-11



STA. 360+24
 PROP. (2) - 96 LF (5' X 2') CONCRETE BOX
 C-12

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z)(FLX)GND(BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

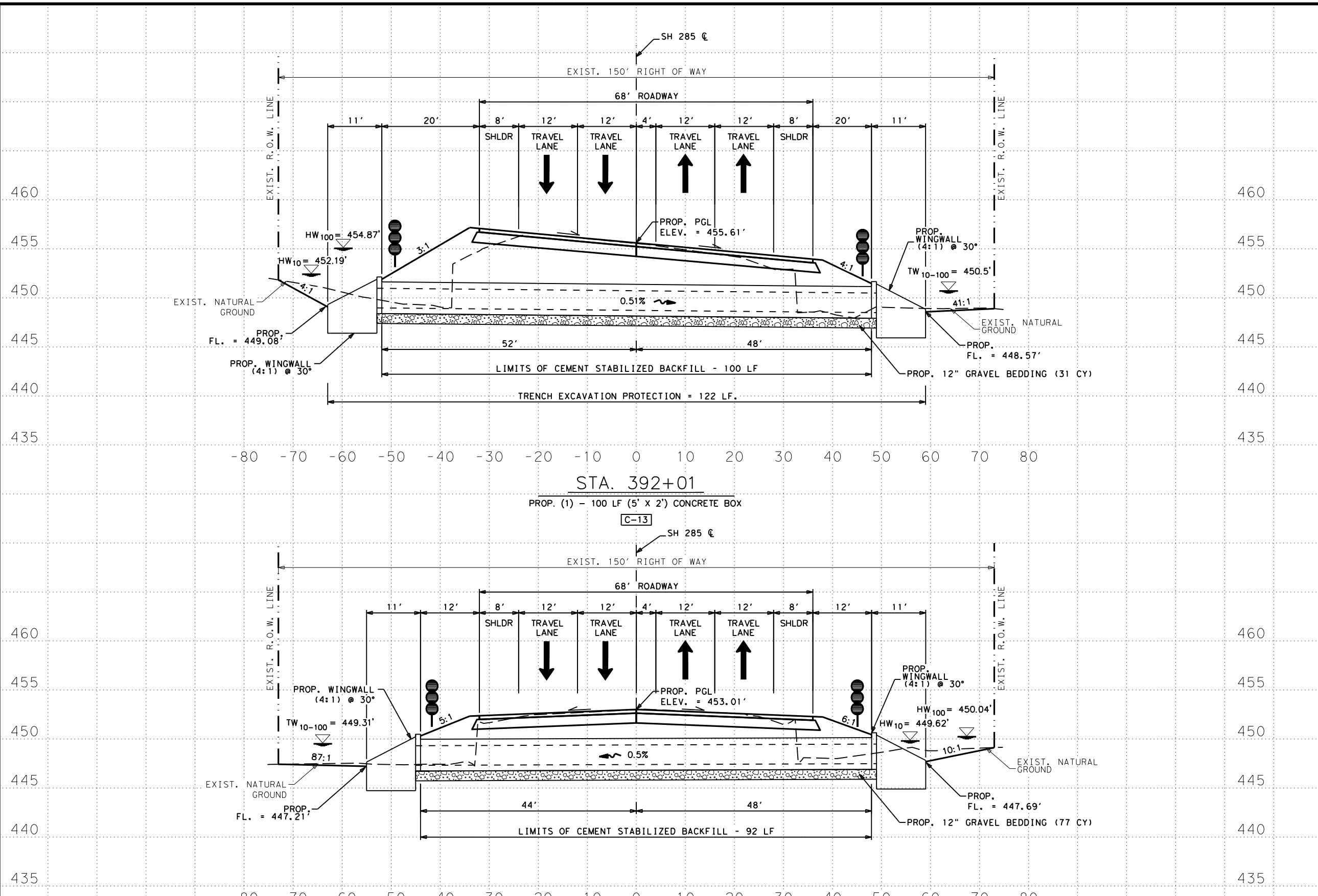
SH 285 CULVERT CROSSING STA. 311+00 & STA. 360+24

SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 6 OF 11

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	243

STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
311+00	193	61	39	-	109	136	-	2	-	-
360+24	238	88	55	-	81	192	-	2	-	-

DATE: 2/7/2024 11:00:53 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z)(FLX)GND(BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

**SH 285
 CULVERT CROSSING
 STA. 392+01 &
 STA. 415+36**

SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 7 OF 11

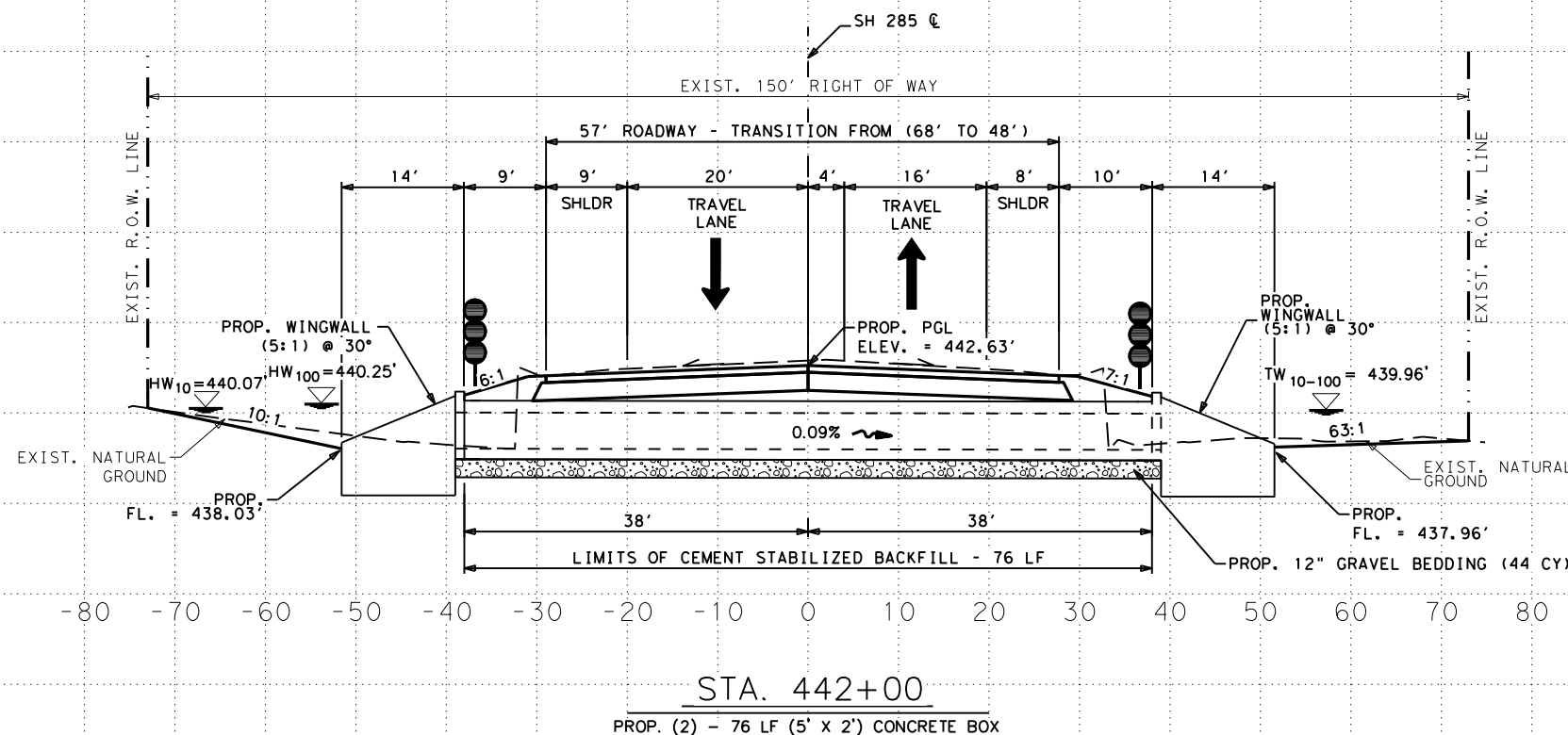
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	244

STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (51N)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
400	400	400	402	432	462	464	466	467	467	
	6005	6010	6001	6002	6006	6060	6150	6390	6392	
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
392+01	160	54	31	122	51	100	-	2	-	-
415+36	278	120	77	-	111	276	-	2	-	-

STA. 415+36
 PROP. (3) - 92 LF (5' X 2') CONCRETE BOX
 C-14

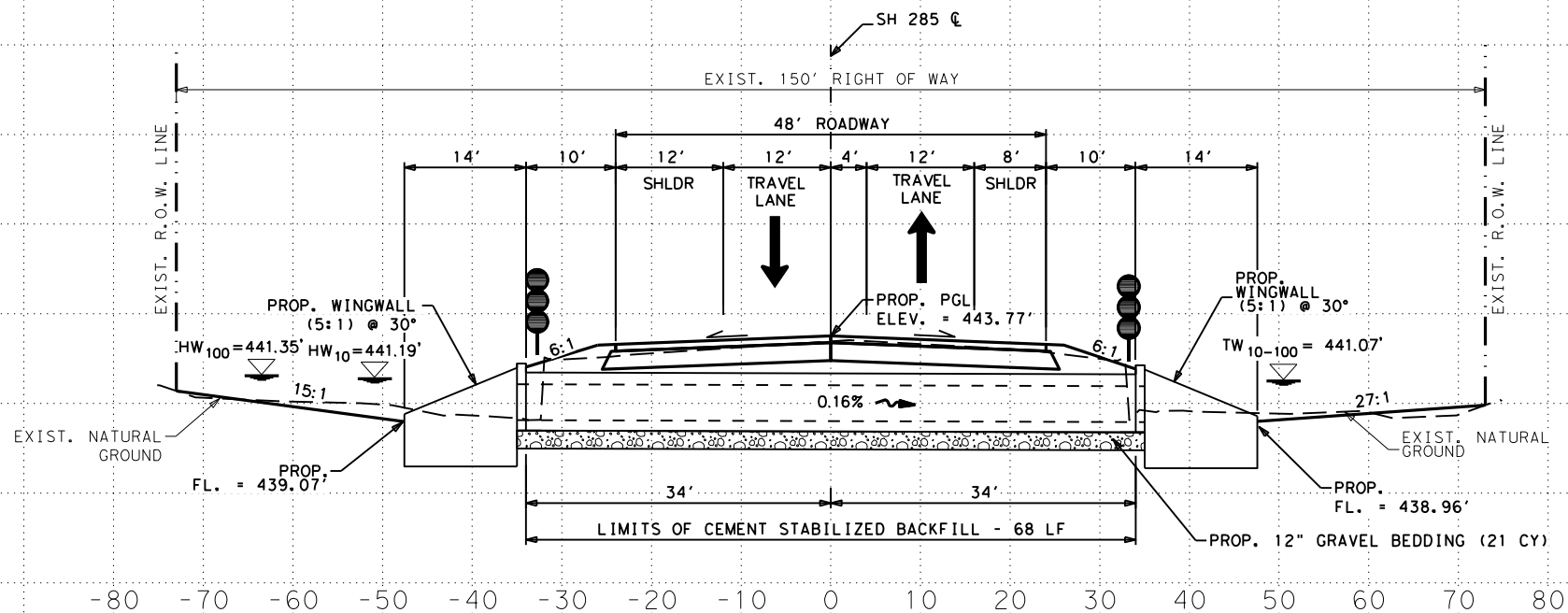
FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:55 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



STA. 442+00
 PROP. (2) - 76 LF (5' X 2') CONCRETE BOX

C-15



STA. 456+74

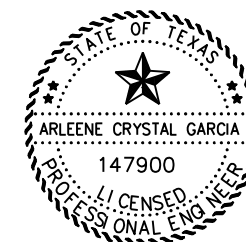
PROP. (1) - 68 LF (5' X 2') CONCRETE BOX

C-16

STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
442+00	184	67	44	-	109	152	-	2	-	-
456+74	121	37	21	-	71	68	-	2	-	-

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-22) (FLX) GND (BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-22) (FLX) GND (BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



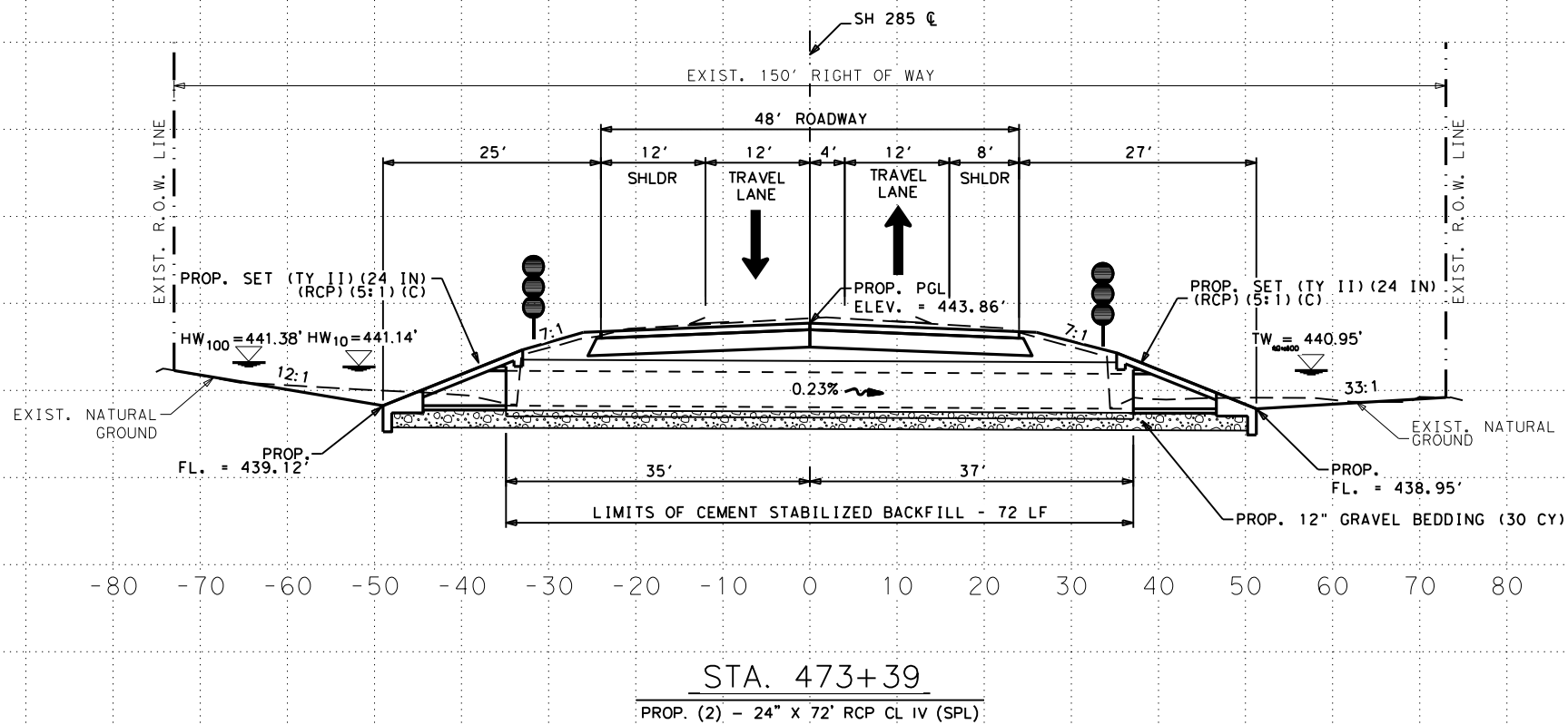
**SH 285
 CULVERT CROSSING
 STA. 442+00 &
 STA. 456+74**

SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 8 OF 11

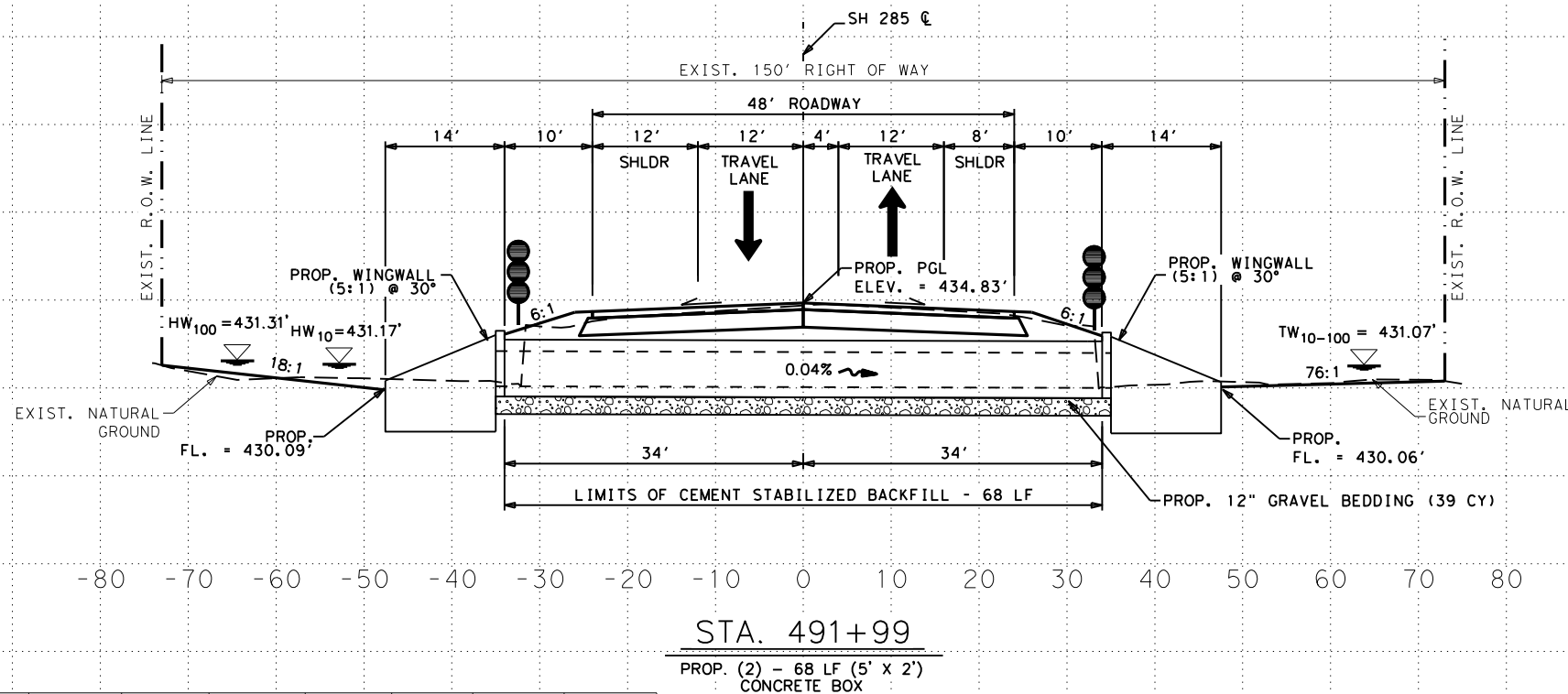
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	245

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:57 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



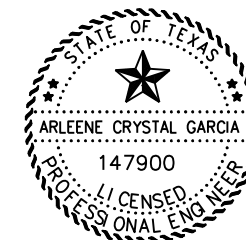
STA. 473+39
 PROP. (2) - 24" X 72" RCP CL IV (SPL)
 C-17



STA. 491+99
 PROP. (2) - 68 LF (5' X 2') CONCRETE BOX
 C-18

- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z) (FLX) GND(BI)
 - * NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - ** REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z) (FLX) GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design



**SH 285
 CULVERT CROSSING
 STA. 473+79 &
 STA. 491+99**

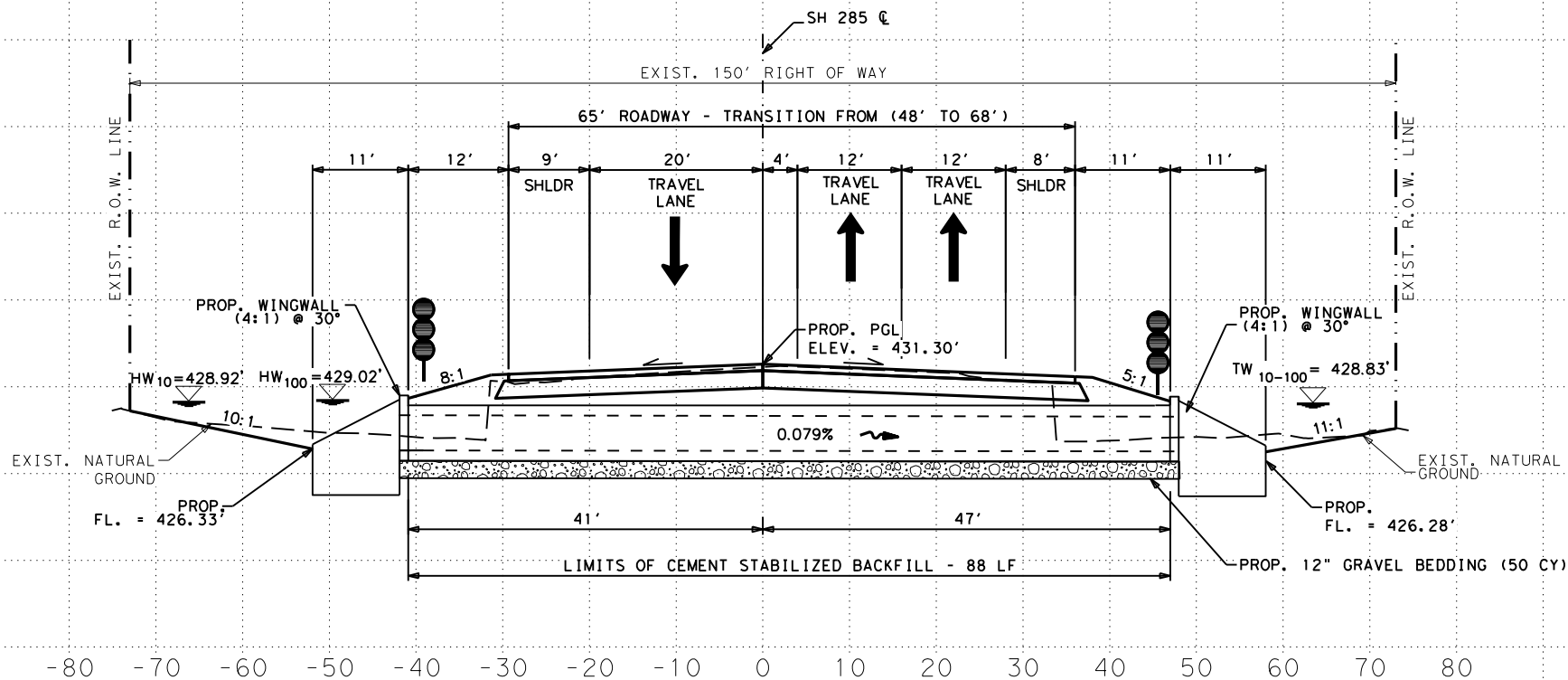
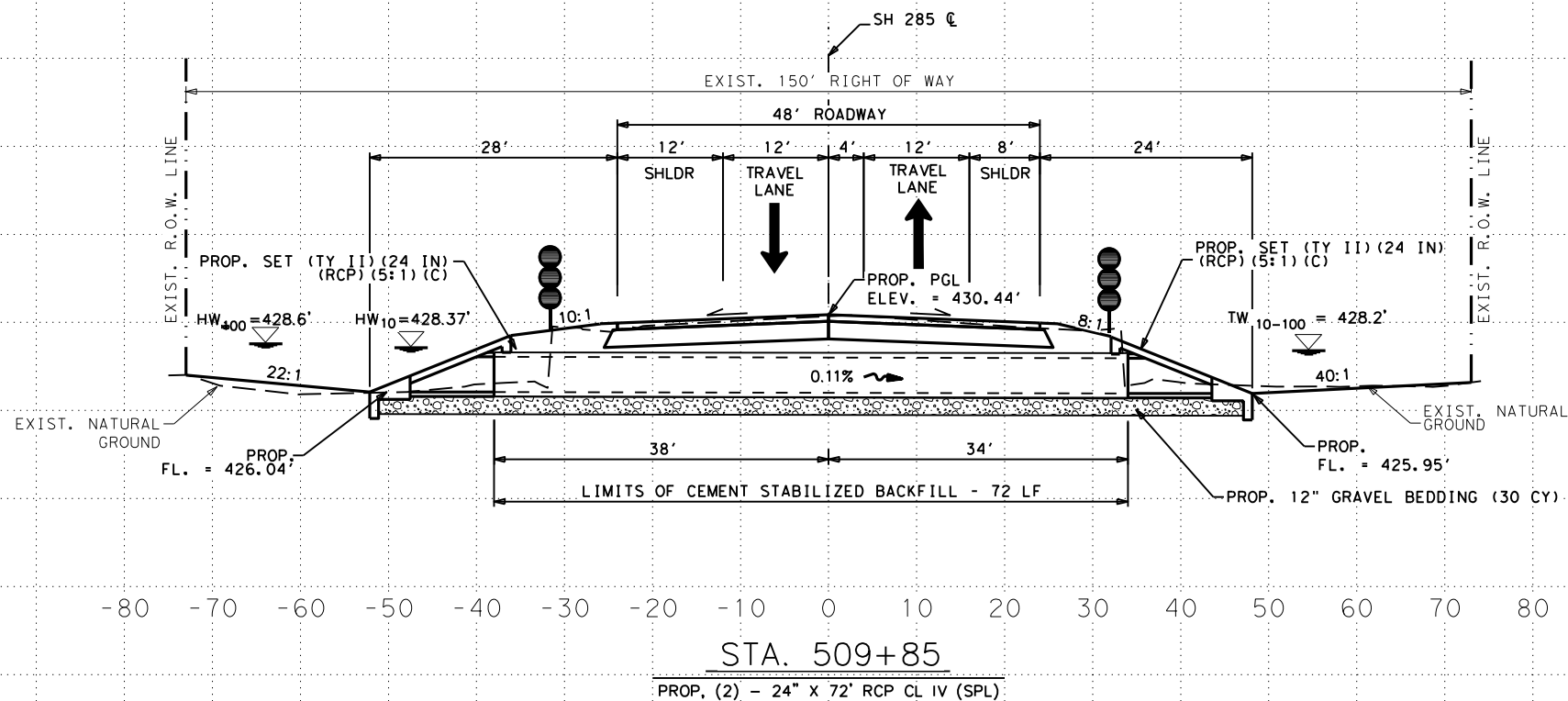
SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 9 OF 11

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	246

STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
	CY	CY	CY	LF	CY	LF	LF	EA	EA	EA
473+39	194	51	30	-	-	-	144	-	-	2
491+99	182	63	39	-	109	136	-	2	-	-

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details

DATE: 2/7/2024 11:00:59 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5. Drainage\SH 285 Culvert Crossing Details



- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-2Z) (FLX) GND (BI)
 - NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-2Z) (FLX) GND (BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Pharr District Central Design
 Texas Department of Transportation

**SH 285
 CULVERT CROSSING
 STA. 509+85 &
 STA. 530+01**

SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 10 OF 11

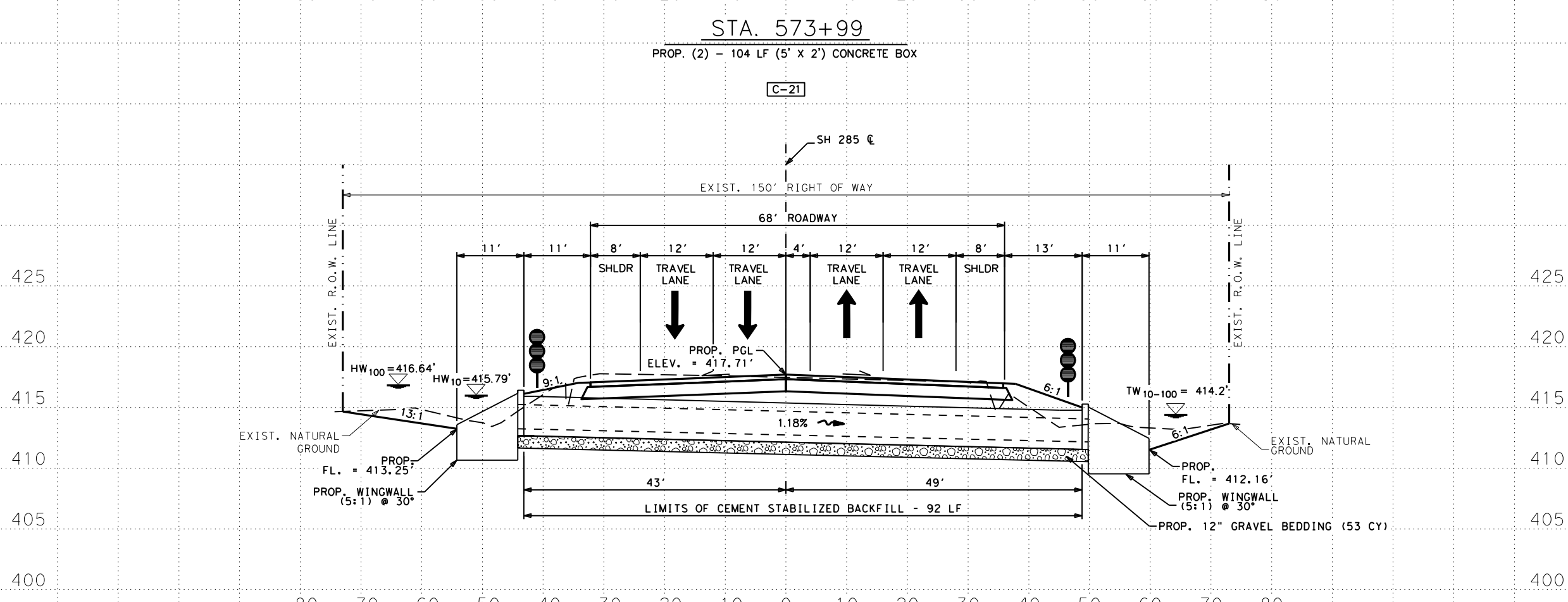
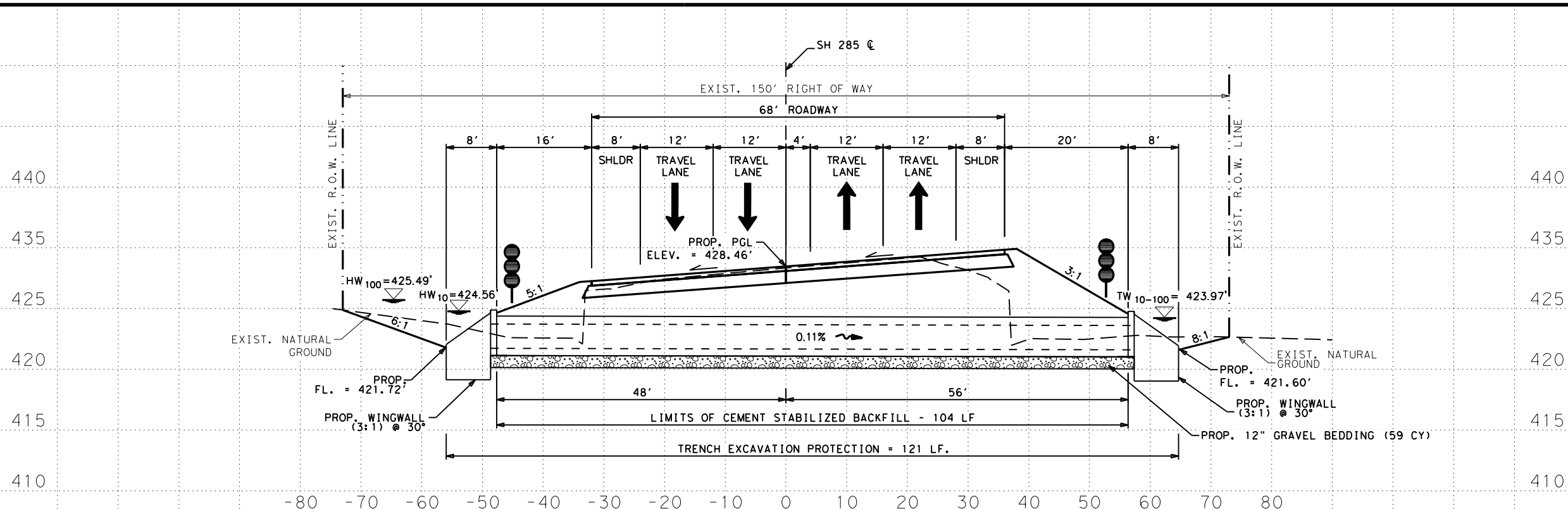
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	247

STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
400		400	400	402	432	462	464	466	467	467
		6005	6010	6001	6002	6006	6060	6150	6390	6392
509+85	78	51	31	-	-	-	144	-	-	2
530+01	199	81	50	-	81	176	-	2	-	-

STA. 530+01
 PROP. (2) - 88 LF (5' X 2')
 CONCRETE BOX
 C-20

FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5 - Drainage\Plan Set\5 - Culvert Crossing Details

DATE: 2/7/2024 11:01:01 AM
 FILE: pw:\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\5 - Drainage\Plan Set\5 - Culvert Crossing Details



- LEGEND:**
- GRAVEL BEDDING
 - PROP. DIRECTION OF TRAFFIC FLOW
 - DIRECTION OF FLOW
 - DESIGN YEAR WATER SURFACE ELEVATION
 - OM ASSM(OM-22)(FLX)GND(BI)
 - NON-PAY ITEM(FOR CONT. INFORMATION ONLY)
 - REFER TO SHEET 249 "CEMENT STABIL. BACKFILL DETAILS" FOR ADD. INFORMATION

- NOTES:**
1. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY OWNERS TO FIELD VERIFY ALL HORIZONTAL AND VERTICAL INFORMATION OF ALL UTILITIES PRIOR TO COMMENCING ANY CULVERT WORK.
 2. ANY DAMAGES TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 3. SEE THE ROADWAY DATA SHEET FOR SH 285 ALIGNMENT.
 4. REFER TO SH 285 PROPOSED TYPICAL SECTIONS FOR PROPOSED PAVEMENT LAYERS.
 5. REFER TO SH 285 REMOVAL LAYOUTS FOR QUANTITIES OF EXISTING PIPES, S.E.T., CONCRETE BOXES, WINGWALLS, AND OTHER ASSOCIATED DRAINAGE STRUCTURES.
 6. OM ASSM(OM-22)(FLX)GND(BI) QUANTITIES REFLECTED IN SIGNING LAYOUTS.



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

SH 285 CULVERT CROSSING STA. 573+99 & STA. 606+22

SCALE: HOR: 1"=20'; VER: 1"=10' SHEET 11 OF 11

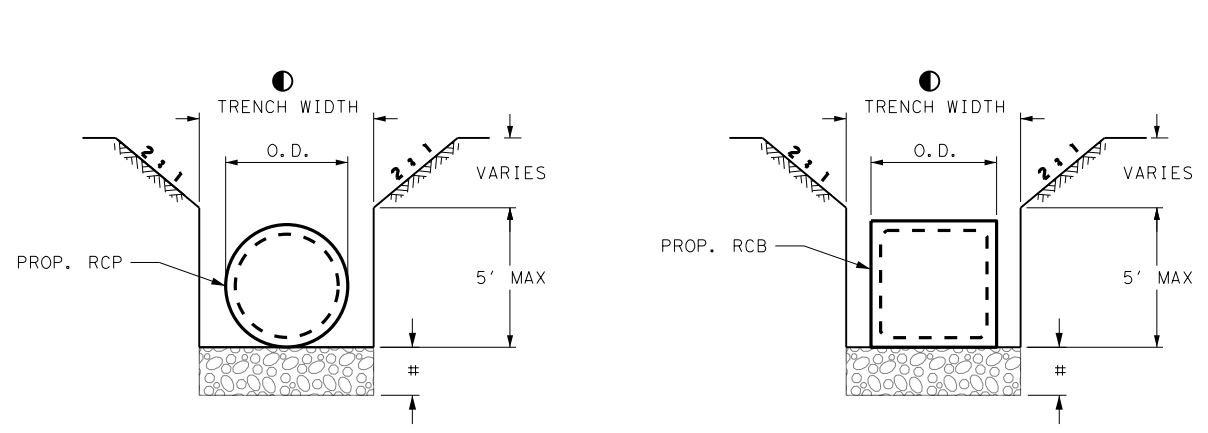
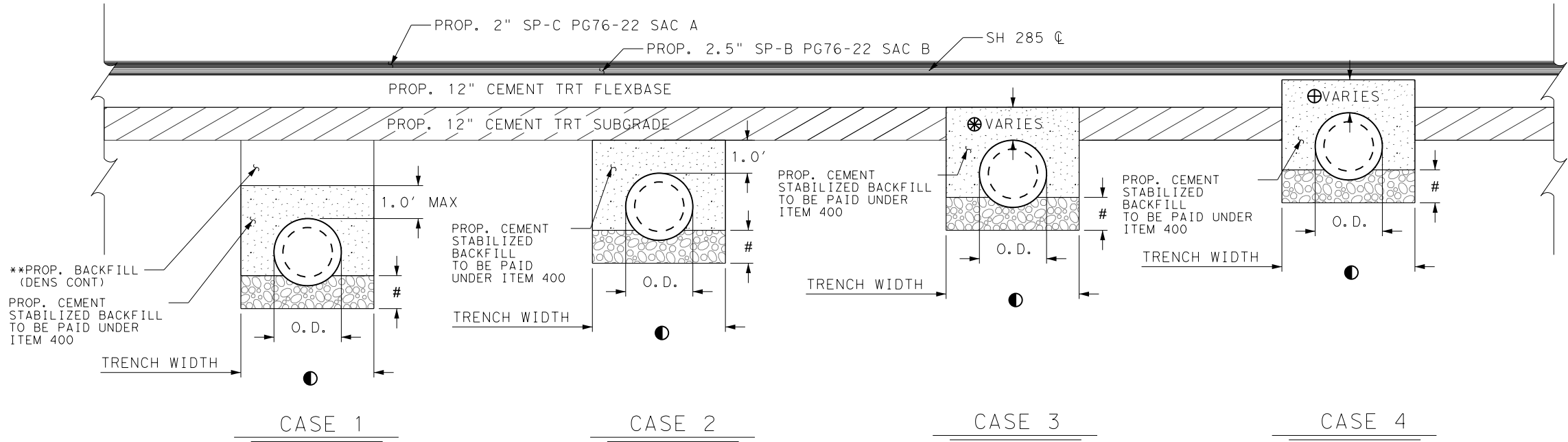
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	248

STATION	* STRUCT. EXCAV.	CEMENT STABIL. BACKFILL	STRUCT. EXCAV. (SPECIAL)	TRENCH EXCAV. PROTEC.	RIPRAP (CONC) (5IN)	CONCRETE BOX CULVERT	RC PIPE (CL IV) (24 IN) (SPL)	WING WALL (FW-0) (HW-3FT)	SET (TY II) (24 IN) (RCP) (4:1) (C)	SET (TY II) (24 IN) (RCP) (5:1) (C)
573+99	304	96	59	121	55	208	-	2	-	-
606+22	234	84	53	-	109	184	-	2	-	-

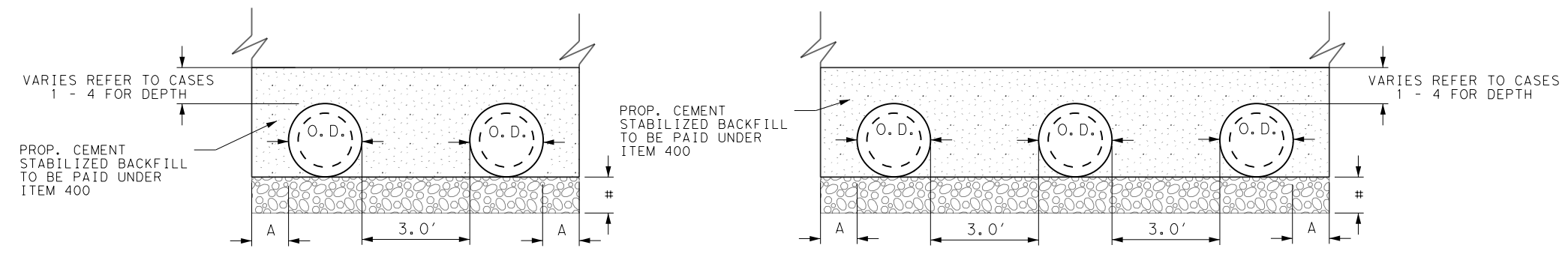
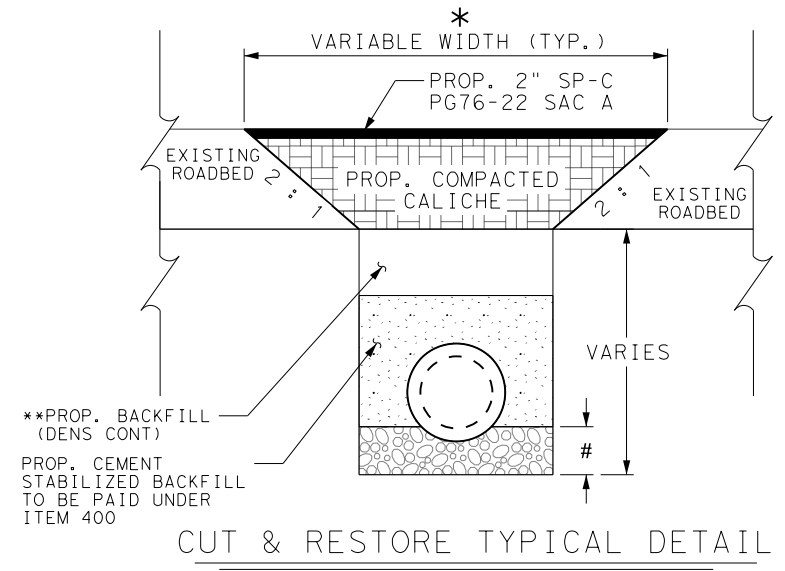
DATE: 2/12/2024 8:10:51 AM
 FILE: \\phr\dot\project\wisonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\Drainage Standards\SH 285 Cement Stabilized Det

NOTES:

- PROF. CEMENT STABILIZED BACKFILL SHALL EXTEND 2.0' BEYOND THE OUTSIDE EDGE OF THE PROF. PAVEMENT.
- * REFER TO TRAFFIC CONTROL PLAN FOR CUT & RESTORE ESTIMATED QUANTITIES.
- # 12" PROP. GRAVEL BEDDING, TO BE PAID UNDER ITEM 400 "STRUCT EXCAV. SPL"
- ⊕ PLACE CEMENT STABILIZED BACKFILL TO FINISH GRADE OF FLEXBASE.
- ⊗ PLACE CEMENT STABILIZED BACKFILL TO FINISH GRADE OF SUBGRADE.
- FOR 42" RCP OR LESS (O.D. + 2 FT)
 FOR 48" RCP OR GREATER (O.D. + 4 FT)
- "O.D." DENOTES "OUTER DIAMETER"
- ** PROP. BACKFILL TO BE PAID SUBSIDIARY TO PERTINENT BID ITEMS PER ITEM 400 SPECIFICATIONS.



TRENCH EXCAVATION TYPICAL DETAILS FOR RCP & RCB
 FOR TRENCH DEPTHS EQUAL TO OR GREATER THAN 5 FT



TYPICAL CEMENT STABILIZED BACKFILL DETAIL FOR MULTIPLE BARREL CULVERTS

A = 1.0 FT FOR 42" RCP OR LESS
 2.0 FT FOR 48" RCP OR GREATER

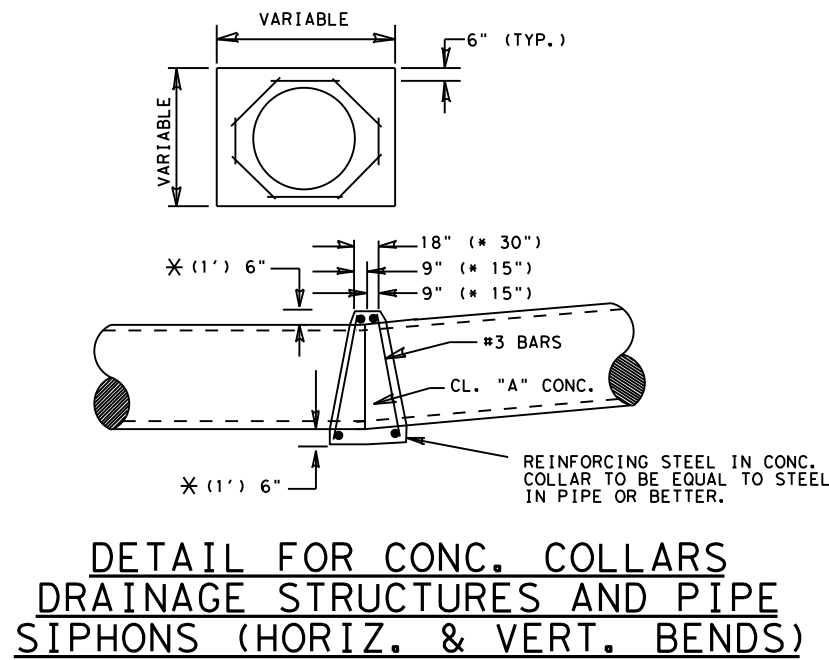
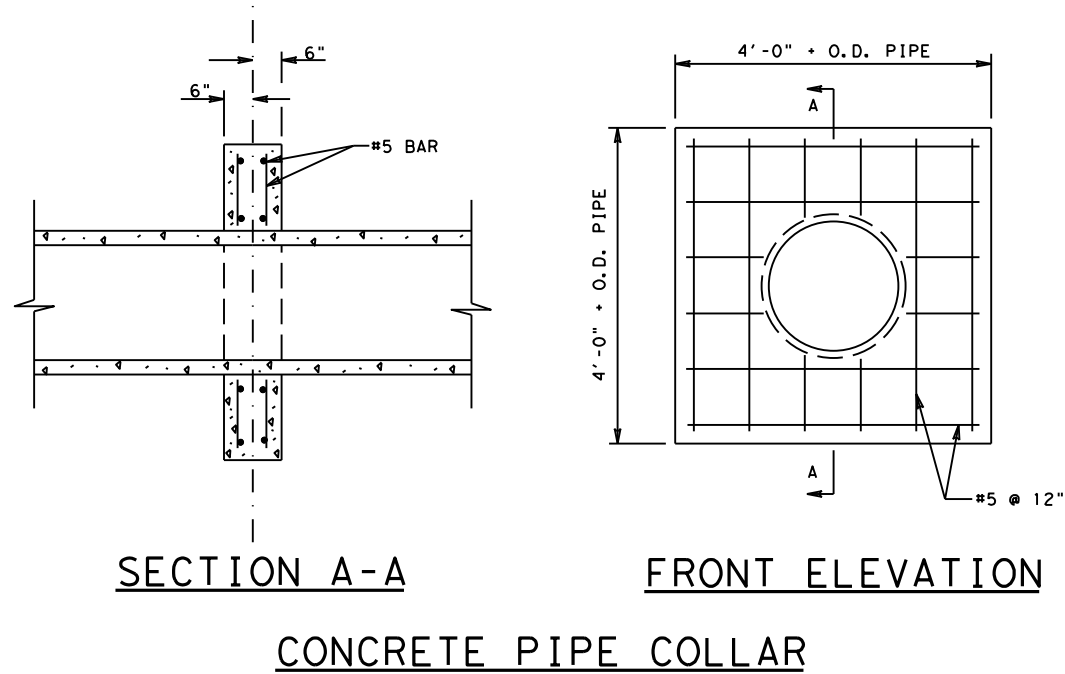
Pharr District Central Design

Texas Department of Transportation

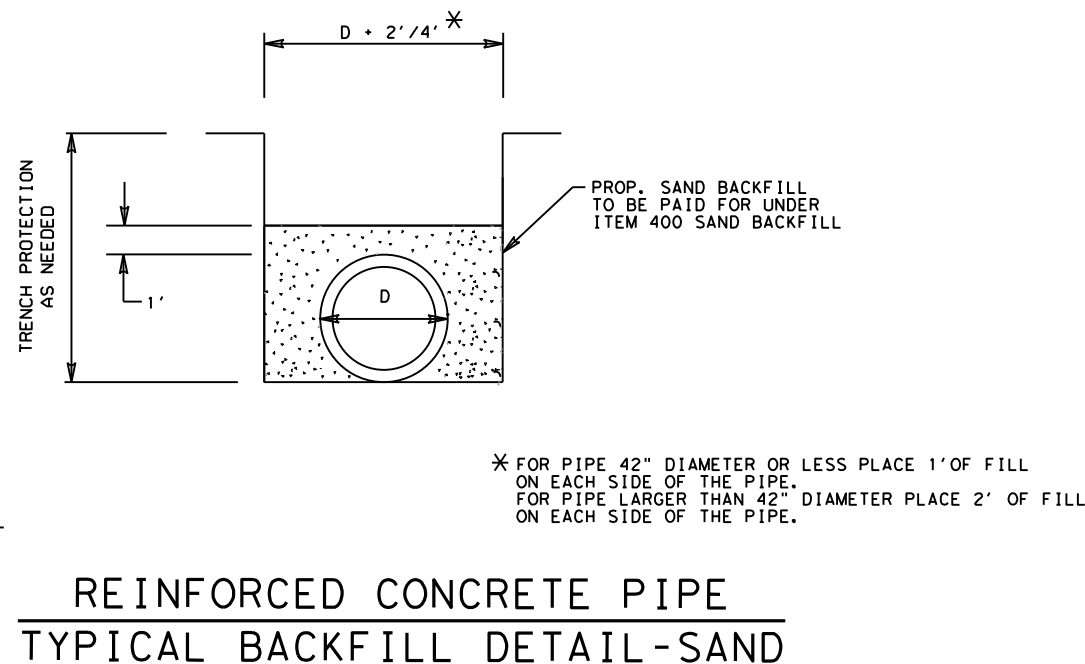
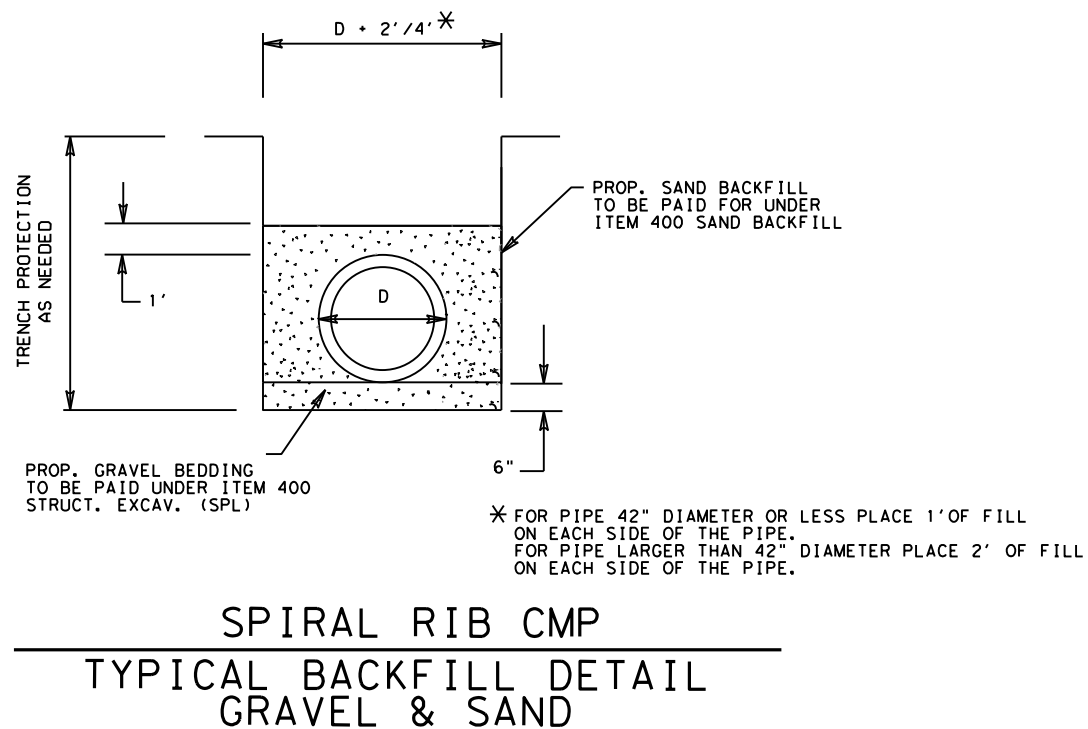
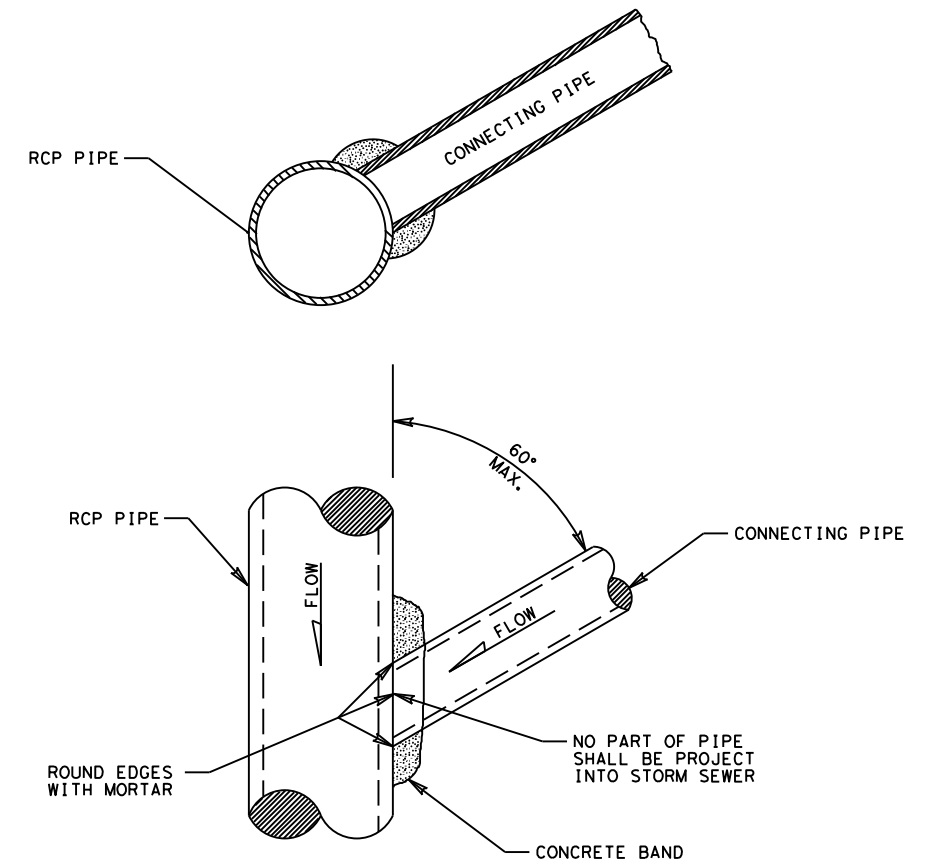
**SH 285
 CEMENT STABILIZED
 BACKFILL DETAILS**

N. T. S.		SHEET 1 OF 1		
DS: 0482	CONT: 01	SECT: 036, etc.	JOB: SH 285	HIGHWAY: SH 285
DW: PHR	CR: JIM HOGG	DIST: COUNTY	SHEET NO. 249	

FILE: \\txdot\project\wisson\line.com\TXDOT\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standards\Drainage Standards\MISC. PIPE.dgn
 DATE: 2/12/2024 8:10:55 AM



NOTE: PROP. CONC. COLLAR WILL NOT BE PAID FOR DIRECTLY BUT WILL BE SUBSIDIARY TO THE BIDS ITEMS INVOLVED.
 * FOR 42" DIAMETER AND LARGER PIPE



© TXDOT 2014 PHARR DISTRICT STANDARD

TEXAS DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS
 PIPE DETAILS

REV. 8/14 COLLAR.DGN

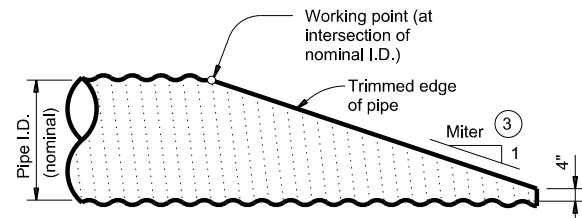
FED. RD. DIST. NO.	FEDERAL AID PROJECT NO.	FILE NO.	SHEET NO.
6			250
STATE	STATE DIST. NO.	COUNTY	CONT. SECT. JOB HIGHWAY NO.
TEXAS	21	JIM HOGG	0482 01 036, etc. SH 285

DATE: 2/12/2024 8:11:03 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design\Projects\Standard Drainage Standards\setpdse-20.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or for the accuracy or completeness of the information provided.

CROSS PIPE LENGTHS AND PIPE RUNNER LENGTHS

① ②

Nominal Culvert I.D.	Pipe Culvert Spa ~ G	Cross Pipe Length	Pipe Runner Length											
			3:1 Side Slope				4:1 Side Slope				6:1 Side Slope			
			0° Skew	15° Skew	30° Skew	45° Skew	0° Skew	15° Skew	30° Skew	45° Skew	0° Skew	15° Skew	30° Skew	45° Skew
24"	1' - 7"	3' - 5"	N/A	N/A	N/A	5' - 10"	N/A	N/A	N/A	8' - 1"	N/A	N/A	N/A	12' - 9"
27"	1' - 8"	3' - 8"	N/A	N/A	5' - 5"	6' - 11"	N/A	N/A	7' - 7"	9' - 7"	N/A	N/A	11' - 11"	14' - 11"
30"	1' - 10"	3' - 11"	N/A	N/A	6' - 4"	8' - 0"	N/A	N/A	8' - 9"	11' - 0"	N/A	N/A	13' - 8"	17' - 0"
33"	1' - 11"	4' - 2"	6' - 2"	6' - 5"	7' - 3"	9' - 1"	8' - 6"	8' - 10"	10' - 0"	12' - 5"	13' - 3"	13' - 9"	15' - 5"	19' - 2"
36"	2' - 1"	4' - 5"	6' - 11"	7' - 3"	8' - 2"	10' - 2"	9' - 6"	9' - 11"	11' - 2"	13' - 10"	14' - 9"	15' - 3"	17' - 2"	21' - 3"
42"	2' - 4"	4' - 11"	8' - 6"	8' - 10"	9' - 11"	12' - 4"	11' - 7"	12' - 0"	13' - 6"	16' - 8"	17' - 9"	18' - 5"	20' - 8"	25' - 7"
48"	2' - 7"	5' - 5"	10' - 1"	10' - 5"	11' - 9"	N/A	13' - 7"	14' - 2"	15' - 10"	N/A	20' - 9"	21' - 6"	24' - 2"	N/A
54"	3' - 0"	5' - 11"	11' - 8"	12' - 1"	N/A	N/A	15' - 8"	16' - 3"	N/A	N/A	23' - 10"	24' - 8"	N/A	N/A
60"	3' - 3"	6' - 5"	13' - 3"	N/A	N/A	N/A	17' - 9"	N/A	N/A	N/A	26' - 10"	N/A	N/A	N/A



NOTE: All pipe runners, calculations, and dimensions are based on the pipe culverts mitered as shown in this detail. Alternate styles of mitered ends will require that appropriate adjustments be made to the values presented on this standard.

SIDE ELEVATION OF TYPICAL PIPE CULVERT MITER

(Showing corrugated metal pipe (CMP) culvert. Details of reinforced concrete pipe (RCP) culvert are similar.)

TYPICAL PIPE CULVERT MITERS

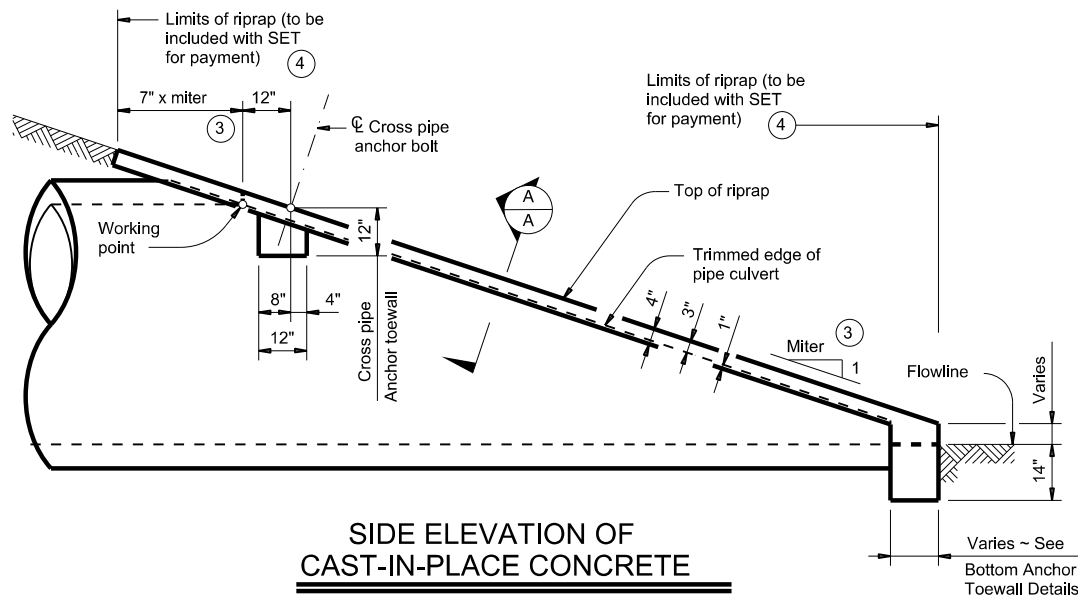
CONDITIONS WHERE PIPE RUNNERS ARE NOT REQUIRED

STANDARD PIPE SIZES AND MAX PIPE RUNNER LENGTHS

Side Slope	0° Skew	15° Skew	30° Skew	45° Skew
3:1	3:1	3.106:1	3.464:1	4.243:1
4:1	4:1	4.141:1	4.619:1	5.657:1
6:1	6:1	6.212:1	6.928:1	8.485:1

Nominal Culvert I.D.	Single Pipe Culvert	Multiple Pipe Culverts
12" thru 21"	Skews thru 45°	Skews thru 45°
24"	Skews thru 45°	Skews thru 30°
27"	Skews thru 30°	Skews thru 15°
30"	Skews thru 15°	Skews thru 15°
33"	Skews thru 15°	Always required
36"	Normal (no skew)	Always required
42" thru 60"	Always required	Always required

Pipe Size	Pipe O.D.	Pipe I.D.	Max Pipe Runner Length
2" STD	2.375"	2.067"	N/A
3" STD	3.500"	3.068"	10' - 0"
4" STD	4.500"	4.026"	19' - 8"
5" STD	5.563"	5.047"	34' - 2"

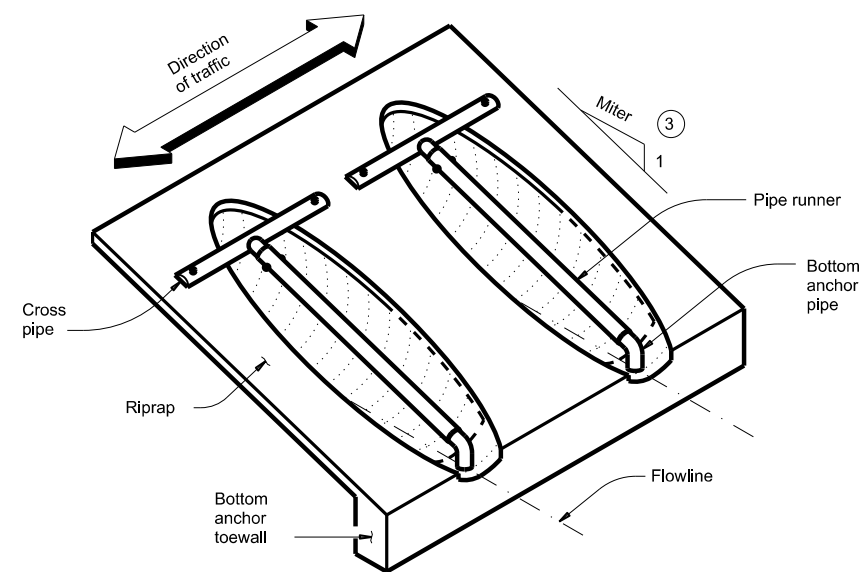


SIDE ELEVATION OF CAST-IN-PLACE CONCRETE

(Showing reinforced concrete pipe (RCP) culvert. Details of corrugated metal pipe (CMP) culvert are similar. Pipe runners not shown for clarity)

ESTIMATED CONCRETE RIPRAP QUANTITIES (CY)

Nominal Culvert I.D.	3:1 Side Slope				4:1 Side Slope				6:1 Side Slope			
	0° Skew	15° Skew	30° Skew	45° Skew	0° Skew	15° Skew	30° Skew	45° Skew	0° Skew	15° Skew	30° Skew	45° Skew
12"	0.4	0.4	0.5	0.5	0.5	0.5	0.5	0.6	0.7	0.7	0.7	0.8
15"	0.5	0.5	0.5	0.6	0.6	0.6	0.6	0.7	0.7	0.7	0.8	0.9
18"	0.5	0.5	0.6	0.6	0.6	0.7	0.7	0.8	0.8	0.8	0.9	1.0
21"	0.6	0.6	0.6	0.7	0.7	0.7	0.8	0.9	0.9	0.9	1.0	1.2
24"	0.6	0.7	0.7	0.8	0.8	0.8	0.8	1.0	1.0	1.0	1.1	1.3
27"	0.7	0.7	0.8	0.9	0.8	0.9	0.9	1.1	1.1	1.1	1.2	1.4
30"	0.8	0.8	0.8	0.9	0.9	0.9	1.0	1.2	1.2	1.2	1.3	1.6
33"	0.8	0.8	0.9	1.0	1.0	1.0	1.1	1.3	1.3	1.4	1.5	1.7
36"	0.9	0.9	0.9	1.1	1.1	1.1	1.2	1.4	1.4	1.5	1.6	1.8
42"	1.0	1.0	1.1	1.3	1.2	1.3	1.3	1.6	1.6	1.7	1.8	2.1
48"	1.1	1.1	1.2	N/A	1.4	1.4	1.5	N/A	1.9	1.9	2.1	N/A
54"	1.3	1.3	N/A	N/A	1.6	1.6	N/A	N/A	2.1	2.1	N/A	N/A
60"	1.4	N/A	N/A	N/A	1.7	N/A	N/A	N/A	2.3	N/A	N/A	N/A



ISOMETRIC VIEW OF TYPICAL INSTALLATION

(Showing installation with no skew.)

① Provide pipe runner of the size shown in the tables. Provide cross pipe of the same size as the pipe runner. Provide cross pipe stub out and bottom anchor pipe of the next smaller size pipe as shown in the Standard Pipe Sizes and Max Pipe Runner Lengths table.

② This standard allows for the placement of only one pipe runner across each culvert pipe opening. In order to limit the clear opening to be traversed by an errant vehicle, the following conditions must be met:

For 60" culvert pipes, the skew must not exceed 0°.
 For 54" culvert pipes, the skew must not exceed 15°.
 For 48" culvert pipes, the skew must not exceed 30°.
 For all culvert pipe sizes 42" and less, the skew must not exceed 45°.

If the above conditions cannot be met, the designer should consider using a safety end treatment with flared wings. For further information, refer to the TxDOT Roadway Design Manual.

③ Miter = slope of mitered end of pipe culvert.

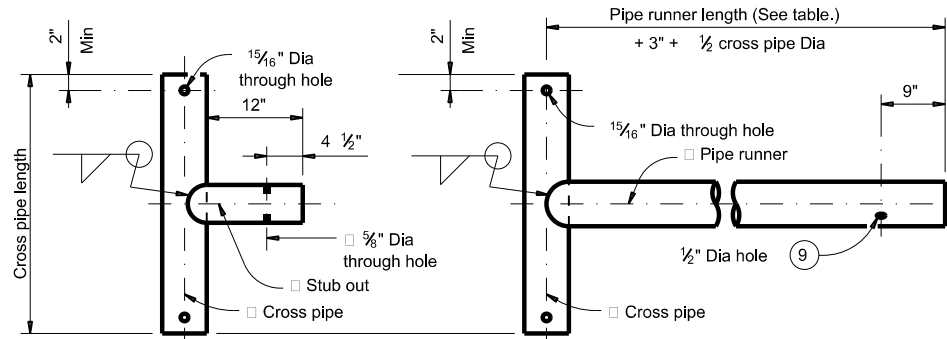
④ Riprap placed beyond the limits shown will be paid for as concrete riprap in accordance with Item 432, "Riprap".

⑤ Quantities shown are for one end of one reinforced concrete pipe (RCP) culvert. For multiple pipe culverts or for corrugated metal pipe (CMP) culverts, quantities will need to be adjusted. Riprap quantities are for Contractor's information only.

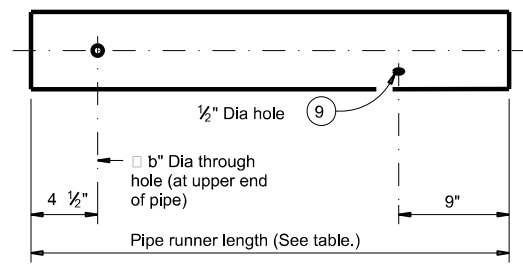
SHEET 1 OF 2

SAFETY END TREATMENT FOR 12" DIA TO 60" DIA PIPE CULVERTS TYPE II ~ CROSS DRAINAGE			
SETP-CD			
FILE: setpdse-20.dgn	DN: GAF	CK: CAT	DW: JRP
©TxDOT February 2020	CONT: 0482	SECT: 01	JOB: 036, etc.
REVISIONS:	DIST: PHR	COUNTY: JIM HOGG	SHEET NO.: 252

DATE: 2/12/2024 8:11:04 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design\Projects\Drainage Standards\setpcdse-20.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to any other format or for any errors or omissions.

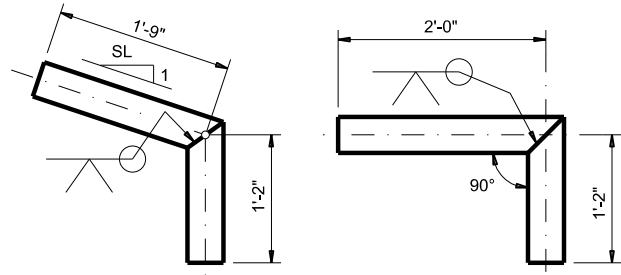


OPTION A1
 OPTION A2
CROSS PIPE AND CONNECTIONS DETAILS

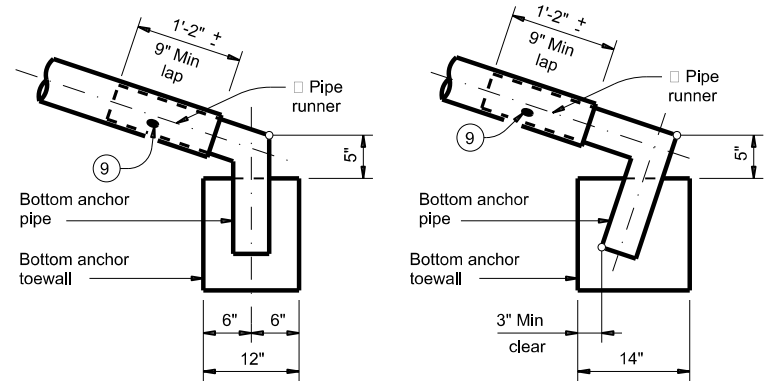


NOTE: The separate pipe runner shown is required when Cross Pipe Connection Option A1 is used.

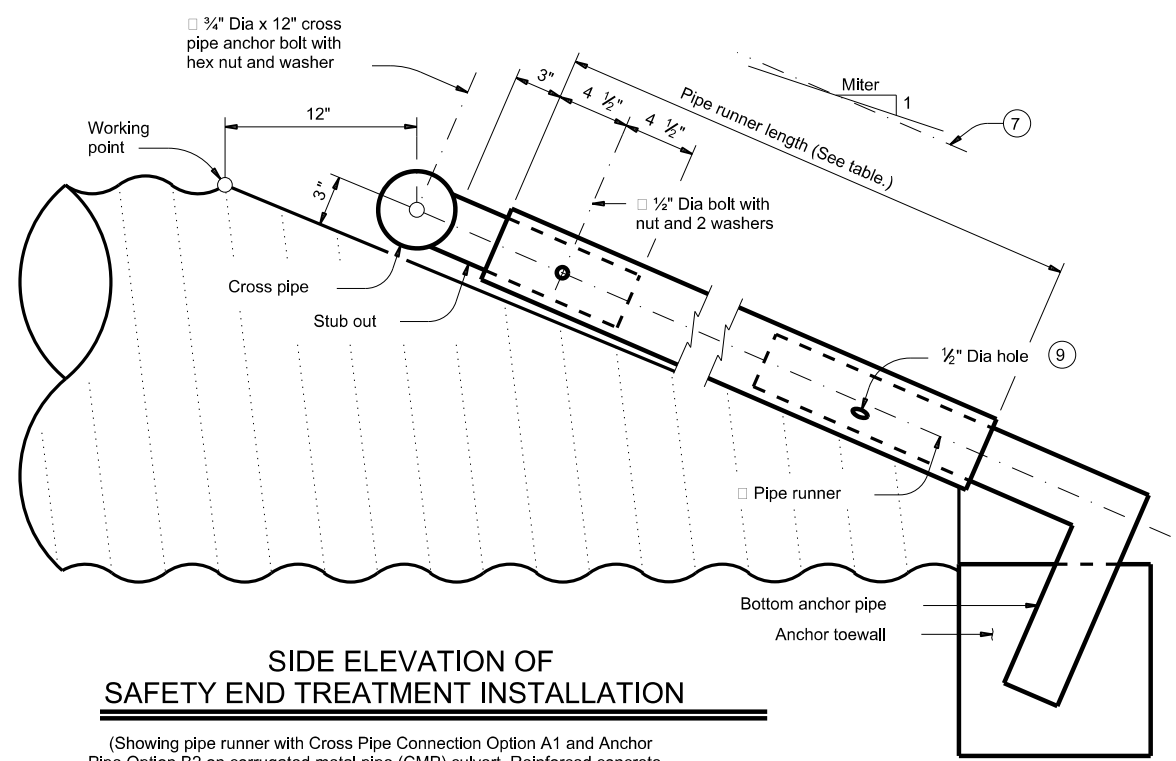
PIPE RUNNER DETAILS



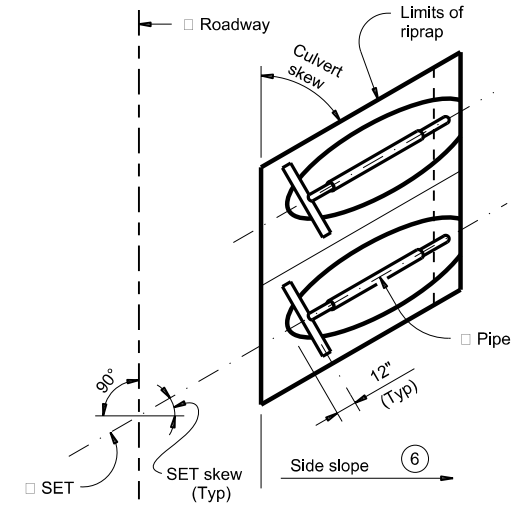
OPTION B1
 OPTION B2
BOTTOM ANCHOR PIPE DETAILS ⑩



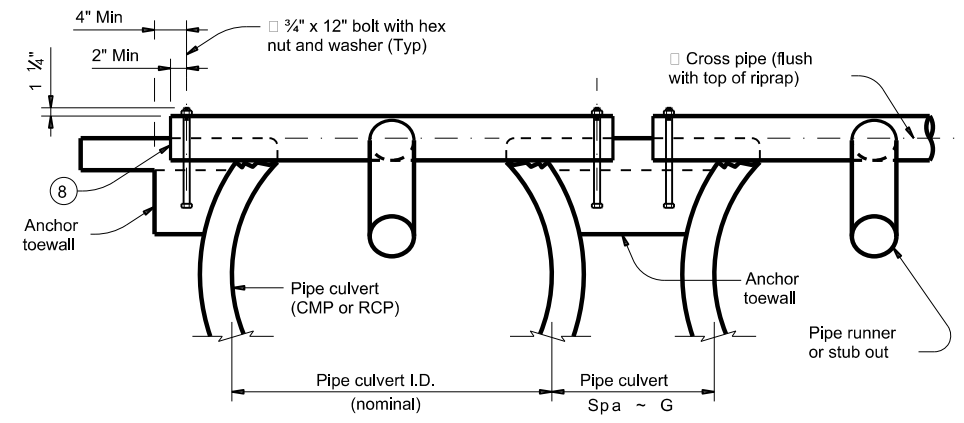
OPTION B1
 OPTION B2
BOTTOM ANCHOR TOEWALL DETAILS
 (Culvert and riprap not shown for clarity.)



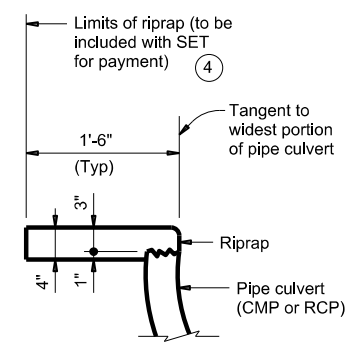
SIDE ELEVATION OF SAFETY END TREATMENT INSTALLATION
 (Showing pipe runner with Cross Pipe Connection Option A1 and Anchor Pipe Option B2 on corrugated metal pipe (CMP) culvert. Reinforced concrete pipe culvert (RCP) details are similar. Riprap not shown for clarity.)



PLAN OF SKEWED INSTALLATION



SECTION A-A
 SHOWING CROSS PIPE AND ANCHOR TOEWALL



SHOWING TYPICAL PIPE CULVERT AND RIPRAP

- ④ Riprap placed beyond the limits shown will be paid for as concrete riprap in accordance with Item 432, "Riprap".
- ⑥ Recommended values of side slope are 3:1, 4:1, and 6:1. All quantities, calculations, and dimensions shown herein are based on these recommended values. Slope of 3:1 or flatter is required for vehicle safety.
- ⑦ Note that actual slope of pipe runner may vary slightly from side slope of riprap and trimmed culvert pipe edge.
- ⑧ Ensure that riprap concrete does not flow into the cross pipe so as to permit disassembly of the bolted connection to allow cleanout access.
- ⑨ After installation, inspect the 1/2 inch hole to ensure that the lap of the pipe runner with the bottom anchor pipe is adequate.
- ⑩ At fabricator's option, a heat bend to a smooth 5" radius or a manufactured elbow (of the same material as the runner) may be substituted for the mitered and welded joint in the bottom anchor pipe.

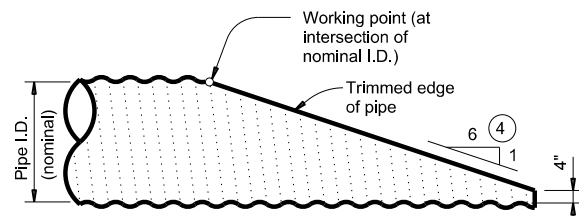
MATERIAL NOTES:
 Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise.
 Provide pipe runners, cross pipes, and anchor pipes conforming to the requirements of ASTM A53 (Type E or S, Gr B), ASTM A500 Gr B, or API 5LX52.
 Provide ASTM A307 bolts and nuts.
 Galvanize all steel components, except concrete reinforcing, after fabrication.
 Repair galvanizing damaged during transport or construction in accordance with the specifications.

GENERAL NOTES:
 Pipe runners are designed for a traversing load of 1,800 pounds at yield as recommended by Research Report 280-1, "Safety Treatment of Roadside Cross-Drainage Structures", Texas Transportation Institute, March 1981.
 Safety end treatments (SET) shown herein are intended for use in those installations where out of control vehicles are likely to traverse the openings approximately perpendicular to the pipe runners.
 Payment for riprap and toewall is included in the price bid for each safety end treatment.
 Construct concrete riprap and all necessary inverts in accordance with the requirements of Item 432, "Riprap".

SHEET 2 OF 2

		Bridge Division Standard	
SAFETY END TREATMENT FOR 12" DIA TO 60" DIA PIPE CULVERTS TYPE II ~ CROSS DRAINAGE			
SETP-CD			
FILE: setpcdse-20.dgn	DN: GAF	CK: CAT	DW: JRP
©TxDOT February 2020	CONT: 0482	SECT: 01	JOB: 036, etc.
REVISIONS	DIST: PHR	COUNTY: JIM HOGG	SH 285 SHEET NO. 253

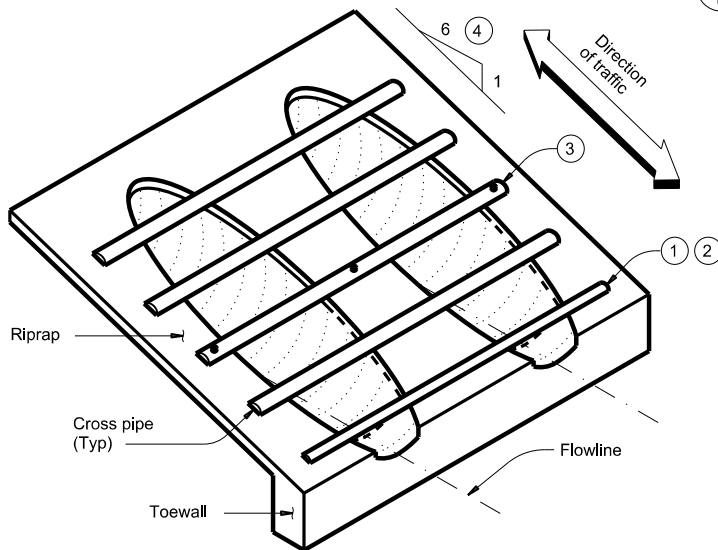
DATE: 2/12/2024 8:11:09 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOT\Documents\21 - PHR\Design\Projects\Drainage Standards\setpdse-20.dgn
 PHR\Design\Projects\Drainage Standards\setpdse-20.dgn
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units.



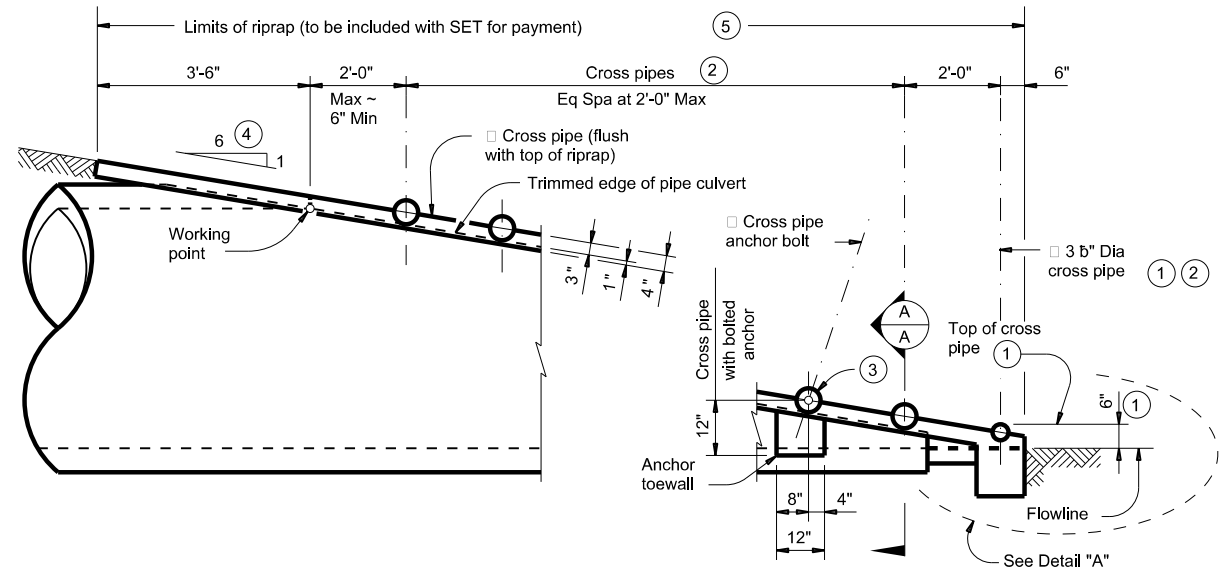
NOTE: All cross pipes, calculations, and dimensions are based on the pipe culverts mitered as shown in this detail. Alternate styles of mitered ends will require that appropriate adjustments be made to the values presented on this standard.

SIDE ELEVATION OF TYPICAL PIPE CULVERT MITER

(Showing corrugated metal pipe (CMP) culvert. Details at reinforced concrete pipe (RCP) culvert are similar.)

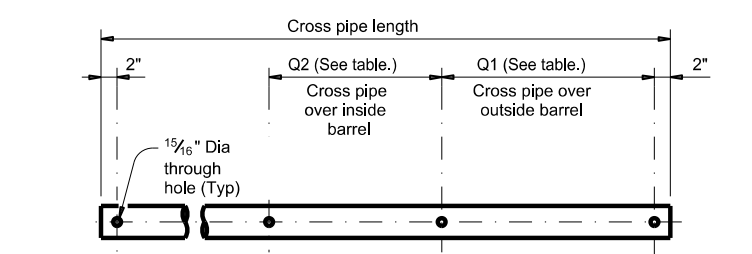


ISOMETRIC VIEW OF TYPICAL INSTALLATION

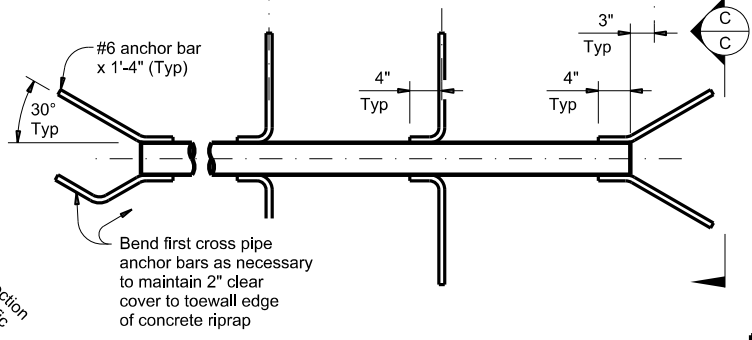


SIDE ELEVATION OF CAST-IN-PLACE CONCRETE

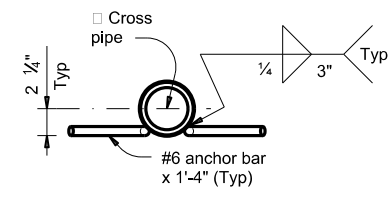
(Showing reinforced concrete pipe (RCP) culvert. Details at corrugated metal pipe (CMP) culvert are similar.)



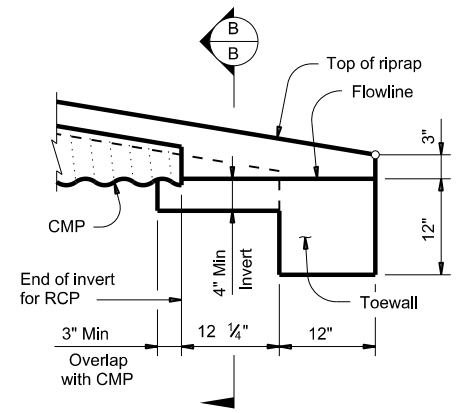
PIPE WITH BOLTED ANCHOR



PIPE WITH ANCHOR BARS

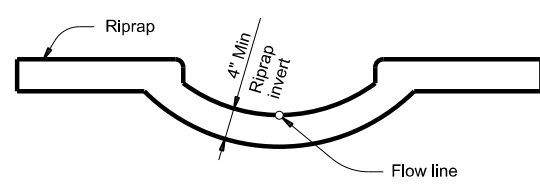


CROSS PIPE DETAILS



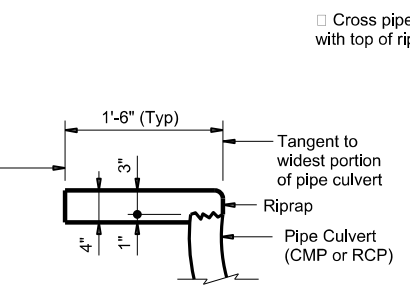
DETAIL "A"

(Showing invert with corrugated metal pipe (CMP) culvert. Reinforced concrete pipe (RCP) culvert details are similar. Cross pipes not shown for clarity.)

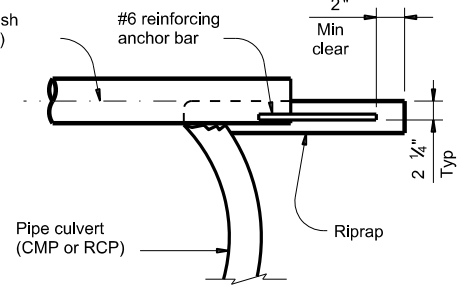


SECTION B-B

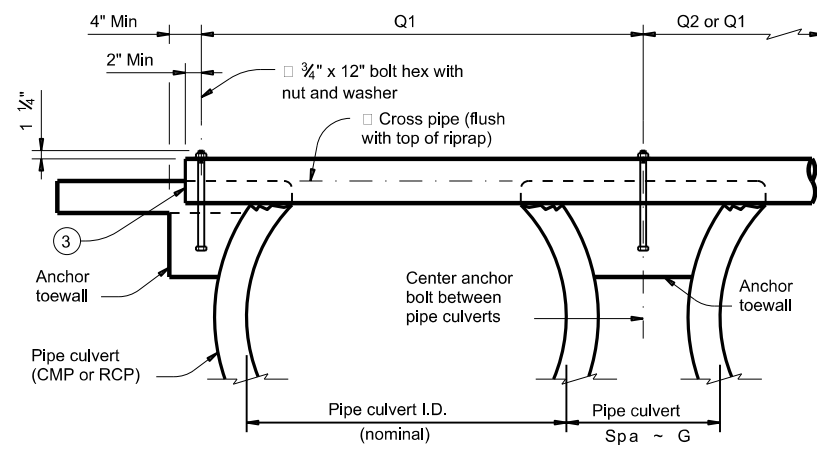
Limits of riprap (to be included with SET for payment)



SHOWING TYPICAL PIPE CULVERT AND RIPRAP



SHOWING CROSS PIPE WITH ANCHOR BAR



SHOWING CROSS PIPE WITH BOLTED ANCHOR

SECTION A-A

CROSS PIPE LENGTHS, REQUIRED PIPE SIZES, AND RIPRAP QUANTITIES

Nominal Culvert I.D.	Conc Riprap (CY) (6)	Pipe Culvert Spa ~ G	Single Barrel ~ Q1	Multi-Barrel ~ Q1	Q2	Conditions for Use of Cross Pipes	Cross Pipe Sizes
12"	0.6	0' - 9"	N/A	2' - 1"	1' - 9"	3 or more pipe culverts	3" Std (3.500" O.D.)
15"	0.7	0' - 11"	N/A	2' - 5"	2' - 2"		
18"	0.8	1' - 2"	N/A	2' - 10"	2' - 8"		
21"	0.9	1' - 4"	N/A	3' - 2"	3' - 1"	3 or more pipe culverts	3 1/2" Std (4.000" O.D.)
24"	0.9	1' - 7"	N/A	3' - 6"	3' - 7"		
27"	1.0	1' - 8"	N/A	3' - 10"	3' - 11"	2 or more pipe culverts	3 1/2" Std (4.000" O.D.)
30"	1.1	1' - 10"	N/A	4' - 2"	4' - 4"	All pipe culverts	
33"	1.2	1' - 11"	4' - 2"	4' - 5"	4' - 8"	All pipe culverts	4" Std (4.500" O.D.)
36"	1.3	2' - 1"	4' - 5"	4' - 9"	5' - 1"		
42"	1.5	2' - 4"	4' - 11"	5' - 5"	5' - 10"	All pipe culverts	5" Std (5.563" O.D.)
48"	1.7	2' - 7"	5' - 5"	6' - 0"	6' - 7"		
54"	2.0	3' - 0"	5' - 11"	6' - 9"	7' - 6"	All pipe culverts	5" Std (5.563" O.D.)
60"	2.2	3' - 3"	6' - 5"	7' - 4"	8' - 3"		
66"	2.4	3' - 3"	6' - 11"	7' - 10"	8' - 9"		
72"	2.7	3' - 4"	7' - 5"	8' - 5"	9' - 4"		

- The proper installation of the first cross pipe is critical for vehicle safety. Place the top of the first cross pipe no more than 6" above the flow line.
- Provide cross pipes, except the first bottom pipe, of the size shown in the table. Provide a 3 1/2" standard pipe (4" O.D.) for the first bottom pipe.
- Install the third cross pipe from the bottom of the culvert using a bolted connection. Ensure that riprap concrete does not flow into the cross pipe so as to permit disassembly of the bolted connection to allow cleanout access. At the Contractor's option, install all other cross pipes using the bolted connection details.
- Match cross slope as shown elsewhere in the plans. Cross slope of 6:1 or flatter is required for vehicle safety.
- Riprap placed beyond the limits shown will be paid for as concrete riprap in accordance with Item 432, "Riprap".
- Quantities shown are for one end of one reinforced concrete pipe (RCP) culvert. For multiple pipe culverts or for corrugated metal pipe (CMP) culverts, quantities will need to be adjusted. Riprap quantities are for contractor's information only.

MATERIAL NOTES:

Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise. Provide cross pipes that meet the requirements of ASTM A53 (Type E or S, Gr B), ASTM A500 (Gr B), or API 5LX52. Provide ASTM A307 bolts and nuts. Galvanize all steel components, except concrete reinforcing, after fabrication. Repair galvanizing damaged during transport or construction in accordance with the specifications.

GENERAL NOTES:

Cross pipes are designed for a traversing load of 10,000 pounds at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981. Safety end treatments (SET) shown herein are intended for use in those installations where out of control vehicles are likely to traverse the openings approximately perpendicular to the cross pipes. Construct concrete riprap and all necessary inverts in accordance with the requirements of Item 432, "Riprap". Payment for riprap and toewall is included in the Price Bid for each Safety End Treatment.

Texas Department of Transportation
Bridge Division Standard

SAFETY END TREATMENT

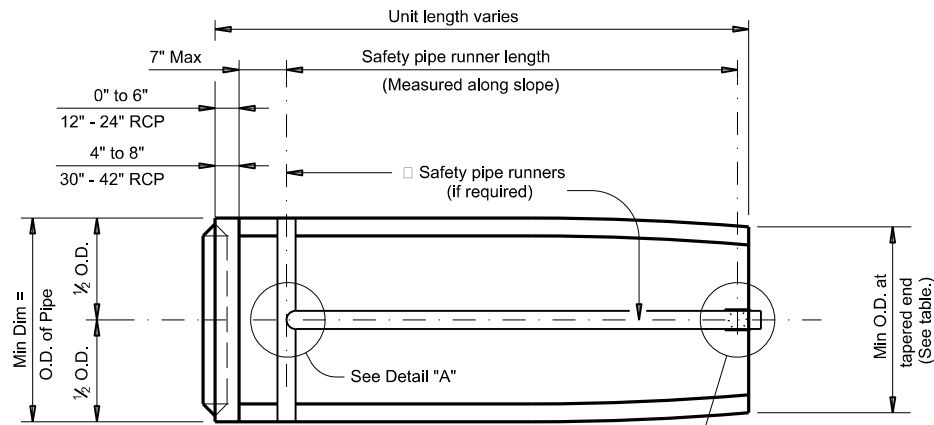
FOR 12" DIA TO 72" DIA PIPE CULVERTS

TYPE II ~ PARALLEL DRAINAGE

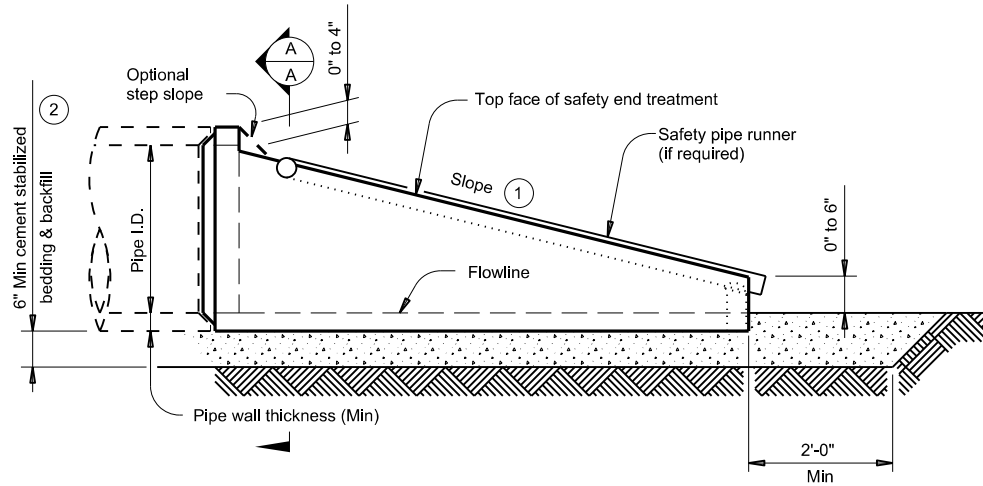
SETP-PD

FILE: setpdse-20.dgn	DN: GAF	CK: CAT	DW: JRP	CK: GAF
©TxDOT February 2020	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.		
PHR	JIM HOGG			254

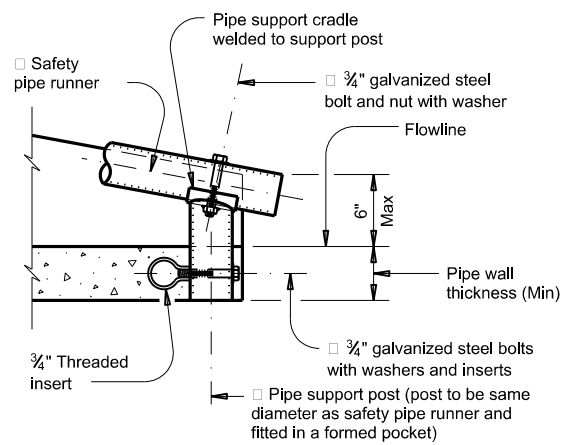
DATE: 2/12/2024 8:11:13 AM
 FILE: \\txdot\project\wison\line.com\TXDOT15\Documents\21 - PHR\Design\Projects\Standard\Drainage Standards\psetrcss-20.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein.



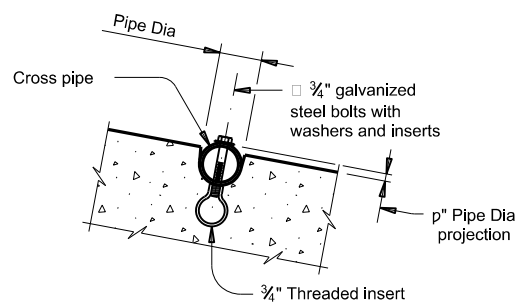
PLAN VIEW
(Showing spigot end connection.)



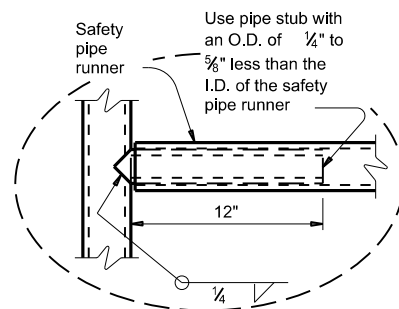
LONGITUDINAL ELEVATION
(Showing spigot end connection.)



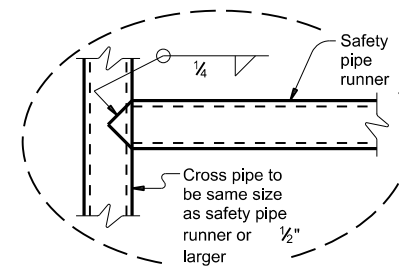
END DETAIL FOR INSTALLATION OF SAFETY PIPE RUNNERS
(If required)



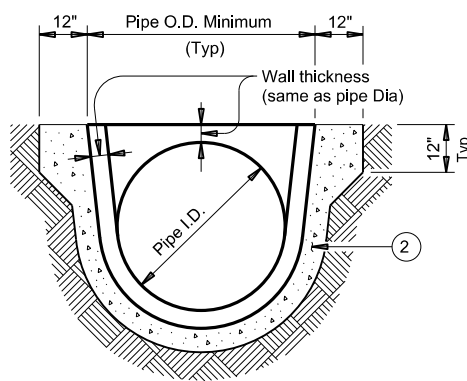
INSTALLATION DETAIL FOR SAFETY PIPE RUNNERS
(If required)



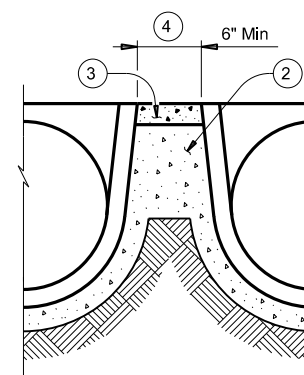
OPTION A



OPTION B



SECTION A-A



MULTIPLE PIPE INSTALLATION

MAX SAFETY PIPE RUNNER LENGTHS AND REQUIRED SAFETY PIPE RUNNER SIZES

Max Safety Pipe Runner Length	Required Pipe Runner Size		
	Pipe Size	Pipe O.D.	Pipe I.D.
11' - 2"	3" STD	3.500"	3.068"
15' - 6"	3 1/2" STD	4.000"	3.548"
20' - 10"	4" STD	4.500"	4.026"
35' - 4"	5" STD	5.563"	5.047"

- Slope as shown elsewhere in the plans. Slope of 3:1 or flatter is required for vehicle safety.
- Provide cement stabilized bedding and backfill in accordance with the Item, "Excavation and Backfill for Structures". Bedding and backfill is considered subsidiary to the Item "Safety End Treatment". When concrete riprap is specified around the safety end treatment, backfill as directed by Engineer.
- Fill the top 4" of void between precast end treatments with concrete riprap. Concrete riprap be considered subsidiary to the Item "Safety End Treatment".
- Adjust clear distance between pipes to provide for the minimum distance between safety end treatments.

REQUIREMENTS FOR CULVERT PIPES AND SAFETY PIPE RUNNERS

Pipe I.D.	Min Wall Thickness	Min O.D.	Min O.D. at Tapered End	Min Reinf Requirements (sq. in. / ft. of pipe)	Slope	Minimum Length of Unit	Single Pipe		Multiple Pipe		
							Skew	Pipe Runners Required	Skew	Pipe Runners Required	
12"	2"	16"	16"	0.07 Circ.	3:1	2' - 0"	≤ 45°	No	≤ 45°	No	
						4:1					2' - 8"
						6:1					4' - 0"
15"	2 1/4"	19 1/2"	19"	0.07 Circ.	3:1	2' - 10"	≤ 45°	No	≤ 45°	No	
						4:1					3' - 9"
						6:1					5' - 8"
18"	2 1/2"	23"	21 1/2"	0.07 Circ.	3:1	3' - 8"	≤ 45°	No	≤ 45°	No	
						4:1					4' - 10"
						6:1					7' - 3"
24"	3"	30"	27"	0.07 Circ.	3:1	5' - 3"	≤ 45°	No	≤ 30°	No	
						4:1			7' - 0"	> 30°	Yes
						6:1			10' - 6"		
30"	3 1/2"	37"	31"	0.18 Circ.	3:1	6' - 3"	≤ 15°	No	≤ 15°	No	
						4:1			8' - 2"	> 15°	Yes
						6:1			12' - 1"		
36"	4"	44"	36"	0.19 Ellip.	3:1	7' - 10"	= 0°	No	≥ 0°	No	
						4:1			10' - 4"	> 0°	Yes
						6:1			15' - 4"		
42"	4 1/2"	51"	41 1/2"	0.23 Ellip.	3:1	9' - 6"	≥ 0°	Yes	≥ 0°	No	
						4:1			12' - 6"		
						6:1			18' - 7"		

MATERIAL NOTES:

Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise. Provide safety pipe runners, cross pipes, pipe support posts, and pipe stubs meeting the requirements of ASTM A53 (Type E or S, Gr B), ASTM A500 Gr B, or API 5LX52. Galvanize all steel components except reinforcing steel after fabrication. Repair galvanizing damaged during transport or construction in accordance with the specifications.

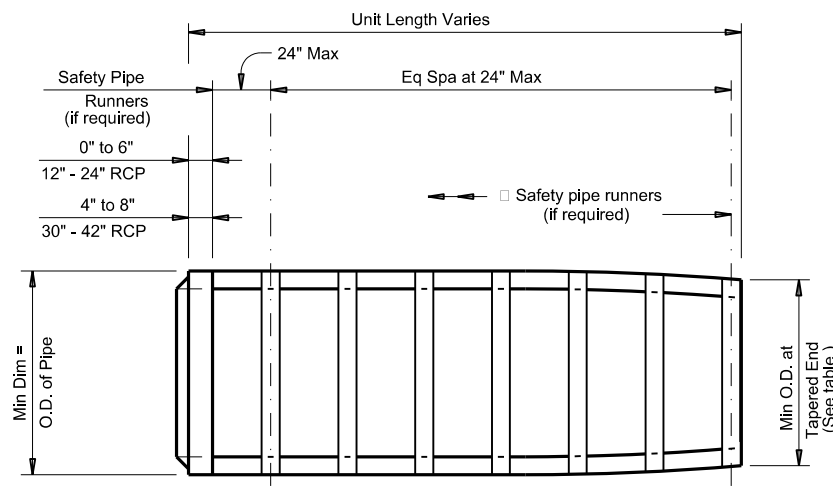
GENERAL NOTES:

Precast safety end treatment for reinforced concrete pipe (CRP) may be used for TYPE II end treatment as specified in Item 467, "Safety End Treatment". When precast safety end treatment is used as a Contractor's alternate to mitered RCP, riprap will not be required unless noted otherwise on the plans. Manufacture precast concrete end sections in accordance with Item 464, "Reinforced Concrete Pipe" and in accordance with ASTM Specification C-76, Class III, Wall B for circular pipe. Provide precast concrete end sections with a spigot or bell end for compatibility to upstream or downstream end conditions with sufficient annular space to allow for grout, mortar, cold applied asphalt joint compound or pre-formed plastic gasket material. Methods of lifting shall be provided by the manufacturer for ease of loading, unloading, and installation. Pipe runners are designed for a traversing load of 1,800 Lbs at yield as recommended by Research Report 280-1, "Safety Treatment of Roadside Cross-Drainage Structures", Texas Transportation Institute, March 1981.

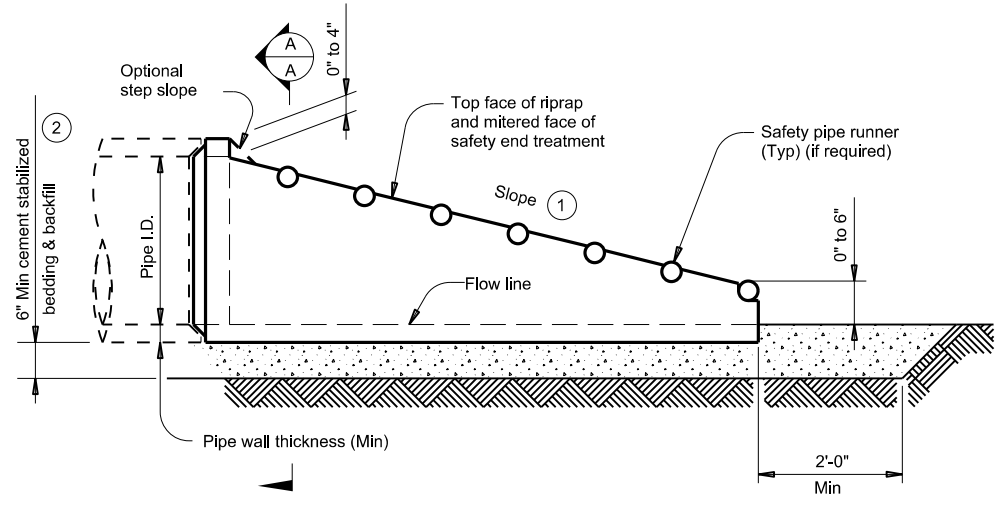
				Bridge Division Standard	
PRECAST SAFETY END TREATMENT TYPE II ~ CROSS DRAINAGE					
PSET-RC					
FILE:	psetrcss-20.dgn	DN:	RLW	CK:	KLR
©/TxDOT	February 2020	CON:	0482	SECT:	01
REVISIONS		JOB:	036, etc.		SH 285
		DIST:	PHR	COUNTY:	JIM HOGG
				SHEET NO.:	255

DISCLAIMER: This standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information herein. This drawing is a revision of a previous drawing.

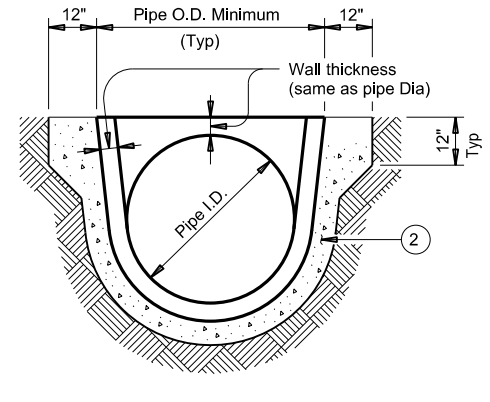
DATE: 2/12/2024 8:11:18 AM
 FILE: \\twdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Project\21-0201-02\Drawings\Standards\PsetRPSS-20.dgn



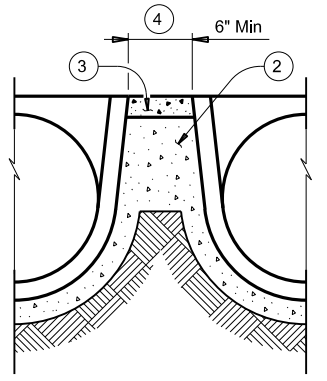
PLAN VIEW - 12" THRU 24"
(Showing spigot end connection.)



LONGITUDINAL ELEVATION - 12" THRU 24"
(Showing spigot end connection.)

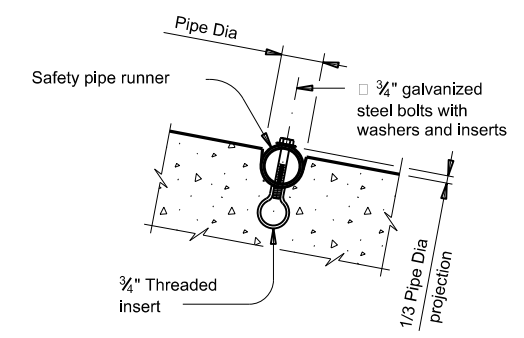


SECTION A-A

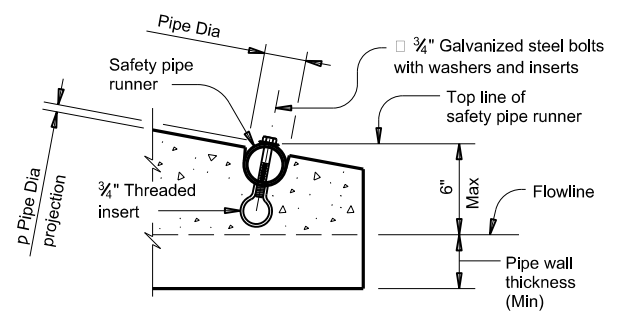


MULTIPLE PIPE INSTALLATION

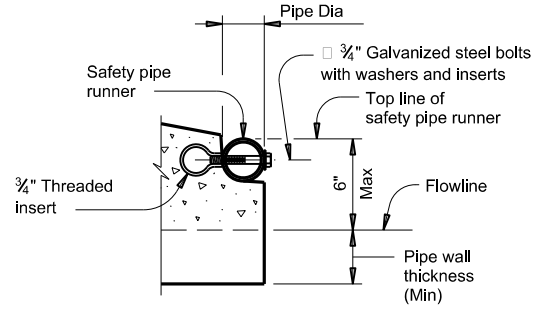
- 1 Slope as shown elsewhere in the plans. Slope of 6:1 or flatter is required for vehicle safety.
- 2 Provide cement stabilized bedding and backfill in accordance with the Item, "Excavation and Backfill for Structures". Bedding and backfill is considered subsidiary to the Item 467, "Safety End Treatment". When concrete riprap is specified around the safety end treatment, backfill as directed by Engineer.
- 3 Fill the top 4" of void between precast end treatments with concrete riprap. Concrete riprap is considered subsidiary to the Item 467, "Safety End Treatment".
- 4 Adjust clear distance between pipes to provide for the minimum distance between safety end treatments.
- 5 Safety pipe runners are required for multiple pipe culverts with more than two pipes.



INSTALLATION DETAIL FOR SAFETY PIPE RUNNERS
(If required)



OPTION A



OPTION B

END DETAILS FOR INSTALLATION OF SAFETY PIPE RUNNERS
(If required)

REQUIREMENTS FOR CULVERT PIPES AND SAFETY PIPE RUNNERS

Pipe I.D.	Min Wall Thickness	Min O.D.	Min O.D. at Tapered End	Min Reinf Requirements (sq. in. per ft. of Pipe)	Max Slope	Min Length of Unit	Pipe Runner Requirements		Required Pipe Runner Sizes		
							Single Pipe	Multiple Pipe	Nominal Dia	O.D.	I.D.
12"	2"	16"	16"	0.07 Circ.	6:1	4' - 0"	No	(5)	3" STD	3.500"	3.068"
15"	2 1/4"	19 1/2"	19"	0.07 Circ.	6:1	5' - 8"	No	(5)	3" STD	3.500"	3.068"
18"	2 1/2"	23"	21 1/2"	0.07 Circ.	6:1	7' - 3"	No	(5)	3" STD	3.500"	3.068"
24"	3"	30"	27"	0.07 Circ.	6:1	10' - 6"	No	(5)	3" STD	3.500"	3.068"
30"	3 1/2"	37"	31"	0.18 Circ.	6:1	12' - 1"	No	Yes	4" STD	4.500"	4.026"
36"	4"	44"	36"	0.19 Ellip.	6:1	15' - 4"	Yes	Yes	4" STD	4.500"	4.026"
42"	4 1/2"	51"	41 1/2"	0.23 Ellip.	6:1	18' - 7"	Yes	Yes	4" STD	4.500"	4.026"

MATERIAL NOTES:

Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise.

Provide pipe runners meeting the requirements of ASTM A53 (Type E or S, Gr B), ASTM A500 Gr B, or API 5LX52.

Galvanize steel components except reinforcing steel after fabrication. Repair galvanizing damaged during transport or construction in accordance with the specifications.

GENERAL NOTES:

Precast safety end treatment for reinforced concrete pipe (RCP) may be used for TYPE II end treatment as specified in Item 467, "Safety End Treatment".

When precast safety end treatment is used as a Contractor's alternate to mitered RCP, riprap will not be required unless noted otherwise on the plans.

Manufacture precast concrete end sections in accordance with Item 464, "Reinforced Concrete Pipe" and in accordance with ASTM Specification C-76, Class III, Wall B for circular pipe.

Provide precast concrete end sections with a spigot or bell end for compatibility to upstream or downstream end conditions with sufficient annular space to allow for grout, mortar, cold applied asphalt joint compound or pre-formed plastic gasket material.

Methods of lifting shall be provided by the manufacturer for ease of loading, unloading and installation.

Pipe runners are designed for a traversing load of 10,000 Lbs at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981.

Bridge Division Standard

PRECAST SAFETY END TREATMENT TYPE II ~ PARALLEL DRAINAGE

PSET-RP

FILE: psetrpss-20.dgn	DN: RLW	CK: KLR	DW: JTR	CK: GAF
©TxDOT February 2020		CONT	SECT	JOB
REVISIONS		0482 01	036, etc.	
		DIST	COUNTY	SHEET NO.
		PHR	JIM HOGG	256

DATE: 2/12/2024 8:11:23 AM
 FILE: \\txdot\project\wisonline.com\TXDOTS\Documents\21 - PHRDesign\Projects\042020\042020-036\042020-036-01\042020-036-01-01.dgn
 PROJECT: 042020-036-01-01.dgn
 DRAWING: 042020-036-01-01.dgn
 TITLE: Standards\fw-0std-20.dgn

TABLE OF DIMENSIONS AND REINFORCING STEEL
(Wings for one structure end)

Maximum Wingwall Height Hw	Dimensions				Variable Reinforcing				Estimated Quantities per ft of wing length (2-wings)	
	W	X	Y	Z	Bars J1		Bars J2		Reinf (Lb/Ft)	Conc (CY/Ft)
					Size	Spa	Size	Spa		
2'-6"	2'-5"	1'-0"	9"	7"	#4	1'-0"	#4	1'-0"	33.73	0.248
3'-0"	2'-5"	1'-0"	9"	7"	#4	1'-0"	#4	1'-0"	37.07	0.261
3'-6"	2'-5"	1'-0"	9"	7"	#4	1'-0"	#4	1'-0"	37.74	0.273
4'-0"	2'-5"	1'-0"	9"	7"	#4	1'-0"	#4	1'-0"	38.41	0.285
4'-6"	3'-2"	1'-6"	1'-0"	7"	#4	1'-0"	#4	1'-0"	41.75	0.330
5'-0"	3'-2"	1'-6"	1'-0"	7"	#4	1'-0"	#4	1'-0"	45.09	0.343
5'-6"	3'-2"	1'-6"	1'-0"	7"	#4	1'-0"	#4	1'-0"	45.75	0.355
6'-0"	3'-2"	1'-6"	1'-0"	7"	#4	1'-0"	#4	1'-0"	46.42	0.367
7'-0"	3'-8"	1'-9"	1'-3"	7"	#4	1'-0"	#4	1'-0"	52.77	0.414
8'-0"	4'-2"	2'-0"	1'-6"	8"	#5	1'-0"	#4	1'-0"	60.19	0.486
9'-0"	4'-8"	2'-3"	1'-9"	8"	#4	6"	#4	6"	81.49	0.535
10'-0"	5'-2"	2'-6"	2'-0"	8"	#5	6"	#4	6"	97.25	0.584
11'-0"	5'-8"	2'-9"	2'-3"	8"	#6	6"	#5	6"	133.65	0.634
12'-0"	6'-2"	3'-0"	2'-6"	9"	#7	6"	#5	6"	162.29	0.721
13'-0"	6'-8"	3'-3"	2'-9"	11"	#7	6"	#5	6"	178.80	0.856
14'-0"	7'-2"	3'-6"	3'-0"	1'-0"	#8	6"	#5	6"	216.78	0.959
15'-0"	7'-8"	4'-0"	3'-0"	1'-1"	#9	6"	#6	6"	283.06	1.068
16'-0"	8'-2"	4'-6"	3'-0"	1'-3"	#9	6"	#6	6"	297.02	1.234

TABLE OF WINGWALL REINFORCING
(2-wings)

Bar	Size	No.	Spa
D	#5	~	1'-0"
E	#4	~	1'-0"
F	#4	~	1'-0"
G	#6	4	~
M	#4	4	~
P	#4	~	1'-0"
R	#5	6	~
V	#4	~	1'-0"

TABLE OF ESTIMATED CULVERT TOEWALL QUANTITIES

Bar	Size	No.	Spa
L	#4	~	1'-6"
Q	#4	1	~
Reinf (Lb/Ft)			2.45
Conc (CY/Ft)			0.037

WING DIMENSION FORMULAS:

(All values are in feet.)

$H_w = H + T + C - 0.250'$
 $A = (H_w - 0.333') (SL)$
 $B = (A) \text{ tangent } (30^\circ)$
 $L_w = (A) + \text{cosine } (30^\circ)$

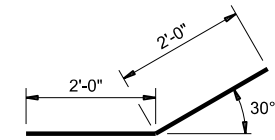
For cast-in-place culverts:
 $L_{tw} = (N) (S) + (N + 1) (U)$

For precast culverts:
 $L_{tw} = (N) (2U + S) + (N - 1) (0.5')$

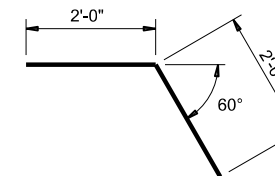
Total wingwall area (two wings - SF) = $(H_w + 0.333') (L_w)$

H_w = Height of wingwall
 $SL:1$ = Side slope ratio (horizontal:1 vertical)
 L_w = Length of wingwall
 L_{tw} = Culvert toewall length
 N = Number of culvert spans

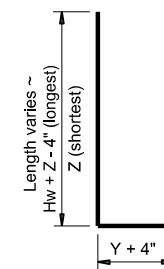
See applicable box culvert standard sheet for H, S, T, and U values.



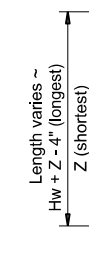
BARS D



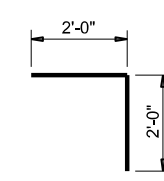
BARS R



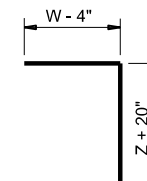
BARS J1



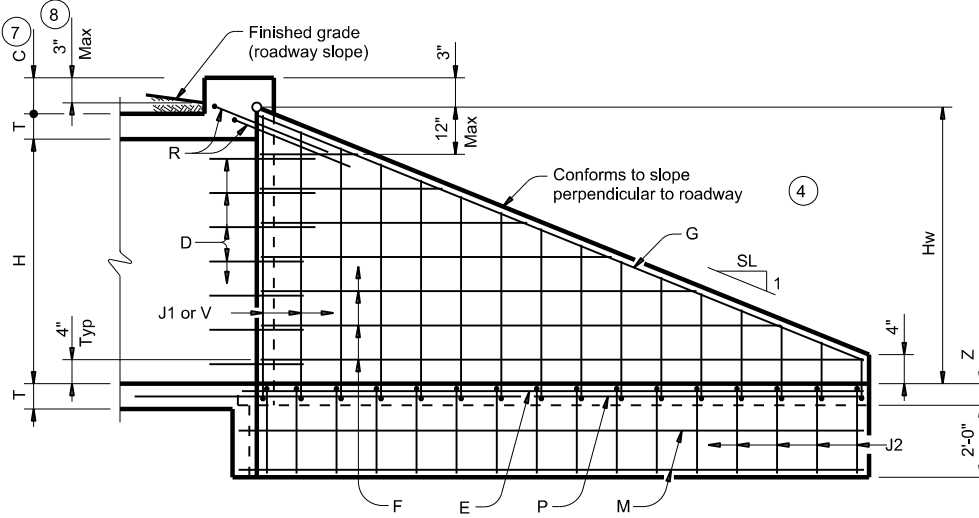
BARS V



BARS L

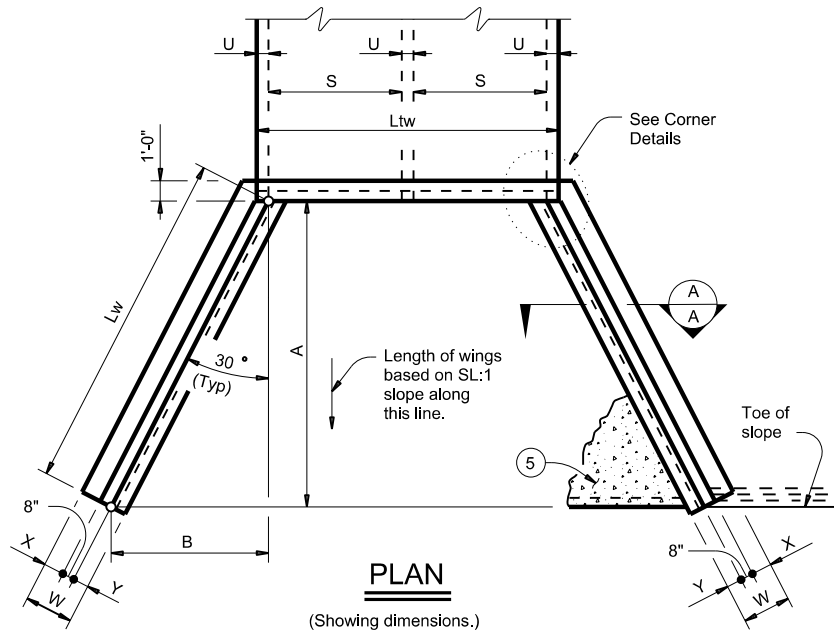


BARS J2



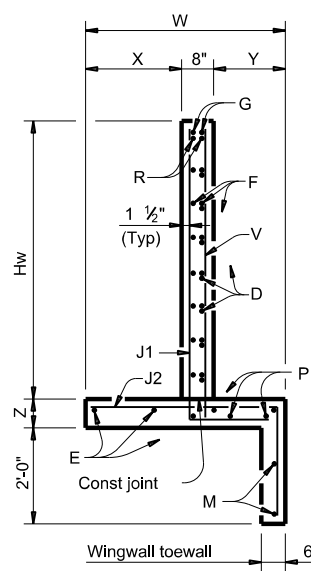
INSIDE ELEVATION

(Showing reinforcing. Culvert and culvert toewall reinforcing not shown for clarity.)

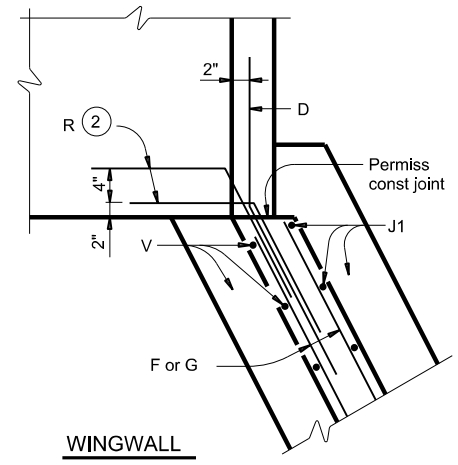


PLAN

(Showing dimensions.)



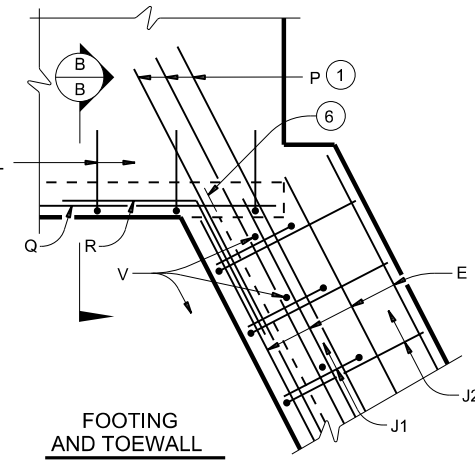
SECTION A-A



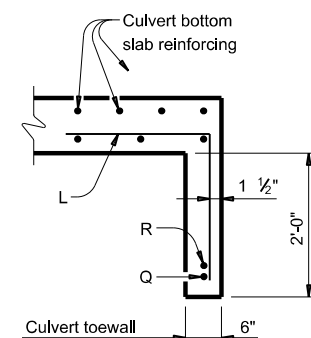
WINGWALL

CORNER DETAILS

(Culvert and culvert toewall reinforcing not shown for clarity.)



FOOTING AND TOEWALL



SECTION B-B

(5)

- Extend Bars P 3'-0" minimum into bottom slab of box culvert.
- Adjust as necessary to maintain 1 #2" clear cover and 4" minimum between bars.
- Quantities shown are based on an average wing height for two wings (one structure end). To determine total quantities for two wings, multiply the tabulated values by Lw.
- Recommended values of side slope are: 2:1, 3:1, 4:1, and 6:1.
- When shown elsewhere on the plans, construct 5" deep concrete riprap. Payment for riprap is as required by Item 432, "Riprap". Unless otherwise shown on the plans or directed by the Engineer, provide a 6" wide by 1'-6" deep reinforced concrete toewall along all edges of the riprap adjacent to natural ground; reinforce the toewall by extending typical riprap reinforcing into the toewall; and extend construction joints or grooved joints oriented in the direction of flow across the full distance of the riprap at intervals of approximately 20'. When such riprap is provided, the culvert toewall shown in SECTION B-B will not be required.
- At Contractor's option, culvert toewall may be ended flush with wingwall toewall. Adjust reinforcing as needed.
- 0" Min to 5'-0" Max. Estimated curb heights are shown elsewhere in the plans. For structures with pedestrian rail or curbs taller than 1'-0", refer to the Extended Curb Details (ECD) standard sheet. For structures with T631 or T631LS bridge rail, refer to the Mounting Details for T631 & T631LS Rails (T631-CM) standard sheet. Refer to the Box Culvert Rail Mounting Details (RAC) standard sheet for structures with bridge rail other than T631 or T631LS.
- For vehicle safety, the following requirements must be met:
 - For structures without bridge rail, construct curbs no more than 3" above finished grade.
 - For structures with bridge rail, construct curbs flush with finished grade.
 Reduce curb heights, if necessary, to meet the above requirements. No changes will be made in quantities and no additional compensation will be allowed for this work.

MATERIAL NOTES:

Provide Class C concrete (fc=3,600 psi).
 Provide Grade 60 reinforcing steel.
 Provide galvanized reinforcing steel if required elsewhere in the plans.
 In riprap concrete synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing unless noted otherwise.

GENERAL NOTES:

Designed according to AASHTO LRFD Bridge Design Specifications.
 When structure is founded on solid rock, depth of toewalls for culverts and wingwalls may be reduced or eliminated as directed by the Engineer.
 See Box Culvert Supplement (BCS) standard sheet for additional dimensions and information.
 The quantities for concrete and reinforcing steel resulting from the formulas given on this sheet are for Contractor's information only.

Cover dimensions are clear dimensions, unless noted otherwise. Reinforcing dimensions are out-to-out of bars.

				Bridge Division Standard	
CONCRETE WINGWALLS WITH FLARED WINGS FOR 0° SKEW BOX CULVERTS					
FW-0					
FILE:	fw-0std-20.dgn	DN:	GAF	CK:	CAT
©/TXDOT	February 2020	CON:	0482	SECT:	01
REVISIONS		JOB:	036, etc.	HIGHWAY:	SH 285
		DIST:	PHR	COUNTY:	JIM HOGG
		SHEET NO.:			257

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or for the accuracy of the information presented herein.

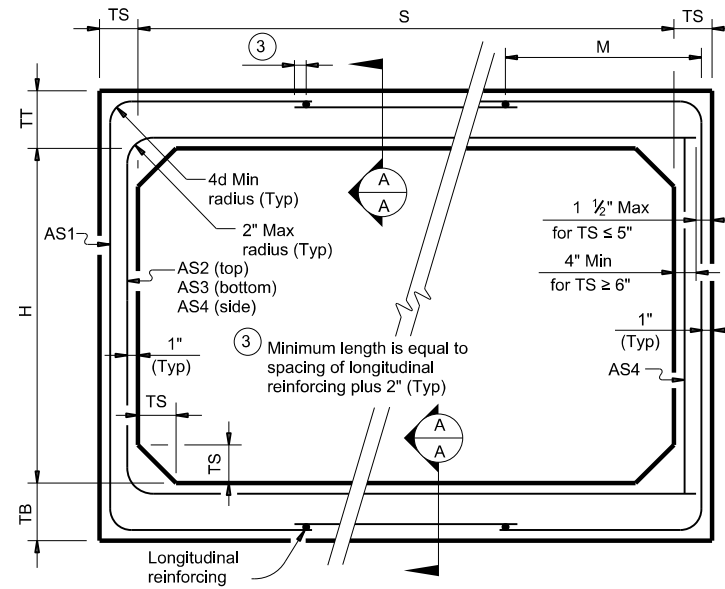
DATE: 2/12/2024 8:11:27 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT15\Documents\21 - PHR\Design\Projects\sc05sts-20.dgn

BOX DATA

SECTION DIMENSIONS					Fill Height (ft.)	M (Min) (in.)	REINFORCING (sq. in. / ft.) ⁽²⁾								Lift Weight (tons) ⁽¹⁾
S (ft.)	H (ft.)	TT (in.)	TB (in.)	TS (in.)			AS1	AS2	AS3	AS4	AS5	AS7	AS8		
5	2	8	7	6	< 2	-	0.19	0.27	0.18	0.14	0.19	0.19	0.17	6.0	
5	2	6	6	6	2 < 3	44	0.22	0.20	0.16	0.14	-	-	-	5.1	
5	2	6	6	6	3 - 5	44	0.16	0.14	0.14	0.14	-	-	-	5.1	
5	2	6	6	6	10	36	0.15	0.14	0.14	0.14	-	-	-	5.1	
5	2	6	6	6	15	36	0.20	0.18	0.18	0.14	-	-	-	5.1	
5	2	6	6	6	20	36	0.26	0.23	0.24	0.14	-	-	-	5.1	
5	2	6	6	6	25	36	0.33	0.29	0.29	0.14	-	-	-	5.1	
5	2	6	6	6	30	36	0.39	0.34	0.35	0.14	-	-	-	5.1	
5	3	8	7	6	< 2	-	0.19	0.31	0.21	0.14	0.19	0.19	0.17	6.6	
5	3	6	6	6	2 < 3	45	0.18	0.24	0.19	0.14	-	-	-	5.7	
5	3	6	6	6	3 - 5	36	0.14	0.17	0.16	0.14	-	-	-	5.7	
5	3	6	6	6	10	36	0.14	0.16	0.17	0.14	-	-	-	5.7	
5	3	6	6	6	15	35	0.16	0.21	0.22	0.14	-	-	-	5.7	
5	3	6	6	6	20	35	0.21	0.27	0.28	0.14	-	-	-	5.7	
5	3	6	6	6	25	35	0.26	0.34	0.34	0.14	-	-	-	5.7	
5	3	6	6	6	30	35	0.31	0.41	0.41	0.14	-	-	-	5.7	
5	4	8	7	6	< 2	-	0.19	0.33	0.24	0.14	0.19	0.19	0.17	7.2	
5	4	6	6	6	2 < 3	45	0.16	0.27	0.22	0.14	-	-	-	6.3	
5	4	6	6	6	3 - 5	45	0.14	0.19	0.18	0.14	-	-	-	6.3	
5	4	6	6	6	10	36	0.14	0.18	0.18	0.14	-	-	-	6.3	
5	4	6	6	6	15	35	0.14	0.23	0.24	0.14	-	-	-	6.3	
5	4	6	6	6	20	35	0.17	0.30	0.31	0.14	-	-	-	6.3	
5	4	6	6	6	25	35	0.21	0.37	0.38	0.14	-	-	-	6.3	
5	4	6	6	6	30	35	0.25	0.44	0.45	0.14	-	-	-	6.3	
5	5	8	7	6	< 2	-	0.19	0.35	0.26	0.14	0.19	0.19	0.17	7.8	
5	5	6	6	6	2 < 3	45	0.14	0.29	0.24	0.14	-	-	-	6.9	
5	5	6	6	6	3 - 5	45	0.14	0.21	0.20	0.14	-	-	-	6.9	
5	5	6	6	6	10	45	0.14	0.19	0.20	0.14	-	-	-	6.9	
5	5	6	6	6	15	36	0.14	0.24	0.25	0.14	-	-	-	6.9	
5	5	6	6	6	20	35	0.15	0.31	0.32	0.14	-	-	-	6.9	
5	5	6	6	6	25	35	0.18	0.38	0.39	0.14	-	-	-	6.9	
5	5	6	6	6	30	35	0.21	0.46	0.47	0.14	-	-	-	6.9	

⁽¹⁾ For box length = 8'-0"

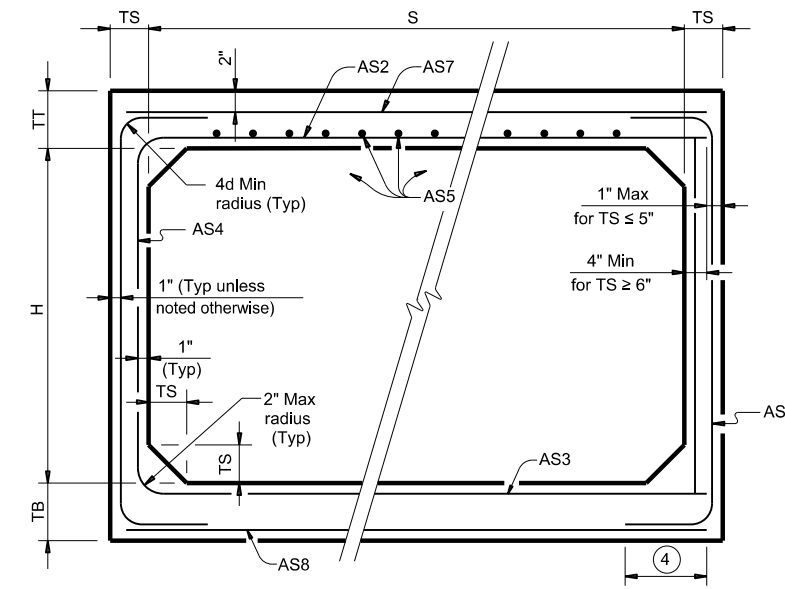
⁽²⁾ AS1 thru AS4, AS7 and AS8 are minimum required areas of reinforcement per linear foot of box length. AS5 is minimum required area of reinforcement per linear foot of box width.



CORNER OPTION "A"

CORNER OPTION "B"

FILL HEIGHT 2 FT AND GREATER

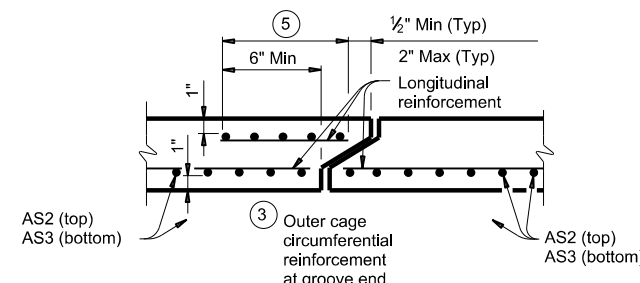


CORNER OPTION "A"

CORNER OPTION "B"

FILL HEIGHT LESS THAN 2 FT

⁽⁴⁾ Length is equal to spacing of longitudinal reinforcing plus 2". (10" Min) (Typ)



SECTION A-A

(Showing top and bottom slab joint reinforcement.)

MATERIAL NOTES:

Provide 0.03 sq. in./ft. minimum longitudinal reinforcement at each face in slabs and walls. This minimum requirement may be met by the transverse wires when wire mesh reinforcement is used.
 Provide Class H concrete (f'c = 5,000 psi).

GENERAL NOTES:

Designs shown conform to ASTM C1577. Refer to ASTM C1577 for information or details not shown.
 See Box Culverts Precast Miscellaneous Details (SCP-MD) standard sheet for details and notes not shown.
 In lieu of furnishing the designs shown on this sheet, the contractor may furnish an alternate design that is equal to or exceeds the box design for the design fill height in the table. Submit shop plans for alternate designs in accordance with Item "Precast Concrete Structural Members (Fabrication)".

HL93 LOADING

		<i>Bridge Division Standard</i>	
SINGLE BOX CULVERTS PRECAST 5'-0" SPAN			
SCP-5			
FILE:	sc05sts-20.dgn	DN:	TxDOT
©TxDOT	February 2020	CR:	TxDOT
REVISIONS		DW:	TxDOT
		CK:	TxDOT
		CON:	0482
		SECT:	01
		JOB:	036, etc.
		HIGHWAY:	SH 285
		DIST:	PHR
		COUNTY:	JIM HOGG
		SHEET NO.:	258

DATE: 2/12/2024 8:12:50 AM
FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\7. Bridge\SH 285 BRIDGE COVER SHEET.dgn

BRIDGE COVER SHEET

Pharr District Central Design



**SH 285
BRIDGE COVER
SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		259

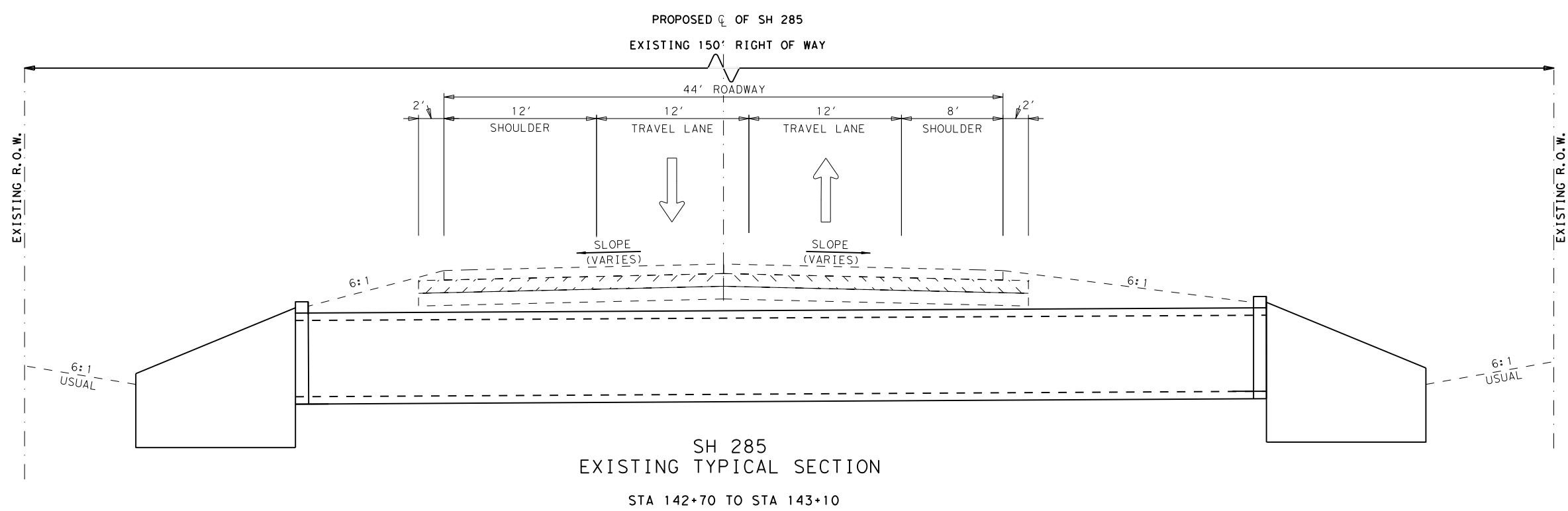
DATE: 2/7/2024 10:35:47 AM
 FILE: P:\xtdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\7. Bridge\SH 285 BRIDGE TYP.dgn

LEGEND

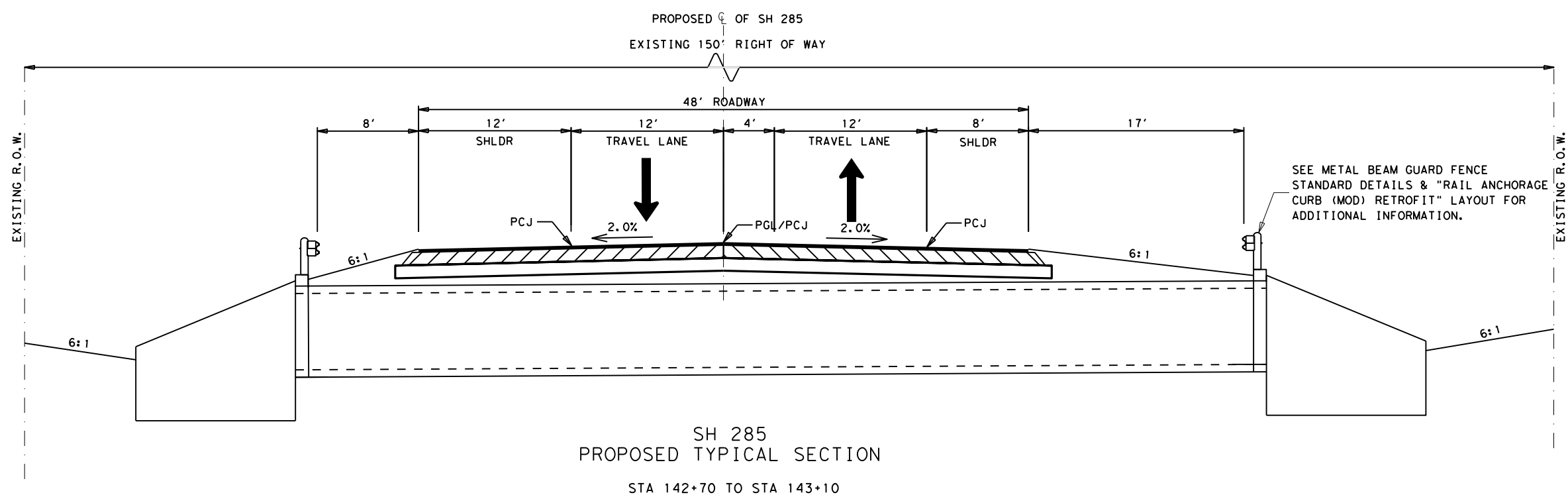
- ℄ - CENTER LINE
- PGL - PROFILE GRADE LINE
- PCJ - PERMISSIBLE CONSTRUCTION JOINT
- R.O.W. - RIGHT OF WAY
- SHLDR - SHOULDER

NOTES:

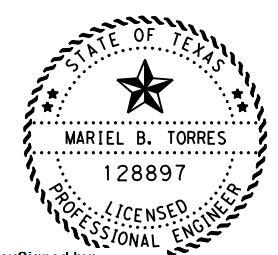
DETOUR FROM TCP OPERATIONS TO REMAIN BETWEEN THE PROPOSED ROADWAY AND THE METAL BEAM GUARD FENCE AREA. THE LIMITS WHERE THE DETOUR SHOULD BE KEPT ARE FROM STA. 141+01.25 TO STA. 145+16.25.



SH 285
 EXISTING TYPICAL SECTION
 STA 142+70 TO STA 143+10



SH 285
 PROPOSED TYPICAL SECTION
 STA 142+70 TO STA 143+10



DocuSigned by:
 Mariel Torres
 C9702DF8C67B467... 2/7/2024

Pharr District Central Design

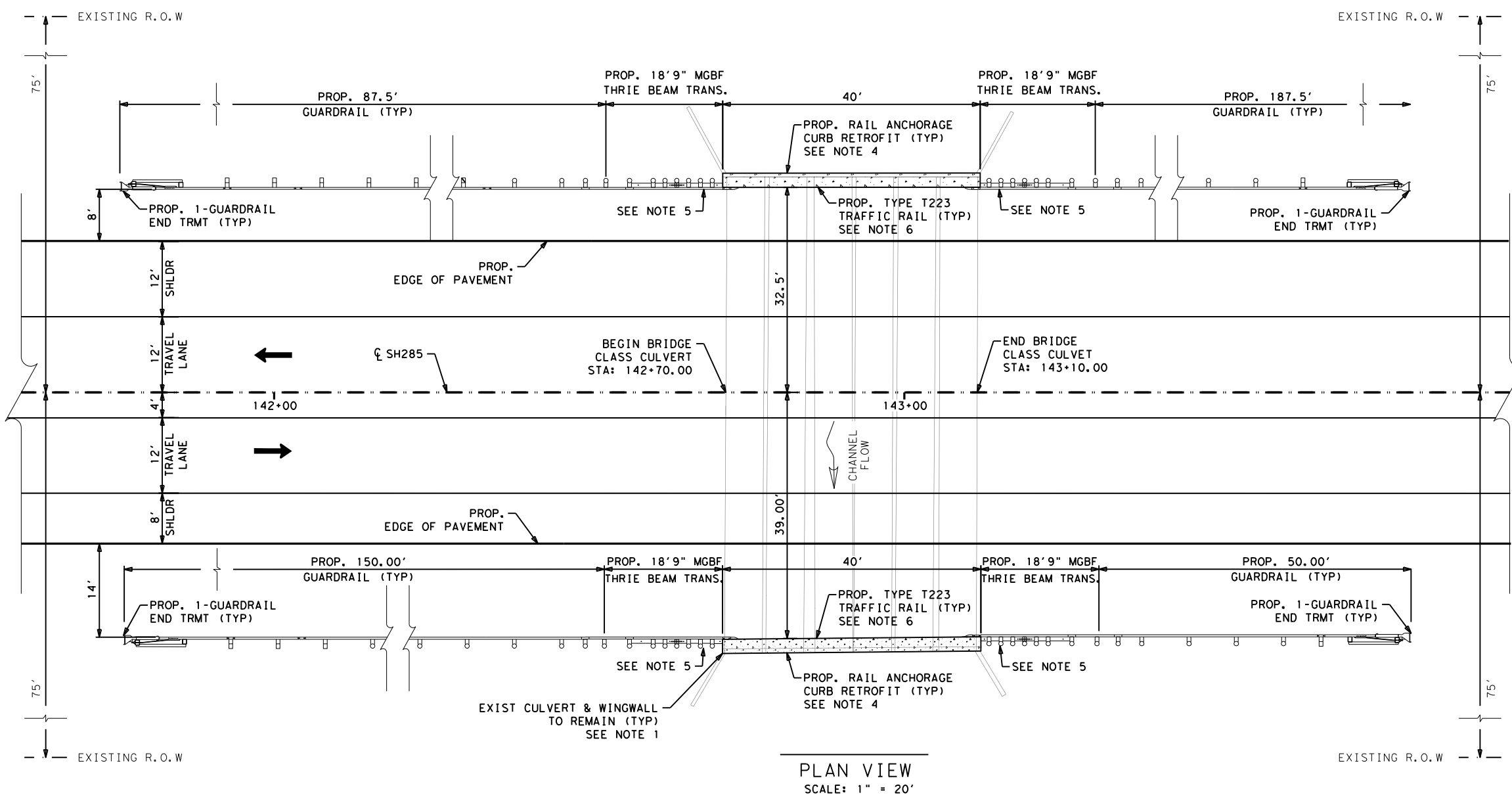
Texas Department of Transportation

**SH 285
 BRIDGE CLASS
 CULVERT
 TYPICAL SECTIONS**

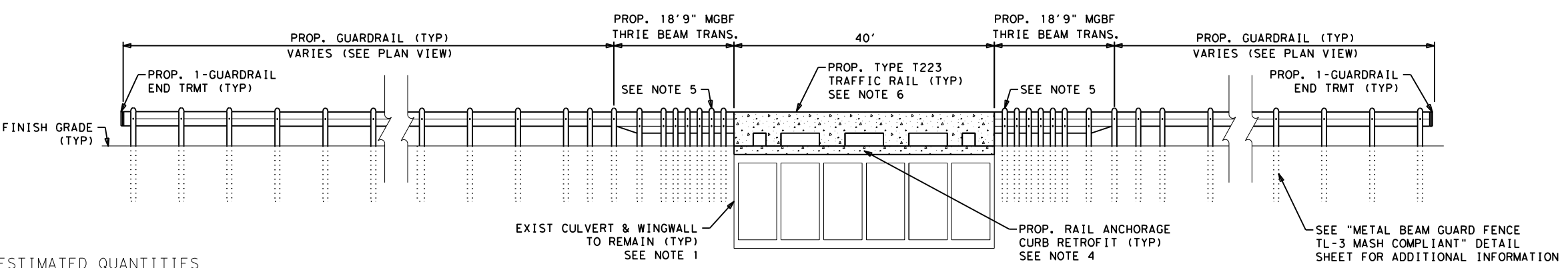
SCALE = 10" = 1' SHEET 1 OF 1

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		260

DATE: 2/7/2024 10:35:55 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\7. Bridge\SH 285 PLAN VIEW.dgn

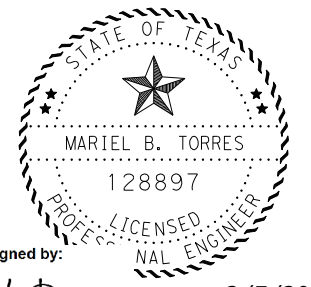


- GENERAL NOTES:**
1. THE CONTRACTOR SHALL PROTECT THE BRIDGE CLASS CULVERT FROM DAMAGE DURING CONSTRUCTION. ANY DAMAGES TO THE BRIDGE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
 2. ALL EXISTING INFORMATION ON PLANS IS FOR INFORMATION PURPOSES ONLY. CONTRACTOR TO VERIFY ALL EXISTING INFORMATION PER "AS BUILT CONDITION".
 3. SEE "PLAN & PROFILE" SHEET FOR ROADWAY DETAILS.
 4. SEE "RAIL ANCHORAGE CURB (MOD) RETROFIT" SHEET FOR RAIL RETROFIT DETAILS.
 5. SEE "METAL BEAM GUARD FENCE" SHEET FOR GUARDRAIL AND TL-3 STANDARD DETAILS.
 6. SEE "TRAFFIC RAIL TYPE T223" STANDARD DETAIL SHEETS FOR ADDITIONAL INFORMATION.
 7. PROPOSED TCP DETOUR TO REMAIN IN PLACE IN GUARDRAIL LIMITS FROM EDGE OF PAVEMENT TO WALL OF EXISTING WINGWALL.

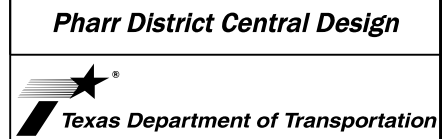


ESTIMATED QUANTITIES

ITEM	450	420	540	540	544
CODE	6006	6136	6001	6006	6001
DESC.	RAIL (TY T223) (LF)	CL C CONC (RAC-R) (CY)	MTL W-BEAM GD FEN (TIM POST) (LF)	MTL BEAM GD FEN TRANS (THRIE-BEAM) (EA)	GUARDRAIL END TRMT (INSTALL) (EA)
TOTAL	80	8	475	4	4



DocuSigned by:
Mariel Torres 2/7/2024
 C9702DF8C67B467...
 NBI: 211250048201001
 H15 LOADING

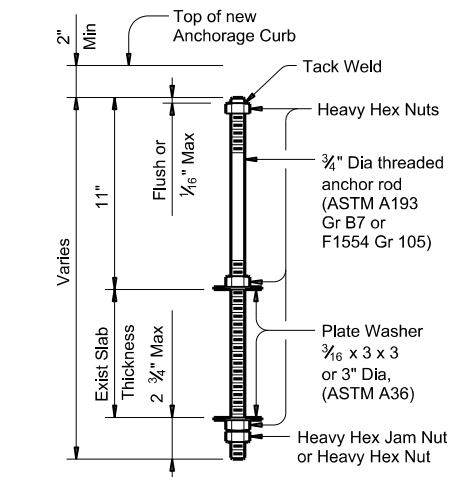


**SH 285
 RAIL ANCHORAGE CURB
 (MOD) RETROFIT LAYOUT**

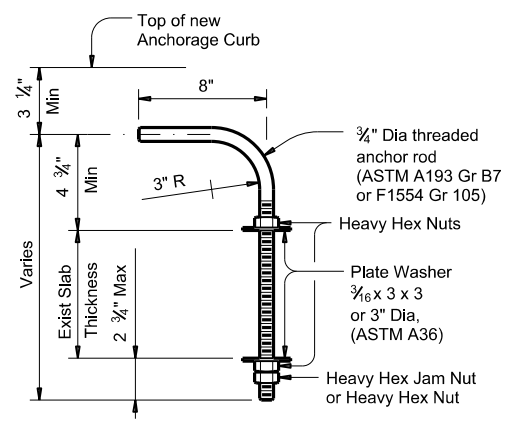
SCALE: AS SHOWN

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	261

DATE: 3/14/2024 1:06:30 PM
 FILE: \\txdot\project\w\iseon\line.com\TXDOT15\Documents\21 - PHR\Design\Project\w\iseon\line.com\TXDOT15_H285_MOD\acst02-20.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein.

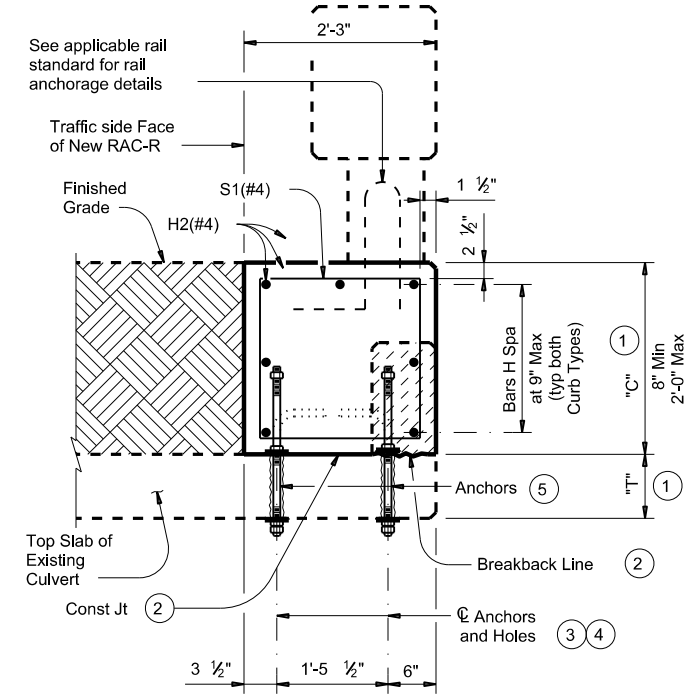
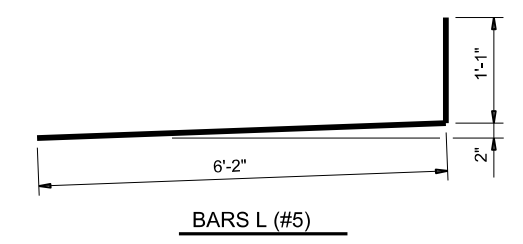
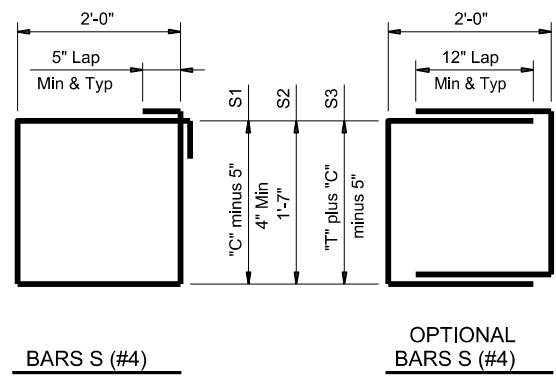


STRAIGHT ANCHOR ⑤



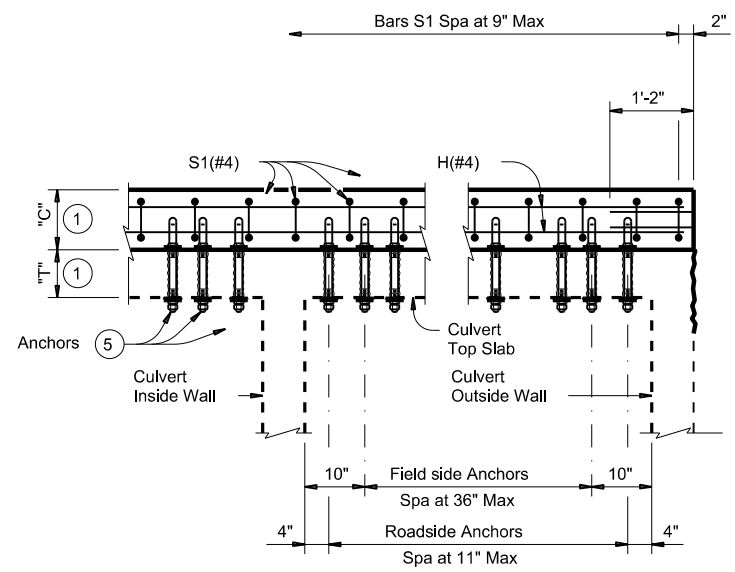
HOOKED ANCHOR ⑤

ANCHOR DETAILS



TYPICAL SECTION ~ TYPE 2

Used when the Retrofit Curb is 8" in height or greater. Showing T223 Rail, other rails similar. (Bars L(#5) on T223 and C223 Rails are not used for this structure). Bars RH(#5) required on standards T80HT, T80SS and T224 are not required when used with the RAC-R standard.



SHOWING CULVERT ANCHORAGE CURB

Showing Anchorage Curb Type 2. Anchor and Bars S spacing are the same for Anchorage Type 1.

TYPICAL ELEVATIONS OF INSTALLATION

- ① "T" is equal to the existing culvert top slab thickness. If "T" is less than 6", a special design will be required. "C" is equal to the Retrofit Rail Anchorage Curb thickness.
- ② Saw cut (score) 1" deep flush with top of existing culvert slab, on the field side face of existing curb, if present. After scoring, remove shaded portion of existing concrete to Breakback Line shown. Do not damage existing reinforcing. Clean, bend and incorporate existing reinforcing into new concrete construction. Note that new anchors, as shown in the detail, are required even when existing reinforcing remains in use. Remove existing overlay and/or base material to flush with top of culvert in areas of new construction. Care must be taken to not damage the existing slab. In order to prevent existing asphalt remnants from acting as a bond breaker between the exposed, existing concrete and the retrofitted concrete curb, clean the newly exposed concrete with abrasive blasting or shot blasting. Remove all loose debris prior to placing new anchorage curb.
- ③ Core drill 1" diameter holes through existing slab. Percussion drilling is not permitted. Patch spalls, when directed by the Engineer, in accordance with Item 429, "Concrete Structure Repair", at the Contractor's expense. Tighten nuts snug tight.
- ④ Space field side anchors at 36" maximum. Space traffic side anchors at 11" maximum. Do not align field side and traffic side anchors transversely.
- ⑤ Use straight anchors if retrofit anchorage curb is 1'-2" or greater in thickness. Use hooked anchors for retrofit anchorage curb less than 1'-2" thick.

CONSTRUCTION NOTES:

Field verify dimensions before commencing work and ordering materials.

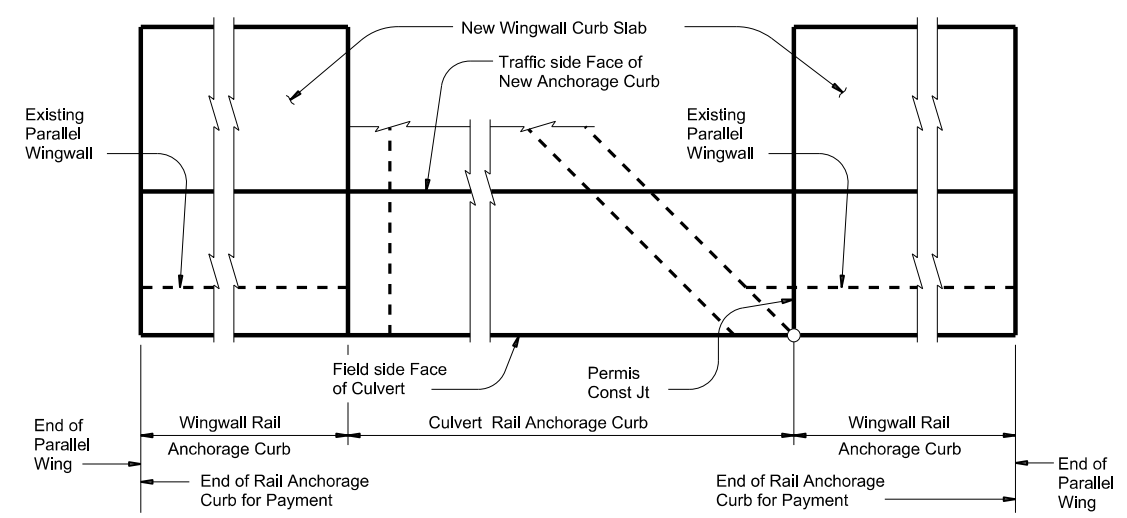
MATERIAL NOTES:

Provide Class "C" concrete (f'c=3,600 psi). Provide Class "C" (HPC) concrete if shown elsewhere in the plans. Chamfer all exposed corners 3/4" unless shown otherwise. Provide Grade 60 reinforcing steel. Galvanize all reinforcing steel if required elsewhere. Provide bar laps, where required, as follows: Uncoated or galvanized - #4 = 1'-11" Galvanize 3/4" Dia threaded rods, heavy hex nuts and plate washers, unless otherwise shown on plans.

GENERAL NOTES:

Designed according to AASHTO LRFD Bridge Design Specifications. The rail anchorage curb details have sufficient strength for use with all standard rail types. See appropriate rail standard for approved speed restrictions, notes and details not shown. For vehicle safety, the top of the new curb must be flush with the finished grade. These details are for use with curbs with a maximum height of 2'-0" only. Curb heights greater than 2'-0" will require special design. Removal and replacement of backfill, subgrade, and asphalt or concrete pavement necessary for this installation is considered subsidiary to the rail anchorage curb. Payment for rail anchorage curb (including wingwall curb slab) will be by CY of Class "C" or Class "C" (HPC) concrete. Not all possible combinations of existing box culverts, curbs, wingwalls etc. have been shown on this sheet. Other combinations and reinforcement arrangements are permissible if they meet the same strength requirements as indicated on this sheet.

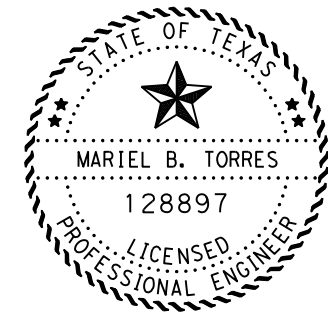
Cover dimensions are clear dimensions, unless noted otherwise. Reinforcing bar dimensions shown are out-to-out of bar.



PARALLEL WINGWALL AT 0° SKEW CULVERT

PARALLEL WINGWALL AT SKEWED CULVERT

Note that Wingwall Rail Anchorage Curb is used only at culverts with parallel wingwalls.

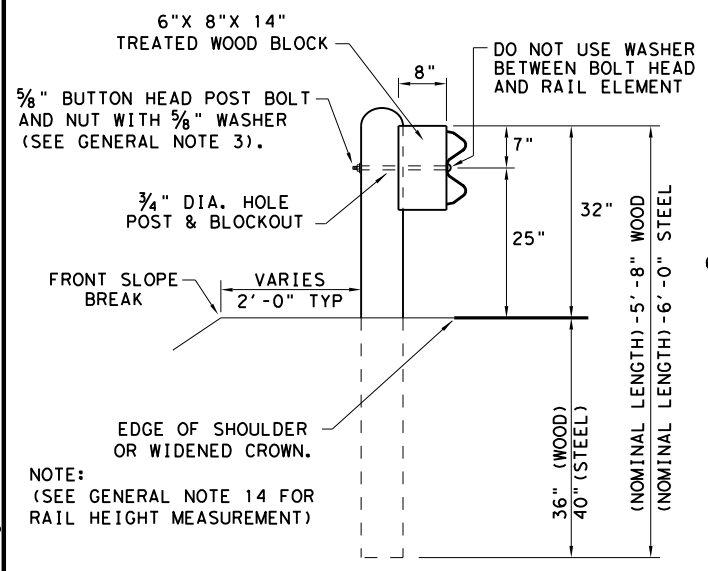


DocuSigned by:
Mariel Torres
 C9702DF8C67B467...
 3/14/2024

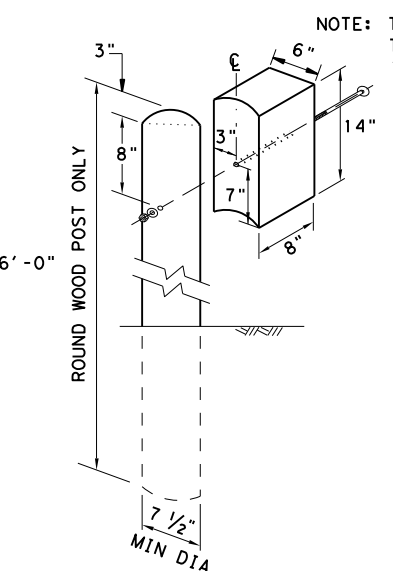
SHEET 1 OF 1

		Bridge Division Standard	
SH 285 RAIL ANCHORAGE CURB (MOD) RETROFIT BOX CULVERT RAIL MOUNTING DETAILS			
RAC-R			
FILE: ract02-20.dgn	DN: TXDOT	CK: TXDOT	DW: TXDOT
©TXDOT February 2020	CONT: 0482	SECT: 01	JOB: 036, etc.
REVISIONS	HIGHWAY: SH 285		SHEET NO. 262
DIST: PHR	COUNTY: JIM HOGG	SHEET NO. 262	

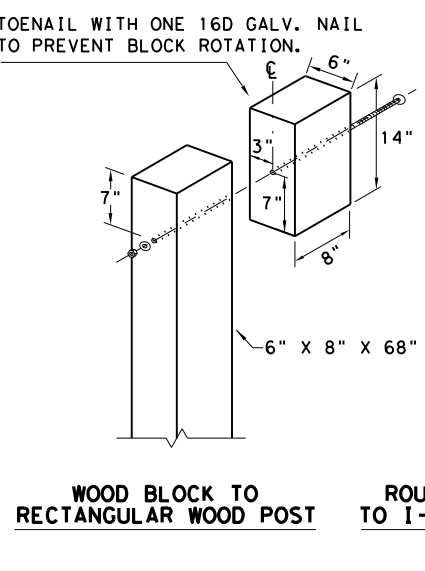
DATE: 2/12/2024
 FILE: \\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\7 - Br\idge\gf3119.dgn
 DISCLAIMER: THE USE OF THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.



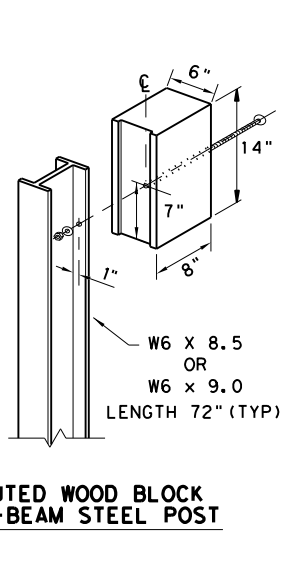
TYPICAL POST PLACEMENT



WOOD BLOCK TO ROUND WOOD POST



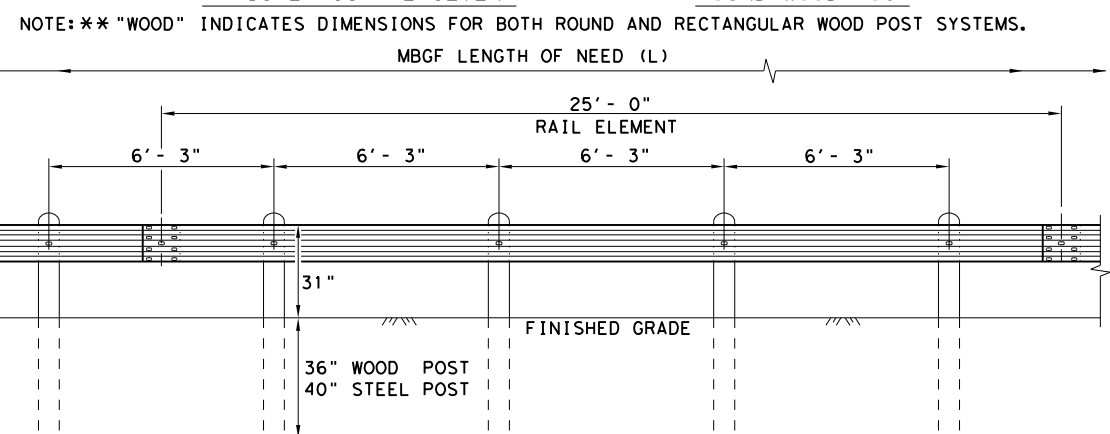
WOOD BLOCK TO RECTANGULAR WOOD POST



ROUTED WOOD BLOCK TO I-BEAM STEEL POST

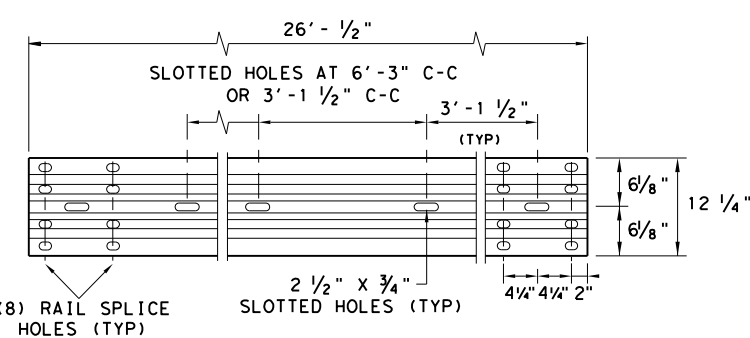
GENERAL NOTES

1. THE TYPE OF POST (ROUND WOOD POST, RECTANGULAR WOOD POST, OR STEEL POST) WILL BE AS SHOWN IN THE PLANS. THE EXACT POSITION OF MBGF SHALL BE SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. STEEL POSTS TO BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING."
2. RAIL ELEMENTS SHALL MEET THE REQUIREMENTS OF ITEM 540, "METAL BEAM GUARD FENCE" EXCEPT AS MODIFIED IN THE PLANS. THE CONTRACTOR MAY FURNISH RAIL ELEMENTS OF 25'-0", OR 12'-6" (NOM.) LENGTHS. RAIL ELEMENTS MAY HAVE SLOTTED HOLES AT 3'-1 1/2" C-C OR 6'-3" C-C. A SPECIAL LENGTH OF RAIL MAY BE MANUFACTURED TO ACCOMMODATE THE DOWNSTREAM ANCHOR TERMINAL (DAT) AND THE TRANSITION SECTIONS OF GUARDRAIL.
3. BUTTON HEAD "POST BOLTS & NUTS" SHALL MEET THE REQUIREMENTS OF (ASTM A307), AND SHALL BE OF SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND 5/8" WASHER (FWC16G) AND NOT MORE THAN 1" BEYOND IT. TRIM REMAINING BOLT LENGTH TO MEET REQUIRED LENGTH.
4. FITTINGS (BOLTS, NUTS, AND WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING." FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
5. CROWN SHALL BE WIDENED TO ACCOMMODATE THE METAL BEAM GUARD FENCE.
6. THE LATERAL APPROACH TO THE GUARD FENCE, SHALL HAVE A MAXIMUM SLOPE OF 1V:10H.
7. IF SHOWN ELSEWHERE IN THE PLANS OR AS DIRECTED BY THE ENGINEER, THE GUARD FENCE MAY BE FLARED AT A RATE OF 25:1 OR FLATTER.
8. UNLESS OTHERWISE SHOWN IN THE PLANS, GUARD FENCE PLACED IN THE VICINITY OF CURBS SHALL BE POSITIONED SO THAT THE FACE OF CURB IS LOCATED DIRECTLY BELOW OR BEHIND THE FACE OF THE RAIL. RAIL PLACED OVER CURBS SHALL BE INSTALLED SO THAT THE POST BOLT IS LOCATED APPROXIMATELY 25 INCHES ABOVE THE GUTTER PAN OR EDGE OF SHOULDER.
9. APPLICATIONS IN SOLID ROCK ARE ONLY ALLOWED WITH STEEL POSTS. IF SOLID ROCK IS ENCOUNTERED WITHIN 0 TO 18" OF THE FINISHED GRADE, DRILL A 24" DIA. HOLE, 24" INTO THE ROCK. IF SOLID ROCK IS ENCOUNTERED BELOW 18", DRILL A 12" DIA. HOLE, 12" INTO THE ROCK OR TO THE STANDARD EMBEDMENT DEPTH, WHICHEVER MAYBE LESS. ANY EXCESS POST LENGTH, AFTER MEETING THESE DEPTHS, MAY BE FIELD CUT TO ENSURE PROPER GUARDRAIL MOUNTING HEIGHT. BACKFILL WITH COARSE AGGREGATE MATERIAL.
10. POSTS SHALL NOT BE SET IN CONCRETE, OF ANY DEPTH.
11. SPECIAL FABRICATION WILL BE REQUIRED AT INSTALLATION LOCATIONS HAVING A CURVATURE OF LESS THAN 150 FT. RADIUS.
12. UNLESS OTHERWISE SHOWN IN THE PLANS, A COMPOSITE MATERIAL BLOCK THAT MEETS THE REQUIREMENTS OF DMS-7210, "COMPOSITE MATERIAL POSTS AND BLOCKS FOR METAL BEAM GUARD FENCE" MAY BE SUBSTITUTED FOR BLOCKS OF SIMILAR DIMENSIONS. THE CONSTRUCTION DIVISION, TXDOT MAINTAINS A MATERIAL PRODUCER LIST (MPL) FOR PRODUCERS OF MATERIALS CONFORMING TO DMS-7210 ONLY PRODUCERS ON THE MPL MAY FURNISH COMPOSITE MATERIAL BLOCKS.
13. FOR THE LOW FILL CULVERT OPTION, POSTS LOCATED PARTIALLY OR WHOLLY BETWEEN PRECAST BOX CULVERT UNITS, THE USE OF A CAST-IN-PLACE CONCRETE CLOSURE BETWEEN BOXES IS REQUIRED. THE LENGTH OF THE CAST-IN-PLACE CONCRETE CLOSURE SHALL ACCOMMODATE THE PLACEMENT OF THE LOW FILL CULVERT OPTION. SEE CONCRETE CLOSURE DETAILS ON BRIDGE STANDARD SCP-MD.
14. GUARDRAIL HEIGHT MEASUREMENT: WHEN THE GUARDRAIL IS LOCATED ABOVE PAVEMENT, MEASURE THE HEIGHT FROM THE PAVEMENT TO THE TOP OF THE W-BEAM RAIL. WHEN THE GUARDRAIL IS LOCATED UP TO 2 FT. OFF OF THE EDGE OF PAVEMENT OR FOR A PAVEMENT OVERLAY, USE A 10-FOOT STRAIGHTEDGE TO EXTEND THE PAVEMENT/SHOULDER SLOPE TO THE BACK OF RAIL, MEASURE FROM THE BOTTOM OF STRAIGHTEDGE TO THE TOP OF RAIL. FOR GUARDRAIL LOCATED DOWN A 10:1 SLOPE, MEASURE FROM THE NOMINAL TERRAIN.



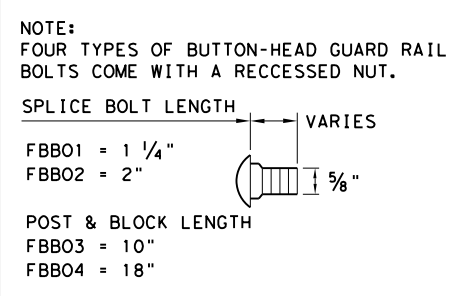
ELEVATION MID-SPAN RAIL SPLICE

NOTE: ** "WOOD" INDICATES DIMENSIONS FOR BOTH ROUND AND RECTANGULAR WOOD POST SYSTEMS.



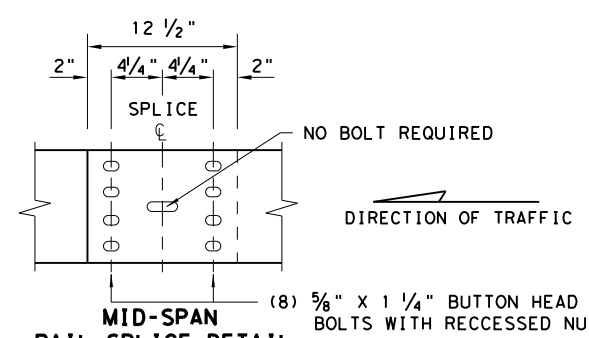
ELEVATION 25'-0" (NOM.) W-BEAM SECTION

NOTES: SEE GENERAL NOTE 2 FOR ALLOWABLE RAIL TYPES. SEE RAIL SPLICE DETAIL FOR REQUIRED HARDWARE.



BUTTON HEAD BOLT

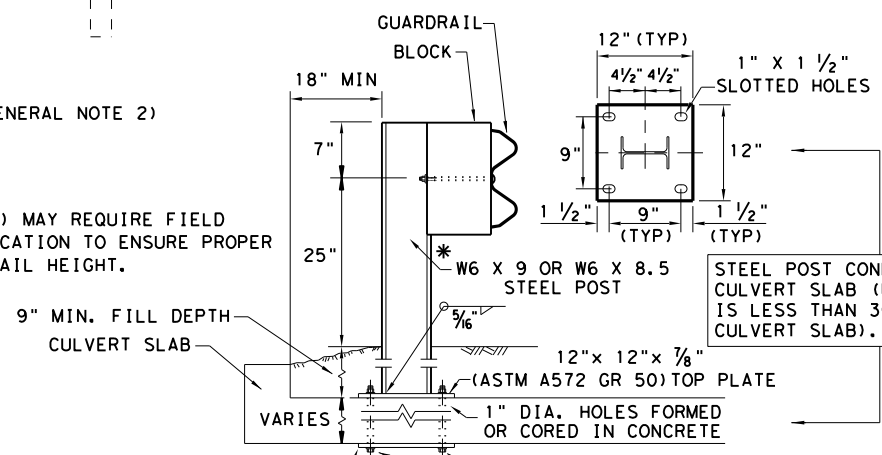
NOTE: SEE GENERAL NOTE 3 FOR SPLICE & POST BOLT DETAILS.



MID-SPAN RAIL SPLICE DETAIL

NOTE: GF(31), MID-SPAN RAIL SPLICES ARE REQUIRED WITH 6'-3" POST SPACINGS.

* POST(S) MAY REQUIRE FIELD MODIFICATION TO ENSURE PROPER GUARDRAIL HEIGHT.



LOW FILL CULVERT POST

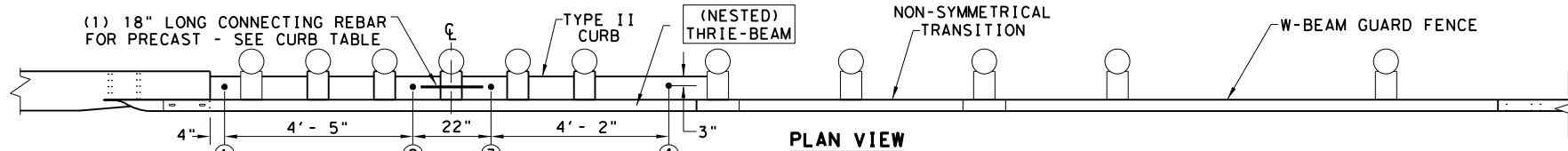
NOTE: TWO INSTALLATION OPTIONS.

1. **BOLT-THROUGH OPTION:** REQUIRES A 6" MIN. SLAB THICKNESS. 5/8" DIA (ASTM A449) HEAVY HEX BOLTS WITH TWO HARDENED WASHER EACH AND HEAVY HEX NUTS. NOTE: BOLT LENGTH = SLAB PLUS 2 1/4" MIN.
2. **EPOXY ANCHOR OPTION:** THIS OPTION MAY ONLY BE USED IF THE CULVERT SLAB IS 9" MIN. THICK. THREADED ANCHOR RODS MUST BE 5/8" DIA. ASTM A449 OR A193 GRADE B7 WITH HEAVY HEX NUT, AND ONE HARDENED WASHER EACH. EMBED ANCHOR RODS 6" WITH HILTI HIT RE 500 EPOXY ADHESIVE. OTHER TYPE III CLASS C EPOXY ADHESIVES MEETING THE REQUIREMENTS OF DMS-6100, "EPOXIES AND ADHESIVES", MAY BE USED IF IT CAN BE DEMONSTRATED THAT THEY MEET OR EXCEED THE STRENGTH OF HILTI HIT RE 500 WITH THE SAME EMBEDMENT DEPTH AND THREADED ROD DIA. FOLLOW THE MANUFACTURER'S REQUIREMENTS FOR INSTALLING EPOXIED THREADED RODS. EXTEND RODS 1/4" MIN. BEYOND NUT.

NOTE: CULVERTS OF 25 FT. OR LESS, SEE GF(31)LS STANDARD FOR "LONG SPAN" OPTION.

				Design Division Standard
METAL BEAM GUARD FENCE TL-3 MASH COMPLIANT GF(31)-19				
FILE: gf3119.dgn	DN: TXDOT	CK: KM	DW: VP	CK: CGL/AG
© TXDOT: NOVEMBER 2019	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		264

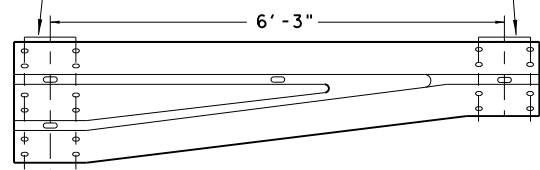
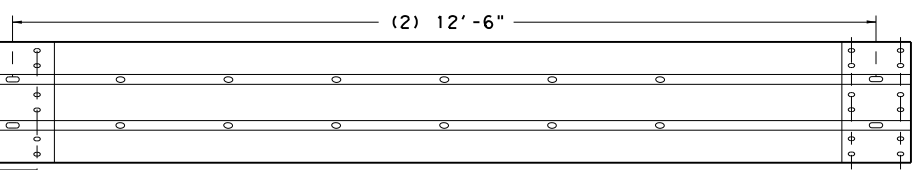
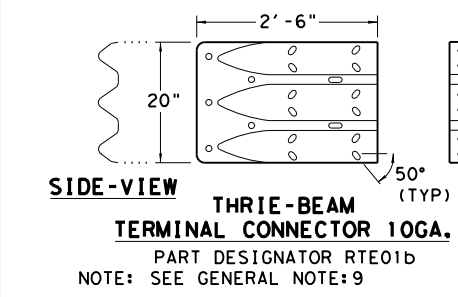
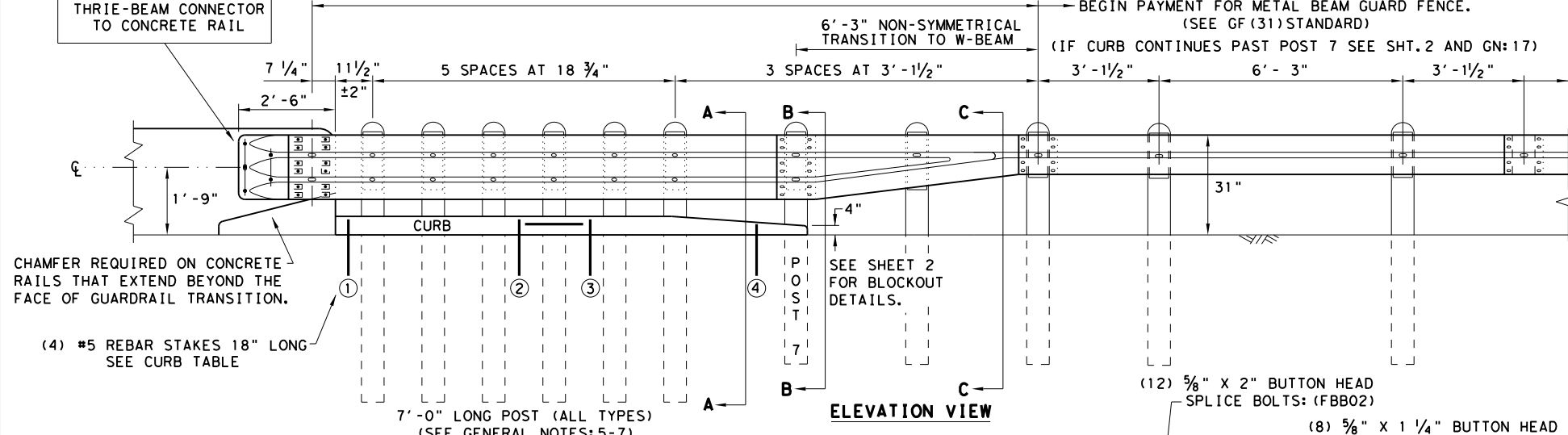
DATE: 2/12/2024
 FILE: \\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\0482010364 - Design\Plan Set\7 - Bridge\gf31tr+1320.dgn
 DISCLAIMER: THE USE OF THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.



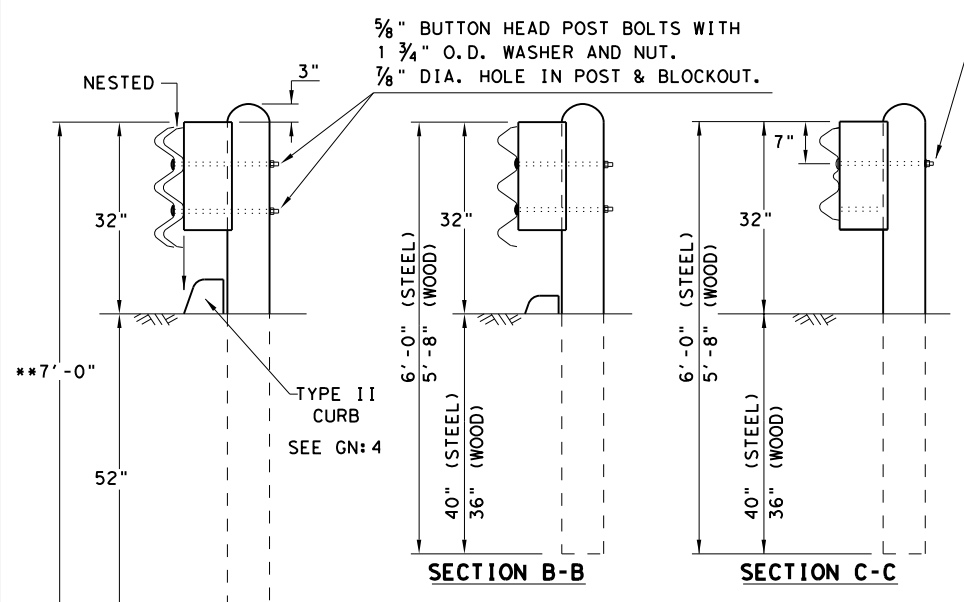
- (5) 1" DIA. HOLES.
- (5) 7/8" DIA. HEAVY HEX HEAD BOLTS (FACING TRAFFIC SIDE) (ASTM F3125 GR A325 OR A449).
- (10) 1 3/4" O.D. WASHER UNDER EACH HEX BOLT HEAD AND NUT.
- (5) 7/8" DIA. HEAVY HEX NUTS (ASTM A194 OR A563).

NOTE:
HEAVY HEX BOLT LENGTH WILL VARY DEPENDING ON WIDTH CONCRETE RAIL, LEAVE 1" OF BOLT LENGTH PAST THE 7/8" HEX NUT. TRIM AS REQUIRED.

NOTE:
CURB IS A REQUIRED COMPONENT FOR THE TRANSITION TO FUNCTION PROPERLY. SEE GENERAL NOTES: 2-4 AND 16-17.

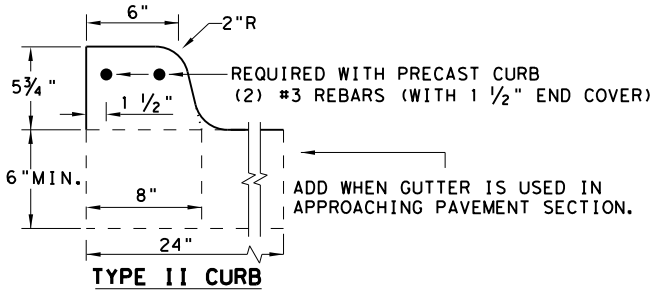


BRIDGE APPROACH - UPSTREAM: THE NESTED RAIL LAPS OVER THE TERMINAL CONNECTOR. PLATE WASHERS ARE INSTALLED UNDER THE SPLICE NUTS AGAINST INSIDE OF CONNECTOR.
 BRIDGE EXIT - DOWNSTREAM: THE TERMINAL CONNECTOR LAPS OVER THE NESTED RAIL. PLATE WASHERS ARE INSTALLED UNDER THE BOLT HEAD AGAINST OUTSIDE OF CONNECTOR.



THRIE-BEAM TERMINAL - CURB TABLE	
PRECAST CURB FULL LENGTH EQUALS 12'- 2"	
THE PRECAST CURB MAY BE FORMED INTO TWO SECTIONS.	
CURB (1) LENGTH	5'- 8"
CURB (2) LENGTH	6'- 6"
TAPER CURB (2) TO A HEIGHT OF 4" AT POST 7	
CONNECTING PRECAST CURB SECTIONS (1) & (2):	
FORM OR CORE	1" DIA. HOLE 9" LONG INTO EACH CURB END.
USE	(1) #5 GR.60 REBAR 18" LONG TO CONNECT BOTH CURBS.
SECURING PRECAST OR CAST-IN-PLACE TO FINISHED GRADE *:	
FORM OR CORE	(4) 1" DIA. HOLES, SEE PLAN AND ELEVATION VIEWS FOR HOLE LOCATIONS. DRIVE (4) #5 GR.60 REBAR STAKES 18" LONG INTO THE GROUND AND 1/2" BELOW TOP OF CURB.
FILL HOLES WITH APPROVED GROUT MIXTURE.	

* NOTES: NOT NEEDED FOR CAST-IN-PLACE. SEE TYPE II CURB DETAIL FOR REBAR AND COVER REQUIREMENTS. PERCUSSION DRILLING IS NOT PERMITTED WITH: TYPE II CURB, BRIDGE RAIL OR CONCRETE TRAFFIC RAIL.



NOTE: OPTIONS FOR TYPE II CURB:
 1. PRECAST
 2. CAST-IN-PLACE

GENERAL NOTES

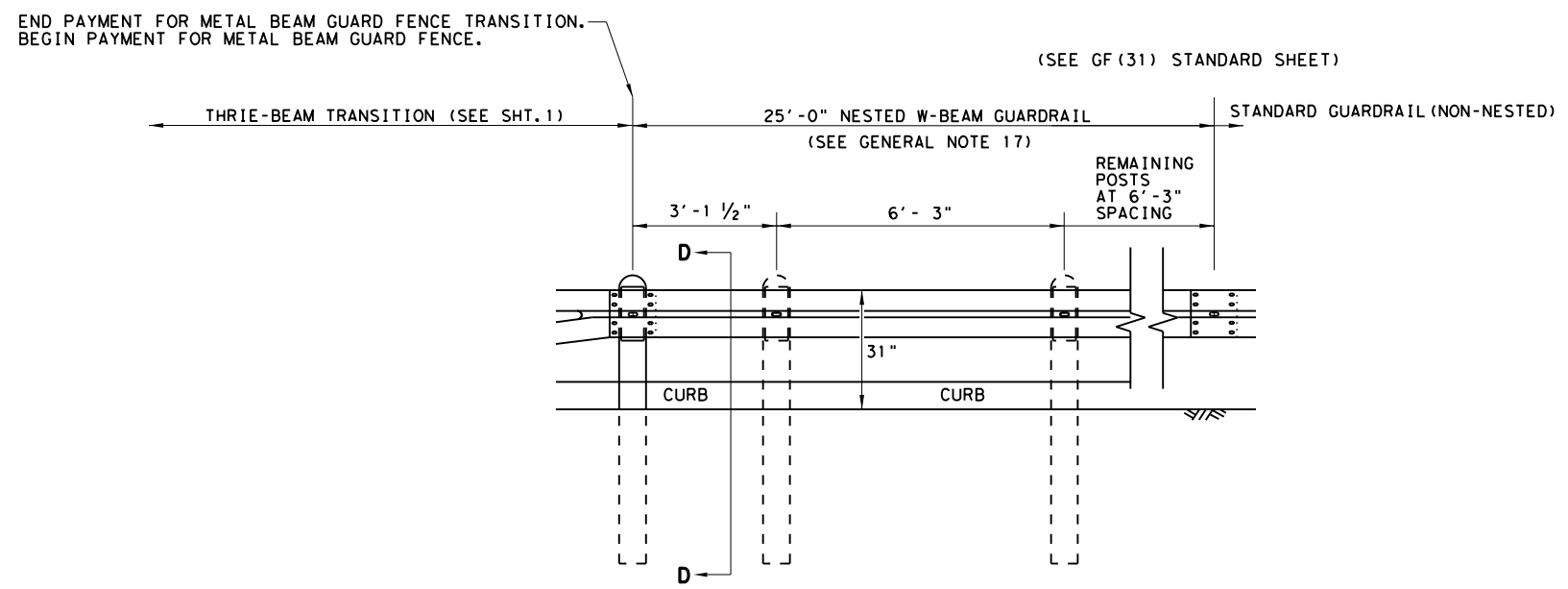
1. CONTACT THE DESIGN DIVISION FOR DRAINAGE CUT OUT OPTIONS NEEDED WITHIN THE CURB SECTION OF THE THRIE-BEAM TRANSITION. (512) 416-2678
2. CONCRETE CURB MAY BE CAST-IN-PLACE OR PRECAST AS SHOWN ON THIS SHEET. WHEN USED IN CONJUNCTION WITH THE THRIE-BEAM TRANSITIONS, CURB SHALL BE TYPE II (5- 3/4" HEIGHT); SEE CURRENT CCG STANDARD SHEET FOR FURTHER DETAILS. IF OTHER CURB HEIGHTS ARE SHOWN IN THE PLANS IN CONJUNCTION WITH THE TRANSITION, THE CURB HEIGHT MAY BE FROM 4" TO 8" WITH A RELATIVELY VERTICAL FACE. CONCRETE CURB SHALL BE CONTINUOUS TO THE SEVENTH POST UNLESS OTHERWISE SHOWN IN THE PLANS. SEE GENERAL NOTE:17 FOR CIRCUMSTANCES WHERE CURB CONTINUES PAST POST 7.
3. CONCRETE CURB TYPE II SUBSIDIARY TO "METAL BEAM GUARD FENCE TRANSITION". IF NO ADDITIONAL CURB IS INDICATED BEYOND THE TRANSITION, THEN ANY CURB HEIGHT GREATER THAN 4" WILL BE TAPERED DOWN BEGINNING AT THE LAST 7 FT. POST TO A MAXIMUM HEIGHT OF 4" AT POST 7. IF SHOWN ELSEWHERE IN THE PLANS, ADDITIONAL CURB UNDERNEATH GUARDRAIL WILL BE PAID FOR BY THE LINEAR FOOT.
4. UNLESS OTHERWISE SHOWN IN THE PLANS, TRANSITIONS SHALL BE PLACED WITH THE BLOCKOUT FACE IN FRONT OF OR DIRECTLY ABOVE THE CURB FACE. SEE SECTION A-A.
5. FOR ROUND WOOD POST SYSTEMS, ALL ROUND WOOD POSTS SHALL BE 7 1/2" DIA. MINIMUM THROUGHOUT THE THRIE-BEAM TRANSITION.
6. THE TYPE OF POST (ROUND WOOD POST, RECTANGULAR WOOD POST OR STEEL POST) WILL BE AS SHOWN IN THE PLANS. REFER TO GF (31) STANDARD SHEET.
7. THE POST LENGTH SHALL BE MARKED ON ALL 7'- 0" LONG POSTS BY THE MANUFACTURER. THE MARK SHALL BE LOCATED WITHIN THE TOP 1 FT. REGION OF THE POST, AT LEAST 5/8" IN HEIGHT, AND VISIBLE AFTER INSTALLATION. WOODEN POSTS SHALL BE MARKED WITH A BRAND, AND STEEL POSTS WITH A STENCIL BEFORE GALVANIZING.
8. POSTS SHALL NOT BE SET IN CONCRETE, OF ANY DEPTH.
9. RAIL ELEMENTS SHALL MEET THE REQUIREMENTS OF ITEM 540, "METAL BEAM GUARD FENCE" EXCEPT AS MODIFIED ON THE PLANS. THE THRIE-BEAM TERMINAL CONNECTOR AND THE THRIE-BEAM TRANSITION TO W-BEAM SHALL BE OF THE SAME MATERIAL, BUT SHALL NOT BE LESS THAN 10 GAUGE. CONTRACTOR SHALL VERIFY THAT THE LOCATIONS OF BOLT HOLES MATCH THOSE IN THE THRIE-BEAM TERMINAL CONNECTOR PRIOR TO ORDERING MATERIALS.
10. BUTTON HEAD "POST BOLTS & NUTS" SHALL MEET THE REQUIREMENTS OF (ASTM A307), AND SHALL BE OF SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND 5/8" WASHER (FWC16G) AND NOT MORE THAN 1" BEYOND IT. TRIM REMAINING BOLT LENGTH TO MEET REQUIRED LENGTH.
11. FITTINGS (BOLTS, NUTS, AND WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING". FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
12. CROWN SHALL BE WIDENED TO ACCOMMODATE TRANSITIONS.
13. WHERE SOLID ROCK IS ENCOUNTERED, CONTACT THE DESIGN DIVISION FOR ADDITIONAL GUIDANCE. (512) 416-2678
14. UNLESS OTHERWISE SHOWN IN THE PLANS, A COMPOSITE MATERIAL BLOCK THAT MEETS THE REQUIREMENTS OF DMS-7210, "COMPOSITE MATERIAL POSTS AND BLOCKS FOR METAL BEAM GUARD FENCE" MAY BE SUBSTITUTED FOR BLOCKS OF SIMILAR DIMENSIONS. TXDOT'S MATERIALS AND TESTS DIVISION MAINTAINS A MATERIAL PRODUCER LIST (MPL) FOR PRODUCERS OF MATERIALS CONFORMING TO DMS-7210. ONLY PRODUCERS ON THE MPL CAN FURNISH COMPOSITE MATERIAL BLOCKS.
15. REFER TO GF (31) STANDARD SHEET & BRIDGE RAILING DETAILS FOR ADDITIONAL DETAILS.
16. THE INSTALLATION OF THE TYPE II CURB IS CRITICAL FOR THE PERFORMANCE OF THE THRIE-BEAM TRANSITION SYSTEM. THE CURB PREVENTS (VEHICLE WHEEL SNAGGING) AT THE CONCRETE RAIL AND IS REQUIRED TO MEET MASH CRASH TEST CRITERIA.
17. IF CURB EXTENDS BEYOND POST 7, 25' OF NESTED W-BEAM GUARDRAIL SHALL BE INSTALLED BEYOND THE PAY LIMITS OF THRIE-BEAM TRANSITION SECTION, (SEE SHT.2). PAYMENT FOR THIS 25' SECTION WILL BE BY LINEAR FOOT, PAY ITEM "0540 6XXX MTL W-BEAM GD FEN (NESTED) (TIM POST)" OR "540 6XXX MTL W-BEAM GD FEN (NESTED) (STEEL POST)" AS APPLICABLE FOR POST TYPE. SEE SHT.2 FOR ADDITIONAL INFORMATION.

**HIGH-SPEED TRANSITION
SHEET 1 OF 2**

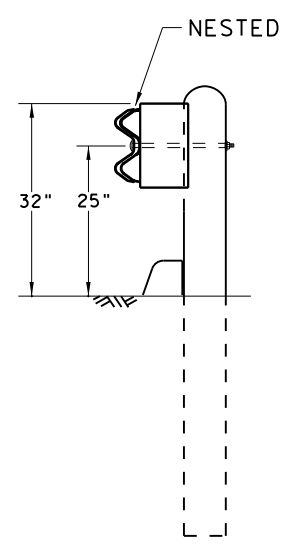
		Design Division Standard	
METAL BEAM GUARD FENCE THRIE-BEAM TRANSITION TL-3 MASH COMPLIANT GF (31) TR TL3-20			
FILE: gf31tr+1320.dgn	DN: TxDOT	CK: KM	DW: VP
© TXDOT: NOVEMBER 2020	CONT: 01	SECT: 036, etc.	SH: 285
DIST: PHR	COUNTY: JIM HOGG	SHEET NO. 265	

DISCLAIMER: THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.
 DATE: 2/12/2024
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\7. Bridge\gf31tr+1320.dgn

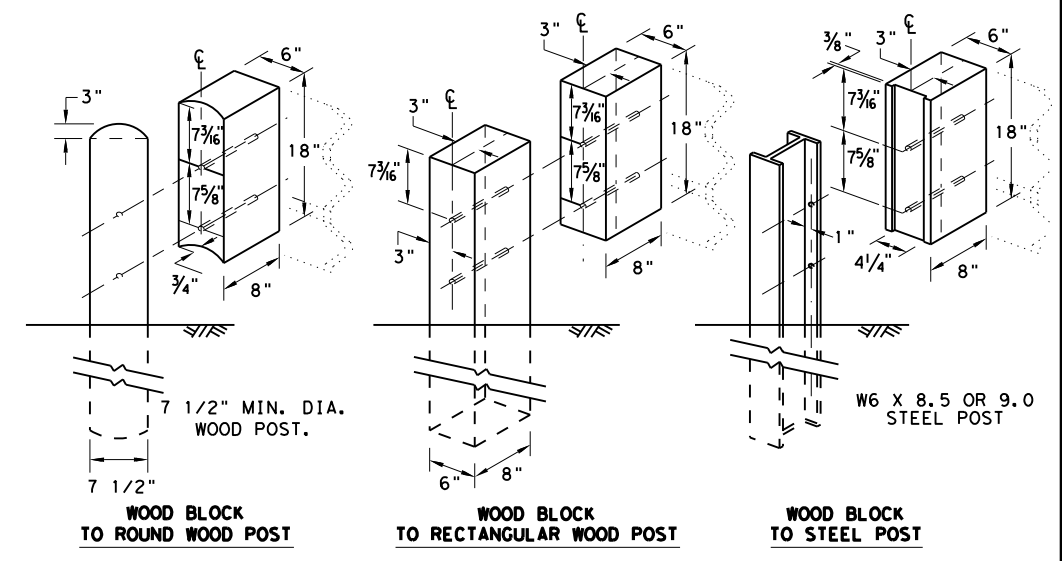
REQUIRED ALTERNATIVE FOR CONTINUOUS CURB EXTENDING PAST POST 7 (SEE SHT. 1 GENERAL NOTE 17)



ELEVATION VIEW



SECTION D-D



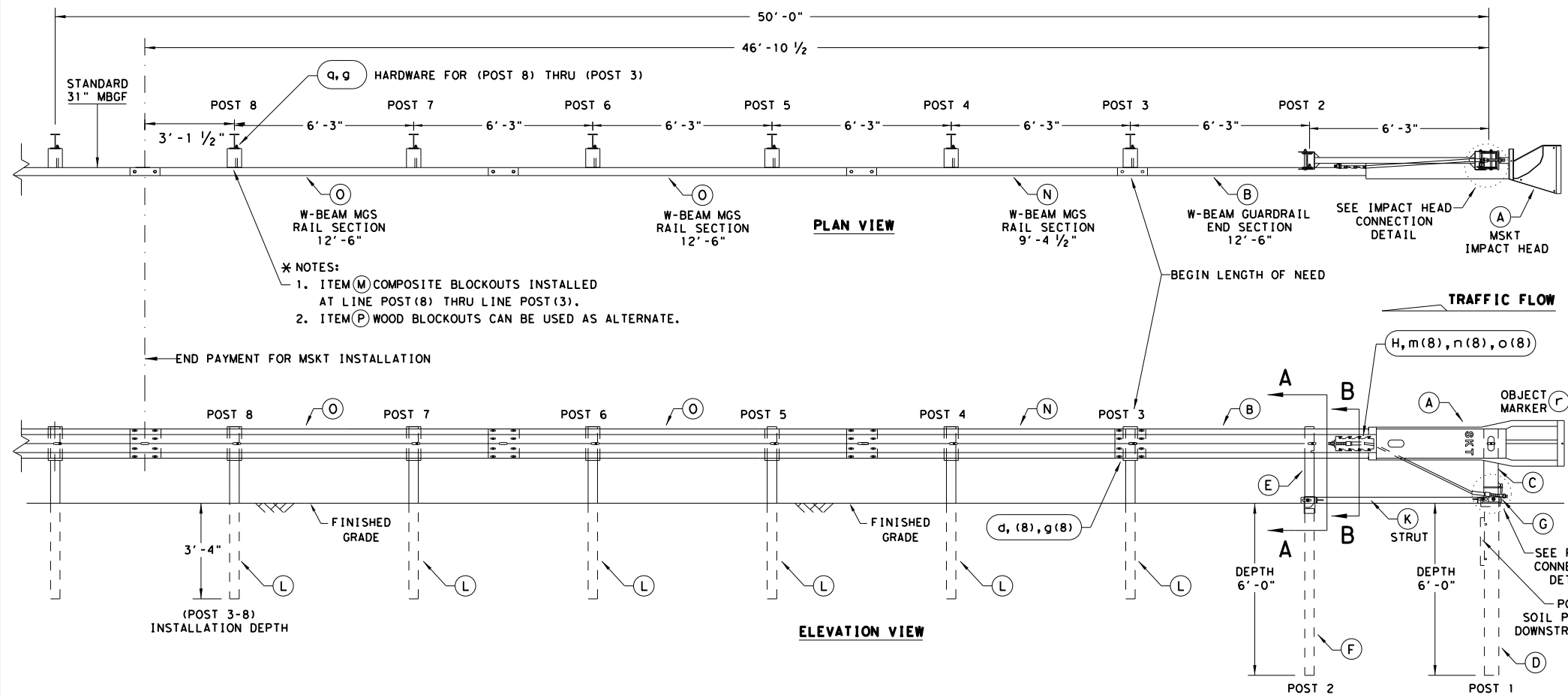
THREE BEAM TRANSITION BLOCKOUT DETAILS

HIGH-SPEED TRANSITION

SHEET 2 OF 2

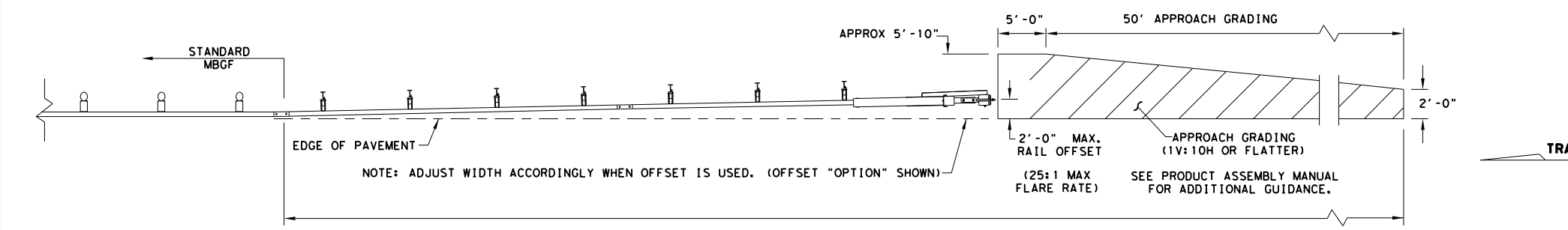
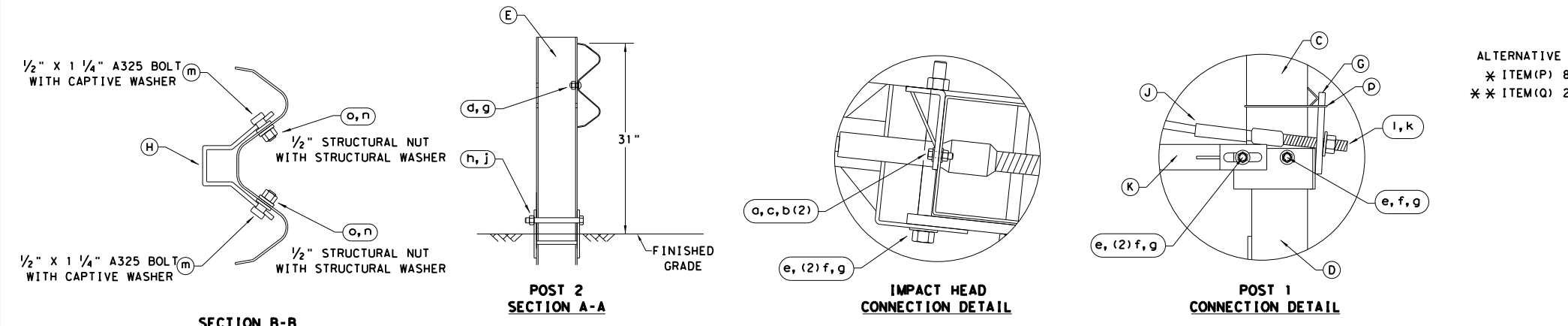
				Design Division Standard
METAL BEAM GUARD FENCE THREE-BEAM TRANSITION TL-3 MASH COMPLIANT GF (31) TR TL3-20				
FILE: gf31tr+1320.dgn	DN: TXDOT	CK: KM	DW: KM	CK: CGL/AG
©TXDOT: NOVEMBER 2020	CONT	SECT	JOB	HIGHWAY
REVISIONS		0482	01	036, etc.
DIST	COUNTY	SHEET NO.		
PHR	JIM HOGG	266		

DISCLAIMER: THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.
 DATE: 2/12/2024
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\7 - Bridge\sgt12s3118.dgn



- GENERAL NOTES**
- FOR SPECIFIC INFORMATION REGARDING INSTALLATION AND TECHNICAL GUIDANCE OF THE SYSTEM, CONTACT: ROAD SYSTEMS, INC. (432)263-2435. 3616 OLD HOWARD COUNTY AIRPORT, BIG SPRING, TX 79720
 - FOR INSTALLATION, REPAIR AND MAINTENANCE REFER TO THE: MSKT END TERMINAL, PRODUCT DESCRIPTION ASSEMBLY MANUAL (PUBLICATION-062717).
 - APPLY HIGH INTENSITY REFLECTIVE SHEETING, "OBJECT MARKER" ON THE FRONT FACE OF THE DEVICE PER MANUFACTURER'S RECOMMENDATIONS. OBJECT MARKER SHALL CONFORM TO THE STANDARDS REQUIRED IN TEXAS MUTCD.
 - FOR POST (LEAVE-OUT) INSTALLATION AND GUIDANCE SEE TXDOT'S LATEST ROADWAY MOW STRIP STANDARD.
 - HARDWARE (BOLTS, NUTS, & WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING". FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
 - SYSTEM SHOWN USING STEEL WIDE FLANGE POSTS WITH COMPOSITE BLOCKOUTS.
 - A COMPOSITE MATERIAL BLOCKOUTS THAT MEETS THE REQUIREMENTS OF DMS-7210, MAY BE SUBSTITUTED FOR BLOCKOUTS OF SIMILAR DIMENSIONS. SEE CONSTRUCTION DIVISION MATERIAL PRODUCER LIST (MPL) FOR CERTIFIED PRODUCERS.
 - IF SOLID ROCK IS ENCOUNTERED IN THE AREA OF (POST 1) AND / OR (POST 2) CONTACT THE MANUFACTURER, & REFER TO THE LATEST ROADWAY MBSG STANDARD FOR INSTALLATION GUIDANCE.
 - POSTS SHALL NOT BE SET IN CONCRETE.
 - SYSTEM MUST BE ATTACHED TO STANDARD 31" MBSG.
 - UNDER NO CIRCUMSTANCES SHALL THE GUARDRAIL WITHIN THE MSKT SYSTEM BE CURVED.
 - A FLARE RATE OF UP TO 25:1 MAY BE USED TO PREVENT THE TERMINAL HEAD FROM ENCRANCHING ON THE SHOULDER. THE FLARE MAY BE DECREASED OR ELIMINATED FOR SPECIFIC INSTALLATIONS, IF DIRECTED BY THE ENGINEER.
 - THE SYSTEM IS SHOWN WITH TWO 12'-6" MBSG PANELS, ONE 25'-0" MBSG PANEL IS ALSO ALLOWED IN ITS PLACE.
 - A DRIVING CAP WITH A TIMBER OR PLASTIC INSERT SHALL BE USED WHEN DRIVING POSTS 3-8 TO PREVENT DAMAGE TO THE GALVANIZING ON TOP OF THE POST. SPECIAL DRIVING CAP TO BE USED ON LOWER POSTS 1 & 2 TO PREVENT DAMAGE TO THE WELDED PLATES.

ITEM	QTY	MAIN SYSTEM COMPONENTS	ITEM NUMBERS
A	1	MSKT IMPACT HEAD	MS3000
B	1	W-BEAM GUARDRAIL END SECTION, 12 Go.	SF1303
C	1	POST 1 - TOP (6" X 6" X 1/8" TUBE)	MTPHP1A
D	1	POST 1 - BOTTOM (6' W6X15)	MTPHP1B
E	1	POST 2 - ASSEMBLY TOP	UHP2A
F	1	POST 2 - ASSEMBLY BOTTOM (6' W6X9)	HP2B
G	1	BEARING PLATE	E750
H	1	CABLE ANCHOR BOX	S760
J	1	BCT CABLE ANCHOR ASSEMBLY	E770
K	1	GROUND STRUT	MS785
L	6	W6X9 OR W6X8.5 STEEL POST	P621
M	6	COMPOSITE BLOCKOUTS	CBSP-14
N	1	W-BEAM MGS RAIL SECTION (9'-4 1/2")	G12025
O	2	W-BEAM MGS RAIL SECTION (12'-6")	G1203A
P	6	WOOD BLOCKOUT 6" X 8" X 14"	P675
Q	1	W-BEAM MGS RAIL SECTION (25'-0")	G1209
SMALL HARDWARE			
a	2	5/8" x 1" HEX BOLT (GRD 5)	B5160104A
b	4	5/8" WASHER	W0516
c	2	5/8" HEX NUT	N0516
d	25	5/8" Dia. x 1 1/4" SPLICE BOLT (POST 2)	B580122
e	2	5/8" Dia. x 9" HEX BOLT (GRD A449)	B580904A
f	3	5/8" WASHER	W050
g	33	5/8" Dia. H.G.R NUT	N050
h	1	3/4" Dia. x 8 1/2" HEX BOLT (GRD A449)	B340854A
j	1	3/4" Dia. HEX NUT	N030
k	2	1 ANCHOR CABLE HEX NUT	N100
l	2	1 ANCHOR CABLE WASHER	W100
m	8	1/2" x 1 1/4" A325 BOLT WITH CAPTIVE WASHER	SB12A
n	8	1/2" STRUCTURAL NUTS	N012A
o	8	1 1/8" O.D. x 3/8" I.D. STRUCTURAL WASHERS	W012A
p	1	BEARING PLATE RETAINER TIE	CT-100ST
q	6	5/8" x 10" H.G.R. BOLT	B581002
r	1	OBJECT MARKER 18" X 18"	E3151



NOTE: TXDOT GENERIC APPROACH GRADING LAYOUT USED FOR ALL TANGENT TYPE END TREATMENTS.

NOTE: THIS STANDARD IS A BASIC REPRESENTATION OF THE MSKT END TERMINAL, IT IS NOT INTENDED TO REPLACE THE PRODUCT DESCRIPTION ASSEMBLY MANUAL.

Design Division Standard

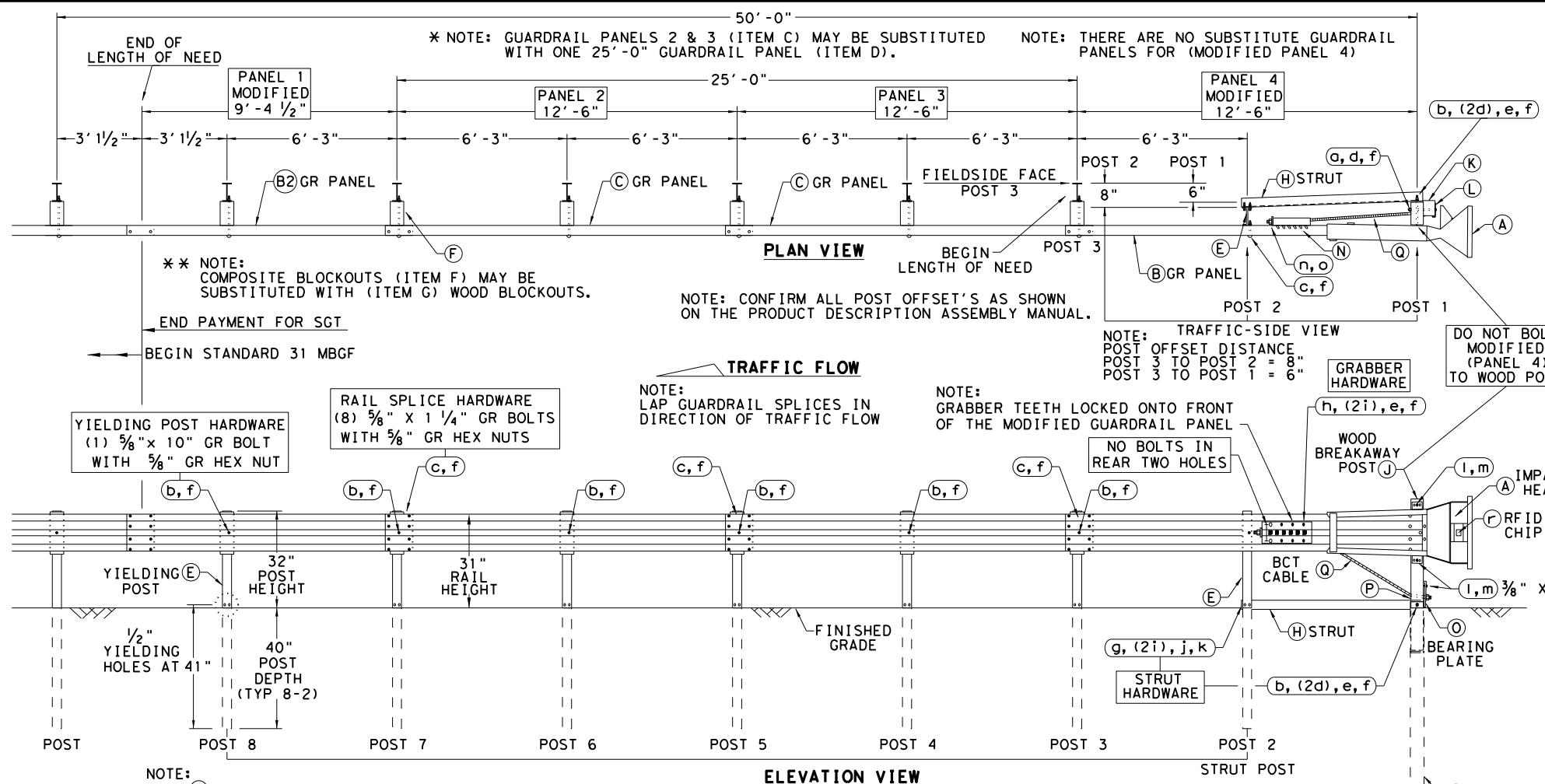
SINGLE GUARDRAIL TERMINAL

MSKT-MASH-TL-3

SGT (12S) 31-18

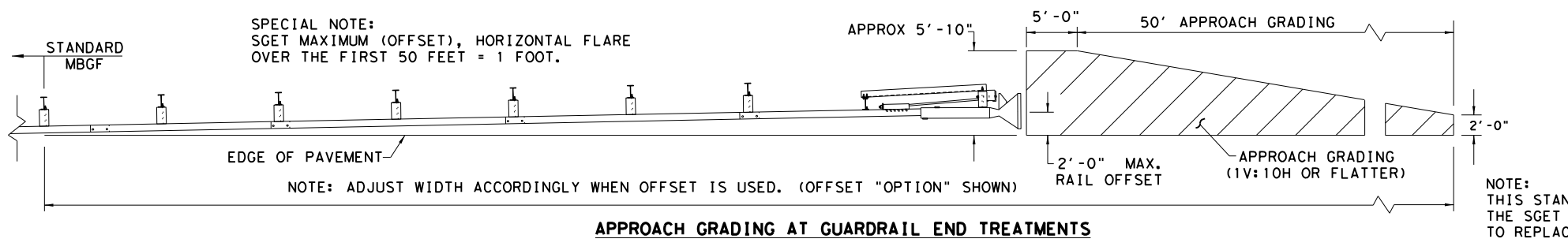
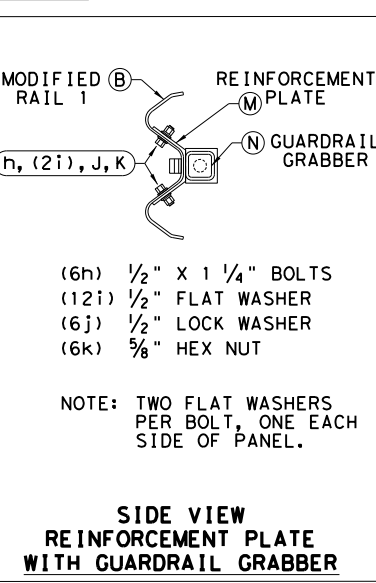
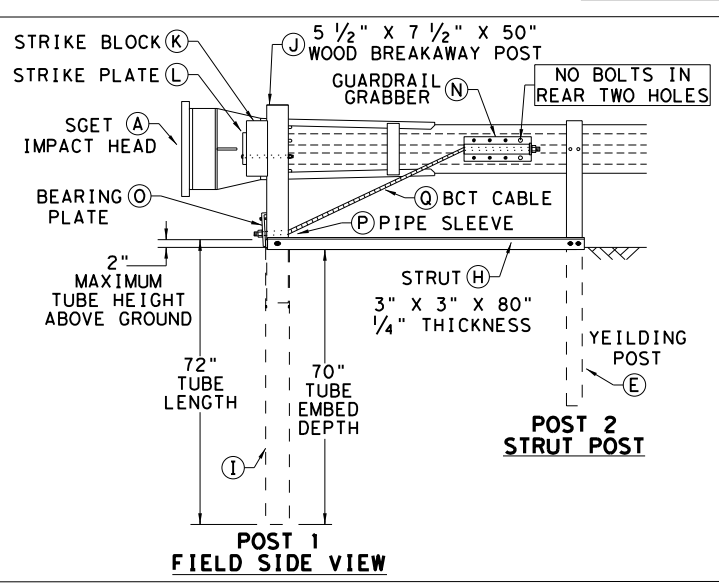
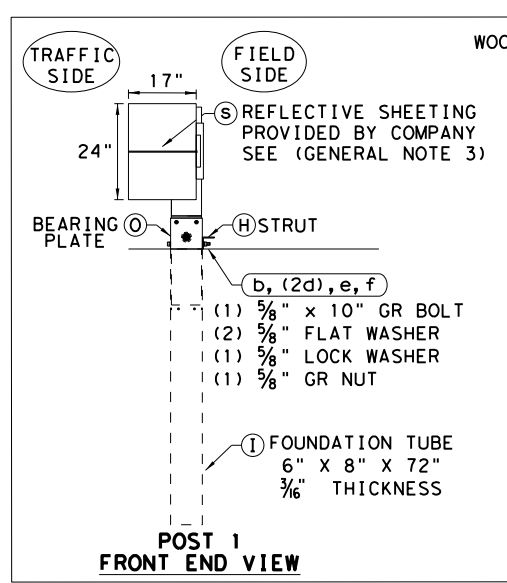
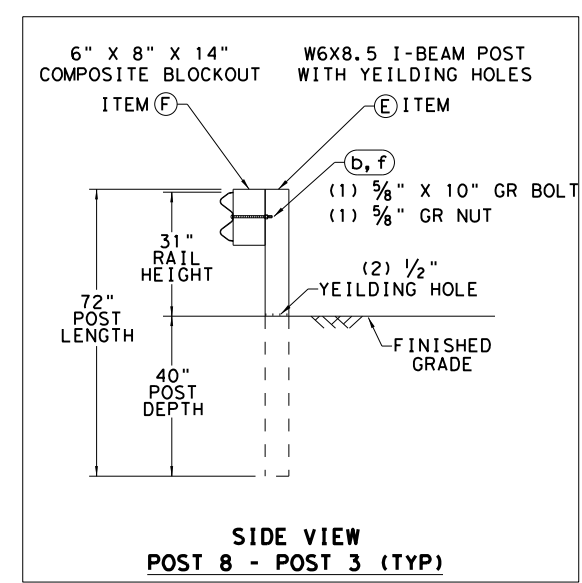
FILE: sgt12s3118.dgn	DN: TXDOT	CK: KM	DW: VP	CK: CL
© TXDOT: APRIL 2018	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
DIST	PHR	COUNTY	JIM HOGG	SHEET NO. 267

DISCLAIMER: THE USE OF THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.



- ### GENERAL NOTES
- FOR SPECIFIC INFORMATION REGARDING INSTALLATION AND TECHNICAL GUIDANCE OF THE SYSTEM, CONTACT: SPIG INDUSTRY, INC. AT 1(267) 644-9510. 14675 INDUSTRIAL PARK RD; BRISTOL, VA 24202
 - FOR INSTALLATION, REPAIR AND MAINTENANCE REFER TO THE MANUFACTURER'S; SGET END TERMINAL, PRODUCT DESCRIPTION ASSEMBLY MANUAL.
 - MANUFACTURER WILL APPLY HIGH INTENSITY REFLECTIVE SHEETING, "OBJECT MARKER" TO THE FACE PLATE OF THE DEVICE PER MANUFACTURER'S RECOMMENDATIONS. THE OBJECT MARKER SHALL CONFORM TO THE STANDARDS REQUIRED IN TEXAS MUTCD.
 - THE NOMINAL HEIGHT OF THE GUARDRAIL BEAM IS 31 INCHES WITH A TOLERANCE OF +/- ONE INCH.
 - FOR POST (LEAVE-OUT) INSTALLATION AND GUIDANCE SEE TXDOT'S LATEST ROADWAY MOW STRIP STANDARD.
 - (POST 2 THROUGH POST 8) ARE MODIFIED STEEL-YIELDING POSTS WITH YIELDING HOLES AT GROUND LEVEL. THERE ARE NO SUBSTITUTE POSTS.
 - POSTS SHALL NOT BE SET IN CONCRETE.
 - IF SOLID ROCK IS ENCOUNTERED FOR ANY OF THE POSTS IN THE SYSTEM, CONTACT THE MANUFACTURER FOR SPECIFIC INSTALLATION GUIDANCE.
 - HARDWARE (BOLTS, NUTS, & WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING". FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
 - A COMPOSITE MATERIAL BLOCKOUT THAT MEETS DMS-7210 REQUIREMENTS MAY BE SUBSTITUTED FOR AN APPROVED WOOD BLOCKOUT. SEE CONSTRUCTION DIVISION MATERIAL PRODUCER LIST (MPL) FOR CERTIFIED PRODUCERS.
 - THE ENTIRE SYSTEM MUST BE INSTALLED IN A STRAIGHT LINE WITHOUT ANY CURVE. HOWEVER, THE SYSTEM CAN BE OFFSET BY TWO FEET AS SHOWN ON THE APPROACH GRADING DETAIL TO HELP OFF-SET THE IMPACT HEAD FROM SHOULDER OF THE ROAD.

ITEM	QTY	MAIN SYSTEM COMPONENTS	ITEM #
A	1	SGET IMPACT HEAD	SIH1A
B	1	MODIFIED GUARDRAIL PANEL 12'-6" 12GA	126SPZGP
B2	1	MODIFIED GUARDRAIL PANEL 9'-4 1/2" 12GA	GP94
C	2	STANDARD GUARDRAIL PANEL 12'-6" 12GA	GP126
D	1	STANDARD GUARDRAIL PANEL 25'-0" 12GA	GP25
E	7	MODIFIED YIELDING I-BEAM POST W6x8.5	YP6MOD
F	6	COMPOSITE BLOCKOUT 6" X 8" X 14"	CBO8
G	6	WOOD BLOCKOUT 6" X 8" X 14"	WBO8
H	1	STRUT 3" X 3" X 80" X 1/4" A36 ANGLE	STR80
I	1	FOUNDATION TUBE 6" X 8" X 72" X 3/8"	FNDT6
J	1	WOOD BREAKAWAY POST 5 1/2" X 7 1/2" X 50"	WBRK50
K	1	WOOD STRIKE BLOCK	WSBK14
L	1	STRIKE PLATE 1/4" A36 BENT PLATE	SPLT8
M	1	REINFORCEMENT PLATE 12 GA. GR55	REPLT17
N	1	GUARDRAIL GRABBER 2 1/2" X 2 1/2" X 16 1/2"	GGR17
O	1	BEARING PLATE 8" X 8 5/8" X 5/8" A36	BPLT8
P	1	PIPE SLEEVE 4 1/4" X 2 3/8" O.D. (2 1/8" I.D.)	PSLV4
Q	1	BCT CABLE 3/4" X 81" LENGTH	CBL81
SMALL HARDWARE			
q	1	5/8" X 12" GUARDRAIL BOLT 307A HDG	12GRBLT
b	7	5/8" X 10" GUARDRAIL BOLT 307A HDG	10GRBLT
c	33	5/8" X 1 1/4" GR SPlice BOLTS 307A HDG	1GRBLT
d	3	5/8" FLAT WASHER F436 A325 HDG	58FW436
e	1	5/8" LOCK WASHER HDG	58LW
f	39	5/8" GUARDRAIL HEX NUT HDG	58HN563
g	2	1/2" X 2" STRUT BOLT A325 HDG	2BLT
h	6	1/2" X 1 1/4" PLATE BOLT A325 HDG	125BLT
i	16	1/2" FLAT WASHER F436 A325 HDG	12FWF436
j	8	1/2" LOCK WASHER HDG	12LW
k	8	1/2" HEX NUT A563 HDG	12HN563
l	4	3/8" X 3" HEX LAG SCREW GR5 HDG	38LS
m	4	3/8" FLAT WASHER F436 A325 HDG	38FW844
n	2	1" FLAT WASHER F436 A325 HDG	1FWF436
o	2	1" HEX NUT A563HD HDG	1HN563
p	1	18" TO 24" LONG ZIP TIE RATED 175-200LB	ZPT18
q	1	1 1/2" X 4" SCH-40 PVC PIPE	PSPCR4
r	1	RFID CHIP RATED MIL-STD-810F	RFID810F
s	1	IMPACT HEAD REFLECTIVE SHEETING	RS30M



NOTE: THIS STANDARD IS A BASIC REPRESENTATION OF THE SGET TERMINAL SYSTEM AND IS NOT INTENDED TO REPLACE THE MANUFACTURER'S ASSEMBLY MANUAL.

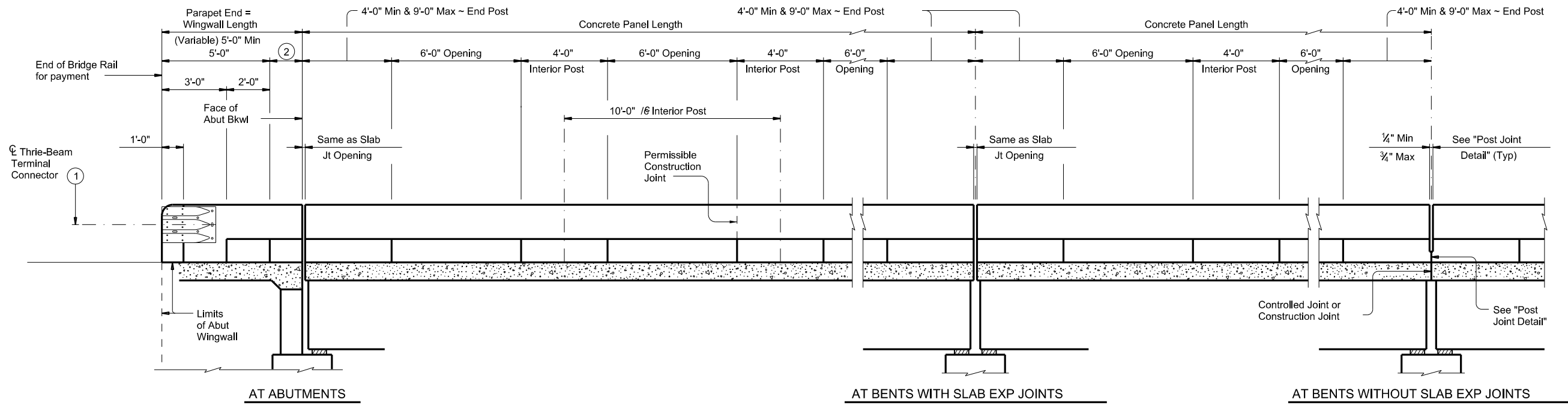
Texas Department of Transportation
Design Division Standard

SPIG INDUSTRY, LLC
SINGLE GUARDRAIL TERMINAL
SGET - TL-3 - MASH
SGT (15) 31-20

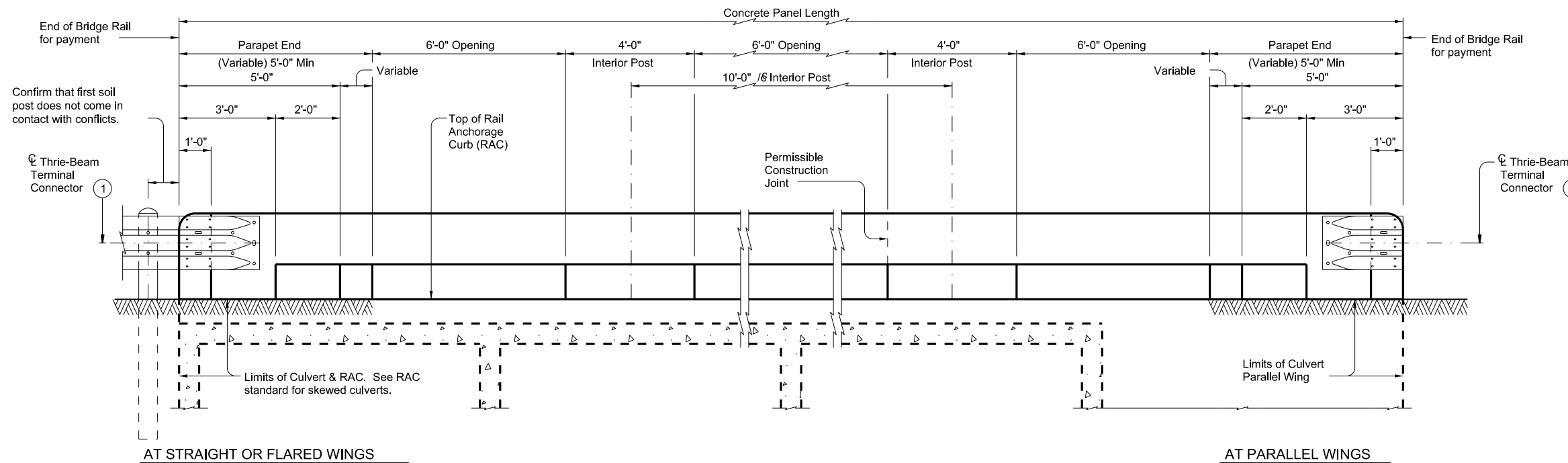
FILE: sg153120.dgn	DN: TXDOT	CK: KM	DW: VP	CK: VP
© TXDOT: APRIL 2020	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	268	

DATE: FILE:

DATE: 2/12/2024 8:14:26 AM
 FILE: \\txdot.projectwiseonline.com\PHR\Design\Projects\048201\036\etc\T-19.dgn
 PHR\Design\Projects\048201\036\etc\T-19.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or for the accuracy of the information contained herein.



ROADWAY ELEVATION OF RAIL ON BRIDGE



ROADWAY ELEVATION OF RAIL ON BOX CULVERTS

Showing 0° skew culvert. Skewed culverts similar. See RAC standard for details not shown.
Vertical joints in concrete rail are not required, unless shown elsewhere.

- ① Terminal Connectors and associated hardware are to be paid for under the Item "Metal Beam Guard Fence". Attach Metal Beam Guard Fence Transitions to the bridge rail and extend along the embankment unless otherwise shown in the plans.
- ② Wingwall Length minus 5'-0" (Varies)

SHEET 1 OF 3

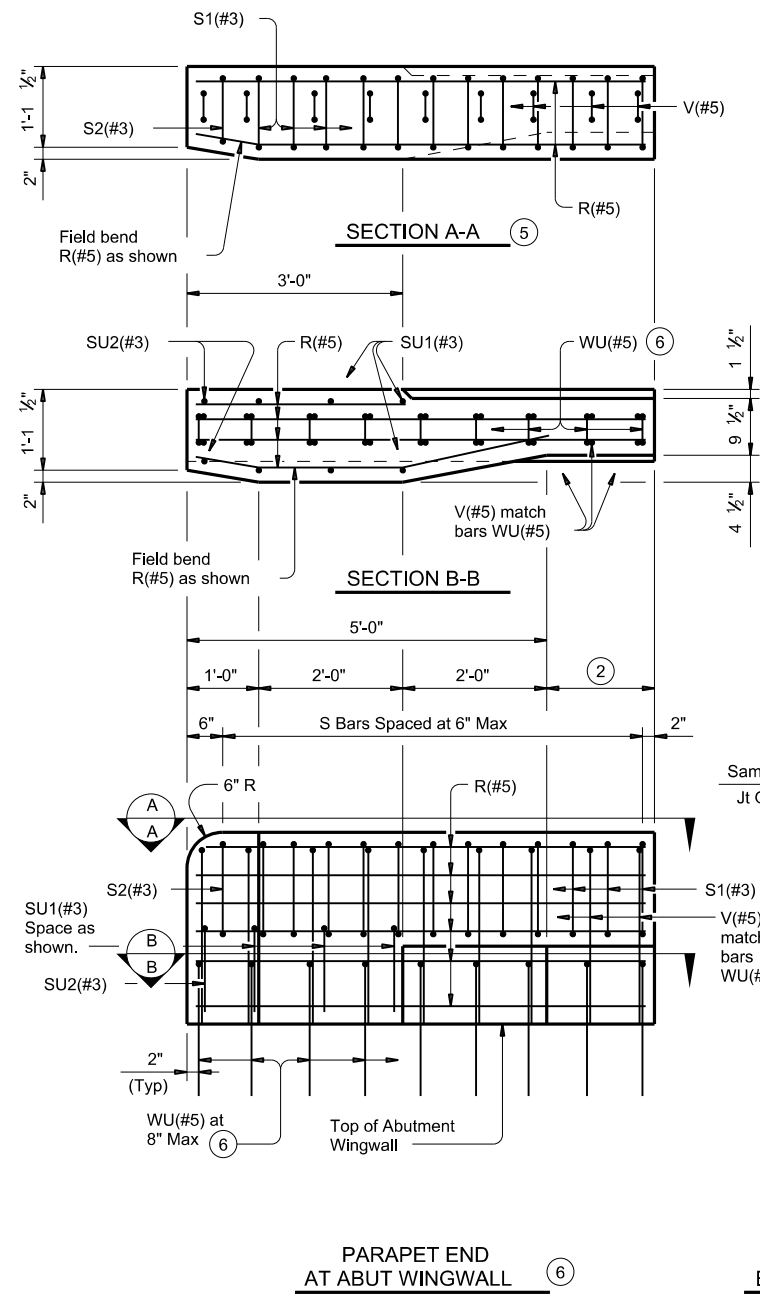


TRAFFIC RAIL

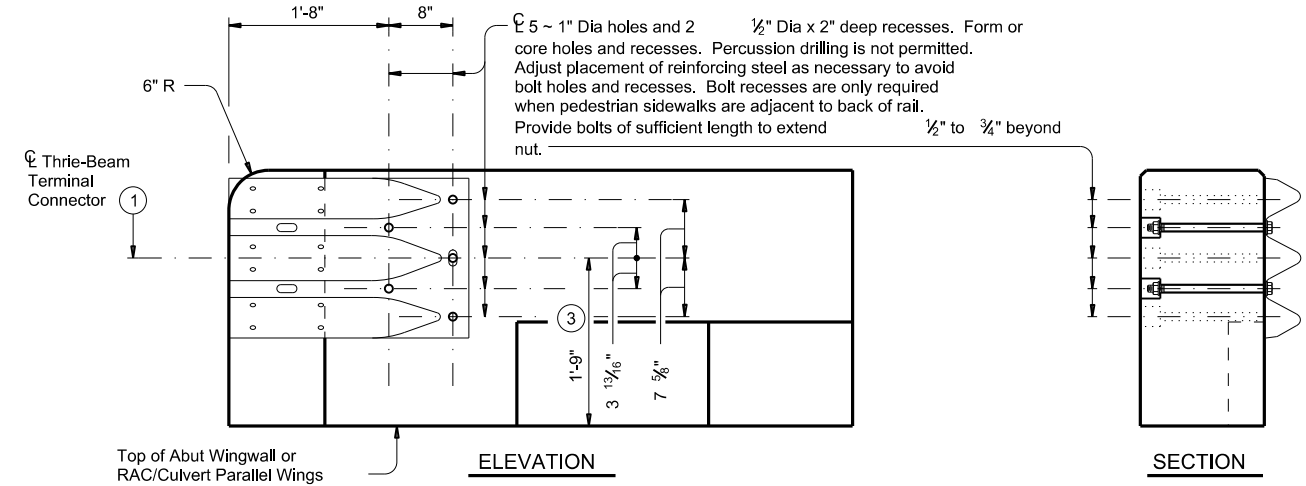
TYPE T223

FILE: rstd005-19.dgn	DN: TxDOT	CK: TxDOT	DW: JTR	CK: AES
©TxDOT	0482 01	036, etc.	SH 285	
REVISIONS	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	269	

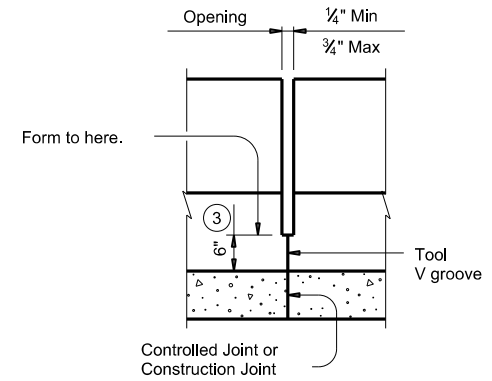
DATE: 2/12/2024 8:14:27 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT15\Documents\21 - PHR\Design\Project\1048201\02010501.dwg or Project\1048201\02010501.dgn
 PHR\Design\Project\1048201\02010501.dwg or Project\1048201\02010501.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information from a drawing to a physical model or vice versa.



PARAPET END AT ABUT WINGWALL ⑥

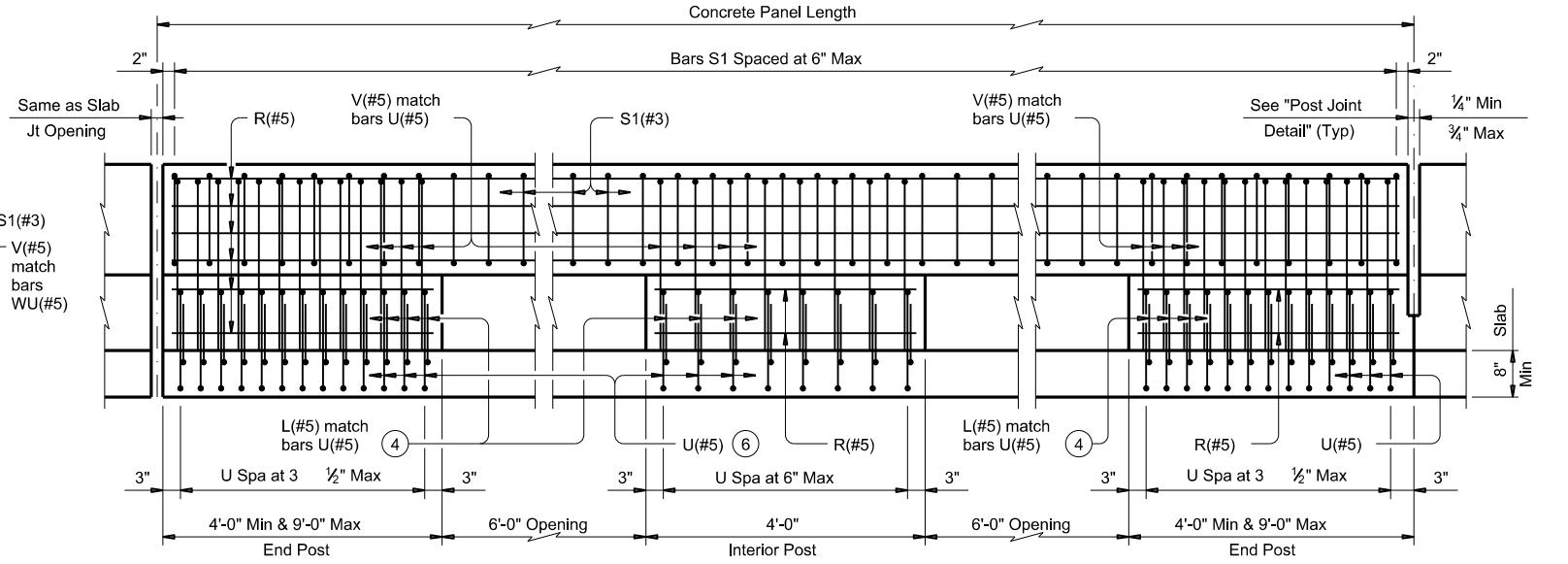


TERMINAL CONNECTION DETAILS



POST JOINT DETAIL

Provide at all interior bents without slab expansion joints.



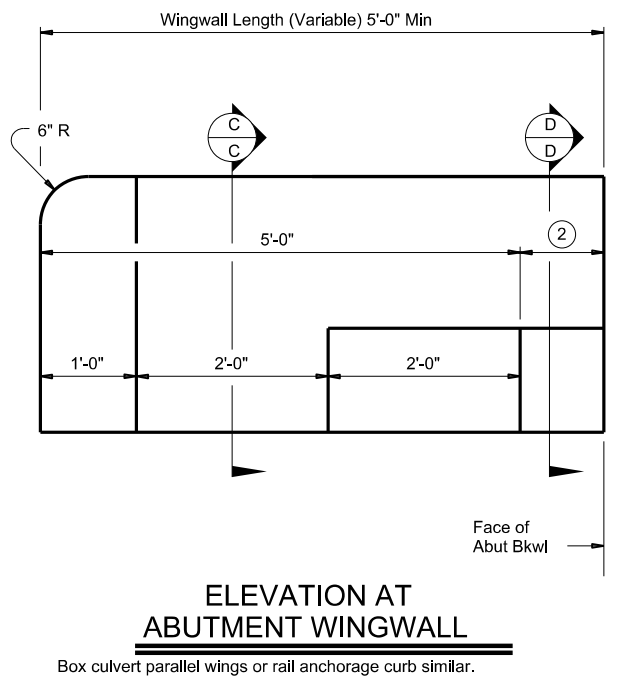
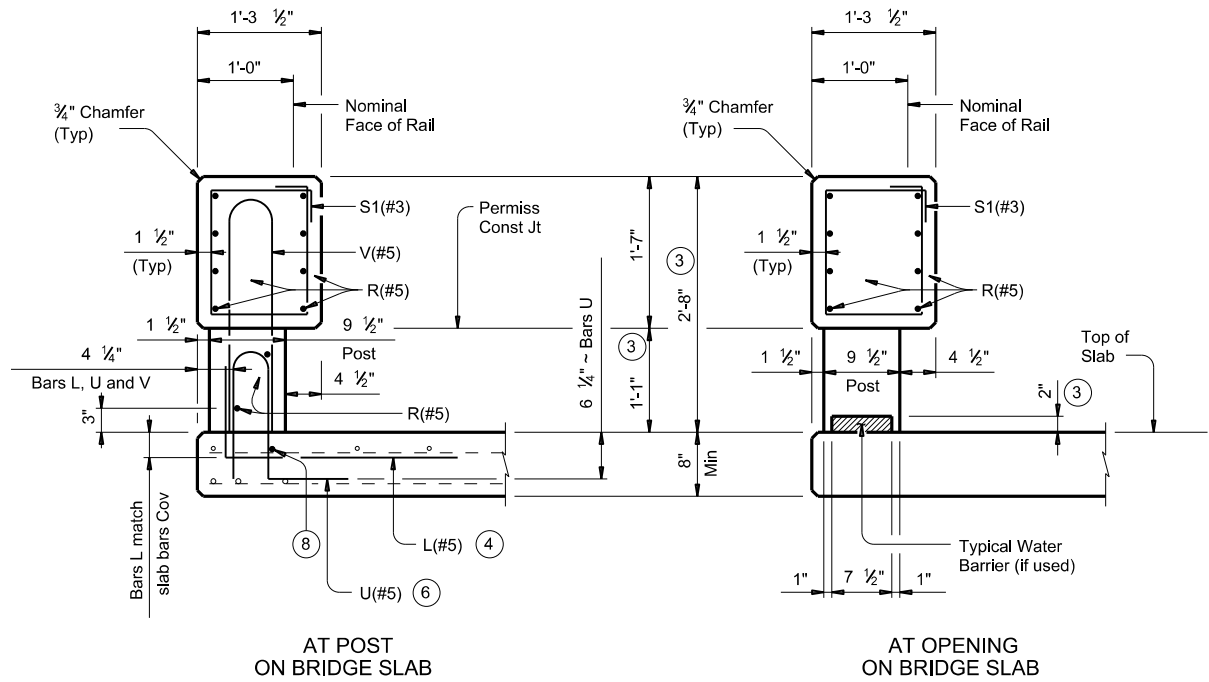
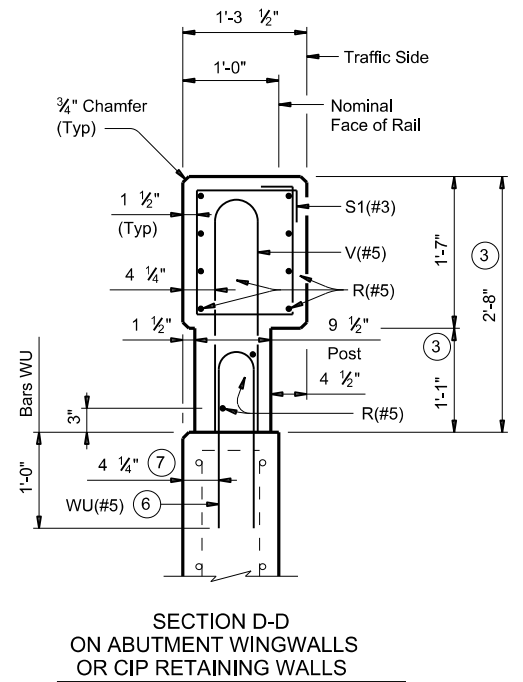
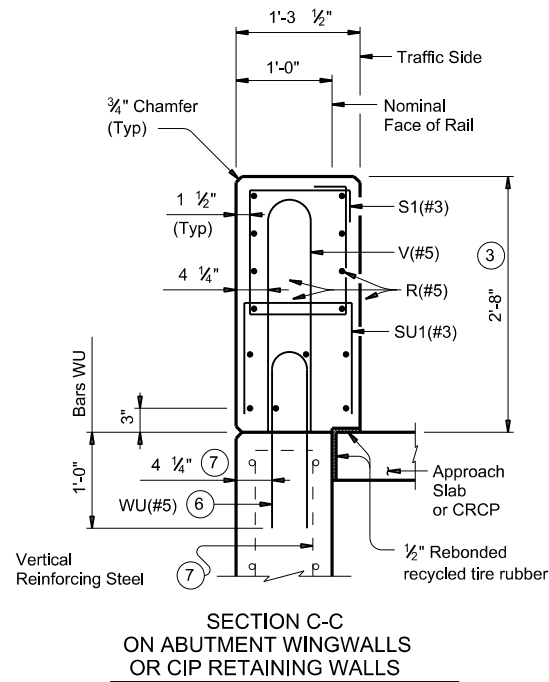
ELEVATION SHOWING TYPICAL REINFORCING PLACEMENT

Showing rail on slab. Rail on box culvert similar.

- ① Terminal Connectors and associated hardware are to be paid for under the Item "Metal Beam Guard Fence". Attach Metal Beam Guard Fence Transitions to the bridge rail and extend along the embankment unless otherwise shown in the plans.
- ② Wingwall Length minus 5'-0" (Varies)
- ③ Increase 2" for structures with overlay.
- ④ Bars L(#5) are part of rail reinforcing and are included in unit price bid for railing. Space with Bars U. Bars L match slab bar cover. Bars L may be bundled with top slab reinforcing if spacing is equivalent.
- ⑤ Bars SU1(#3), SU2(#3) and WU(#5) not shown for clarity.
- ⑥ Substitute Bars U(#5) for Bars WU(#5) when parapet end is located on anchorage curb over culvert top slab. Use Bars WU(#5) in culvert parallel wings.

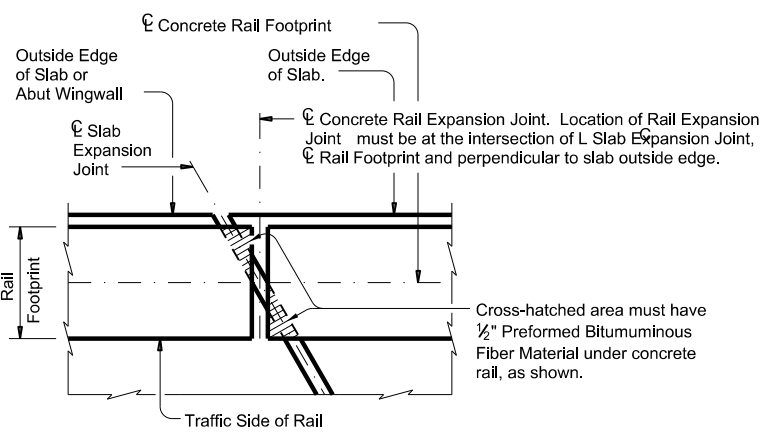
		Bridge Division Standard	
<h1>TRAFFIC RAIL</h1>			
<h2>TYPE T223</h2>			
FILE: rdst005-19.dgn	DN: TxDOT	CK: TxDOT	DW: JTR
©TxDOT September 2019	CONT: 0482	SECT: 01	JOB: 036, etc.
REVISIONS	DIST: PHR		COUNTY: JIM HOGG
			SHEET NO. 270

DATE: 2/12/2024 8:14:28 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Project\1040000000\1040000000.dgn
 PHR\Design Project\1040000000\1040000000.dgn
 No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this drawing to any other format or for the use of this drawing in any other project.



SECTIONS THRU RAIL
Sections on box culverts similar.

- ② Wingwall Length minus 5'-0" (Varies)
- ③ Increase 2" for structures with overlay.
- ④ Bars L(#5) are part of rail reinforcing and are included in unit price bid for railing. Space with Bars U. Bars L match slab bar cover. Bars L may be bundled with top slab reinforcing if spacing is equivalent.
- ⑥ Substitute Bars U(#5) for Bars WU(#5) when parapet end is located on anchorage curb over culvert top slab. Use Bars WU(#5) in culvert parallel wings.
- ⑦ When vertical reinforcing has closer clear cover over horizontal reinforcing in abutment wingwalls on traffic side of wall, move the horizontal wingwall/retaining wall reinforcing to the inside of Bars WU where bars conflict.
- ⑧ Top longitudinal slab bar may be adjusted laterally 3" plus or minus to tie reinforcing.
- ⑨ At the Contractor's option, Bars V may be replaced by extending Bars U to 2'-5" $\frac{1}{4}$ " above the roadway surface without overlay.



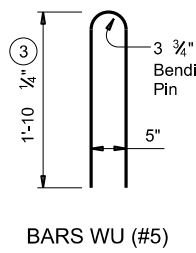
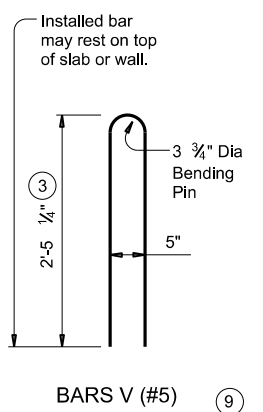
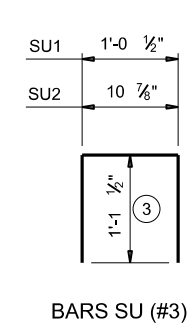
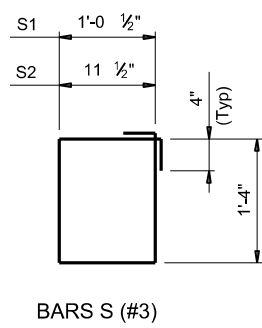
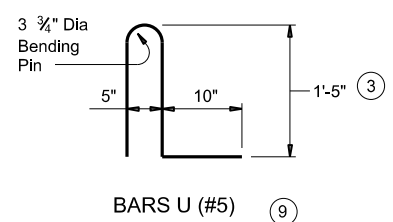
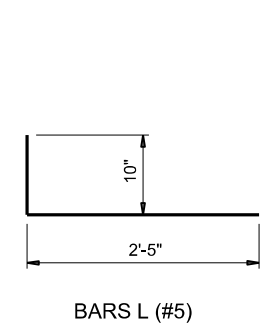
PLAN OF RAIL AT EXPANSION JOINTS
Example showing Slab Expansion Joints without breakbacks.

CONSTRUCTION NOTES:
 Face of rail and parapet must be vertical transversely unless otherwise shown in the plans or approved by the Engineer.
 Provide water barriers at openings draining onto undercrossing roadways and sidewalks. They may be cast-in-place or precast in convenient lengths and bonded to the bridge deck with an approved epoxy cement.
 Chamfer all exposed corners.

MATERIAL NOTES:
 Provide Class "C" concrete. Provide Class "C" (HPC) if required elsewhere.
 Provide Grade 60 reinforcing steel.
 Epoxy coat or galvanize all reinforcing steel if slab bars are epoxy coated or galvanized.
 Deformed Welded Wire Reinforcing (WWR) (ASTM A1064) of equal size and spacing may be substituted for Bars U, V, and WU unless noted otherwise. Provide the same laps as required for reinforcing bars.
 Provide bar laps, where required, as follows:
 Uncoated or galvanized ~ #5 = 2'-0"
 Epoxy coated ~ #5 = 3'-0"

GENERAL NOTES:
 This rail has been evaluated by full-scale crash test to meet MASH TL-3 criteria. This rail can be used for speeds of 50 mph and greater when a TL-3 rated guard fence transition is used. When a TL-2 rated guard fence transition is used, this rail can only be used for speeds of 45 mph and less.
 Do not use this railing on bridges with expansion joints providing more than 5" movement.
 Rail anchorage details shown on this standard may require modification for select structure types. See appropriate details elsewhere in plans for these modifications.
 Shop drawings are not required for this rail.
 Average weight of railing with no overlay is 358 plf.

Cover dimensions are clear dimensions, unless noted otherwise.
Reinforcing bar dimensions shown are out-to-out of bar.



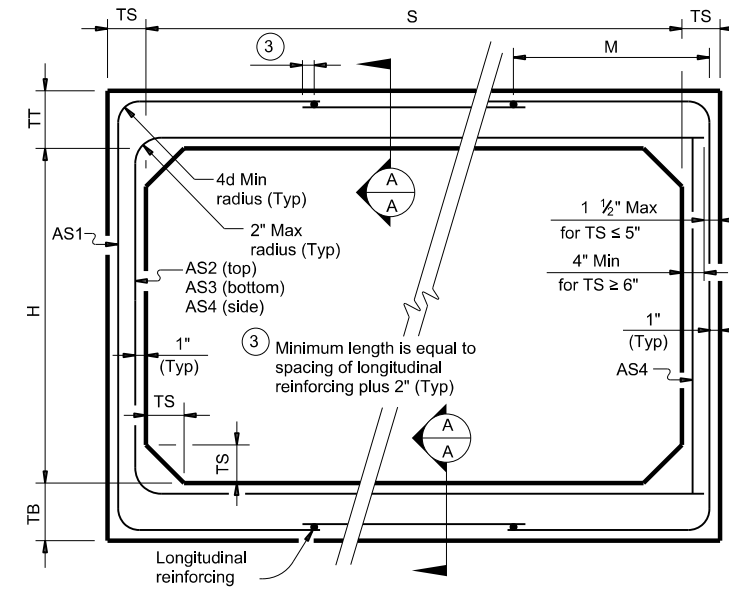
SHEET 3 OF 3

		Bridge Division Standard	
<h1>TRAFFIC RAIL</h1>			
<h2>TYPE T223</h2>			
FILE: tstd005-19.dgn	DN: TxDOT	CK: TxDOT	DW: JTR
©TxDOT September 2019	CONT	SECT	JOB
REVISIONS	0482 01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	271	

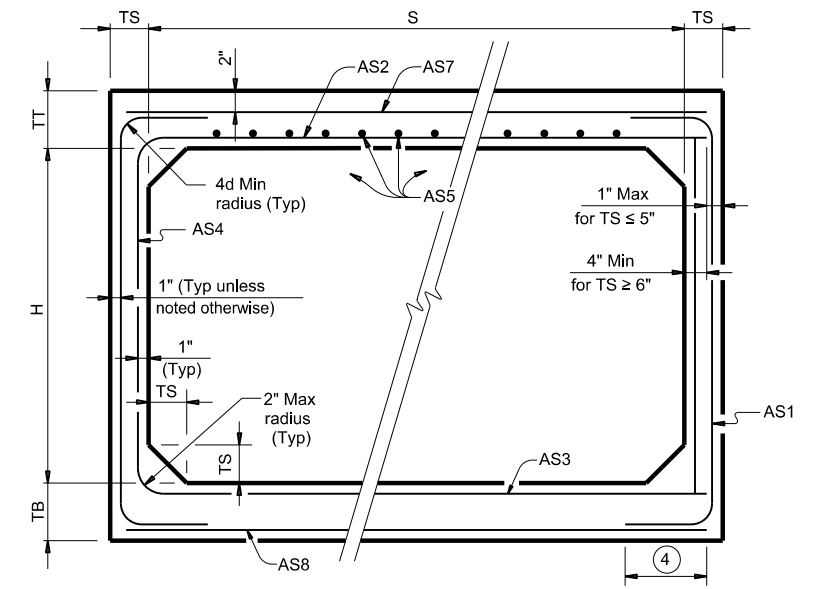
DATE: 2/12/2024 8:14:32 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design\Projects\06sts-20.dgn
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units.

BOX DATA

SECTION DIMENSIONS					Fill Height (ft.)	M (Min) (in.)	REINFORCING (sq. in. / ft.) ^②								Lift Weight (tons) ^①
S (ft.)	H (ft.)	TT (in.)	TB (in.)	TS (in.)			AS1	AS2	AS3	AS4	AS5	AS7	AS8		
6	2	8	7	7	< 2	-	0.23	0.27	0.19	0.17	0.19	0.19	0.17	7.2	
6	2	7	7	7	2 < 3	43	0.25	0.21	0.17	0.17	-	-	-	6.8	
6	2	7	7	7	3 - 5	43	0.20	0.17	0.17	0.17	-	-	-	6.8	
6	2	7	7	7	10	39	0.20	0.17	0.17	0.17	-	-	-	6.8	
6	2	7	7	7	15	39	0.26	0.20	0.20	0.17	-	-	-	6.8	
6	2	7	7	7	20	39	0.34	0.26	0.26	0.17	-	-	-	6.8	
6	2	7	7	7	25	39	0.43	0.32	0.32	0.17	-	-	-	6.8	
6	2	7	7	7	30	39	0.52	0.38	0.39	0.17	-	-	-	6.8	
6	3	8	7	7	< 2	-	0.20	0.31	0.22	0.17	0.19	0.19	0.17	7.9	
6	3	7	7	7	2 < 3	43	0.21	0.24	0.19	0.17	-	-	-	7.5	
6	3	7	7	7	3 - 5	39	0.17	0.18	0.17	0.17	-	-	-	7.5	
6	3	7	7	7	10	39	0.17	0.18	0.19	0.17	-	-	-	7.5	
6	3	7	7	7	15	38	0.22	0.24	0.24	0.17	-	-	-	7.5	
6	3	7	7	7	20	38	0.28	0.31	0.31	0.17	-	-	-	7.5	
6	3	7	7	7	25	38	0.35	0.38	0.39	0.17	-	-	-	7.5	
6	3	7	7	7	30	38	0.42	0.46	0.46	0.17	-	-	-	7.5	
6	4	8	7	7	< 2	-	0.19	0.34	0.25	0.17	0.19	0.19	0.17	8.6	
6	4	7	7	7	2 < 3	43	0.19	0.27	0.21	0.17	-	-	-	8.2	
6	4	7	7	7	3 - 5	39	0.17	0.21	0.19	0.17	-	-	-	8.2	
6	4	7	7	7	10	39	0.17	0.20	0.21	0.17	-	-	-	8.2	
6	4	7	7	7	15	38	0.18	0.27	0.27	0.17	-	-	-	8.2	
6	4	7	7	7	20	38	0.24	0.34	0.35	0.17	-	-	-	8.2	
6	4	7	7	7	25	38	0.29	0.43	0.42	0.17	-	-	-	8.2	
6	4	7	7	7	30	38	0.35	0.51	0.52	0.17	-	-	-	8.2	
6	5	8	7	7	< 2	-	0.19	0.37	0.28	0.17	0.19	0.19	0.17	9.3	
6	5	7	7	7	2 < 3	43	0.17	0.30	0.24	0.17	-	-	-	8.9	
6	5	7	7	7	3 - 5	43	0.17	0.23	0.21	0.17	-	-	-	8.9	
6	5	7	7	7	10	39	0.17	0.22	0.23	0.17	-	-	-	8.9	
6	5	7	7	7	15	38	0.17	0.28	0.29	0.17	-	-	-	8.9	
6	5	7	7	7	20	38	0.20	0.37	0.38	0.17	-	-	-	8.9	
6	5	7	7	7	25	38	0.25	0.45	0.46	0.17	-	-	-	8.9	
6	5	7	7	7	30	38	0.30	0.54	0.55	0.17	-	-	-	8.9	
6	6	8	7	7	< 2	-	0.19	0.38	0.30	0.17	0.19	0.19	0.17	10	
6	6	7	7	7	2 < 3	52	0.17	0.32	0.26	0.17	-	-	-	9.6	
6	6	7	7	7	3 - 5	52	0.17	0.24	0.22	0.17	-	-	-	9.6	
6	6	7	7	7	10	43	0.17	0.23	0.24	0.17	-	-	-	9.6	
6	6	7	7	7	15	39	0.17	0.29	0.31	0.17	-	-	-	9.6	
6	6	7	7	7	20	39	0.18	0.38	0.39	0.17	-	-	-	9.6	
6	6	7	7	7	25	38	0.23	0.46	0.48	0.17	-	-	-	9.6	
6	6	7	7	7	30	38	0.27	0.55	0.57	0.17	-	-	-	9.6	



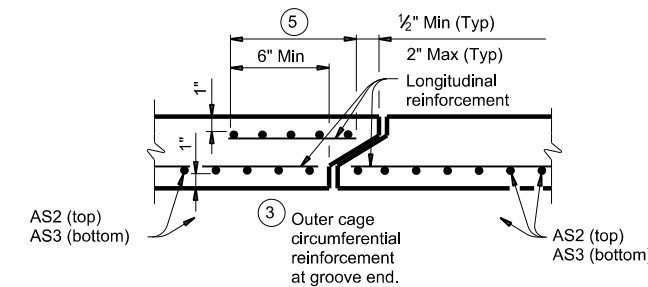
CORNER OPTION "A"



CORNER OPTION "B"

FILL HEIGHT 2 FT AND GREATER

FILL HEIGHT LESS THAN 2 FT



SECTION A-A
(Showing top and bottom slab joint reinforcement.)

MATERIAL NOTES:
 Provide 0.03 sq. in./ft. minimum longitudinal reinforcement at each face in slabs and walls. This minimum requirement may be met by the transverse wires when wire mesh reinforcement is used.
 Provide Class H concrete (f'c = 5,000 psi).

GENERAL NOTES:
 Designs shown conform to ASTM C1577. Refer to ASTM C1577 for information or details not shown.
 See Box Culverts Precast Miscellaneous Details (SCP-MD) standard sheet for details and notes not shown.
 In lieu of furnishing the designs shown on this sheet, the contractor may furnish an alternate design that is equal to or exceeds the box design for the design fill height in the table. Submit shop plans for alternate designs in accordance with Item "Precast Concrete Structural Members (Fabrication)".

HL93 LOADING

		Bridge Division Standard	
SINGLE BOX CULVERTS PRECAST 6'-0" SPAN			
SCP-6			
FILE:	scp06sts-20.dgn	DN: TxDOT	CR: TxDOT
©TxDOT	February 2020	CON: 0482	SECT: 01
REVISIONS		JOB: 036, etc.	HIGHWAY: SH 285
DIST: PHR	COUNTY: JIM HOGG	SHEET NO. 272	

① For box length = 8'-0"
 ② AS1 thru AS4, AS7 and AS8 are minimum required areas of reinforcement per linear foot of box length. AS5 is minimum required area of reinforcement per linear foot of box width.

DATE: 2/12/2024 8:15:50 AM
FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 COVER SHEET.dgn

SIGNING & DELINEATION COVER SHEET

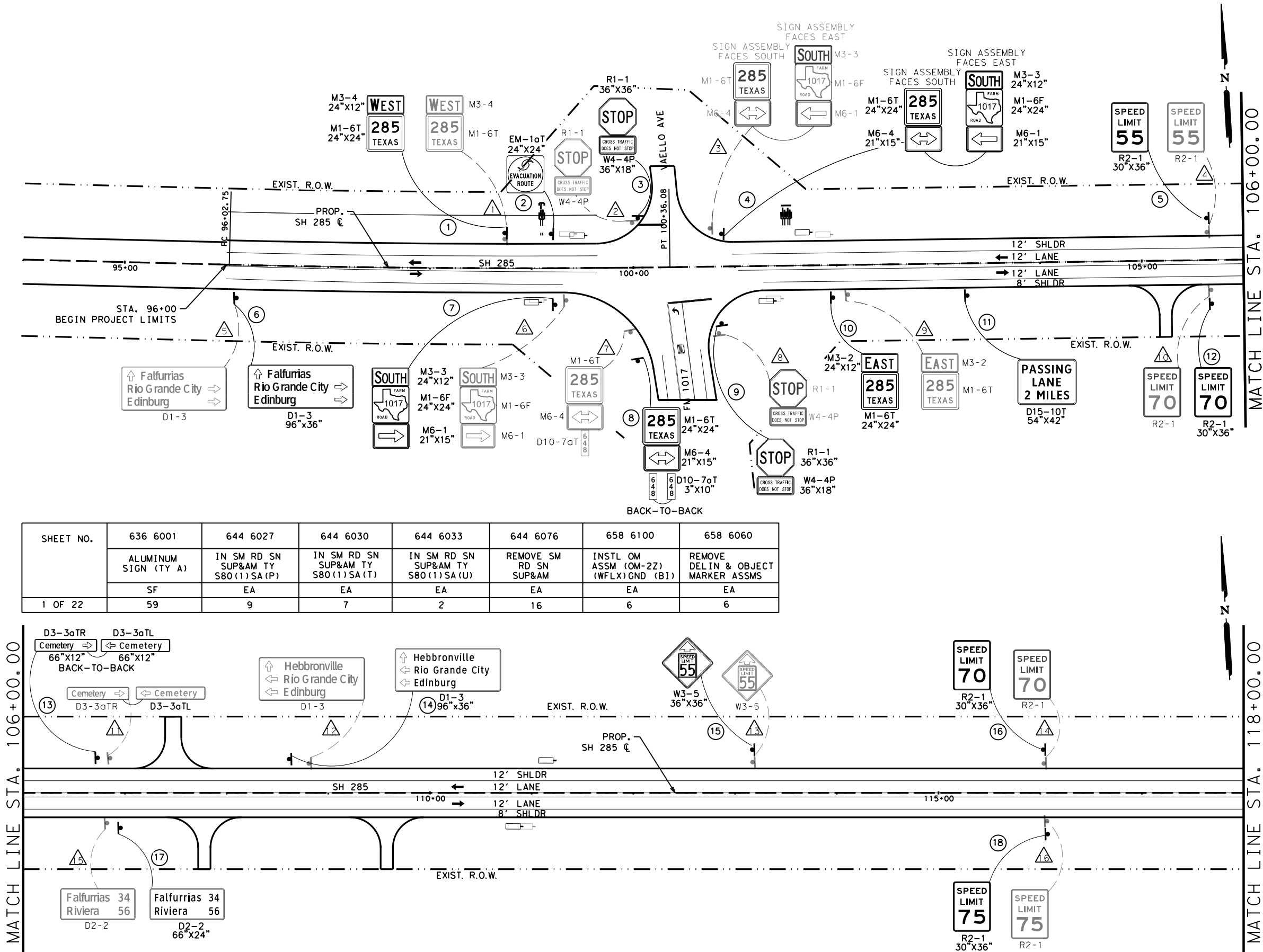
Pharr District Central Design



**SH 285
SIGNING &
DELINEATION
COVER SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		273

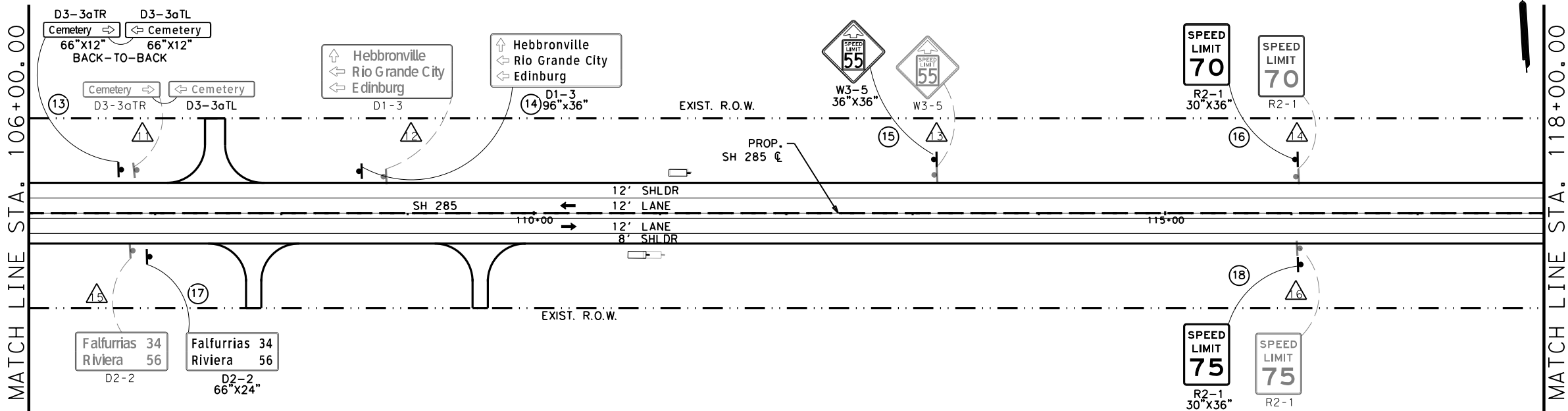
DATE: 2/29/2024 11:48:20 AM
 FILE: \\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN01.dgn



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - ▭ PROPOSED OBJECT MARKER
 - ▭ EXISTING OBJECT MARKER
 - ⊠ PROPOSED SIGN
 - ⊠ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DEL IN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
1 OF 22	59	9	7	2	16	6	6



02/29/2024

Pharr District Central Design

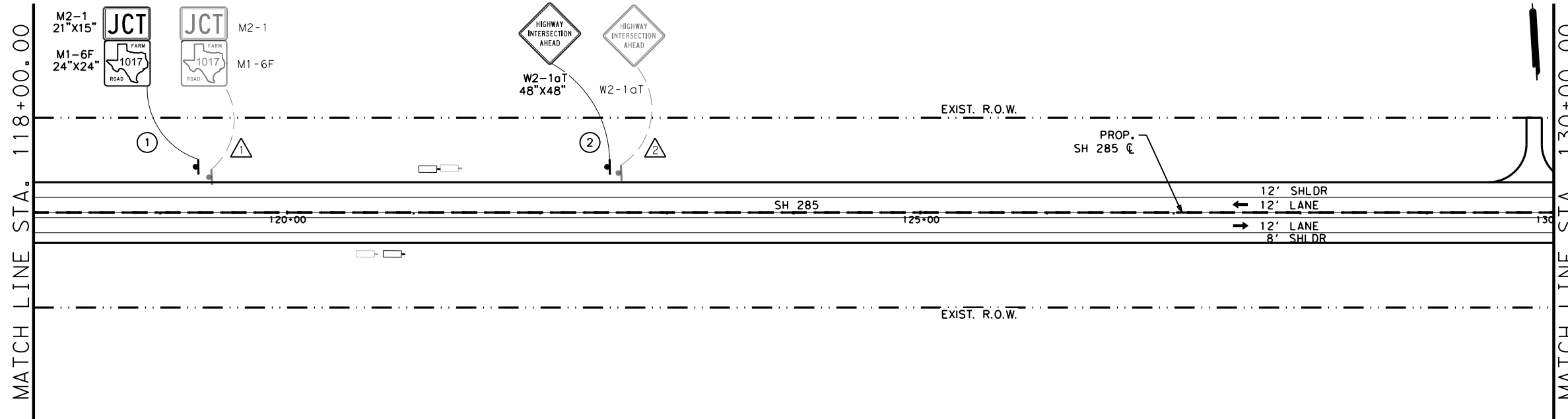
Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 1 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	274	

DATE: 2/29/2024 11:48:33 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN02.dgn

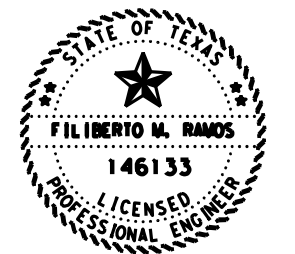
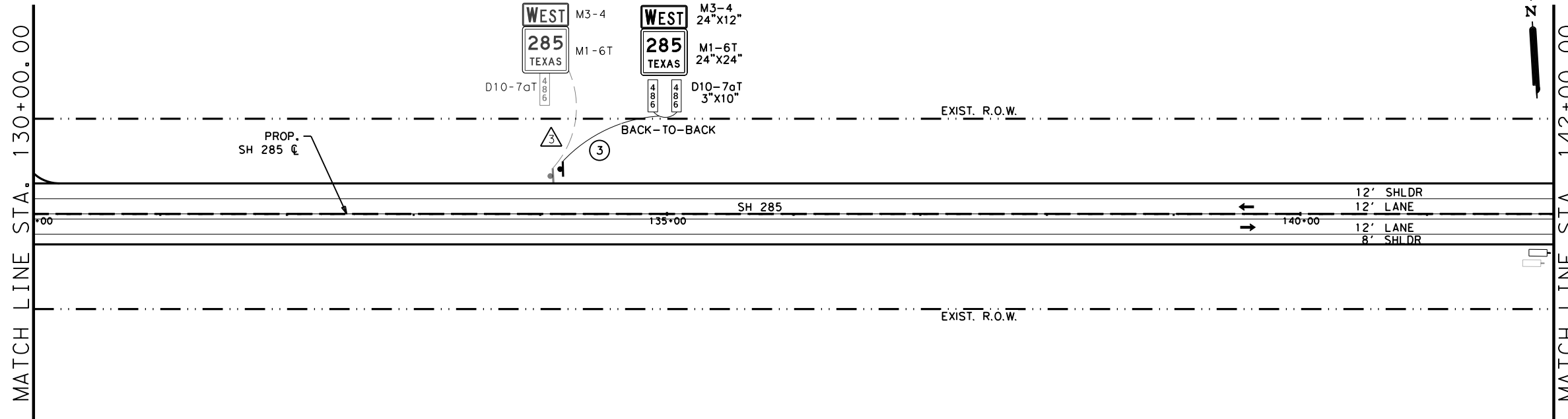


- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:

406 Falfurrias Hwy
Hebbronville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
2 OF 22	-	2	1	-	3	3	3



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 2 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	275

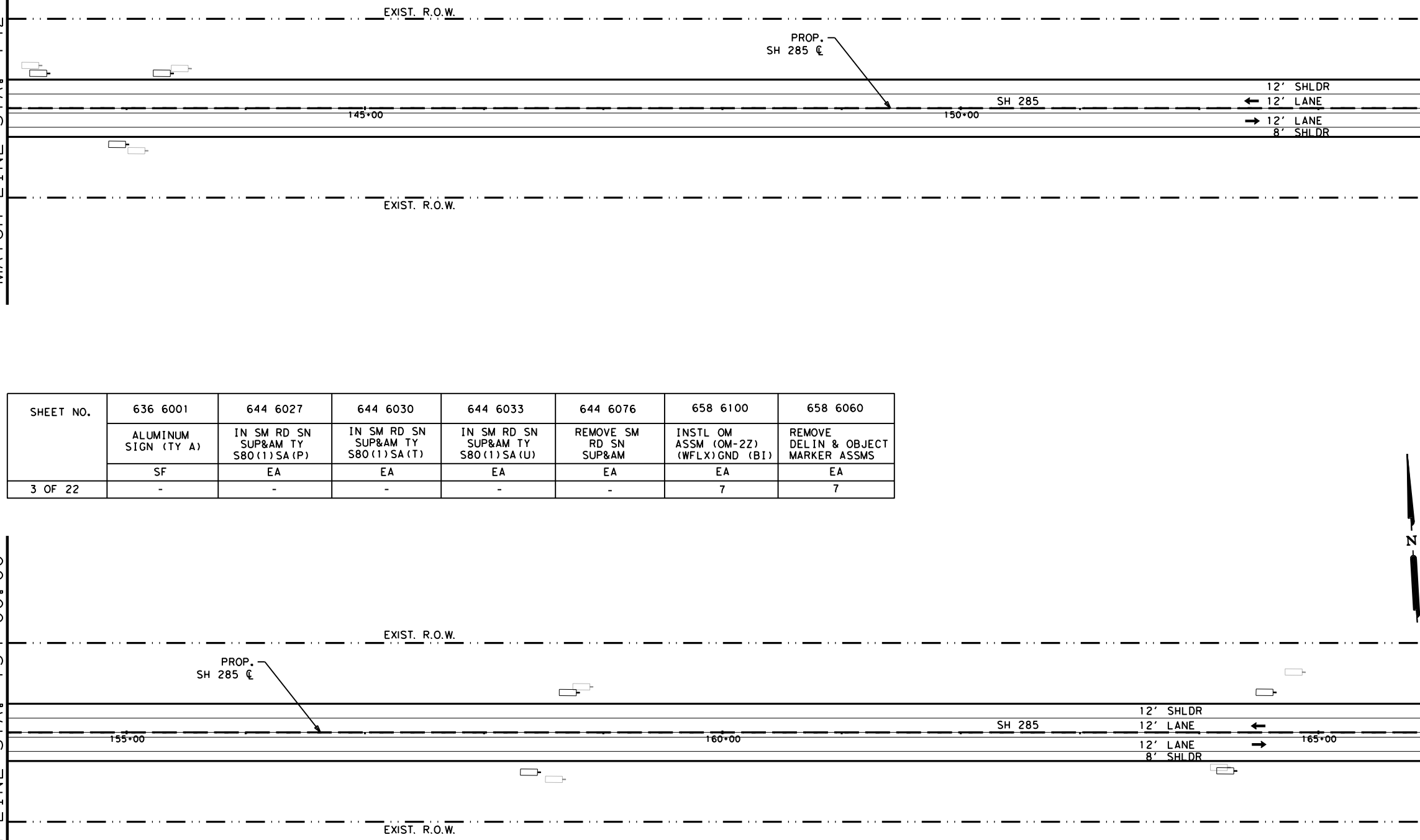
DATE: 2/29/2024 11:48:43 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN03.dgn

MATCH LINE STA. 142+00.00

MATCH LINE STA. 154+00.00

MATCH LINE STA. 154+00.00

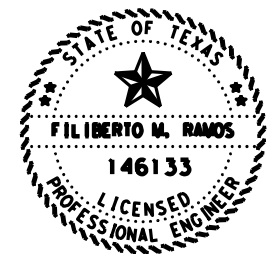
MATCH LINE STA. 166+00.00



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ⇨ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - ▲ PROPOSED SIGN
 - EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebronville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
3 OF 22	-	-	-	-	-	7	7



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 3 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		276

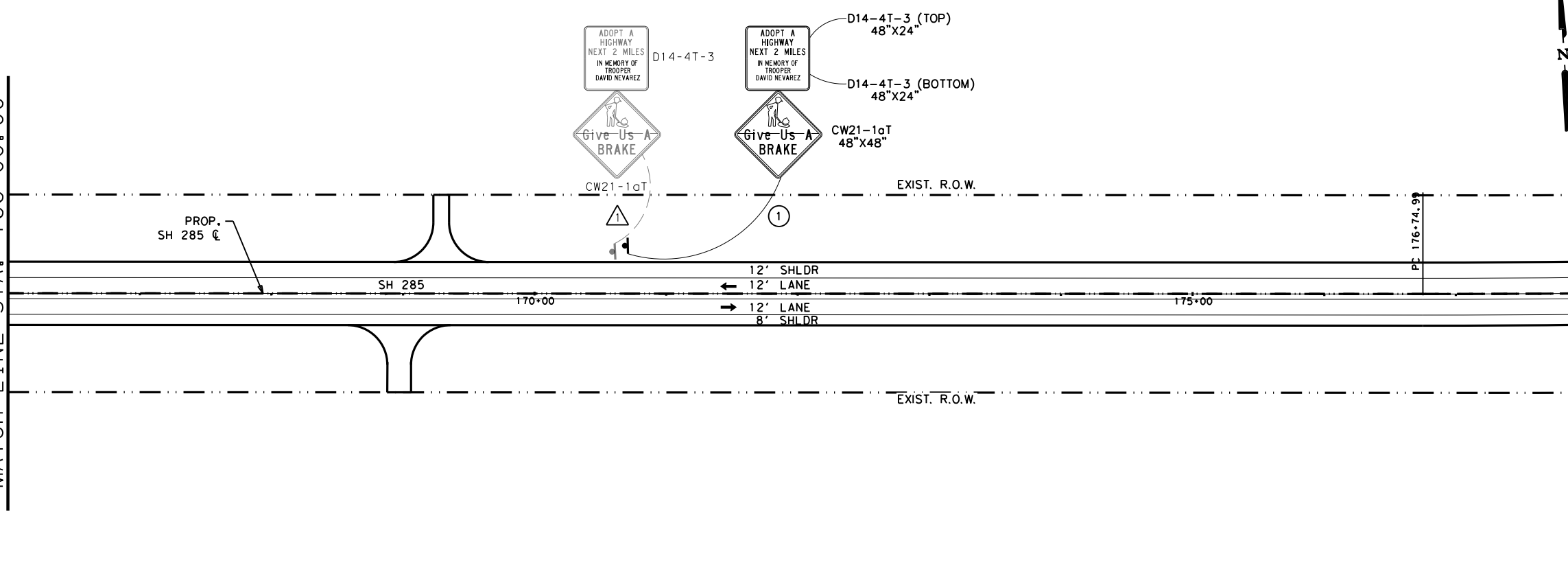
DATE: 2/29/2024 11:48:54 AM
 FILE: \\phr\dot\project\wiseon\line.com:TXDOT\5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN04.dgn

MATCH LINE STA. 166+00.00

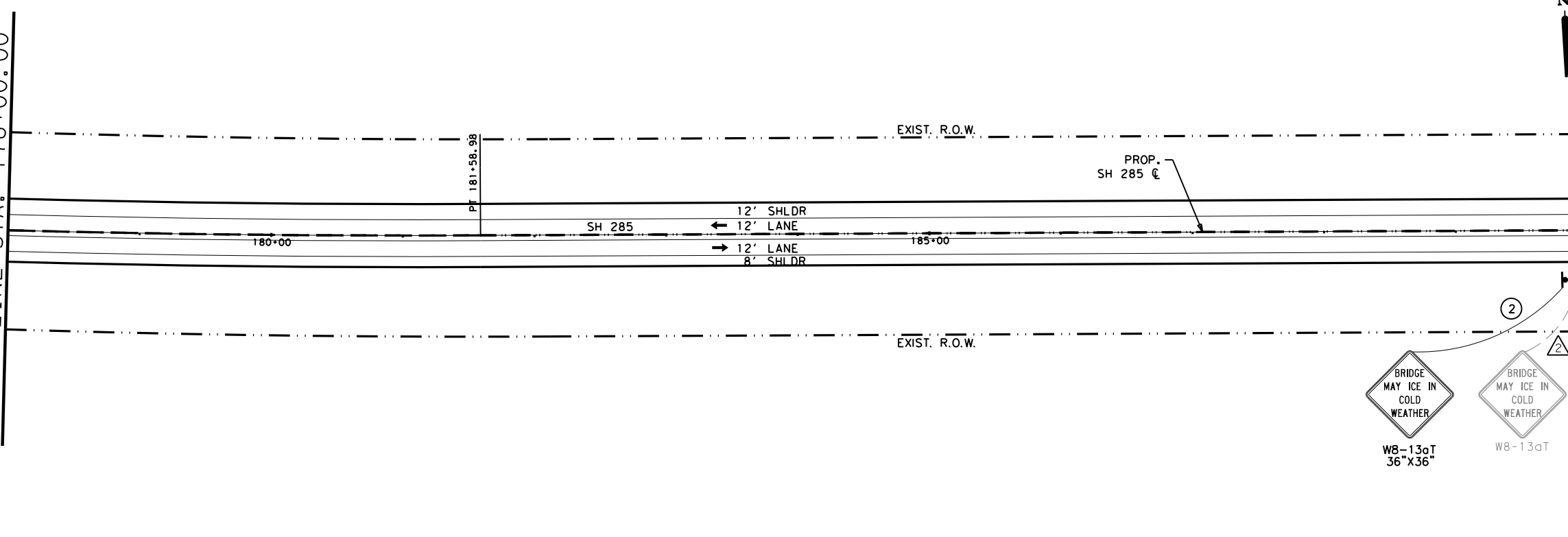
MATCH LINE STA. 178+00.00

MATCH LINE STA. 178+00.00

MATCH LINE STA. 190+00.00

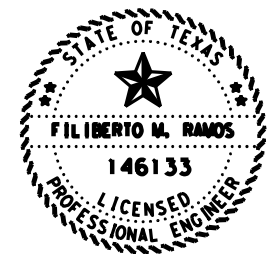


SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
4 OF 22	16	-	2	-	2	-	-



- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 4 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	277

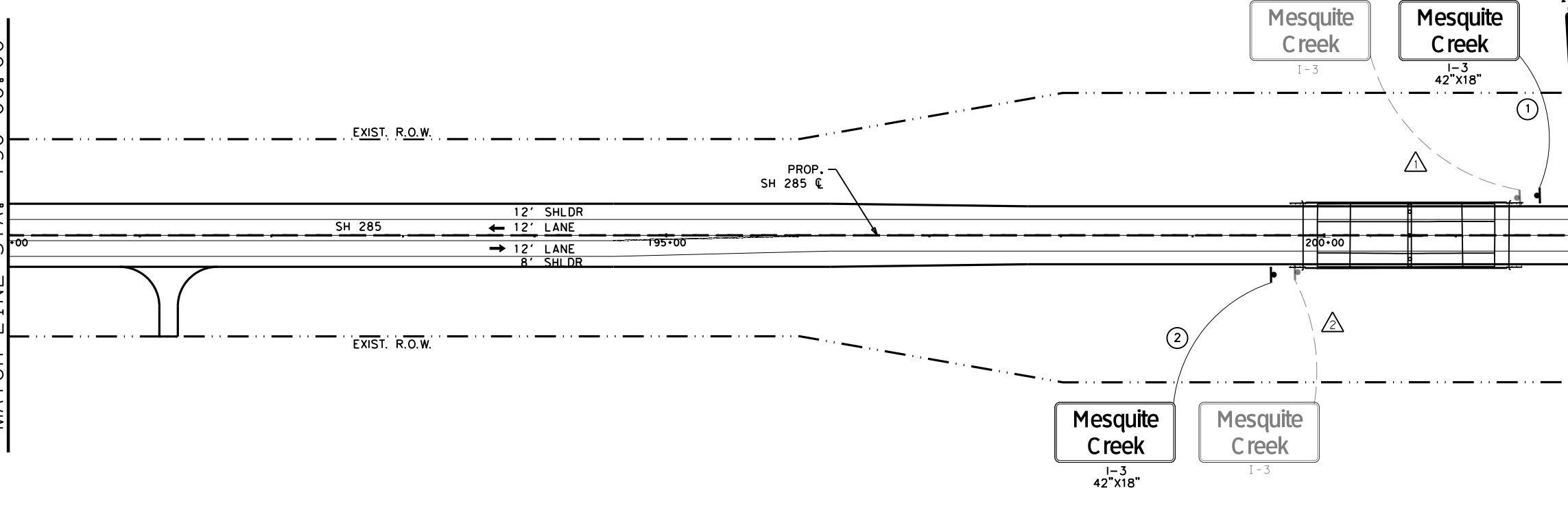
DATE: 2/29/2024 11:49:03 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN05.dgn

MATCH LINE STA. 190+00.00

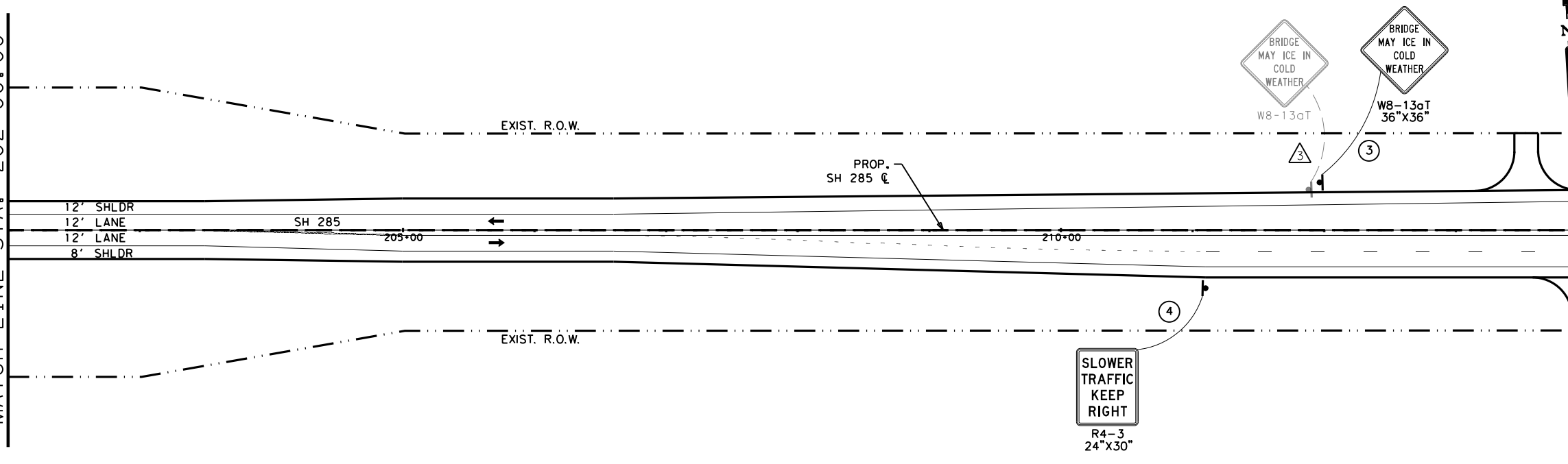
MATCH LINE STA. 202+00.00

MATCH LINE STA. 202+00.00

MATCH LINE STA. 214+00.00



SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
5 OF 22	10.5	3	1	-	3	-	-

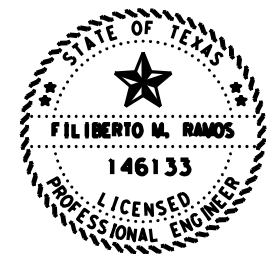


LEGEND:

- ⊛ EXISTING SIGN TO REMAIN IN PLACE
- SIGN TO BE INSTALLED (ITEM 644)
- △ SIGN TO BE REMOVED (ITEM 644)
- ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
- ➔ TRAFFIC FLOW INDICATOR
- PROPOSED OBJECT MARKER
- ◻ EXISTING OBJECT MARKER
- PROPOSED SIGN
- ▣ EXISTING SIGN

NOTES:

1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

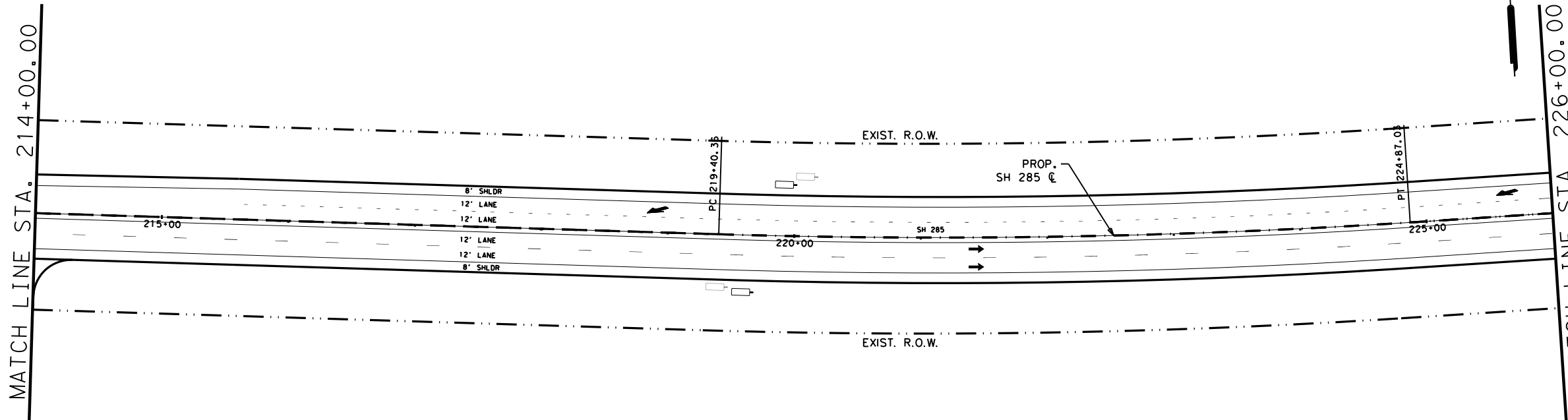
Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

SCALE: 1"=100' SHEET 5 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	278

DATE: 2/29/2024 11:49:12 AM
 FILE: D:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN06.dgn

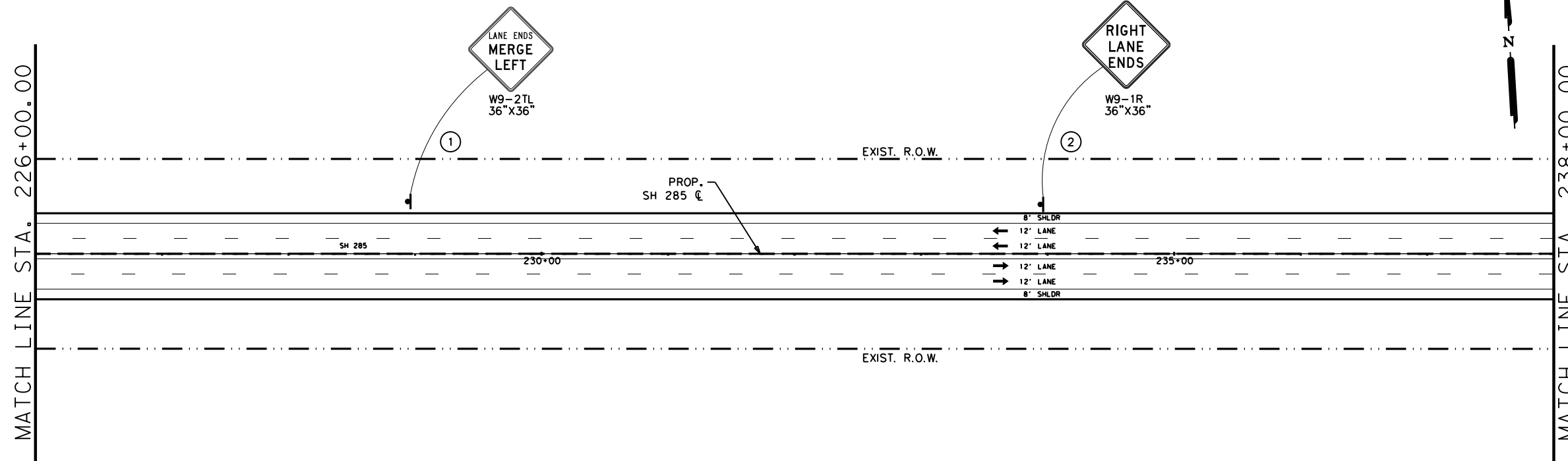


- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	IN STL OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
6 OF 22	-	-	2	-	-	2	2

4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 6 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	279	

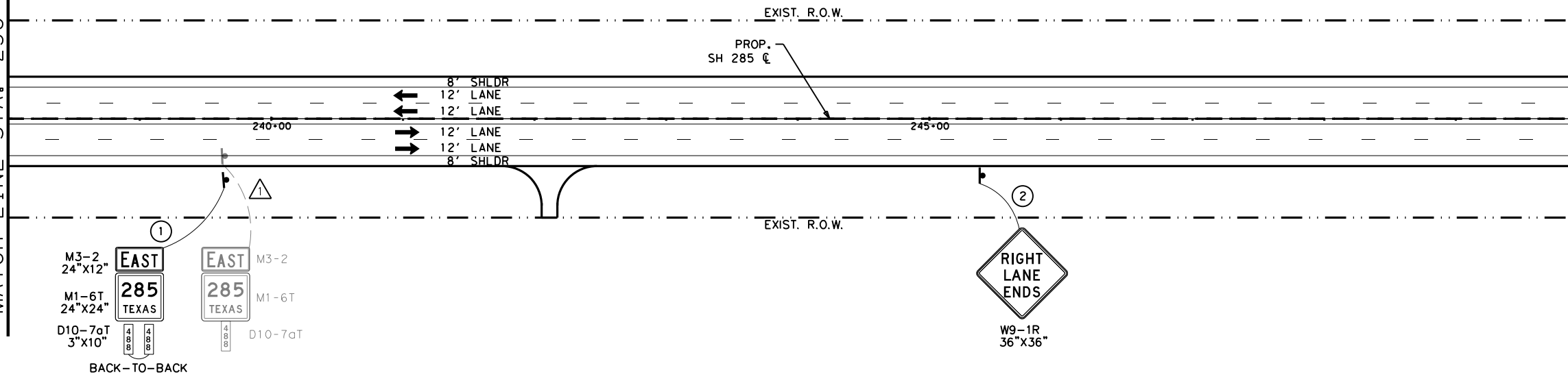
DATE: 2/29/2024 11:49:21 AM
 FILE: P:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN07.dgn

MATCH LINE STA. 238+00.00

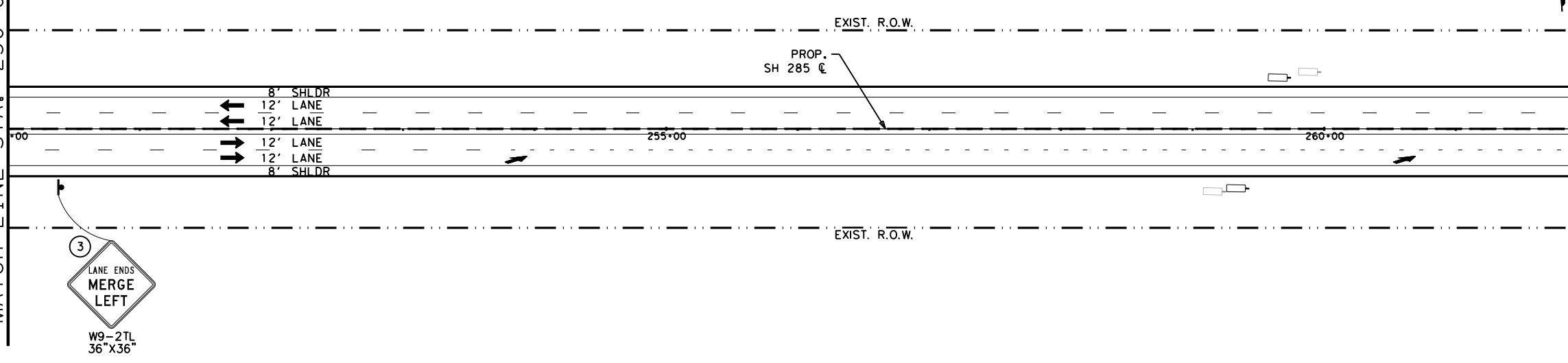
MATCH LINE STA. 250+00.00

MATCH LINE STA. 250+00.00

MATCH LINE STA. 262+00.00



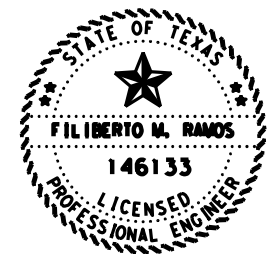
SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
7 OF 22	-	1	2	-	1	2	2



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - ▲ PROPOSED SIGN
 - ▴ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:

406 Falfurrias Hwy
Hebbronville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

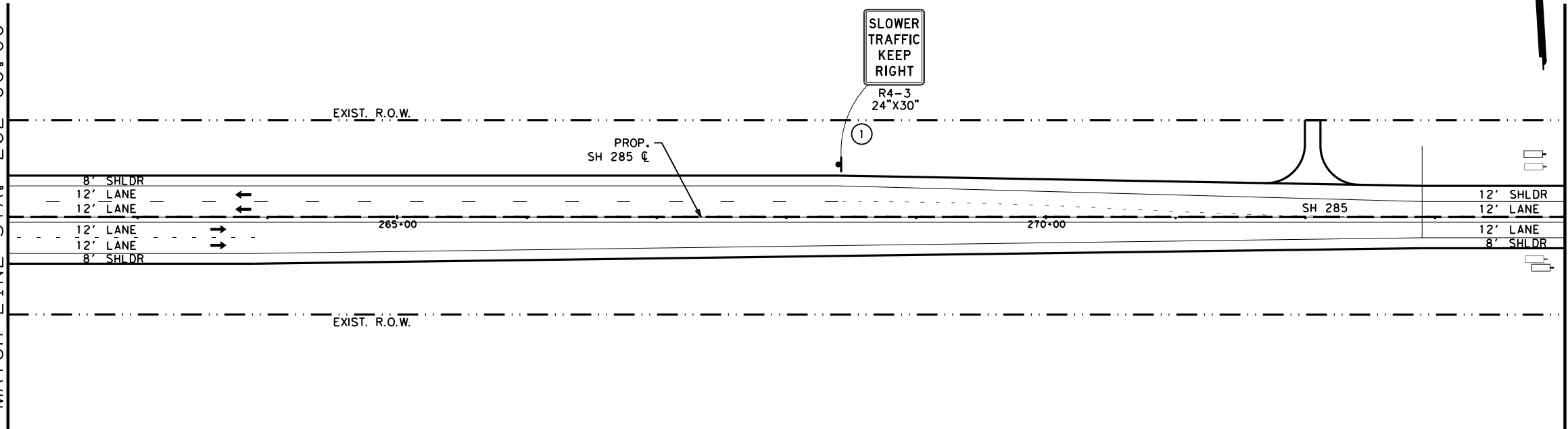
SCALE: 1"=100' SHEET 7 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	280	

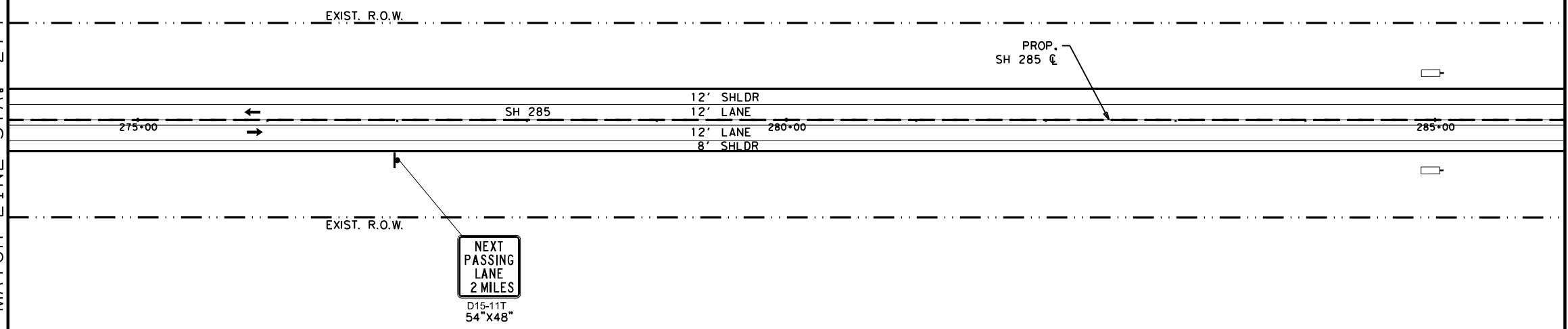
DATE: 2/29/2024 11:49:28 AM
 FILE: P:\txdot\project\wiseon\line.com\TXDOT\Documents\21 - PHR\Design Projects\0482\036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN08.dgn

MATCH LINE STA. 262+00.00

MATCH LINE STA. 274+00.00

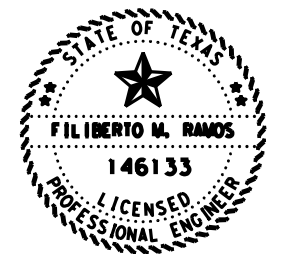


SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1) SA (P)	IN SM RD SN SUP&AM TY S80(1) SA (T)	IN SM RD SN SUP&AM TY S80(1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (B1)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
8 OF 22	-	1	-	1	-	4	2



- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

SCALE: 1"=100' SHEET 8 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		281

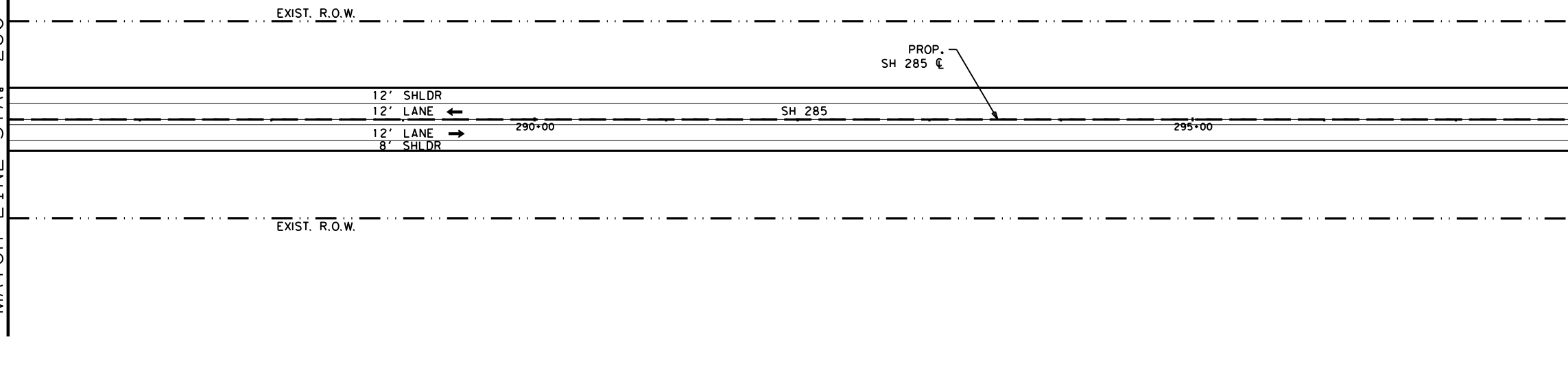
DATE: 2/29/2024 11:49:37 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN09.dgn

MATCH LINE STA. 286+00.00

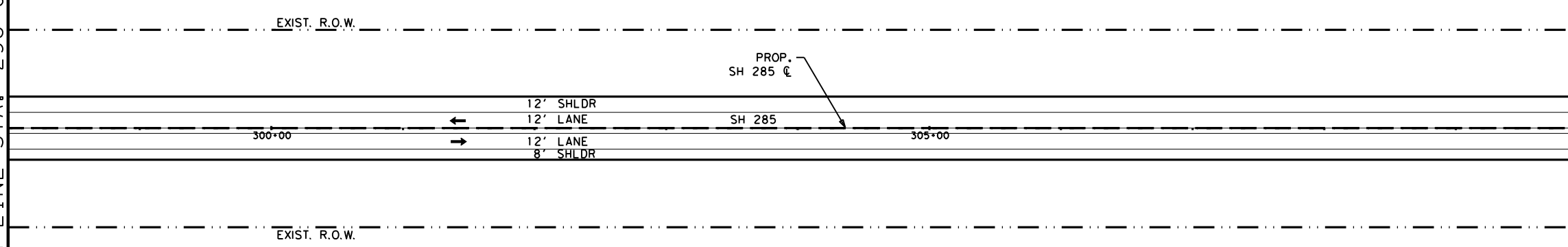
MATCH LINE STA. 298+00.00

MATCH LINE STA. 298+00.00

MATCH LINE STA. 310+00.00

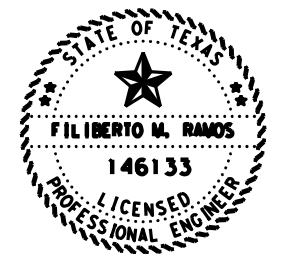


SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
9 OF 22	-	-	-	-	-	-	-



- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ⇨ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

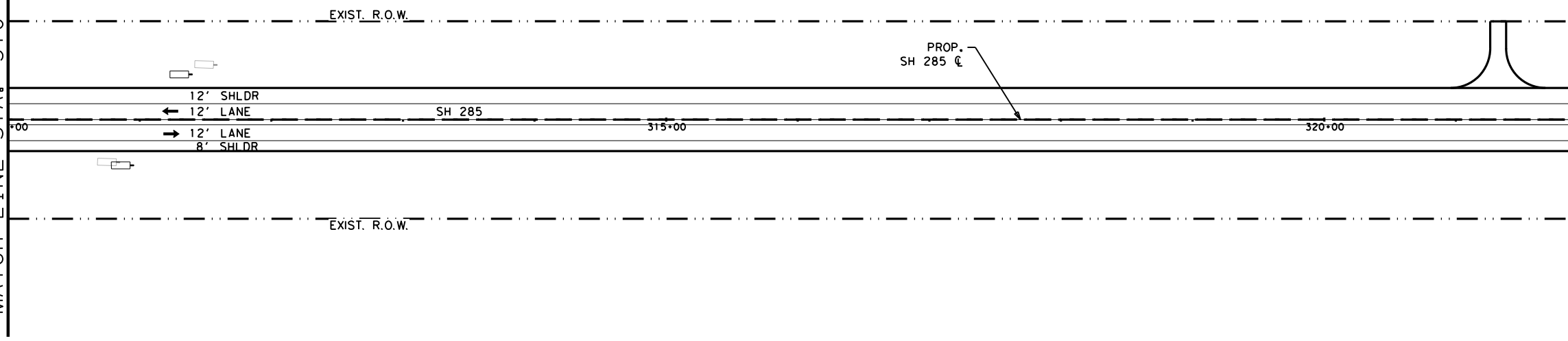
SCALE: 1"=100' SHEET 9 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		282

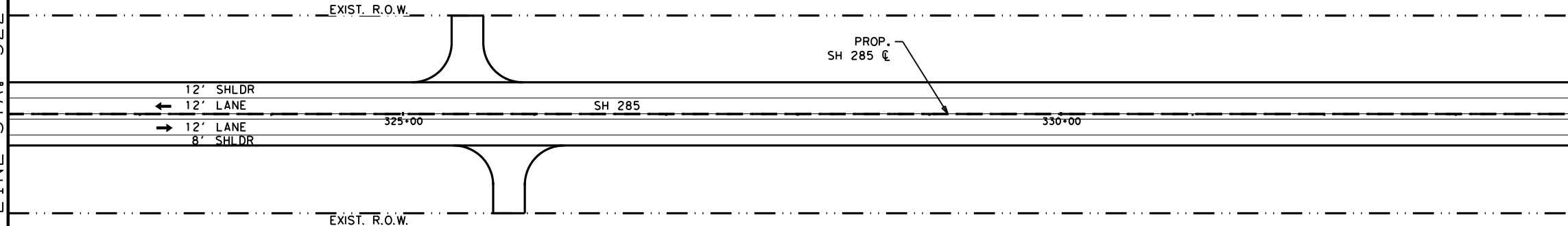
DATE: 2/29/2024 11:49:44 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN010.dgn

MATCH LINE STA. 310+00.00

MATCH LINE STA. 322+00.00



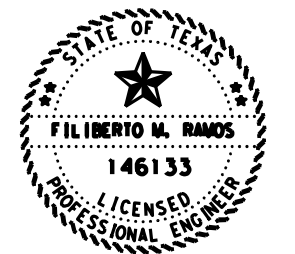
SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
10 OF 22	-	-	-	-	-	2	2



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:

406 Falfurrias Hwy
Hebbronville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

SCALE: 1"=100' SHEET 10 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		283

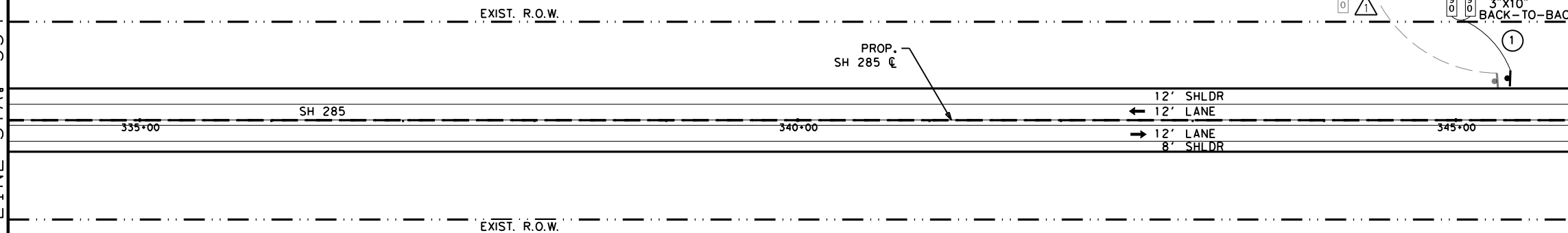
DATE: 2/29/2024 11:49:54 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN011.dgn

MATCH LINE STA. 334+00.00

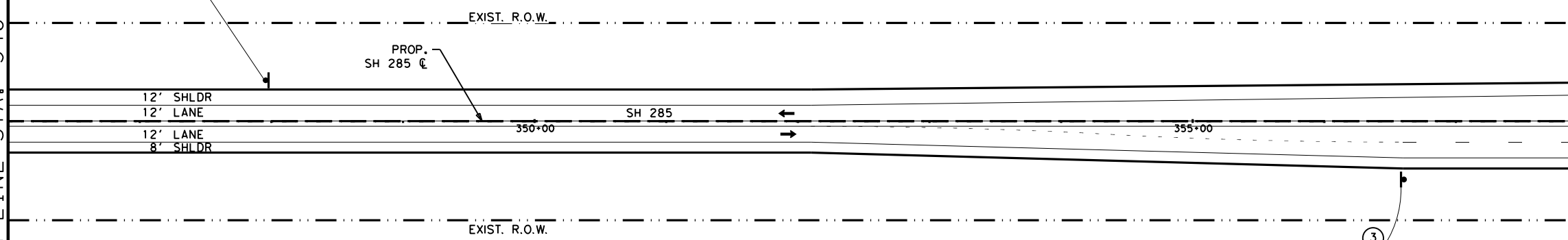
MATCH LINE STA. 346+00.00

MATCH LINE STA. 346+00.00

MATCH LINE STA. 358+00.00



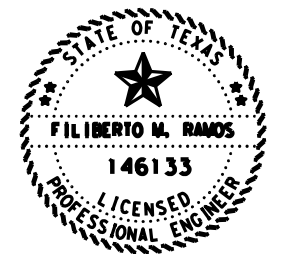
SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1) SA (P)	IN SM RD SN SUP&AM TY S80(1) SA (T)	IN SM RD SN SUP&AM TY S80(1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
11 OF 22	-	2	-	1	1	-	-



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▬ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:

406 Falfurrias Hwy
Hebbronville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

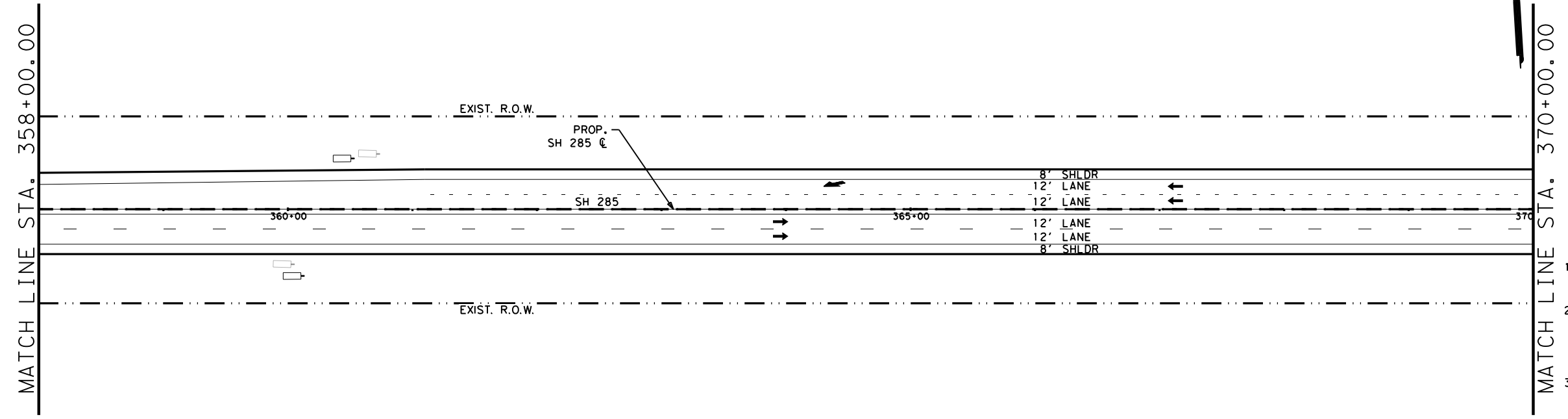
Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

SCALE: 1"=100' SHEET 11 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	284

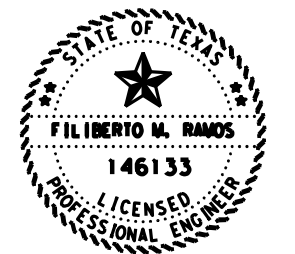
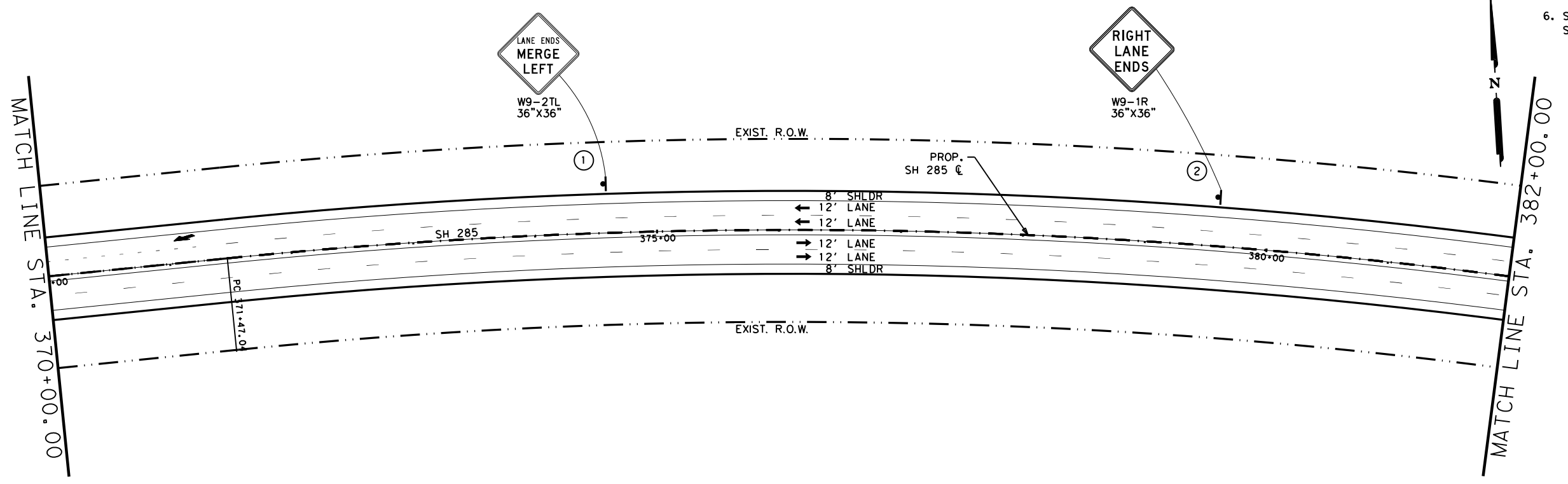
DATE: 2/29/2024 11:50:03 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN012.dgn



- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ⇨ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - ▲ PROPOSED SIGN
 - EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
12 OF 22	-	-	2	-	-	2	2



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

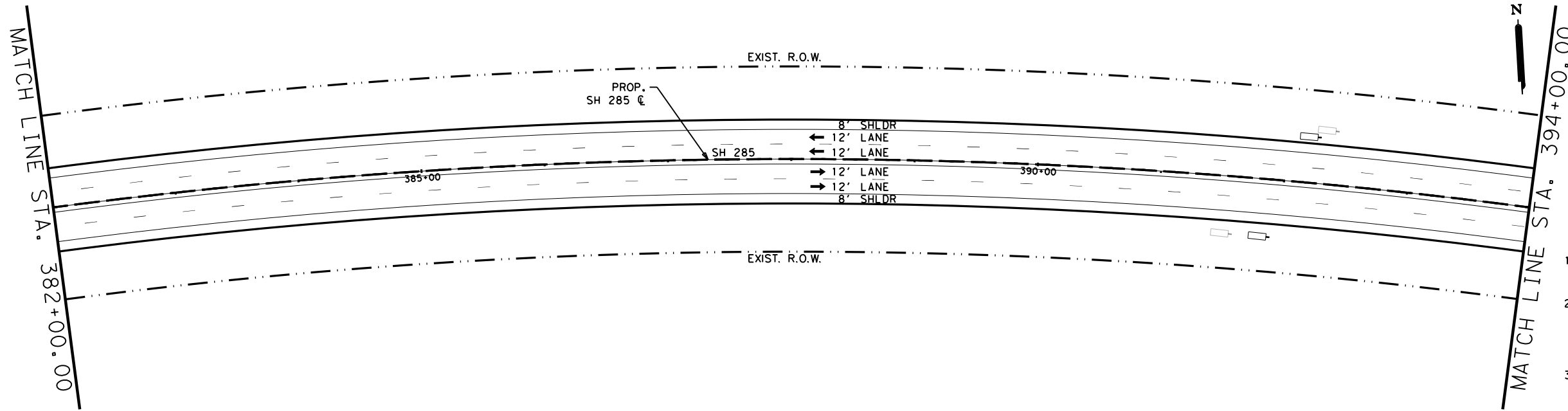
Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 12 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	285	

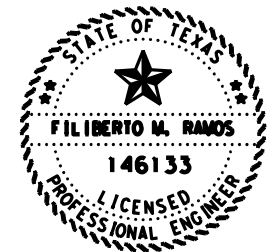
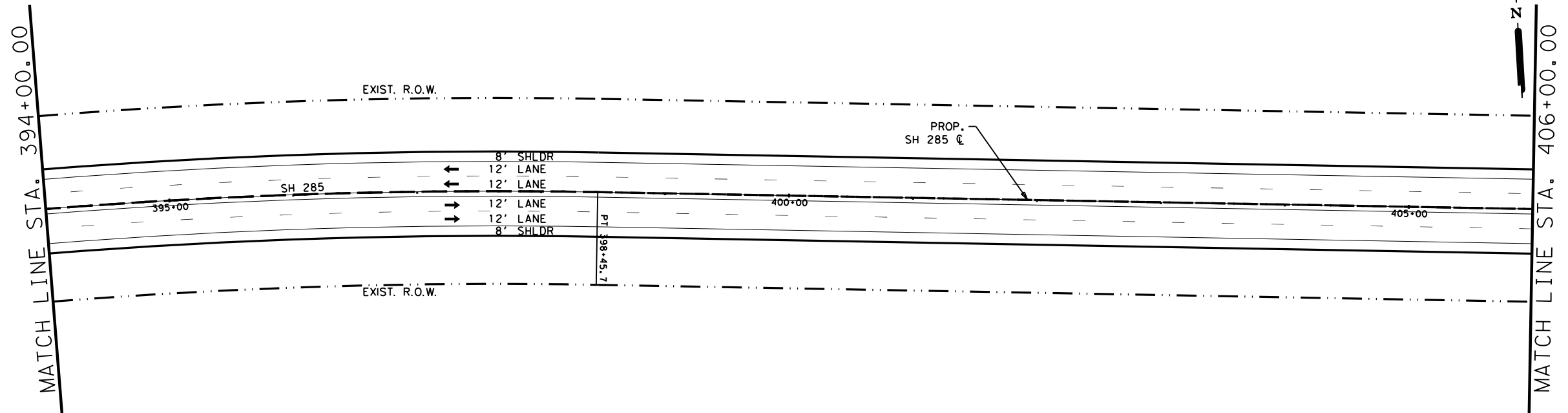
DATE: 2/29/2024 11:50:14 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN013.dgn



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - ▭ PROPOSED OBJECT MARKER
 - ▭ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
13 OF 22	-	-	-	-	-	2	2



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

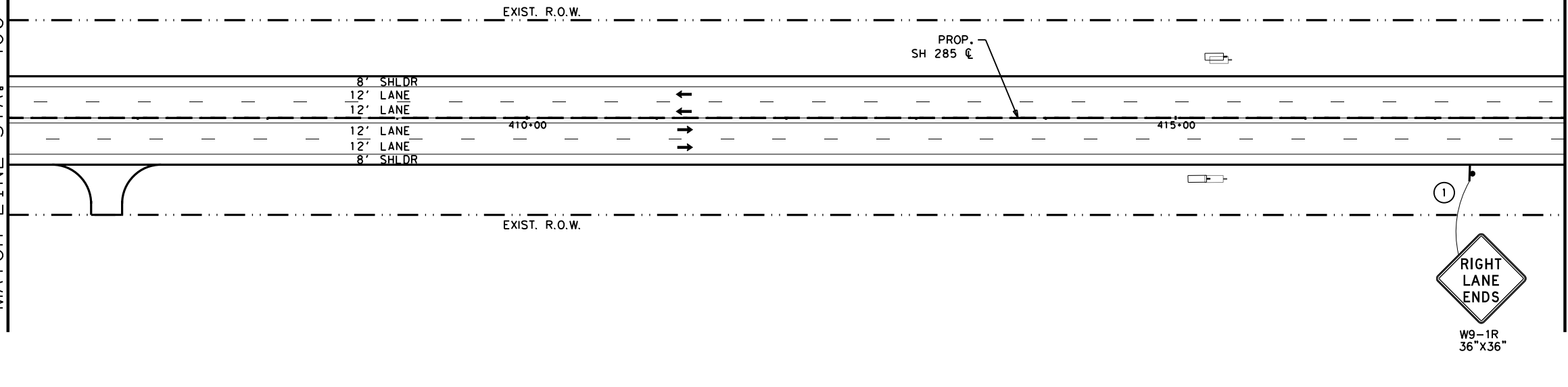
SCALE: 1"=100' SHEET 13 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	286	

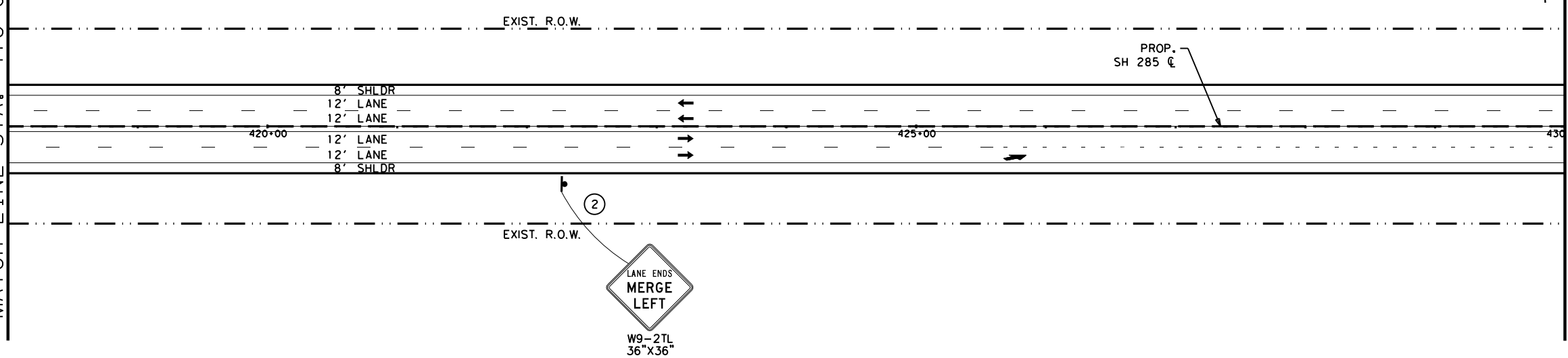
DATE: 2/29/2024 11:50:24 AM
 FILE: D:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN014.dgn

MATCH LINE STA. 406+00.00

MATCH LINE STA. 418+00.00

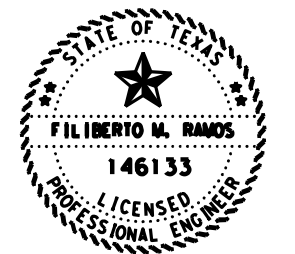


SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTR OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DEL IN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
14 OF 22	-	-	2	-	-	2	2



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - ▭ PROPOSED OBJECT MARKER
 - ▭ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 SIGNING &
 DELINEATION
 LAYOUTS**

SCALE: 1"=100' SHEET 14 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		287

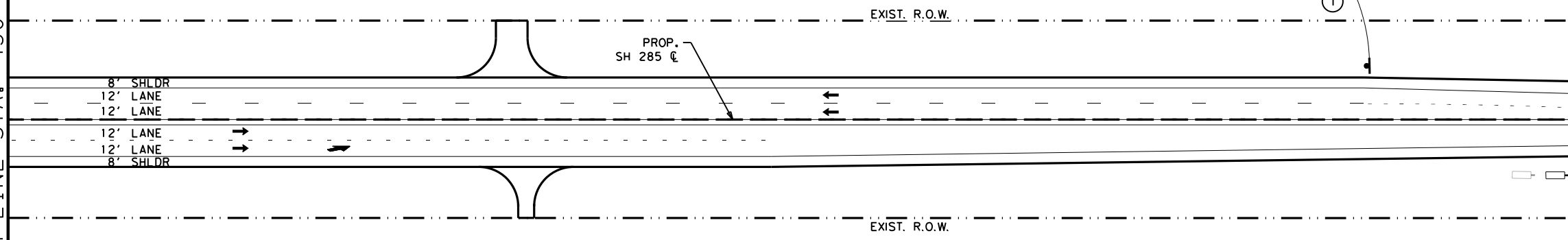
DATE: 2/29/2024 11:50:35 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN015.dgn

MATCH LINE STA. 430+00.00

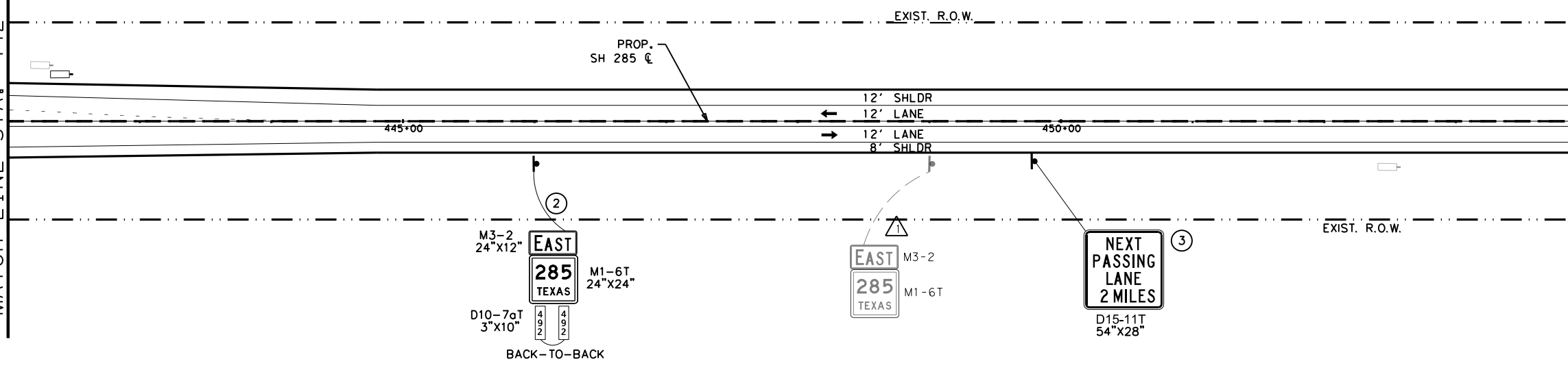
MATCH LINE STA. 442+00.00

MATCH LINE STA. 442+00.00

MATCH LINE STA. 454+00.00

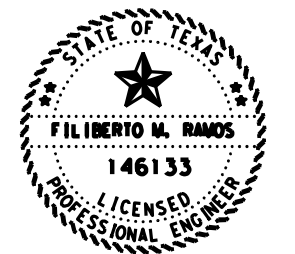


SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTR. OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
15 OF 22	-	2	-	1	1	2	3



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 15 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	288	

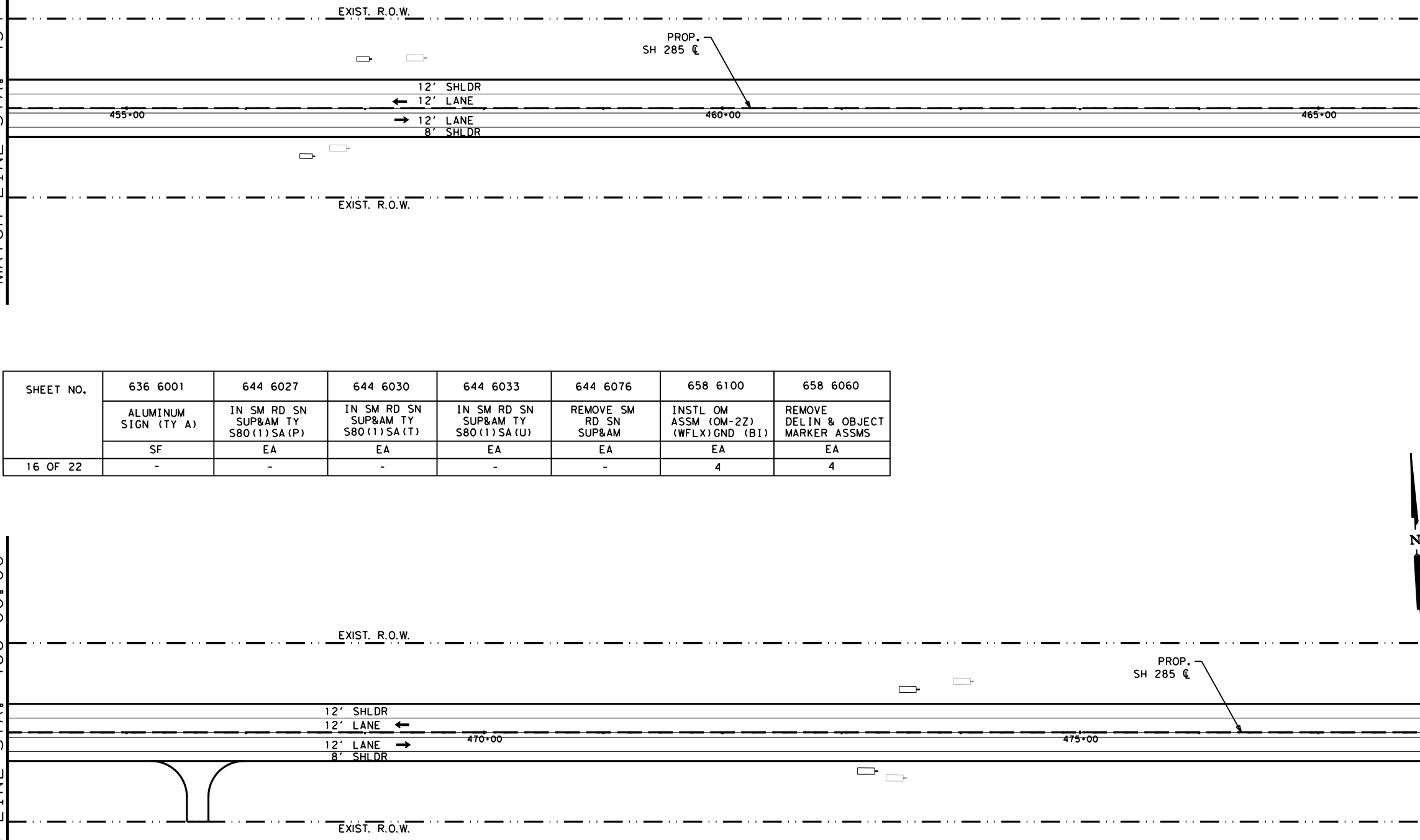
DATE: 2/29/2024 11:50:44 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN016.dgn

MATCH LINE STA. 454+00.00

MATCH LINE STA. 466+00.00

MATCH LINE STA. 466+00.00

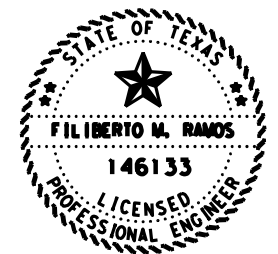
MATCH LINE STA. 478+00.00



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ⇨ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX)GND (B1)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
16 OF 22	-	-	-	-	-	4	4



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 16 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	289	

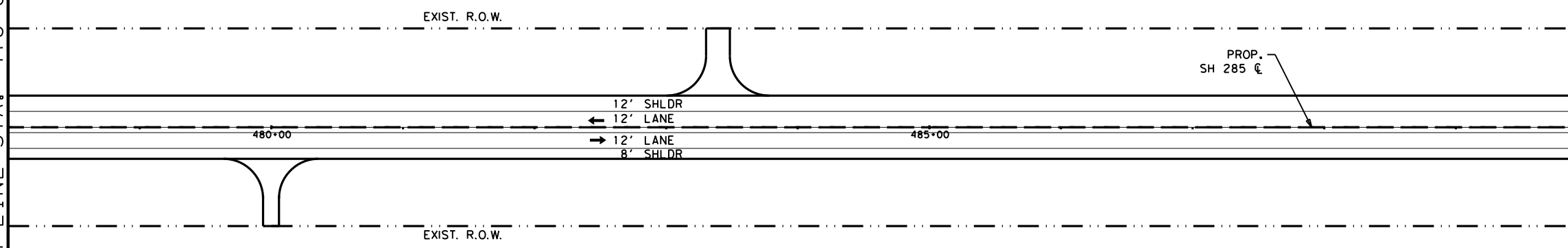
DATE: 2/29/2024 11:50:53 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SIGN017.dgn

MATCH LINE STA. 478+00.00

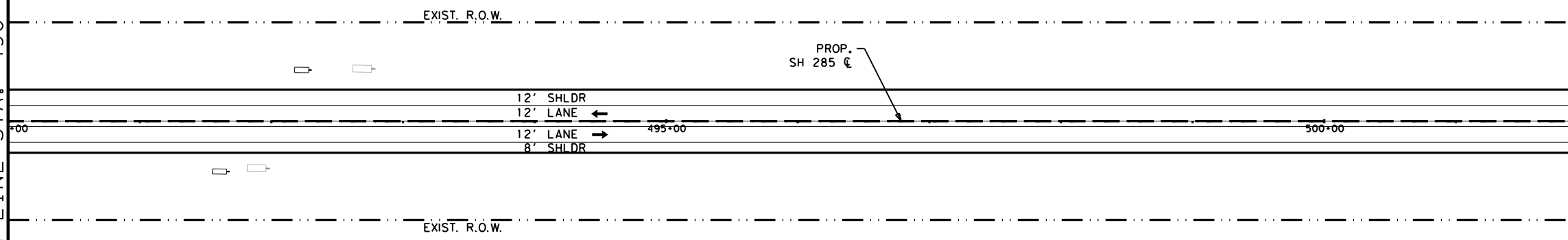
MATCH LINE STA. 490+00.00

MATCH LINE STA. 490+00.00

MATCH LINE STA. 502+00.00



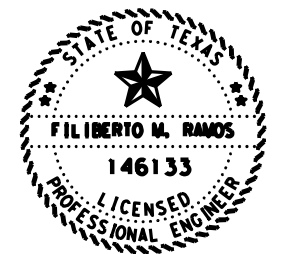
SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	INSTR OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
17 OF 22	-	-	-	-	-	2	2



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:

406 Falfurrias Hwy
Hebbronville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 17 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		290

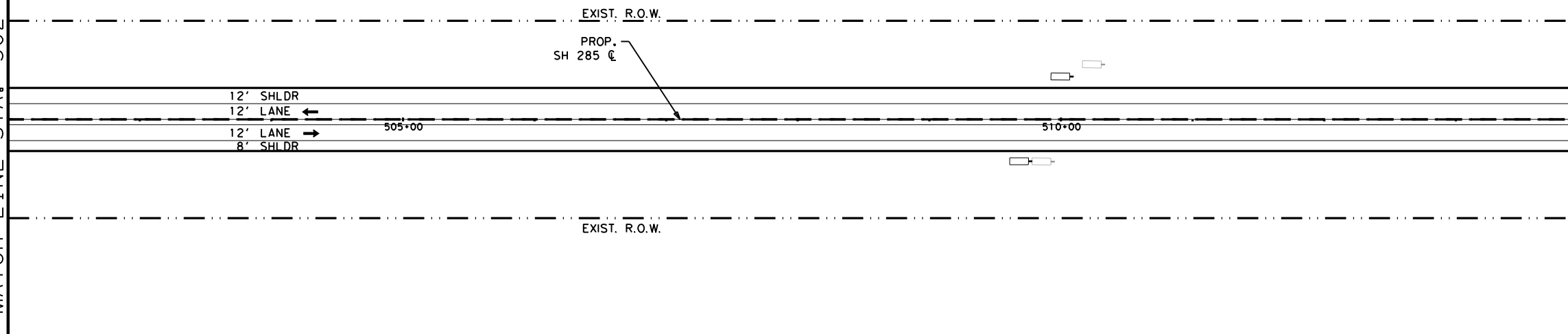
DATE: 2/29/2024 11:51:04 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN018.dgn

MATCH LINE STA. 502+00.00

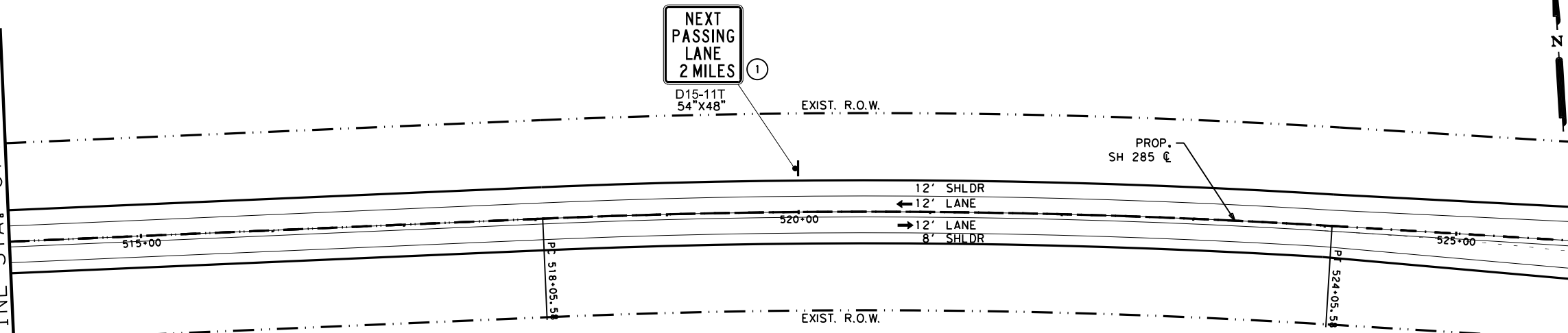
MATCH LINE STA. 514+00.00

MATCH LINE STA. 514+00.00

MATCH LINE STA. 526+00.00

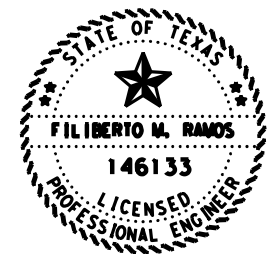


SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
18 OF 22	-	-	-	1	-	2	2



- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ⇨ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



[Signature]
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

SCALE: 1"=100' SHEET 18 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		291

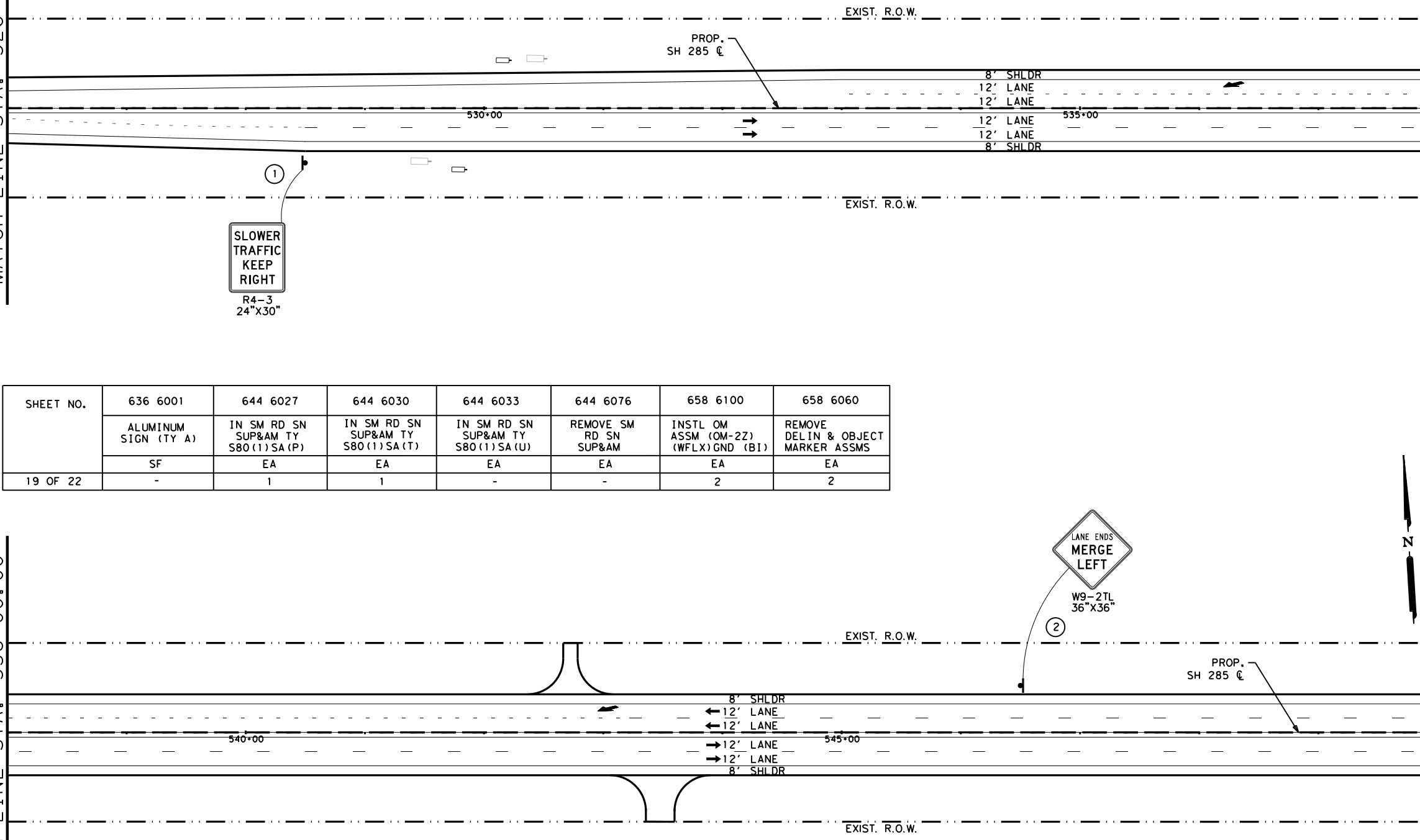
DATE: 2/29/2024 11:51:13 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN019.dgn

MATCH LINE STA. 526+00.00

MATCH LINE STA. 538+00.00

MATCH LINE STA. 538+00.00

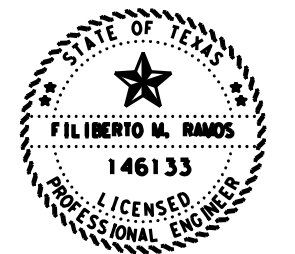
MATCH LINE STA. 550+00.00



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▒ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	INSTR OM ASSM (OM-2Z) (WFLX)GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
19 OF 22	-	1	1	-	-	2	2



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

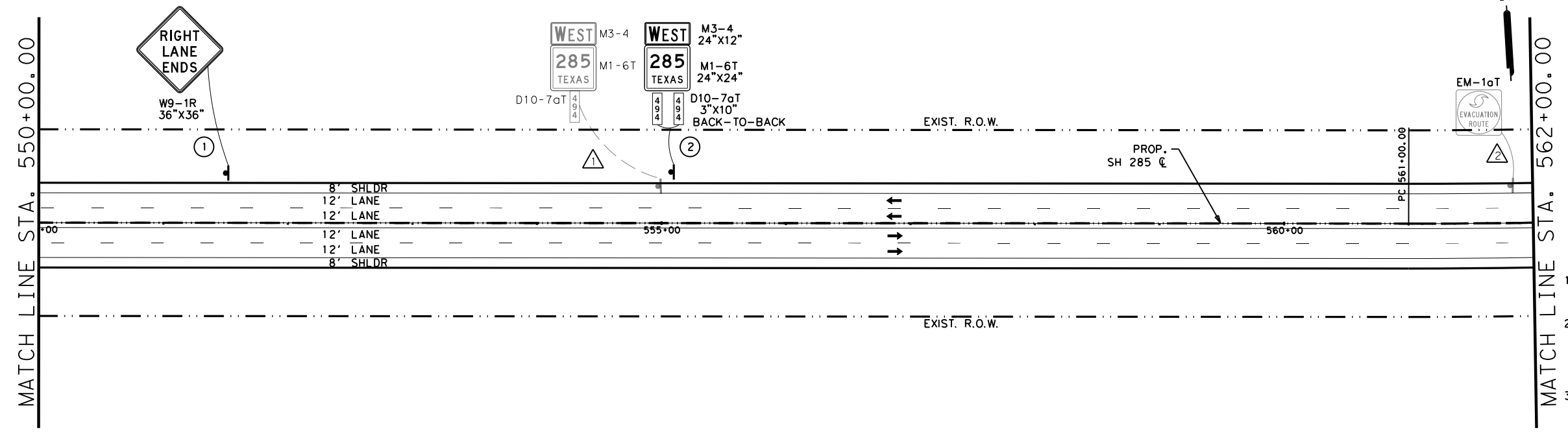
Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

SCALE: 1"=100' SHEET 19 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		292

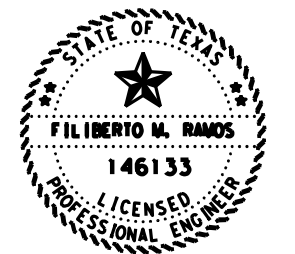
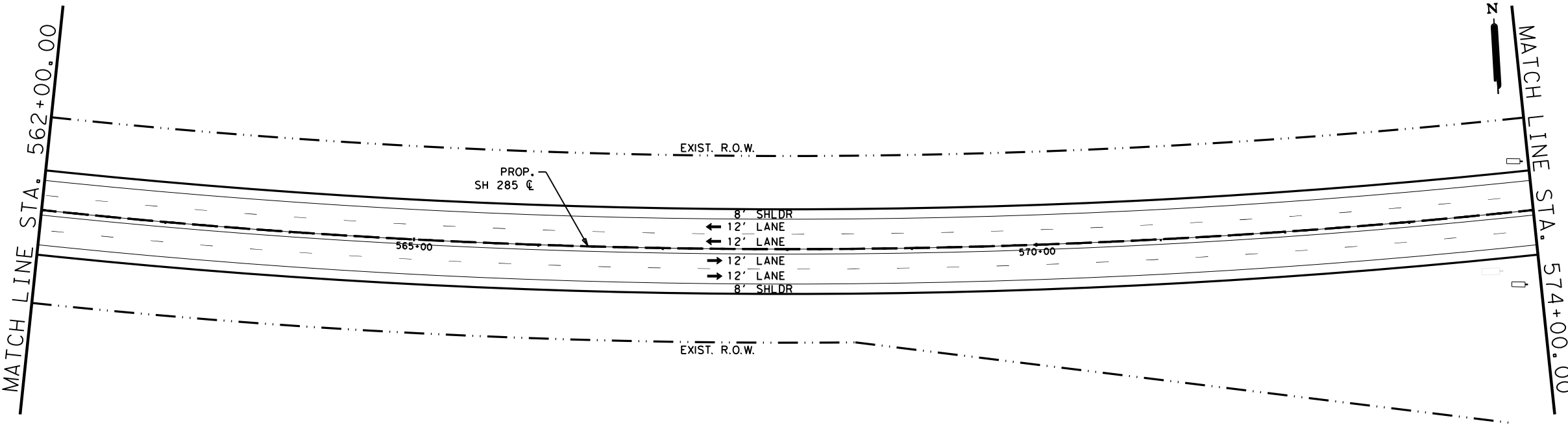
DATE: 2/29/2024 11:51:24 AM
 FILE: pw:\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN020.dgn



- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - ▭ PROPOSED OBJECT MARKER
 - ▭ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80(1)SA(P)	IN SM RD SN SUP&AM TY S80(1)SA(T)	IN SM RD SN SUP&AM TY S80(1)SA(U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-22) (WFLX)GND (B1)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
20 OF 22	-	1	1	-	2	2	-



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

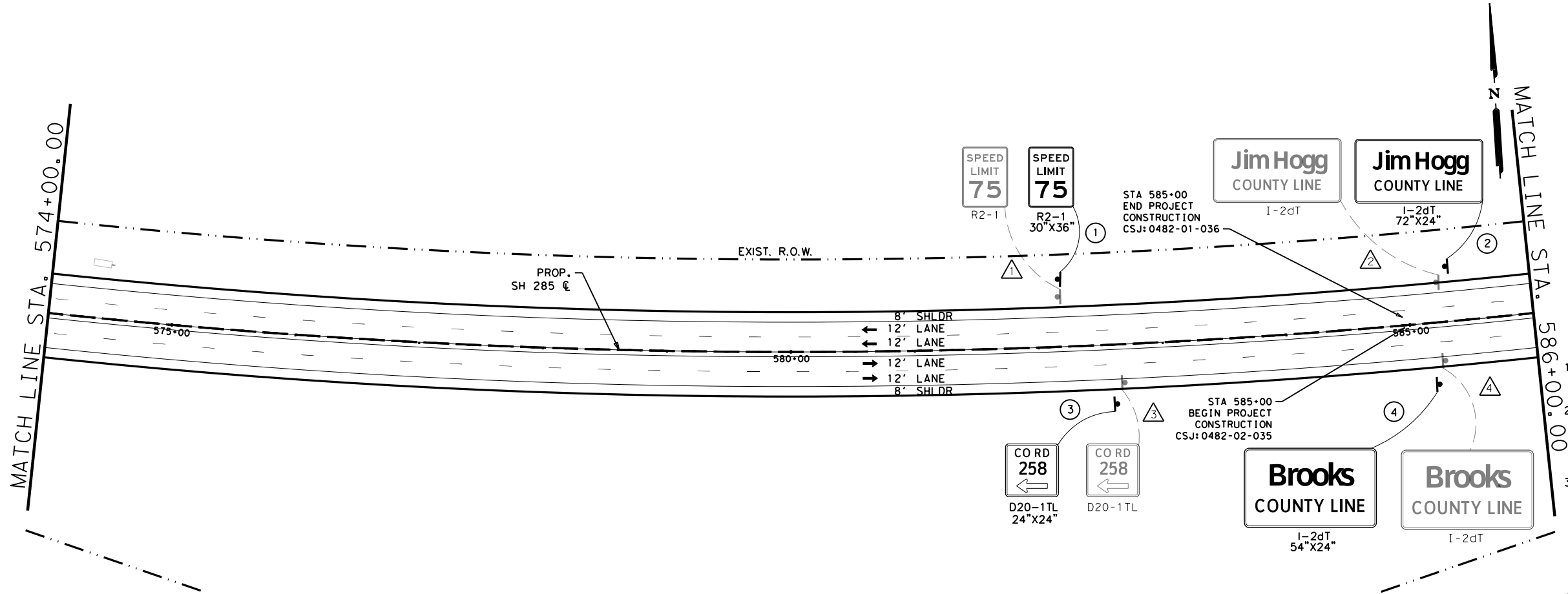
Texas Department of Transportation

**SH 285
SIGNING &
DELINEATION
LAYOUTS**

SCALE: 1"=100' SHEET 20 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	293

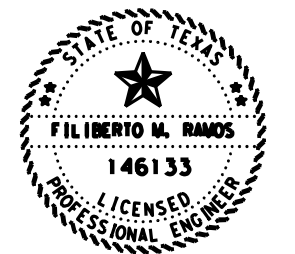
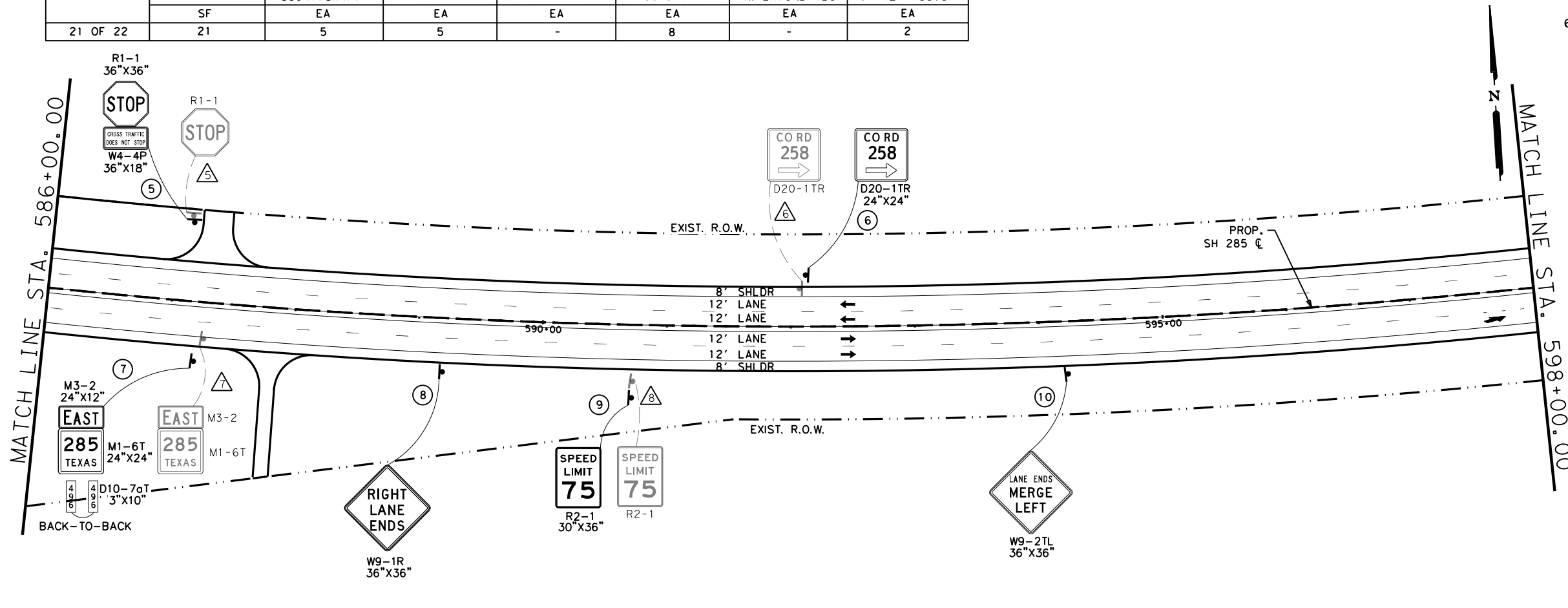
DATE: 2/29/2024 11:51:36 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285_SIGN021.dgn



- LEGEND:**
- ⊛ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - PROPOSED SIGN
 - ▬ EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.

SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
21 OF 22	21	5	5	-	8	-	2



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 SIGNING & DELINEATION LAYOUTS

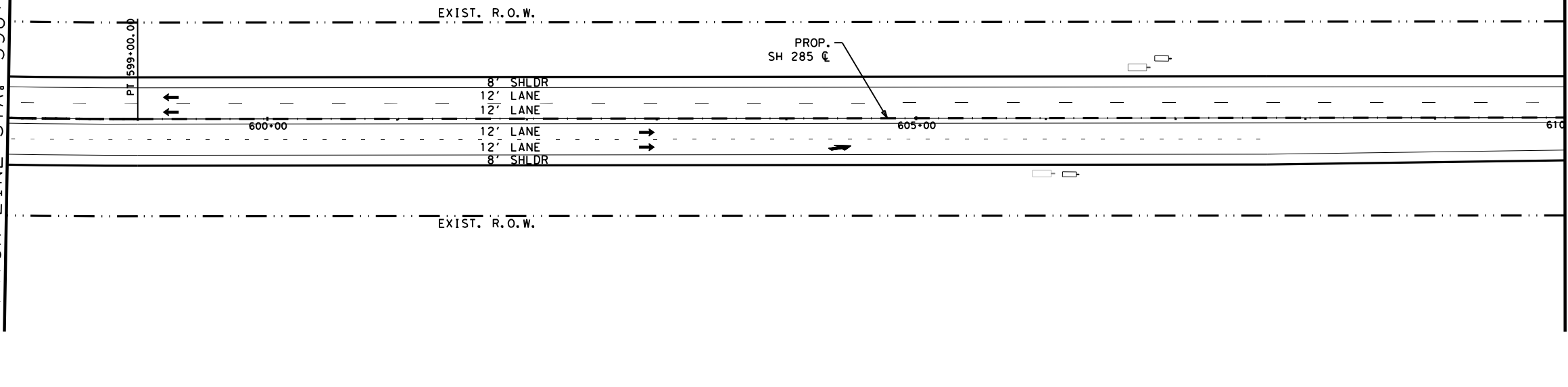
SCALE: 1"=100' SHEET 21 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	294	

DATE: 3/14/2024 2:52:17 PM
 FILE: pw:\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\0482\036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285_SIGN022.dgn

MATCH LINE STA. 598+00.00

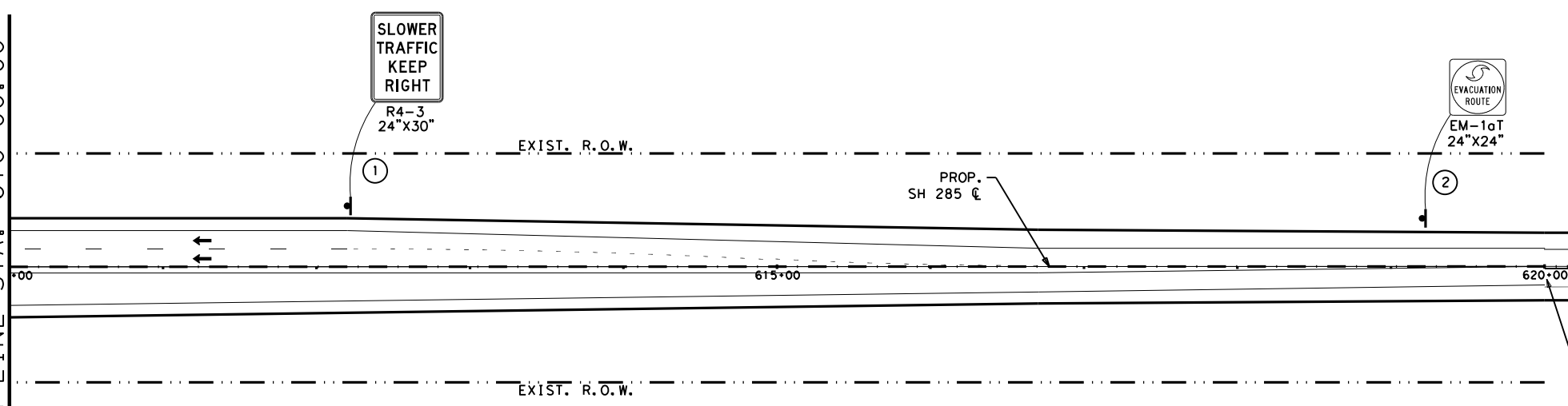
MATCH LINE STA. 610+00.00



SHEET NO.	636 6001	644 6027	644 6030	644 6033	644 6076	658 6100	658 6060
	ALUMINUM SIGN (TY A)	IN SM RD SN SUP&AM TY S80 (1) SA (P)	IN SM RD SN SUP&AM TY S80 (1) SA (T)	IN SM RD SN SUP&AM TY S80 (1) SA (U)	REMOVE SM RD SN SUP&AM	INSTL OM ASSM (OM-2Z) (WFLX) GND (BI)	REMOVE DELIN & OBJECT MARKER ASSMS
	SF	EA	EA	EA	EA	EA	EA
22 OF 22	-	2	-	1	-	2	2

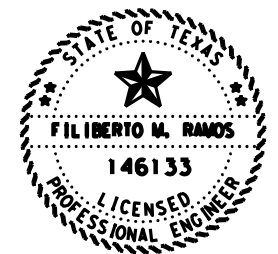
- LEGEND:**
- ⊗ EXISTING SIGN TO REMAIN IN PLACE
 - SIGN TO BE INSTALLED (ITEM 644)
 - △ SIGN TO BE REMOVED (ITEM 644)
 - ☆ EXISTING STREET NAME SIGNS TO BE RELOCATED BY OTHERS
 - ➔ TRAFFIC FLOW INDICATOR
 - PROPOSED OBJECT MARKER
 - ◻ EXISTING OBJECT MARKER
 - ▲ PROPOSED SIGN
 - EXISTING SIGN

- NOTES:**
1. PROPOSED SMALL SIGNS SHALL BE INSTALLED USING NEW POSTS AND PANELS.
 2. REMOVAL OF EXISTING SMALL SIGNS SHALL INCLUDE POST, PANEL, & FOUNDATION SHALL BE REMOVED TO 2 FT. BELOW FINISHED GRADE.
 3. ALL SIGNS TO BE REMOVED ARE TO BE DELIVERED TO THE TXDOT MAINTENANCE YARD IN HEBBRONVILLE, TEXAS LOCATED AT:
 406 Falfurrias Hwy
 Hebbroville, TX 78361
 4. EXISTING STREET SIGNS TO BE REPLACED BY CITY OF HEBBRONVILLE.
 5. ALL PROPOSED SMALL SIGNS TO BE INSTALLED OUTSIDE OF PROPOSED EDGE OF PAVEMENT PER STANDARD SHEET SMD(GEN)-08.
 6. SEE APPLICABLE TXDOT SIGNING STANDARD SHEETS FOR ADDITIONAL INFORMATION.



SEE TS2(PL-2)-23 FOR PLACEMENT

STA 620+00
 END PROJECT
 CONSTRUCTION
 CSJ: 0482-02-035



Filiberto M. Ramos
 03/14/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 SIGNING &
 DELINEATION
 LAYOUTS**

SCALE: 1" = 100' SHEET 22 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		295

SUMMARY OF SMALL SIGNS

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information from any source into digital form.

DATE: 2/29/2024 11:51:51 AM
 FILE: \\txdot\project\wiseonline.com:TXDOT15\Documents\21 - PHR\Design Project\010610\010610.dwg

PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)	
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION		
							FRP = Fiberglass TWT = Thin-Wall 10BWG = 10 BWG S80 = Sch 80	1 or 2	UA=Universal Conc UB=Universal Bolt SA=Slipbase-Conc SB=Slipbase-Bolt WS=Wedge Steel WP=Wedge Plastic	PREFABRICATED P = "Plain" T = "T" U = "U"		1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel EXAL= Extruded Alum Sign Panels
1 OF 22	1	M3-4 M1-6T	WEST 	24x12 24x24	✓		S80	1	SA	P		TY = TYPE TY N TY S
	2	EM-1aT		24x24	✓		S80	1	SA	P		
	3	R1-1 W4-4P	STOP 	36x36 36x18	✓		S80	1	SA	T		
	4	M1-6T M6-4 M3-3 M1-6F M6-1	285 TEXAS SOUTH 	24x24 21x15 24x12 24x24 21x15	✓		S80	1	SA	U		
	5	R2-1	SPEED LIMIT 55	30x36	✓		S80	1	SA	P		
	6	D1-3		96x36	✓		S80	1	SA	T		
	7	M3-3 M1-6F M6-1	SOUTH 	24x12 24x24 21x15	✓		S80	1	SA	P		
	8	M1-6T M6-4 D10-7aT D10-7aT	285 TEXAS 	24x24 21x15 3x10 3x10	✓		S80	1	SA	P		

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.
<http://www.txdot.gov/>

NOTE:

1. Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
2. For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
3. For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

Pharr District Central Design

SH 285
SUMMARY OF
SMALL SIGNS

SHEET 1 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		296

SUMMARY OF SMALL SIGNS

DATE: 2/29/2024 11:51:53 AM
 FILE: \\txdotproject\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\03032024\1017 Farm Road\1017 Farm Road\Summary of Small Signs

PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION	
							FRP = Fiberglass TWT = Thin-Wall 10BWG = 10 BWG S80 = Sch 80	1 or 2	UA=Universal Conc UB=Universal Bolt SA=Slipbase-Conc SB=Slipbase-Bolt WS=Wedge Steel WP=Wedge Plastic	PREFABRICATED P = "Plain" T = "T" U = "U"	1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel EXAL= Extruded Alum Sign Panels
	9	R1-1 W4-4P		36x36 36x18	✓		S80	1	SA	T	
	10	M3-2 M1-6T	EAST 285 TEXAS	24x12 24x24	✓		S80	1	SA	P	
	11	D15-10T	PASSING LANE 2 MILES	54x42	✓		S80	1	SA	U	
	12	R2-1	SPEED LIMIT 70	30x36	✓		S80	1	SA	P	
	13	D3-3oTR D3-3oTL	Cemetery → ← Cemetery	66x12 66x12	✓		S80	1	SA	T	
	14	D1-3	↑ Hebronnville ← Rio Grande City ← Edinburg	96x36	✓		S80	1	SA	T	
	15	W3-5	SPEED LIMIT 55	36x36	✓		S80	1	SA	T	
	16	R2-1	SPEED LIMIT 70	30x36	✓		S80	1	SA	P	
	17	D2-2	Falfurrias 34 Riviera 56	66x24	✓		S80	1	SA	T	
	18	R2-1	SPEED LIMIT 75	30x36	✓		S80	1	SA	P	
2 OF 22	1	M2-1 M1-6F	JCT FARM ROAD 1017	21x15 24x24	✓		S80	1	SA	P	

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website:
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

Pharr District Central Design





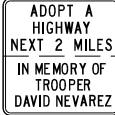






SH 285 SUMMARY OF SMALL SIGNS

SHEET 2 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	297

SUMMARY OF SMALL SIGNS

DATE: 2/29/2024 11:51:56 AM
 FILE: \\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Project\036\036.ctb
 PHR\Design Project\036\036.ctb
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units of measurement.


PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)	
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION		
										1EXT or 2EXT = # of Ext		BM = Extruded Wind Beam
	2	W2-1aT		48x48	✓		S80	1	SA	T		
	3	M3-4 M1-6T D10-7aT D10-7aT	  	24x12 24x24 3x10 3x10	✓		S80	1	SA	P		
4 OF 22	1	D14-4T-3 (TOP) D14-4T-3 (BOTTOM)		48x24 48x24	✓		S80	1	SA	T		
		CW21-1aT		48x48								
	2	W8-13aT		36x36	✓		S80	1	SA	T		
5 OF 22	1	I-3		42x18	✓		S80	1	SA	P		
	2	I-3		42x18	✓		S80	1	SA	P		
	3	W8-13aT		36x36	✓		S80	1	SA	T		
	4	R4-3		24x30	✓		S80	1	SA	P		

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

Pharr District Central Design















SH 285
SUMMARY OF
SMALL SIGNS

SHEET 3 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	298

SUMMARY OF SMALL SIGNS

DATE: 2/29/2024 11:51:58 AM
 FILE: \\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Project\040624\040624.dwg
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of any information contained herein.


PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION	
							FRP = Fiberglass TWT = Thin-Wall 10BWG = 10 BWG S80 = Sch 80 1 or 2 UA=Universal Conc UB=Universal Bolt SA=Slipbase-Conc SB=Slipbase-Bolt WS=Wedge Steel WP=Wedge Plastic PREFABRICATED P = "Plain" T = "T" U = "U" 1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel EXAL= Extruded Alum Sign Panels				
6 OF 22	1	W9-2TL		36x36	✓		S80	1	SA	T	
	2	W9-1R		36x36	✓		S80	1	SA	T	
7 OF 22	1	M3-2 M1-6T D10-7aT D10-7aT	  	24x12 24x24 3x10 3x10	✓		S80	1	SA	P	
	2	W9-1R		36x36	✓		S80	1	SA	T	
	3	W9-2TL		36x36	✓		S80	1	SA	T	
8 OF 22	1	R4-3		24x30	✓		S80	1	SA	P	
	2	D15-11T		54x48	✓		S80	1	SA	U	
11 OF 22	1	M3-4 M1-6T D10-7aT D10-7aT	  	24x12 24x24 3x10 3x10	✓		S80	1	SA	P	

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website:
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

Pharr District Central Design


Texas Department of Transportation

SH 285
SUMMARY OF
SMALL SIGNS












SHEET 4 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	299

SUMMARY OF SMALL SIGNS

DATE: 2/29/2024 11:52:00 AM
 FILE: \\txdot.projectwiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\0306\01\036\01\0000\Summary of Small Signs

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the use of any other standard or specification.


PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2) TY = TYPE	
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION		
										1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel		PREFABRICATED
	2	D15-11T		54x48	✓		S80	1	SA	U		
	3	R4-3		24x30	✓		S80	1	SA	P		
12 OF 22	1	W9-2TL		36x36	✓		S80	1	SA	T		
	2	W9-1R		36x36	✓		S80	1	SA	T		
14 OF 22	1	W9-1R		36x36	✓		S80	1	SA	T		
	2	W9-2TL		36x36	✓		S80	1	SA	T		
15 OF 22	1	R4-3		24x30	✓		S80	1	SA	P		
	2	M3-2		24x12	✓		S80	1	SA	P		
		M1-6T		24x24								
		D10-7aT		3x10								
		D10-7aT		3x10								
			BK TO BK									

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

Pharr District Central Design








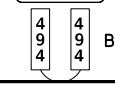



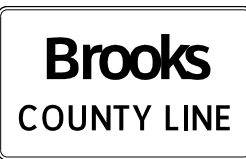


**SH 285
SUMMARY OF
SMALL SIGNS**

SHEET 5 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		300

SUMMARY OF SMALL SIGNS


PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)	
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION		
							FRP = Fiberglass TWT = Thin-Wall 10BWG = 10 BWG S80 = Sch 80				1 or 2	
	3	D15-11T		54x48	✓		S80	1	SA	U		
18 OF 22												
	1	D15-11T		54x48	✓		S80	1	SA	U		
19 OF 22												
	1	R4-3		24x30	✓		S80	1	SA	P		
	2	W9-2TL		36x36	✓		S80	1	SA	T		
20 OF 22												
	1	W9-1R		36x36	✓		S80	1	SA	T		
	2	M3-4 M1-6T D10-7aT D10-7aT	  	24x12 24x24 3x10 3x10	✓		S80	1	SA	P		
21 OF 22												
	1	R2-1		30x36	✓		S80	1	SA	P		
	2	I-2dT		72x24	✓		S80	1	SA	T		
	3	D20-1TL		24x24	✓		S80	1	SA	P		
	4	I-2dT		54x24	✓		S80	1	SA	T		

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

Pharr District Central Design



**SH 285
SUMMARY OF
SMALL SIGNS**

SHEET 6 OF 7

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	301

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units. The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units.

DATE: 2/29/2024 11:52:02 AM
 FILE: \\txdotproject\wisonline.com:TXDOT\Documents\21 - PHR\Design Projects\0482\01\036\etc\036\etc\SUMMARY OF SMALL SIGNS

SUMMARY OF SMALL SIGNS

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units. The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units.

PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN	DIMENSIONS	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)	
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION		
										PREFABRICATED		1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel EXAL= Extruded Alum Sign Panels
	5	R1-1 W4-4P		36x36 36x18	✓		S80	1	SA	T		
	6	D20-1TR		24x24	✓		S80	1	SA	P		
	7	M3-2 M1-6T D10-7aT D10-7aT		24x12 24x24 3x10 3x10	✓		S80	1	SA	P		
	8	W9-1R		36x36	✓		S80	1	SA	T		
	9	R2-1		30x36	✓		S80	1	SA	P		
	10	W9-2TL		36x36	✓		S80	1	SA	T		
22 OF 22	1	R4-3		24x30	✓		S80	1	SA	P		
	2	EM-1aT		24x24	✓		S80	1	SA	P		
	3	D15-10T		54x42	✓		S80	1	SA	U		

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).

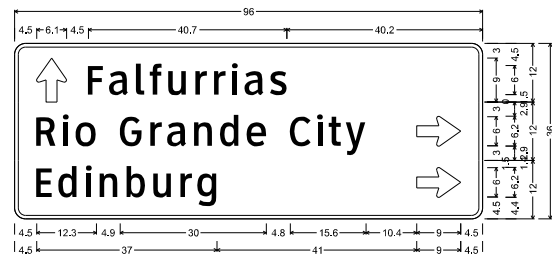
Pharr District Central Design

SH 285
SUMMARY OF
SMALL SIGNS

SHEET 7 OF 7

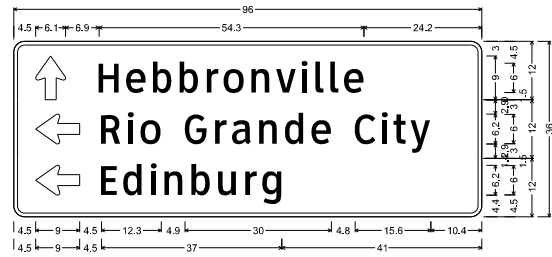
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		302

SHEET 1 OF 22
SIGN 6



D1-3 6in UP-RT-RT;
 2.3" Radius, 0.8" Border, White on Green;
 Standard Arrow Custom 9.0" X 6.1" 90"; "Falfurrias", ClearviewHwy-3-W;
 2.3" Radius, 0.8" Border, White on Green;
 "Rio Grande City", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0";
 2.3" Radius, 0.8" Border, White on Green;
 "Edinburg", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0";

SHEET 1 OF 22
SIGN 14



D1-3 6in UP-L-LT;
 2.3" Radius, 0.8" Border, White on Green;
 Standard Arrow Custom 9.0" X 6.1" 90"; "Hebronville", ClearviewHwy-3-W;
 2.3" Radius, 0.8" Border, White on Green;
 Standard Arrow Custom 9.0" X 6.1" 180"; "Rio Grande City", ClearviewHwy-3-W;
 2.3" Radius, 0.8" Border, White on Green;
 Standard Arrow Custom 9.0" X 6.1" 180"; "Edinburg", ClearviewHwy-3-W;

SHEET 1 OF 22
SIGN 17



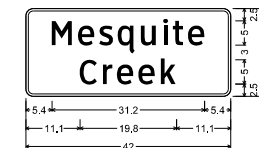
D2-2 6in;
 1.5" Radius, 0.8" Border, White on Green;
 "Falfurrias", ClearviewHwy-3-W; "34", ClearviewHwy-3-W;
 1.5" Radius, 0.8" Border, White on Green;
 "Riviera", ClearviewHwy-3-W; "56", ClearviewHwy-3-W;

SHEET 4 OF 22
SIGN 1



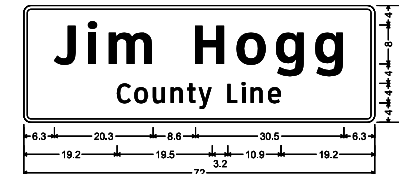
D14-4T-L_48x48;
 3.0" Radius, 1.0" Border, White on Blue;
 "ADOPT A", C: "HIGHWAY", C;
 "NEXT 2 MILES", C;
 3.0" Radius, 1.0" Border, White on Blue;
 "HEBRONVILLE", C: "ELEMENTARY", C;
 "LEADERSHIP", C: "TEAM", C;

SHEET 5 OF 22
SIGNS 1 & 2



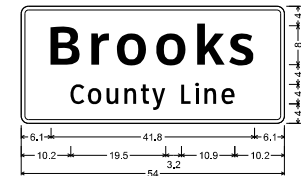
I-3 6in;
 1.5" Radius, 0.5" Border, White on Green;
 "Mesquite", ClearviewHwy-3-W;
 "Creek", ClearviewHwy-3-W;

SHEET 21 OF 22
SIGN 2



I-2aT 6in;
 1.5" Radius, 0.8" Border, White on Green;
 "Jim Hogg", ClearviewHwy-5-W-R; "County Line", ClearviewHwy-3-W;

SHEET 21 OF 22
SIGN 4



I-2aT 6in;
 1.5" Radius, 0.8" Border, White on Green;
 "Brooks", ClearviewHwy-5-W-R;
 "County Line", ClearviewHwy-3-W;

Pharr District Central Design



SH 285
SMALL SIGN DETAILS

SHEET 1 OF 1


© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, et c.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		303

DATE: 2/16/2024 6:30:48 PM
 FILE: \\txdot\projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 SIGNING\SH 285 SUMMARY OF SIGNS TO BE REMOVED

SUMMARY OF SIGNS TO BE REMOVED					
PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN TEXT	DIMENSIONS (INCHES)	644
					6076
					REMOVE SM RD SN SUP&AM TY S80
					EA
SHEET 1 OF 22					
	1	M3-4 M1-6T	WEST 285 TEXAS	24x12 24x24	1
	2	R1-1 W4-4P	STOP CROSS TRAFFIC DOES NOT STOP	36x36 36x18	1
	3	M1-6T M6-4 M3-3 M1-6F M6-1	285 TEXAS DOUBLE ARROW SOUTH FM 1017 DIRECTIONAL ARROW	24x24 12x15 24x12 24x24 21x15	1
	4	R2-1	SPEED LIMIT	24x30	1
	5	D1-3	FALFURRIAS/RGC/EDINBURG	126x42	1
	6	M3-3 M1-6F M6-1	SOUTH FM 1017 DIRECTIONAL ARROW	24x12 24x24 21x15	1
	7	M1-6T M6-4 D10-7gT	285 TEXAS DOUBLE ARROW MILE MARKER	24x24 21x15 3x10	1
	8	R1-1 W4-4P	STOP CROSS TRAFFIC DOES NOT STOP	36x36 36x18	1
	9	M3-2 M1-6T	EAST 285 TEXAS	24x12 24x24	1
	10	R2-1	SPEED LIMIT	24x30	1
	11	D3-3gTR D3-3gTL	CEMETERY CEMETERY	66x12 66x12	1
	12	D1-3	HEBBRONVILLE/RGC/EDINBURG	126x42	1
	13	W3-5	REDUCED SPEED LIMIT AHEAD	36x36	1
	14	R2-1	SPEED LIMIT	24x30	1
	15	D2-2	FALFURRIAS/RIVIERA	90x30	1
	16	R2-1	SPEED LIMIT	24x30	1
SHEET 2 OF 22					
	1	M2-1 M1-6F	JCT FM 1017	21x15 24x24	1
	2	W2-1gT	HIGHWAY INTERSECTION AHEAD	48x48	1
	3	M3-4 M1-6T D10-7gT	WEST 285 TEXAS MILE MARKER	24x12 24x24 3x10	1
SHEET 4 OF 22					
	1	D14-4T-3 CW21-1gT	ADOPT A HIGHWAY WORKER AHEAD	48x48 36"x36"	1
	2	W8-13gT	BRIDGE MAY ICE IN COLD WEATHER	36x36	1
SHEET 5 OF 22					
	1	I-3	MESQUITE CREEK	36x18	1
	2	I-3	MESQUITE CREEK	36x18	1
	3	W8-13gT	BRIDGE MAY ICE IN COLD WEATHER	36x36	1
SHEET 7 OF 22					
	1	M3-2 M1-6T D10-7gT	EAST 285 TEXAS MILE MARKER	24x12 24x24 3x10	1
SHEET 11 OF 22					
	1	M3-4 M1-6T D10-7gT	WEST 285 TEXAS MILE MARKER	24x12 24x24 3x10	1

SUMMARY OF SIGNS TO BE REMOVED					
PLAN SHEET NO.	SIGN NO.	SIGN NOMENCLATURE	SIGN TEXT	DIMENSIONS (INCHES)	644
					6076
					REMOVE SM RD SN SUP&AM TY S80
					EA
SHEET 15 OF 22					
	1	M3-2 M1-6T	EAST 285 TEXAS	24x12 24x24	1
SHEET 20 OF 22					
	1	M3-4 M1-6T D10-7gT	WEST 285 TEXAS MILE MARKER	24x12 24x24 3x10	1
	2	EM-1gT	EVACUATION ROUTE	24x24	1
SHEET 21 OF 22					
	1	R2-1	SPEED LIMIT	24x30	1
	2	I-2dT	JIM HOGG CTY LINE	72x36	1
	3	D20-1TL	CO RD 258	24x24	1
	4	I-2dT	BROOKS CTY LINE	60x36	1
	5	R1-1	STOP	36x36	1
	6	D20-1TL	CO RD 258	24x24	1
	7	M3-2 M1-6T	EAST 285 TEXAS	24x12 24x24	1
	8	R2-1	SPEED LIMIT	24x30	1
TOTAL :					37

Pharr District Central Design



**SH 285
SUMMARY OF
SIGNS TO BE
REMOVED**

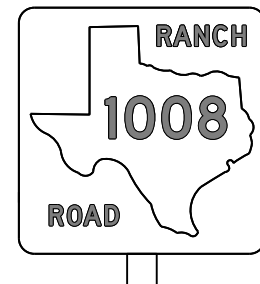
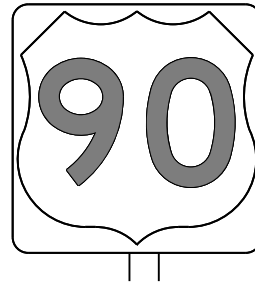
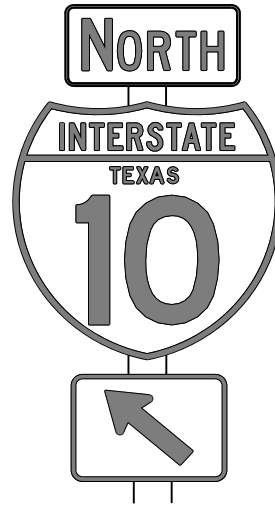
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		304

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to any other format or for any errors or omissions that may appear hereon.

DATE: 2/12/2024 8:21:14 AM
 FILE: \\txdot\projectwiseonline.com\TXDOT\Documents\21 - PHR\Design Projects\21-036\01\036-13.dgn

REQUIREMENTS FOR INDEPENDENT MOUNTED ROUTE SIGNS

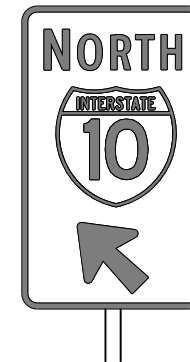
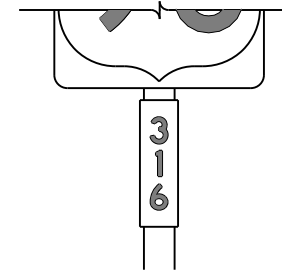
SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	WHITE	TYPE A SHEETING
BACKGROUND	ALL OTHERS	TYPE B OR C SHEETING
LEGEND & BORDERS	WHITE	TYPE A SHEETING
LEGEND & BORDERS	BLACK	ACRYLIC NON-REFLECTIVE FILM
LEGEND & BORDERS	ALL OTHERS	TYPE B or C SHEETING



TYPICAL EXAMPLES

REQUIREMENTS FOR BLUE, BROWN & GREEN D AND I SERIES GUIDE SIGNS

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	ALL	TYPE B OR C SHEETING
LEGEND & BORDERS	WHITE	TYPE D SHEETING
LEGEND, SYMBOLS & BORDERS	ALL OTHERS	TYPE B OR C SHEETING



TYPICAL EXAMPLES

GENERAL NOTES

- Signs to be furnished shall be as detailed elsewhere in the plans and/or as shown on sign tabulation sheet. Standard sign designs and arrow dimensions can be found in the "Standard Highway Sign Designs for Texas" (SHSD).
- White legend shall use the Clearview Alphabet. The following Clearview fonts shall be used to replace the existing white Federal Highway Administration (FHWA) Standard Highway Alphabets, when not specified in the SHSD, or in the plans.

B	CV-1W
C	CV-2W
D	CV-3W
E	CV-4W
Emod	CV-5WR
F	CV-6W

- Route sign legend (ie. IH, US, SH and FM shields) shall use the Federal Highway Administration (FHWA) Standard Highway Alphabets B, C, D, E, Emod or F).
- Lateral spacing between letters and numerals shall conform with the SHSD, and any approved changes thereto. Lateral spacing of legend shall provide a balanced appearance when spacing is not shown.
- Independent mounted route sign with white or colored legend and borders shall be applied by screening process with transparent color ink, transparent colored overlay film to white background sheeting or cut-out white sheeting to colored background sheeting, or combination thereof. White legend, symbols and borders on all other signs shall be cut-out white sheeting applied to colored background sheeting.
- Information regarding borders and radii for signs is found in the "Standard Highway Sign Designs for Texas". Dimensions shown and described for borders and corner radii on parent sign are nominal. Borders may vary in width as much as 1/2 inch. Corner radii above 3 inches may vary in width as much as 1 inch. Borders and corner radii within a parent sign must be of matching widths. The sign area outside the corner radius should be trimmed or rounded.
- Sign substrate shall be any material that meets the Departmental Material Specification requirements of DMS-7110 or approved alternative.
- Mounting details of roadside signs are shown in the "SMD series" Standard Plan Sheets.

DEPARTMENTAL MATERIAL SPECIFICATIONS	
ALUMINUM SIGN BLANKS	DMS-7110
SIGN FACE MATERIALS	DMS-8300

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080
7.5 to 15	0.100
Greater than 15	0.125

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.

<http://www.txdot.gov/>



TYPICAL SIGN REQUIREMENTS

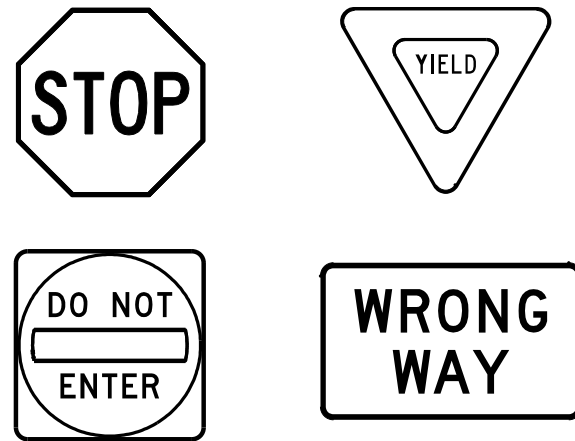
TSR(3) - 13

FILE: tsr3-13.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
© TxDOT October 2003	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
12-03 7-13	DIST	COUNTY	SHEET NO.	
9-08	PHR	JIM HOGG	305	

DATE: 2/12/2024 8:21:18 AM
 FILE: \\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201\036\etc\SH 285\tsr4-13.dgn
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to any other format.

REQUIREMENTS FOR RED BACKGROUND REGULATORY SIGNS

(STOP, YIELD, DO NOT ENTER AND WRONG WAY SIGNS)



REQUIREMENTS FOR FOUR SPECIFIC SIGNS ONLY

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	WHITE	TYPE B OR C SHEETING
LEGEND & BORDERS	WHITE	TYPE B OR C SHEETING
LEGEND	RED	TYPE B OR C SHEETING

REQUIREMENTS FOR WHITE BACKGROUND REGULATORY SIGNS

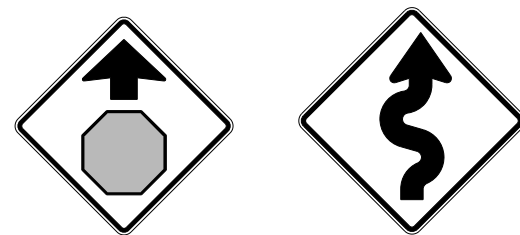
(EXCLUDING STOP, YIELD, DO NOT ENTER AND WRONG WAY SIGNS)



TYPICAL EXAMPLES

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	WHITE	TYPE A SHEETING
BACKGROUND	ALL OTHERS	TYPE B OR C SHEETING
LEGEND, BORDERS AND SYMBOLS	BLACK	ACRYLIC NON-REFLECTIVE FILM
LEGEND, BORDERS AND SYMBOLS	ALL OTHER	TYPE B OR C SHEETING

REQUIREMENTS FOR WARNING SIGNS



TYPICAL EXAMPLES

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	FLOURESCENT YELLOW	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND & BORDERS	BLACK	ACRYLIC NON-REFLECTIVE FILM
LEGEND & SYMBOLS	ALL OTHER	TYPE B OR C SHEETING

REQUIREMENTS FOR SCHOOL SIGNS



TYPICAL EXAMPLES

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	WHITE	TYPE A SHEETING
BACKGROUND	FLOURESCENT YELLOW GREEN	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND, BORDERS AND SYMBOLS	BLACK	ACRYLIC NON-REFLECTIVE FILM
SYMBOLS	RED	TYPE B OR C SHEETING

GENERAL NOTES

- Signs to be furnished shall be as detailed elsewhere in the plans and/or as shown on sign tabulation sheet. Standard sign designs and arrow dimensions can be found in the "Standard Highway Sign Designs for Texas" (SHSD).
- Sign legend shall use the Federal Highway Administration (FHWA) Standard Highway Alphabets (B, C, D, E, Emod or F).
- Lateral spacing between letters and numerals shall conform with the SHSD, and any approved changes thereto. Lateral spacing of legend shall provide a balanced appearance when spacing is not shown.
- Black legend and borders shall be applied by screening process or cut-out acrylic non-reflective black film to background sheeting, or combination thereof.
- White legend and borders shall be applied by screening process with transparent colored ink, transparent colored overlay film to white background sheeting or cut-out white sheeting to colored background sheeting, or combination thereof.
- Colored legend shall be applied by screening process with transparent colored ink, transparent colored overlay film or colored sheeting to background sheeting, or combination thereof.
- Sign substrate shall be any material that meets the Departmental Material Specification requirements of DMS-7110 or approved alternative.
- Mounting details for roadside mounted signs are shown in the "SMD series" Standard Plan Sheets.

ALUMINUM SIGN BLANKS THICKNESS

Square Feet	Minimum Thickness
Less than 7.5	0.080
7.5 to 15	0.100
Greater than 15	0.125

DEPARTMENTAL MATERIAL SPECIFICATIONS

ALUMINUM SIGN BLANKS	DMS-7110
SIGN FACE MATERIALS	DMS-8300

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.

<http://www.txdot.gov/>



TYPICAL SIGN REQUIREMENTS

TSR(4) - 13

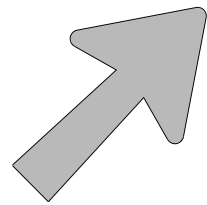
FILE:	tsr4-13.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CK:	TxDOT
© TxDOT	October 2003	CONT	SECT	JOB	HIGHWAY				
REVISIONS		0482	01	036, etc.	SH 285				
12-03	7-13	DIST	COUNTY	SHEET NO.					
9-08		PHR	JIM HOGG	306					

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or for the accuracy of the information contained herein. The user of this standard is advised to consult the Texas Engineering Practice Act and the Texas Administrative Code for the latest regulations.

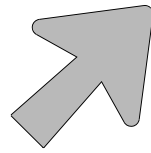
DATE: 2/12/2024 8:21:22 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201\036, etc.\SH 285.dgn

ARROW DETAILS

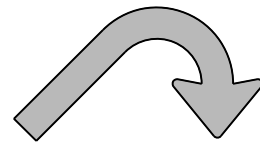
for Large Ground-Mounted and Overhead Guide Signs



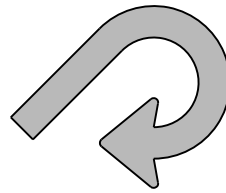
Type A



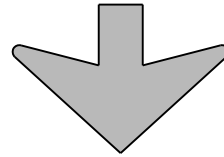
Type B



E-3



E-4



Down Arrow

TYPE	LETTER SIZE	USE
A-1	10.67" U/L and 10" Caps	Single Lane Exits
A-2	13.33" U/L and 12" Caps	
A-3	16" & 20" U/L	
B-1	10.67" U/L and 10" Caps	Multiple Lane Exits
B-2	13.33" U/L and 12" Caps	
B-3	16" & 20" U/L	

CODE	USED ON SIGN NO.
E-3	E5-1aT
E-4	E5-1bT

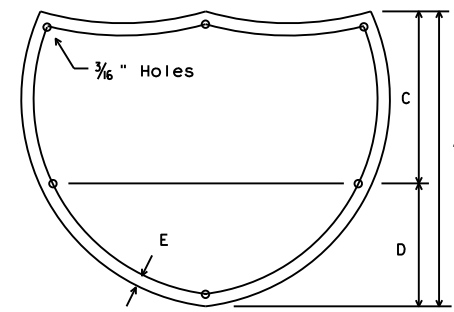
NOTE

Arrow dimensions are shown in the "Standard Highway Sign Designs for Texas" manual.

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.

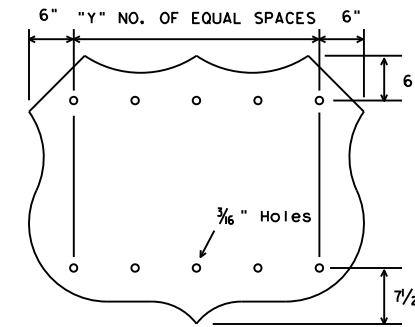
<http://www.txdot.gov/>

SIGN BLANK PUNCHING DETAILS FOR ATTACHMENTS WHEN SPECIFIED TO BE TYPE A ALUMINUM SIGNS (FOR MOUNTING TO GUIDE SIGN FACE)



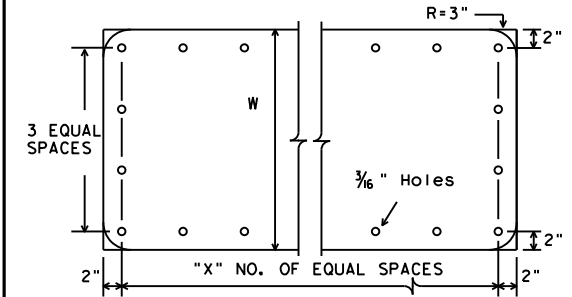
INTERSTATE ROUTE MARKERS

A	C	D	E
36	21	15	1 1/2
48	28	20	1 3/4



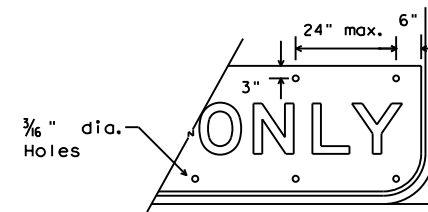
U.S. ROUTE MARKERS

Sign Size	"Y"
24x24	2
30x24	3
36x36	3
45x36	4
48x48	4
60x48	5



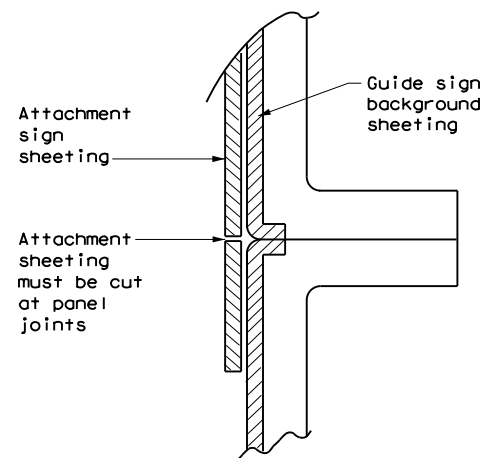
STATE ROUTE MARKERS

No. of Digits	W	X
4	24	4
4	36	5
4	48	6
3	24	3
3	36	4
3	48	5



EXIT ONLY PANEL

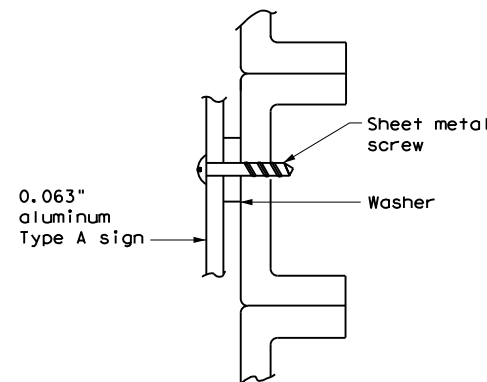
MOUNTING DETAILS OF ATTACHMENTS TO GUIDE SIGN FACE ("EXIT ONLY" AND "LEFT EXIT" PANELS, ROUTE MARKERS AND OTHER ATTACHMENTS)



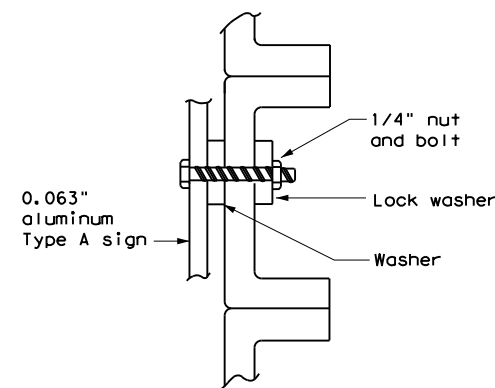
DIRECT APPLIED ATTACHMENT

NOTE:

- Sheeting for legend, symbols, and borders must be cut at panel joints.
- Direct applied attachment signs will be subsidiary to "Aluminum Signs" or "Fiberglass Signs".



SCREW ATTACHMENT

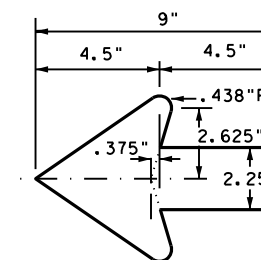


NUT/BOLT ATTACHMENT

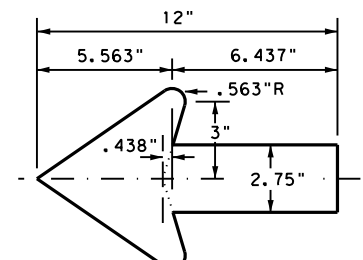
NOTE:

Furnish Type A aluminum sign attachments only when specified in the plans. These signs will be paid for under "Aluminum Signs".

ARROW DETAILS for Destination Signs (Type D)



Standard arrow to be used with 6 inch letters.



Standard arrow to be used with 8 inch letters.



TYPICAL SIGN REQUIREMENTS

TSR (5) - 13

FILE: tsr5-13.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CR: TxDOT
© TxDOT October 2003	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
12-03 7-13	DIST	COUNTY	SHEET NO.	
9-08	PHR	JIM HOGG	307	

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 8:21:26 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - Sign\Plan Set\14 - Sign\Standards\Signing_Standards\smgen.dgn

SIGN SUPPORT DESCRIPTIVE CODES

(Descriptive Codes correspond to project estimate and quantities sheets)

SM RD SGN ASSM TY XXXXX(X)XX(X-XXXX)

Post Type

FRP = Fiberglass Reinforced Plastic Pipe (see SMD(FRP))
 TWT = Thin-Walled Tubing (see SMD(TWT))
 10BWG = 10 BWG Tubing (see SMD(SLIP-1) to (SLIP-3))
 S80 = Schedule 80 Pipe (see SMD(SLIP-1) to (SLIP-3))

Number of Posts (1 or 2)

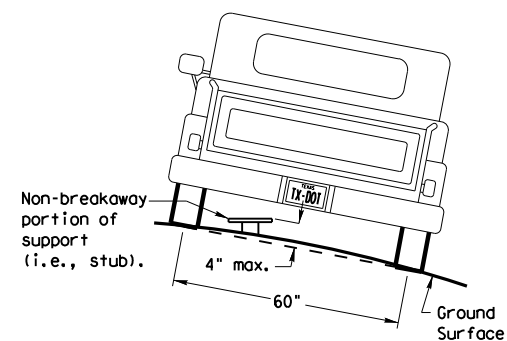
Anchor Type

UA = Universal Anchor - Concreted (see SMD(FRP) and (TWT))
 UB = Universal Anchor - Bolted down (see SMD(FRP) and (TWT))
 WS = Wedge Anchor Steel - (see SMD(TWT))
 WP = Wedge Anchor Plastic (see SMD(TWT))
 SA = Slipbase - Concreted (see SMD(SLIP-1) to (SLIP-3))
 SB = Slipbase - Bolted Down (see SMD(SLIP-1) to (SLIP-3))

Sign Mounting Designation

P = Prefab. "Plain" (see SMD(SLIP-1) to (SLIP-3), (TWT), (FRP))
 T = Prefab. "T" (see SMD(SLIP-1) to (SLIP-3), (TWT))
 U = Prefab. "U" (see SMD(SLIP-1) to (SLIP-3))
 IF REQUIRED
 1EXT or 2EXT = Number of Extensions (see SMD(SLIP-1) to (SLIP-3), (TWT))
 BM = Extruded Wind Beam (see SMD(SLIP-1) to (SLIP-3))
 WC = 1.12 #/ft Wing Channel (see SMD(SLIP-1) to (SLIP-3))
 EXAL = Extruded Aluminum Sign Panels (see SMD(SLIP-3))

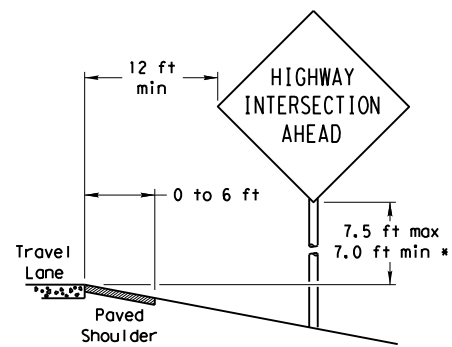
REQUIRED CLEARANCE FOR BREAKAWAY SUPPORT



To avoid vehicle undercarriage snagging, any substantial remains of a breakaway support, when it is broken away, should not project more than 4 inches above a 60-inch chord (i.e., typical space between wheel paths).

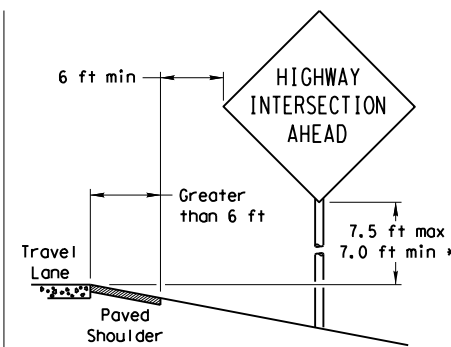
SIGN LOCATION

PAVED SHOULDERS



LESS THAN 6 FT. WIDE

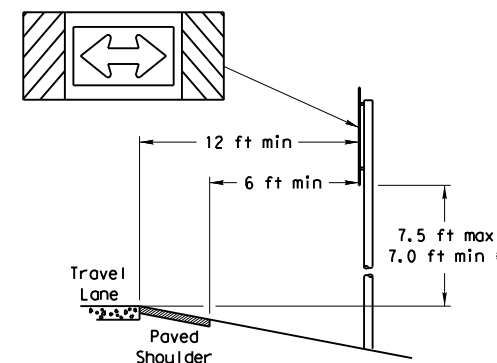
When the shoulder is 6 ft. or less in width, the sign must be placed at least 12 ft. from the edge of the travel lane.



GREATER THAN 6 FT. WIDE

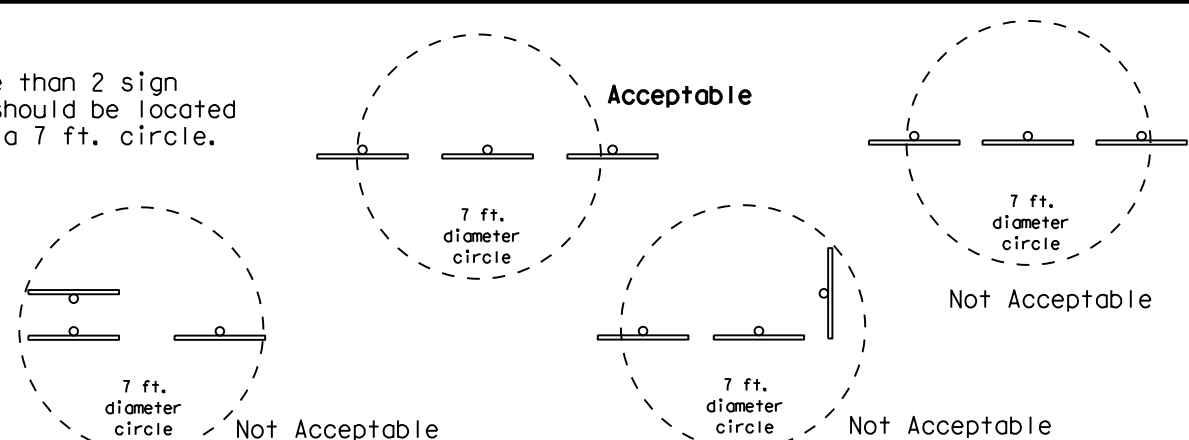
When the shoulder is greater than 6 ft in width, the sign must be placed at least 6 ft. from the edge of the shoulder.

T-INTERSECTION

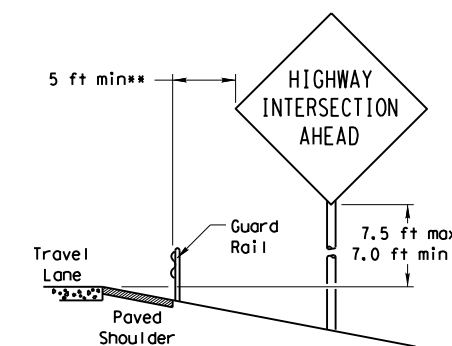


When this sign is needed at the end of a two-lane, two way roadway, the right edge of the sign should be in line with the centerline of the roadway. Place as close to ROW as practical.

No more than 2 sign posts should be located within a 7 ft. circle.

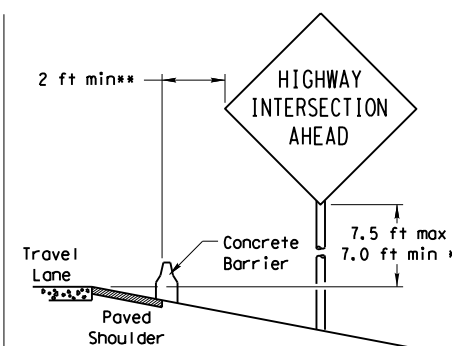


BEHIND BARRIER



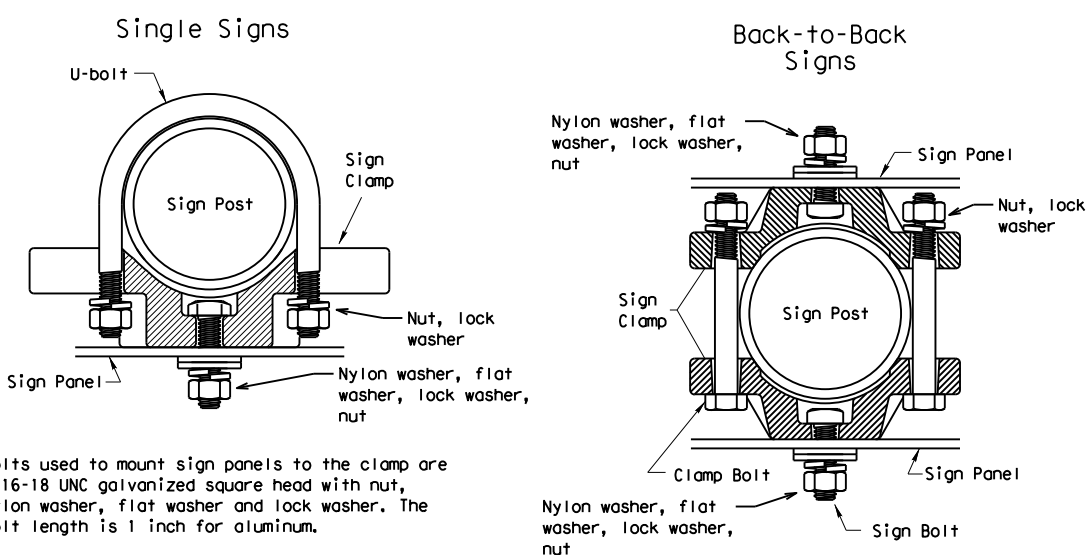
BEHIND GUARDRAIL

**Sign clearance based on distance required for proper guard rail or concrete barrier performance.



BEHIND CONCRETE BARRIER

TYPICAL SIGN ATTACHMENT DETAIL



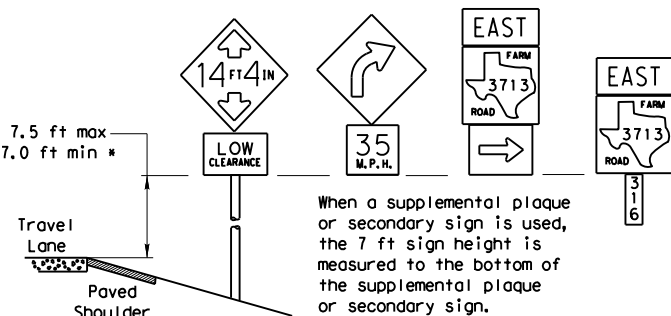
Bolts used to mount sign panels to the clamp are 5/16-18 UNC galvanized square head with nut, nylon washer, flat washer and lock washer. The bolt length is 1 inch for aluminum.

When two sign clamps are used to mount signs back-to-back, use a 5/16-18 UNC galvanized hex head per ASTM A307 with nut and helical-spring lock washer. The approximate bolt lengths for various post sizes and sign clamp types are given in the table at right. The bolt length may need to be adjusted depending upon field conditions.

Sign clamps may be either the specific size clamp or the universal clamp.

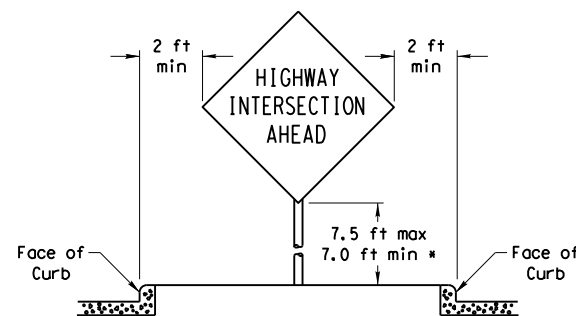
Pipe Diameter	Approximate Bolt Length	
	Specific Clamp	Universal Clamp
2" nominal	3"	3 or 3 1/2"
2 1/2" nominal	3 or 3 1/2"	3 1/2 or 4"
3" nominal	3 1/2 or 4"	4 1/2"

SIGNS WITH PLAQUES

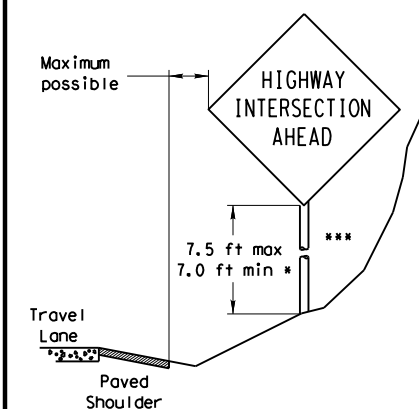


When a supplemental plaque or secondary sign is used, the 7 ft sign height is measured to the bottom of the supplemental plaque or secondary sign.

CURB & GUTTER OR RAISED ISLAND



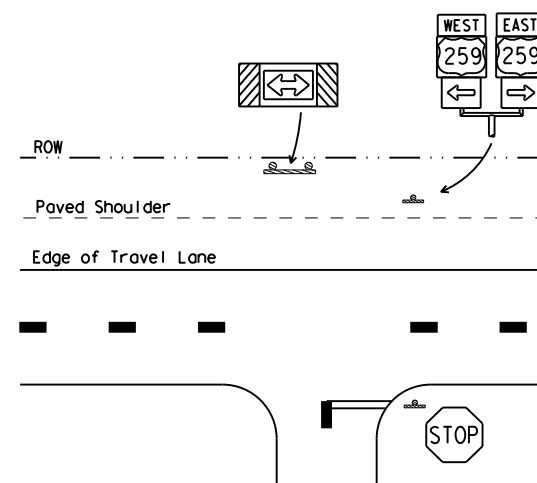
RESTRICTED RIGHT-OF-WAY (When 6 ft min. is not possible.)



Right-of-way restrictions may be created by rocks, water, vegetation, forest, buildings, a narrow island, or other factors.

In situations where a lateral restriction prevents the minimum horizontal clearance from the edge of the travel lane, signs should be placed as far from the travel lane as practical.

*** Post may be shorter if protected by guardrail or if Engineer determines the post could not be hit due to extreme slope.



* Signs shall be mounted using the following condition that results in the greatest sign elevation:

- (1) a minimum of 7 to a maximum of 7.5 feet above the edge of the travel lane or
- (2) a minimum of 7 to a maximum of 7.5 feet above the grade at the base of the support when sign is installed on the backslope.

The maximum values may be increased when directed by the Engineer.

See the Traffic Operations Division website for detailed drawings of sign clamps, Triangular Slipbase System components and Wedge Anchor System components.

The website address is:
<http://www.txdot.gov/publications/traffic.htm>

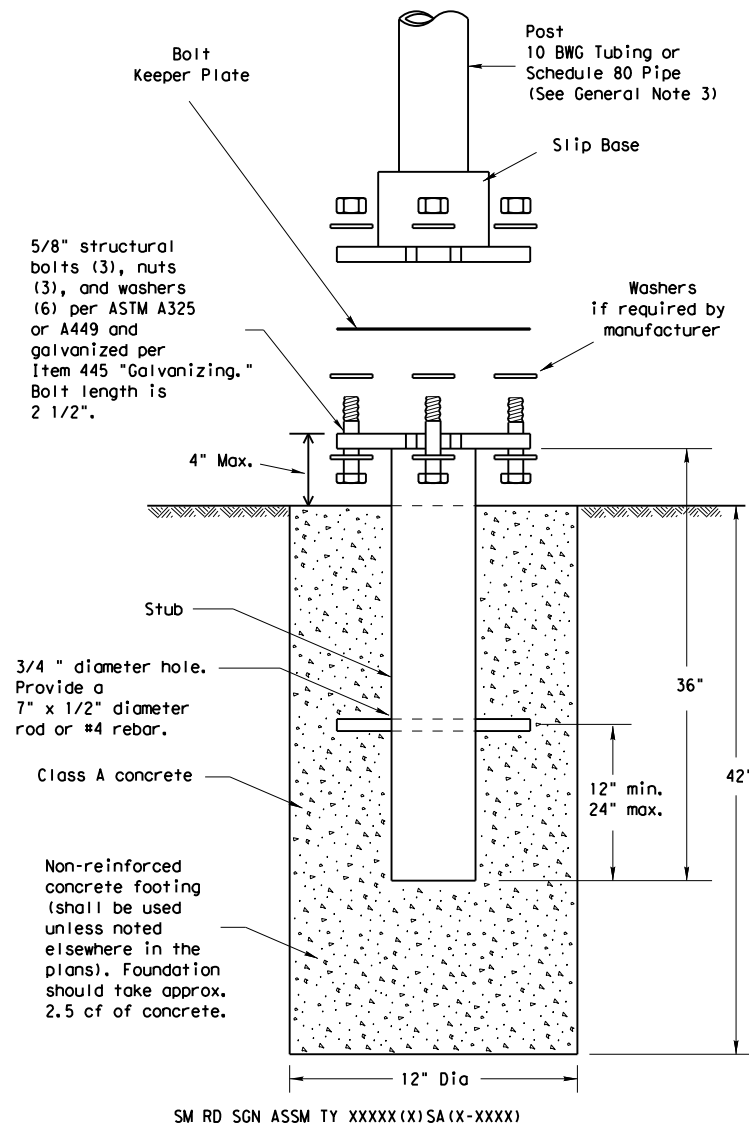
Texas Department of Transportation
 Traffic Operations Division

SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS GENERAL NOTES & DETAILS

SMD (GEN) - 08

© TxDOT July 2002		DN: TXDOT	CK: TXDOT	DW: TXDOT	CK: TXDOT
9-08	REVISIONS	CONT	SECT	JOB	HIGHWAY
		0482	01	036, etc.	SH 285
		DIST	COUNTY		SHEET NO.
		PHR	JIM HOGG		308

TRIANGULAR SLIPBASE INSTALLATION GENERAL REQUIREMENTS



NOTE

There are various devices approved for the Triangular Slipbase System. Please reference the Material Producer List for approved slip base systems. http://www.txdot.gov/business/producer_list.htm The devices shall be installed per manufacturers' recommendations. Installation procedures shall be provided to the Engineer by Contractor.

GENERAL NOTES:

- Slip base shall be permanently marked to indicate manufacturer. Method, design, and location of marking are subject to approval of the TxDOT Traffic Standards Engineer.
- Material used as post with this system shall conform to the following specifications:
 - 10 BWG Tubing (2.875" outside diameter)
 - 0.134" nominal wall thickness
 - Seamless or electric-resistance welded steel tubing or pipe
 - Steel shall be HSLAS Gr 55 per ASTM A1011 or ASTM A1008
 - Other steels may be used if they meet the following:
 - 55,000 PSI minimum yield strength
 - 70,000 PSI minimum tensile strength
 - 20% minimum elongation in 2"
 - Wall thickness (uncoated) shall be within the range of 0.122" to 0.138"
 - Outside diameter (uncoated) shall be within the range of 2.867" to 2.883"
 - Galvanization per ASTM A123 or ASTM A653 G210. For precoated steel tubing (ASTM A653), recoat tube outside diameter weld seam by metallizing with zinc wire per ASTM B833.
 - Schedule 80 Pipe (2.875" outside diameter)
 - 0.276" nominal wall thickness
 - Steel tubing per ASTM A500 Gr C
 - Other seamless or electric-resistance welded steel tubing or pipe with equivalent outside diameter and wall thickness may be used if they meet the following:
 - 46,000 PSI minimum yield strength
 - 62,000 PSI minimum tensile strength
 - 21% minimum elongation in 2"
 - Wall thickness (uncoated) shall be within the range of 0.248" to 0.304"
 - Outside diameter (uncoated) shall be within the range of 2.855" to 2.895"
 - Galvanization per ASTM A123
- See the Traffic Operations Division website for detailed drawings of sign clamps and Texas Universal Triangular Slipbase System components. The website address is: <http://www.txdot.gov/publications/traffic.htm>
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.

ASSEMBLY PROCEDURE

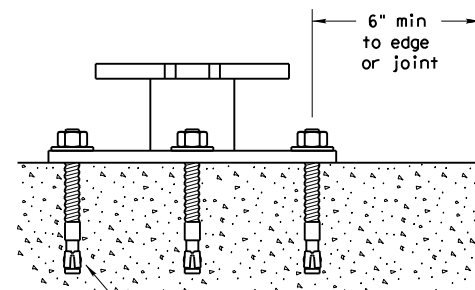
Foundation

- Prepare 12-inch diameter by 42-inch deep hole. If solid rock is encountered, the depth of the foundation may be reduced such that it is embedded a minimum of 18 inches into the solid rock.
- The Engineer may permit batches of concrete less than 2 cubic yards to be mixed with a portable, motor-driven concrete mixer. For small placements less than 0.5 cubic yards, hand mixing in a suitable container may be allowed by Engineer. Concrete shall be Class A.
- Push the pipe end of the slip base stub into the center of the concrete. Rotate the stub back and forth while pushing it down into the concrete to assure good contact between the concrete and stub. Continue to work the stub into the concrete until it is between 2 to 4 inches above the ground.
- Plumb the stub. Allow a minimum of 4 days to set, unless otherwise directed by the Engineer.
- The triangular slipbase system is multidirectional and is designed to release when struck from any direction.

Support

- Cut support so that the bottom of the sign will be 7 to 7.5 feet above the edge of the travelway (i.e., edge of the closest lane) when slip plate is below the edge of pavement or 7 to 7.5 feet above slip plate when the slip plate is above the edge of the travelway. The cut shall be plumb and straight.
- Attach sign to support using connections shown. When multiple signs are installed on the same support, ensure the minimum clearance between each sign is maintained. See SMD(SLIP-2) for clearances based on sign types.

CONCRETE ANCHOR



5/8" diameter Concrete Anchor - 8 places (embed a minimum of 5 1/2" and torque to min. of 50 ft-lbs). Anchor may be expansion or adhesive type.

SM RD SGN ASSM TY XXXXX(X)SB(X-XXXX)

Concrete anchor consists of 5/8" diameter stud bolt with UNC series bolt threads on the upper end. Heavy hex nut per ASTM A563, and hardened washer per ASTM F436. The stud bolt shall have a minimum yield and ultimate tensile strength of 50 and 75 KSI, respectively. Nuts, bolts and washers shall be galvanized per Item 445, "Galvanizing." Adhesive type anchors shall have stud bolts installed with Type III epoxy per DMS-6100, "Epoxyes and Adhesives." Adhesive anchors may be loaded after adequate epoxy cure time per the manufacturer's recommendations. Top of bolt shall extend at least flush with top of the nut when installed. The anchor, when installed in 4000 psi normal-weight concrete with a 5 1/2" minimum embedment, shall have a minimum allowable tension and shear of 3900 and 3100 psi, respectively.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 8:21:30 AM

FILE: \\txdot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standards\Signing_Standards\smas1.dgn



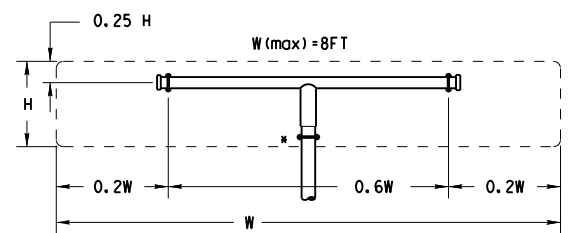
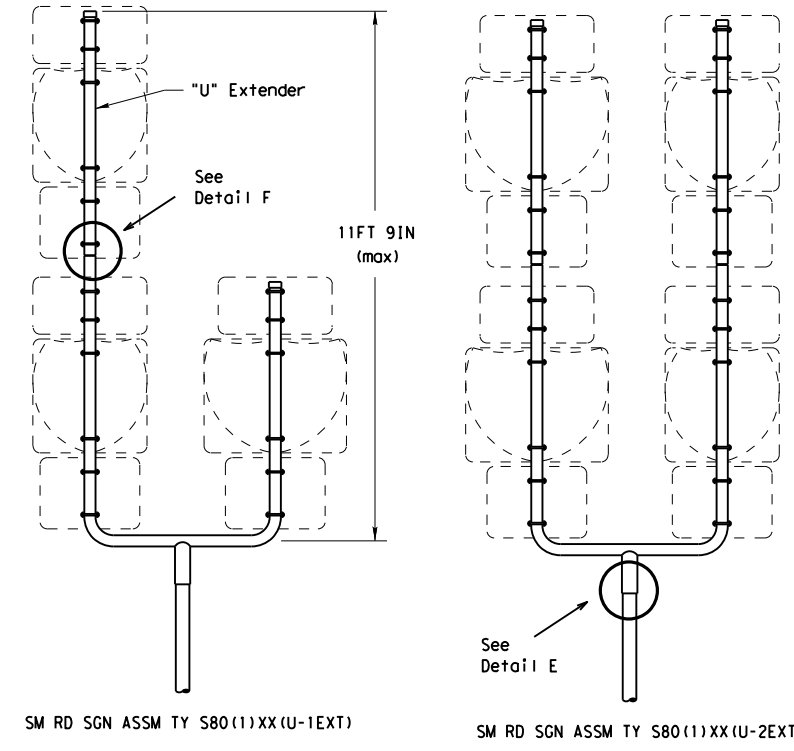
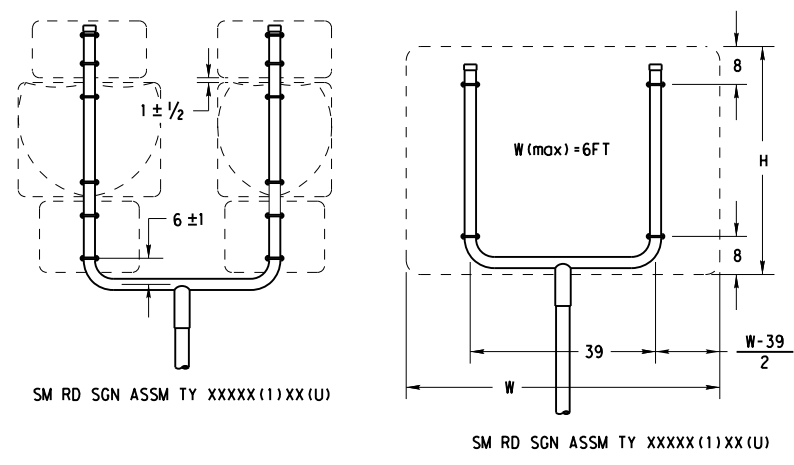
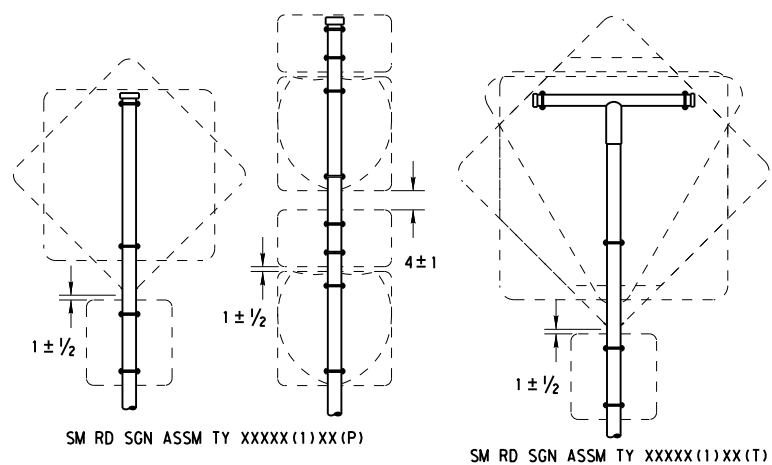
SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS TRIANGULAR SLIPBASE SYSTEM

SMD(SLIP-1)-08

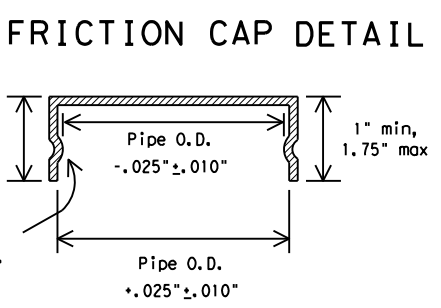
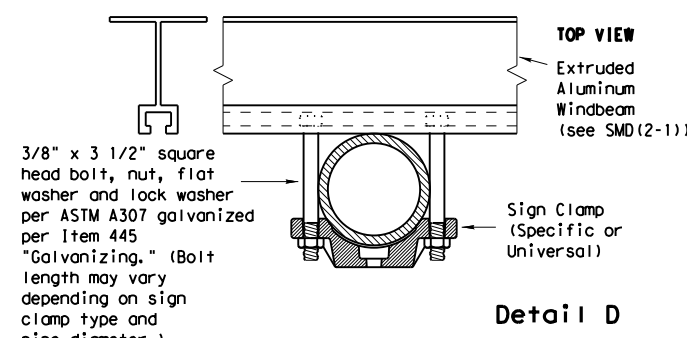
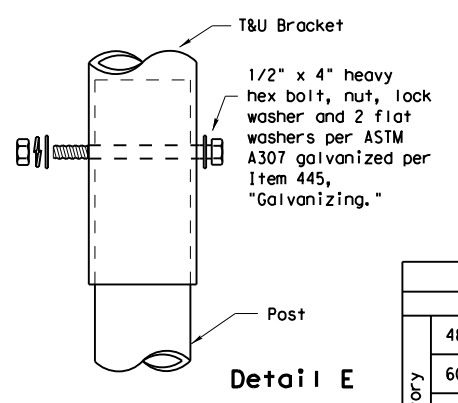
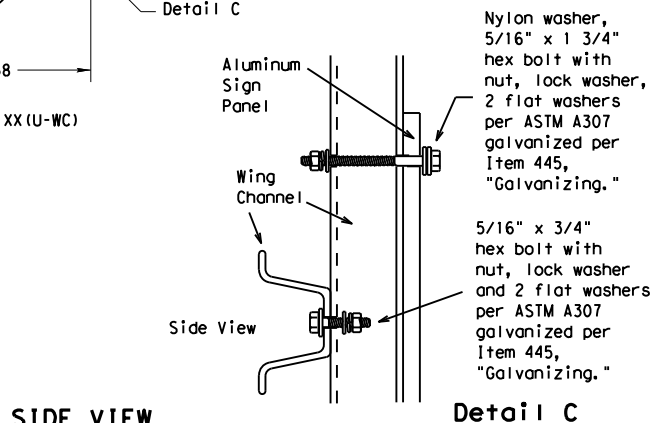
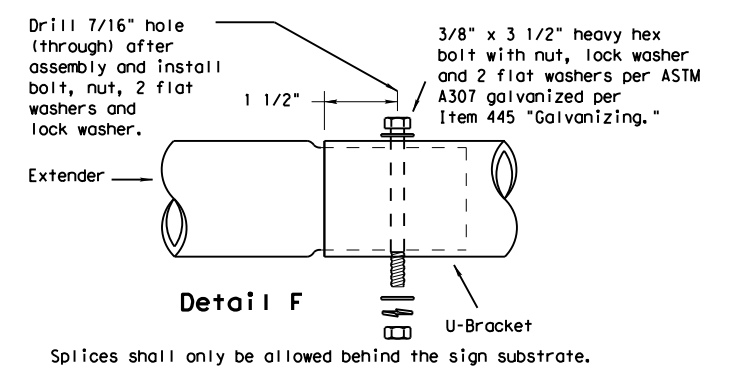
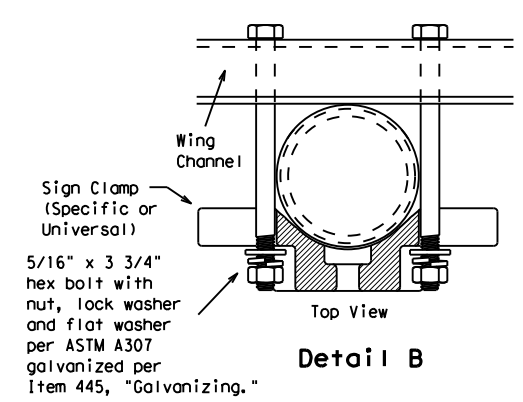
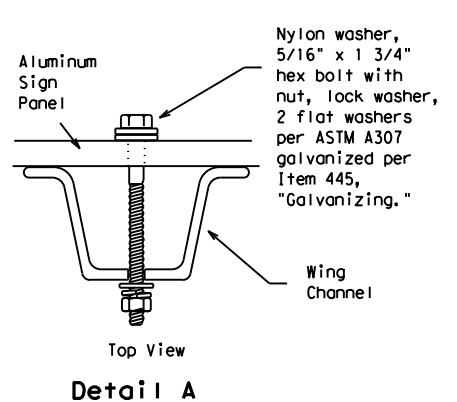
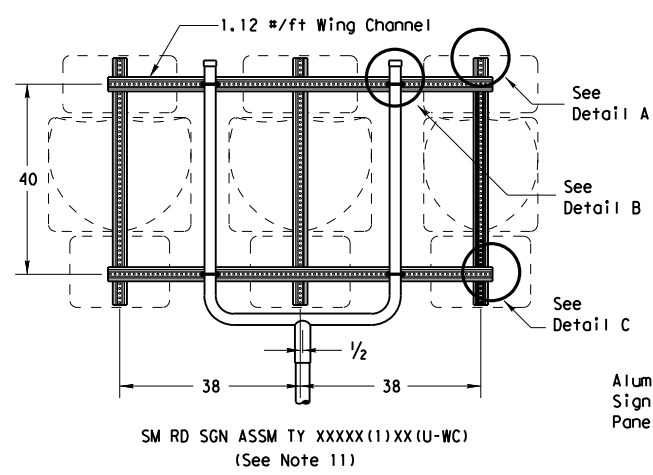
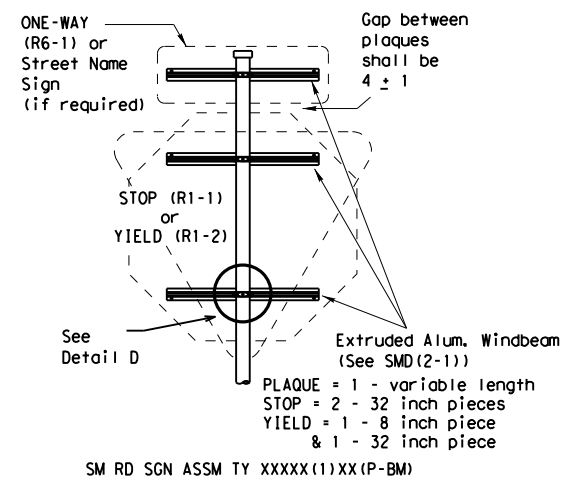
© TxDOT July 2002		DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
9-08	REVISIONS	CONT	SECT	JOB	HIGHWAY
		0482	01	036, etc.	SH 285
		DIST	COUNTY		SHEET NO.
		PHR	JIM HOGG		309

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 8:21:34 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\0482010364 - Design\Plan Set\14 - Sign\Plan Set\14 - Design\Standards\Signing_Standards\smds2.dgn



All dimensions are in english unless detailed otherwise.



Friction caps may be manufactured from hot rolled or cold rolled steel sheets. The minimum sheet metal thickness shall be 24 gauge for all cap sizes. The rim edges shall be reasonably straight and smooth. Caps shall be sized and formed in such a manner as to produce a drive-on friction fit and have no tendency to rock when seated on the pipe. The depth shall be sufficient to give positive protection against entrance of rainwater. They shall be free of sharp creases or indentations and show no evidence of metal fracture. Caps shall have an electrodeposited coating of zinc in accordance with the requirements of ASTM B633 Class FE/ZN 8.

GENERAL NOTES:

- SIGN SUPPORT # OF POSTS MAX. SIGN AREA

10 BWG	1	16 SF
10 BWG	2	32 SF
Sch 80	1	32 SF
Sch 80	2	64 SF
- The Engineer may require that a Schedule 80 post be used in place of a 10 BWG where a sign height is abnormally high due to a fill slope.
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.
- Aluminum sign blanks shall conform to Departmental Material Specifications DMS-7110 and shall have the following minimum thicknesses: 0.080 for signs less than 7.5 sq. ft., 0.100 for signs 7.5 to 15 sq. ft., and 0.125 for signs greater than 15 sq. ft.
- Signs that require specific supports due to reasons in addition to windloading are indicated on the "REQUIRED SUPPORT" table on this sheet.
- For horizontal rectangular signs fabricated from flat aluminum, T-brackets are used for signs 24 inches or less in height. U-brackets are used for signs of greater height.
- When two triangular slipbase supports are used to support a single sign, they shall not be "rigidly" connected to each other except through the sign panel. This will allow each support to act independently when impacted by an errant vehicle.
- Wing channel shall meet ASTM A 1011 SS Gr 50 and be galvanized per ASTM A 123.
- Excess pipe, wing channel, or windbeam shall be cut off so that it does not extend beyond the sign panel (i.e., excess support shall not be visible when the sign is viewed from the front.) Repair galvanized coating at cut support ends per Item 445, "Galvanizing."
- Additional route markers may be added vertically, provided the total sign area does not exceed the maximum allowable amount per Note 1.
- Additional sign clamp required on the "T-bracket" post for 24 inch height signs. Place the clamp 3 inches above bottom of sign when possible.
- Post open ends shall be fitted with Friction Caps.
- Sign blanks shall be the sizes and shapes shown on the plans.

		REQUIRED SUPPORT	
		SIGN DESCRIPTION	SUPPORT
Regulatory	48-inch STOP sign (R1-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)	
	60-inch YIELD sign (R1-2)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)	
	48x16-inch ONE-WAY sign (R6-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)	
Warning	36x48, 48x36, and 48x48-inch signs	TY 10BWG(1)XX(T)	
	48x60-inch signs	TY S80(1)XX(T)	
	48x48-inch signs (diamond or square)	TY 10BWG(1)XX(T)	
	48x60-inch signs	TY S80(1)XX(T)	
	48-inch Advance School X-ing sign (S1-1)	TY 10BWG(1)XX(T)	
	48-inch School X-ing sign (S2-1)	TY 10BWG(1)XX(T)	
	Large Arrow sign (W1-6 & W1-7)	TY 10BWG(1)XX(T)	

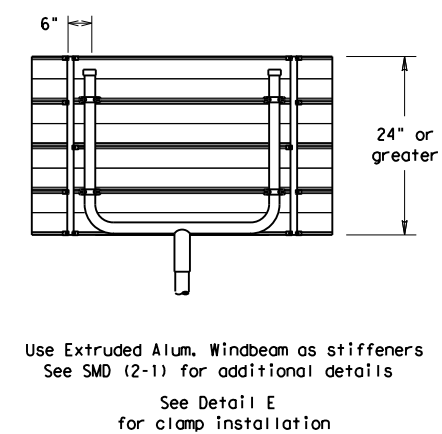
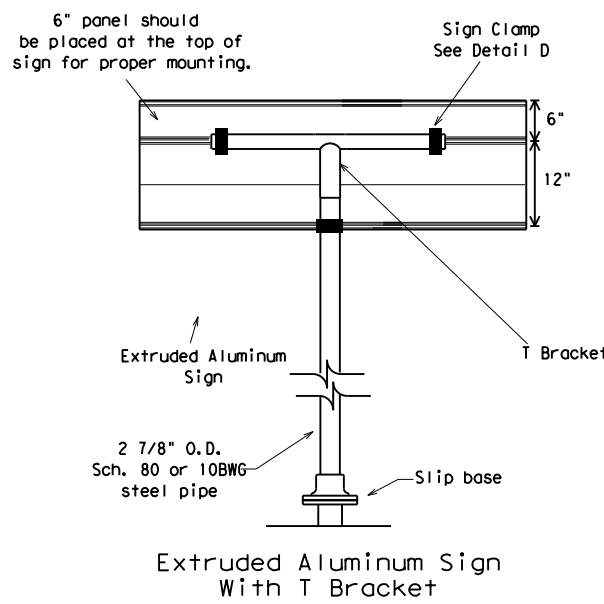
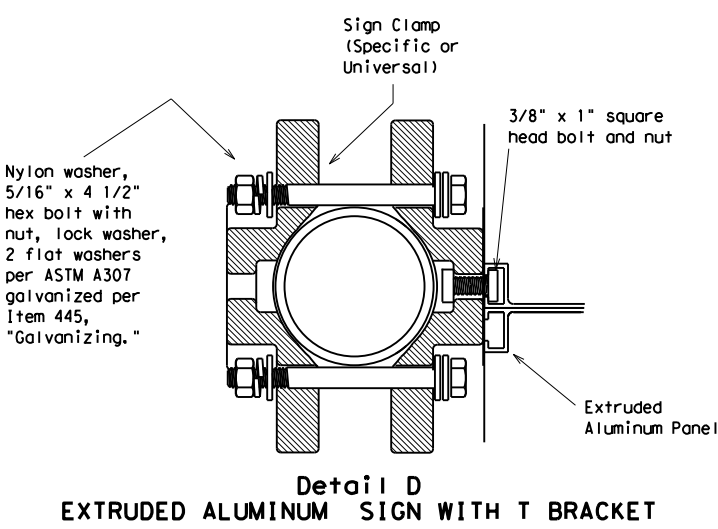
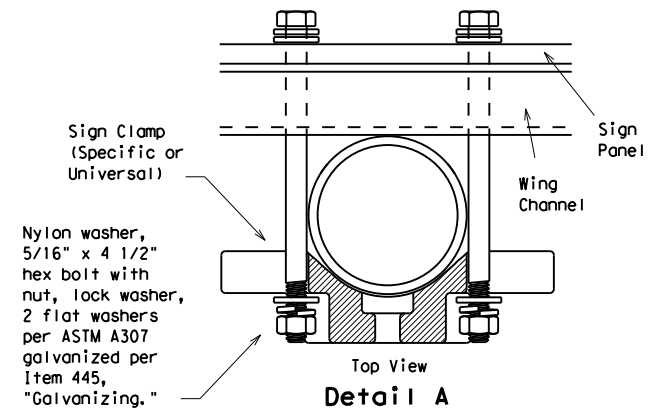
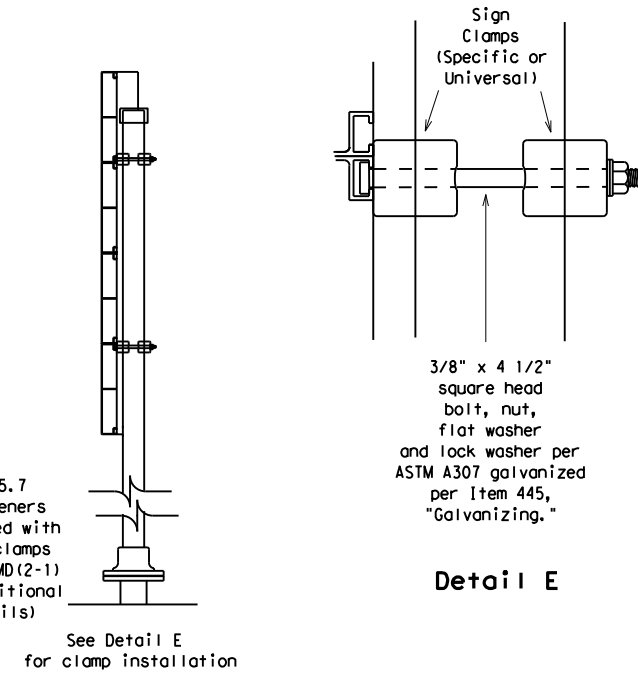
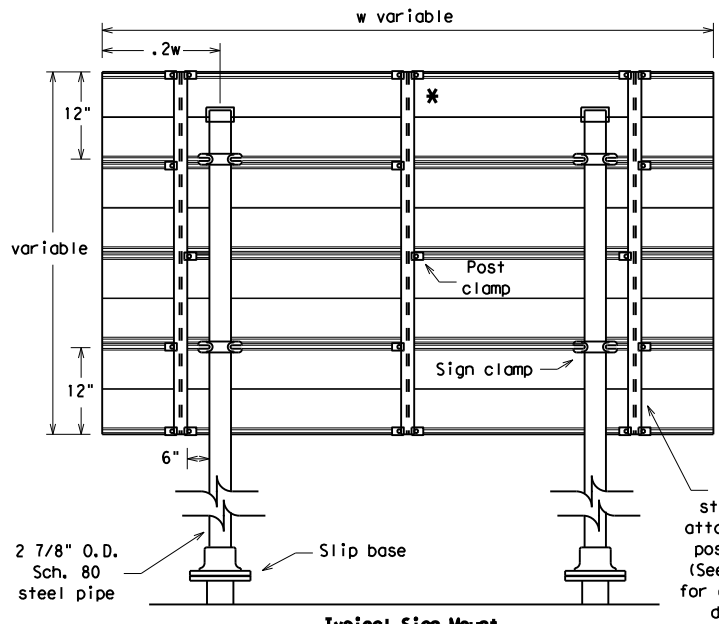
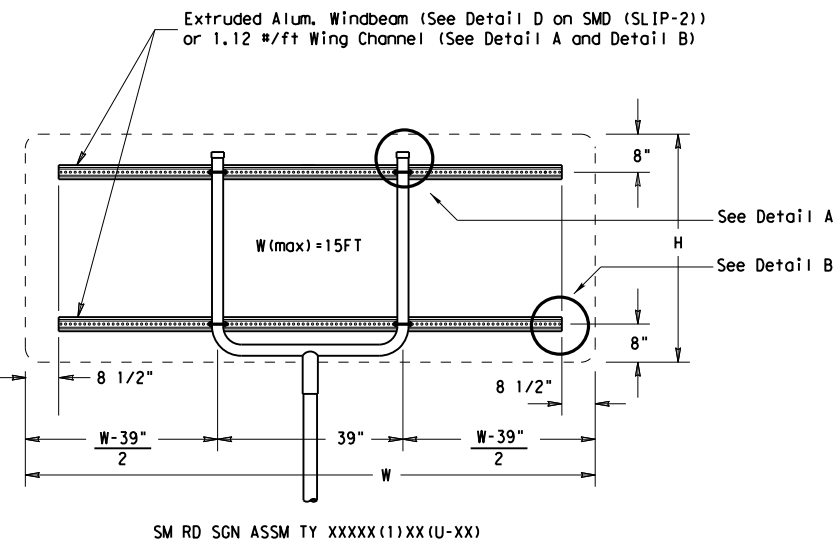
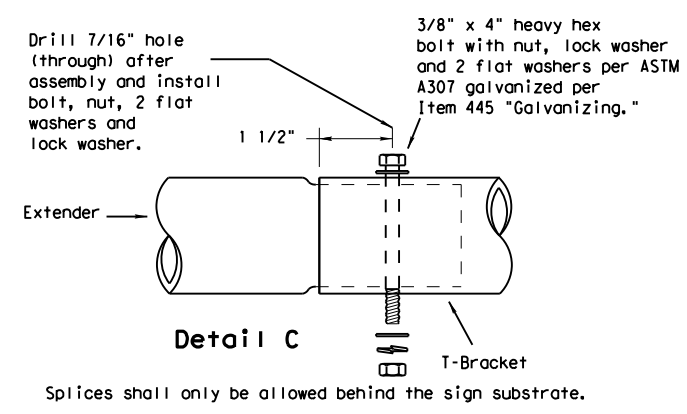
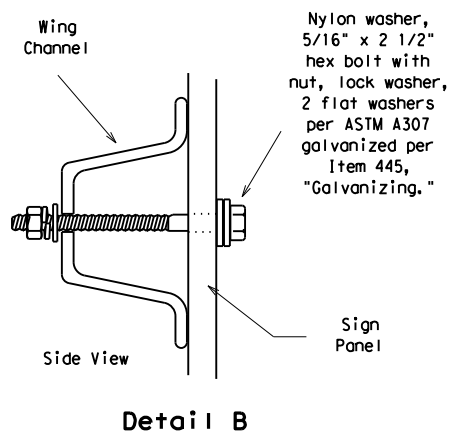
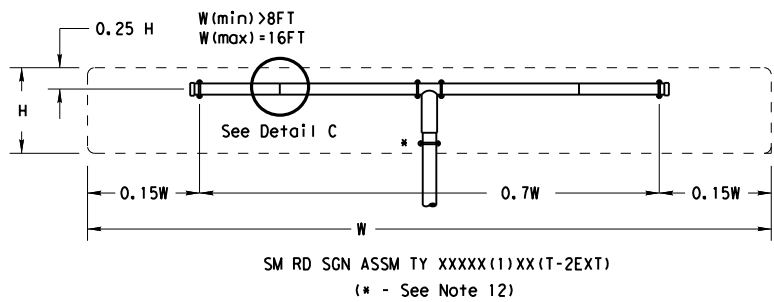


**SIGN MOUNTING DETAILS
 SMALL ROADSIDE SIGNS
 TRIANGULAR SLIPBASE SYSTEM
 SMD(SLIP-2)-08**

© TxDOT July 2002		DN: TXDOT	CK: TXDOT	DW: TXDOT	CK: TXDOT
9-08	REVISIONS	CON: 0482	SECT: 01	JOB: 036, etc.	HIGHWAY: SH 285
		DIST: PHR	COUNTY: JIM HOGG	SHEET NO.: 310	

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024 8:21:39 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\0482010364 - Design\Plan Set\14 - Sign\Plan Set\14 - SMD Standards\Signing_Standards\smds3.dgn



GENERAL NOTES:

- | SIGN SUPPORT | # OF POSTS | MAX. SIGN AREA |
|--------------|------------|----------------|
| 10 BWG | 1 | 16 SF |
| 10 BWG | 2 | 32 SF |
| Sch 80 | 1 | 32 SF |
| Sch 80 | 2 | 64 SF |
- The Engineer may require that a Schedule 80 post be used in place of a 10 BWG where a sign height is abnormally high due to a fill slope.
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.
- Aluminum sign blanks shall conform to Departmental Material Specifications DMS-7110 and shall have the following minimum thicknesses: 0.080 for signs less than 7.5 sq. ft., 0.100 for signs 7.5 to 15 sq. ft., and 0.125 for signs greater than 15 sq. ft.
- Signs that require specific supports due to reasons in addition to windloading are indicated on the "REQUIRED SUPPORT" table on this sheet.
- For horizontal rectangular signs fabricated from flat aluminum, T-brackets are used for signs 24 inches or less in height. U-brackets are used for signs of greater height.
- When two triangular slipbase supports are used to support a single sign, they shall not be "rigidly" connected to each other except through the sign panel. This will allow each support to act independently when impacted by an errant vehicle.
- Wing channel shall meet ASTM A 1011 SS Gr 50 and be galvanized per ASTM A 123.
- Excess pipe, wing channel, or windbeam shall be cut off so that it does not extend beyond the sign panel (i.e., excess support shall not be visible when the sign is viewed from the front.) Repair galvanized coating at cut support ends per Item 445, "Galvanizing."
- Sign blanks shall be the sizes and shapes shown on the plans.
- Additional sign clamp required on the "T-bracket" post for 24 inch high signs. Place the clamp 3 inches above bottom of sign when possible.
- Post open ends shall be fitted with Friction Caps.

REQUIRED SUPPORT		
	SIGN DESCRIPTION	SUPPORT
Regulatory	48-inch STOP sign (R1-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	60-inch YIELD sign (R1-2)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	48x16-inch ONE-WAY sign (R6-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	36x48, 48x36, and 48x48-inch signs	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
Warning	48x48-inch signs (diamond or square)	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
	48-inch Advance School X-ing sign (S1-1)	TY 10BWG(1)XX(T)
	48-inch School X-ing sign (S2-1)	TY 10BWG(1)XX(T)
	Large Arrow sign (W1-6 & W1-7)	TY 10BWG(1)XX(T)



**SIGN MOUNTING DETAILS
 SMALL ROADSIDE SIGNS
 TRIANGULAR SLIPBASE SYSTEM
 SMD(SLIP-3)-08**

© TxDOT July 2002		DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
9-08	REVISIONS	CONT	SECT	JOB	HIGHWAY
		0482	01	036, etc.	SH 285
		DIST	COUNTY		SHEET NO.
		PHR	JIM HOGG		311

DATE: 2/12/2024 8:21:49 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\0422\0422.dgn
 PROJECT: 0422 - PHR\Design Projects\0422\0422.dgn
 DRAWING: 0422-01-036-etc.
 SHEET: 285
 TITLE: REFLECTOR UNIT SIZES FOR DELINEATORS AND OBJECT MARKERS

REFLECTOR UNIT SIZES FOR DELINEATORS AND OBJECT MARKERS				DELINEATORS				D & OM DESCRIPTIVE CODES	
DEVICE	SIZE 1	SIZE 2	SIZE 3	SIZE 4	DEVICE	SINGLE	DOUBLE	INSTL DEL ASSM (D-XX)SZ X (XXXX)XXX(XX)	
								NUMBER OF REFLECTORS S = Single D = Double COLOR OF REFLECTORS W = White Y = Yellow R = Red REFLECTOR UNIT SIZE 1 or 2 TYPE OF POST OR DELINEATOR WC = Wing Channel Post YFLX = Yellow Flexible Post WFLX = White Flexible Post BRF = Barrier Reflector TYPE OF MOUNT GND = Embedded (drivable or set in concrete) CTB = Concrete Barrier Mount GF1 or GF2 = Guard Fence Attachment SRF = Surface Mount DIRECTION If Required BI = Bi-Directional BR = Bi-Directional with red on back	
SHEETING	Yellow, White or Red Type B or C reflective sheeting				SHEETING	Yellow, White or Red Type B or C Reflective Sheeting			
NOTE	1. Size 1 and 4 - Direct applied reflective sheeting for use on flexible post (fix). 2. Size 2 and 3 - For use on wing channel (wc) post only. Use approved metal, plastic or fiberglass backplate with 17/64" mounting holes.				POST TYPE	WC	YFLX, WFLX	WC	YFLX, WFLX
					MOUNT TYPE	GND	GND, SRF	GND	GND, SRF

OBJECT MARKERS								D & OM DESCRIPTIVE CODES		
DEVICE	Type 1 (OM-1)	Type 2 (OM-2)			Type 3 (OM-3)			Type 4 (OM-4)	INSTL OM ASSM (OM-XX) (XXXX)XXX(XX)	
		OM-1	OM-2X	OM-2Y	OM-2Z	OM-3L	OM-3R	OM-3C	OM-4	TYPE OF OBJECT MARKER 1, 2, 3, or 4 NUMBER OF REFLECTORS OR DIRECTION X = 3-Size 2 reflector unit (Type 2 only) Y = 1-Size 3 reflector unit (Type 2 only) Z = 3-Size 1 or 1-Size 4 reflector unit(s) (Type 2 only) L = Left Side (Type 3 Object Marker only) R = Right Side (Type 3 Object Marker only) C = Center (Type 3 Object Marker only) TYPE OF POST WC = Wing Channel Post WFLX = White Flexible Post TWT = Thin Walled Tubing TYPE OF MOUNT GND = Embedded (drivable) SRF = Surface Mount WAS = Wedge Anchor Steel WAP = Wedge Anchor Plastic DIRECTION If Required BI = Bi-Directional
SHEETING	Yellow-Type B _{FL} or C _{FL} Sheeting	Yellow - Type B or C Sheeting			Alternating acrylic black and retroreflective yellow - Type B _{FL} or C _{FL} Sheeting			Red -Type B _{FL} or C _{FL} Sheeting	DEPARTMENTAL MATERIAL SPECIFICATIONS FLEXIBLE DELINEATOR & OBJECT MARKER POSTS (EMBEDDED & SURFACE MOUNT TYPES) DMS-4400 SIGN FACE MATERIALS DMS-8300 DELINEATORS, OBJECT MARKERS AND BARRIER REFLECTORS DMS-8600	
POST TYPE	TWT	WC	WC	WFLX	TWT			TWT		
MOUNT TYPE	WAS, WAP	GND	GND	GND, SRF	WAS, WAP			WAS, WAP		

BARRIER REFLECTORS (BRF)			CHEVRONS				ONE DIRECTION LARGE ARROW		NOTE: Delineator and object marker substrates and sign substrates shall be 0.080" Aluminum sign blank to conform to ASTM B-209 Alloy 6061-T6 or approved alternative.	
DEVICE	GF1	GF2	CTB	 W1-8				 W1-6		
	1. Barrier reflectors shall meet the requirements of DMS 8600. 2. Approved Barrier Reflectors are listed on the "Barrier Reflectors" Material Producer List at: www.txdot.gov.		SIZE (W x L)	18" x 24" (Conventional)	24" x 30" (Conventional Oversize)	30" x 36" (Expressway)	36" x 48" (Freeway)	SIZE (W x L)	48" x 24" (Conventional)	60" x 30" (Expressway & Freeway)
			MOUNTING HEIGHT	4'-0" or 7'-0"		7'-0" Only		MOUNTING HEIGHT	7'-0"	
NOTE	1. Reflective sheeting shall have a minimum dimension of 3 inches and minimum surface area of 9 square inches.		NOTE	1. CHEVRON (W1-8) signs and ONE DIRECTION LARGE ARROW (W1-6) Signs shall be installed per Sign Mounting Details (SMD) Standard Sheets and paid under Item 644 (Small Roadside Sign Assemblies). 2. When there is a need to increase conspicuity, the Texas version of the ONE DIRECTION LARGE ARROW sign (W1-9T) may be used instead of the ONE DIRECTION LARGE ARROW (W1-6).						
SHEETING	Yellow, White, Red									

Texas Department of Transportation
 Traffic Safety Division Standard

DELINEATOR & OBJECT MARKER MATERIAL DESCRIPTION

D & OM(1)-20

FILE: dom1-20.dgn	DN: TXDOT	CK: TXDOT	OW: TXDOT	CR: TXDOT
© TXDOT August 2004	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
10-09 3-15	DIST	COUNTY	SHEET NO.	
4-10 7-20	PHR	JIM HOGG	312	

DATE: 2/12/2024 8:21:54 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\2024\20240220\20240220.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information into a digital format.

POST TYPE AND SUPPORT FOUNDATION DETAILS

TYPE OF BARRIER MOUNTS

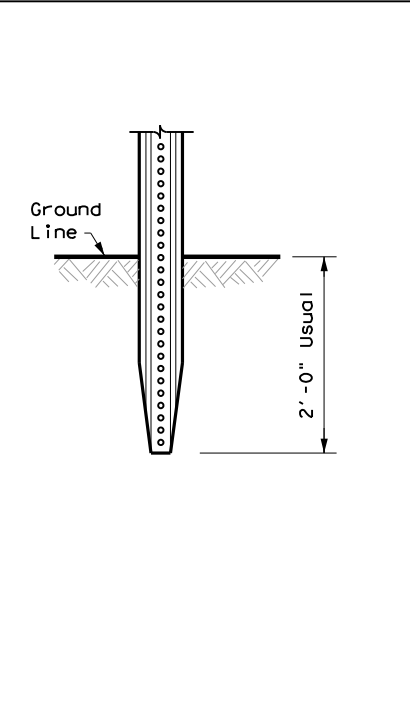
WING CHANNEL (WC)

FLEXIBLE POSTS (YFLX, WFLX)

WEDGE ANCHOR SYSTEMS

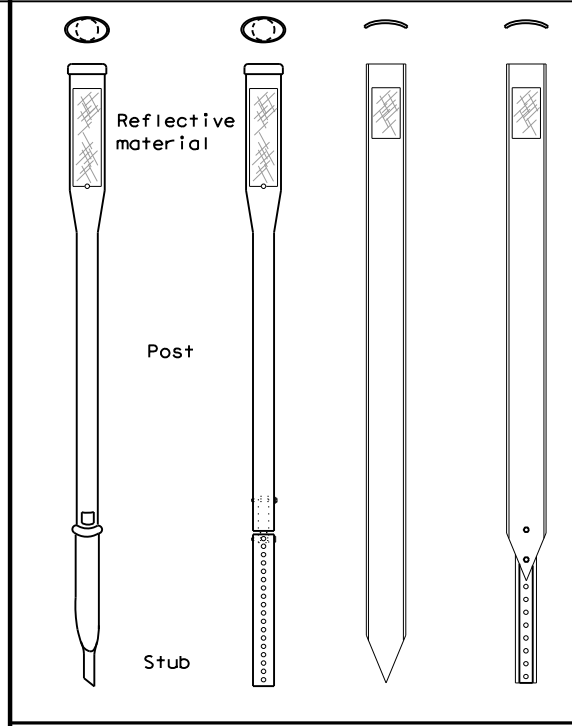
GUARD FENCE ATTACHMENT

GND



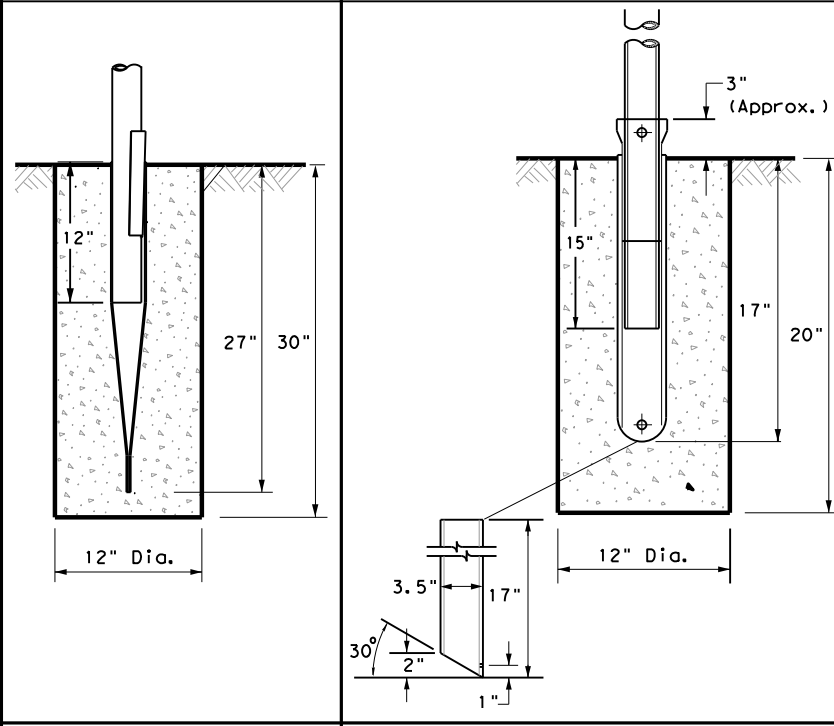
GND

SRF



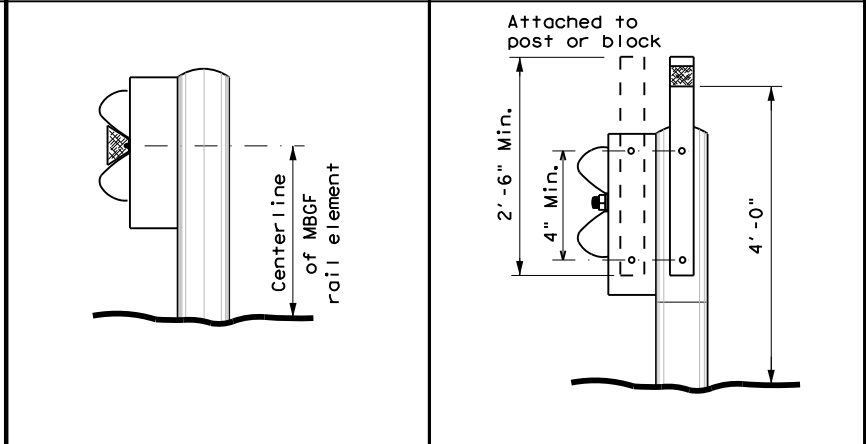
WAS

WAP



GF 1

GF 2



NOTES

1. Embedded Wing Channel (WC) post option may be used for Type 2 Object Markers and Delineators only.
2. 1.12 lbs/ft steel per ASTM A 1011 SS Gr. 50, or ASTM A499.

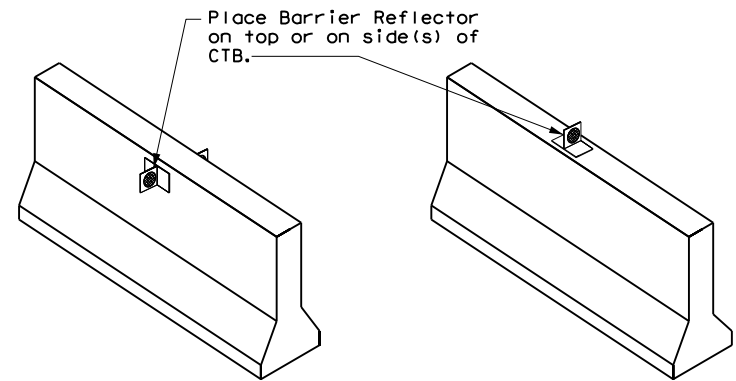
NOTES

1. See "Flexible Delineator and Object Marker Posts" Material Producer List for approved devices.
2. Install per manufacturer's recommendations.
3. Post length may vary to meet field conditions.
4. When using yellow delineators with flexible posts to separate opposing direction of travel, such as centerline or median use, the flexible posts shall be yellow.

NOTE

1. Install per manufacturer's recommendations.

CONCRETE TRAFFIC BARRIER (CTB)



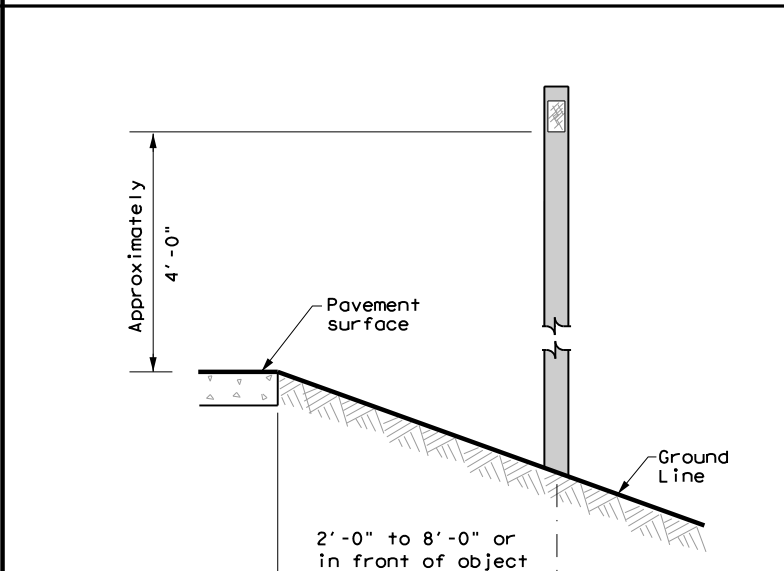
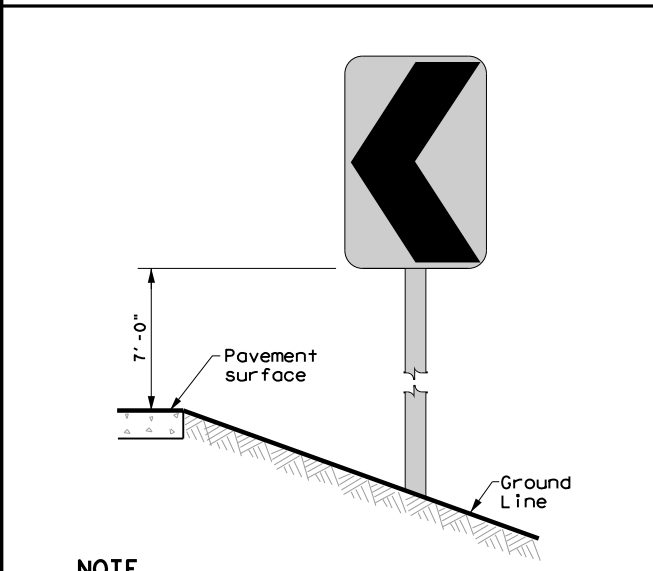
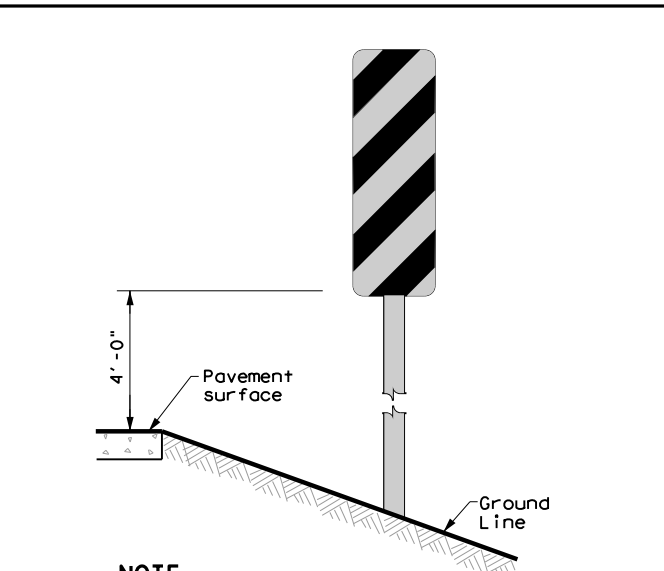
GENERAL NOTES

1. Place delineators on a section of roadway at a consistent distance from the edge of pavement.
2. Where a restriction prevents consistent placement from the pavement edge, place the affected object markers in line with the innermost edge of the obstruction.
3. When Type 2 object markers and delineators are more than 8'-0" from the edge of the pavement, it may not be possible to maintain a height of approximately 4'-0". If this is the case, place the object marker or delineator as close to the desired height as possible.
4. Install all delineators, object markers and barrier reflectors in accordance with the manufacturer's recommendation.
5. Barrier reflectors should be installed a minimum of 18 inches above the edge of the pavement surface.
6. Diagonal stripes on Type 3 object markers shall slope down toward the intended travel lane.

TYPES 1,3, AND 4 OBJECT MARKERS AND CHEVRONS

CHEVRONS AND ONE DIRECTION LARGE ARROW SIGN

DELINEATORS AND TYPE 2 OBJECT MARKERS



NOTE

Mounting at 4 feet to the bottom of the chevron is permitted for chevrons that will not exceed a height of 6'-6" to the top of the chevron (sizes 24" x 30" and smaller)

NOTE

Chevrons 30" x 36" and larger shall be mounted at a height of 7' to the bottom of the chevron. Chevron sign and ONE DIRECTION LARGE ARROW sign (W1-9T) shall be installed per SMD standard sheets and paid under item 644.

See general notes 1, 2 and 3.



DELINEATOR & OBJECT MARKER INSTALLATION

D & OM(2) - 20

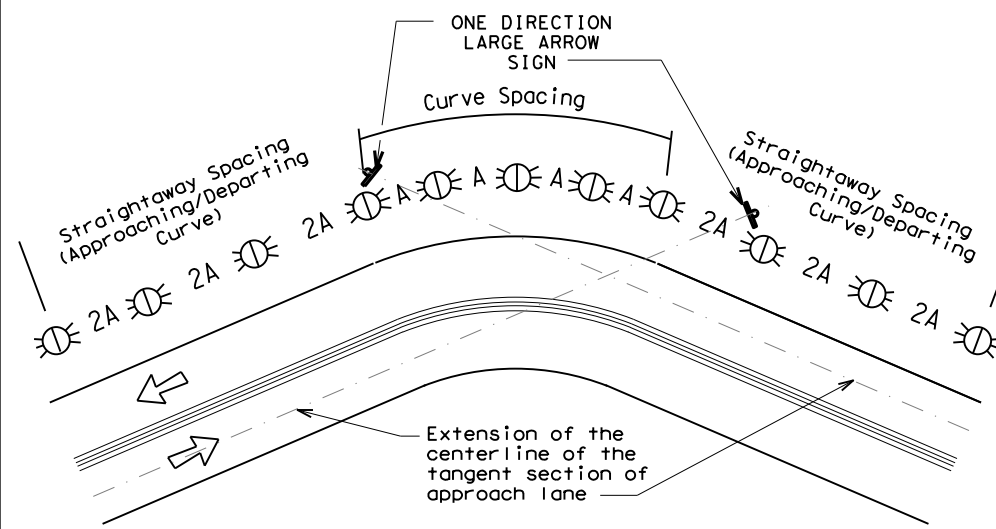
FILE: dom2-20.dgn	DN: TXDOT	CK: TXDOT	DW: TXDOT	CK: TXDOT
© TXDOT August 2004	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
10-09 3-15	DIST	COUNTY	SHEET NO.	
4-10 7-20	PHR	JIM HOGG		313

DATE: 2/12/2024 8:21:58 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\036\036-036\036-036.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein.

MINIMUM WARNING DEVICES AT CURVES WITH ADVISORY SPEEDS

Amount by which Advisory Speed is less than Posted Speed	Curve Advisory Speed	
	Turn (30 MPH or less)	Curve (35 MPH or more)
5 MPH & 10 MPH	• RPMs	• RPMs
15 MPH & 20 MPH	• RPMs and One Direction Large Arrow sign	• RPMs and Chevrons; or • RPMs and One Direction Large Arrow sign where geometric conditions or roadside obstacles prevent the installation of chevrons.
25 MPH & more	• RPMs and Chevrons; or • RPMs and One Direction Large Arrow sign where geometric conditions or roadside obstacles prevent the installation of chevrons	• RPMs and Chevrons

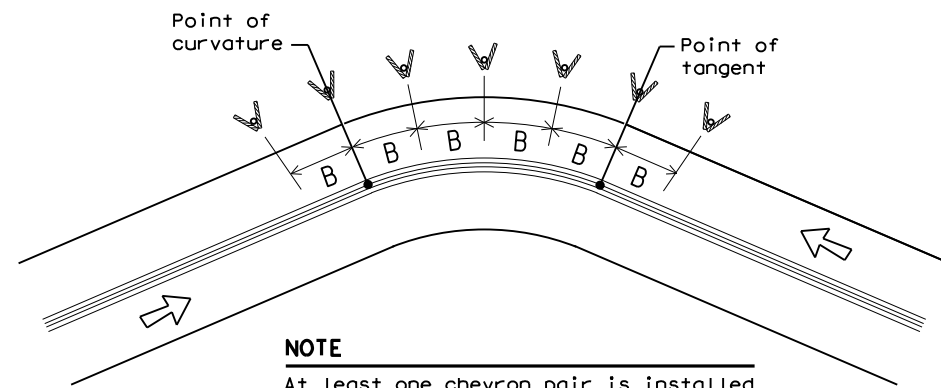
SUGGESTED SPACING FOR DELINEATORS ON HORIZONTAL CURVES



NOTE

ONE DIRECTION LARGE ARROW (W1-6) sign should be located at approximately and perpendicular to the extension of the centerline of the tangent section of approach lane.

SUGGESTED SPACING FOR CHEVRONS ON HORIZONTAL CURVES



NOTE

At least one chevron pair is installed beyond the point of tangent in tangent section.

DELINEATOR AND CHEVRON SPACING

WHEN DEGREE OF CURVE OR RADIUS IS KNOWN				
Degree of Curve	FEET			
	Radius of Curve	Spacing in Curve	Spacing in Straightaway	Chevron Spacing in Curve
		A	2A	B
1	5730	225	450	—
2	2865	160	320	—
3	1910	130	260	200
4	1433	110	220	160
5	1146	100	200	160
6	955	90	180	160
7	819	85	170	160
8	716	75	150	160
9	637	75	150	120
10	573	70	140	120
11	521	65	130	120
12	478	60	120	120
13	441	60	120	120
14	409	55	110	80
15	382	55	110	80
16	358	55	110	80
19	302	50	100	80
23	249	40	80	80
29	198	35	70	40
38	151	30	60	40
57	101	20	40	40

Curve delineator approach and departure spacing should include 3 delineators spaced at 2A. This spacing should be used during design preparation or when the degree of curve is known.

DELINEATOR AND CHEVRON SPACING

WHEN DEGREE OF CURVE OR RADIUS IS NOT KNOWN			
Advisory Speed (MPH)	Spacing in Curve	Spacing in Straightaway	Chevron Spacing in Curve
	A	2xA	B
65	130	260	200
60	110	220	160
55	100	200	160
50	85	170	160
45	75	150	120
40	70	140	120
35	60	120	120
30	55	110	80
25	50	100	80
20	40	80	80
15	35	70	40

If the degree of curve is not known, delineator spacing may be determined based on the Advisory Speed of the curve. Use the delineator curve spacing for each Advisory Speed (MPH).

DELINEATOR AND OBJECT MARKER APPLICATION AND SPACING

CONDITION	REQUIRED TREATMENT	MINIMUM SPACING
Frwy./Exp. Tangent	RPMs	See PM-series and FPM-series standard sheets
Frwy./Exp. Curve	Single delineators on right side	See delineator spacing table
Frwy/Exp. Ramp	Single delineators on at least one side of ramp (should be on outside of curves) (see Detail 3 on D&OM(4))	100 feet on ramp tangents Use delineator spacing table for ramp curves ("straightway spacing" does not apply to ramp curves)
Acceleration/Deceleration Lane	Double delineators (see Detail 3 on D&OM(4))	100 feet (See Detail 3 on D & OM (4))
Truck Escape Ramp	Single red delineators on both sides	50 feet
Bridge Rail (steel or concrete) and Metal Beam Guard Fence	Bi-Directional Delineators when undivided with one lane each direction Single Delineators when multiple lanes each direction	Equal spacing (100' max) but not less than 3 delineators
Concrete Traffic Barrier (CTB) or Steel Traffic Barrier	Barrier reflectors matching the color of the edge line	Equal spacing 100' max
Cable Barrier	Reflectors matching the color of the edge line	Every 5th cable barrier post (up to 100' max)
Guard Rail Terminus/Impact Head	Divided highway - Object marker on approach end Undivided 2-lane highways - Object marker on approach and departure end	Requires reflective sheeting provided by manufacturer per D & OM (VIA) or a Type 3 Object Marker (OM-3) in front of the terminal end See D & OM (5) and D & OM (6)
Bridges with no Approach Rail	Type 3 Object Marker (OM-3) at end of rail and 3 single delineators approaching rail	See D & OM(5)
Reduced Width Approaches to Bridge Rail	Type 2 and Type 3 Object Markers (OM-3) and 3 single delineators approaching bridge	Requires reflective sheeting provided by manufacturer per D & OM (VIA) or a Type 3 Object Marker (OM-3) in front of the terminal end See D & OM (5)
Culverts without MBGF	Type 2 Object Markers	See Detail 2 on D & OM(4)
Crossovers	Double yellow delineators and RPMs	See Detail 1 on D & OM (4)
Pavement Narrowing (lane merge) on Freeways/Expressway	Single delineators adjacent to affected lane for full length of transition	100 feet

NOTES

- Unless indicated otherwise, the delineator or barrier reflector color shall conform to the color of the pavement edge line on the side of the road where the delineators or barrier reflectors are placed.
- Barrier reflectors may be used to replace required delineators.
- Single red delineators may be mounted on the back side of delineator posts for wrong way driver applications

LEGEND	
	Bi-directional Delineator
	Delineator
	Sign

Texas Department of Transportation
Traffic Safety Division Standard

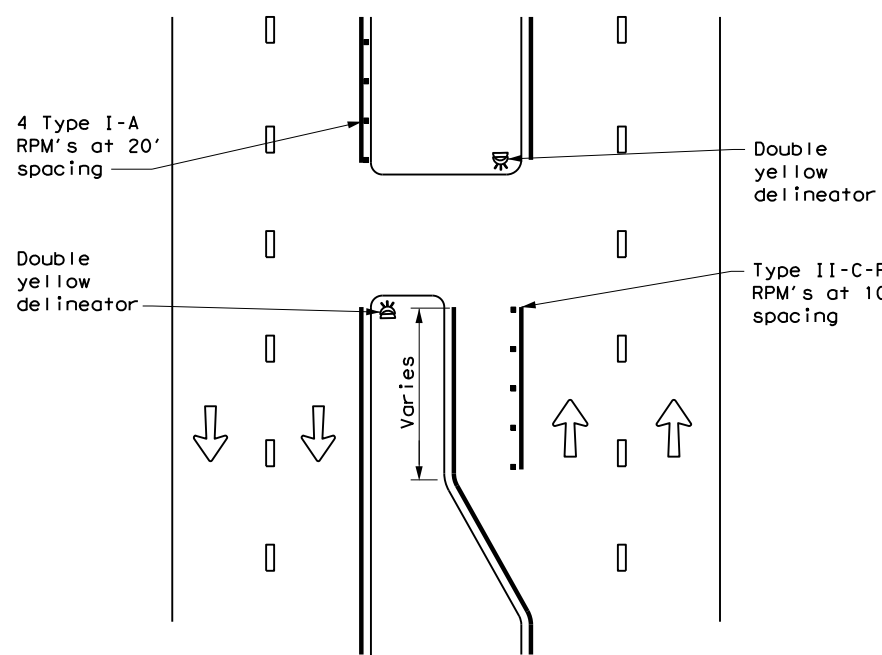
DELINEATOR & OBJECT MARKER PLACEMENT DETAILS

D & OM(3)-20

FILE: dom3-20.dgn	DW: TXDOT	CK: TXDOT	OW: TXDOT	CR: TXDOT
© TXDOT August 2004	CONT	SECT	JOB	HIGHWAY
REVISIONS		0482 01	036, etc.	SH 285
3-15 8-15	DIST	COUNTY	SHEET NO.	
8-15 7-20	PHR	JIM HOGG	314	

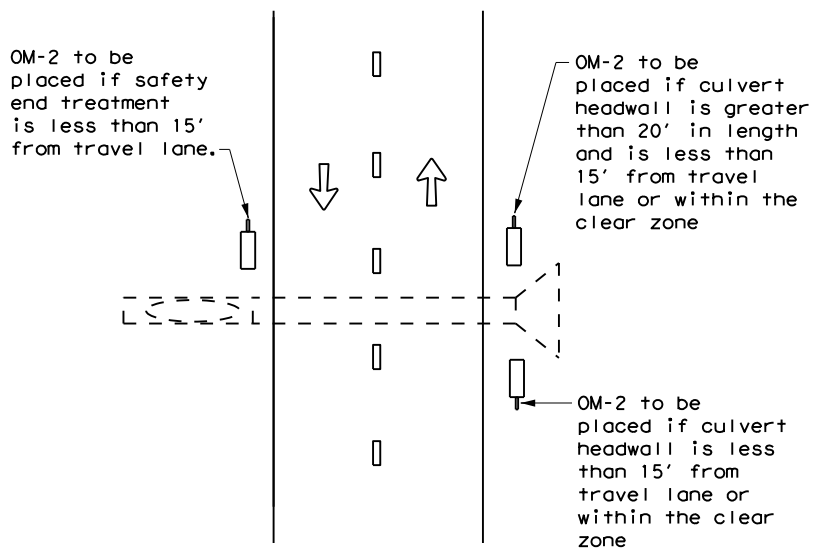
DATE: 2/12/2024 8:22:02 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\0482\01\036\etc\0482-20.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information into any other format or for the use of any information in any other format.

CROSSOVERS



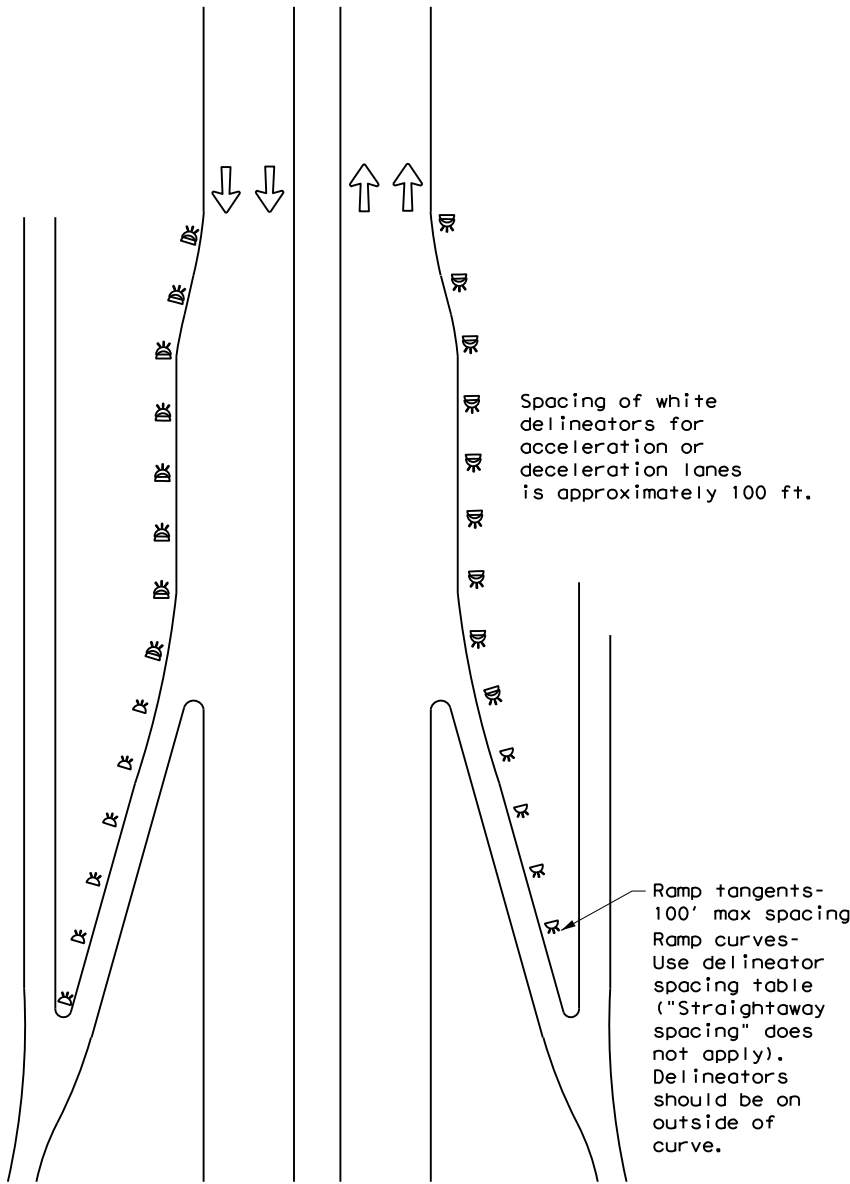
DETAIL 1

FOR CULVERTS WITHOUT MBGF



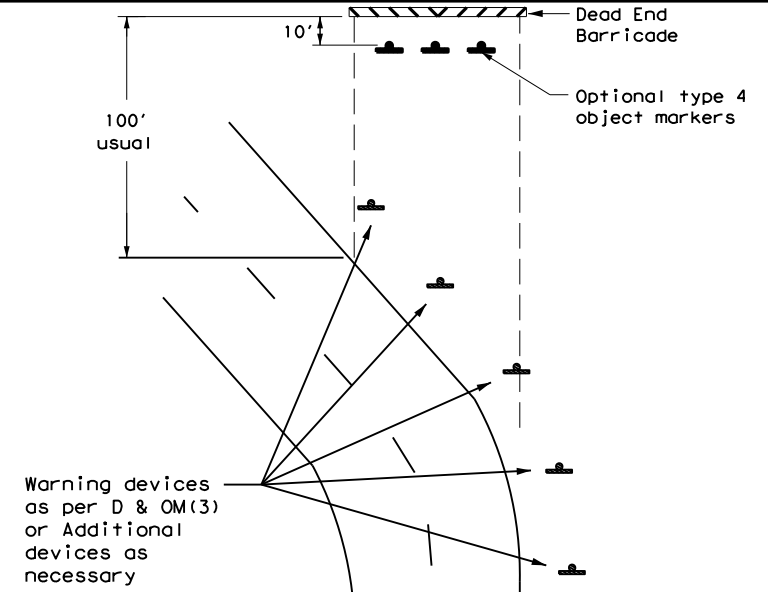
DETAIL 2

FREEWAY DELINEATION FOR RAMPS AND ACCELERATION/DECELERATION LANES



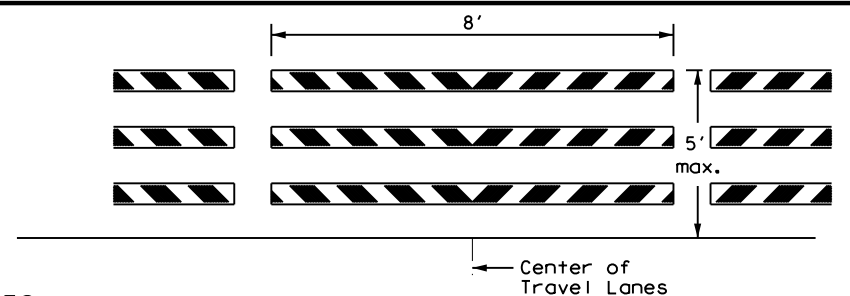
DETAIL 3

TYPICAL APPLICATION OF DEAD END BARRICADE



DETAIL 4

TYPICAL DEAD END BARRICADE INSTALLATION



NOTES

- Barricade striping shall be red and white reflective sheeting for all permanent road closures.
- Barricade striping is red and white sloping toward the center of the roadway.
- Type 3 Barricade Supports should be anchored to soil or pavement as described in compliant Work Zone Traffic Control Devices List, section D.2.f and D.2.g.

DETAIL 5

LEGEND	
	Bidirectional Delineator
	Delineator
	OM-3
	Barricade
	Sign
	OM-2
	Double Delineator



DELINEATOR & OBJECT MARKER PLACEMENT DETAILS

D & OM(4) -20

FILE: dom4-20.dgn	DN: TXDOT	CK: TXDOT	OW: TXDOT	CR: TXDOT
© TXDOT August 2004	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
3-15	DIST	COUNTY	SHEET NO.	
7-20	PHR	JIM HOGG	315	

DATE: 2/29/2024 9:38:03 AM
FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PVMRK COVER SHEET.dgn

PAVEMENT MARKINGS COVER SHEET

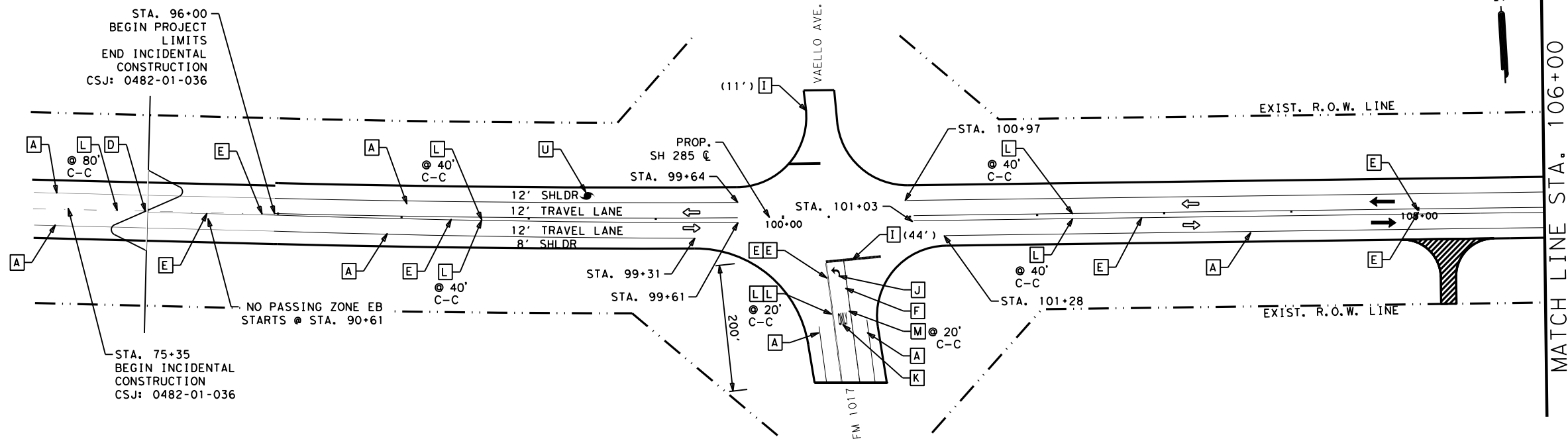
Pharr District Central Design



**SH 285
PAVEMENT MARKINGS
COVER SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		316

DATE: 2/29/2024 9:38:11 AM
 FILE: \\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PMI.dgn



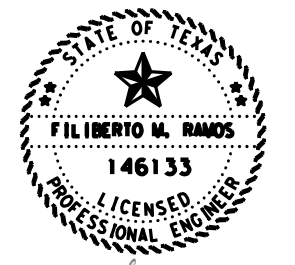
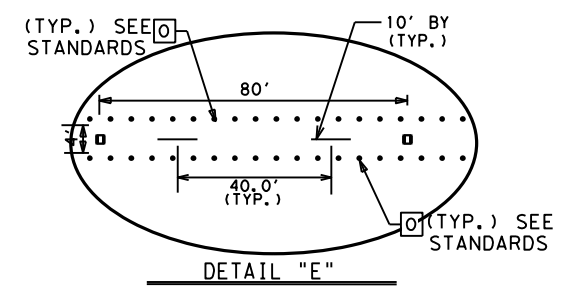
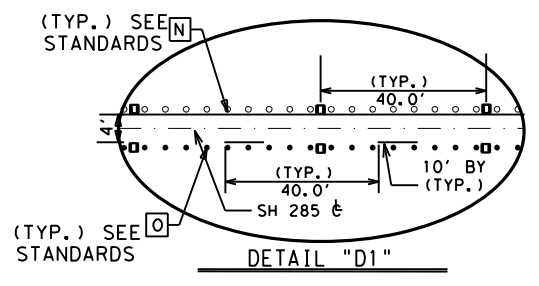
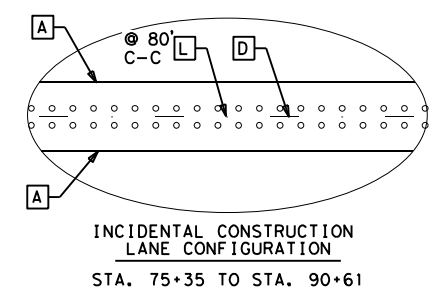
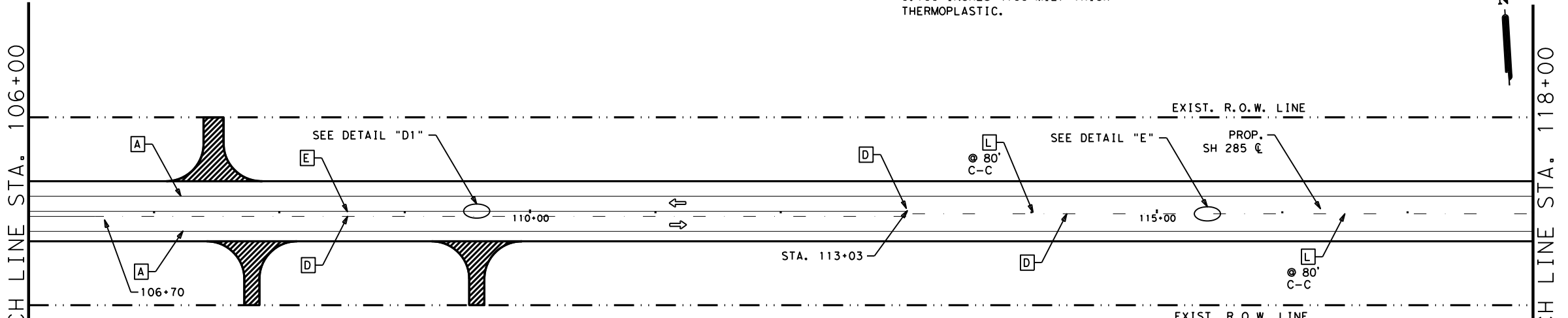
LEGEND:

- A REF PROF PAV MRK TY I(W)6"SLD(100MIL)
- B RE PM W/RET REQ TY I(W)6"BRK(100MIL)
- C RE PM W/RET REQ TY I(W)6"DOT(100MIL)
- D RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
- E RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
- F REFL PAV MRK TY I(W)8"SLD(100MIL)
- G REFL PAV MRK TY I(W)12"SLD(100MIL)
- H REFL PAV MRK TY I(Y)12"SLD(100MIL)
- I REFL PAV MRK TY I(W)24"SLD(100MIL)
- J PREFAB PAV MRK TY C (W) (ARROW)
- K PREFAB PAV MRK TY C (W) (WORD)
- L REFL PAV MRKR TY II-A-A
- M REFL PAV MRKR TY I-C
- N TRAFFIC BUTTON TY Y
- O TRAFFIC BUTTON TY B
- P PAV SURF PREP FOR MRK (6")
- Q MULTIPOLYMER PAV MRK (W) (6") (SLD)
- R MULTIPOLYMER PAV MRK (W) (6") (BRK)
- S MULTIPOLYMER PAV MRK (Y) (6") (SLD)
- T PREFAB PAV MRK TY C (W) (LNDP ARROW)
- U PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
- V REFL PAV MRK TY I(W)6"LNDR(100MIL)

EOP - EDGE OF PAVEMENT
 ← - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

666	666	666	666	666	666	666	666	666	668	668	672	672	672	672	678	6038	6038	6038	668	668	666
6343	6306	6018	6318	6321	6036	6042	6141	6048	6077	6085	6009	6007	6017	6018	6002	6004	6005	6017	6083	6116	6021
(LF)	(LF)	(LF)	(LF)	(LF)	(LF)	(LF)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(LF)	(LF)	(LF)	(LF)	(EA)	(EA)	(LF)
8298	-	-	845	2319	98	-	-	55	1	1	118	5	465	1186	-	-	-	-	-	1	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



[Signature]
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

SH 285 PAVEMENT MARKING LAYOUT

SCALE: 1"=100' SHEET 1 OF 22

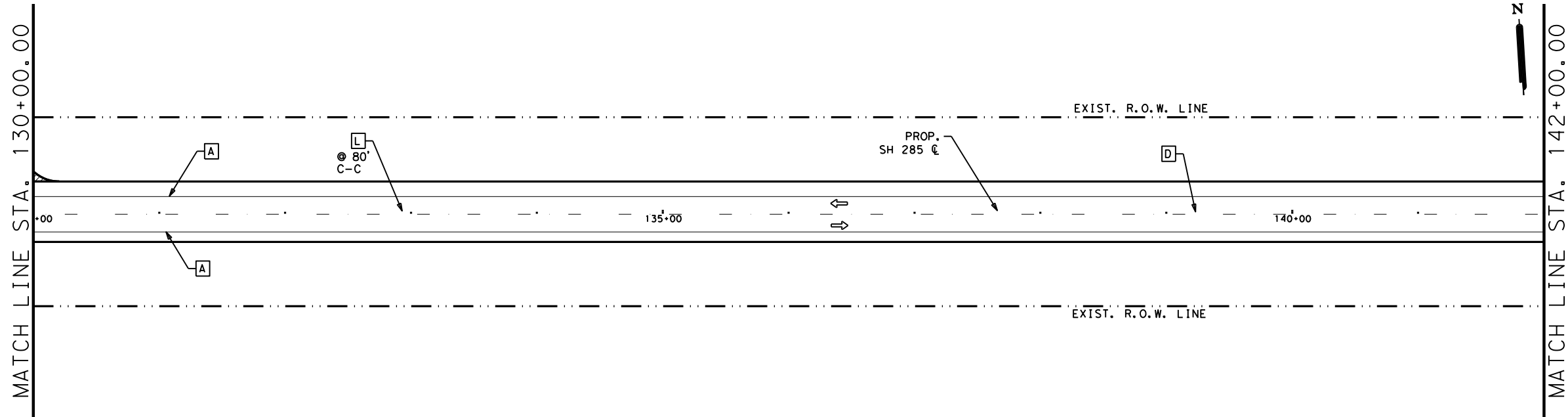
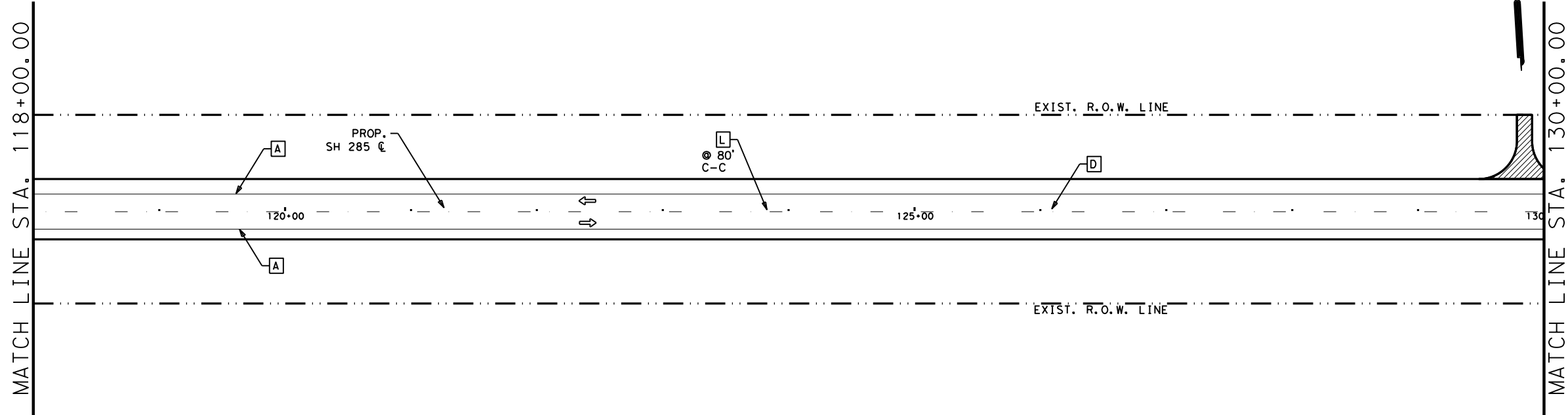
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	317

DATE: 2/29/2024 9:38:20 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS.OP2\SH 285 PM2.dgn

LEGEND:

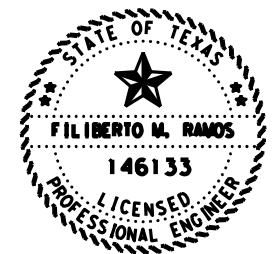
A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	600	-	-	-	-	-	-	-	30	-	-	960	-	-	-	-	-	-	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 2 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		318

DATE: 2/29/2024 9:38:30 AM
 FILE: P:\txdot\project\wisonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS.OP2\SH 285 PM3.dgn

MATCH LINE STA. 142+00.00

MATCH LINE STA. 154+00.00

MATCH LINE STA. 154+00.00

MATCH LINE STA. 166+00.00

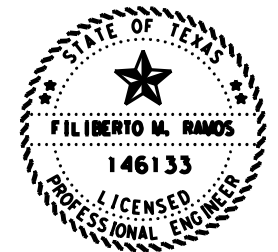
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	600	-	-	-	-	-	-	-	30	-	-	960	-	-	-	-	-	-	-

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

LEGEND:

- A REF PROF PAV MRK TY I(W)6"SLD(100MIL)
 - B RE PM W/RET REQ TY I(W)6"BRK(100MIL)
 - C RE PM W/RET REQ TY I(W)6"DOT(100MIL)
 - D RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
 - E RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
 - F REFL PAV MRK TY I(W)8"SLD(100MIL)
 - G REFL PAV MRK TY I(W)12"SLD(100MIL)
 - H REFL PAV MRK TY I(Y)12"SLD(100MIL)
 - I REFL PAV MRK TY I(W)24"SLD(100MIL)
 - J PREFAB PAV MRK TY C (W) (ARROW)
 - K PREFAB PAV MRK TY C (W) (WORD)
 - L REFL PAV MRKR TY II-A-A
 - M REFL PAV MRKR TY I-C
 - N TRAFFIC BUTTON TY Y
 - O TRAFFIC BUTTON TY B
 - P PAV SURF PREP FOR MRK (6")
 - Q MULTIPOLYMER PAV MRK (W) (6") (SLD)
 - R MULTIPOLYMER PAV MRK (W) (6") (BRK)
 - S MULTIPOLYMER PAV MRK (Y) (6") (SLD)
 - T PREFAB PAV MRK TY C (W) (LNDP ARROW)
 - U PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
 - V REFL PAV MRK TY I(W)6"LNDP(100MIL)
- EOP - EDGE OF PAVEMENT
 ↕ - TRAFFIC FLOW
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
PAVEMENT MARKING
LAYOUT**

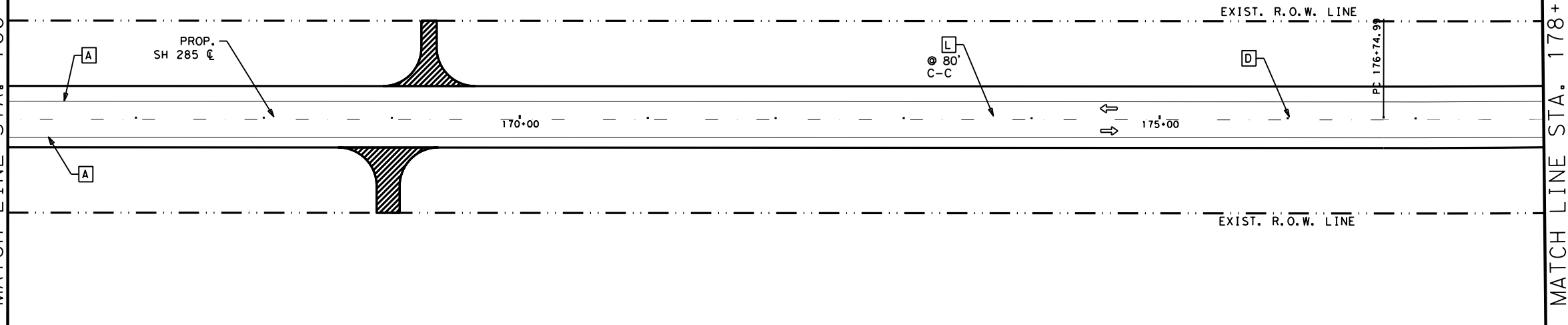
SCALE: 1"=100' SHEET 3 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		319

DATE: 2/29/2024 9:38:39 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM4.dgn

MATCH LINE STA. 166+00.00

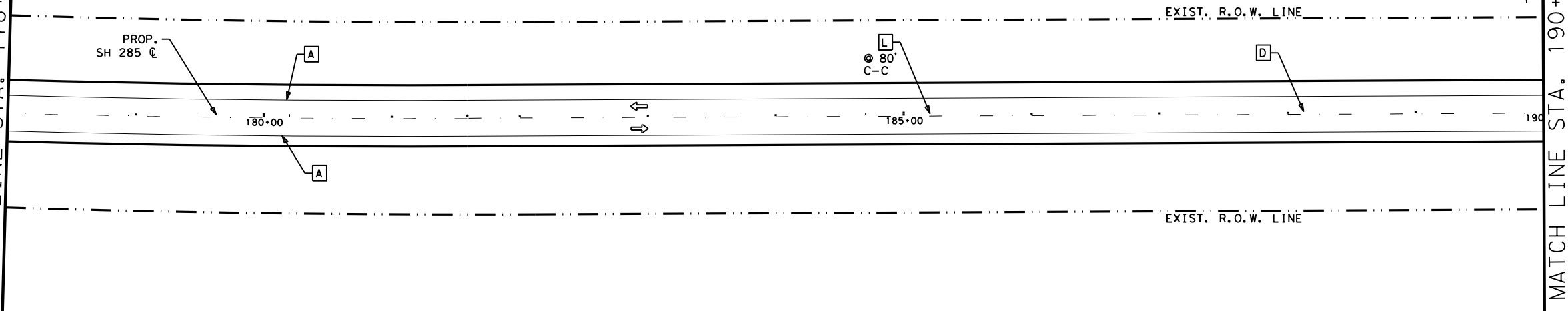
MATCH LINE STA. 178+00.00



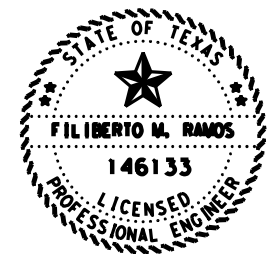
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	600	-	-	-	-	-	-	-	30	-	-	960	-	-	-	-	-	-	-

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



- LEGEND:**
- A REF PROF PAV MRK TY I(W)6"SLD(100MIL)
 - B RE PM W/RET REQ TY I(W)6"BRK(100MIL)
 - C RE PM W/RET REQ TY I(W)6"DOT(100MIL)
 - D RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
 - E RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
 - F REFL PAV MRK TY I(W)8"SLD(100MIL)
 - G REFL PAV MRK TY I(W)12"SLD(100MIL)
 - H REFL PAV MRK TY I(Y)12"SLD(100MIL)
 - I REFL PAV MRK TY I(W)24"SLD(100MIL)
 - J PREFAB PAV MRK TY C (W) (ARROW)
 - K PREFAB PAV MRK TY C (W) (WORD)
 - L REFL PAV MRKR TY II-A-A
 - M REFL PAV MRKR TY I-C
 - N TRAFFIC BUTTON TY Y
 - O TRAFFIC BUTTON TY B
 - P PAV SURF PREP FOR MRK (6")
 - Q MULTIPOLYMER PAV MRK (W) (6") (SLD)
 - R MULTIPOLYMER PAV MRK (W) (6") (BRK)
 - S MULTIPOLYMER PAV MRK (Y) (6") (SLD)
 - T PREFAB PAV MRK TY C (W) (LNDP ARROW)
 - U PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
 - V REFL PAV MRK TY I(W)6"LNDP(100MIL)
- EOP - EDGE OF PAVEMENT
 ↕ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
PAVEMENT MARKING
LAYOUT**

SCALE: 1"=100' SHEET 4 OF 22

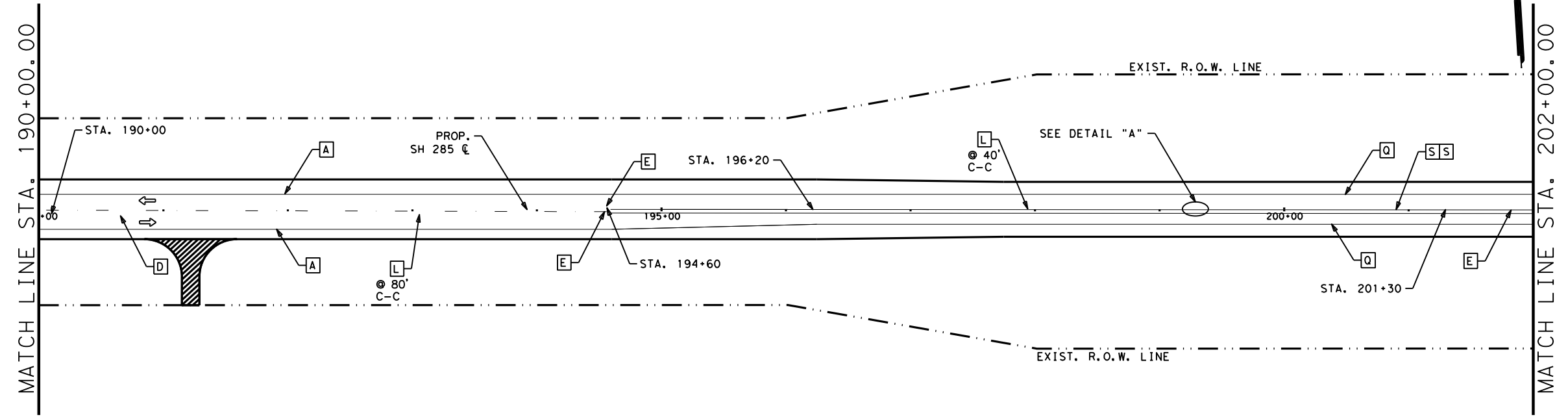
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		320

DATE: 2/29/2024 9:38:48 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM5.dgn

LEGEND:

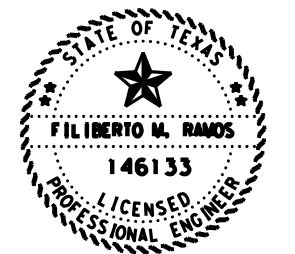
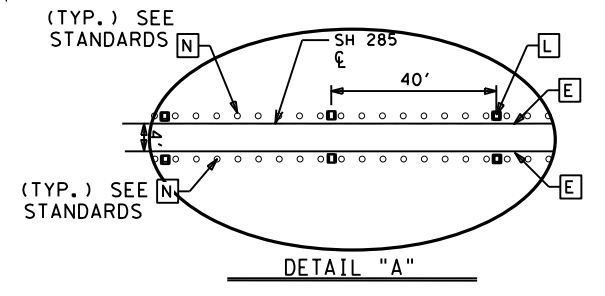
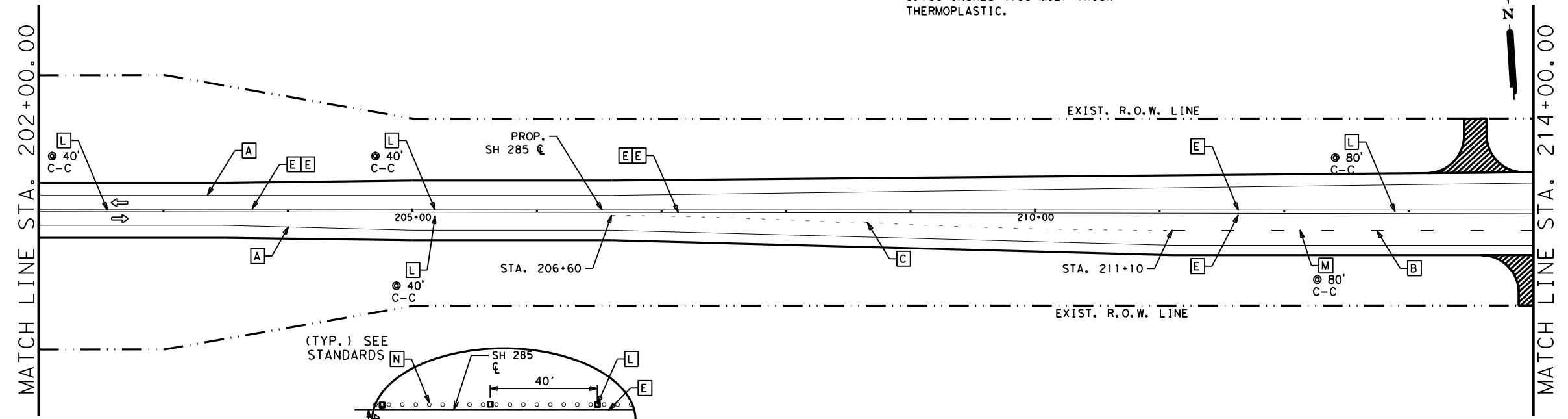
A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↔ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
(A) (LF)	(B) (LF)	(C) (LF)	(D) (LF)	(E) (LF)	(F) (LF)	(G) (LF)	(H) (EA)	(I) (EA)	(J) (EA)	(K) (EA)	(L) (EA)	(M) (EA)	(N) (EA)	(O) (EA)	(P) (LF)	(Q) (LF)	(R) (LF)	(S) (LF)	(T) (EA)	(U) (EA)	(V) (LF)
4530	80	114	115	3880	-	-	-	-	-	-	93	4	776	184	540	270	-	270	-	-	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 5 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		321

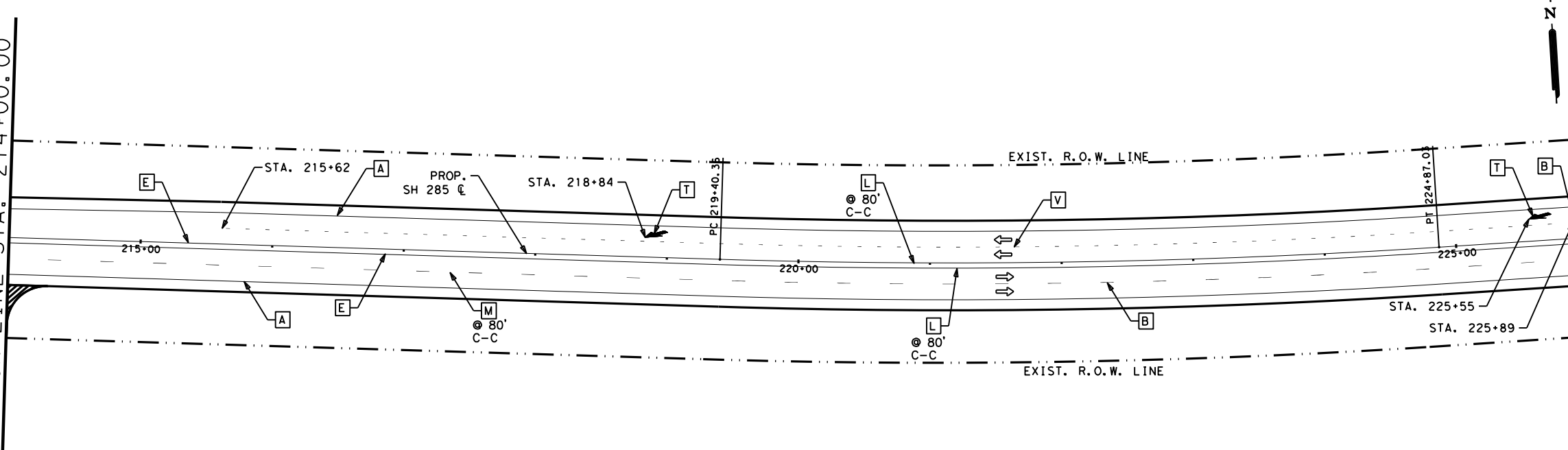
DATE: 2/29/2024 9:38:58 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM6.dgn

MATCH LINE STA. 214+00.00

MATCH LINE STA. 226+00.00

MATCH LINE STA. 226+00.00

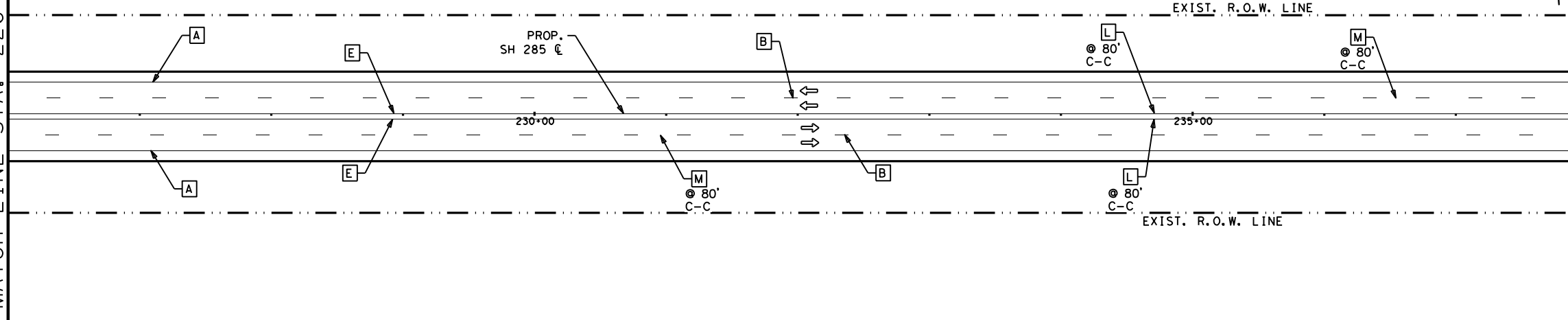
MATCH LINE STA. 238+00.00



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	910	-	-	4800	-	-	-	-	-	-	60	45	960	-	-	-	-	-	2	-	255

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDRP(100MIL)

EOP - EDGE OF PAVEMENT
 ↕ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 6 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		322

DATE: 2/29/2024 9:39:06 AM
 FILE: P:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS.OP2\SH 285 PM7.dgn

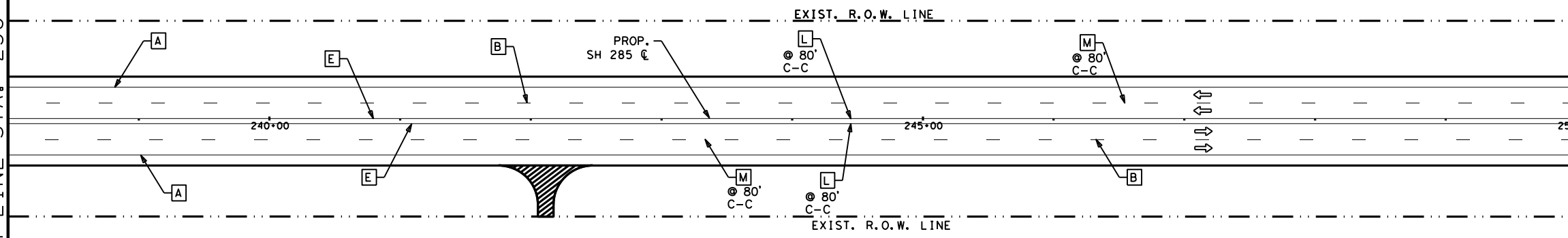
LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 @ - AT
 W/ - WITH

MATCH LINE STA. 238+00.00

MATCH LINE STA. 250+00.00



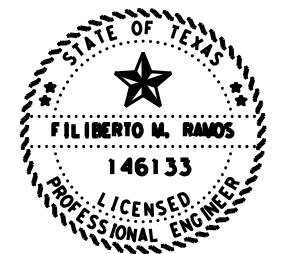
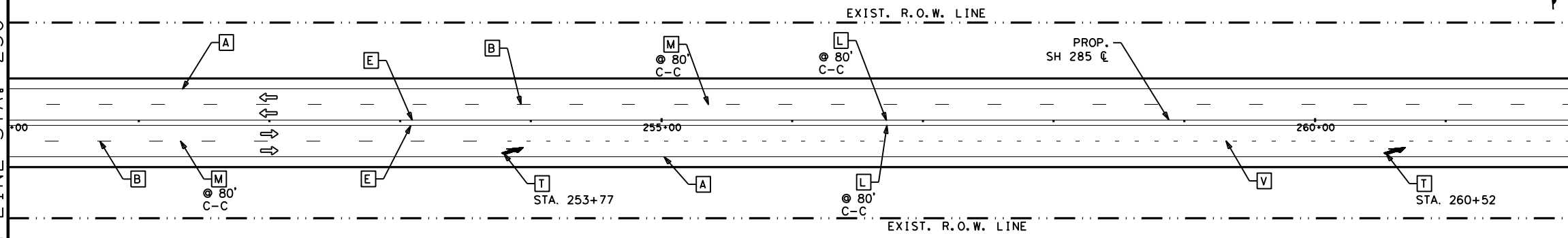
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	990	-	-	4800	-	-	-	-	-	-	60	50	960	-	-	-	-	-	2	-	207

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

MATCH LINE STA. 250+00.00

MATCH LINE STA. 262+00.00



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
PAVEMENT MARKING
LAYOUT**

SCALE: 1"=100' SHEET 7 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		323

DATE: 2/29/2024 9:39:11 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM8.dgn

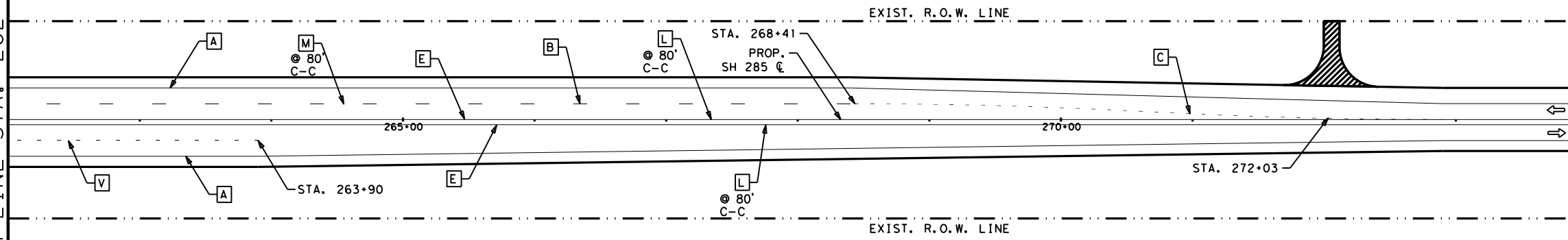
LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHIT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

MATCH LINE STA. 262+00.00

MATCH LINE STA. 274+00.00



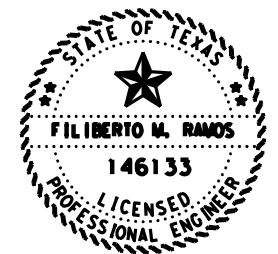
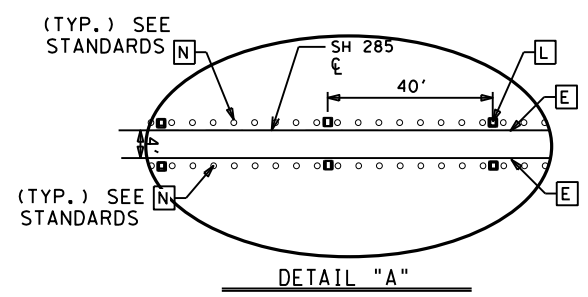
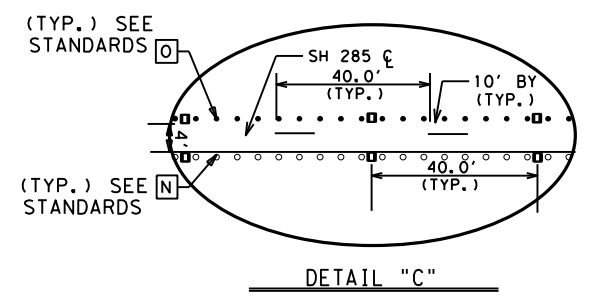
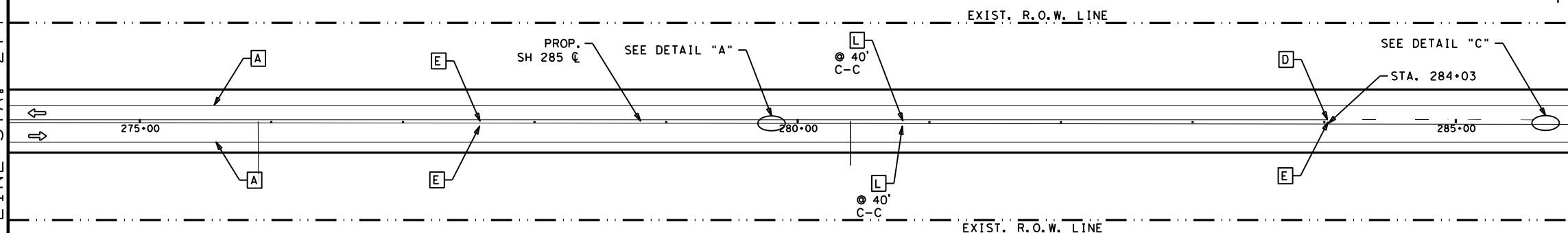
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	160	114	50	4406	-	-	-	-	-	-	83	8	921	40	-	-	-	-	-	-	48

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

MATCH LINE STA. 274+00.00

MATCH LINE STA. 286+00.00



[Signature]
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 8 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		324

DATE: 2/29/2024 9:39:17 AM
 FILE: P:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS.OP2\SH 285 PM9.dgn

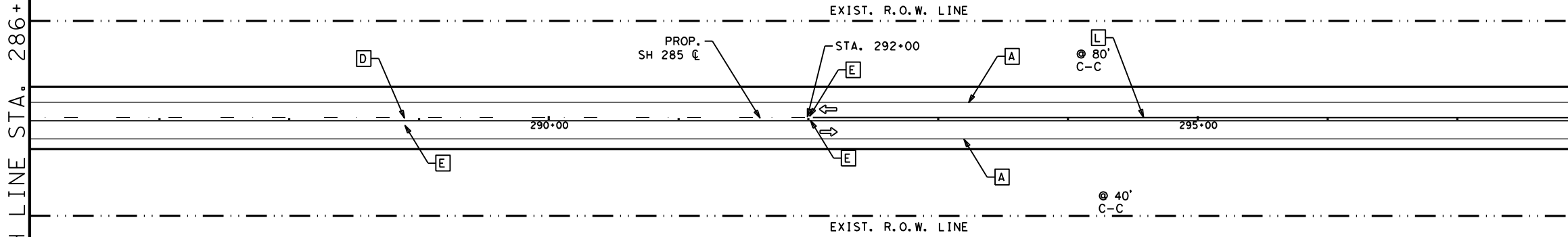
LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↔ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

MATCH LINE STA. 286+00.00

MATCH LINE STA. 298+00.00



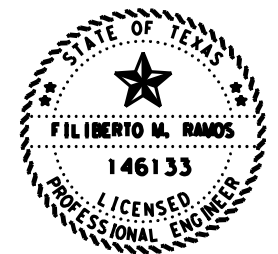
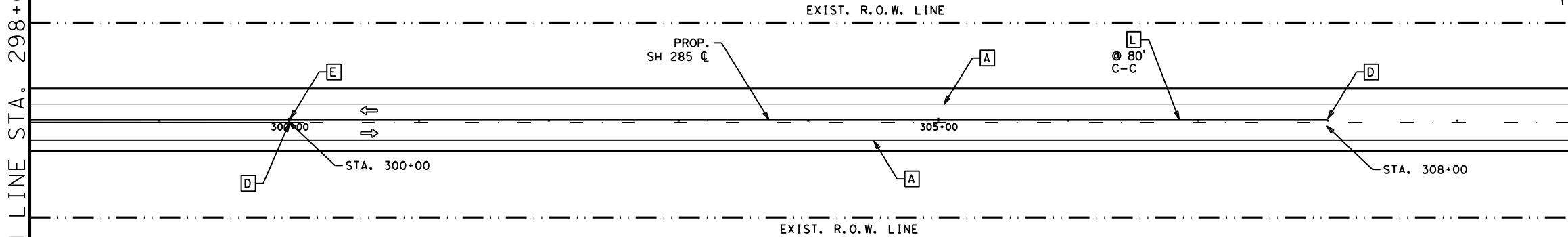
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	400	3000	-	-	-	-	-	-	114	-	600	360	-	-	-	-	-	-	-

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

MATCH LINE STA. 298+00.00

MATCH LINE STA. 310+00.00



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design



**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 9 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		325

DATE: 2/29/2024 9:39:24 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM10.dgn

MATCH LINE STA. 310+00

MATCH LINE STA. 322+00

MATCH LINE STA. 322+00

MATCH LINE STA. 334+00

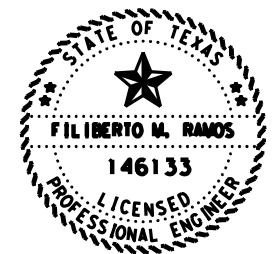
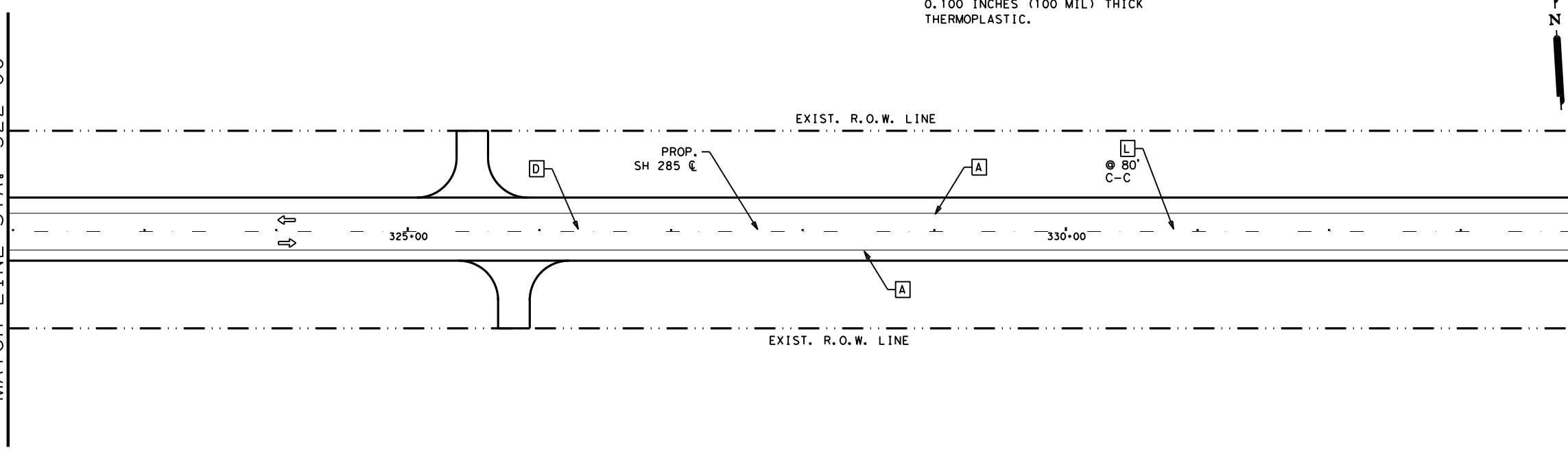
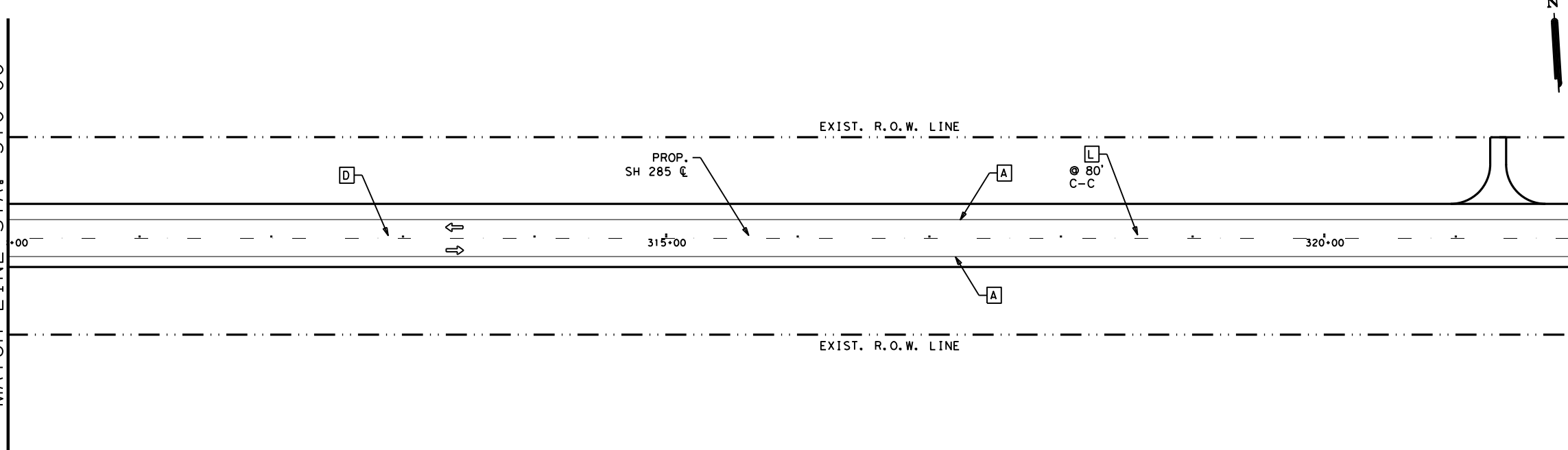
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	600	-	-	-	-	-	-	-	30	-	-	960	-	-	-	-	-	-	-

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

LEGEND:

- A REF PROF PAV MRK TY I(W)6"SLD(100MIL)
 - B RE PM W/RET REQ TY I(W)6"BRK(100MIL)
 - C RE PM W/RET REQ TY I(W)6"DOT(100MIL)
 - D RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
 - E RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
 - F REFL PAV MRK TY I(W)8"SLD(100MIL)
 - G REFL PAV MRK TY I(W)12"SLD(100MIL)
 - H REFL PAV MRK TY I(Y)12"SLD(100MIL)
 - I REFL PAV MRK TY I(W)24"SLD(100MIL)
 - J PREFAB PAV MRK TY C (W) (ARROW)
 - K PREFAB PAV MRK TY C (W) (WORD)
 - L REFL PAV MRKR TY II-A-A
 - M REFL PAV MRKR TY I-C
 - N TRAFFIC BUTTON TY Y
 - O TRAFFIC BUTTON TY B
 - P PAV SURF PREP FOR MRK (6")
 - Q MULTIPOLYMER PAV MRK (W) (6") (SLD)
 - R MULTIPOLYMER PAV MRK (W) (6") (BRK)
 - S MULTIPOLYMER PAV MRK (Y) (6") (SLD)
 - T PREFAB PAV MRK TY C (W) (LNDP ARROW)
 - U PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHIT)
 - V REFL PAV MRK TY I(W)6"LNDP(100MIL)
- EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

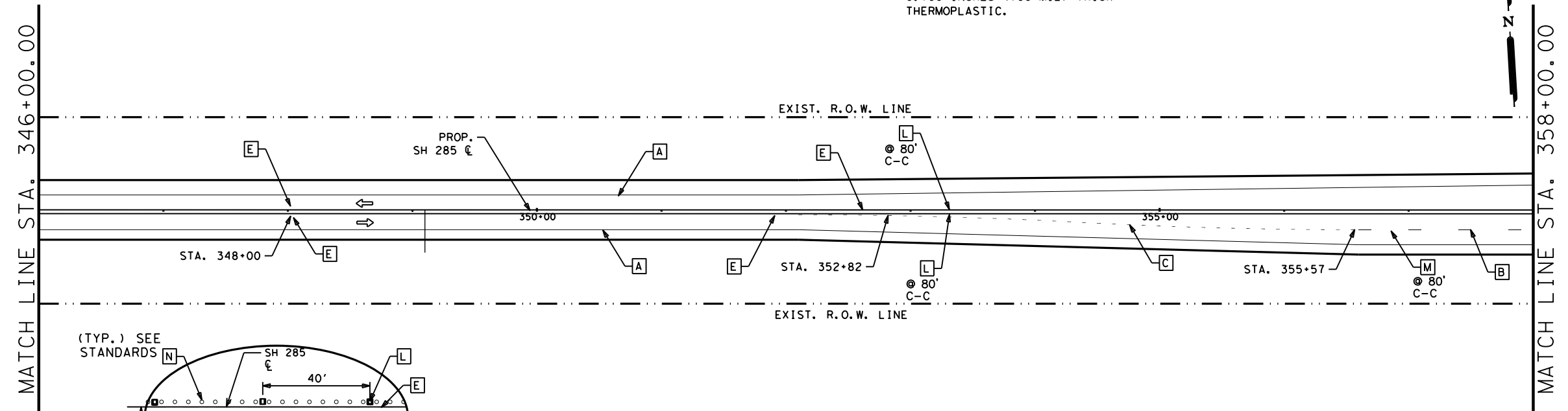
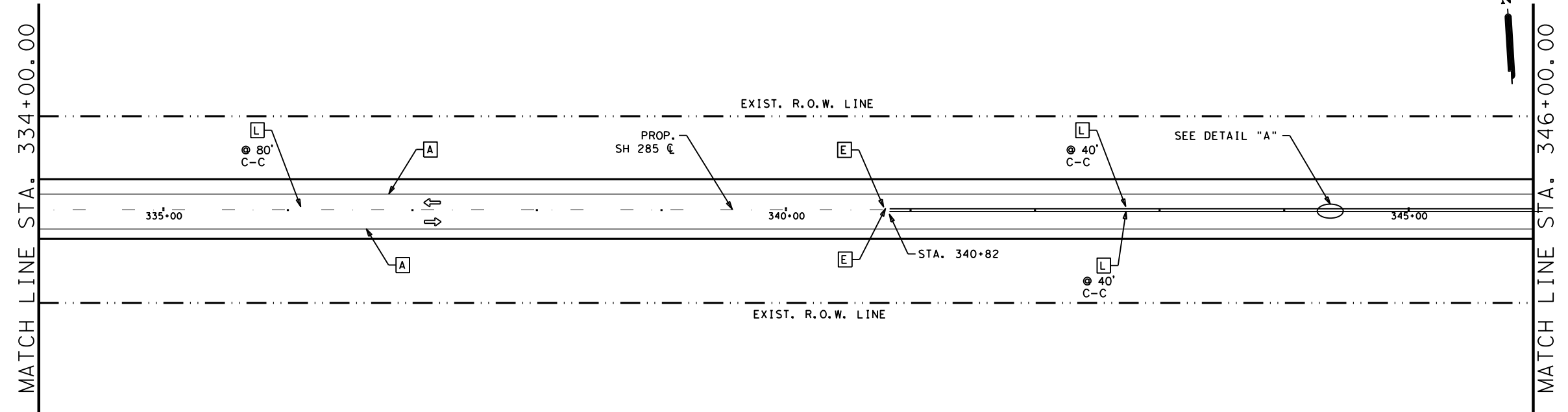
**SH 285
PAVEMENT MARKING
LAYOUT**

SCALE: 1"=100' SHEET 10 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		326

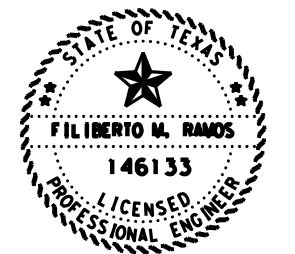
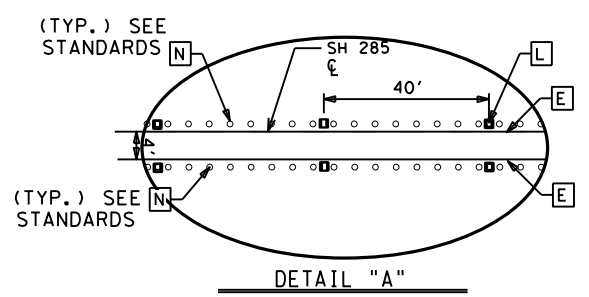
DATE: 2/29/2024 9:39:30 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS.OP2\SH 285 PM11.dgn

- LEGEND:**
- A REF PROF PAV MRK TY I(W)6"SLD(100MIL)
 - B RE PM W/RET REQ TY I(W)6"BRK(100MIL)
 - C RE PM W/RET REQ TY I(W)6"DOT(100MIL)
 - D RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
 - E RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
 - F REFL PAV MRK TY I(W)8"SLD(100MIL)
 - G REFL PAV MRK TY I(W)12"SLD(100MIL)
 - H REFL PAV MRK TY I(Y)12"SLD(100MIL)
 - I REFL PAV MRK TY I(W)24"SLD(100MIL)
 - J PREFAB PAV MRK TY C (W) (ARROW)
 - K PREFAB PAV MRK TY C (W) (WORD)
 - L REFL PAV MRKR TY II-A-A
 - M REFL PAV MRKR TY I-C
 - N TRAFFIC BUTTON TY Y
 - O TRAFFIC BUTTON TY B
 - P PAV SURF PREP FOR MRK (6")
 - Q MULTIPOLYMER PAV MRK (W) (6") (SLD)
 - R MULTIPOLYMER PAV MRK (W) (6") (BRK)
 - S MULTIPOLYMER PAV MRK (Y) (6") (SLD)
 - T PREFAB PAV MRK TY C (W) (LNDP ARROW)
 - U PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHIT)
 - V REFL PAV MRK TY I(W)6"LNDP(100MIL)
- EOP - EDGE OF PAVEMENT
 ↔ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	40	105	171	3436	-	-	-	-	-	-	79	2	687	273	-	-	-	-	-	-	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 11 OF 22

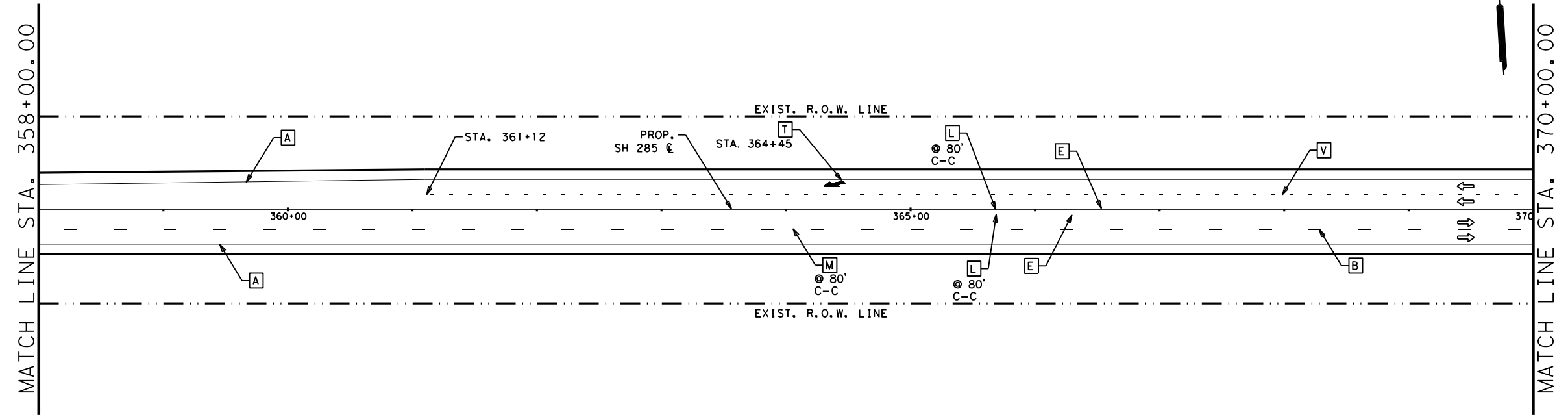
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		327

DATE: 2/29/2024 9:39:39 AM
 FILE: D:\txdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM12.dgn

LEGEND:

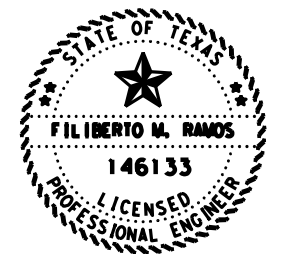
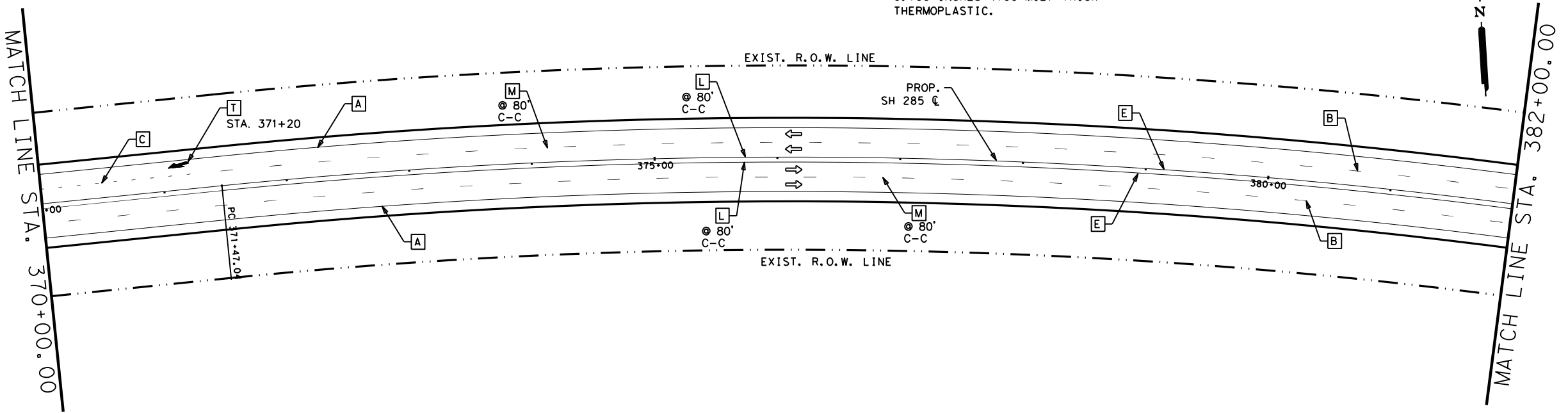
A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDRP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	870	-	-	4800	-	-	-	-	-	-	60	45	960	-	-	-	-	-	2	-	252

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



[Signature]
 02/29/2024

Pharr District Central Design

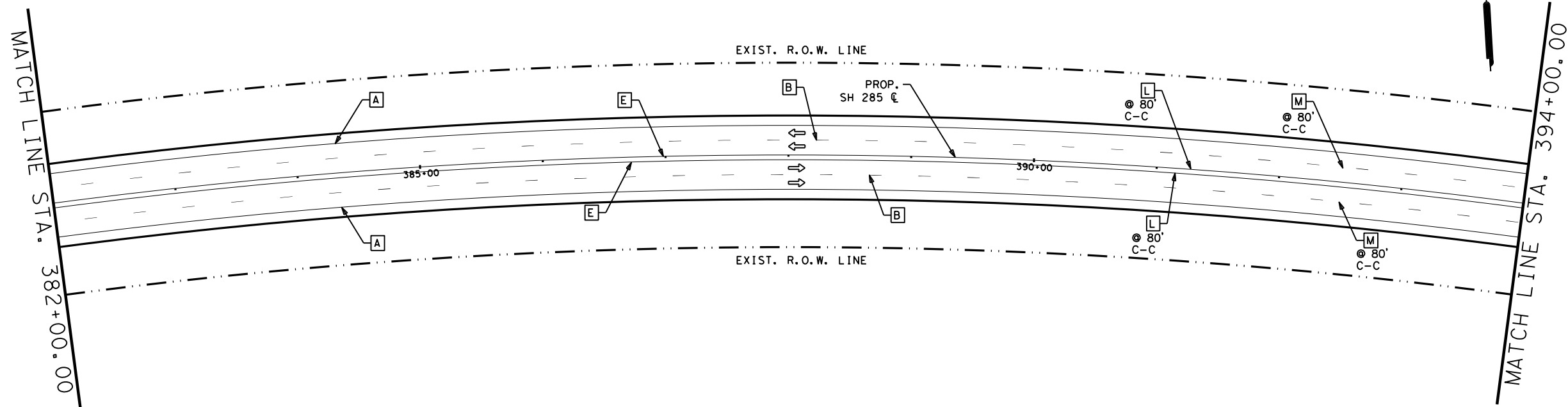
Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 12 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		328

DATE: 2/29/2024 9:39:47 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 Pavement Markings\02\SH 285 PM13.dgn



LEGEND:

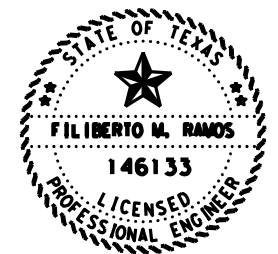
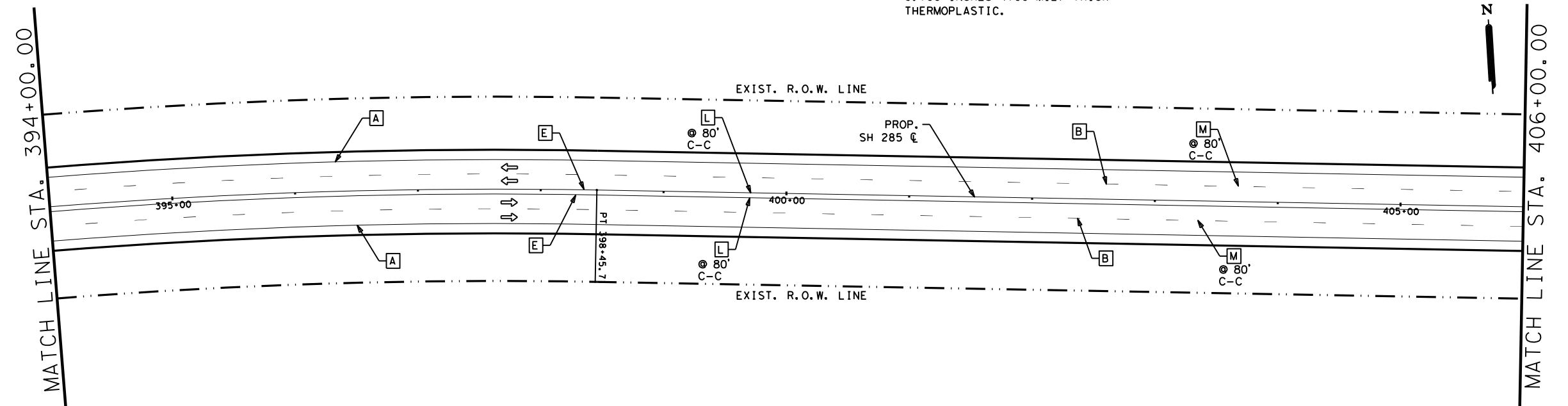
- A REF PROF PAV MRK TY I(W)6"SLD(100MIL)
- B RE PM W/RET REQ TY I(W)6"BRK(100MIL)
- C RE PM W/RET REQ TY I(W)6"DOT(100MIL)
- D RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
- E RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
- F REFL PAV MRK TY I(W)8"SLD(100MIL)
- G REFL PAV MRK TY I(W)12"SLD(100MIL)
- H REFL PAV MRK TY I(Y)12"SLD(100MIL)
- I REFL PAV MRK TY I(W)24"SLD(100MIL)
- J PREFAB PAV MRK TY C (W) (ARROW)
- K PREFAB PAV MRK TY C (W) (WORD)
- L REFL PAV MRKR TY II-A-A
- M REFL PAV MRKR TY I-C
- N TRAFFIC BUTTON TY Y
- O TRAFFIC BUTTON TY B
- P PAV SURF PREP FOR MRK (6")
- Q MULTIPOLYMER PAV MRK (W) (6") (SLD)
- R MULTIPOLYMER PAV MRK (W) (6") (BRK)
- S MULTIPOLYMER PAV MRK (Y) (6") (SLD)
- T PREFAB PAV MRK TY C (W) (LNDP ARROW)
- U PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
- V REFL PAV MRK TY I(W)6"LNDRP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

666	666	666	666	666	666	666	666	666	668	668	672	672	672	672	678	6038	6038	6038	668	668	666
6343	6306	6018	6318	6321	6036	6042	6141	6048	6077	6085	6009	6007	6017	6018	6002	6004	6005	6017	6083	6116	6021
A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V
(LF)	(LF)	(LF)	(LF)	(LF)	(LF)	(LF)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(LF)	(LF)	(LF)	(LF)	(EA)	(EA)	(LF)
4800	1200	-	-	4800	-	-	-	-	-	-	60	60	960	-	-	-	-	-	-	-	-

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 13 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		329

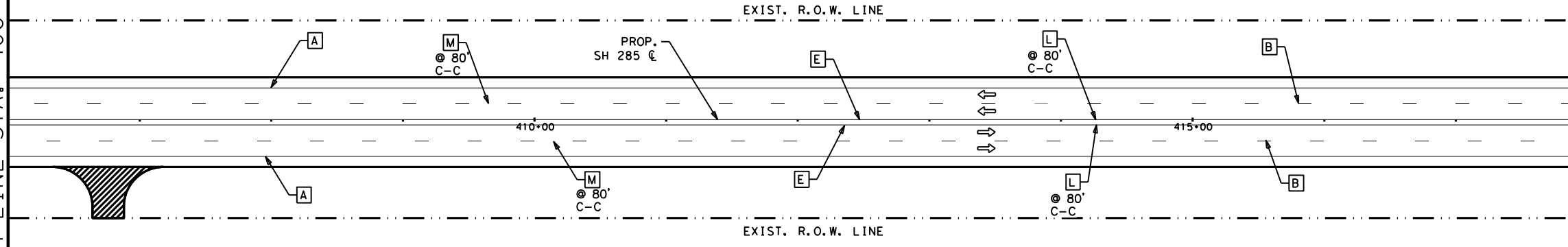
DATE: 2/29/2024 9:39:55 AM
 FILE: \\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PMI 4.dgn

MATCH LINE STA. 406+00.00

MATCH LINE STA. 418+00.00

MATCH LINE STA. 418+00.00

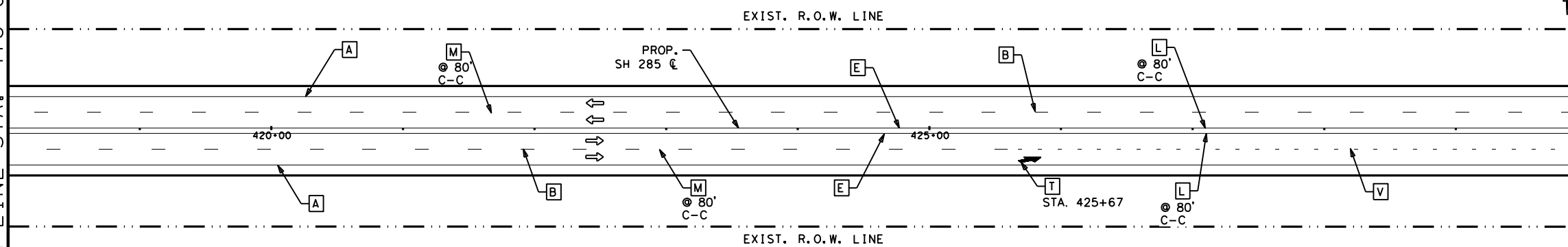
MATCH LINE STA. 430+00.00



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	1090	-	-	4800	-	-	-	-	-	-	60	54	960	-	-	-	-	-	1	-	108

NOTES:

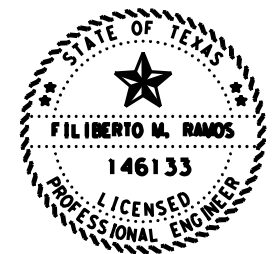
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 14 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		330

DATE: 2/29/2024 9:40:01 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM15.dgn

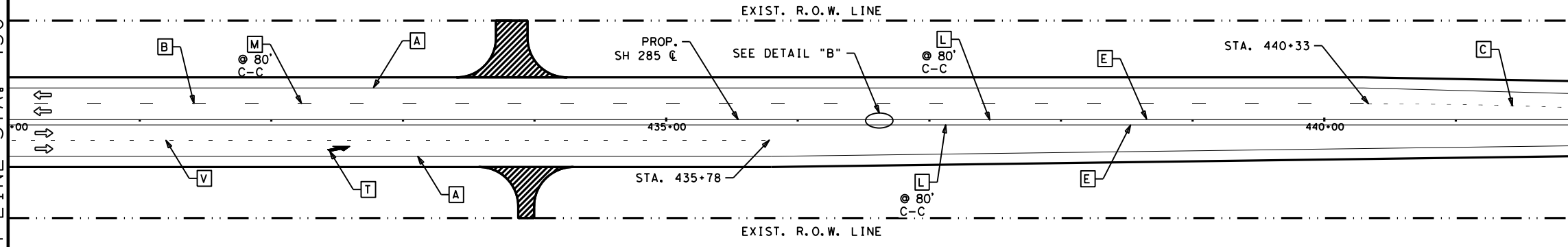
LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

MATCH LINE STA. 430+00.00

MATCH LINE STA. 442+00.00



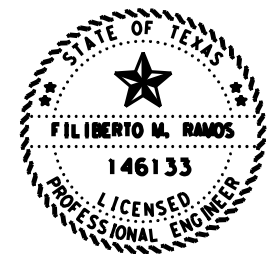
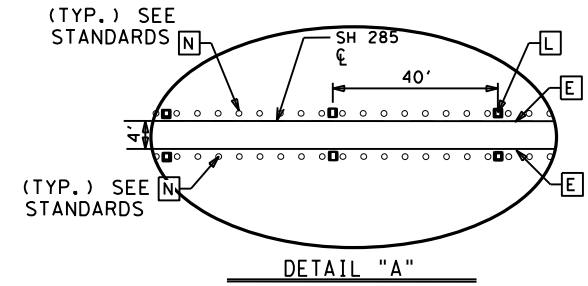
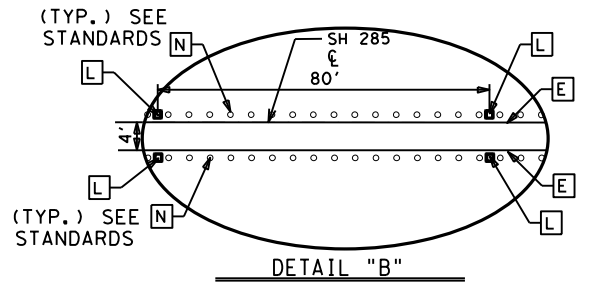
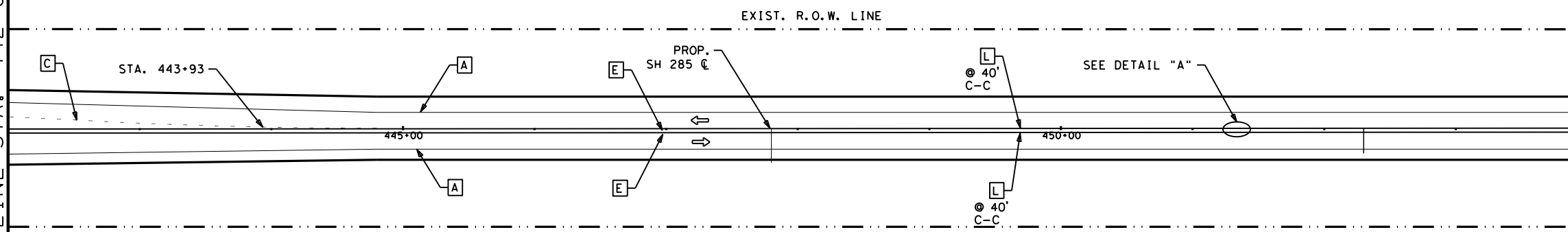
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	260	114	-	4800	-	-	-	-	-	-	81	13	960	-	-	-	-	-	1	-	147

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

MATCH LINE STA. 442+00.00

MATCH LINE STA. 454+00.00



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design
 Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 15 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST		COUNTY	SHEET NO.
	PHR		JIM HOGG	331

DATE: 2/29/2024 9:40:08 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM16.dgn

LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

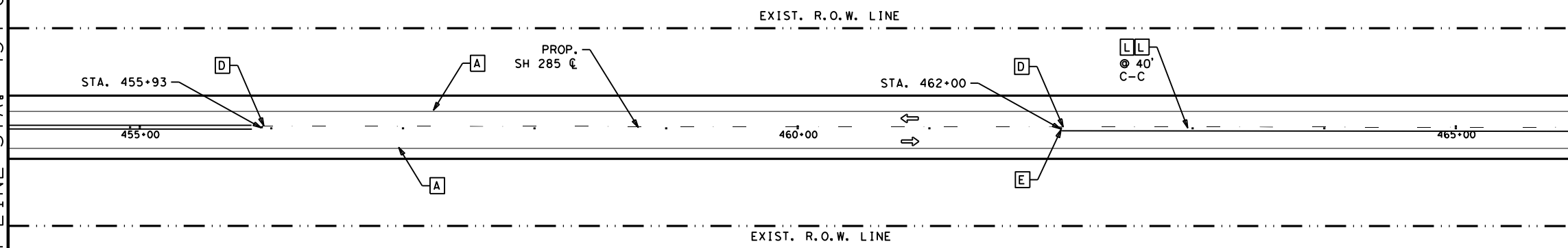
EOP - EDGE OF PAVEMENT
 ↔ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

MATCH LINE STA. 454+00.00

MATCH LINE STA. 466+00.00

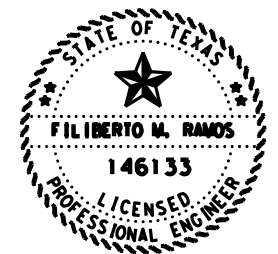
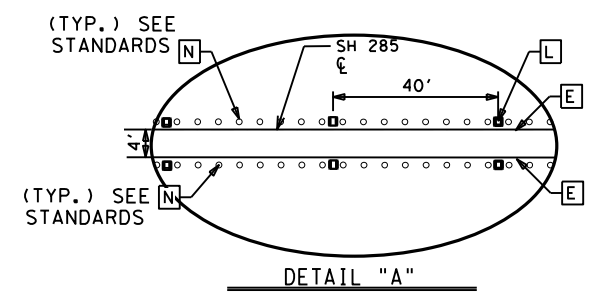
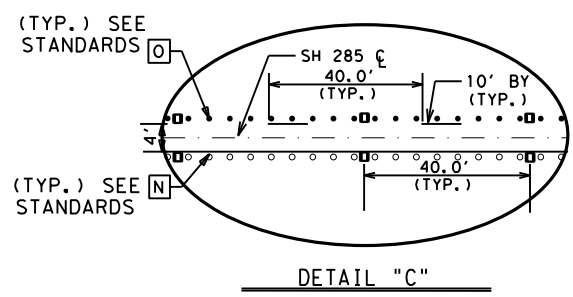
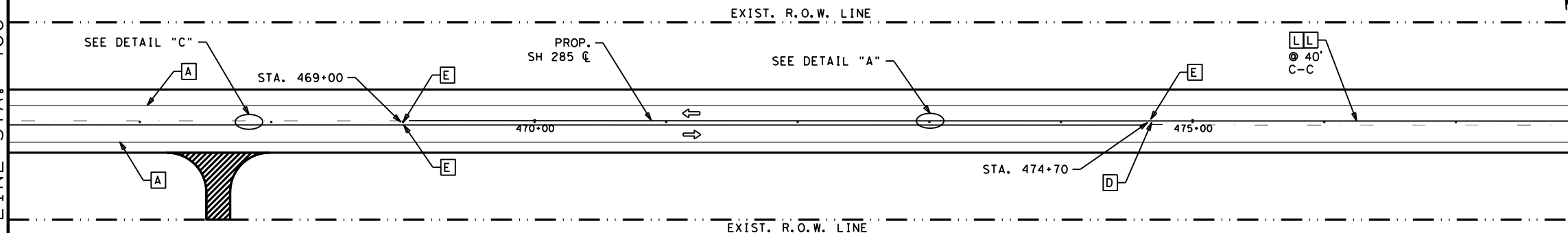
MATCH LINE STA. 466+00.00

MATCH LINE STA. 478+00.00



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	410	2556	-	-	-	-	-	-	97	-	511	449	-	-	-	-	-	-	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 16 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		332

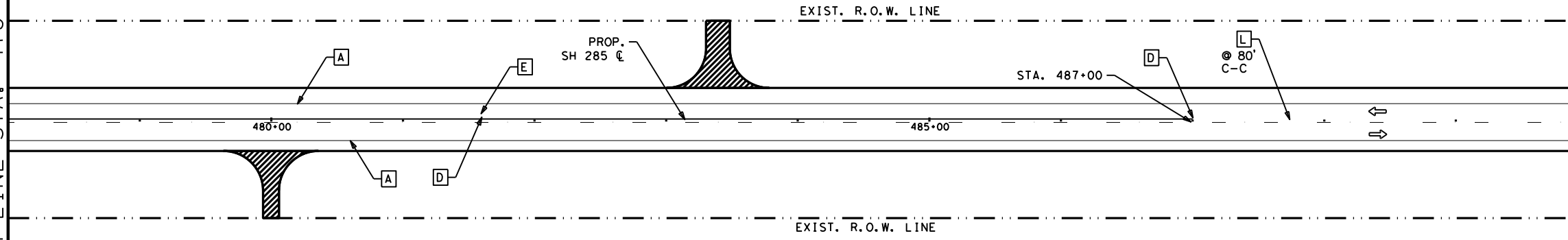
DATE: 2/29/2024 9:40:15 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM17.dgn

MATCH LINE STA. 478+00.00

MATCH LINE STA. 490+00.00

MATCH LINE STA. 490+00.00

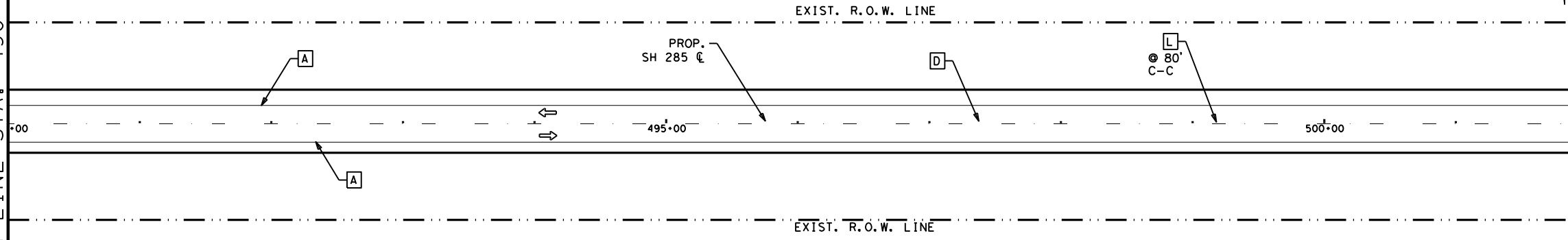
MATCH LINE STA. 502+00.00



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	-	600	900	-	-	-	-	-	-	63	-	180	780	-	-	-	-	-	-	-

NOTES:

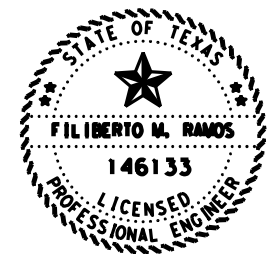
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↕ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 17 OF 22

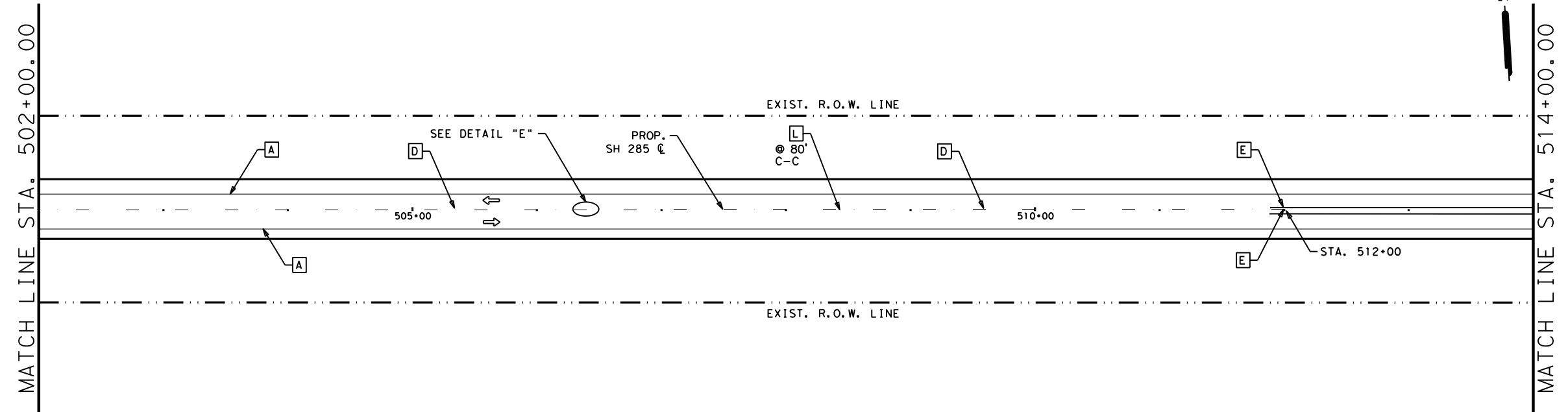
© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		333

DATE: 2/29/2024 9:40:21 AM
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM18.dgn

LEGEND:

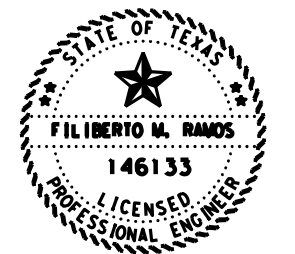
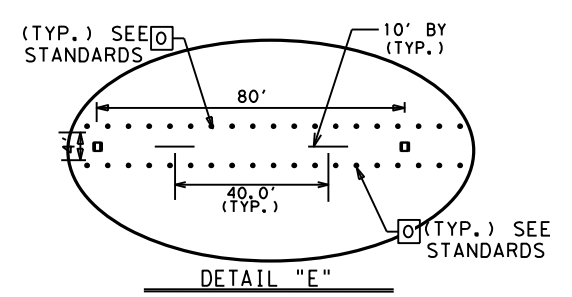
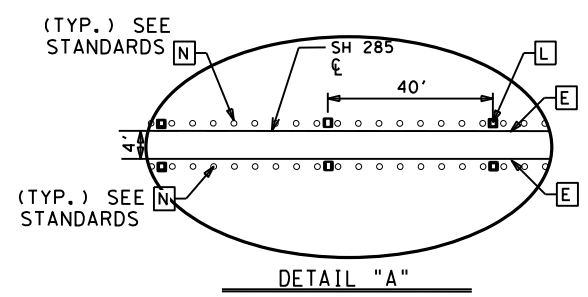
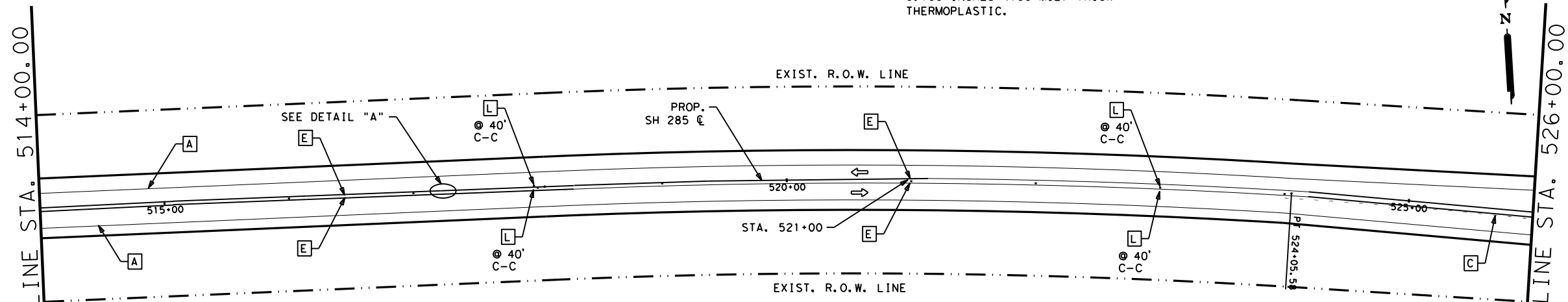
A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (40'LD)
R	MULTIPOLYMER PAV MRK (W) (C-C)(BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↔ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	-	48	250	2800	-	-	-	-	-	-	83	-	560	400	-	-	-	-	-	-	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



[Signature]
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 18 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	334	

DATE: 2/29/2024 9:40:29 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM19.dgn

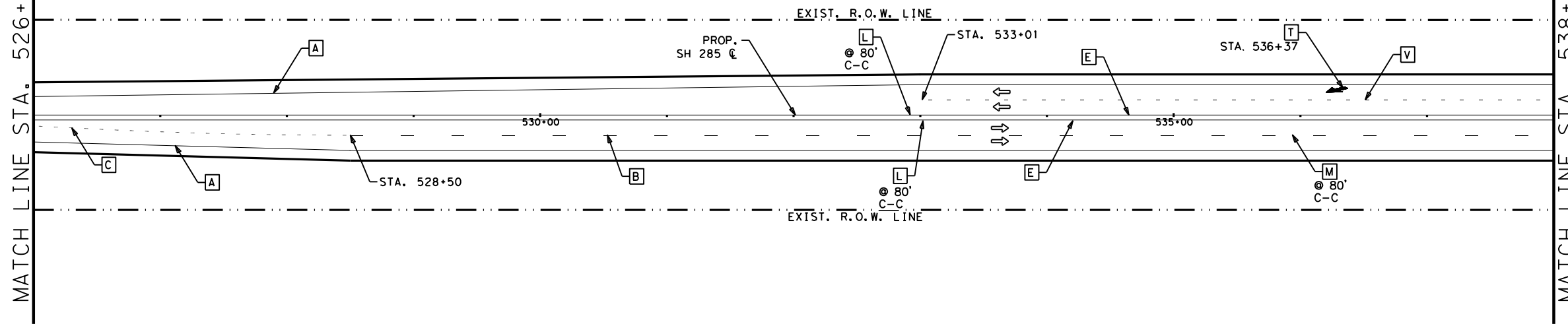
LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↕ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH

MATCH LINE STA. 526+00.00

MATCH LINE STA. 538+00.00



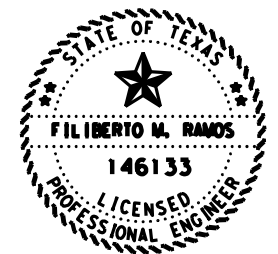
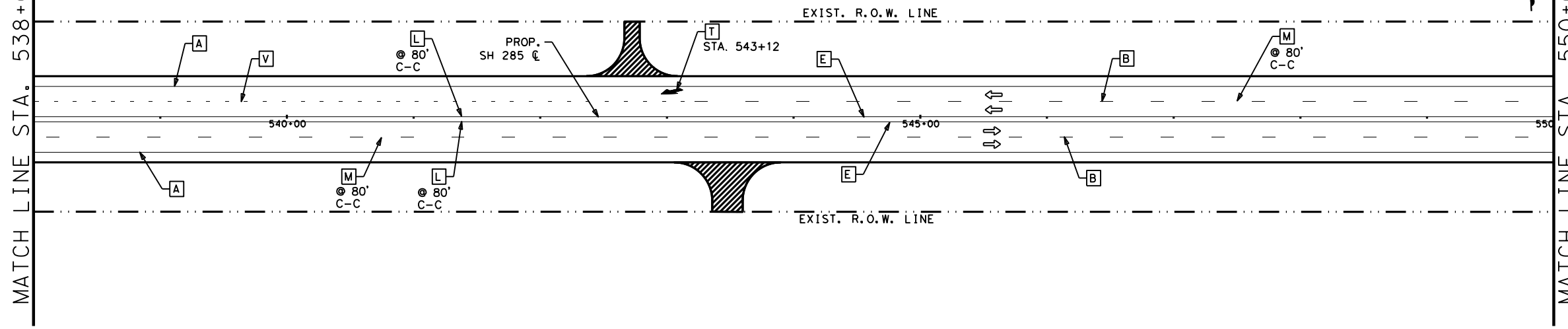
666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	710	64	-	4800	-	-	-	-	-	-	60	34	960	-	-	-	-	-	2	-	255

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.

MATCH LINE STA. 538+00.00

MATCH LINE STA. 550+00.00



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 19 OF 22

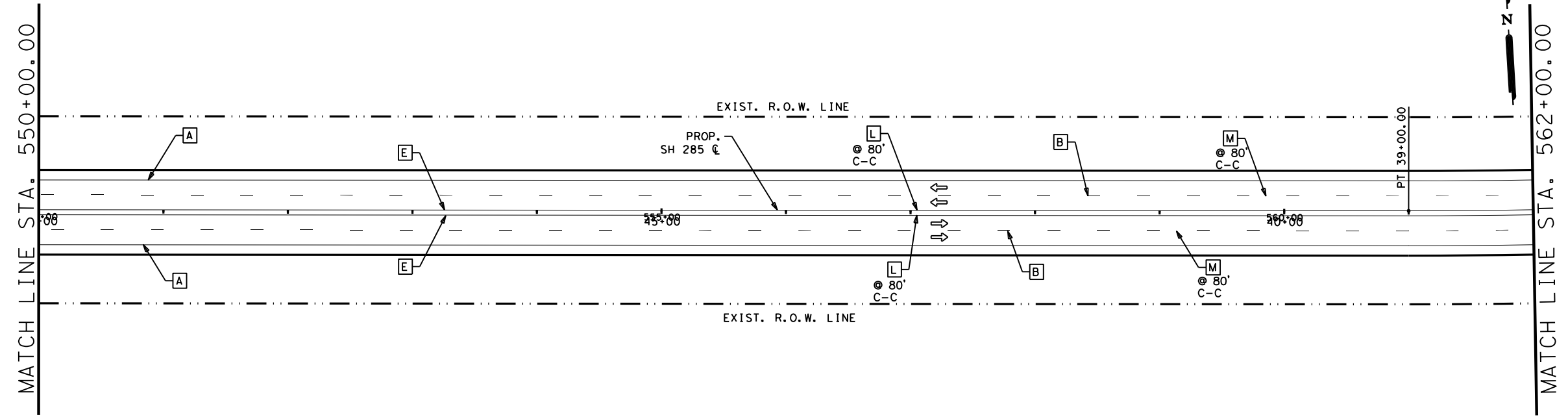
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		335

DATE: 2/29/2024 9:40:36 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM20.dgn

LEGEND:

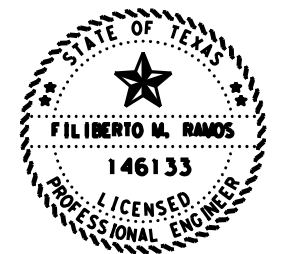
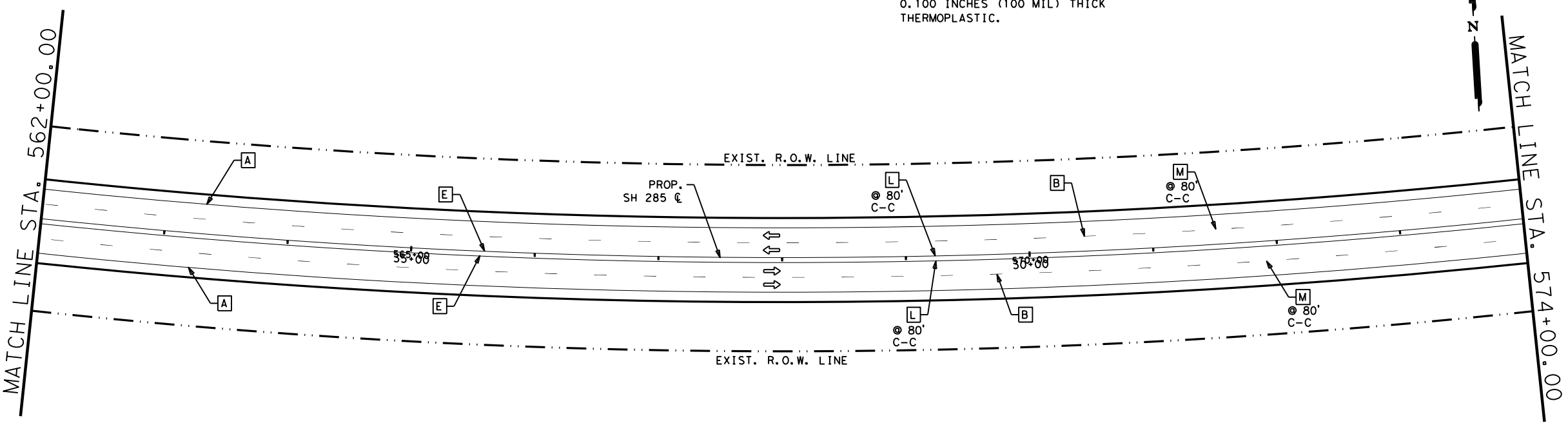
A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 @ - AT
 C-C - CENTER TO CENTER
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	1200	-	-	4800	-	-	-	-	-	-	60	60	960	-	-	-	-	-	-	-	-

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

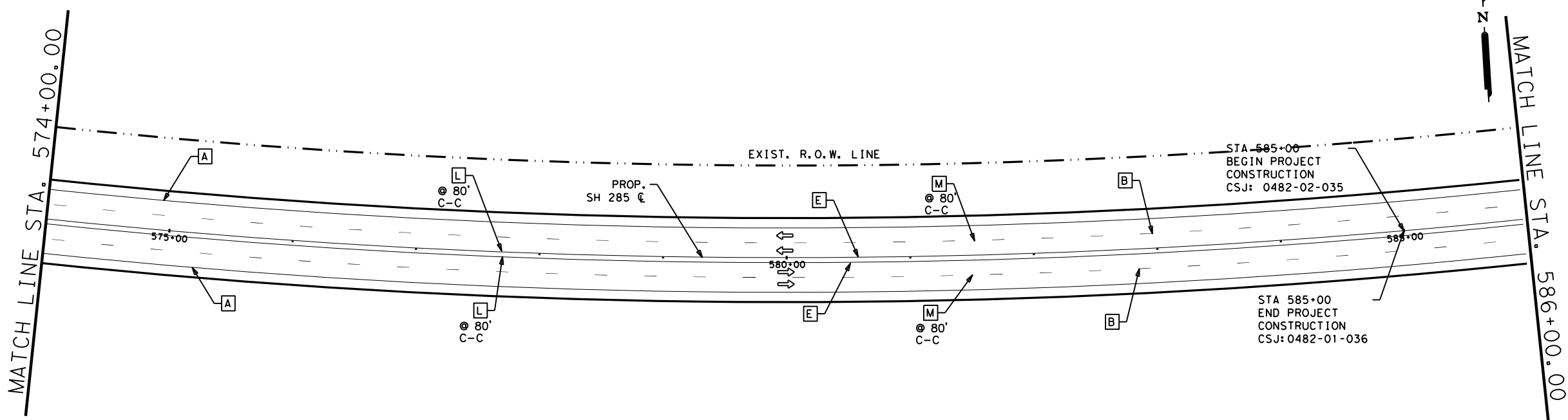
Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 20 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		336

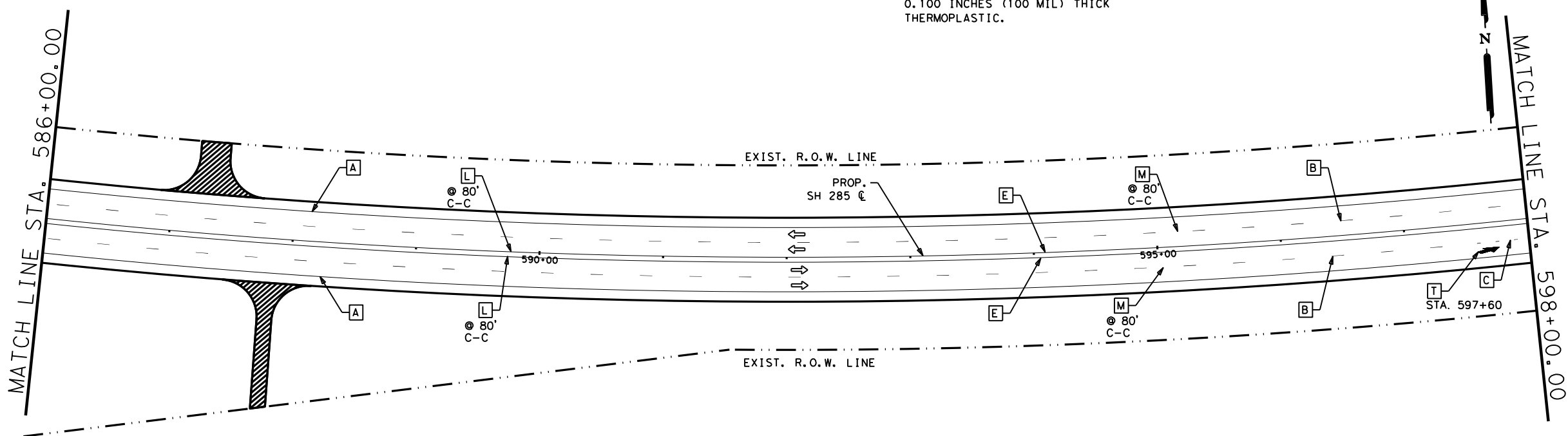
DATE: 2/29/2024 9:40:42 AM
 FILE: D:\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM21.dgn



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
4800	1200	12	-	4800	-	-	-	-	-	-	60	59	960	-	-	-	-	-	1		
2200	550			2200							28	28	440						1		
2600	650	10		2600							32	32	520								

NOTES:

1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



LEGEND:

A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDP(100MIL)

EOP - EDGE OF PAVEMENT
 ↕ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



Filiberto M. Ramos
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 21 OF 22

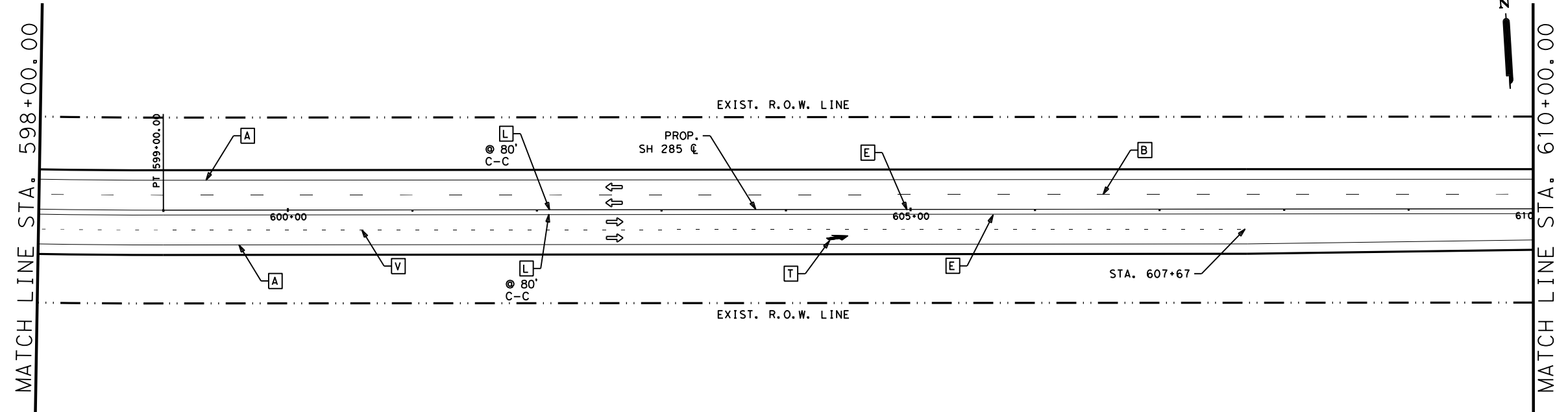
CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	337	

DATE: 2/29/2024 9:40:48 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\8. Traffic\SH 285 PAVEMENT MARKINGS_OP2\SH 285 PM22.dgn

LEGEND:

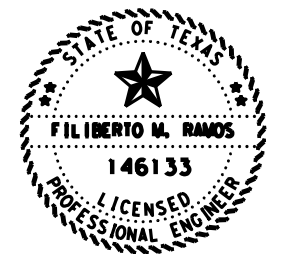
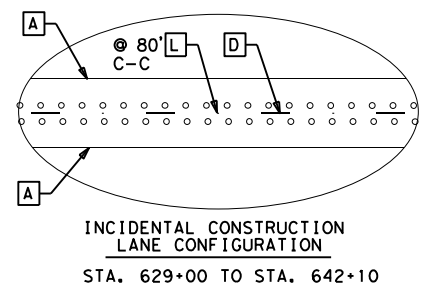
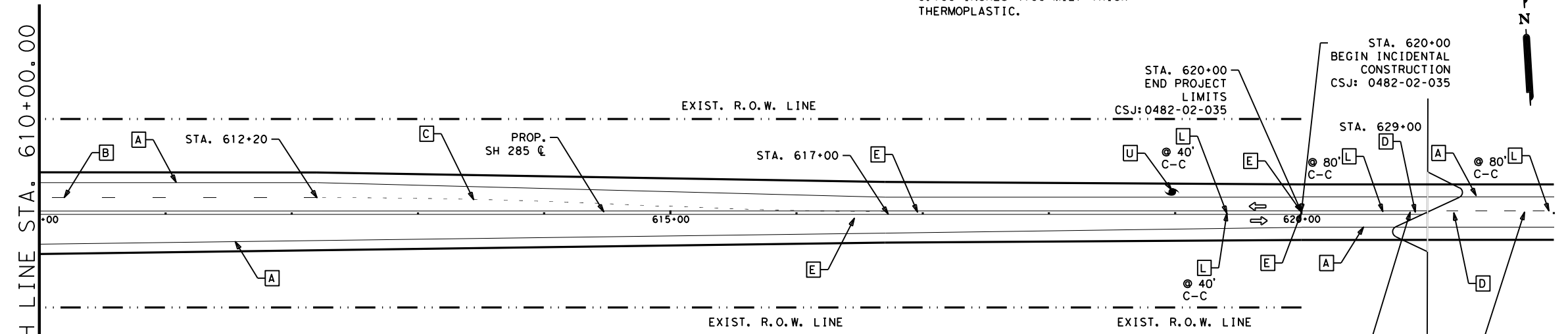
A	REF PROF PAV MRK TY I(W)6"SLD(100MIL)
B	RE PM W/RET REQ TY I(W)6"BRK(100MIL)
C	RE PM W/RET REQ TY I(W)6"DOT(100MIL)
D	RE PM W/RET REQ TY I(Y)6"BRK(100MIL)
E	RE PM W/RET REQ TY I(Y)6"SLD(100MIL)
F	REFL PAV MRK TY I(W)8"SLD(100MIL)
G	REFL PAV MRK TY I(W)12"SLD(100MIL)
H	REFL PAV MRK TY I(Y)12"SLD(100MIL)
I	REFL PAV MRK TY I(W)24"SLD(100MIL)
J	PREFAB PAV MRK TY C (W) (ARROW)
K	PREFAB PAV MRK TY C (W) (WORD)
L	REFL PAV MRKR TY II-A-A
M	REFL PAV MRKR TY I-C
N	TRAFFIC BUTTON TY Y
O	TRAFFIC BUTTON TY B
P	PAV SURF PREP FOR MRK (6")
Q	MULTIPOLYMER PAV MRK (W) (6") (SLD)
R	MULTIPOLYMER PAV MRK (W) (6") (BRK)
S	MULTIPOLYMER PAV MRK (Y) (6") (SLD)
T	PREFAB PAV MRK TY C (W) (LNDP ARROW)
U	PREFAB PAV MRK TY C (EVAC SYM, BLUE/WHT)
V	REFL PAV MRK TY I(W)6"LNDRP(100MIL)

EOP - EDGE OF PAVEMENT
 ↑ - TRAFFIC FLOW
 C-C - CENTER TO CENTER
 @ - AT
 W/ - WITH



666 6343	666 6306	666 6018	666 6318	666 6321	666 6036	666 6042	666 6141	666 6048	668 6077	668 6085	672 6009	672 6007	672 6017	672 6018	678 6002	6038 6004	6038 6005	6038 6017	668 6083	668 6116	666 6021
A (LF)	B (LF)	C (LF)	D (LF)	E (LF)	F (LF)	G (LF)	H (EA)	I (EA)	J (EA)	K (EA)	L (EA)	M (EA)	N (EA)	O (EA)	P (LF)	Q (LF)	R (LF)	S (LF)	T (EA)	U (EA)	V (LF)
8820	360	114	328	6200	-	-	-	-	-	-	104	18	1240	524	-	-	-	-	1	1	243

- NOTES:**
1. THE PAVEMENT SURFACE SHALL BE FREE OF DELETERIOUS MATERIAL BEFORE APPLICATION OF PERMANENT STRIPING AND PAVEMENT MARKERS. IF THE SURFACE NEEDS TO BE CLEANED, AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL PREPARE SURFACE IN ACCORDANCE WITH ITEM 678, "PAVEMENT SURFACE PREPARATION FOR MARKINGS." THE PREPARATION OF PAVEMENT SURFACE SHALL BE SUBSIDIARY TO ITEM 666, 668 AND 672.
 2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE 0.100 INCHES (100 MIL) THICK THERMOPLASTIC.
 3. SEE APPLICABLE TXDOT PAVEMENT MARKING STANDARDS FOR ADDITIONAL INFORMATION.
 4. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS ARE INCLUDED IN "SUMMARY OF PAVEMENT MARKING ITEMS" TABLE.



[Signature]
 02/29/2024

Pharr District Central Design

Texas Department of Transportation

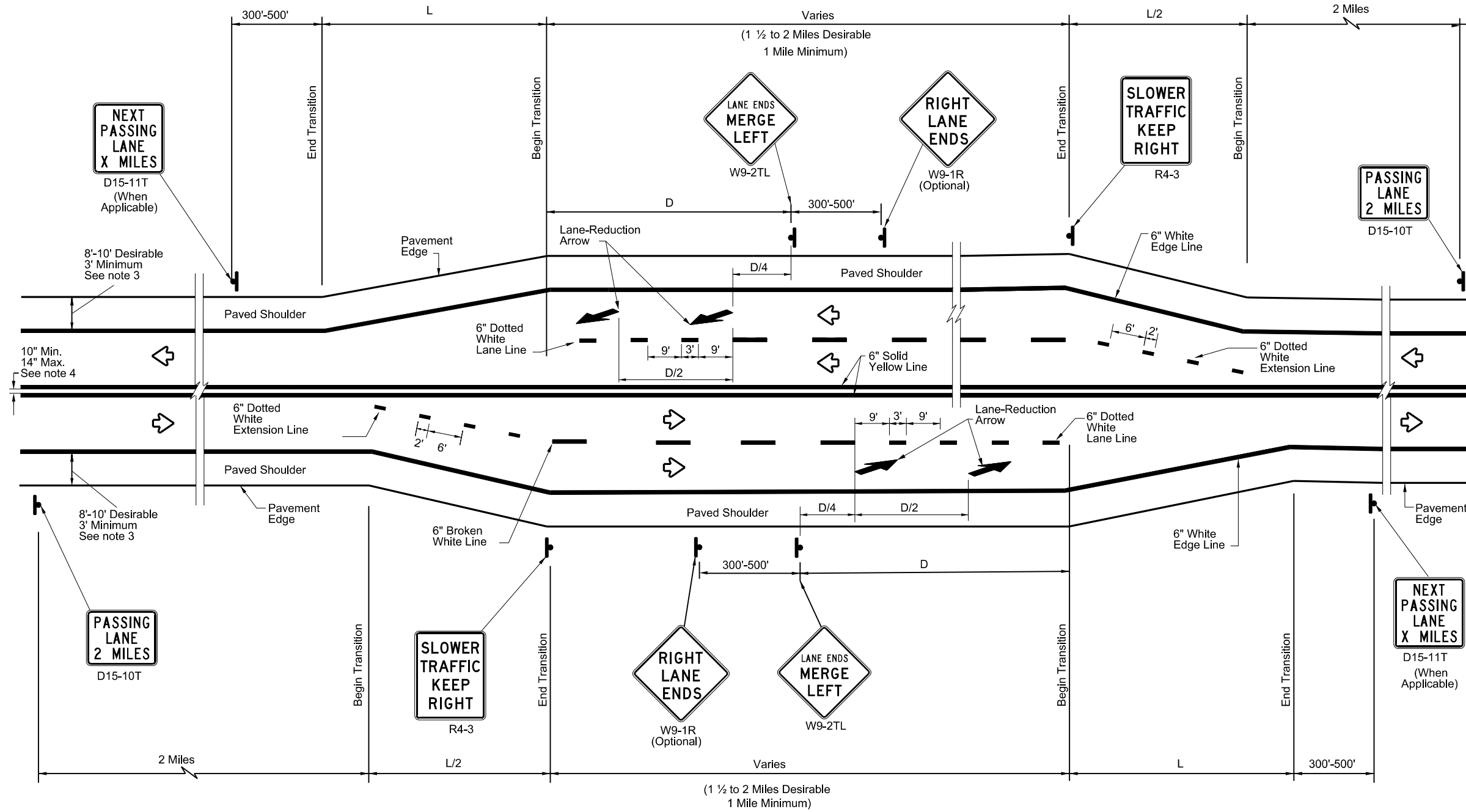
**SH 285
 PAVEMENT MARKING
 LAYOUT**

SCALE: 1"=100' SHEET 22 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		338

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or for any errors or omissions that may appear in this standard or for any damages resulting from its use.

DATE: 2/12/2024 8:31:11 AM
 FILE: p:\twdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan



SIDE BY SIDE PASSING LANES

LEGEND	
	Sign
	Traffic Flow

TYPICAL TAPER LENGTH (L)	
Formula *	$L = WS$

* Transition length should be rounded up to nearest 5 foot increment.

L=Length of Transition (FT)
 W=Width of Offset (FT)
 S=Posted Speed (MPH)

EXAMPLE

A 12 foot lane is added on a 70 mph roadway. The length of the transition should be:

$L = 12 \times 70 = 840 \text{ ft}$

TABLE 1 ADVANCE WARNING SIGN DISTANCE (D)	
Posted Speed	D (FT)
40	670
45	775
50	885
55	990
60	1100
65	1200
70	1250
75	1350

GENERAL NOTES

- For minimum and desirable design details, see the Roadway Design Manual, Chapter 4, Section 6, Super 2 Highways.
- For Raised Pavement Markers (RPM) details, see Pavement Markings Standard sheet, PM(2) - Centerline for All Two Lane Two-Way Roadways. Note that RPMs are not recommended on the 6" dotted white extension lines.
- For rumble strip options available for the designed shoulder width, see Rumble Strip Standard sheet RS(2).
- For pavement marking details, see Pavement Marking Standard sheet PM(1).



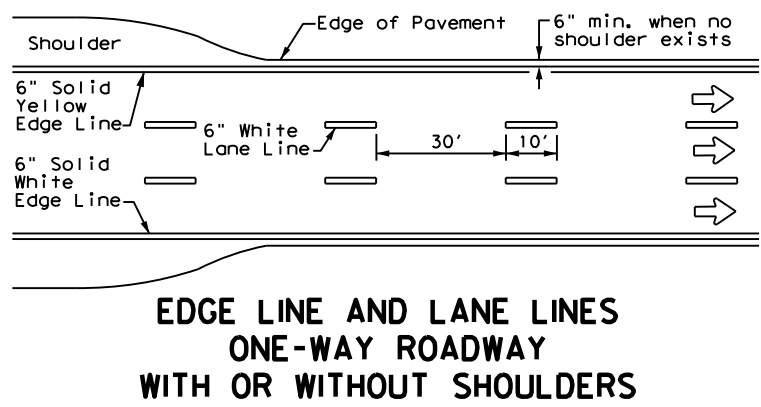
Texas Department of Transportation
 Traffic Safety Division Standard

**TEXAS SUPER 2
PASSING LANES**

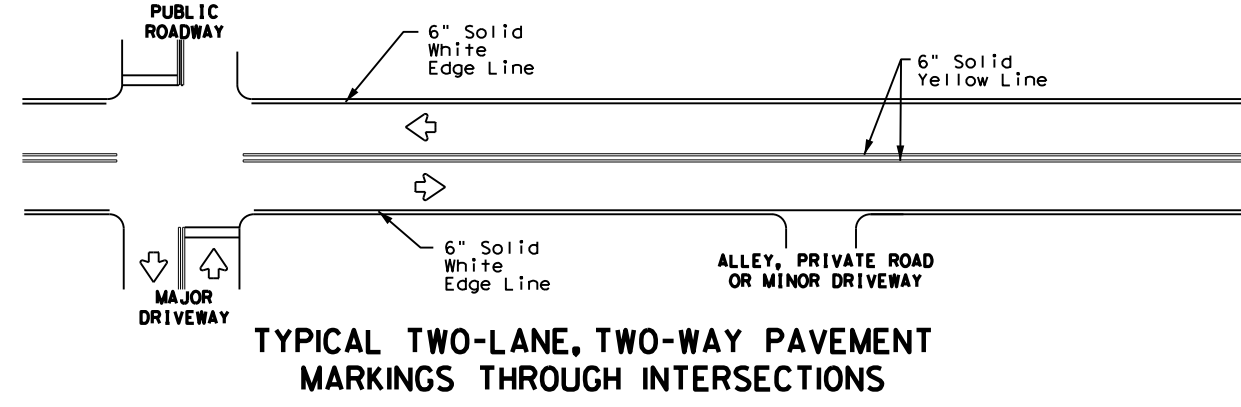
TS2(PL-2)-23

FILE: ts2-23.dgn	DN:	CK:	DW:	CK:
©TxDOT February 2023	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
5-10 3-18	DIST	COUNTY	SHEET NO.	
2-12 2-23	PHR	JIM HOGG	339	
3-12				

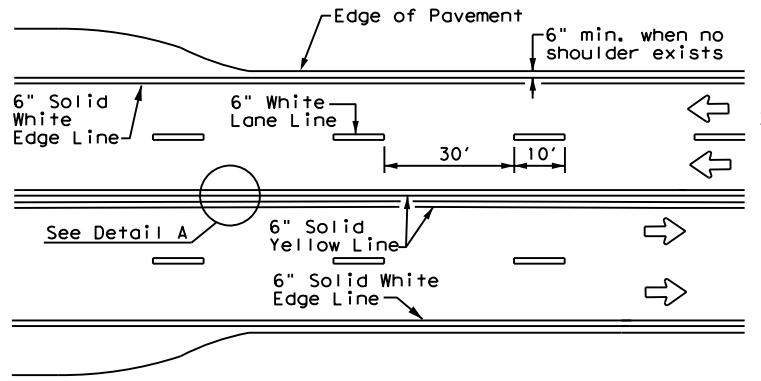
DATE: 2/12/2024 8:31:16 AM
 FILE: \\txdot\project\wiseon\line.com\TXDOTS\Documents\21 - PHR\Design Projects\0482\01\036\etc\pml-22.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of any information from a digital file to a printed format.



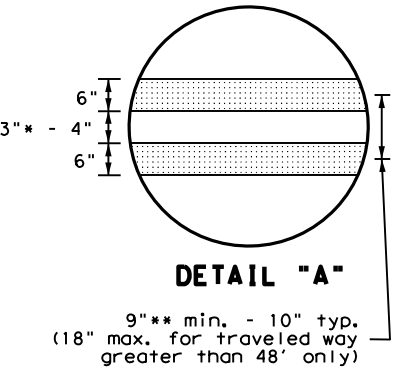
**EDGE LINE AND LANE LINES
ONE-WAY ROADWAY
WITH OR WITHOUT SHOULDERS**



**TYPICAL TWO-LANE, TWO-WAY PAVEMENT
MARKINGS THROUGH INTERSECTIONS**

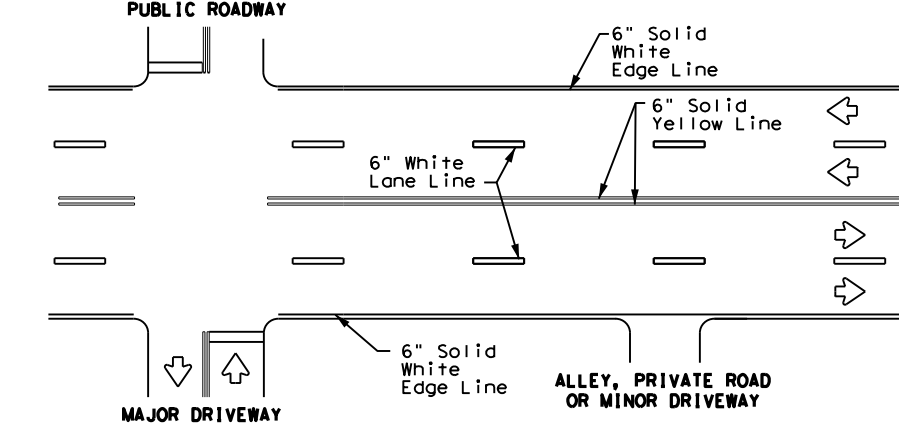


**CENTERLINE AND LANE LINES
FOUR LANE TWO-WAY ROADWAY
WITH OR WITHOUT SHOULDERS**

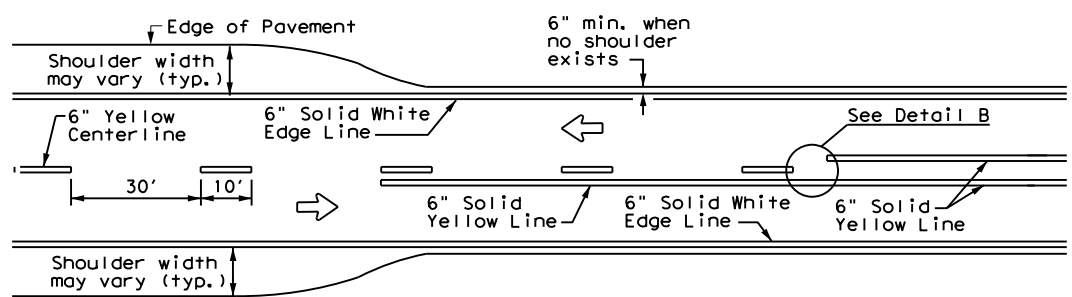


DETAIL "A"
 9" min. - 10" typ.
 (18" max. for traveled way
 greater than 48' only)

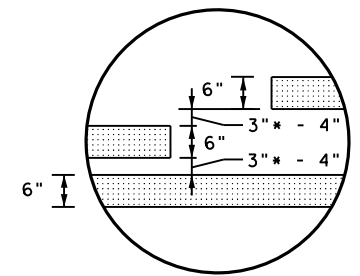
* 2" minimum for restripe projects when approved by the Engineer.
 ** 8" minimum for restripe projects when approved by the Engineer.



**TYPICAL MULTI-LANE, TWO-WAY PAVEMENT
MARKINGS THROUGH INTERSECTIONS**

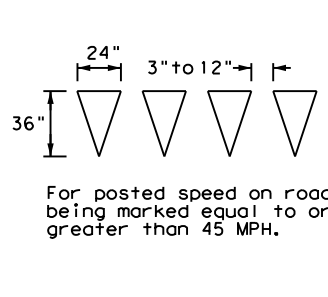


**TWO LANE TWO-WAY ROADWAY
WITH OR WITHOUT SHOULDERS**

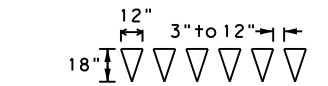


DETAIL "B"
 16" min. - 20" max.
 (16" minimum for restripe projects
 when approved by the Engineer.)

* 2" minimum for restripe projects when approved by the Engineer.



YIELD LINES

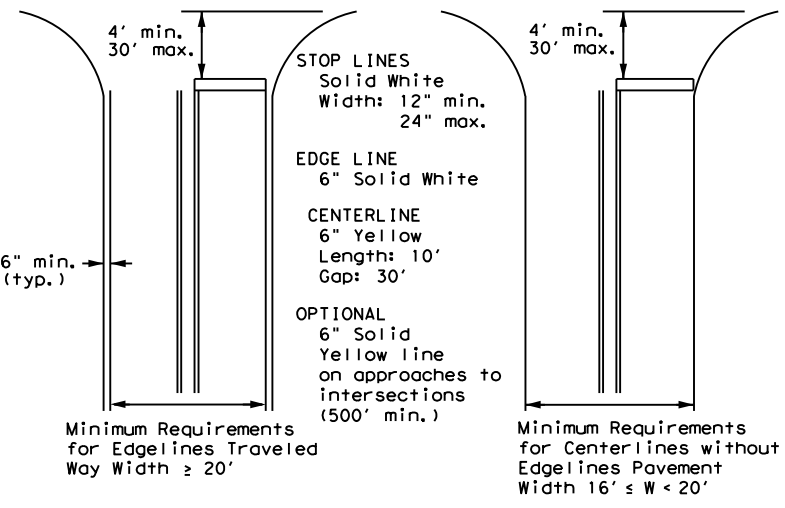


For posted speed on road being marked equal to or less than 40 MPH.

- GENERAL NOTES**
- Edge line striping shall be as shown in the plans or as directed by the Engineer. The edge line should not be placed less than 6 inches from the edge of pavement. This distance may vary due to pavement raveling or other conditions. Edge lines are not required in curb and gutter sections of roadways.
 - The traveled way includes only that portion of the roadway used for vehicular travel. It does not include the parking lanes, sidewalks, berms and shoulders. The traveled ways shall be measured from the center of edge line to the center of edge line of a two lane roadway.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.

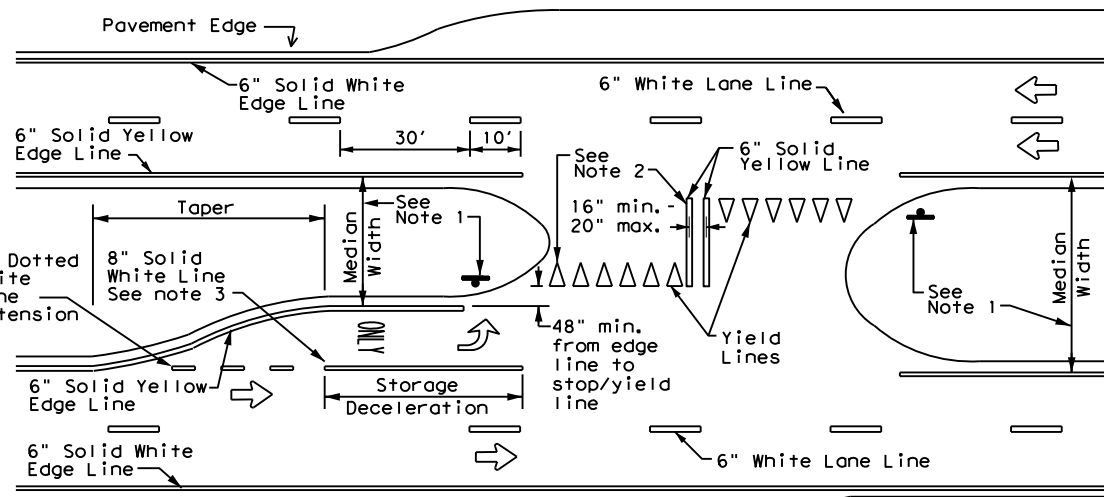


NOTE: Traveled way is exclusive of shoulder widths. Refer to General Note 2 for additional details.

**GUIDE FOR PLACEMENT OF STOP LINES,
EDGE LINE & CENTERLINE**
 Based on Traveled Way and Pavement Widths
 for Undivided Roadways

NOTES

- Where divided highways are separated by median widths at the median opening itself of 30 feet or more, median openings shall be signed as two separate intersections. Each median opening has two width measurements, with one measurement for each approach. The narrow median width will be the controlling width to determine if signs are required. Yield signs are the typical intersection control. Stop signs and stop bars are optional as determined by the Engineer.
- Install median striping (double yellow centerlines and stop lines/yield lines) when a 50' or greater median centerline can be placed. Stop lines shall only be used with stop signs. Yield lines shall only be used with yield signs.
- Length of turn bays, including taper, deceleration, and storage lengths shall be as shown on the plans or as directed by the Engineer.



FOUR LANE DIVIDED ROADWAY CROSSOVERS

Texas Department of Transportation
 Traffic Safety Division Standard

**TYPICAL STANDARD
PAVEMENT MARKINGS**

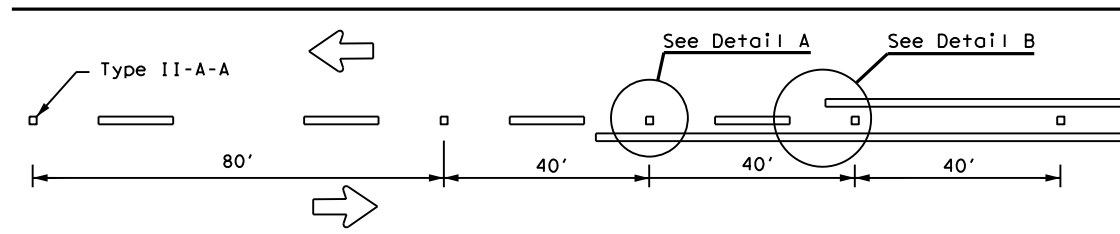
PM(1) - 22

FILE: pml-22.dgn	DN:	CK:	DW:	CK:
© TxDOT December 2022	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
11-78 8-00 6-20	DIST	COUNTY	SHEET NO.	
8-95 3-03 12-22	PHR	JIM HOGG	340	
5-00 2-12				

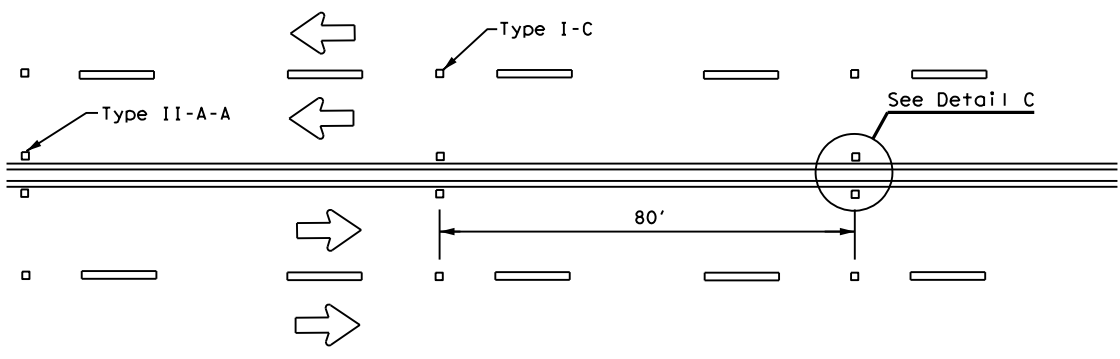
22A

REFLECTIVE RAISED PAVEMENT MARKERS FOR VEHICLE POSITIONING GUIDANCE

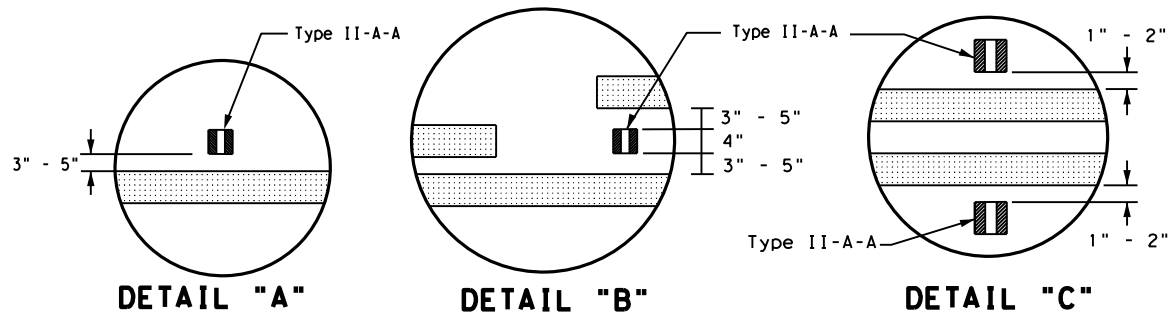
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to metric units. The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to metric units.



CENTERLINE FOR ALL TWO LANE TWO-WAY ROADWAYS



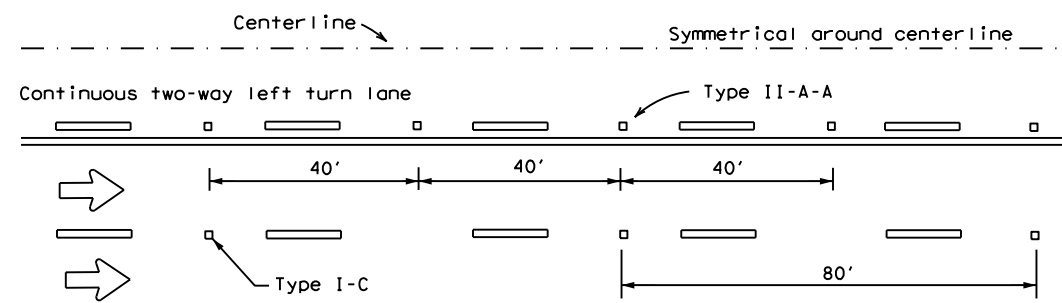
**CENTERLINE & LANE LINES
FOR FOUR LANE TWO-WAY ROADWAYS**



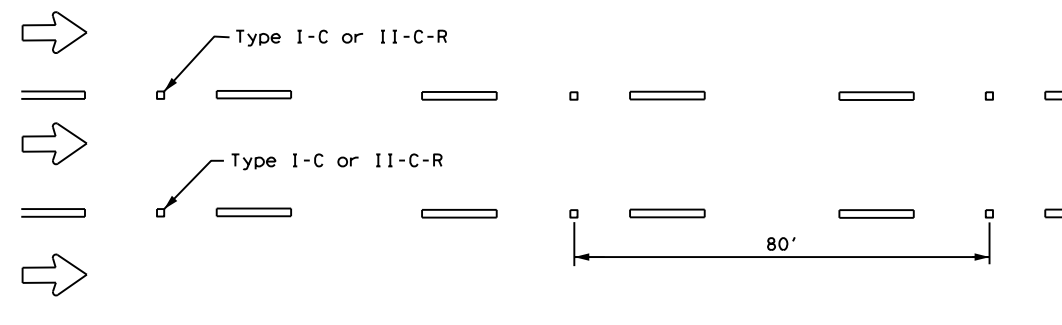
DETAIL "A"

DETAIL "B"

DETAIL "C"

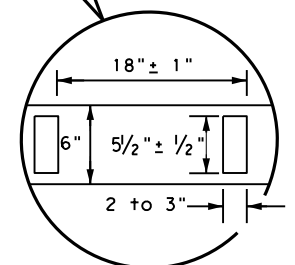
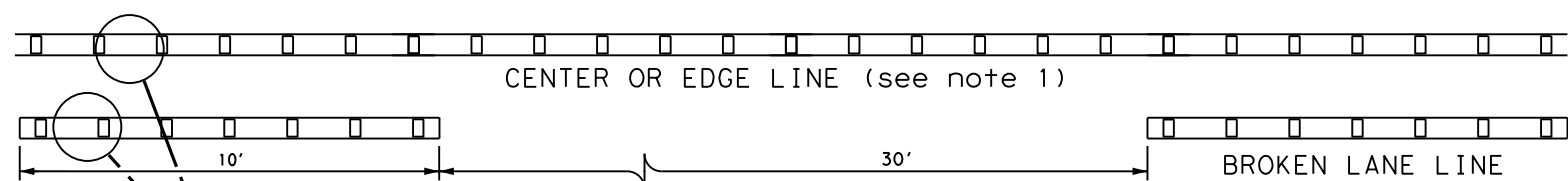


CENTERLINE AND LANE LINES FOR TWO-WAY LEFT TURN LANE



LANE LINES FOR ONE-WAY ROADWAY (NON-FREEWAY FACILITIES)

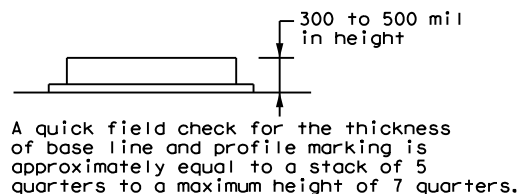
Raised pavement markers Type II-C-R shall have clear face toward normal traffic and red face toward wrong-way traffic.
 See Note 3.



**REFLECTORIZED PROFILE
PATTERN DETAIL**

USING REFLECTIVE PROFILE PAVEMENT MARKINGS

6" EDGE LINE, 6" CENTERLINE
OR 6" LANE LINE



A quick field check for the thickness of base line and profile marking is approximately equal to a stack of 5 quarters to a maximum height of 7 quarters.

NOTES

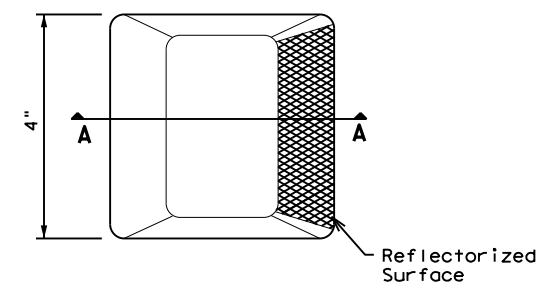
1. Edge lines should typically be 6" wide and the materials shall be specified in the plans.
2. Profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.

GENERAL NOTES

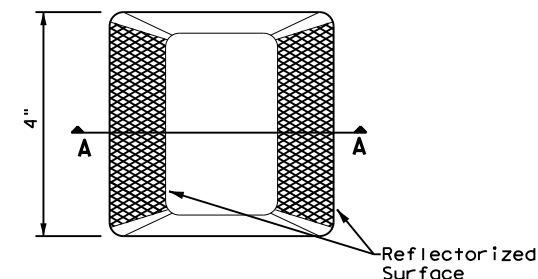
1. All raised pavement markers placed along broken lines shall be placed in line with and midway between the stripes.
2. On concrete pavements the raised pavement markers should be placed to one side of the longitudinal joints.
3. Use raised pavement marker Type I-C with undivided roadways, flush medians and two way left turn lanes. Use raised pavement marker Type II-C-R with divided highways and raised medians.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

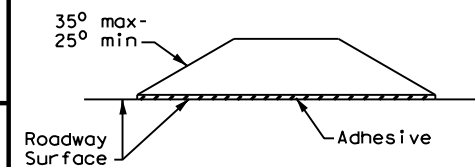
All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.



Type I (Top View)



Type II (Top View)



SECTION A

RAISED PAVEMENT MARKERS

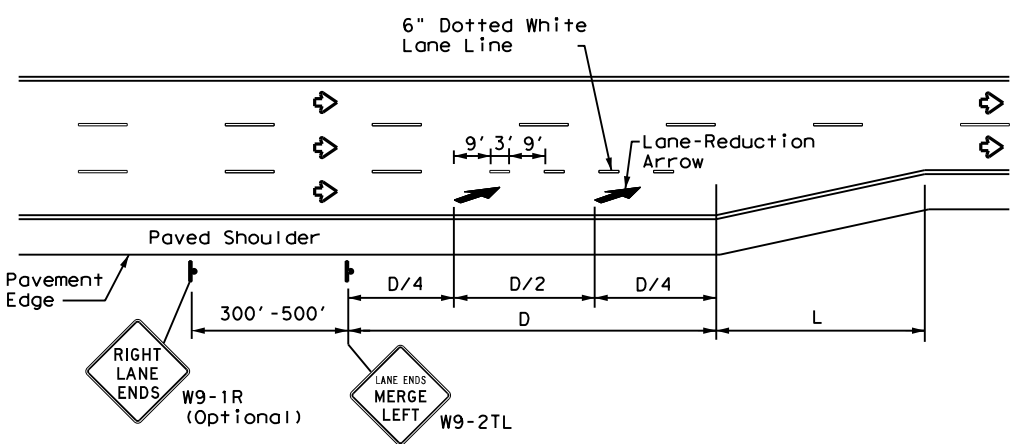


**POSITION GUIDANCE USING
RAISED MARKERS
REFLECTORIZED PROFILE
MARKINGS
PM(2) - 22**

FILE: pm2-22.dgn	DN:	CK:	DW:	CK:
© TxDOT December 2022	CONT	SECT	JOB	HIGHWAY
4-77 8-00 6-20	0482	01	036, etc.	SH 285
4-92 2-10 12-22	DIST	COUNTY		SHEET NO.
5-00 2-12	PHR	JIM HOGG		341

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein.

DATE: 2/12/2024 8:31:24 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT\Documents\21 - PHR\Design Projects\036\036-001\036-001.dgn



LANE REDUCTION

NOTES

- Lane reduction pavement markings are used where the number of through lanes is reduced because of narrowing of the roadway or because of a section of on-street parking in what would otherwise be a through lane. For Texas Super 2 Passing Lanes, see TS2(PL) standard sheets.
- On divided highways, an additional RIGHT LANE ENDS (W9-1R) sign may be installed in the median aligned with the W9-1R sign on the right side of the highway.
- Lane reduction arrows are required for speeds of 45 mph or greater. An optional third lane reduction arrow may be added based on engineering judgement. If used, the optional third lane reduction arrow should be centered between the first and last lane reduction arrows.
- For lane reductions on Freeways and Expressways, signing shall conform to the TxDOT Freeway Signing Handbook.

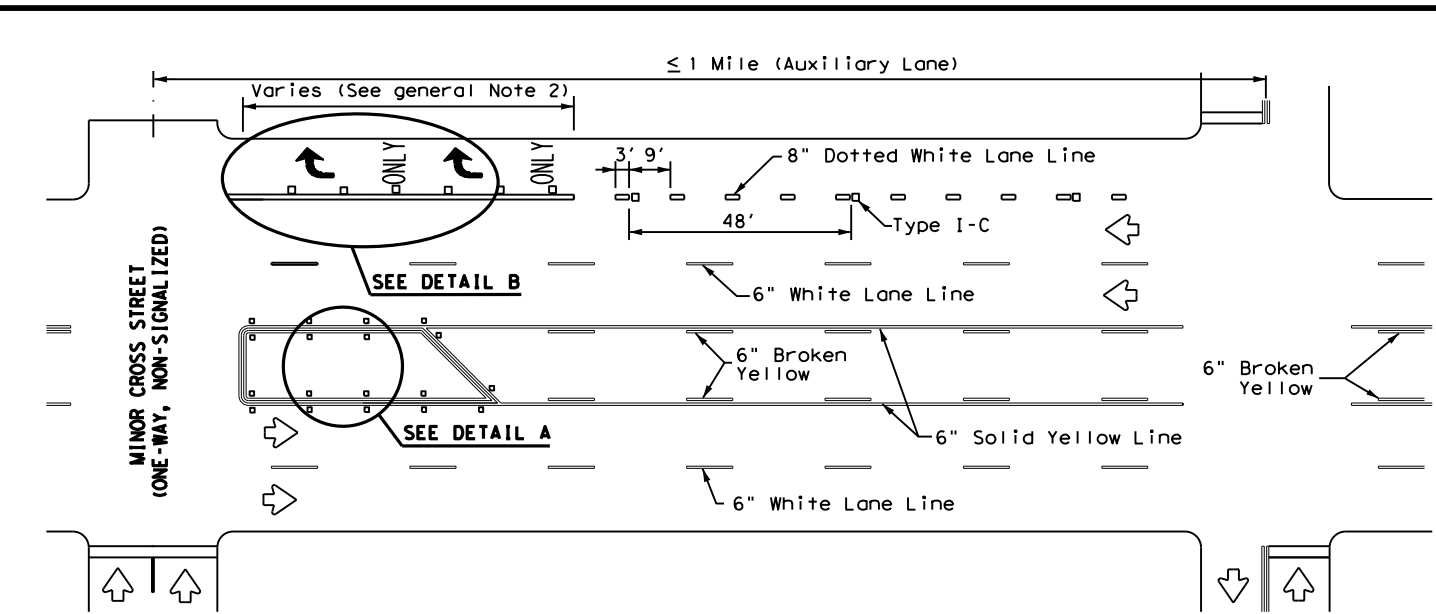
ADVANCED WARNING SIGN DISTANCE (D)		
Posted Speed	D (ft)	L (ft)
30 MPH	460	$L = \frac{WS^2}{60}$
35 MPH	565	
40 MPH	670	
45 MPH	775	L=WS
50 MPH	885	
55 MPH	990	
60 MPH	1,100	
65 MPH	1,200	
70 MPH	1,250	
75 MPH	1,350	

GENERAL NOTES

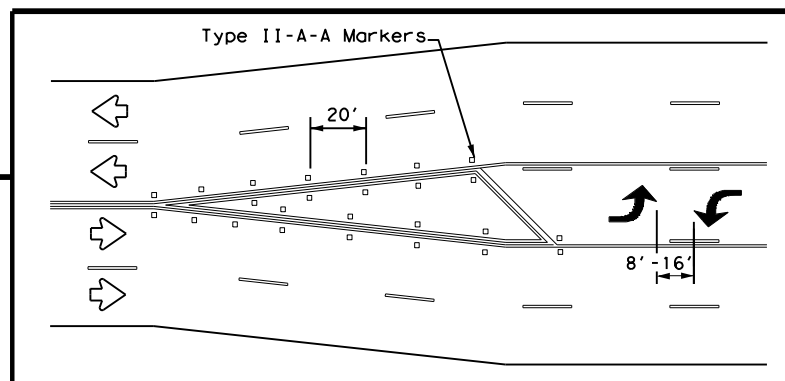
- Lane use word and arrow markings shall be used where through lanes approaching an intersection become mandatory turn lanes. Lane use word and arrow markings should be used in auxiliary lanes of substantial length. Lane use arrow markings or word and arrow markings may be used in other lanes and turn bays for emphasis. Details for words and arrows are as shown in the Standard Highway Sign Designs for Texas.
- When lane-use words and arrow markings are used, two sets of arrows should be used if the length of the bay is greater than 180 feet. When a single lane use arrow or word and arrow marking is used for a short turn lane, it should be located at or near the upstream end of the full-width turn lane.
- Use raised pavement marker Type I-C with undivided highways, flush medians and two way left turn lanes. Use raised pavement marker Type II-C-R with divided highways and raised medians.
- Length of turn bays, including taper, deceleration, and storage lengths shall be as shown on the plans or as directed by the Engineer. See Chapter 3 of the Roadway Design Manual for additional information on turning lanes or storage lengths.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

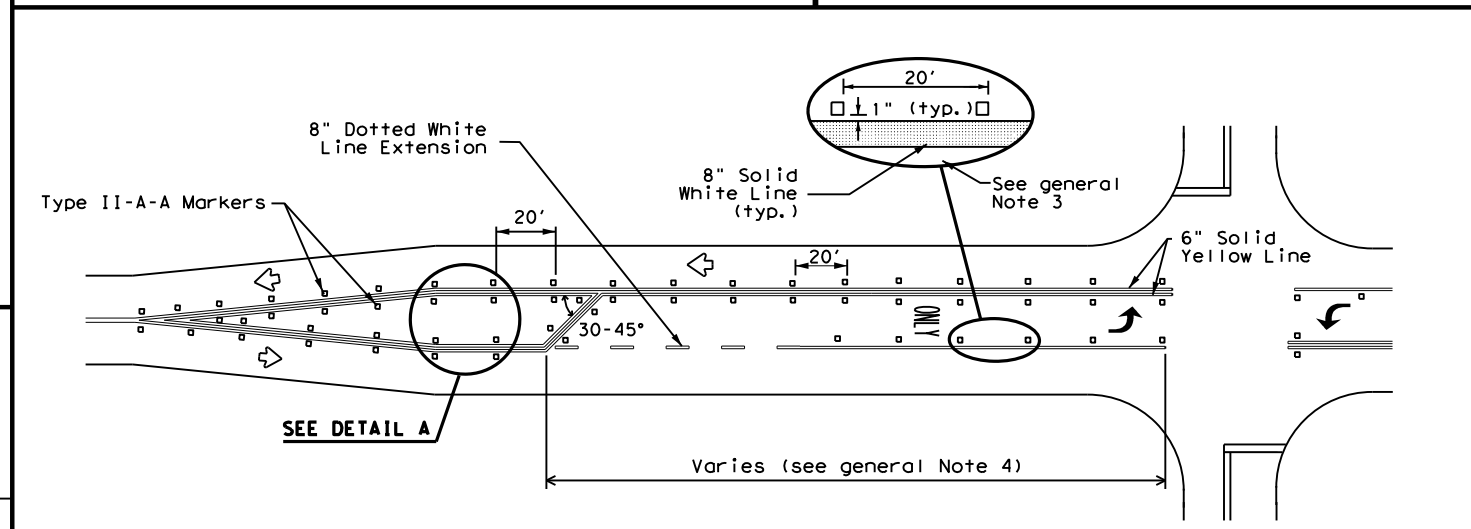
All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.



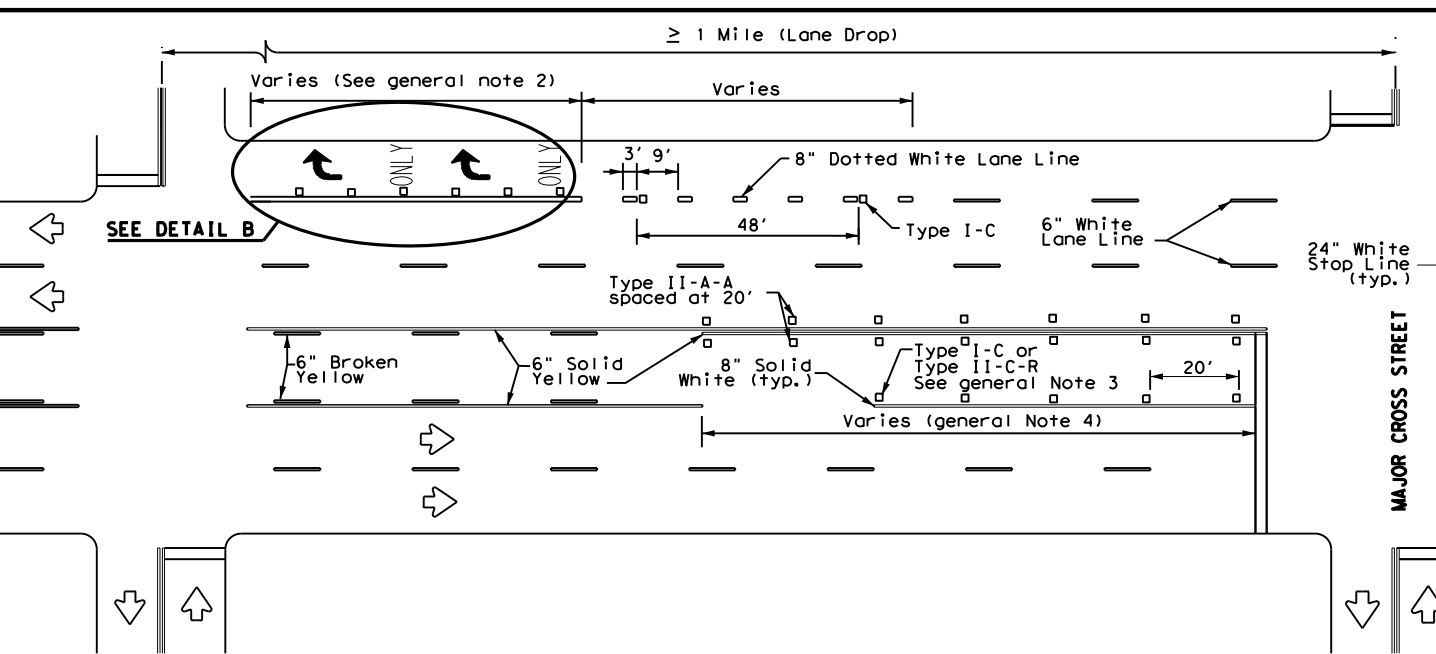
TYPICAL TWLTL AT ONE-WAY STREET AND RIGHT TURN AUXILIARY LANE



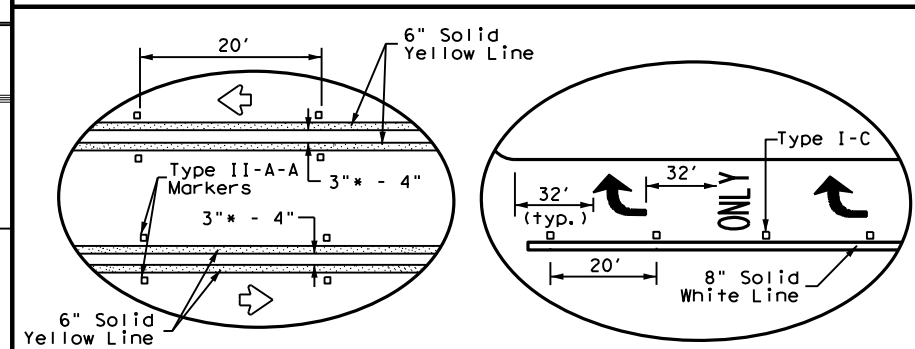
TYPICAL TRANSITION FOR TWLTL AND DIVIDED HIGHWAY



TYPICAL TWO-LANE ROADWAY INTERSECTION WITH LEFT TURN BAYS



TYPICAL TWLTL AT TWO-WAY CROSS STREET AND RIGHT TURN LANE DROP



DETAIL A

DETAIL B

* 2" minimum allowed for restripe projects when approved by the Engineer.

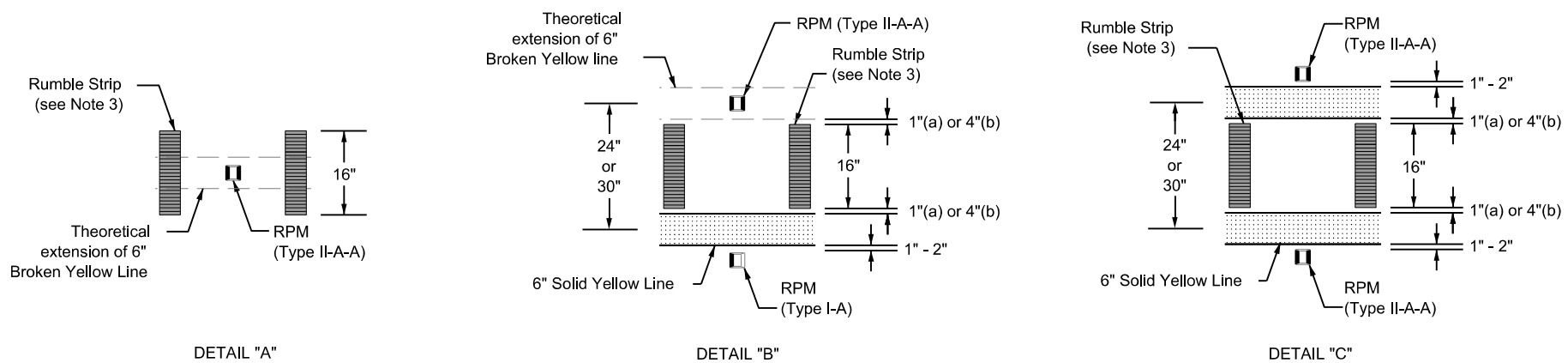
Texas Department of Transportation
 Traffic Safety Division Standard

**TWO-WAY LEFT TURN LANES,
 RURAL LEFT TURN BAYS,
 AND LANE REDUCTION
 PAVEMENT MARKINGS
 PM(3) - 22**

FILE: pm3-22.dgn	DN:	CK:	DW:	CK:
© TxDOT December 2022	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285
4-98 3-03 6-20	DIST	COUNTY	SHEET NO.	
5-00 2-10 12-22	PHR	JIM HOGG	342	
8-00 2-12				

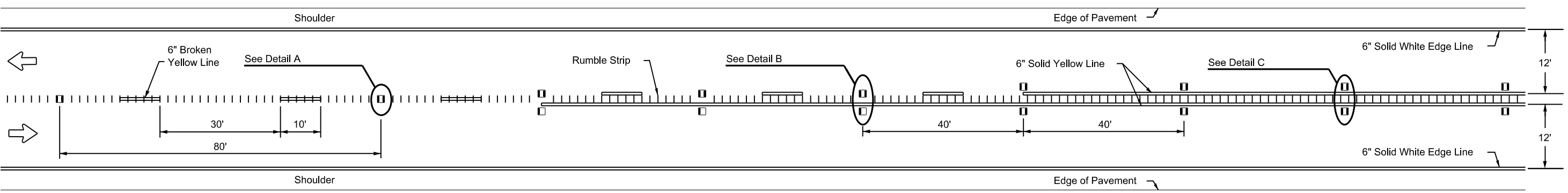
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: FILE:

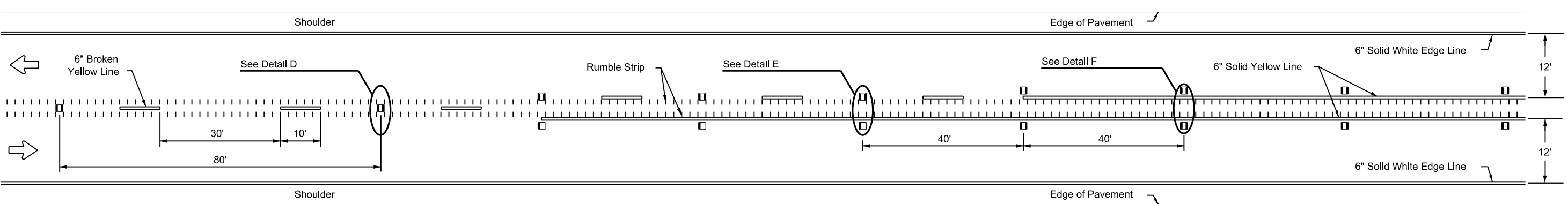


GENERAL NOTES:

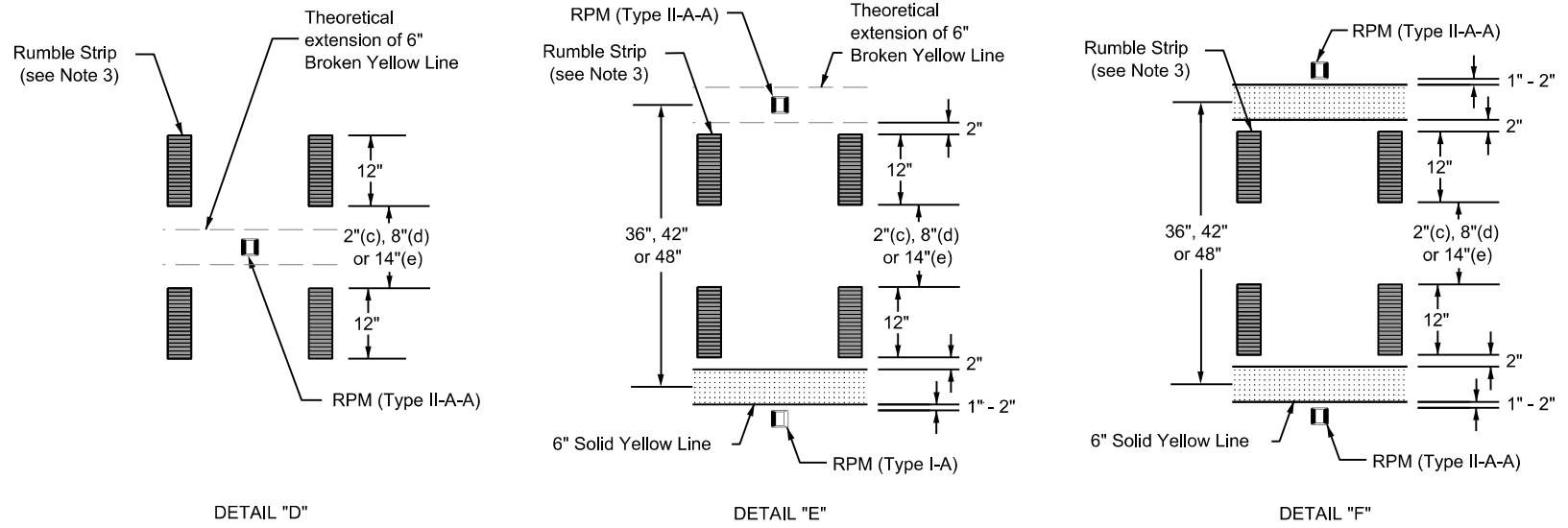
1. A buffer shall not be implemented if it will require the width of travel lanes to be less than 12 feet.
2. See standard sheet PM(2) for additional details regarding retroreflectorized raised pavement markers (RPMs).
3. This sheet shows the application of milled rumble strips, though other types may be used. See the Rumble Strips (RS) standard for installation details.
4. Dimension notations (a) through (e) correspond to the following buffer widths: a = 24 inches; b = 30 inches; c = 36 inches; d = 42 inches; and e = 48 inches.
5. The Engineer must consider bicycle accommodation during the planning and implementation of all construction and rehabilitation projects. See standard sheet RS(6) and the TxDOT Roadway Design Manual (RDM) Bicycle Facilities section for applicable policies, references and guidance.



CENTERLINE BUFFER FOR TWO-LANE UNDIVIDED ROADWAYS
 FOR BUFFER WIDTHS OF 24 INCHES(a) or 30 INCHES(b)



WIDE CENTERLINE BUFFER FOR TWO-LANE UNDIVIDED ROADWAYS
 FOR BUFFER WIDTHS OF 36 INCHES(c), 42 INCHES(d) OR 48 INCHES(e)



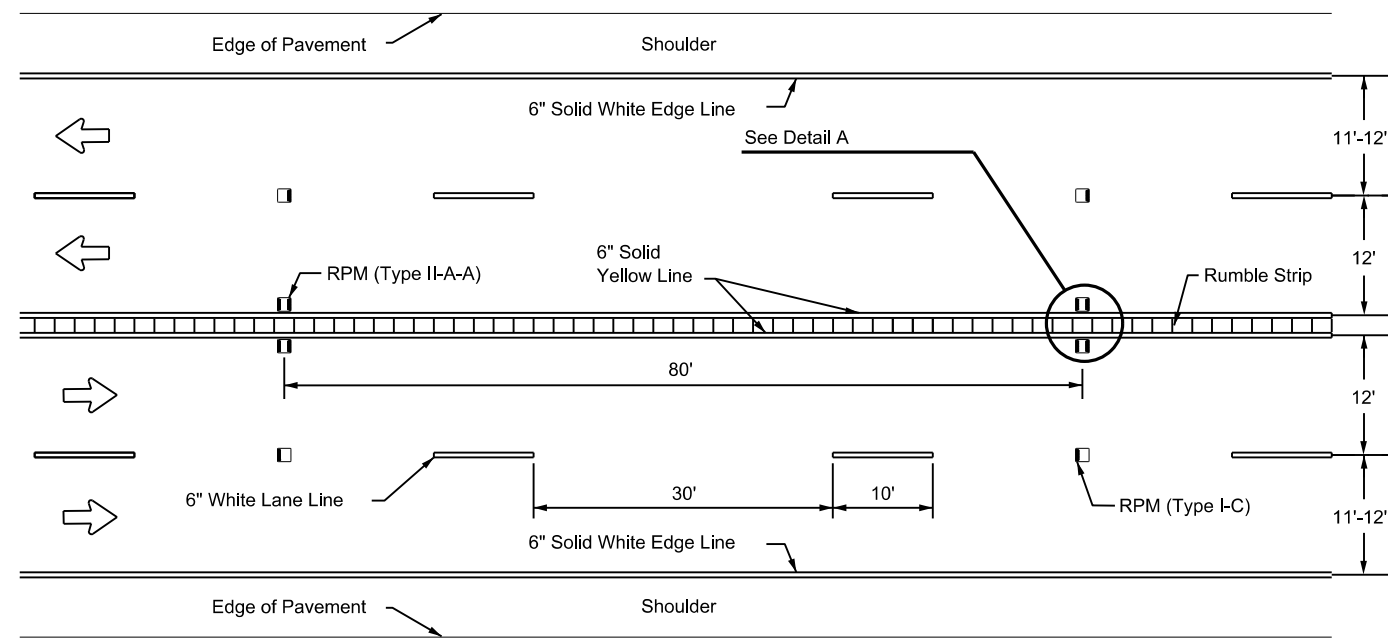
				Traffic Safety Division Standard	
CENTERLINE BUFFER TWO-LANE ROADWAYS					
CLB(2)-23					
FILE:	clb2-23.dgn	DN:	CK:	DW:	CK:
© TxDOT	September 2023	CONT	SECT	JOB	HIGHWAY
REVISIONS	0482	01	036, etc.	SH 285	
	DIST	COUNTY		SHEET NO.	
	PHR	JIM HOGG		343	

MATERIAL SPECIFICATIONS	
Pavement Markers (ReflectORIZED)	DMS-4200
Epoxies and Adhesives	DMS-6100
Bituminous Adhesive for Pavement Markers	DMS-6130
Traffic Paint	DMS-8200
Hot Applied Thermoplastic	DMS-8220
Permanent Prefabricated Pavement Markings	DMS-8240

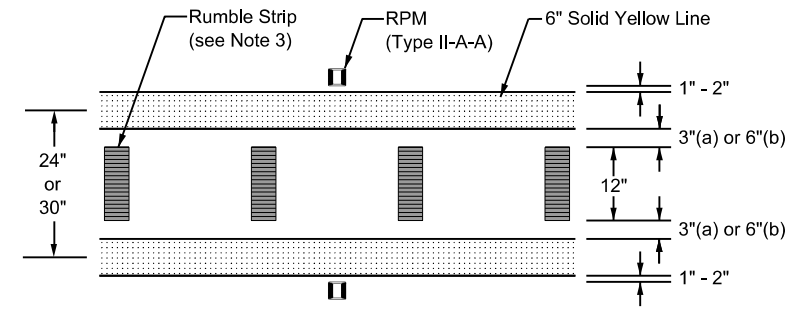
All pavement marking materials shall meet the required Departmental Material Specifications.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information presented in this standard.

DATE: 2/12/2024 8:31:33 AM
 FILE: p:\dot\projectwiseonline.com\TXDOT\5\Documents\21 - PHRD\Design Projects\04820103614\CLB(1)-23.dgn



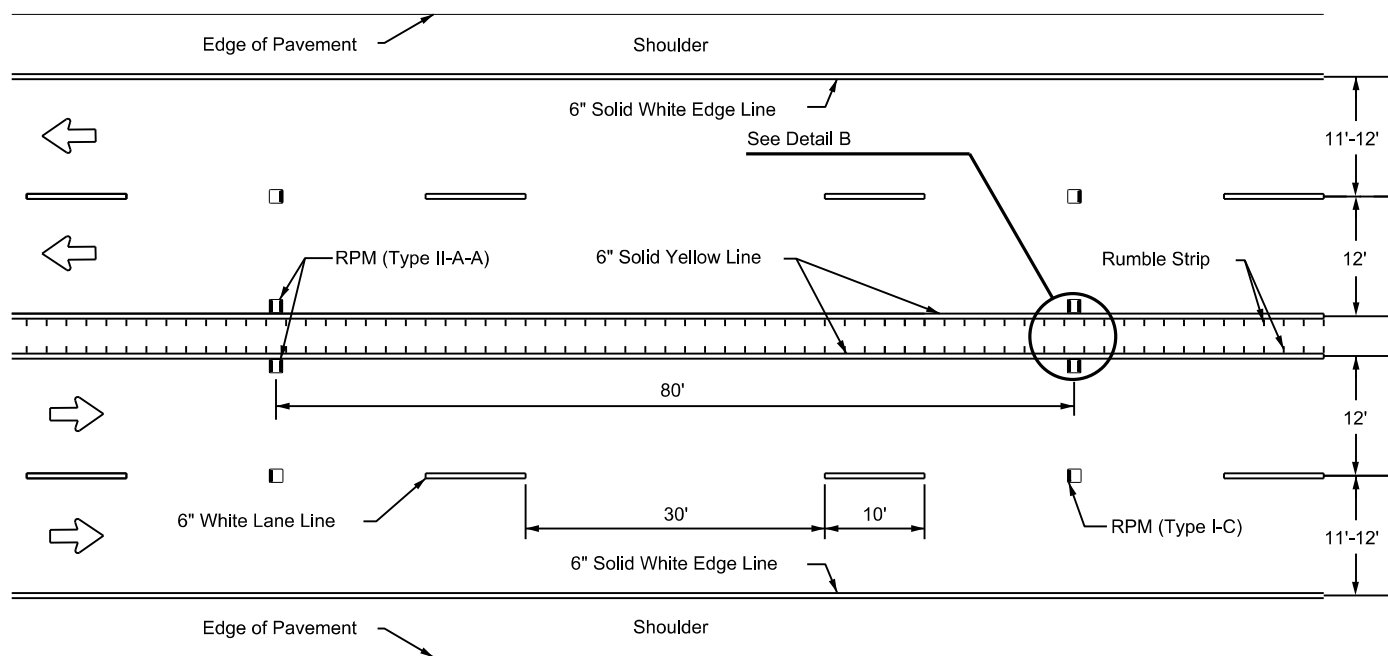
CENTERLINE BUFFER FOR MULTI-LANE UNDIVIDED ROADWAYS
 FOR BUFFER WIDTHS OF 24 INCHES(a) OR 30 INCHES(b)



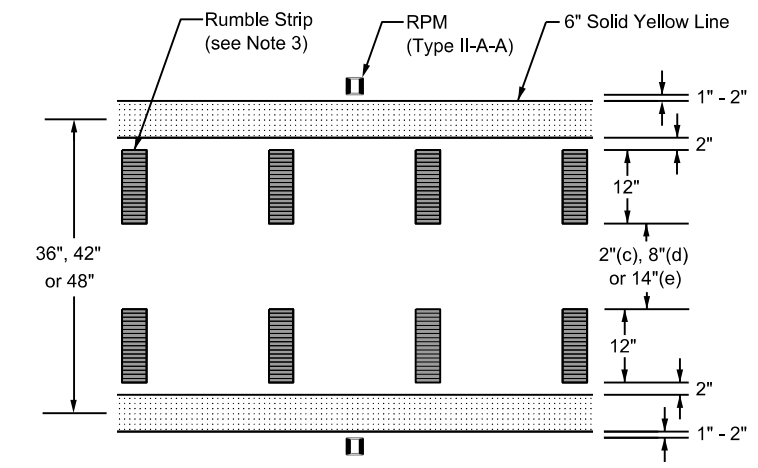
DETAIL "A"

GENERAL NOTES:

1. A buffer shall not be implemented if it will require reducing the width of inside travel lanes to be less than 12 feet.
2. See standard sheet PM(2) for additional details regarding retroreflectorized raised pavement markers (RPMs).
3. This sheet shows the application of milled rumble strips, though other types may be used. See the Rumble Strips (RS) standard for installation details.
4. Dimension notations (a) through (e) correspond to the following buffer widths: a = 24 inches; b = 30 inches; c = 36 inches; d = 42 inches; and e = 48 inches.
5. The Engineer must consider bicycle accommodation during the planning and implementation of all construction and rehabilitation projects. See standard sheet RS(6) and the TxDOT Roadway Design Manual (RDM) Bicycle Facilities section for applicable policies, references and guidance.



WIDE CENTERLINE BUFFER FOR MULTI-LANE UNDIVIDED ROADWAYS
 FOR BUFFER WIDTHS OF 36 INCHES(c), 42 INCHES(d) OR 48 INCHES(e)



DETAIL "B"

MATERIAL SPECIFICATIONS	
Pavement Markers (Reflectorized)	DMS-4200
Epoxies and Adhesives	DMS-6100
Bituminous Adhesive for Pavement Markers	DMS-6130
Traffic Paint	DMS-8200
Hot Applied Thermoplastic	DMS-8220
Permanent Prefabricated Pavement Markings	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications.



**CENTERLINE BUFFER
MULTI-LANE ROADWAYS**

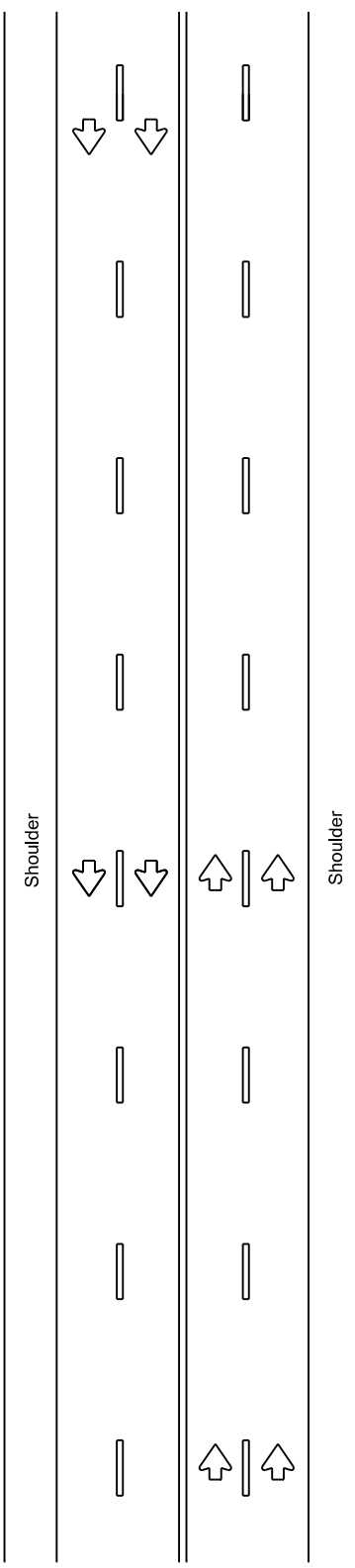
CLB(1)-23

FILE: clb1-23.dgn	DN:	CK:	DW:	CK:
© TxDOT	September 2023	CONT	SECT	JOB
		0482	01	036, etc.
		DIST	COUNTY	SHEET NO.
		PHR	JIM HOGG	344

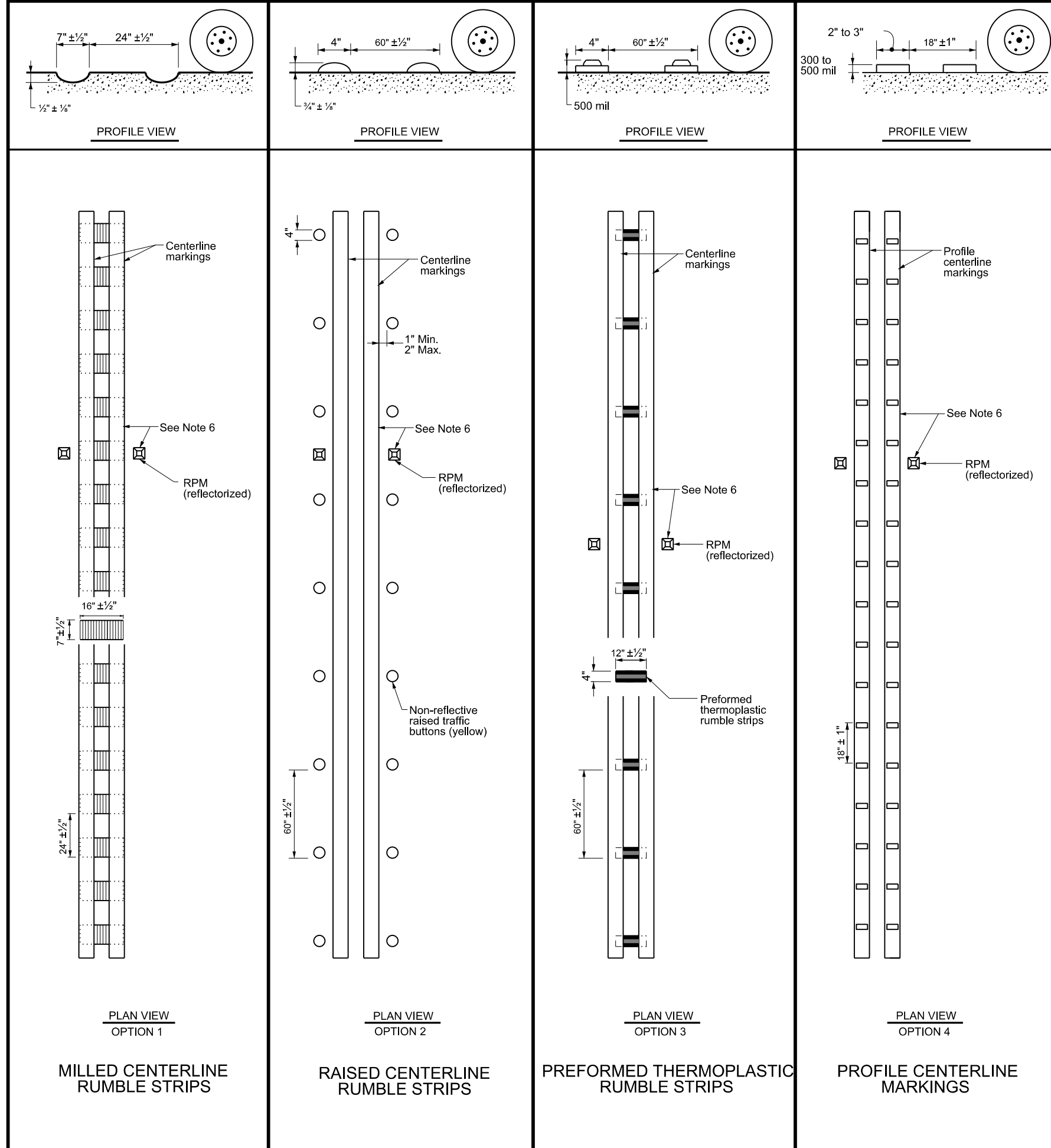
DATE: 2/12/2024 8:31:37 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\2020 to 2024\Design\PHR\Standards\rs(3)-23.dgn

DISCLAIMER:
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for correct results or damages resulting from its use.

MULTILANE UNDIVIDED HIGHWAY WITH SHOULDER



CENTERLINE RUMBLE STRIPS



GENERAL NOTES

1. This standard sheet provides guidelines for installing centerline rumble strips on multilane undivided highways.
2. Centerline and edge line rumble strips or profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.
3. Milled rumble strips are preferred when adequate pavement depth is available. If pavement thickness is less than 2 inches, milled rumble strips shall not be used. Rumble strips shall not be milled or depressed into bridge decks.
4. See dimensions for milled rumble strips. Other shapes and dimensions may be used if approved by the Traffic Safety Division.
5. Breaks in milled centerline rumble strips shall occur at least 50 feet and no more than 150 feet in advance of bridges, railroad crossing, intersections or driveways with high usage of large trucks.
6. Use standard sheet PM(2) for positioning, dimensioning, and spacing of all reflective raised pavement markers, pavement markings and profile markings.
7. Consideration should be given to noise levels when centerline rumble strips are to be installed near residential areas, schools, churches, etc. A 3/8 inch deep (minimum) milled rumble strip may be considered in these areas.
8. Pavement markings must be applied over milled centerline rumble strips for normal centerline spacing. For wider medians, specify in the plans the exact placement of the rumble strips. Place the rumble strips under each centerline marking or centered in the middle of the median.

WHEN INSTALLING CENTERLINE RUMBLE STRIPS:

9. Raised rumble strips consisting of non-reflective raised traffic buttons may be used. Non-reflective raised traffic buttons can be affixed to asphalt or concrete with bitumen or adhesives, as per manufacturer's recommendations.
10. When using non-reflective raised traffic buttons as a centerline rumble strip, the button shall be placed adjacent to the pavement marking delineating the centerline. The color of the button should be yellow for a continuous no passing roadway. The button will be paid for under Item 672, "Raised Pavement Markers." Non-reflective traffic buttons must meet the requirements of DMS-4300.
11. Consideration shall be given to bicyclists. See RS(6).

WHEN INSTALLING EDGE LINE RUMBLE STRIPS WITH OR WITHOUT CENTERLINE RUMBLE STRIPS ON UNDIVIDED HIGHWAYS:

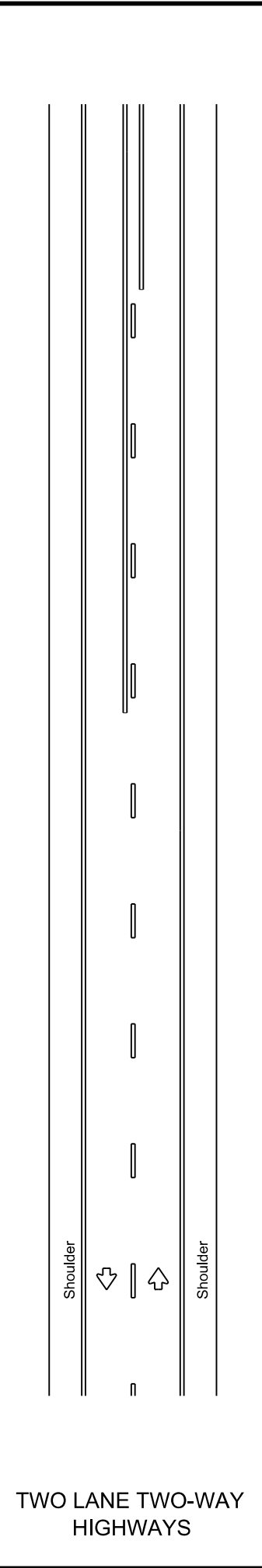
12. See standard sheet RS(2).

Texas Department of Transportation
 Traffic Safety Division Standard

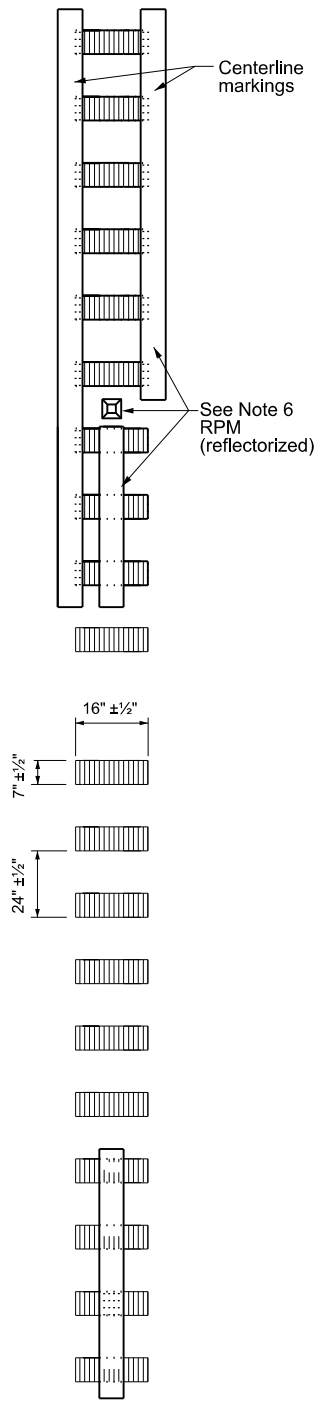
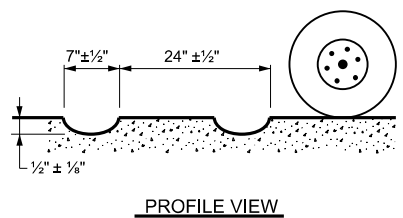
CENTERLINE RUMBLE STRIPS ON MULTILANE UNDIVIDED HIGHWAYS RS(3)-23

FILE:	rs(3)-23.dgn	DWG:	TxDOT	CHK:	TxDOT	DWG:	TxDOT	CHK:	TxDOT
© TxDOT	January 2023	CONT:	0482	SECT:	01	JOB:	036, etc.	HIGHWAY:	SH 285
10-13	1-23	DIST:	PHR	COUNTY:	JIM HOGG	SHEET NO.:	345		

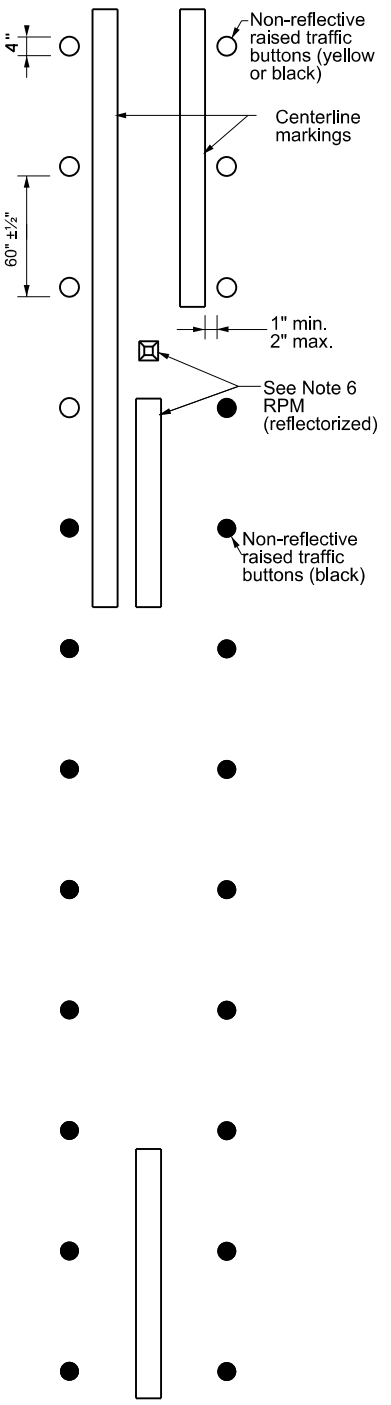
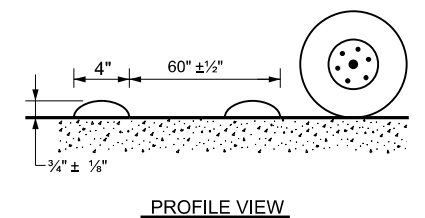
DATE: 2/12/2024 8:31:41 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\042010364 Design\042010364 Pavement Marking Standards\rs(4)-23.dgn



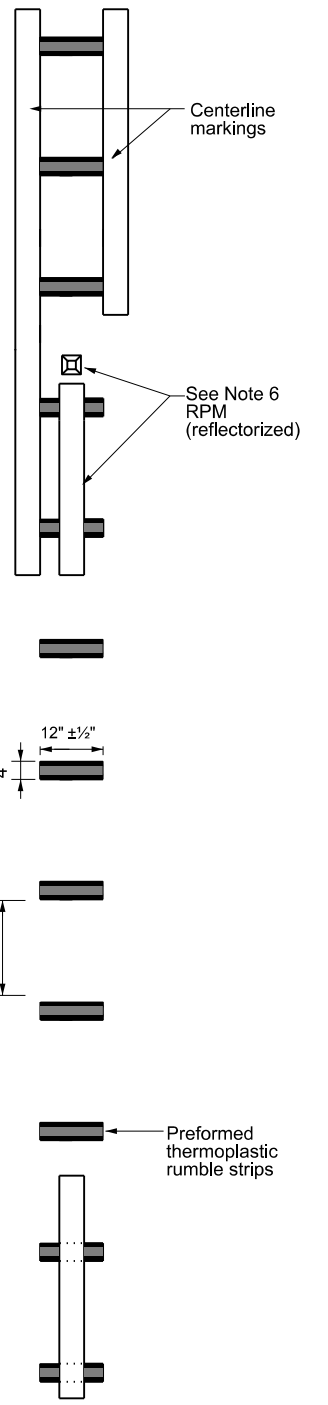
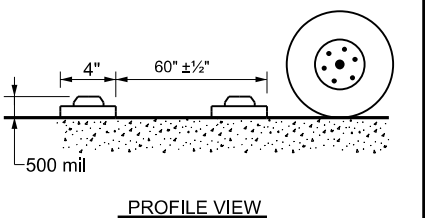
CENTERLINE RUMBLE STRIPS



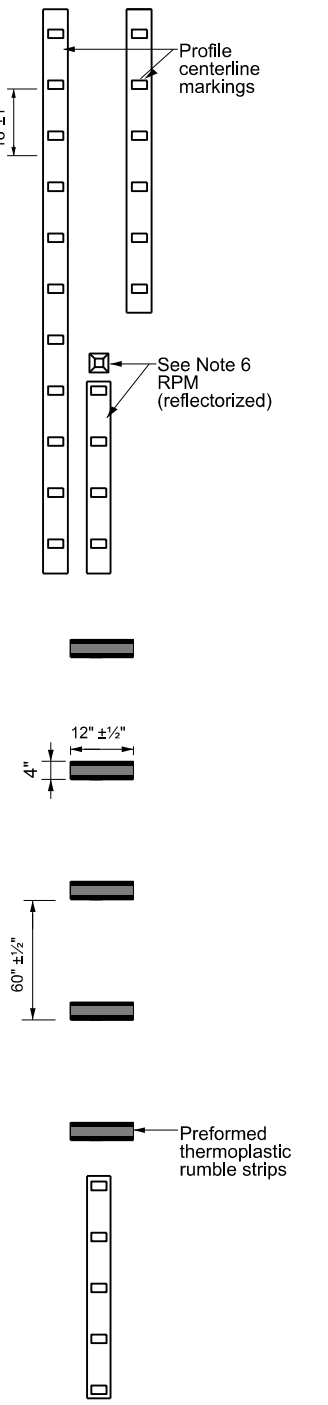
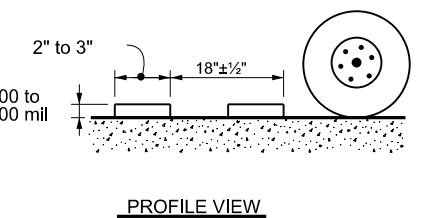
MILLED CENTERLINE RUMBLE STRIPS



RAISED CENTERLINE RUMBLE STRIPS



PREFORMED THERMOPLASTIC RUMBLE STRIPS



PROFILE CENTERLINE MARKINGS AND PREFORMED THERMOPLASTIC RUMBLE STRIPS

GENERAL NOTES

- This standard sheet provides guidelines for installing centerline rumble strips on two-lane highways with or without shoulders.
- Centerline and edge line rumble strips or profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.
- Milled rumble strips are preferred when adequate pavement depth is available. If pavement thickness is less than 2 inches, milled rumble strips shall not be used. Rumble strips shall not be milled or depressed into bridge decks.
- See dimensions for milled rumble strips. Other shapes and dimensions may be used if approved by the Traffic Safety Division.
- Breaks in milled centerline rumble strips shall occur at least 50 feet and no more than 150 feet in advance of bridges, railroad crossings, intersections or driveways with high usage of large trucks.
- Use standard sheet PM(2) for positioning, dimensioning, and spacing of all reflective raised pavement markers, pavement markings and profile markings.
- Consideration should be given to noise levels when centerline rumble strips are to be installed near residential areas, schools, churches, etc. A 3/8 inch deep (minimum) milled rumble strip may be considered in these areas.
- Pavement markings must be applied over milled centerline rumble strips.

WHEN INSTALLING CENTERLINE RUMBLE STRIPS:

- Raised rumble strips consisting of non-reflective raised traffic buttons may be used. Non-reflective raised traffic buttons can be affixed to asphalt or concrete with bitumen or adhesives, as per manufacturer's recommendations.
- When using non-reflective raised traffic buttons as a centerline rumble strip, the button shall be placed adjacent to the pavement marking delineating the centerline. The buttons will be paid for under Item 672, "Raised Pavement Markers." Non-reflective traffic buttons must meet the requirements of DMS-4300.
- The color of the button should be yellow for a continuous no passing roadway. Black buttons should be used in areas where passing is allowed.
- Consideration shall be given to bicyclists. See RS(6).

WHEN INSTALLING EDGE LINE RUMBLE STRIPS WITH OR WITHOUT CENTERLINE RUMBLE STRIPS ON UNDIVIDED HIGHWAYS:

- See standard sheet RS(2).

<h2>CENTERLINE RUMBLE STRIPS ON TWO LANE TWO-WAY HIGHWAYS</h2> <h3>RS(4)-23</h3>			
FILE:	rs(4)-23.dgn	DN:	TxDOT
© TxDOT	January 2023	CONT:	0482
REVISIONS		SECT:	01
		JOB:	036, etc.
		HIGHWAY:	SH 285
10-13 1-23		DIST:	COUNTY
		PHR:	JIM HOGG
		SHEET NO.:	346

DATE: 2/12/2024 8:33:50 AM
FILE: P:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENVIRONMENTAL COVER.dgn

ENVIRONMENTAL COVER SHEET

Pharr District Central Design



**SH 285
ENVIRONMENTAL
COVER SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		347

STORMWATER POLLUTION PREVENTION PLAN (SWP3):

This SWP3 has been developed in accordance with the TPDES Construction General Permit TXR150000 (CGP). The Texas Department of Transportation (TxDOT) ensures that project specifications include adequate best management practices (BMPs) for this project.

For all projects with soil disturbing activity and for projects that have Environmental, Permits, Issues, and Commitments (EPICs) dependent on stormwater controls and water quality measures TxDOT will maintain a SWP3 with all pertinent records, correspondence, environmental documents, etc. at the project field office, Area Office, or electronically.

This SWP3 is consistent with requirements specified in applicable stormwater plans and the projects environmental permits, issues, and commitments (EPICs). A copy of the CGP is included in Attachment 2.12 of the SWP3 binder.

1.0 SITE/PROJECT DESCRIPTION

1.1 PROJECT CONTROL SECTION JOB (CSJ):
0482-01-036

1.2 PROJECT LIMITS:

From: FM 1017
To: JIM HOGG/BROOKS COUNTY LINE

1.3 PROJECT COORDINATES:

BEGIN: (Lat) 27.298275, (Long) -98.663075
END: (Lat) 27.265359, (Long) -98.523342

1.4 TOTAL PROJECT AREA (Acres): ~ 181 Acres

1.5 TOTAL AREA TO BE DISTURBED (Acres): ~ 181 Acres

1.6 NATURE OF CONSTRUCTION ACTIVITY:

Construction of Preventive Maintenance and Rehabilitation (Super 2) consisting of cement treated subgrade, cement treated flexbase, asphalt concrete pavement, signing and pavement markings.

1.7 MAJOR SOIL TYPES:

Soil Type	Description
Comitas Loamy Fine Sand, 0 to 3% slopes	Very deep, well drained soil, low runoff.
Delfina Loamy Fine Sand, 0 to 2% slopes	Very deep, moderately well drained soil, low runoff.
Delmita and Bruni Soils, 0 to 3% slopes	Deep, well drained to moderately drained soil.
Delmita Fine Sandy Loam, 0 to 3% slopes	Moderately deep, well drained to moderately drained soil.
Nueces-Sarita Assoc., 0 to 3% slopes	Deep, moderately rapidly permeable soil, low runoff.

1.8 PROJECT SPECIFIC LOCATIONS (PSLs):

PSLs must be depicted on the Environmental Layout Sheets in Attachment 1.2 of this SWP3. PSLs may be identified during preconstruction meetings or during the construction process. Please choose from the options below:
 PSLs determined during preconstruction meeting
 PSLs determined during construction
 No PSLs planned for construction

Type	Sheet #s

All off-ROW PSLs required by the Contractor are the Contractor's responsibility. The Contractor shall secure all permits required by local, state, federal laws for off-ROW PSLs. The contractor shall provide diagrams, areas of disturbance, acreage, and BMPs for all off-ROW PSLs within one mile of the project.

1.9 CONSTRUCTION ACTIVITIES:

(Use the following list as a starting point when developing the Construction Activity Schedule and Ceasing Record in Attachment 2.5.)
 Mobilization
 Install sediment and erosion controls
 Blade existing topsoil into windrows, prep ROW, clear and grub
 Remove existing pavement
 Grading operations, excavation, and embankment
 Excavate and prepare subgrade for proposed pavement widening
 Remove existing culverts, safety end treatments (SETs)
 Remove existing metal beam guard fence (MBGF), bridge rail
 Install proposed pavement per plans
 Install culverts, culvert extensions, SETs
 Install mow strip, MBGF, bridge rail
 Place flex base
 Rework slopes, grade ditches
 Blade windrowed material back across slopes
 Revegetation of unpaved areas
 Achieve site stabilization and remove sediment and erosion control measures
 Other: _____
 Other: _____
 Other: _____

1.10 POTENTIAL POLLUTANTS AND SOURCES:

Sediment laden stormwater from stormwater conveyance over disturbed area
 Fuels, oils, and lubricants from construction vehicles, equipment, and storage
 Solvents, paints, adhesives, etc. from various construction activities
 Transported soils from offsite vehicle tracking
 Construction debris and waste from various construction activities
 Contaminated water from excavation or dewatering pump-out water
 Sanitary waste from onsite restroom facilities
 Trash from various construction activities/receptacles
 Long-term stockpiles of material and waste

 Other: _____
 Other: _____
 Other: _____

1.11 RECEIVING WATERS:

Receiving waters must be depicted on the Environmental Layout Sheets in Attachment 1.2 of this SWP3. Include Segment # for receiving waters.

Tributaries	Classified Waterbody
Noriacitas Creek	Unclassified
Palo Blanco Creek	Unclassified

* Add (*) for impaired waterbodies with pollutant in ().

1.12 ROLES AND RESPONSIBILITIES: TxDOT

Development of plans and specifications
 Submit Notice of Intent (NOI) to TCEQ (≥5 acres)
 Post Construction Site Notice
 Submit NOI/CSN to local MS4
 Perform SWP3 inspections
 Maintain SWP3 records and update to reflect daily operations
 Complete and submit Notice of Termination to TCEQ
 Maintain SWP3 records for 3 years
 Other: _____
 Other: _____
 Other: _____

1.13 ROLES AND RESPONSIBILITIES: CONTRACTOR

Day To Day Operational Control
 Submit Notice of Intent (NOI) to TCEQ (≥5 acres)
 Post Construction Site Notice
 Submit NOI/CSN to local MS4
 Maintain schedule of major construction activities
 Install, maintain and modify BMPs
 Complete and submit Notice of Termination to TCEQ
 Maintain SWP3 records for 3 years
 Other: _____
 Other: _____
 Other: _____

1.14 LOCAL MUNICIPAL SEPARATE STORM SEWER SYSTEM (MS4) OPERATOR COORDINATION:

MS4 Entity
N/A



Arleene Crystal Garcia

03.14.2024

STORMWATER POLLUTION PREVENTION PLAN (SWP3)

July 2023 Sheet 1 of 2

Texas Department of Transportation

FED. RD. DIV. NO.	PROJECT NO.			SHEET NO.
				348
STATE	STATE DIST.	COUNTY		
TEXAS	PHARR	JIM HOGG		
CONT.	SECT.	JOB	HIGHWAY NO.	
0482	01	036, ETC.	SH 285	

STORMWATER POLLUTION PREVENTION PLAN (SWP3):

2.0 BEST MANAGEMENT PRACTICES (BMPs) AND CONTROLS, INSPECTION, AND MAINTENANCE

The Contractor shall be the responsible party for implementing the BMPs described herein and for complying with the SWP3 for control of erosion and sedimentation during day-to-day operations. The Contractor shall implement changes to this SWP3 approved by TxDOT within the times specified in this SWP3 or the CGP.

2.1 EROSION CONTROL AND SOIL STABILIZATION BMPs:

T / P

- Protection of Existing Vegetation
- Vegetated Buffer Zones
- Soil Retention Blankets
- Geotextiles
- Mulching/ Hydromulching
- Soil Surface Treatments
- Temporary Seeding
- Permanent Planting, Sodding or Seeding
- Biodegradable Erosion Control Logs
- Rock Filter Dams/ Rock Check Dams
- Vertical Tracking
- Interceptor Swale
- Riprap
- Diversion Dike
- Temporary Pipe Slope Drain
- Embankment for Erosion Control
- Paved Flumes
- Other: _____
- Other: _____
- Other: _____
- Other: _____

2.2 SEDIMENT CONTROL BMPs:

T / P

- Biodegradable Erosion Control Logs
- Dewatering Controls
- Inlet Protection
- Rock Filter Dams/ Rock Check Dams
- Sandbag Berms
- Sediment Control Fence
- Stabilized Construction Exit
- Floating Turbidity Barrier
- Vegetated Buffer Zones
- Vegetated Filter Strips
- Other: _____
- Other: _____
- Other: _____
- Other: _____

Refer to the Environmental Layout Sheets/ SWP3 Layout Sheets located in Attachment 1.2 of this SWP3

Sediment control BMPs requiring design capacity calculations (See SWP3 Attachment 1.3.):

T / P

- Sediment Trap
 - Calculated volume runoff from 2-year, 24-hour storm for each acre of disturbed area
 - 3,600 cubic feet of storage per acre drained
- Sedimentation Basin
 - Not required (<10 acres disturbed)
 - Required (>10 acres) and implemented.
 - Calculated volume runoff from 2-year, 24-hour storm for each acre of disturbed area
 - 3,600 cubic feet of storage per acre drained
 - Required (>10 acres), but not feasible due to:
 - Available area/Site geometry
 - Site slope/Drainage patterns
 - Site soils/Geotechnical factors
 - Public safety
 - Other: _____

2.3 PERMANENT CONTROLS:

(Coordinate post-construction BMPs with appropriate TxDOT maintenance sections.)

BMPs To Be Left In Place Post Construction:

Type	Stationing	
	From	To
N/A		

Refer to the Environmental Layout Sheets/ SWP3 Layout Sheets located in Attachment 1.2 of this SWP3

2.4 OFFSITE VEHICLE TRACKING CONTROLS:

- Excess dirt/mud on road removed daily
- Haul roads dampened for dust control
- Loaded haul trucks to be covered with tarpaulin
- Stabilized construction exit
- Daily street sweeping
- Other: _____
- Other: _____
- Other: _____
- Other: _____

2.5 POLLUTION PREVENTION MEASURES:

- Chemical Management
- Concrete and Materials Waste Management
- Debris and Trash Management
- Dust Control
- Sanitary Facilities
- Other: _____
- Other: _____
- Other: _____
- Other: _____

2.6 VEGETATED BUFFER ZONES:

Natural vegetated buffers shall be maintained as feasible to protect adjacent surface waters. If vegetated natural buffer zones are not feasible due to site geometry, the appropriate additional sediment control measures have been incorporated into this SWP3.

Type	Stationing	
	From	To
N/A		

Refer to the Environmental Layout Sheets/ SWP3 Layout Sheets located in Attachment 1.2 of this SWP3

2.7 ALLOWABLE NON-STORMWATER DISCHARGES:

- Fire hydrant flushings
- Irrigation drainage
- Pavement washwater (where spills or leaks have not occurred, and detergents are not used)
- Potable water sources
- Springs
- Uncontaminated groundwater
- Water used to wash vehicles or control dust
- Other allowable non-stormwater discharges as allowed by TPDES GP TXR150000.

2.8 DEWATERING:

Dewatering discharges of accumulated stormwater, groundwater, and surface water including discharges from dewatering of trenches, excavations, foundations, vaults, and other points of accumulation are prohibited unless managed by appropriate controls to prevent and minimize the offsite discharge of sediment and other pollutants.

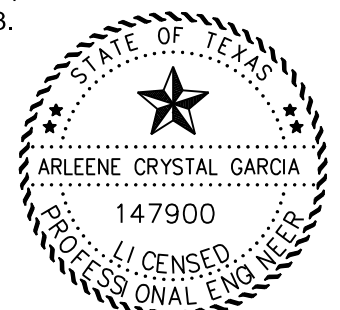
2.9 INSPECTIONS:

All disturbed areas and erosion and sediment control devices shall be inspected at least once every seven (7) days. Inspections shall be performed by TxDOT as indicated on the Field Inspection and Maintenance Report Form 2118 and retained in Attachment 2.5 of this SWP3.

When dewatering activities are present, a daily inspection will be conducted once per day during those activities and documented in accordance with CGP and TxDOT requirements.

2.10 MAINTENANCE:

Control measures shall be properly installed according to specifications. If it is determined that a BMP or control measure is not operating effectively, maintenance must be accomplished as soon as possible and before the next anticipated rain event, but in no case later than 7 calendar days after being able to access the site. Maintenance shall be performed by the Contractor as indicated on the Field Inspection and Maintenance Report Form 2118 and retained in Attachment 2.5 of this SWP3.



Arleene Crystal Garcia
03.14.2024

STORMWATER POLLUTION PREVENTION PLAN (SWP3)

© 2023 July 2023 Sheet 2 of 2

Texas Department of Transportation

FED. RD. DIV. NO.	PROJECT NO.			SHEET NO.
				349
STATE	STATE DIST.	COUNTY		
TEXAS	PHARR	JIM HOGG		
CONT.	SECT.	JOB	HIGHWAY NO.	
0482	01	036, ETC.	SH 285	

During the planning phase of project development, the following Environmental Permits, Issues and Commitments have been developed during coordination with resource agencies, local governmental entities and the general public. Any change orders and/or deviations from the final design must be reported to the Engineer prior to the commencement of construction activities as additional environmental clearances may be required.

I. Clean Water Act, Section 402; Stormwater Pollution Prevention

Action Items Required : No Action Required

- 1. The contractor must implement the SW3P by installing Best Management Practices (BMPs) as indicated in the construction plans and maintained appropriately throughout construction. BMPs must be in place prior to the start of construction. The SW3P may need to be revised as necessary as construction progresses.
- 2. For all construction PSL's off the ROW, the contractor must certify compliance with all applicable laws, rules and regulations pertaining to the preservation of cultural resources, natural resources and the environment.
- 3. Based on the acreage of impact, select the appropriate box below:
 - This project will disturb less than 1 acre of soil and is not part of a larger common plan of development; therefore, a NOI and TPDES Site Notice are not required for this project.
 - or
 - This project will disturb equal to or more than 1 acre of soil but less than 5 acres; therefore a NOI is not required but a TPDES Site Notice is required. The Construction Site Notice (CSN) is required to be posted at the construction site in a publicly accessible location for review by the public, TCEQ, EPA and other Inspectors.
 - or
 - This project will disturb equal to or more than 5 acres of soil and will require a NOI and TPDES Site Notice. The NOI and Site Notice are required to be posted at the construction site in a publicly accessible location.
- 4. Need to address MS4 requirements (Cameron & Hidalgo Counties only) MS4 requirements not needed

II. Clean Water Act, Sections 401 and 404 Compliance

Action Items Required : No Action Required

- 1. Filling, dredging or excavating in any water bodies, rivers, creeks, streams, wetlands or wet areas is prohibited unless specified in the USACE permit and approved by the Engineer. The contractor shall adhere to all agreements, mitigation plans, and BMPs required by the NWP as regulated by the USACE.

The Contractor must adhere to all of the terms and conditions associated with the following permit(s):

- No Permit Required
- Nationwide Permit 14 - PCN not Required (less than 1/10th acre waters or wetlands affected)
- Nationwide Permit 14 - PCN Required (1/10th to <1/2 acre, 1/3 in tidal waters)
- Individual 404 Permit Required
- Other Nationwide Permit Required: NWP# _____

- 2. The contractor is responsible for obtaining new or revised Section 404 permit(s) for Contractor initiated changes in construction methods that change Impacts To Waters Of The U.S., including wetlands. The Contractor will ensure that the water quality of the State will be maintained and not degraded.

- 3. Best Management Practices for applicable Section 401 General Conditions:

General Condition 12 - Categories I and II BMPs required

Category I (Erosion Control)

- Temporary Vegetation Interceptor Swale Mulch Filter Berms and/or Socks
- Blankets, Matting Diversion Dike Compost Filter Berms and/or Socks
- Mulch Erosion Control Compost Compost Blankets
- Sodding

Category II (Sedimentation Control)

- Silt Fence Hay (Straw) Bale Dike Mulch Filter Berms and/or Socks
- Rock Berm Brush Berms Compost Filter Berms and/or Socks
- Triangular Filter Dike Sediment Basins Stone Outlet Sediment Traps
- Sand Bag Berm Erosion Control Compost

General Condition 21 - Category III BMPs required

Category III (Post-Construction TSS Control)

- Vegetative Filter Strips Wet Basins Mulch Filter Berms and/or Socks
- Retention/Irrigation Grassy Swales Compost Filter Berms and/or Socks
- Extended Detention Basin Vegetation-Lined Ditches Sand Filter Systems
- Constructed Wetlands Erosion Control Compost Sedimentation Chambers

II. Clean Water Act, Sections 401 and 404 Compliance - Continued:

- 4. The Contractor's designated and qualified Contractor Responsible Person Environmental (CRPe) will monitor the project site daily to ensure compliance with SW3P and TPDES General Permit TXR 150000. Daily Monitoring Reports shall be provided to TxDOT within 48 hours, in accordance with Item 506.3.1.
- 5. Other Project Specific Actions:
 - 1. Contractor must sweep roadway & remove aggregate upon completed daily operations.
 - 2. Contractor shall not place removed aggregate along with adjacent grass areas.

III. Cultural Resources

Action Items Required : No Action Required

- 1. Refer to the 2014 TxDOT Standard Specifications For Construction And Maintenance Of Highways, Streets, And Bridges, Item 7.7.1., in the event historical issues or archeological artifacts are found during construction. Upon discovery of archeological artifacts (bones, burnt rock, flint, pottery, etc.) cease work in the immediate area and contact the Engineer immediately.
- 2. Other Project Specific Actions:

IV. Vegetation Resources

Action Items Required : No Action Required

- 1. In accordance with the 2014 TxDOT Standard Specifications; Item 164 - Seeding For Erosion Control; provide and install temporary or permanent seeding for erosion control as shown on the plans or as directed by the Engineer for all seeding and replanting of right of way where possible. (Required for Urban Settings)
- 2. In accordance with Executive Order 13112 on invasive species and the Executive Memorandum on Beneficial Landscaping, native species of plants shall be used for all seeding and replanting of right of way where possible for rural roadways. (Required for Rural Settings)
- 3. Preserve vegetation where possible throughout the project and minimize clearing, grubbing and excavation within stream banks, bed and approach sections.
- 4. Other Project Specific Actions:
 - 1. Vegetation clearing activities would be avoided during the general bird nesting season, Feb, 1 to Oct. 1, to minimize adverse impacts to birds.
 - 2. Colonization by invasive plants would be actively prevented. Vegetation management would include the removal of invasive species as soon as practicable while allowing the existing native plants to revegetate disturbed areas.

Pharr District Contact No. 956-702-6100

Revised 01/30/2017

List of Abbreviations

BMP: Best Management Practice	NWP: Nationwide Permit
CGP: Construction General Permit	PCN: Pre-Construction Notification
CRPe: Contractor Responsible Person Environmental	PSL: Project Specific Location
DSHS: Texas Department of State Health Services	SPCC: Spill Prevention Control and Countermeasure
FEMA: Federal Emergency Management Agency	SW3P: Storm Water Pollution Prevention Plan
FHWA: Federal Highway Administration	TCEQ: Texas Commission on Environmental Quality
MOA: Memorandum of Agreement	THC: Texas Historical Commission
MOU: Memorandum of Understanding	TPDES: Texas Pollutant Discharge Elimination System
MS4: Municipal Separate Stormwater Sewer System	TPWD: Texas Parks and Wildlife Department
MSAT: Mobile Source Air Toxic	TxDOT: Texas Department of Transportation
MBTA: Migratory Bird Treaty Act	T&E: Threatened and Endangered Species
NOI: Notice of Intent	USACE: U.S. Army Corp of Engineers
NOT: Notice of Termination	USFWS: U.S. Fish and Wildlife Service



ENVIRONMENTAL PERMITS, ISSUES AND COMMITMENTS (EPIC)

SHEET 1 OF 2

FED. RD. DIV. NO.	PROJECT NO.		HIGHWAY NO.
6			SH 285
STATE	DISTRICT	COUNTY	SHEET NO.
TEXAS	PHR	JIM HOGG	
CONTROL	SECTION	JOB	
0482	01	036, etc.	350

V. Federal Listed, and Proposed Threatened and Endangered Species, Critical Habitat, State Listed Species, Candidate Species and Migratory Birds

Action Items Required : No Action Required

- 1. Under the Migratory Bird Treaty Act (MBTA) of 1918, codified at 16 U.S.C. § 703-712 and as enforced by the USFWS, the proposed construction work will not remove active nests from bridges, trees, ground and other structures during migratory bird nesting season, (February 1st. through October 1st.). If the Contractor needs to perform work within the right of way during nesting season, a qualified Biologist shall conduct a survey to determine if active nests are present. If present, the Contractor shall maintain a buffer zone around the nest(s) as directed by the Biologist. The buffer zone will be protected from clearing and disturbance until such time as the Biologist has determined that the nest(s) is no longer active. Prior to the nesting season, existing bridges and culverts should be treated against migratory bird nesting by utilizing Bird Exclusion Methods. Bird Exclusion Methods should be monitored and maintained throughout the nesting season. Refer to Standard Bird Exclusion Details.
- 2. There is the potential for the presence of state-listed species & species of concern in the project area and state law prohibits the taking (incidental or otherwise) of state-listed species. Taking is defined as the collection, hooking, hunting, netting, shooting, or share by any means or devices. If any listed species are observed, cease work in the immediate area, do not disturb species or habitat and contact the Engineer immediately.
- 3. Other Project Specific Actions:
 - 1. The removal of unoccupied, inactive nests would be avoided, where practicable.
 - 2. The establishment of active nest during the nesting season on TxDOT owned and operated facilities and structures proposed for replacement or repair would be prevented.
 - 3. The collection, capture, relocation, or transportation of birds, eggs, young, or active nests without a permit would be prohibited.
 - 4. Due to increased activity (mating) of reptiles during the spring, construction activities such as clearing and grading, where practicable, would be scheduled outside of the spring (April-May) season.
 - 5. Ground-disturbing activities, where practicable, would be scheduled before October when reptiles become less active and may be using burrows within the protected area.

VI. Hazardous Materials on Contamination Issues

Action Items Required : No Action Required

General (applies to all projects):

Comply with the Hazard Communication Act (HCA) for personnel who will be working with hazardous materials by conducting safety meetings prior to beginning construction and making workers aware of potential hazards in the workplace. Ensure that all workers are provided with personal protective equipment appropriate for any hazardous materials used.

Obtain and keep on-site Material Safety Data Sheets (MSDS) for all hazardous products used on the project, which may include but are not limited to the following categories: Paints, acids, solvents, asphalt products, chemical additives, fuels and concrete curing compounds or additives. Provide protected storage, off bare ground and covered, for products which may be hazardous. Maintain product labelling as required by the HCA.

Maintain an adequate supply of on-site spill response materials as indicated in the MSDS. In the event of a spill, take immediate action to mitigate the spill as indicated in the MSDS and in accordance with safe work practices. Contact the TxDOT Pharr District Spill Coordinator immediately. The Contractor shall be responsible for the proper containment and cleanup of all product spills.

Contact the Engineer if any of the following are detected:

- Dead or distressed vegetation (identified as not normal)
- Trash piles, drums, canisters, barrels, etc.
- Undesirable smells or odors
- Evidence of leaching or seepage of contaminant substances

Any other evidence indicating possible hazardous materials or contamination discovered on site.

- 1. If potentially hazardous material and/or contaminated media (i.e.: soil, groundwater, surface water, sediment, building materials) are unexpectedly encountered during construction, assure that such materials and contamination are handled according to applicable federal and state regulations, cease work in the immediate area and contact the Engineer immediately.

VI. Hazardous Materials on Contamination Issues - Continued:

- 2. Does the project involve any bridge class structure rehabilitation or replacements (bridge class structures not including box culverts)?
 - Yes No

If "No", then no further action required.
If "Yes", then TxDOT is responsible for completing an asbestos assessment/inspection.
- 3. Are the results of the asbestos inspection positive (is asbestos present)?
 - Yes No

If "Yes", then TxDOT must retain a Texas Department of State Health Services (DSHS) licensed asbestos consultant to assist with the notification, develop abatement/mitigation procedures, and perform management activities as necessary. The notification form to DSHS must be postmarked at least 15 working days prior to scheduled abatement activities and/or demolition.

If "No", then TxDOT is still required to notify DSHS 15 working days prior to any scheduled demolition.
- 4. The Contractor is responsible for providing the date(s) for abatement activities and/or demolition with careful coordination between the Engineer and an Asbestos Consultant in order to minimize construction delays and subsequent claims.

VII. Other Environmental Issues

Action Items Required : No Action Required

- 1. Noise

Contractor shall make every reasonable effort to minimize construction noise through abatement measures such as work hour controls and proper maintenance of equipment mufflers.
- 2. Air

Contractor shall practice common dust control techniques such as surface chemical treatment or watering of unpaved road surfaces and vehicle speed reduction shall be implemented to minimize and prevent airborne dust during construction.


Contractor should minimize MSAT by utilizing measures to encourage use of EPA required cleaner diesel fuels, limits on idling, increase use of cleaner burning diesel engines, and other emission limitation techniques, as appropriate.

Pharr District Contact No. 956-702-6100

Revised 01/30/2017

List of Abbreviations

BMP: Best Management Practice	NWP: Nationwide Permit
CGP: Construction General Permit	PCN: Pre-Construction Notification
CRPe: Contractor Responsible Person Environmental	PSL: Project Specific Location
DSHS: Texas Department of State Health Services	SPCC: Spill Prevention Control and Countermeasure
FEMA: Federal Emergency Management Agency	SW3P: Storm Water Pollution Prevention Plan
FHWA: Federal Highway Administration	TCEQ: Texas Commission on Environmental Quality
MOA: Memorandum of Agreement	THC: Texas Historical Commission
MOU: Memorandum of Understanding	TPDES: Texas Pollutant Discharge Elimination System
MS4: Municipal Separate Stormwater Sewer System	TPWD: Texas Parks and Wildlife Department
MSAT: Mobile Source Air Toxic	TxDOT: Texas Department of Transportation
MBTA: Migratory Bird Treaty Act	T&E: Threatened and Endangered Species
NOI: Notice of Intent	USACE: U.S. Army Corp of Engineers
NOT: Notice of Termination	USFWS: U.S. Fish and Wildlife Service



Texas Department of Transportation
© 2016

PHARR DISTRICT

**ENVIRONMENTAL PERMITS,
ISSUES AND COMMITMENTS
(EPIC)**

SHEET 2 OF 2

FED. RD. DIV. NO.	PROJECT NO.			HIGHWAY NO.
6	STATE	DISTRICT	COUNTY	SH 285
TEXAS	PHR	JIM HOGG		SHEET NO.
CONTROL	SECTION	JOB		351
0482	01	036, etc.		

Date Printed: X-X-XX

TPWD BMPs

Under Section 12.0011 of the Texas Parks and Wildlife Code, Texas Parks and Wildlife Department (TPWD) is charged with "providing recommendations that will protect fish and wildlife resources to local, state, and federal agencies that approve, permit, license, or construct developmental projects" and "providing information on fish and wildlife resources to any local, state, and federal agencies or private organizations that make decisions affecting those resources."

The purpose of this section is to provide beneficial management practices (BMP) that should be implemented during construction, and maintenance activities statewide for transportation projects with the goal of avoidance and minimization of impacts to natural resources. Statewide Standard BMP pertain to all fish and wildlife species, including state-listed species and other Species of Greatest Conservation Need (SGCN). Implementing the recommendations as outlined below will improve conservation of species and their habitat.

General Design/Construction BMPs

- Prior to start of construction, information will be provided to personnel of the potential for all state-listed threatened species or other SGCN to occur within the project area and should be advised of relevant rules and regulations to protect plants, fish, and wildlife.
- Contractor should avoid harming all wildlife species if encountered and allow them to safely leave the project site. Due diligence should be used to avoid killing or harming any wildlife species in the implementation of transportation projects.
- Contractors should install wildlife exclusion fencing and should examine the inside of the exclusion area daily to determine if any wildlife species have been trapped inside the area of impact and provide safe egress opportunities prior to initiation of construction activities.
- Apply hydromulching and/or hydroseeding in areas for soil stabilization and/or revegetation of disturbed areas around wetlands and in riparian areas.
- Contractor should use woven natural fiber netting in which the mesh design allows the threads to move, therefore allowing expansion of the mesh openings. Plastic netting should be avoided.
- Project staging areas, stockpiles, temporary construction easements, and other project related sites should be situated in previously disturbed areas to avoid or minimize impacts to sensitive or unique habitats including intact native vegetation, floodplains, riparian corridors, wetlands, playa lakes, and habitat for wildlife species.
- When lighting is added, consider wildlife impacts from light pollution and incorporating dark-sky practices into design strategies. Minimize sky glow by focusing light downward, with full cutoff luminaires to avoid light emitting above the horizontal. The minimum amount of night-time lighting needed for safety and security should be used.

Vegetation BMPs

- Minimize the amount of vegetation cleared. Removal of native vegetation, particularly mature native trees and shrubs should be avoided. Impacted vegetation should be replaced with in-kind on-site replacement /restoration of native vegetation.
- It is strongly recommended that trees greater than 12 inches in diameter at breast height (DBH) that are removed be replaced. TPWD's experience indicates that for ecologically effective replacement, a ratio of three trees for every one (3:1) lost should be provided to either on-site or off-site. Trees less than 12 inches DBH should be replaced at a 1:1 ratio.
- The use of any non-native vegetation in landscaping and revegetation is discouraged. Locally adapted native species should be used.
- The use of seed mix that contains seeds from only regional ecotype native species is recommended

Invasive Species BMPs

- For all work in water bodies designated as 1/32 infested or 1/32 positive for invasive zebra (Dreissena polymorpha) OR quagga mussels (Dreissena bugensis) as well as waters downstream of these lakes, all machinery, equipment, vessels, or vehicles coming in contact with such waters should be cleaned prior to leaving the site to remove any mud, plants, organisms, or debris, water drained (if applicable), and dried completely before use in another water body to prevent the potential spread of invasive mussels.
- Care should be taken to prevent the spread of aquatic and terrestrial invasive plants during construction activities.
- Care should be taken to avoid the spread of aquatic invasive plants such as giant Salvinia (Salvinia molesta), common salvinia (Salvinia minima), hydrilla (Hydrilla verticillata), water hyacinth (Eichhornia spp.), Eurasian watermilfoil (Myriophyllum spicatum), water lettuce (Pistia stratiotes), and alligatorweed (Alternanthera philoxeroides) from infested water bodies into areas not currently infested. All machinery, equipment, vessels, boat trailers, or vehicles coming in contact with waters containing aquatic invasive plant species should be cleaned prior to leaving the site to remove all aquatic plant material and dried completely before use on another water body to prevent the potential spread of invasive plants. Removed plants should be transported for disposal in a secure manner to prevent dispersal.
- Only native or non-invasive plants should be planted. Care should be taken to avoid mowing invasive giant reed (Arundo donax), which spreads by fragmentation, and to clean equipment if inadvertently mowed to prevent spread. If using hay bales for sediment control, use locally grown weed-free hay to prevent the spread of invasive species. Leave the hay bales in place and allow them to break down, as this acts as mulch assisting in revegetation.

Stream Crossings BMPs

- Riparian buffer zones should remain undisturbed.

Dewatering BMPs

- Impact avoidance measures for aquatic organisms, including all native fish and freshwater mussel species, regardless of state-listing status, should be considered during project planning and construction activities.

Wildlife Crossing BMPs

- Incorporate wildlife crossings with fencing, particularly in areas that bisect wildlife travel corridors or seasonal movement routes to avoid further habitat fragmentation and minimize wildlife-vehicle interactions.

Rare Plant BMPs

- Avoid impacts and minimize unavoidable impacts. Plant locations should be protected with temporary barrier fencing and contractors should be instructed to avoid protected areas. Conducting construction outside of the growing season or after a plant has produced mature fruit is the preferred way to avoid/minimize impacts to SGCN plant populations. Staging areas, stockpiles, and other project related sites on TxDOT ROW should not impact SGCN plant populations. After construction begins, minimize herbicide use near SGCN plant populations (if possible, use hand-held spot sprayers, several meters from rare plants, on still or days with little wind).

Pharr District Contact No. 956-702-6100

Rare Plants BMPs (Continued)

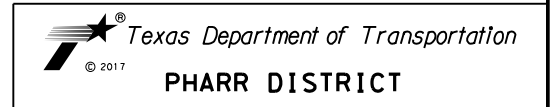
- If there are unintended impacts to SGCN populations, these impacts should be reported to TPWD Transportation Staff.
- During project period, conduct work during times of the year when plants are dormant and/or conditions minimize disturbance of the habitat.

Bird BMPs

- Avoid vegetation clearing activities during the general bird nesting season, February 15th to October 1st to minimize adverse impacts to birds.
- Do not collect, capture, relocate, or transport birds, eggs, young, or active nests without a permit.
- Minimize extended human presence near nesting birds during construction and maintenance activities. Protect sensitive habitat areas with temporary barriers or fencing to limit human foot-traffic and off-road vehicle use to alert and discourage contractors from causing any unintentional impacts.
- Minimize construction noise above ambient levels during general bird nesting season to minimize adverse impacts on birds.
- Minimize construction lighting during the general bird nesting season by scheduling work activities between dawn and dusk.

Rookeries BMPs

- In general, nesting dates for herons and egrets range from early February to late August in Texas, depending on the species. Great blue herons (GBHE) (Ardea herodias) are usually the first to nest. When GBHE get disrupted from the nest and abandon nesting, then the other species of herons and egrets may not attempt to nest at the colony that year.
- If rookeries are encountered, avoid and minimize disturbance during nesting to protect rookery species and their habitat.
- Vegetation clearing in a primary buffer area of 300 meters (984 feet) from a rookery or heronry periphery should be avoided. Utilizing areas that have already been cleared within this buffer area may be acceptable depending on site-specific characteristics. Additionally, human foot-traffic or machinery use should not occur within this buffer area during the nesting season.
- Clearing activities or construction using heavy machinery in a secondary buffer area of 1000 meters (3281 feet) from the heronry periphery should be avoided during the breeding season (courting and nesting).



EPIC SHEET SUPPLEMENTALS
TPWD BMPs

SHEET 1 OF 3

FED. RD. DIV. NO.	PROJECT NO.		HIGHWAY NO.
6			SH 285
STATE	DISTRICT	COUNTY	
TEXAS	PHR	JIM HOGG	
CONTROL	SECTION	JOB	
0482	01	036, etc.	
			352

List of Abbreviations

BMP: Best Management Practice
CGP: Construction General Permit
CRPe: Contractor Responsible Person Environmental
DSHS: Texas Department of State Health Services
FEMA: Federal Emergency Management Agency
FHWA: Federal Highway Administration
MOA: Memorandum of Agreement
MOU: Memorandum of Understanding
MS4: Municipal Separate Stormwater Sewer System

MSAT: Mobile Source Air Toxic
MBTA: Migratory Bird Treaty Act
NOI: Notice of Intent
NOT: Notice of Termination
NWP: Nationwide Permit
PCN: Pre-Construction Notification
PSL: Project Specific Location
SPCC: Spill Prevention Control and Countermeasure
SW3P: Storm Water Pollution Prevention Plan

TCEQ: Texas Commission on Environmental Quality
THC: Texas Historical Commission
TPDES: Texas Pollutant Discharge Elimination System
TPWD: Texas Parks and Wildlife Department
TxDOT: Texas Department of Transportation
T&E: Threatened and Endangered Species
USACE: U.S. Army Corp of Engineers
USFWS: U.S. Fish and Wildlife Service

Fish BMPs

- The following Fish BMP apply to projects for all fish species in waters of the state to minimize impacts to water quality and aquatic passage from transportation projects.
- For projects in waters of the state and work is adjacent to water: follow Water Quality and Stream Crossing BMPs.
- For projects in waters of the state and work is in the water: follow Water Quality, Stream Crossing, and Dewatering BMP.

Aquatic Invertebrate BMPs

- For projects within the range of a SGCN or state-listed species and work is adjacent to water: Water Quality and Stream Crossing BMP
- For projects within the range of a SGCN or state-listed species and work is in the water: Water Quality, Stream Crossing, and Dewatering BMP.
- For spring-seep associated caddisflies (*Cheumatopsyche morsei*, *Chimarra holzenthali*, and *Hydroptila ouachita*): Avoid or minimize impacts to the natural riparian buffer along stream channel including native shrubs and trees.

Crayfish BMP

- For projects within the range of a SGCN or state-listed species and work is adjacent to water: Water Quality and Stream Crossing BMP.
- For projects within the range of a SGCN or state-listed species and work is in the water: Water Quality, Stream Crossing, and Dewatering BMP.
- Avoid or minimize impacts to the natural riparian buffer that provides terrestrial and aquatic plant matter for the diet of most crayfish species.

Freshwater Mussel BMP

- In addition to Water Quality and Stream Crossing BMP, follow the most recent, 1/32 TPWD^{3/2} TxDOT Annual Work Plan for Pre-Construction Surveys, Aquatic Resources Relocations, and Other Best Management Practices to Avoid, Minimize, and Mitigate Impacts to Freshwater Resources.^{7/32}
- When work is adjacent to the water: Water Quality BMP implemented as part of the Texas Commission on Environmental Quality (TCEQ) Stormwater Pollution Prevention Plan (SWPPP) for a construction general permit or any conditions of the 401 Water Quality Certification for the project will be implemented.

Insect Pollinator BMP

- Deep soil disturbances, such as, tilling or deep disking in areas that host aggregations of ground-nesting bees should be avoided. Tilling and disking also may promote the invasion or germination of non-native plants. Different species of native ground-nesting bees prefer different soil conditions, although research suggests that many ground nesting bees prefer sandy, loamy sand or sandy loam soils. In areas with these soil types consider leaving open patches of soil.
- Allow dead trees to stand (so long as they do not pose a risk to property or people) and protect shrubs and herbaceous plants with pithy or hollow stems (e.g., cane fruits, sumac, elderberry), as these provide nesting habitat for tunnel-nesting native bees. Retain dead or dying branches whenever it is safe and practical at the edges of the ROW. Wood-boring beetle larvae often fill dead trees and branches with narrow tunnels into which tunnel-nesting bees will establish nests. Additionally, bumble bees may choose to nest in wood piles.
- Retain rotting logs at edges of the ROW where some bee species may burrow tunnels in which to nest.

Insect Pollinator BMP (Continued)

- Protect sloped or well-drained ground sites where plants are sparse and direct access to soil is available. These are the areas where ground-nesting bees may dig nests. Turning the soil destroys all ground nests that are present at that depth and hinders the emergence of bees that are nesting deeper in the ground.
- Protect grassy thickets, or other areas of dense, low cover from mowing or other disturbance. These are the sites where bumble bees might find the nest cavities they need, as well as annual and perennial wildflowers that can provide important food resources.
- Where available and economical, native plants and seed should be procured from local eco-type providers. Seed mixes should be diverse and include as many ecoregion natives as possible ensuring full season floral resources. Species by Texas ecoregion can be found in the Texas Management Recommendations for Native Insect Pollinators in Texas document: https://tpwd.texas.gov/publications/pwdpubs/media/pwd*bk*w7000*1813.pdf
- Planting at least three different native flowering plants within each of three blooming periods are recommended (spring, summer, early fall) in high rainfall regions of Texas. In drier regions of the state, a target of three native flowering plants within each of two blooming periods can be used.

Small Mammal BMP

For Coues' rice rat (*Oryzomys couesi aquaticus*):

- Minimize impacts to wetland, resaca, oxbow Conversion of property containing cave or cliff features to transportation purposes should be avoided. lake, and marsh habitats
- Water Quality BMP

Fossorial Mammal BMP

- When a construction zone is adjacent to active BTPD burrows or pocket gopher mounds, erect barriers to discourage individuals moving through or into the construction area.
- When seeding or revegetation is planned in an area adjacent to BTPD burrows or pocket gopher mounds, a vegetative barrier should be considered in the planting to discourage dispersal into the ROW.

Bat BMP

- For activities that have the potential to impact structures, cliffs or caves, or trees; a qualified biologist will perform a habitat assessment and occupancy survey of the feature(s) with roost potential as early in the planning process as possible or within one year before project letting.
- For roosts where occupancy is strongly suspected but unconfirmed during the initial survey, revisit feature(s) at most four weeks prior to scheduled disturbance to confirm absence of bats.
- If bats are present or recent signs of occupation (i.e., piles of guano, distinct musky odor, or staining and rub marks at potential entry points) are observed, take appropriate measures to ensure that bats are not harmed, such as implementing non-lethal exclusion activities or timing or phasing of construction.
- Exclusion devices can be installed by a qualified individual between September 1 and March 31. Exclusion devices should be used for a minimum of seven days when minimum nighttime temperatures are above 50°F AND minimum daytime temperatures are above 70°F. Prior to exclusion, ensure that alternate roosting habitat is available in the immediate area. If no suitable roosting habitat is available, installation of alternate roosts is recommended to replace the loss of an occupied roost. If alternate roost sites are not provided, bats may seek shelter in other inappropriate sites, such as buildings, in the surrounding area.

Pharr District Contact No. 956-702-6100

Bat BMP (Continued)

- If feature(s) used by bats are removed as a result of construction, replacement structures should incorporate bat-friendly design or artificial roosts should be constructed to replace these features.
- Avoid unnecessary removal of dead fronds on native and ornamental palm trees in south Texas (Cameron, Hidalgo, Willacy, Kenedy, Brooks, Kleberg, Nueces, and San Patricio counties) from April 1 through October 31. If removal of dead fronds is necessary at other times of the year, limit frond removal to extended warm periods (nighttime temperatures = 55°F for at least two consecutive nights), so bats can move away from the disturbance and find new roosts.
- Large hollow trees, snags (dead standing trees), and trees with shaggy bark should be surveyed for colonies and, if found, should not be disturbed until the bats are no longer occupying these features. Post-occupancy surveys should be conducted by a qualified biologist prior to tree removal from the landscape.
- Retain mature, large diameter hardwood forest species and native/ornamental palm trees.
- In all instances, avoid harm or death to bats. Bats should only be handled as a last resort and after communication with TPWD.

Aquatic Amphibian and Reptile BMP

For projects within existing right-of-way (ROW) when work is in water or will permanently impact a water feature and potential habitat exists for the target species complete the following:

- Minimize impacts to wetlands, temporary and permanent open water features, including depressions, and riverine habitats.
- Maintain the existing hydrologic regime and any connections between wetlands and other aquatic features.
- Use barrier fencing to direct animal movements away from construction activities and areas of potential wildlife-vehicle collisions in construction areas directly adjacent, or that may directly impact, potential habitat for the target species.
- Apply hydromulching and/or hydroseeding in areas for soil stabilization and/or revegetation of disturbed areas around wetlands and in riparian areas. If erosion control blankets or mats will be used, the product should not contain netting, but should only contain loosely woven natural fiber netting in which the mesh design allows the threads to move, therefore allowing expansion of the mesh openings. Plastic netting should be avoided.
- Project specific locations (PSLs) proposed within state-owned ROW should be located in uplands away from aquatic features.
- When work is directly adjacent to the water, minimize impacts to shoreline basking sites (e.g., downed trees, sand bars, exposed bedrock) and refugia/overwinter sites (e.g., brush and debris piles, crayfish burrows, aquatic logjams, and leaf packs).



EPIC SHEET SUPPLEMENTALS
TPWD BMPs

SHEET 2 OF 3

FED. RD. DIV. NO.	PROJECT NO.		HIGHWAY NO.
6			SH 285
STATE	DISTRICT	COUNTY	
TEXAS	PHR	JIM HOGG	SHEET NO.
CONTROL	SECTION	JOB	
0482	01	036, etc.	353

List of Abbreviations

BMP: Best Management Practice
CGP: Construction General Permit
CRPe: Contractor Responsible Person Environmental
DSHS: Texas Department of State Health Services
FEMA: Federal Emergency Management Agency
FHWA: Federal Highway Administration
MOA: Memorandum of Agreement
MOU: Memorandum of Understanding
MS4: Municipal Separate Stormwater Sewer System

MSAT: Mobile Source Air Toxic
MBTA: Migratory Bird Treaty Act
NOI: Notice of Intent
NOT: Notice of Termination
NWP: Nationwide Permit
PCN: Pre-Construction Notification
PSL: Project Specific Location
SPCC: Spill Prevention Control and Countermeasure
SW3P: Storm Water Pollution Prevention Plan

TCEQ: Texas Commission on Environmental Quality
THC: Texas Historical Commission
TPDES: Texas Pollutant Discharge Elimination System
TPWD: Texas Parks and Wildlife Department
TxDOT: Texas Department of Transportation
T&E: Threatened and Endangered Species
USACE: U.S. Army Corp of Engineers
USFWS: U.S. Fish and Wildlife Service

Aquatic Amphibian and Reptile BMP (Continued)

- If gutters and curbs are part of the roadway design, install gutters that do not include the side box inlet and include sloped (i.e., mountable) curbs to allow small animals to leave roadway. If this modification to the entire curb system is not possible, install sections of sloped curb on either side of the storm water drain for several feet to allow small animals to leave the roadway. Priority areas for these design recommendations are those with nearby wetlands or other aquatic features.

For projects that require acquisition of additional ROW and work within that new ROW is in water or will permanently impact a water feature, implement BMP for projects within existing ROW above plus those below:

- For sections of roadway adjacent to wetlands or other aquatic features, install wildlife barriers that prevent climbing. Barriers should terminate at culvert openings in order to funnel animals under the road. The barriers should be of the same length as the adjacent feature or 80 feet long in each direction, or whichever is the lesser of the two.
- For culvert extensions and culvert replacement/installation, incorporate measures to funnel animals toward culverts such as concrete wingwalls and barrier walls with overhangs.
- When riprap or other bank stabilization devices are necessary, their placement should not impede the movement of terrestrial or aquatic wildlife through the water feature. Biotechnical streambank stabilization methods using live native vegetation, or a combination of vegetative and structural materials should be used.

Terrestrial Amphibian and Reptile BMP

- For open trenches and excavated pits, install escape ramps at an angle of less than 45 degrees (1:1) in areas left uncovered. Visually inspect excavation areas for trapped wildlife prior to backfilling
- Avoid or minimize disturbing or removing cover objects, such as downed trees, rotting stumps, brush piles, and leaf litter. If avoidance or minimization is not practicable, consider removing cover objects prior to the start of the project and replace them at project completion.
- Examine heavy equipment stored on site before use, particularly after rain events when reptile and amphibian movements occur more often, to ensure use will not harm individuals that might be seeking temporary refuge.
- Due to increased activity (mating) of reptiles and amphibian during the spring, construction activities like clearing or grading should attempt to be scheduled outside of the spring (March-May) season. Also, timing ground disturbing activities before October when reptiles and amphibians become less active and may be using burrows in the project area is also encouraged.
- If Texas tortoises (*Gopherus berlandieri*) or box turtles (*Terrepena* spp.) are present in a project area, they should be removed from the area and relocated between 100 and 200 meters from the project area. After removal of the individuals, the area that will be disturbed during active construction and project specific locations should be fenced off to exclude reentry by turtles, tortoises, and other reptiles. The exclusion fence should be constructed and maintained as follows:
 - The exclusion fence should be constructed with metal flashing or drift fence material.
 - Rolled erosion control mesh material should not be used.
 - The exclusion fence should be buried at least 6 inches deep and be at least 24 inches high.
 - The exclusion fence should be maintained for the life of the project and only removed after the construction is completed and the disturbed site has been revegetated.

Terrestrial Amphibian and Reptile BMP (Continued)

- After project is complete, revegetate disturbed areas with an appropriate locally sourced native seed mix. If erosion control blankets or mats will be used, the product should not contain nylon netting, but should only contain loosely woven natural fiber netting in which the mesh design allows the threads to move, therefore allowing expansion of the mesh openings. Plastic netting should be avoided.

Black-spotted newt/Mexican Burrowing toad/ Mexican treefrog/ Strecker's chorus frog/White-lipped frog/Woodhouse's toad

- Aquatic Amphibian and Reptile BMP
- Terrestrial Amphibian and Reptile BMP
- Water Quality BMP
- Vegetation BMP

Sheep Frog

- Minimize disturbance to burrows or downed woody debris
- Aquatic Amphibian and Reptile BMP
- Terrestrial Amphibian and Reptile BMP
- Water Quality BMP
- Vegetation BMP

South Texas Siren (Large Form)

- Minimize impacts to warm, shallow waters with vegetative cover such as ponds and ditches
- Aquatic Amphibian and Reptile BMP
- Water Quality BMP

Black-striped snake/ Eastern box turtle/Northern cat-eyed snake/Plateau spot-tailed earless lizard/ Reticulate collared lizard/ Slender glass lizard/ Speckler racer/Tamaulipan spot-tailed earless lizard/ Texas Indigo snake/ Western box turtle/Western hognose snake/Western massasauga

- Terrestrial Amphibian and Reptile BMP
- Vegetation BMP

Rio Grande River Cooter

- Aquatic Amphibian and Reptile BMP
- Water Quality BMP

Texas Horned Lizard

- Avoid harvester ant mounds in the selection of Project Specific Locations (PSLs).
- Terrestrial Amphibian and Reptile BMP
- Vegetation BMP

Texas Tortoise

- Utility trenches should be covered overnight or visually inspected before filling to avoid burial of the species
- Terrestrial Amphibian and Reptile BMP
- Vegetation BMP

OTHER PERTINENT INFORMATION

Trifold Available

- Ocelot information
- Pelican information
- Ashy dogweed

Stockcards Available

- Mitigatory Bird Treaty Act
- Texas Tortoise
- Harvester Ants and Horn Lizards

Pharr District Contact No. 956-702-6100

Revised 02/24/2022

List of Abbreviations

BMP: Best Management Practice
 CGP: Construction General Permit
 CRPe: Contractor Responsible Person Environmental
 DSHS: Texas Department of State Health Services
 FEMA: Federal Emergency Management Agency
 FHWA: Federal Highway Administration
 MOA: Memorandum of Agreement
 MOU: Memorandum of Understanding
 MS4: Municipal Separate Stormwater Sewer System

MSAT: Mobile Source Air Toxic
 MBTA: Migratory Bird Treaty Act
 NOI: Notice of Intent
 NOT: Notice of Termination
 NWP: Nationwide Permit
 PCN: Pre-Construction Notification
 PSL: Project Specific Location
 SPCC: Spill Prevention Control and Countermeasure
 SW3P: Storm Water Pollution Prevention Plan

TCEQ: Texas Commission on Environmental Quality
 THC: Texas Historical Commission
 TPDES: Texas Pollutant Discharge Elimination System
 TPWD: Texas Parks and Wildlife Department
 TxDOT: Texas Department of Transportation
 T&E: Threatened and Endangered Species
 USACE: U.S. Army Corp of Engineers
 USFWS: U.S. Fish and Wildlife Service



**EPIC SHEET SUPPLEMENTALS
 TPWD BMPs**

SHEET 3 OF 3

FED. RD. DIV. NO.	PROJECT NO.		HIGHWAY NO.
6			SH 285
STATE	DISTRICT	COUNTY	
TEXAS	PHR	JIM HOGG	
CONTROL	SECTION	JOB	
0482	01	036, etc.	
			SHEET NO. 354

DATE: 2/7/2024 11:05:12 AM
 FILE: P:\t\dot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH 285 ENV 1.dgn

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	27389
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	27389
*	FERTILIZER	TON	0.283
168 6001	VEGETATIVE WATERING	MG	476
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	156
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	156
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	720
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	720
5088 6001	BIRD EXCLUSION METHOD	SF	152

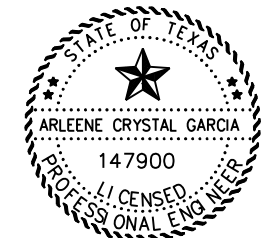
* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

LEGEND:

- PROP. SEEDING AREA
- PROP. CONSTRUCTION EXIT (TY 2)
- PROP. ROCK FILTER DAM (TYPE 2)
- PROP. ROCK FILTER DAM (TYPE 3)
- PROP. TEMP SEDIMENT CONTROL FENCE
- PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

NOTES:

1. THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
2. EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
3. EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
4. CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

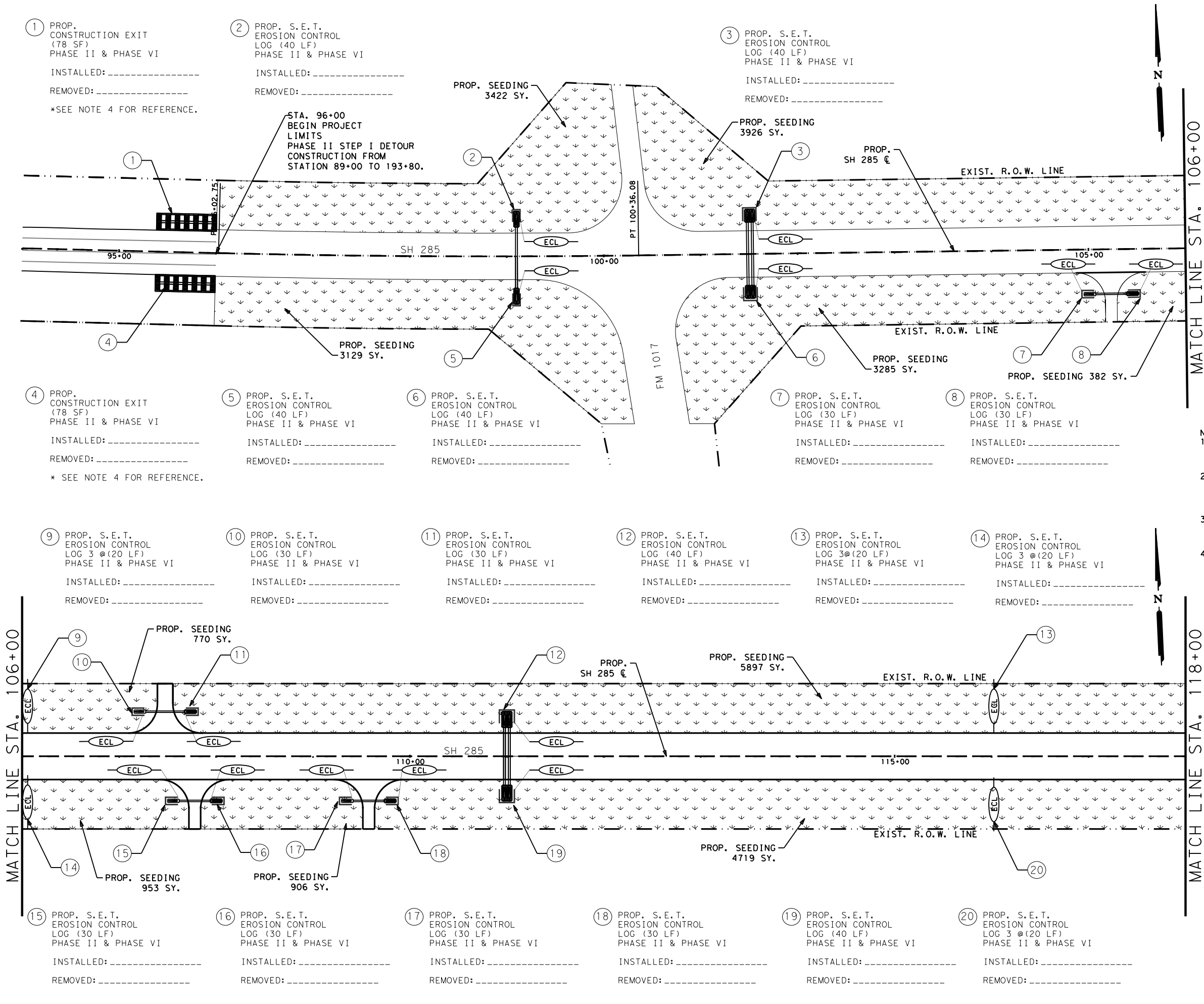
02.13.2024

Pharr District Central Design
 Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1"=100' SHEET 1 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	355



1 PROP. CONSTRUCTION EXIT (78 SF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
 *SEE NOTE 4 FOR REFERENCE.

2 PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

3 PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

4 PROP. CONSTRUCTION EXIT (78 SF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
 * SEE NOTE 4 FOR REFERENCE.

5 PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

6 PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

7 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

8 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

9 PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

10 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

11 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

12 PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

13 PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

14 PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

15 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

16 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

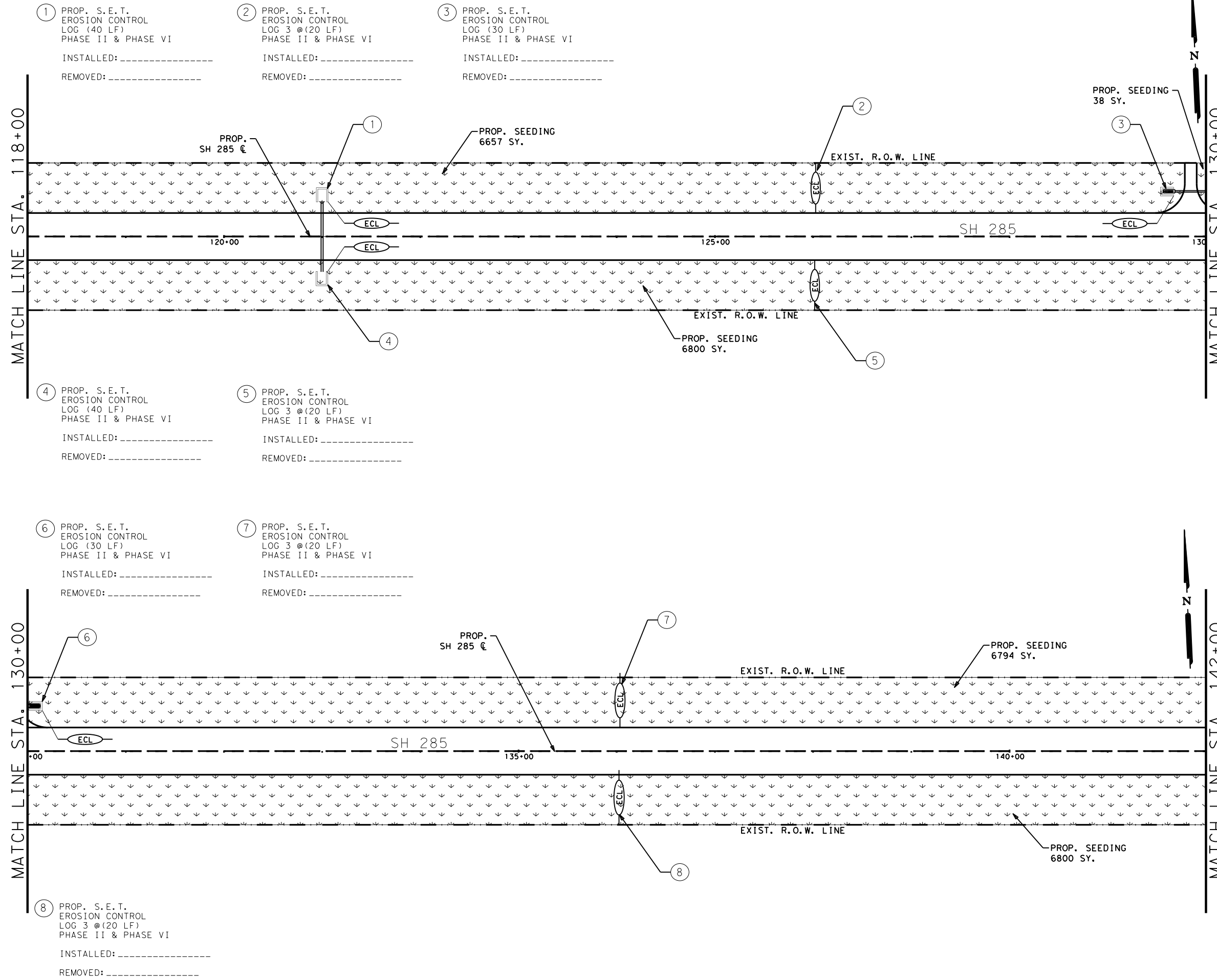
17 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

18 PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

19 PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

20 PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

DATE: 2/7/2024 11:05:20 AM
 FILE: P:\txdot\project\wiseon\line.com\TXDOT15\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 2.dgn



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	27051
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	27051
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	469
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	380
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	380
5088 6001	BIRD EXCLUSION METHOD	SF	-

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

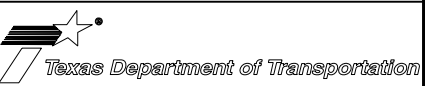
- NOTES:
1. THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 2. EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 3. EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 4. CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 2 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	356

DATE: 2/7/2024 11:05:29 AM
 FILE: D:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 3.dgn

MATCH LINE STA. 142+00.00

MATCH LINE STA. 154+00.00

MATCH LINE STA. 154+00.00

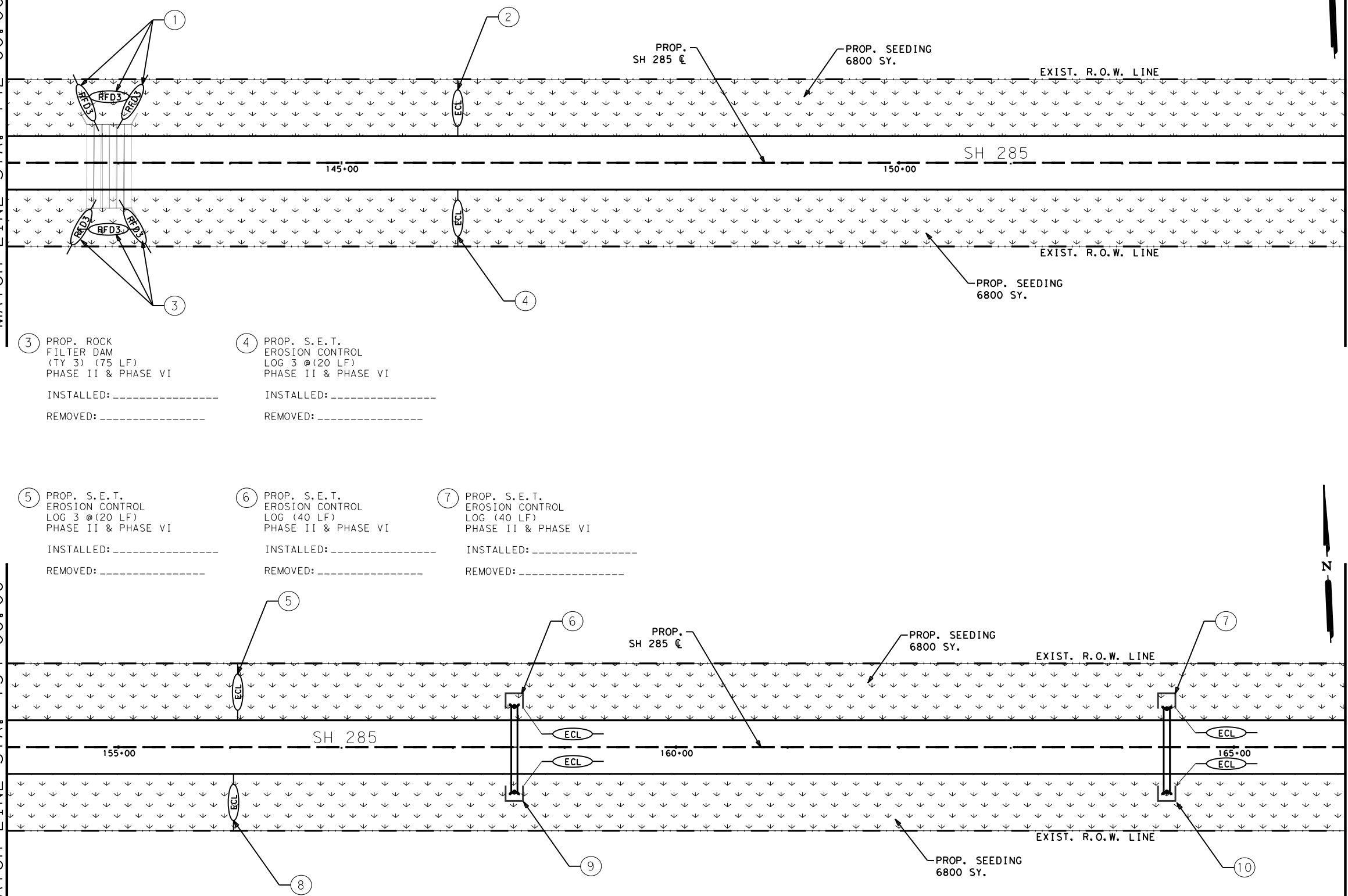
MATCH LINE STA. 166+00.00

- ① PROP. ROCK FILTER DAM (TY 3) (75 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ③ PROP. ROCK FILTER DAM (TY 3) (75 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ④ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑤ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑥ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑦ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑧ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑨ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑩ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----



SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	27200
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	27200
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	472
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	150
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	150
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	400
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	400
5088 6001	BIRD EXCLUSION METHOD	SF	304

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

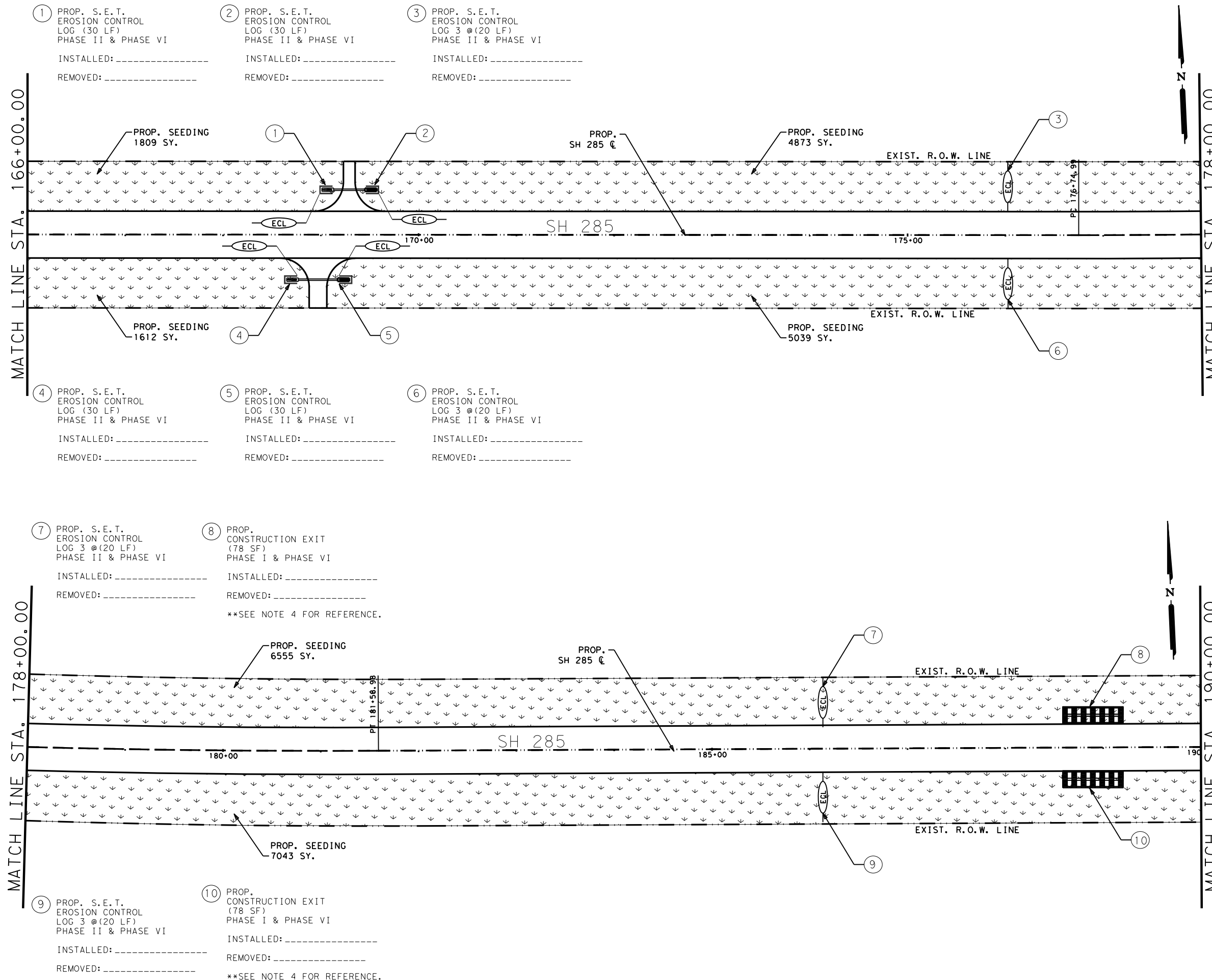
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 3 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	357

DATE: 2/7/2024 11:05:36 AM
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 4.dgn



- ① PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ③ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ④ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑤ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑥ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑦ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑧ PROP. CONSTRUCTION EXIT (78 SF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----
 **SEE NOTE 4 FOR REFERENCE.

- ⑨ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE II & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑩ PROP. CONSTRUCTION EXIT (78 SF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----
 **SEE NOTE 4 FOR REFERENCE.

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	26931
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	26931
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	467
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	156
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	156
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	360
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	360
5088 6001	BIRD EXCLUSION METHOD	SF	-

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

Texas Department of Transportation

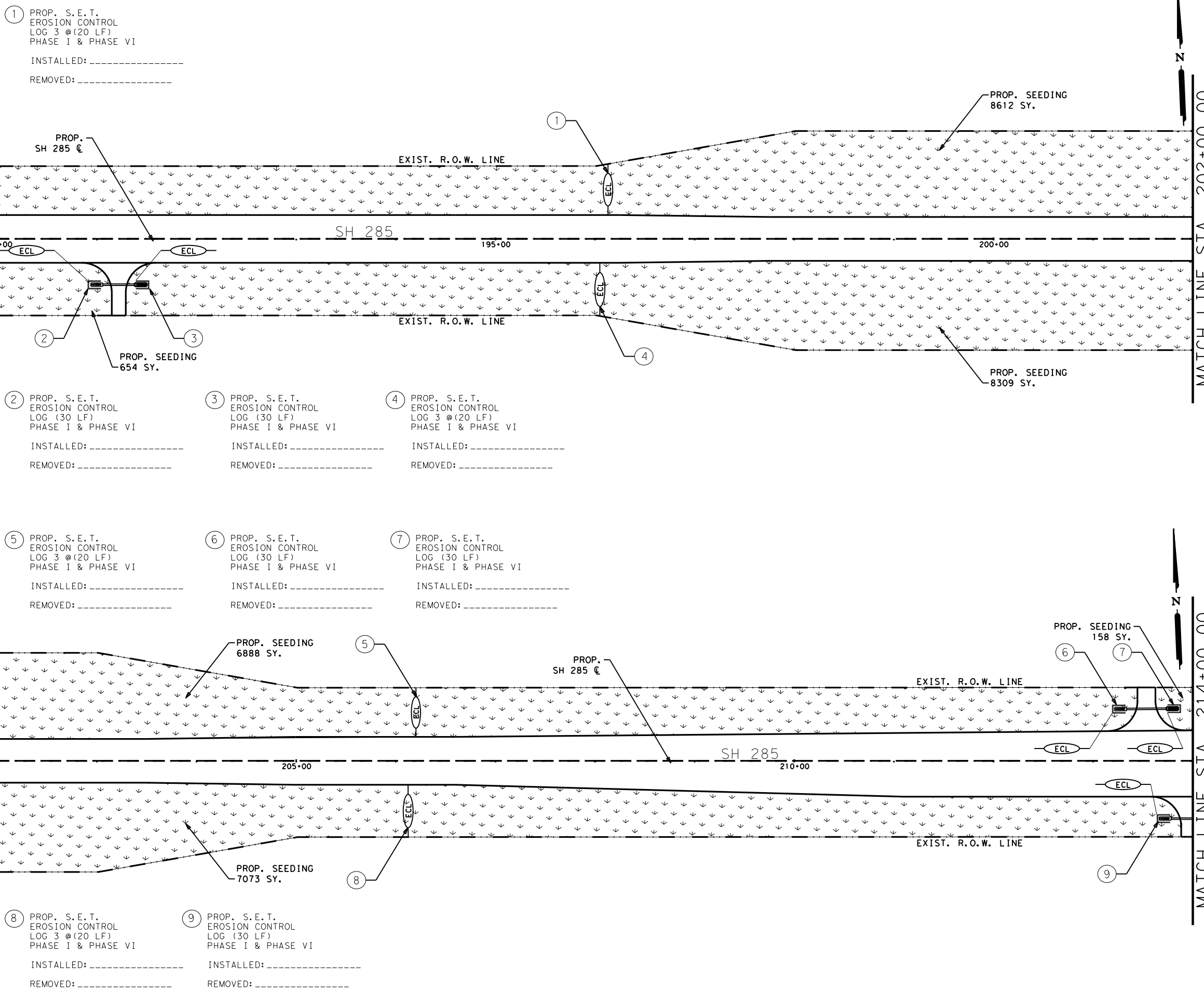
SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 4 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	358

DATE: 2/7/2024 11:05:40 AM
 FILE: D:\twdot\project\wiseon\line.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 5.dgn

MATCH LINE STA. 190+00.00
 MATCH LINE STA. 202+00.00
 MATCH LINE STA. 202+00.00
 MATCH LINE STA. 214+00.00



① PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

② PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

⑤ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

⑧ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

③ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

⑥ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

⑨ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

④ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

⑦ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I & PHASE VI
 INSTALLED: -----
 REMOVED: -----

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	31694
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	31694
*	FERTILIZER	TON	0.33
168 6001	VEGETATIVE WATERING	MG	550
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	390
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	390
5088 6001	BIRD EXCLUSION METHOD	SF	-

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

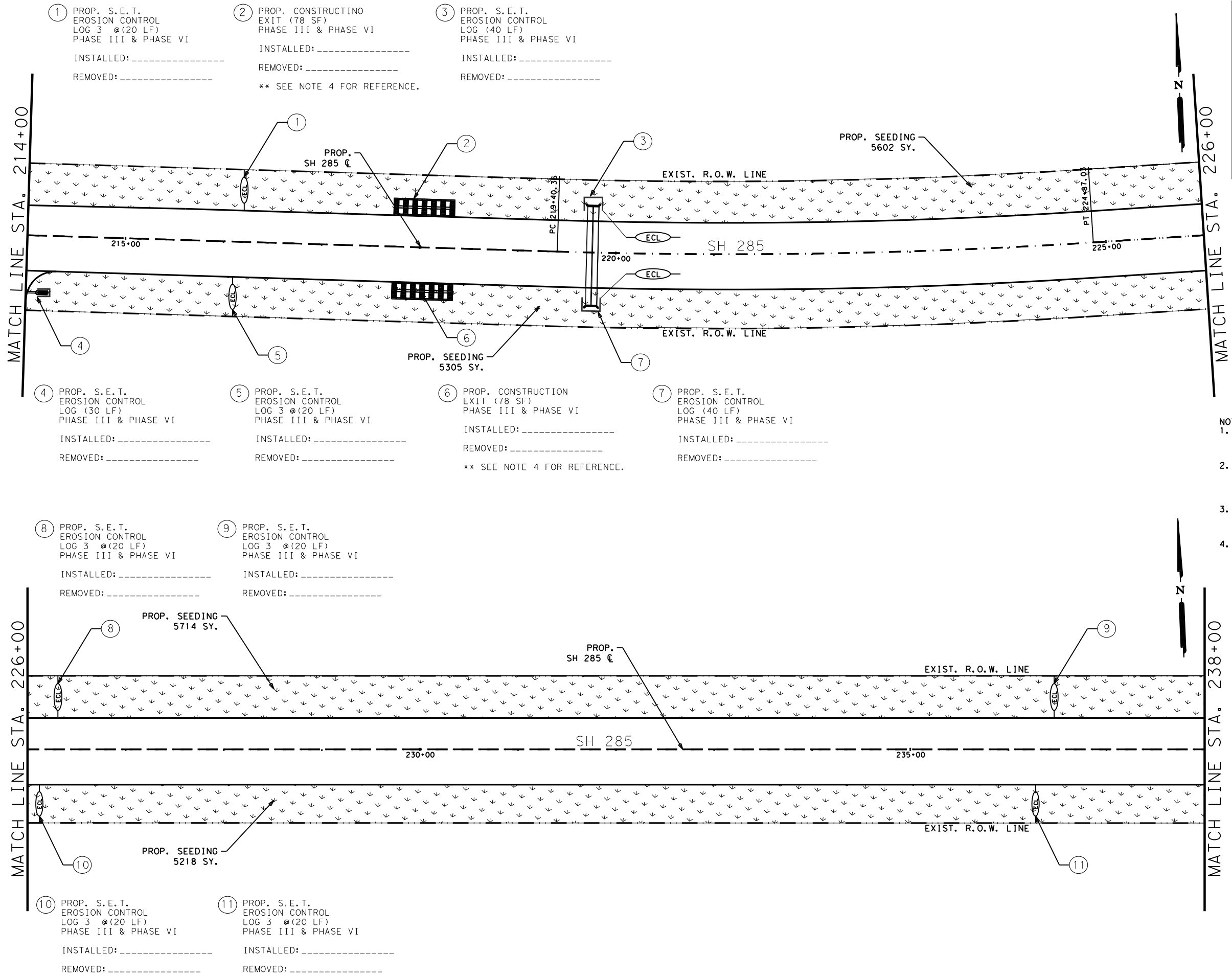
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 5 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	359

DATE: 2/7/2024 11:05:46 AM
 FILE: D:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 6.dgn

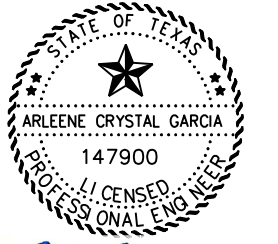


SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21839
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21839
*	FERTILIZER	TON	0.23
168 6001	VEGETATIVE WATERING	MG	379
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	156
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	156
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	470
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	470
5088 6001	BIRD EXCLUSION METHOD	SF	136

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

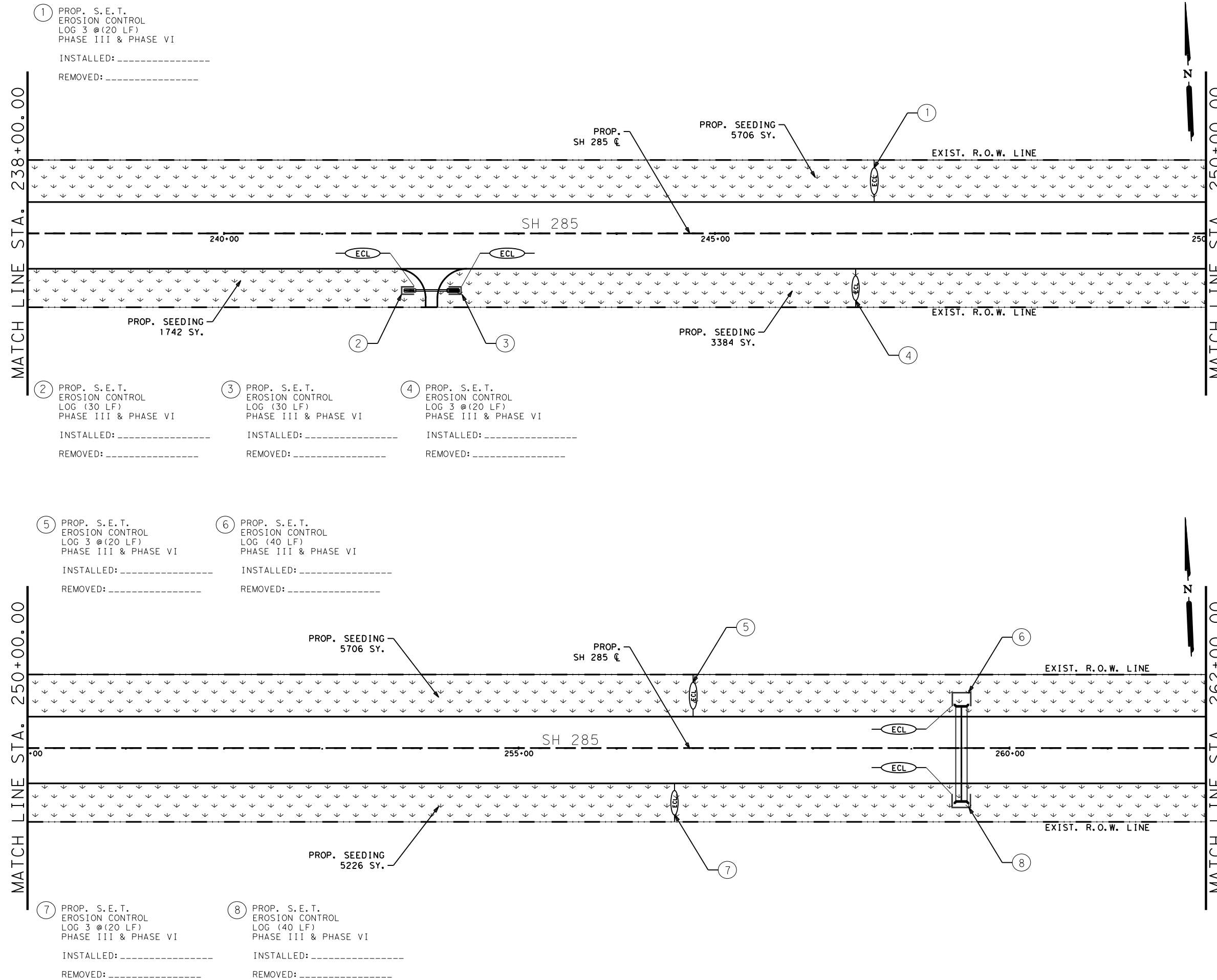


**SH 285
SW3P LAYOUTS**

SCALE: 1"=100' SHEET 6 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	360

DATE: 2/7/2024 11:05:50 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH 285 ENV 7.dgn



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21764
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21764
*	FERTILIZER	TON	0.22
168 6001	VEGETATIVE WATERING	MG	378
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVE)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	380
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	380
5088 6001	BIRD EXCLUSION METHOD	SF	152

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

LEGEND:

- PROP. SEEDING AREA
- PROP. CONSTRUCTION EXIT (TY 2)
- PROP. ROCK FILTER DAM (TYPE 2)
- PROP. ROCK FILTER DAM (TYPE 3)
- PROP. TEMP SEDIMENT CONTROL FENCE
- PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
SW3P LAYOUTS**

SCALE: 1" = 100' SHEET 7 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	361

DATE: 2/7/2024 11:05:57 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH 285 ENV 8.dgn

MATCH LINE STA. 262+00

MATCH LINE STA. 274+00

MATCH LINE STA. 274+00

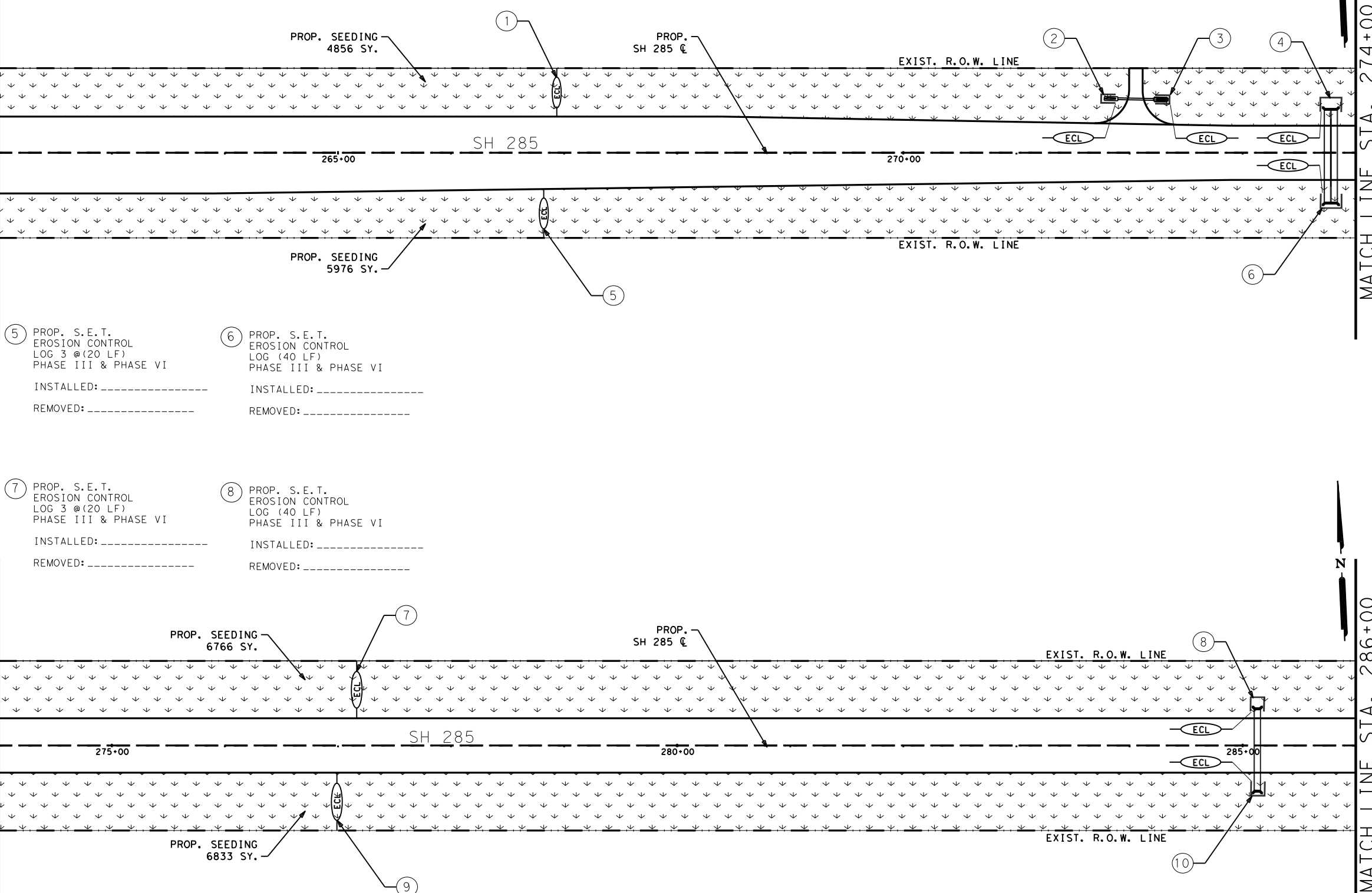
MATCH LINE STA. 286+00

- ① PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____
- ② PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____
- ③ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____
- ④ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____

- ⑤ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____
- ⑥ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____

- ⑦ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____
- ⑧ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____

- ⑨ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____
- ⑩ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE III & PHASE VI
 INSTALLED: _____
 REMOVED: _____



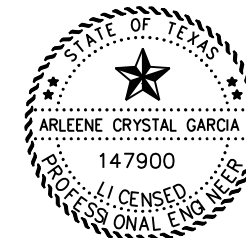
SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	25467
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	25467
*	FERTILIZER	TON	0.26
168 6001	VEGETATIVE WATERING	MG	442
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	460
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	460
5088 6001	BIRD EXCLUSION METHOD	SF	660

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

LEGEND:

- PROP. SEEDING AREA
- PROP. CONSTRUCTION EXIT (TY 2)
- PROP. ROCK FILTER DAM (TYPE 2)
- PROP. ROCK FILTER DAM (TYPE 3)
- PROP. TEMP SEDIMENT CONTROL FENCE
- PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

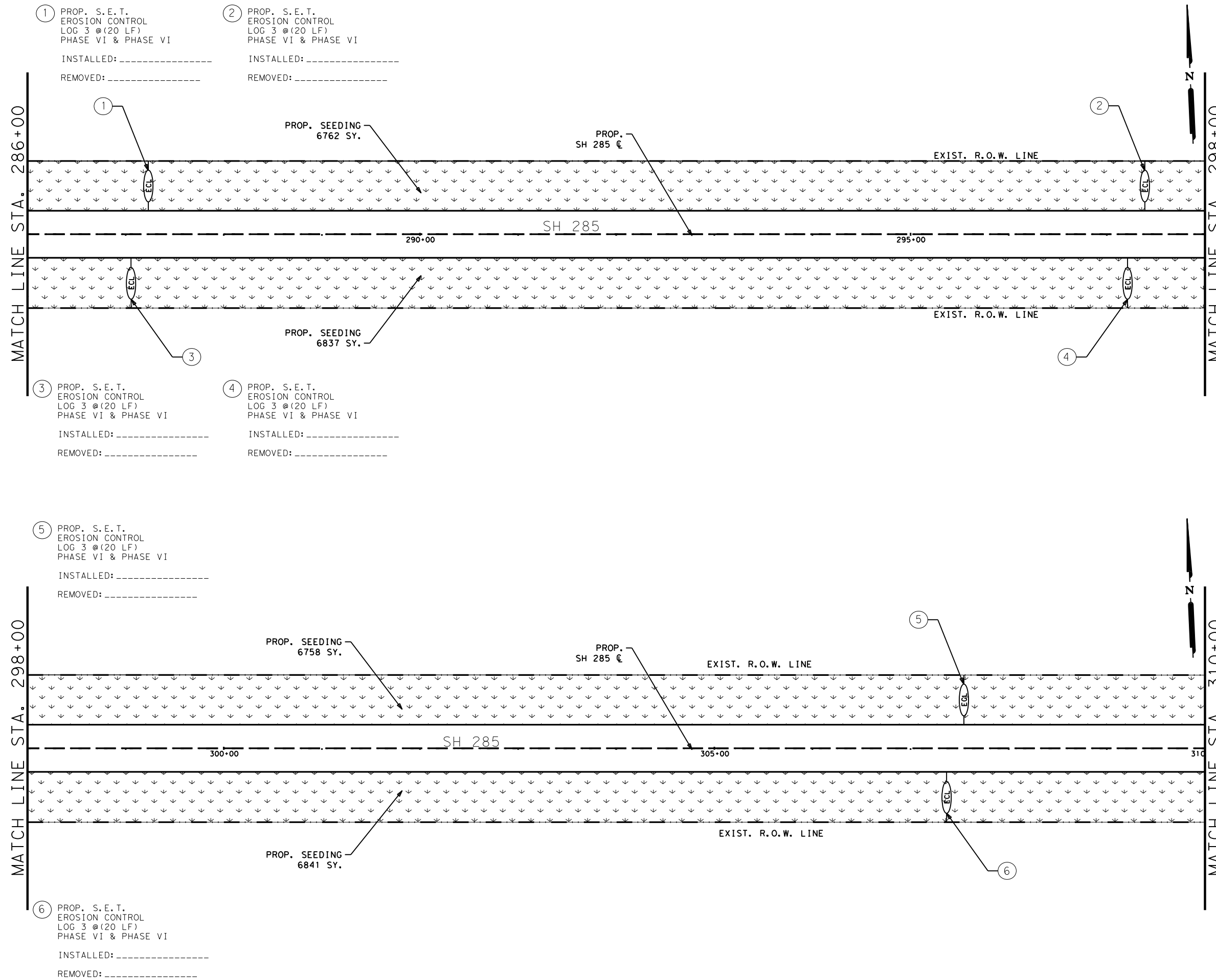


SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 8 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	362

DATE: 2/7/2024 11:06:01 AM
 FILE: D:\projects\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 9.dgn



SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	27198
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	27198
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	472
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	360
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	360
5088 6001	BIRD EXCLUSION METHOD	SF	-

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

LEGEND:

- PROP. SEEDING AREA
- PROP. CONSTRUCTION EXIT (TY 2)
- PROP. ROCK FILTER DAM (TYPE 2)
- PROP. ROCK FILTER DAM (TYPE 3)
- PROP. TEMP SEDIMENT CONTROL FENCE
- PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

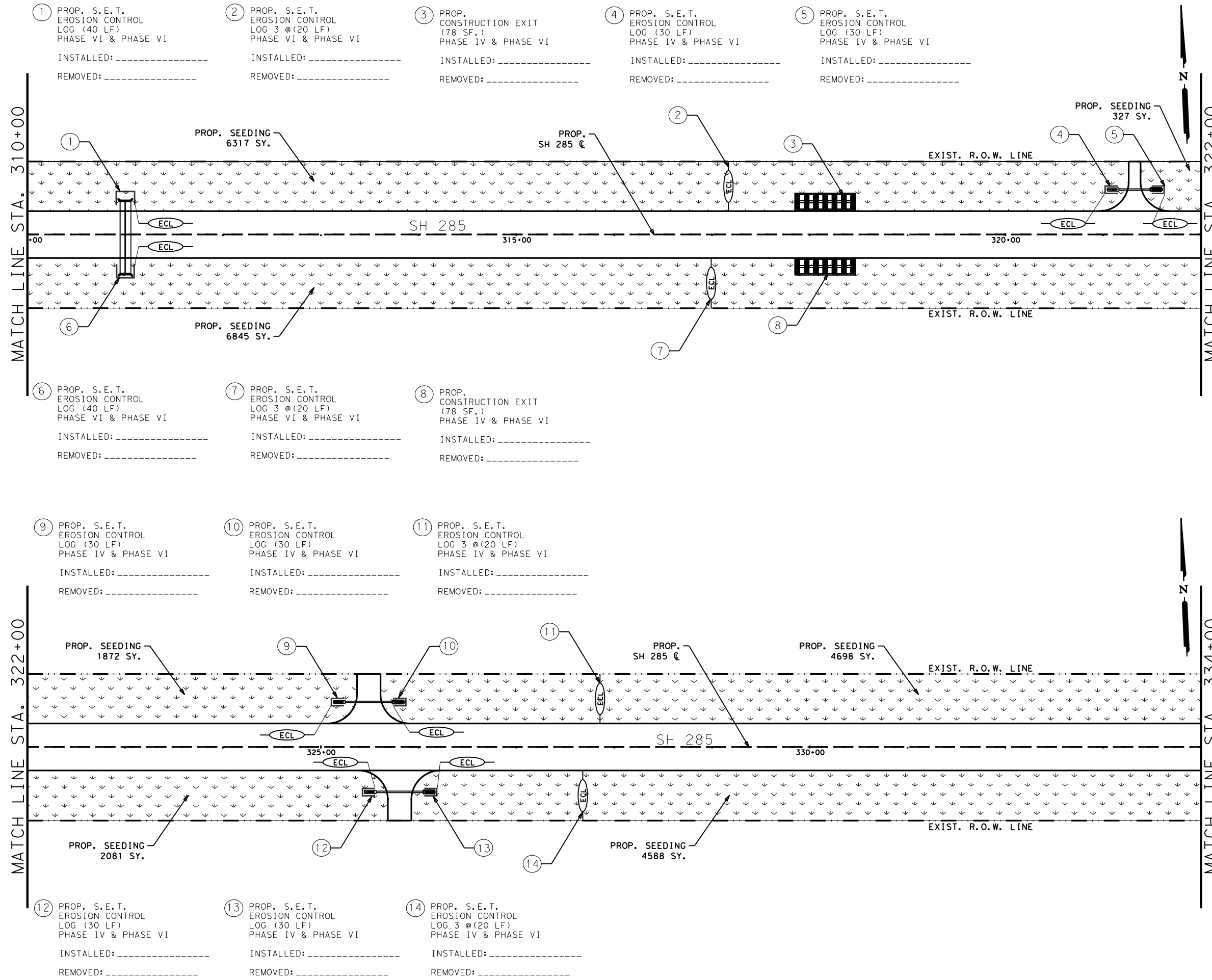


**SH 285
SW3P LAYOUTS**

SCALE: 1" = 100' SHEET 9 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	363

DATE: 2/7/2024 11:06:08 AM
 FILE: \\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH 285 ENV 10.dgn



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED(PERM) (RURAL) (CLAY)	SY	26728
164 6029	CELL FBR MLCH SEED(TEMP) (WARM)	SY	26728
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	464
506 6003	ROCK FIL. DAM(TY3)INSTALL	LF	-
506 6011	ROCK FIL. DAM(TY3)REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	156
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	156
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	500
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	500
5088 6001	BIRD EXCLUSION METHOD	SF	330

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

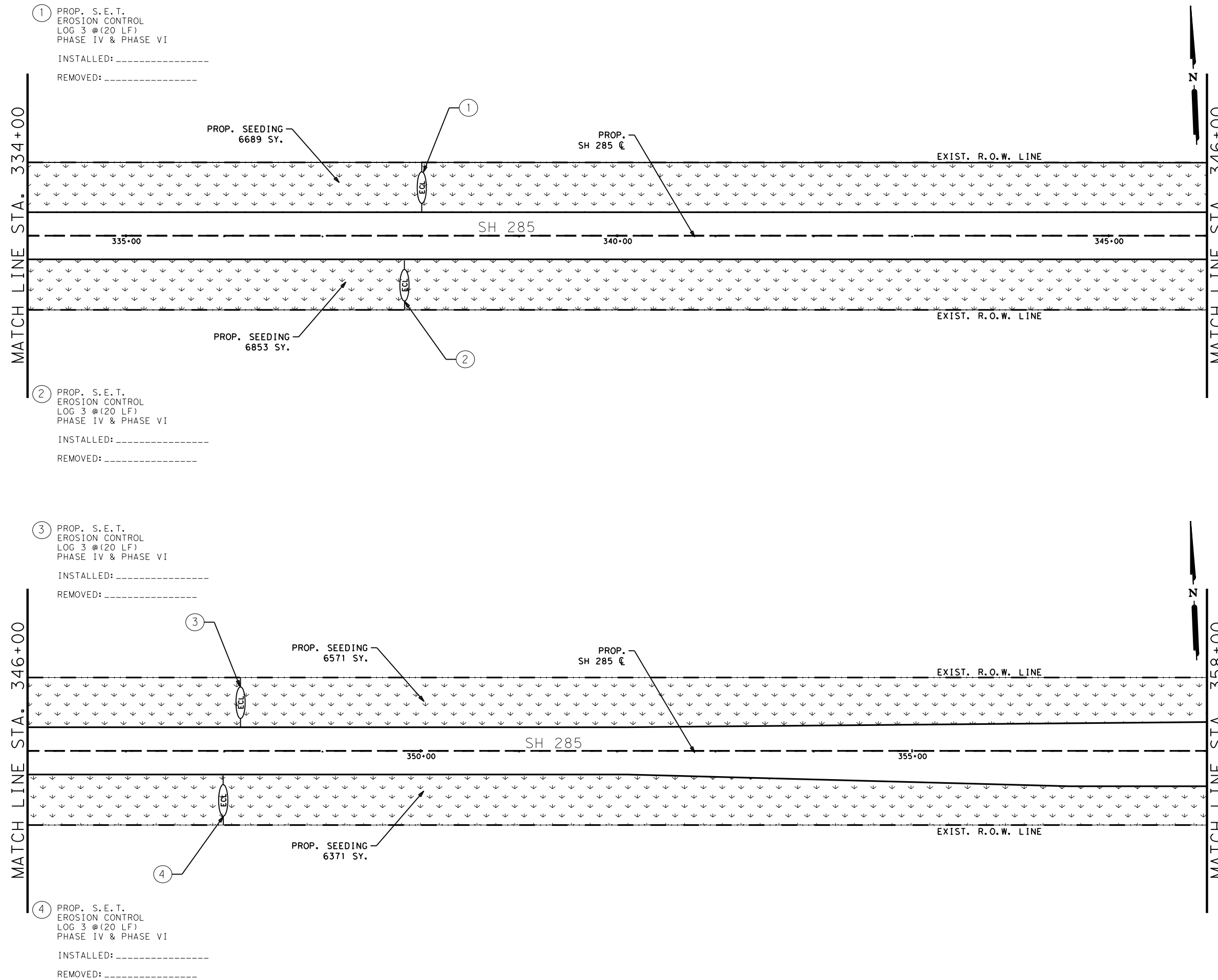
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 10 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	364

DATE: 2/7/2024 11:06:12 AM
 FILE: D:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 11.dgn



- ① PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ③ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ④ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	26484
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	26484
*	FERTILIZER	TON	0.27
168 6001	VEGETATIVE WATERING	MG	460
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVE)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	240
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	240
5088 6001	BIRD EXCLUSION METHOD	SF	-

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
1. THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 2. EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 3. EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 4. CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

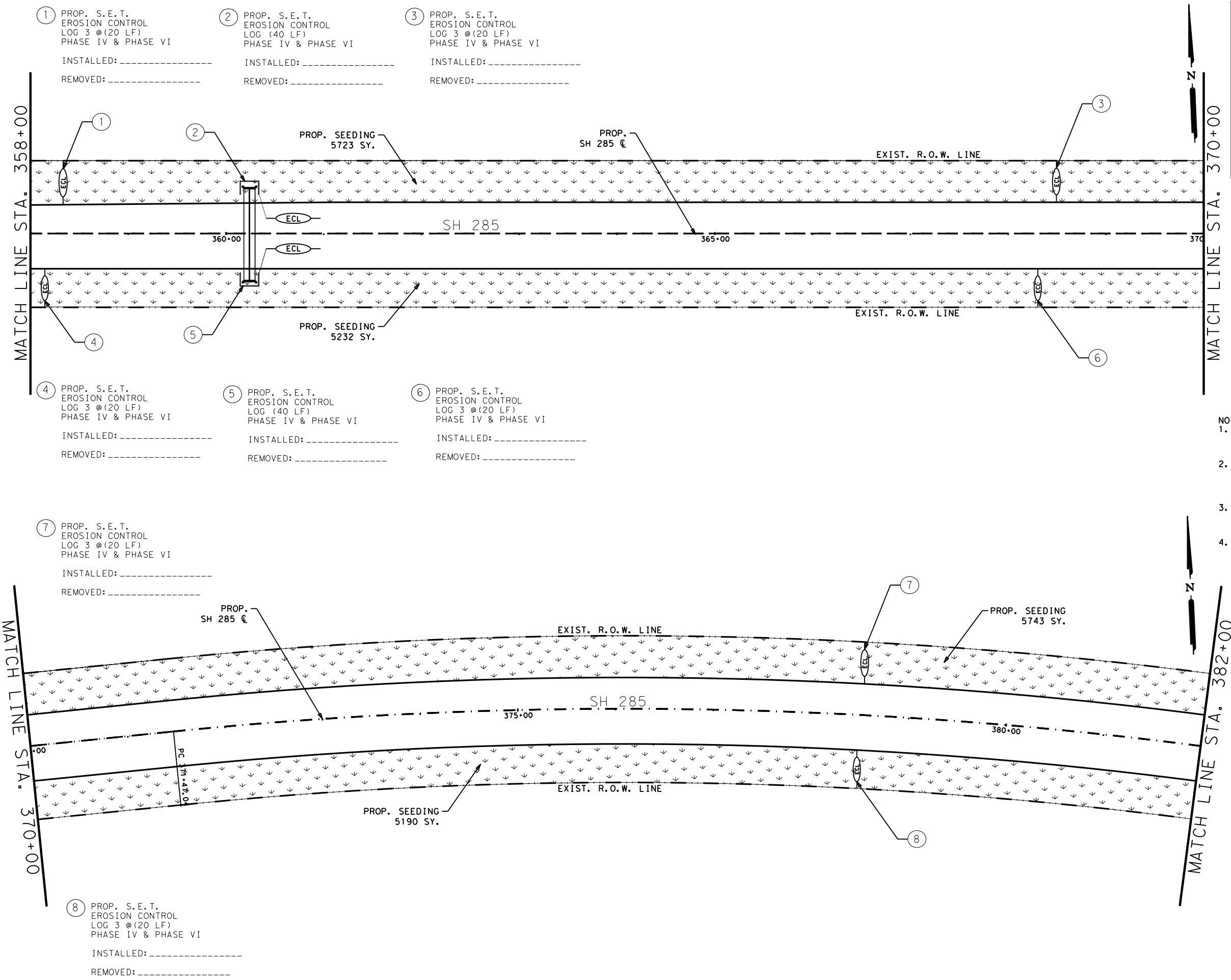
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 11 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	365

DATE: 2/7/2024 11:06:18 AM
 FILE: \\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 12.dgn



- ① PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ③ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ④ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑤ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑥ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑦ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑧ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21888
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21888
*	FERTILIZER	TON	0.23
168 6001	VEGETATIVE WATERING	MG	380
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	440
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	440
5088 6001	BIRD EXCLUSION METHOD	SF	462

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

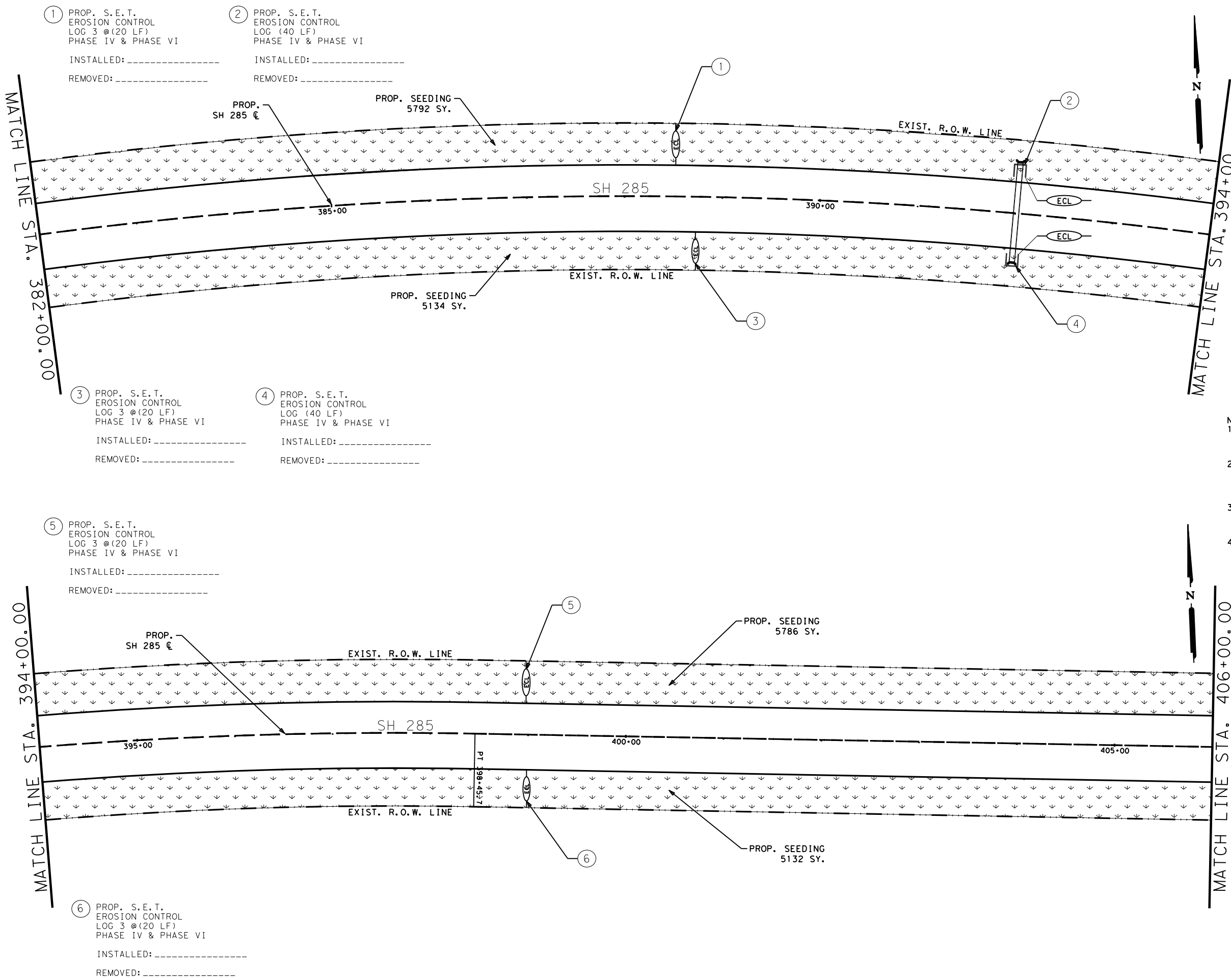
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 12 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	366

DATE: 2/7/2024 11:06:23 AM
 FILE: D:\t\dot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 13.dgn



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21844
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21844
*	FERTILIZER	TON	0.23
168 6001	VEGETATIVE WATERING	MG	379
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	320
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	320
5088 6001	BIRD EXCLUSION METHOD	SF	213

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

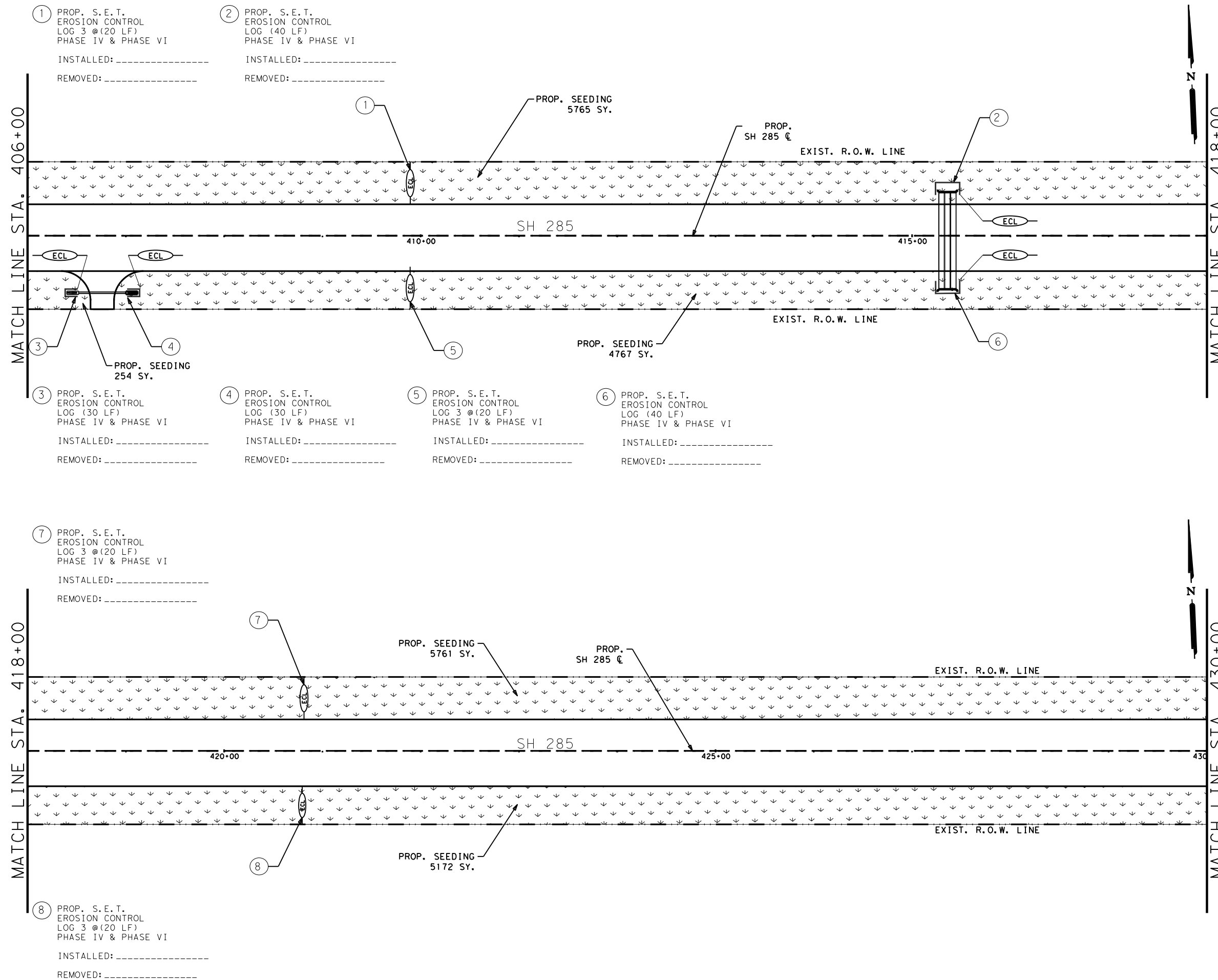
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 13 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	367

DATE: 2/7/2024 11:06:29 AM
 FILE: D:\txdot\project\wiseon\line.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 14.dgn



- ① PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ③ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ④ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑤ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ⑥ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑦ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑧ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE IV & PHASE VI
 INSTALLED: -----
 REMOVED: -----

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21719
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21719
*	FERTILIZER	TON	0.22
168 6001	VEGETATIVE WATERING	MG	377
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	380
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	380
5088 6001	BIRD EXCLUSION METHOD	SF	660

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

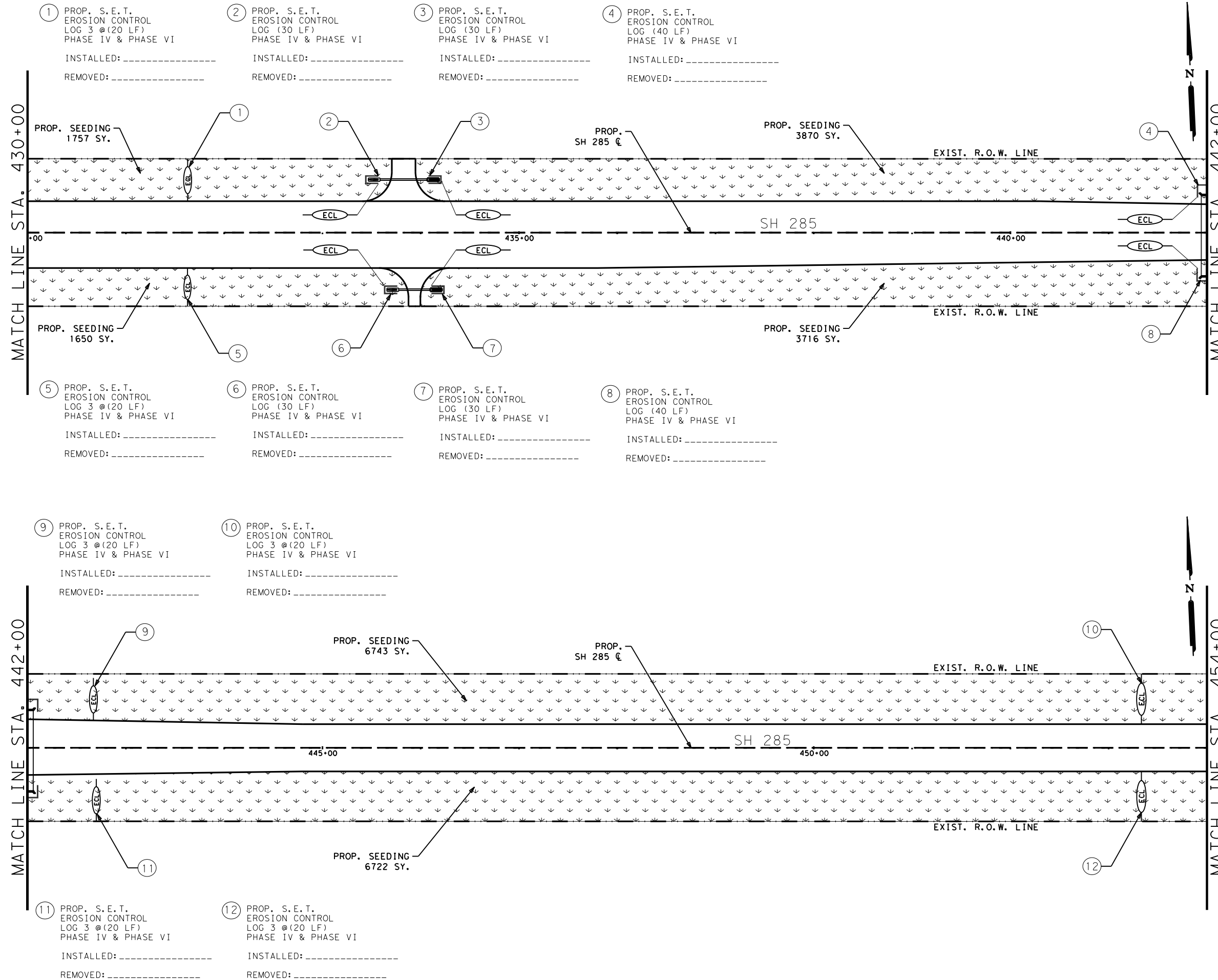
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 14 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	368

DATE: 2/7/2024 11:06:36 AM
 FILE: D:\projects\TXDOT\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH 285 ENV 15.dgn



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED(PERM) (RURAL) (CLAY)	SY	24458
164 6029	CELL FBR MLCH SEED(TEMP) (WARM)	SY	24458
*	FERTILIZER	TON	0.25
168 6001	VEGETATIVE WATERING	MG	424
506 6003	ROCK FIL. DAM(TY3)INSTALL	LF	-
506 6011	ROCK FIL. DAM(TY3)REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	560
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	560
5088 6001	BIRD EXCLUSION METHOD	SF	396

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

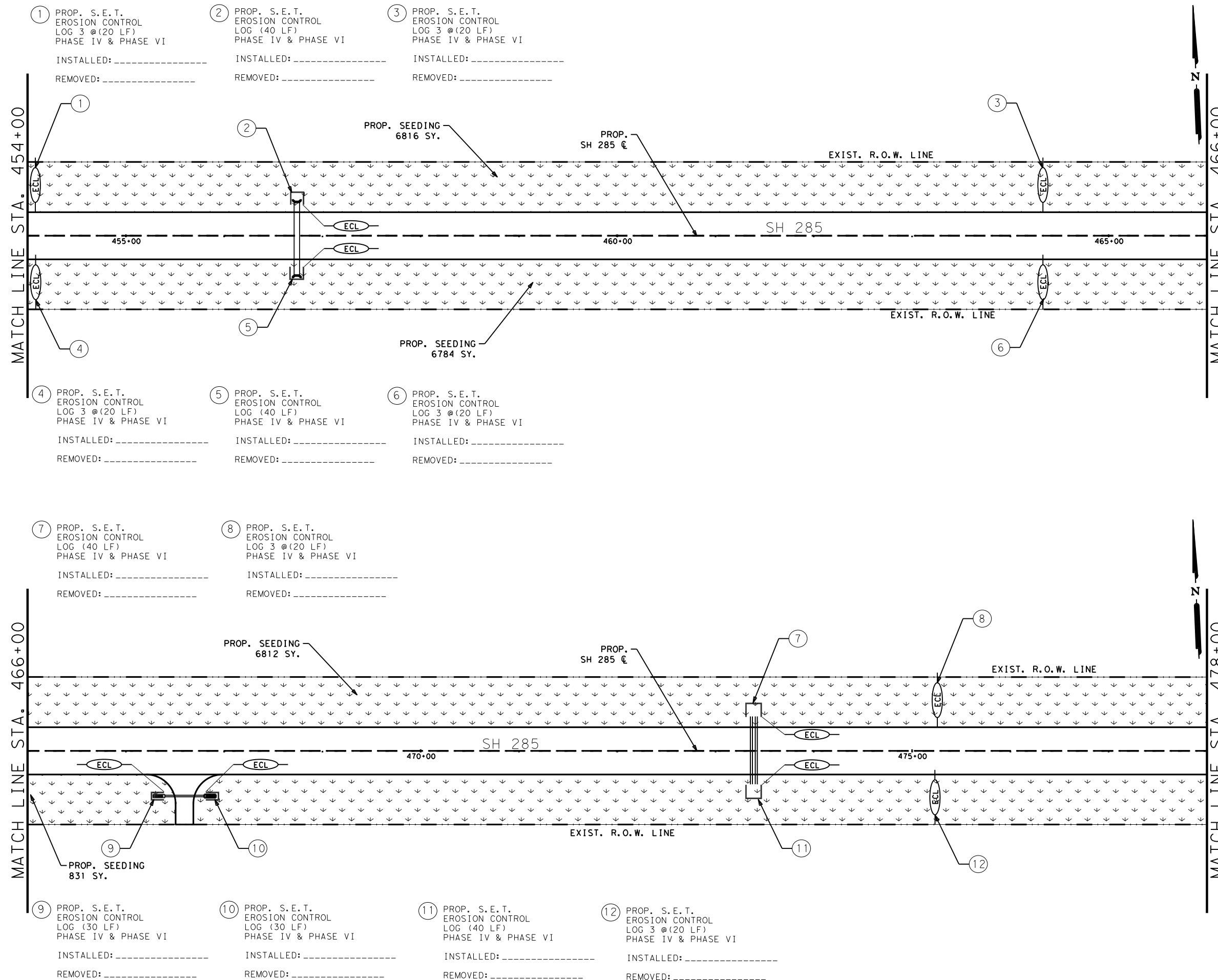
Texas Department of Transportation

**SH 285
SW3P LAYOUTS**

SCALE: 1" = 100' SHEET 15 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	369

DATE: 2/7/2024 11:06:40 AM
 FILE: P:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH 285 ENV 16.dgn



SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	27055
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	27055
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	470
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	580
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	580
5088 6001	BIRD EXCLUSION METHOD	SF	330

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

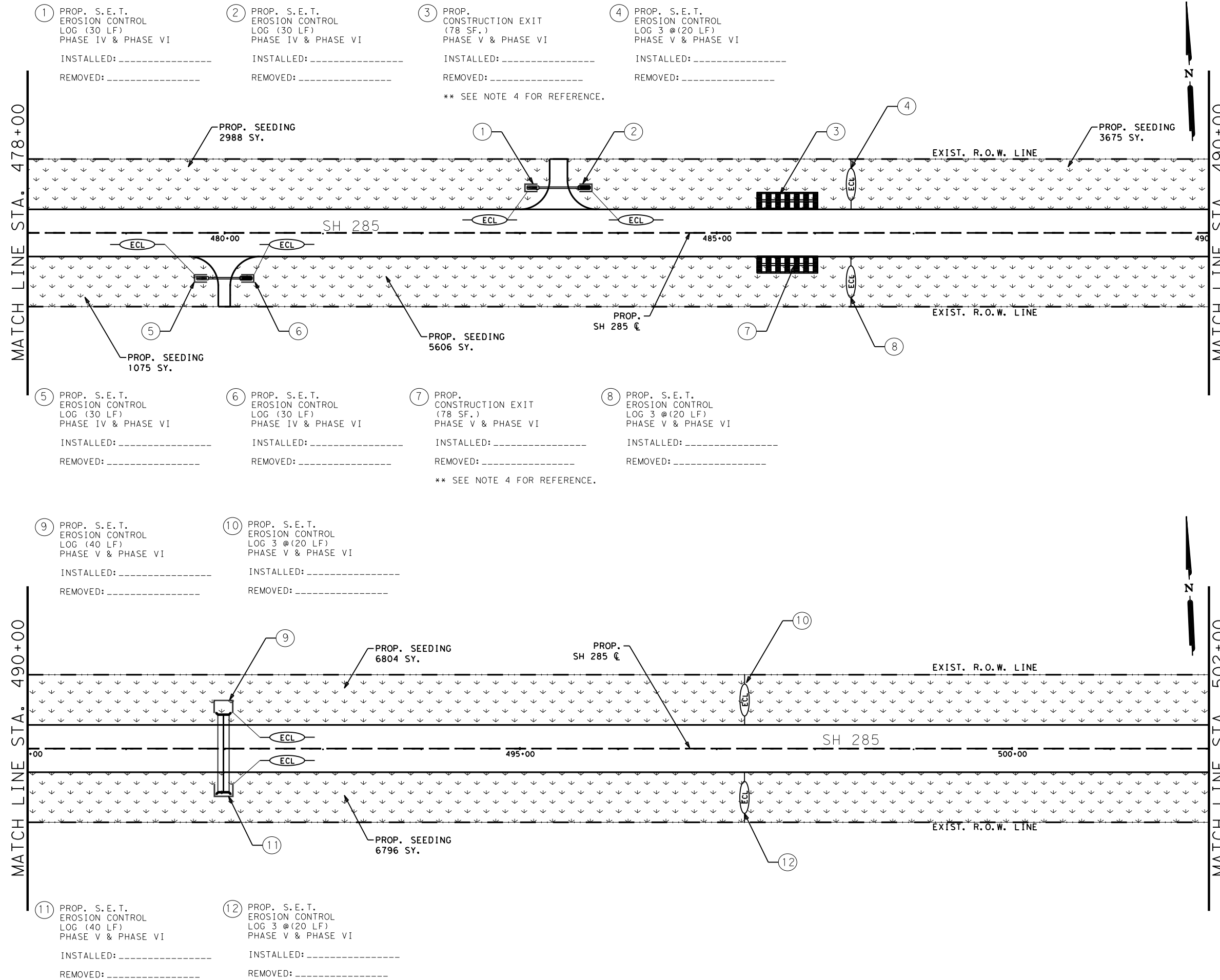
Texas Department of Transportation

**SH 285
SW3P LAYOUTS**

SCALE: 1" = 100' SHEET 16 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	370

DATE: 2/7/2024 11:06:47 AM
 FILE: P:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 17.dgn



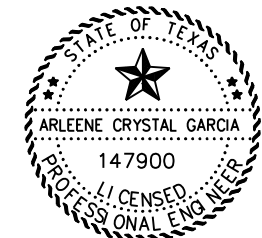
SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED(PERM) (RURAL) (CLAY)	SY	26944
164 6029	CELL FBR MLCH SEED(TEMP) (WARM)	SY	26944
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	468
506 6003	ROCK FIL. DAM(TY3)INSTALL	LF	-
506 6011	ROCK FIL. DAM(TY3)REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	156
506 6024	CONSTRUCTION EXITS (REMOVE)	EA	156
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	420
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	420
5088 6001	BIRD EXCLUSION METHOD	SF	660

* FOR CONTRACTOR INFORMATION ONLY
(NON-PAY ITEM)

LEGEND:

- PROP. SEEDING AREA
- PROP. CONSTRUCTION EXIT (TY 2)
- PROP. ROCK FILTER DAM (TYPE 2)
- PROP. ROCK FILTER DAM (TYPE 3)
- PROP. TEMP SEDIMENT CONTROL FENCE
- PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
1. THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 2. EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 3. EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 4. CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

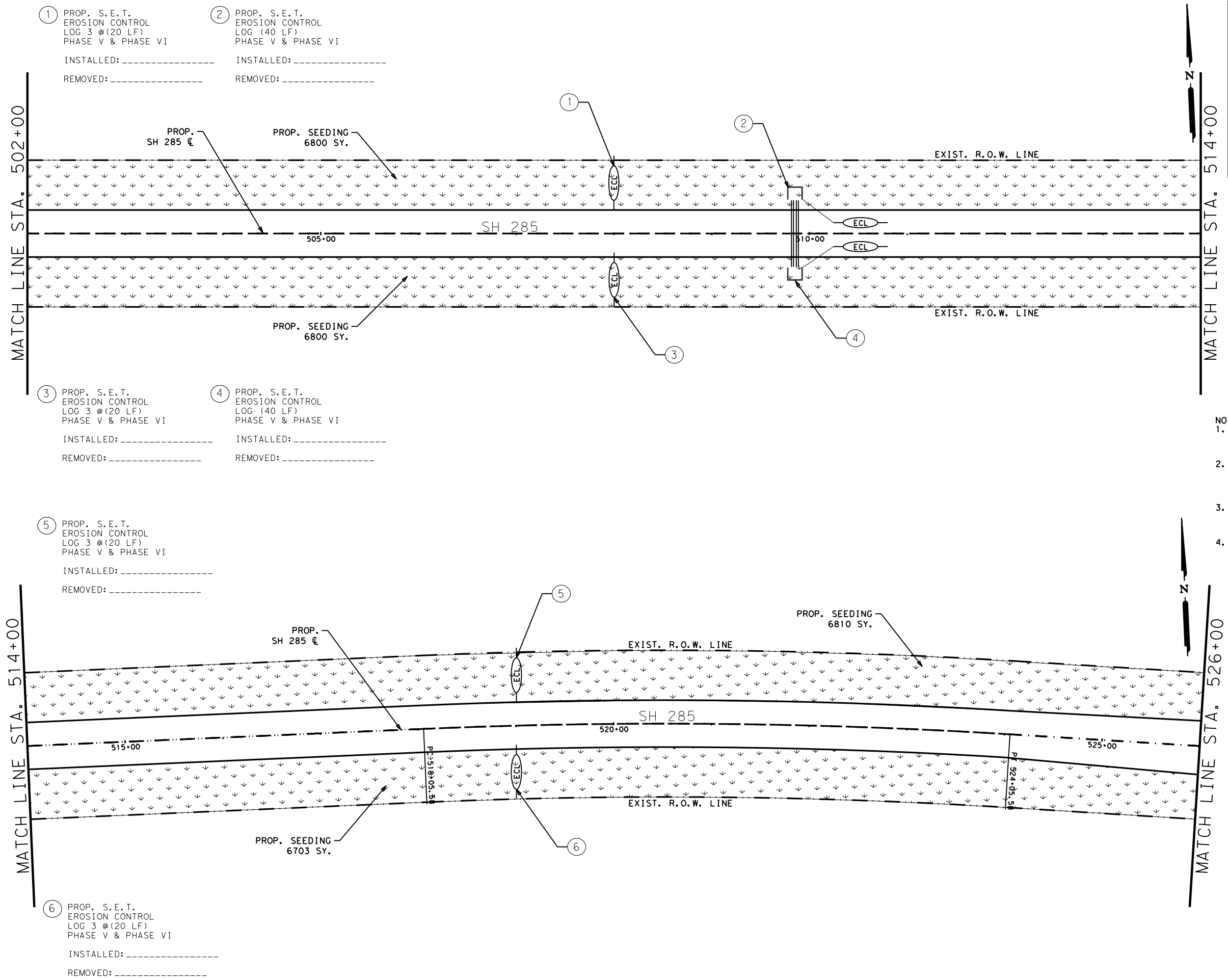


SH 285
SW3P LAYOUTS

SCALE: 1"=100' SHEET 17 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	371

DATE: 2/7/2024 11:06:51 AM
 FILE: D:\t\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 18.dgn



- ① PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE V & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE V & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ③ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE V & PHASE VI
 INSTALLED: -----
 REMOVED: -----
- ④ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE V & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑤ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE V & PHASE VI
 INSTALLED: -----
 REMOVED: -----

- ⑥ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE V & PHASE VI
 INSTALLED: -----
 REMOVED: -----

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	27113
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	27113
*	FERTILIZER	TON	0.28
168 6001	VEGETATIVE WATERING	MG	471
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVE)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	320
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	320
5088 6001	BIRD EXCLUSION METHOD	SF	198

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
SW3P LAYOUTS**

SCALE: 1" = 100' SHEET 18 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	372

DATE: 2/7/2024 11:06:58 AM
 FILE: D:\txdot\project\wiseon\line.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 19.dgn

MATCH LINE STA. 526+00.00

MATCH LINE STA. 538+00.00

MATCH LINE STA. 538+00.00

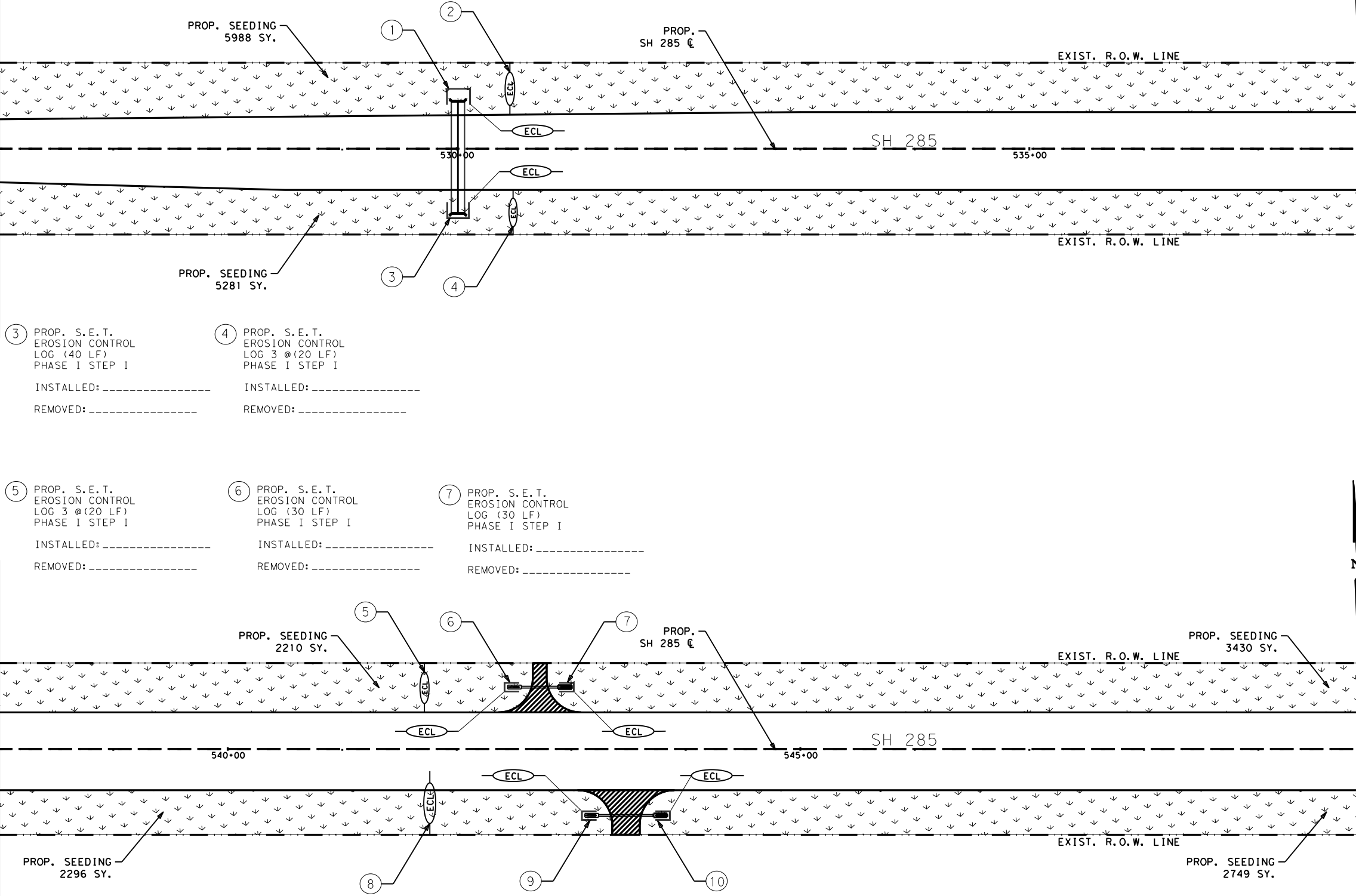
MATCH LINE STA. 550+00.00

- ① PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----
- ② PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----

- ③ PROP. S.E.T. EROSION CONTROL LOG (40 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----
- ④ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----

- ⑤ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----
- ⑥ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----
- ⑦ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----

- ⑧ PROP. S.E.T. EROSION CONTROL LOG 3 @ (20 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----
- ⑨ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----
- ⑩ PROP. S.E.T. EROSION CONTROL LOG (30 LF) PHASE I STEP I
 INSTALLED: -----
 REMOVED: -----



SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21954
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21954
*	FERTILIZER	TON	0.23
168 6001	VEGETATIVE WATERING	MG	381
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	440
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	440
5088 6001	BIRD EXCLUSION METHOD	SF	660

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

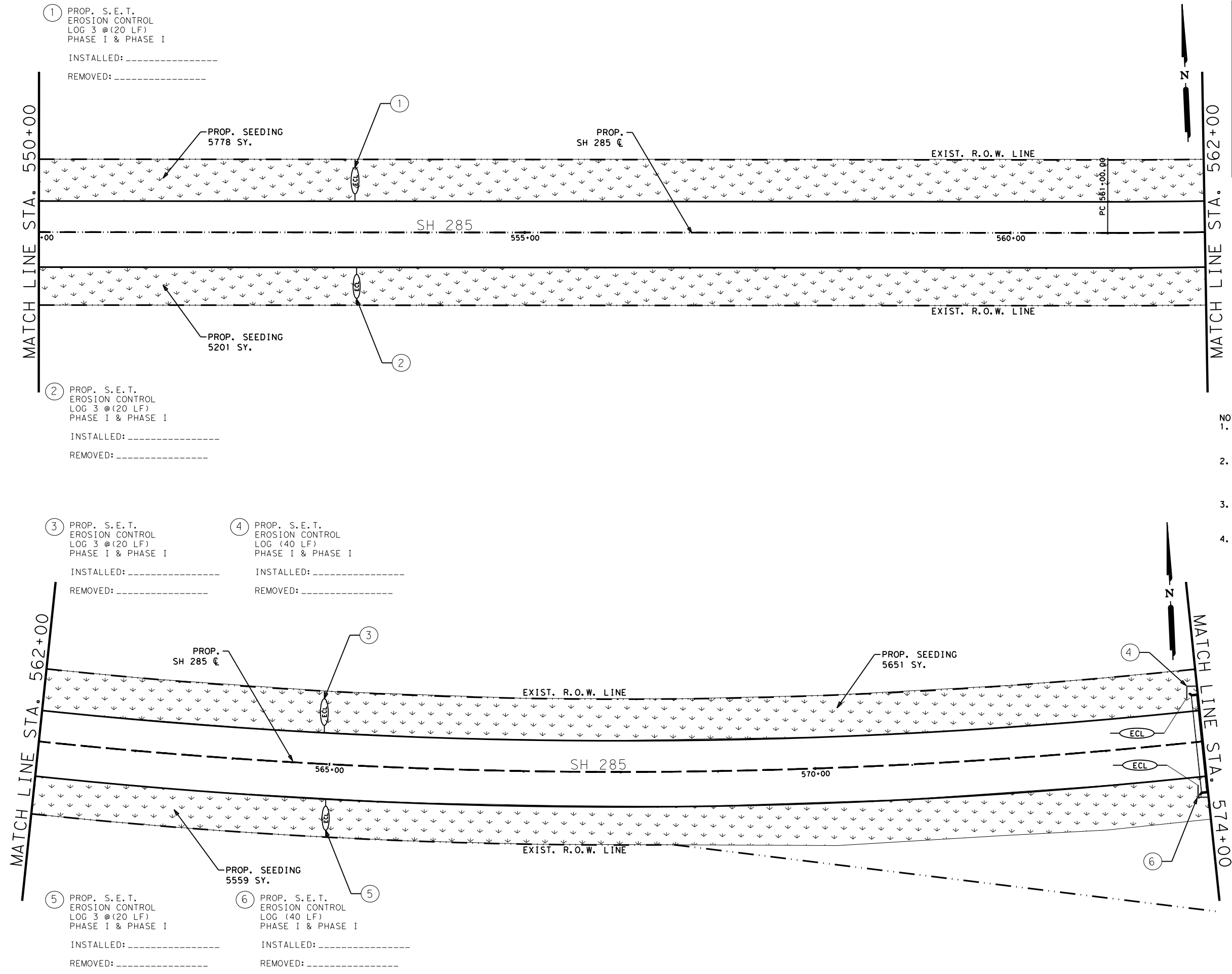
Texas Department of Transportation

SH 285 SW3P LAYOUTS

SCALE: 1" = 100' SHEET 19 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	373

DATE: 2/7/2024 11:07:02 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 20.dgn

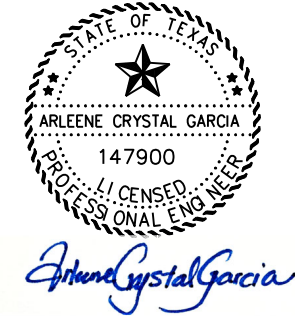


SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	22189
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	22189
*	FERTILIZER	TON	0.23
168 6001	VEGETATIVE WATERING	MG	385
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	320
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	320
5088 6001	BIRD EXCLUSION METHOD	SF	-

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
- THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 - EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 - EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

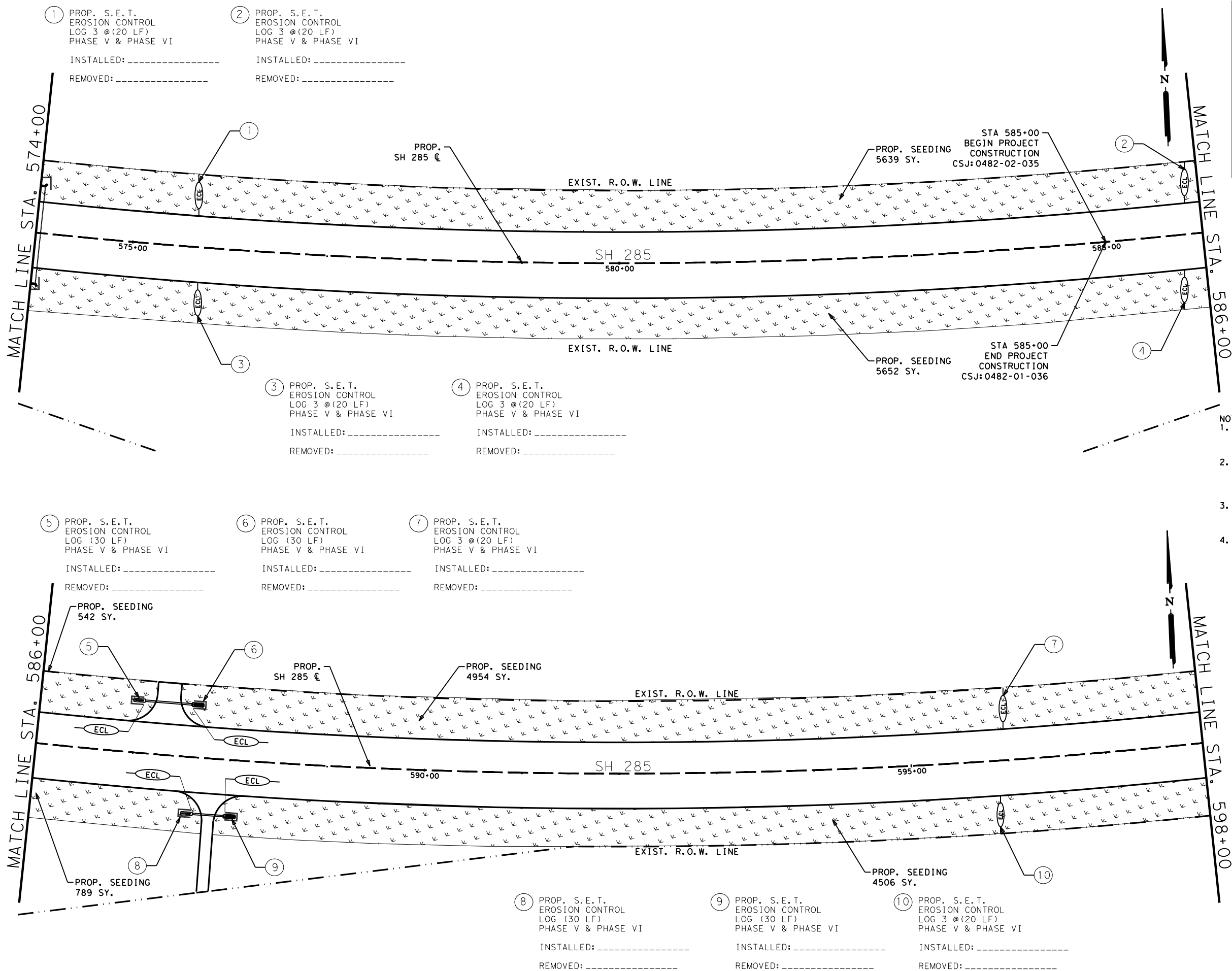
Texas Department of Transportation

**SH 285
 SW3P LAYOUTS**

SCALE: 1"=100' SHEET 20 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	374

DATE: 2/7/2024 11:07:08 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9 - Environmental\SH_285 ENV 21.dgn



SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	22082
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	22082
*	FERTILIZER	TON	0.23
168 6001	VEGETATIVE WATERING	MG	383
506 6003	ROCK FIL. DAM (TY3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	-
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	-
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	480
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	480
5088 6001	BIRD EXCLUSION METHOD	SF	360

* FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)

LEGEND:

- PROP. SEEDING AREA
- PROP. CONSTRUCTION EXIT (TY 2)
- PROP. ROCK FILTER DAM (TYPE 2)
- PROP. ROCK FILTER DAM (TYPE 3)
- PROP. TEMP SEDIMENT CONTROL FENCE
- PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
1. THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 2. EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 3. EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 4. CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

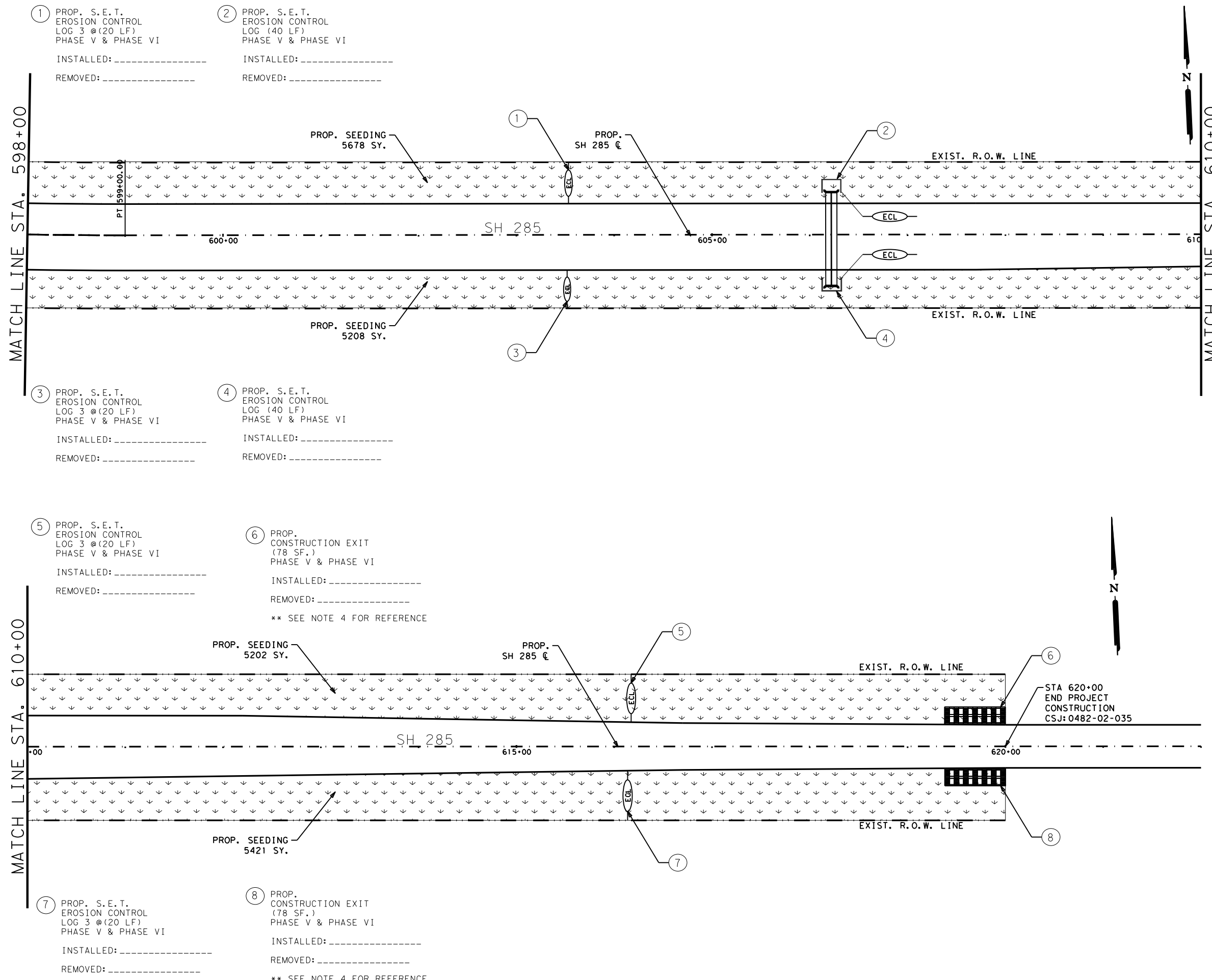


SH 285
SW3P LAYOUTS

SCALE: 1"=100' SHEET 21 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	375

DATE: 2/7/2024 11:07:13 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\9. Environmental\SH 285 ENV 22.dgn

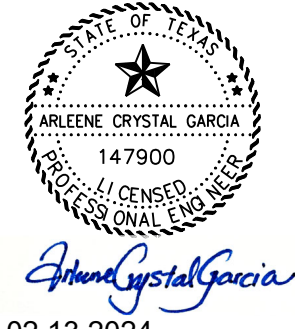


SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
164 6023	CELL FBR MLCH SEED (PERM) (RURAL) (CLAY)	SY	21509
164 6029	CELL FBR MLCH SEED (TEMP) (WARM)	SY	21509
*	FERTILIZER	TON	0.22
168 6001	VEGETATIVE WATERING	MG	373
506 6003	ROCK FIL. DAM (TY 3) INSTALL	LF	-
506 6011	ROCK FIL. DAM (TY 3) REMOVE	LF	-
506 6021	CONSTRUCTION EXITS (INSTALL)	EA	156
506 6024	CONSTRUCTION EXITS (REMOVAL)	EA	156
506 6038	TEMP. SED. CNTL FENCE (INSTALL)	EA	-
506 6039	TEMP. SED. CNTL FENCE (REMOVE)	EA	-
506 6041	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (INSTALL)	LF	320
506 6043	BIODEGRADABLE EROSION CNTRL LOGS (12 INCH DIAM) (REMOVE)	LF	320
5088 6001	BIRD EXCLUSION METHOD	SF	-

- * FOR CONTRACTOR INFORMATION ONLY (NON-PAY ITEM)
- LEGEND:
- PROP. SEEDING AREA
 - PROP. CONSTRUCTION EXIT (TY 2)
 - PROP. ROCK FILTER DAM (TYPE 2)
 - PROP. ROCK FILTER DAM (TYPE 3)
 - PROP. TEMP SEDIMENT CONTROL FENCE
 - PROP. BIODEGRADABLE EROSION CONTROL LOG (12")

- NOTES:
1. THE CONTRACTOR MUST CONTACT THE ENGINEER BEFORE PLACEMENT OF ANY EROSION CONTROL DEVICES.
 2. EROSION CONTROL DEVICES SHALL ONLY BE PLACED DURING PHASE CONSTRUCTION. DEVICES SHALL NOT BE PLACED ALL AT THE SAME TIME.
 3. EROSION CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 4. CONSTRUCTION EXITS ARE TO BE LOCATED AND INSTALLED BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. THE LOCATION OF THE CONSTRUCTION EXITS SHOULD BE SUFFICIENT FOR CONTINUOUS USE BY THE CONTRACTOR DURING CONSTRUCTION.



02.13.2024

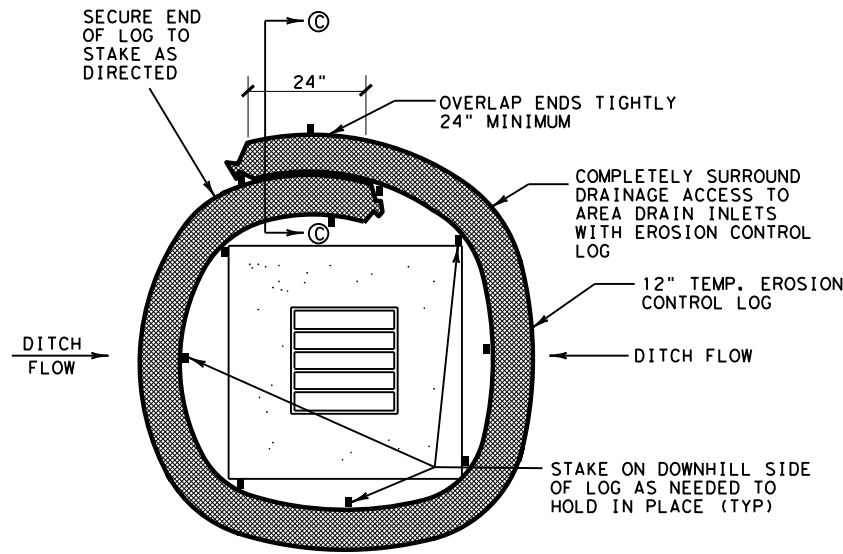
Pharr District Central Design
 Texas Department of Transportation

SH 285
 SW3P LAYOUTS

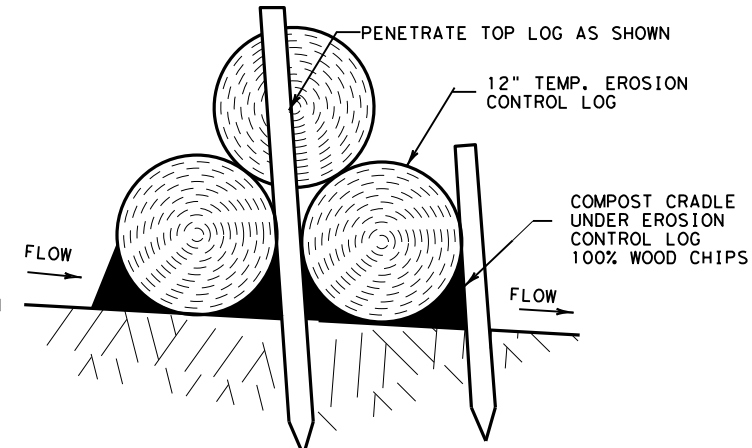
SCALE: 1" = 100' SHEET 22 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	376

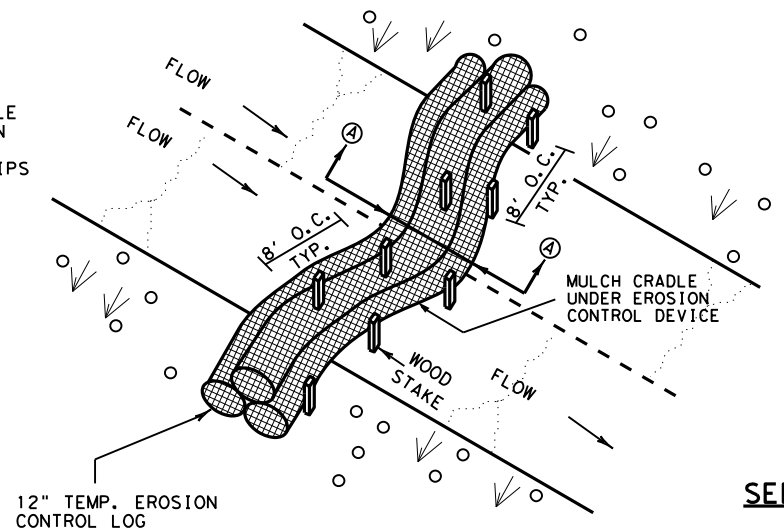
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.



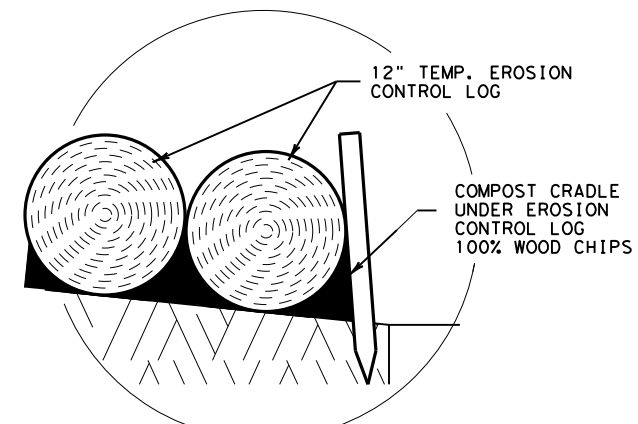
DROP INLET SEDIMENT TRAP
DI-ST



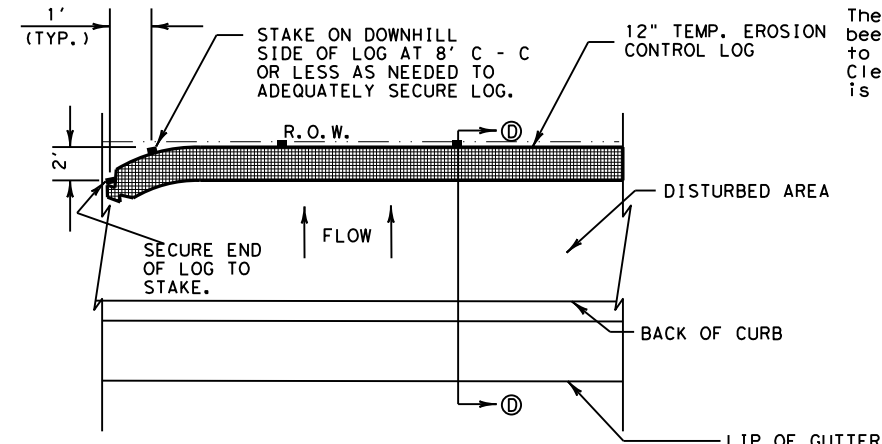
SECTION A-A DITCH LINE SEDIMENT TRAP A-A
DL-ST



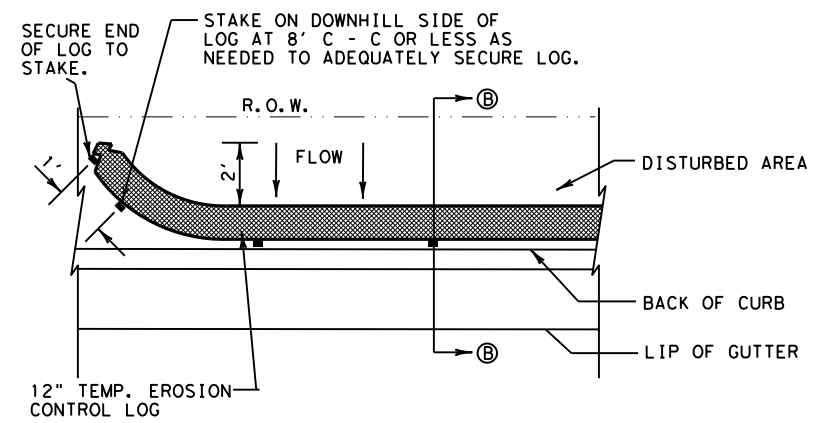
DITCH LINE SEDIMENT TRAP
DL-ST



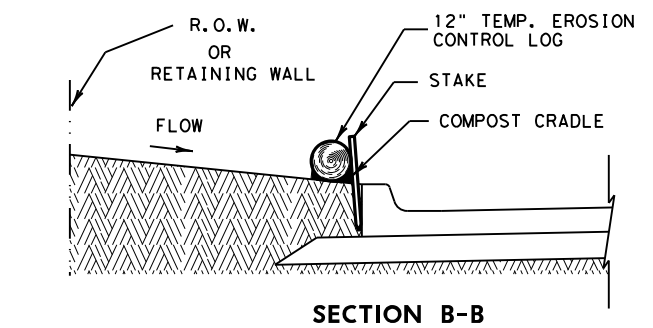
SECTION C-C OVERLAP WITH COMPOST CRADLE
OVERLAP DETAIL PLAN VIEW
NTS



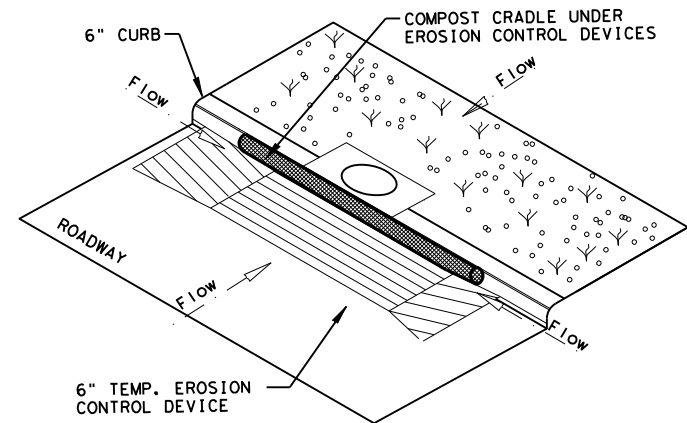
PLAN VIEW
NTS



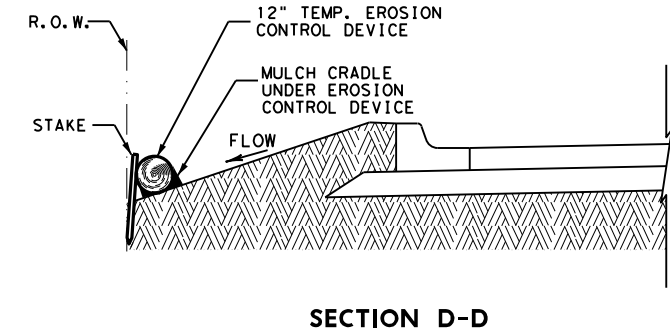
PLAN VIEW
NTS



SECTION B-B BACK OF CURB INLET SEDIMENT TRAP
BOCI-ST



SECTION D-D CURB INLET SEDIMENT TRAP
CI-ST



SECTION D-D RIGHT-OF-WAY SEDIMENT TRAP
ROW-ST

PLANS SHEET LEGEND

- DI-ST DROP INLET SEDIMENT TRAP
- DL-ST DITCH LINE SEDIMENT TRAP
- BOCI-ST BACK OF CURB INLET SEDIMENT TRAP
- ROW-ST RIGHT OF WAY SEDIMENT TRAP
- CI-ST CURB INLET SEDIMENT TRAP

SEDIMENT BASIN & TRAP USAGE GUIDELINES

A sediment trap may be used to precipitate sediment out of runoff draining from an unstabilized area.

Traps: the drainage area for a sediment trap should not exceed 5 acres. The trap capacity should be 1800 CF/Acre (0.5" over the drainage area).

Sediment traps should be placed in the following locations:

1. Immediately preceding drain inlets
2. Just before the drainage enters a water course
3. Just before the drainage leaves the right of way
4. Just before the drainage leaves the construction limits where drainage flows away from the project

The trap should be cleaned when the capacity has been reduced by 1/2 or the sediment has accumulated to a depth of 1', whichever is less. Cleaning and removal of accumulated sediment deposits is incidental and will not be paid for separately.

GENERAL NOTES

1. LENGTHS OF EROSION CONTROL LOGS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND AS REQUIRED FOR THE PURPOSE INTENDED. MAXIMUM LENGTH OF LOGS SHALL BE 30' FOR 12" DIAMETER LOGS.
2. UNLESS OTHERWISE DIRECTED, USE BIODEGRADABLE OR PHOTODEGRADABLE CONTAINMENT MESH ONLY WHERE LOG WILL REMAIN IN PLACE AS PART OF A VEGETATIVE SYSTEM. FOR TEMPORARY INSTALLATIONS, USE RECYCLABLE CONTAINMENT MESH. STUFF LOGS WITH SUFFICIENT FILTER MATERIAL TO ACHIEVE DENSITY THAT WILL HOLD SHAPE WITHOUT EXCESSIVE DEFORMATION.
3. STAKES SHALL BE 2" X 2" WOOD 4' LONG, EMBEDDED SUCH THAT 2" PROTRUDES ABOVE LOG.
4. COMPOST CRADLE MATERIAL IS INCIDENTAL AND WILL NOT BE PAID FOR SEPARATELY.

PHARR DISTRICT STANDARD

Texas Department of Transportation
© TxDOT 2017

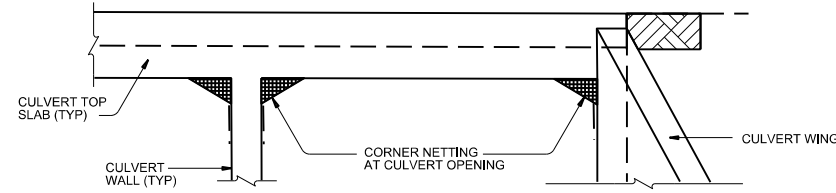
**TEMPORARY EROSION CONTROL LOGS
TECL-17 (PHR)**

FED. RD. DIV. NO. 6	PROJECT NO.		HIGHWAY NO. SH 285
STATE TEXAS	DISTRICT PHARR	COUNTY JIM HOGG	SHEET NO. 377
CONTROL 0482	SECTION 01	JOB 036, etc.	

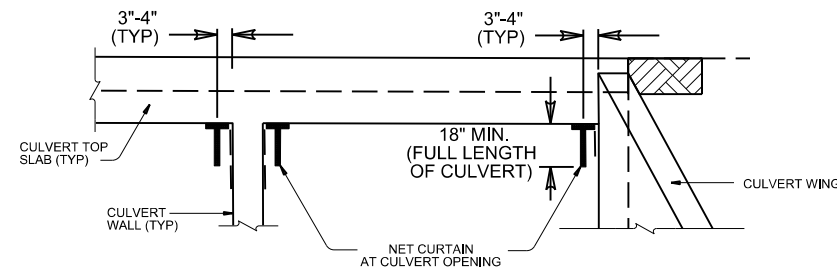
LEVELS DISPLAYED
1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16
17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32
33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48
49 50 51 52 53 54 55 56 57 58 59 60 61 62 63

TYPICAL BIRD EXCLUSION MEASURE INSTALLATION DETAILS FOR A BOX CULVERT

TYPICAL CORNER NETTING INSTALLATION



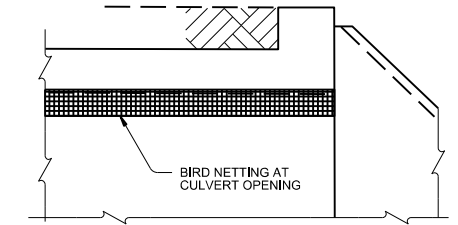
TYPICAL PARAPET CURTAIN INSTALLATION DETAILS



GENERAL NOTES:

1. CLIFF SWALLOWS AND OTHER MIGRATORY BIRDS ARE FEDERALLY PROTECTED UNDER THE FEDERAL MIGRATORY BIRD TREATY ACT. THIS STATUTE INCLUDES LIVE AND DEAD BIRDS, ANY PART, NEST, OR EGG. UNLESS PERMITTED BY REGULATIONS, IT PROHIBITS PURSUIT, HUNTING, TAKING, CAPTURING, KILLING, AND ATTEMPTING TO TAKE ANY MIGRATORY BIRD. NESTING ACTIVITY TYPICALLY OCCURS DURING THE MONTHS OF FEBRUARY THROUGH SEPTEMBER.
2. ANY PROJECT WITH A CULVERT(S) HAS POTENTIAL FOR NESTING BY SWALLOWS OR OTHER MIGRATORY BIRDS. ROADWAY CONSTRUCTION THAT DOES NOT INCLUDE SPECIFIC WORK ON CULVERT(S) WITHIN THE PROJECT AREA MAY ALSO HAVE THE POTENTIAL TO RESULT IN A 'TAKE' OF MIGRATORY BIRDS.
3. THE CONTRACTOR SHALL IMPLEMENT EXCLUSIONARY MEASURES TO PREVENT SWALLOWS FROM BUILDING NEW NESTS PRIOR TO OR DURING THE NESTING SEASON (FEBRUARY 15 - OCTOBER 1) ON ANY CULVERT(S) WITH EVIDENCE OF PRIOR NESTING.
4. 'NON-ACTIVE' NESTS SHOULD BE REMOVED PRIOR TO OR DURING THE NESTING SEASON IN ACCORDANCE WITH STANDARD SPECIFICATION ITEM 427 BLAST CLEANING OR MANUFACTURER DIRECTION. HOWEVER, NO NESTS SHOULD BE REMOVED PRIOR TO COORDINATION WITH THE DISTRICT ENVIRONMENTAL COORDINATOR.
5. EXCLUSION MEASURES GENERALLY INCLUDE THE FOLLOWING: FOR CULVERTS A VARIETY OF OPTIONS EXIST; PAINTING OF SPECIAL 'STICKY' COATINGS TO THE UPPER SURFACES, INSTALLATION OF NET CURTAINS FROM THE CEILING, AND INSTALLATION OF TRIANGULAR 'CORNER' STRUCTURES. SEE LAYOUTS FOR MORE DETAIL.
6. FOR LIQUID OR GEL METHODS, THE SURFACE PREPARATION AND COATING INSTALLATION WILL BE MADE IN ACCORDANCE WITH ITEM 427 SURFACE FINISH FOR CONCRETE OR MANUFACTURER DIRECTION. LIQUID OR GEL USED SHALL BE NON-TOXIC, TACKY BIRD REPELLENT LIQUID - BIRD X BIRD PROOF (OR EQUIVALENT). PAYMENT WILL BE SUBSIDIARY TO THE STRUCTURE ITEMS.
7. NETTING APPLICATIONS FOR TRIANGULAR 'CORNER' STRUCTURES AND/OR 'CURTAINS' WILL BE IN ACCORDANCE WITH SPECIAL SPECIFICATION BIRD EXCLUSION METHOD.
8. ALL EXCLUSION MEASURES MUST BE ACCOMPANIED BY ACTIVE (DAILY) MONITORING BY A TRAINED OBSERVER (CONTRACTOR PROVIDED). NOTE: BIRDS AND OTHER WILDLIFE MAY BECOME ENTRAPPED WITH ANY EXCLUSION MEASURE.
9. THE CONTRACTOR SHALL REMOVE ALL STRUCTURAL EXCLUSIONARY MEASURES IMMEDIATELY AFTER PROJECT COMPLETION.

TYPICAL CORNER NETTING INSTALLATION



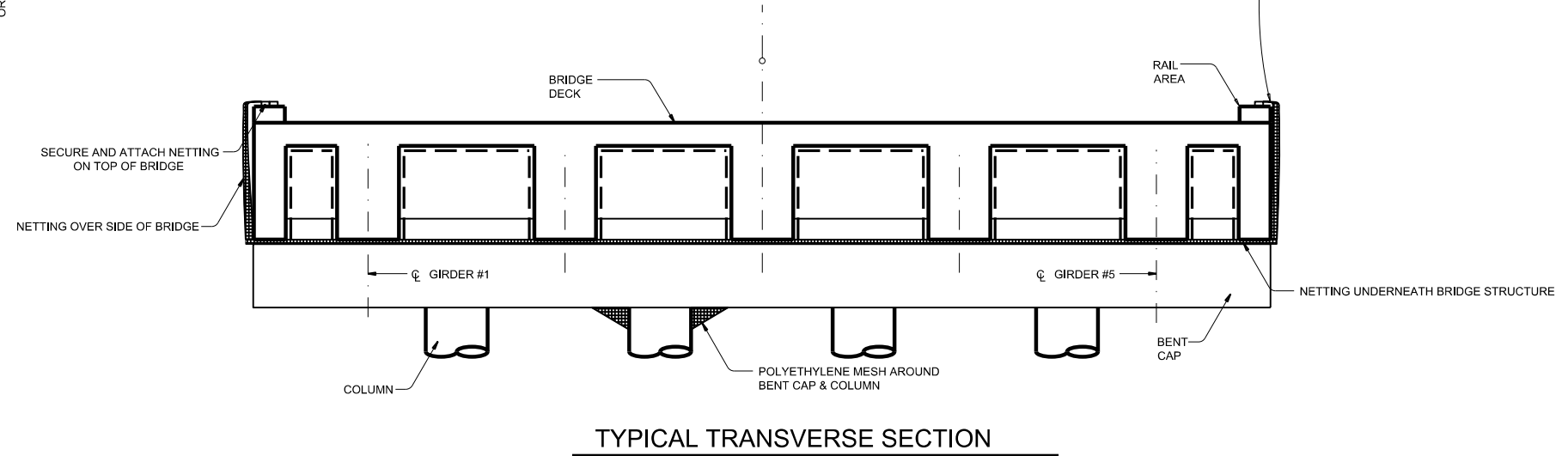
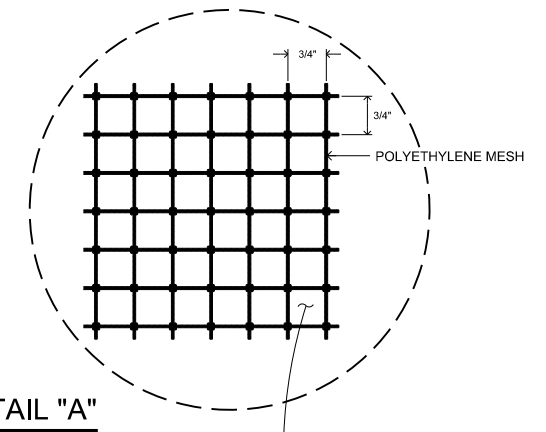
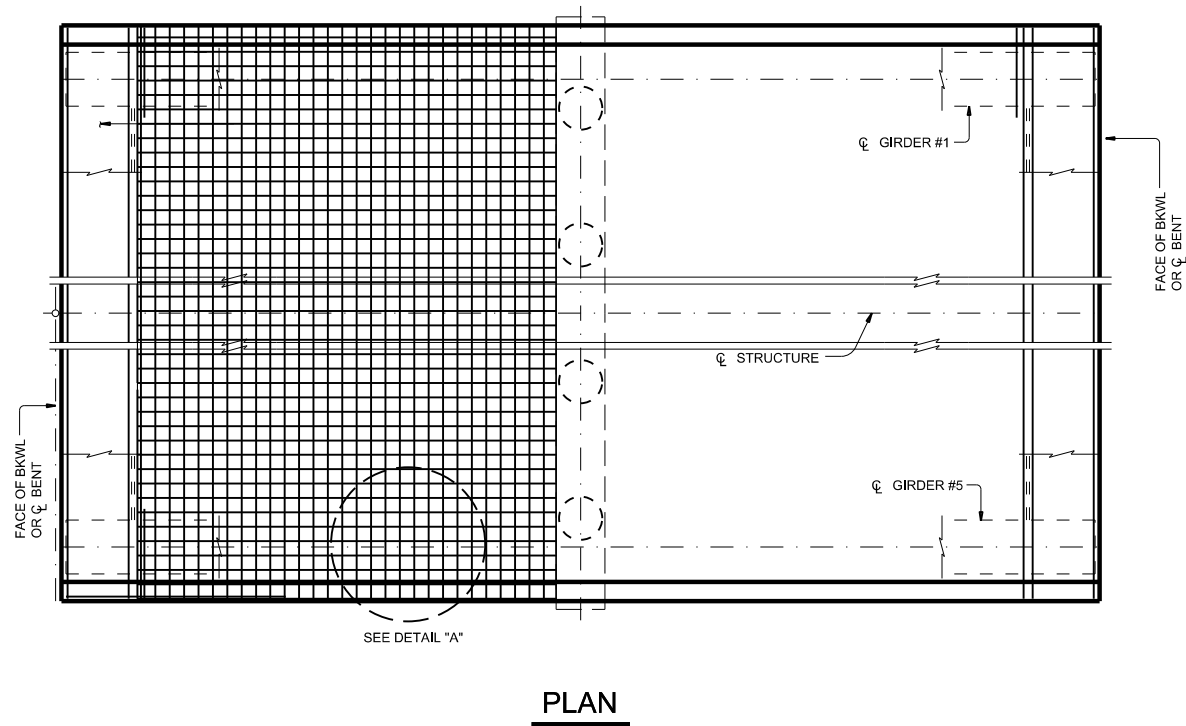
TYPICAL LONGITUDINAL SECTION AT CULVERT OPENING

DISCLAIMER:
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2023/01/24
 FILE: DOCUMENT NAME

Texas Department of Transportation				Pharr District Standard	
<h2 style="margin: 0;">BIRD EXCLUSION DETAILS</h2>					
FILE:	DN:	CK:	DW:	CK:	
©TxDOT	November 2017	CONT	SECT	JOB	HIGHWAY
REVISIONS		0482	01	036, etc.	SH 285
DIST	COUNTY			SHEET NO.	
PHR	JIM HOGG			378	

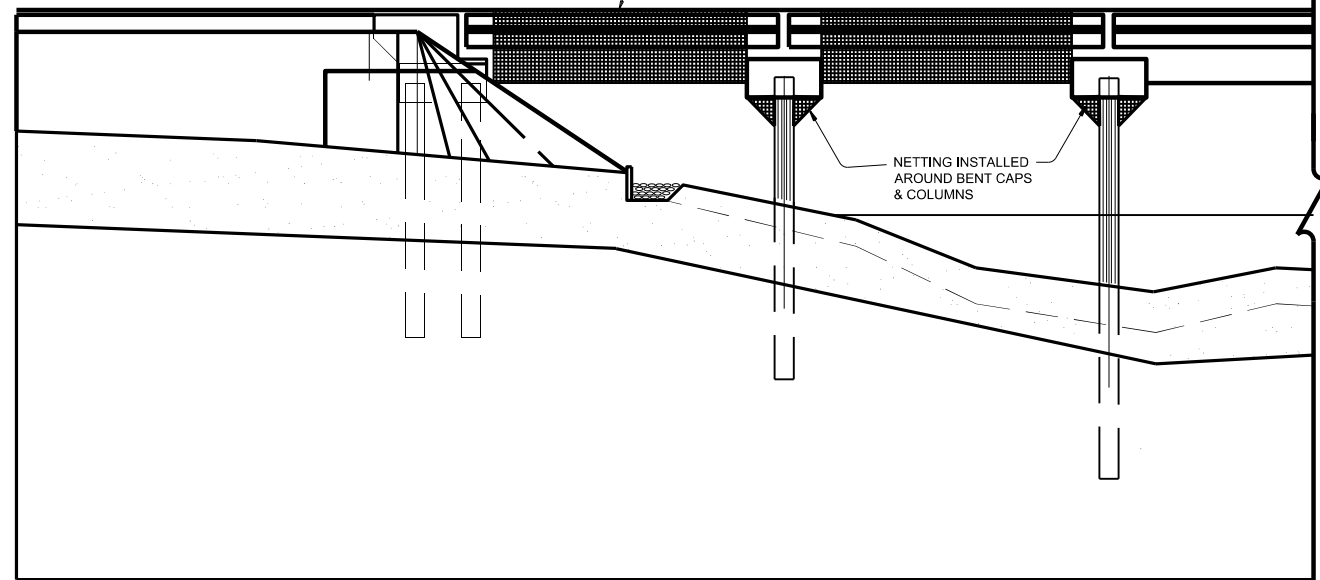
TYPICAL BIRD NETTING INSTALLATION DETAILS FOR A BRIDGE



UV TREATED BIRD NETTING SHALL BE SECURELY ATTACHED ONTO THE TOP OF BRIDGE DECK ALONG THE LENGTH OF THE RAIL AREA

BIRD NETTING SHALL COVER AND WRAP AROUND BOTH SIDES AND UNDERNEATH THE SUPPORTING BRIDGE STRUCTURE TO ELIMINATE BIRD ACCESS UNDERNEATH THE BRIDGE

BIRD NETTING SHOULD BE DRAWN TIGHT WITH NO LOOSE ENDS OR BUNCHING



GENERAL NOTES:

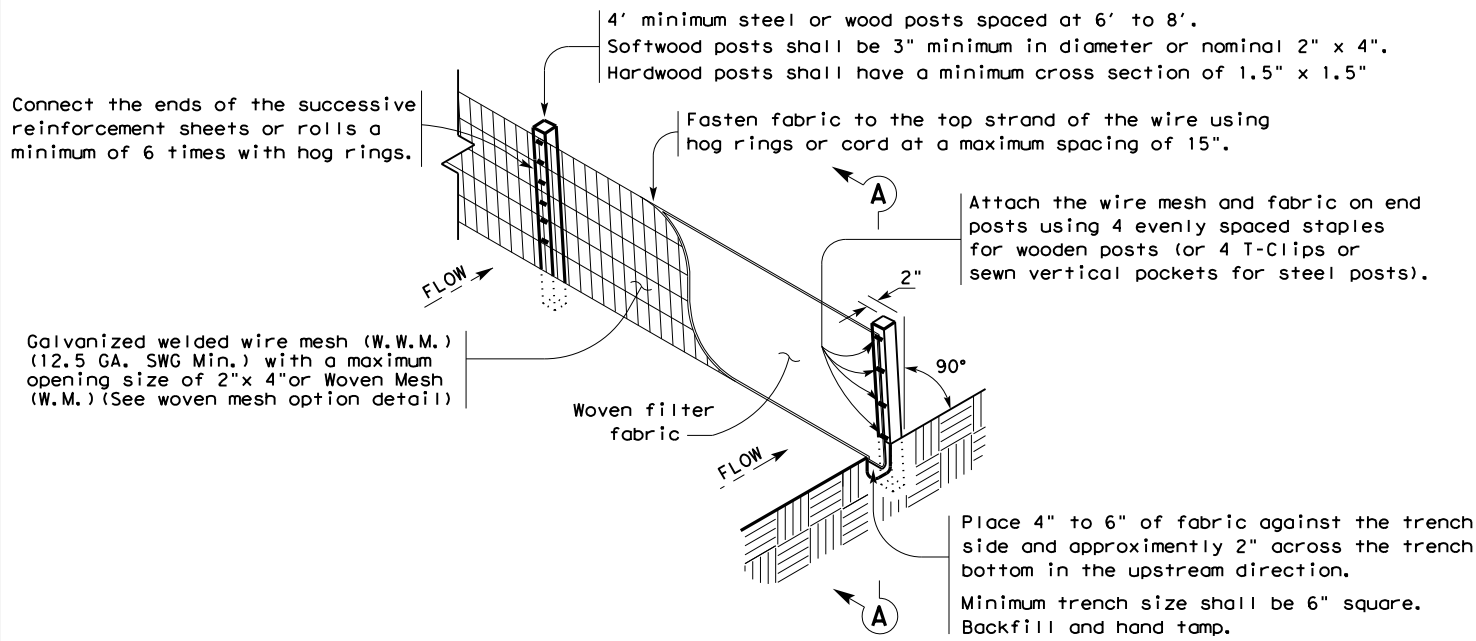
- CLIFF SWALLOWS AND OTHER MIGRATORY BIRDS ARE FEDERALLY PROTECTED UNDER THE FEDERAL MIGRATORY BIRD TREATY ACT. THIS STATUTE INCLUDES LIVE AND DEAD BIRDS, ANY PART, NEST, OR EGG. UNLESS PERMITTED BY REGULATIONS, IT PROHIBITS PURSUIT, HUNTING, TAKING, CAPTURING, KILLING, AND ATTEMPTING TO TAKE ANY MIGRATORY BIRD. NESTING ACTIVITY TYPICALLY OCCURS DURING THE MONTHS OF FEBRUARY THROUGH SEPTEMBER.
- ANY PROJECT WITH A BRIDGE(S) HAS POTENTIAL FOR NESTING BY SWALLOWS OR OTHER MIGRATORY BIRDS. ROADWAY CONSTRUCTION THAT DOES NOT INCLUDE SPECIFIC WORK ON BRIDGE(S) WITHIN THE PROJECT AREA MAY ALSO HAVE THE POTENTIAL TO RESULT IN A 'TAKE' OF MIGRATORY BIRDS.
- THE CONTRACTOR SHALL IMPLEMENT EXCLUSIONARY MEASURES TO PREVENT SWALLOWS FROM BUILDING NEW NESTS PRIOR TO OR DURING THE NESTING SEASON (FEBRUARY 15 - OCTOBER 1) ON ANY BRIDGE(S) WITH EVIDENCE OF PRIOR NESTING.
- 'NON-ACTIVE' NESTS SHOULD BE REMOVED PRIOR TO OR DURING THE NESTING SEASON IN ACCORDANCE WITH STANDARD ITEM 427 BLAST CLEANING OR MANUFACTURER DIRECTION. HOWEVER, NO NESTS SHOULD BE REMOVED PRIOR TO COORDINATION WITH THE DISTRICT ENVIRONMENTAL COORDINATOR.
- EXCLUSION MEASURES GENERALLY INCLUDE THE FOLLOWING: FOR BRIDGES, NETTING IS THE MOST EFFECTIVE OPTION, NOTING INSTALLATION WILL BE IN ACCORDANCE WITH SPECIAL SPECIFICATION BIRD EXCLUSION METHOD. SEE LAYOUTS FOR MORE DETAIL.
- ALL EXCLUSION MEASURES MUST BE ACCOMPANIED BY ACTIVE (DAILY) MONITORING BY A TRAINED OBSERVER (CONTRACTOR PROVIDED). NOTE: BIRDS AND OTHER WILDLIFE MAY BECOME ENTRAPPED WITH ANY EXCLUSION MEASURE.
- THE CONTRACTOR SHALL REMOVE ALL STRUCTURAL EXCLUSIONARY MEASURES AFTER PROJECT COMPLETION.

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2023/01/24
FILE: DOCUMENT NAME

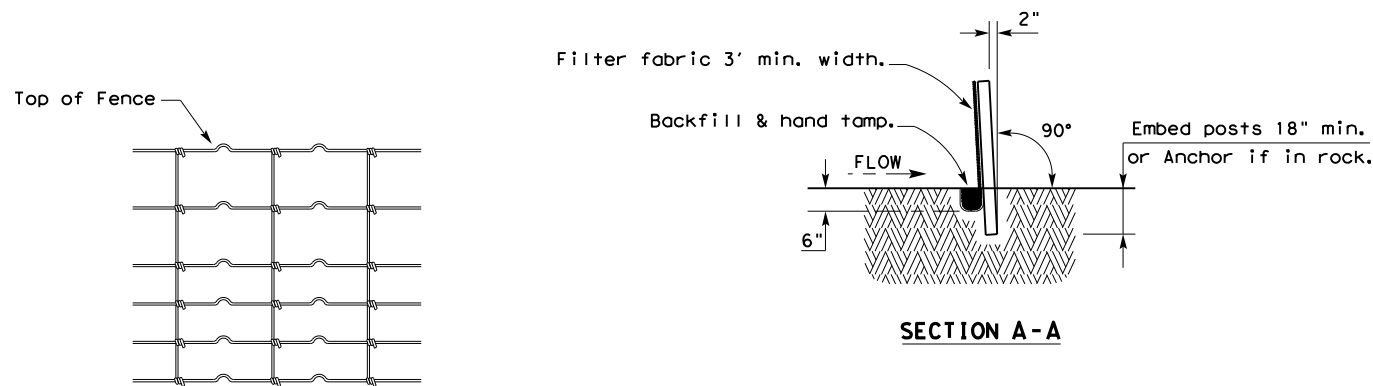
				Pharr District Standard	
<h3>BIRD EXCLUSION DETAILS</h3>					
FILE:	DN:	CK:	DW:	CK:	
©TxDOT	November 2017	CONT	SECT	JOB	HIGHWAY
REVISIONS		0482	01	036, etc.	SH 285
		DIST	COUNTY	SHEET NO.	
		PHR	JIM HOGG	379	

20A 2/2024
 D:\US\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14 - SH 285 Standards\Environmental Standards\ec116.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.



TEMPORARY SEDIMENT CONTROL FENCE

SCF



HINGE JOINT KNOT WOVEN MESH (OPTION) DETAIL

Galvanized hinge joint knot woven mesh (12.5 GA. SWG Min.) requires a minimum of five horizontal wires spaced at a maximum of 12 inches apart and all vertical wires spaced at a maximum of 12 inches apart.

SEDIMENT CONTROL FENCE USAGE GUIDELINES

A sediment control fence may be constructed near the downstream perimeter of a disturbed area along a contour to intercept sediment from overland runoff. A 2 year storm frequency may be used to calculate the flow rate to be filtered.

Sediment control fence should be sized to filter a maximum flow through rate of 100 GPM/FT². Sediment control fence is not recommended to control erosion from a drainage area larger than 2 acres.

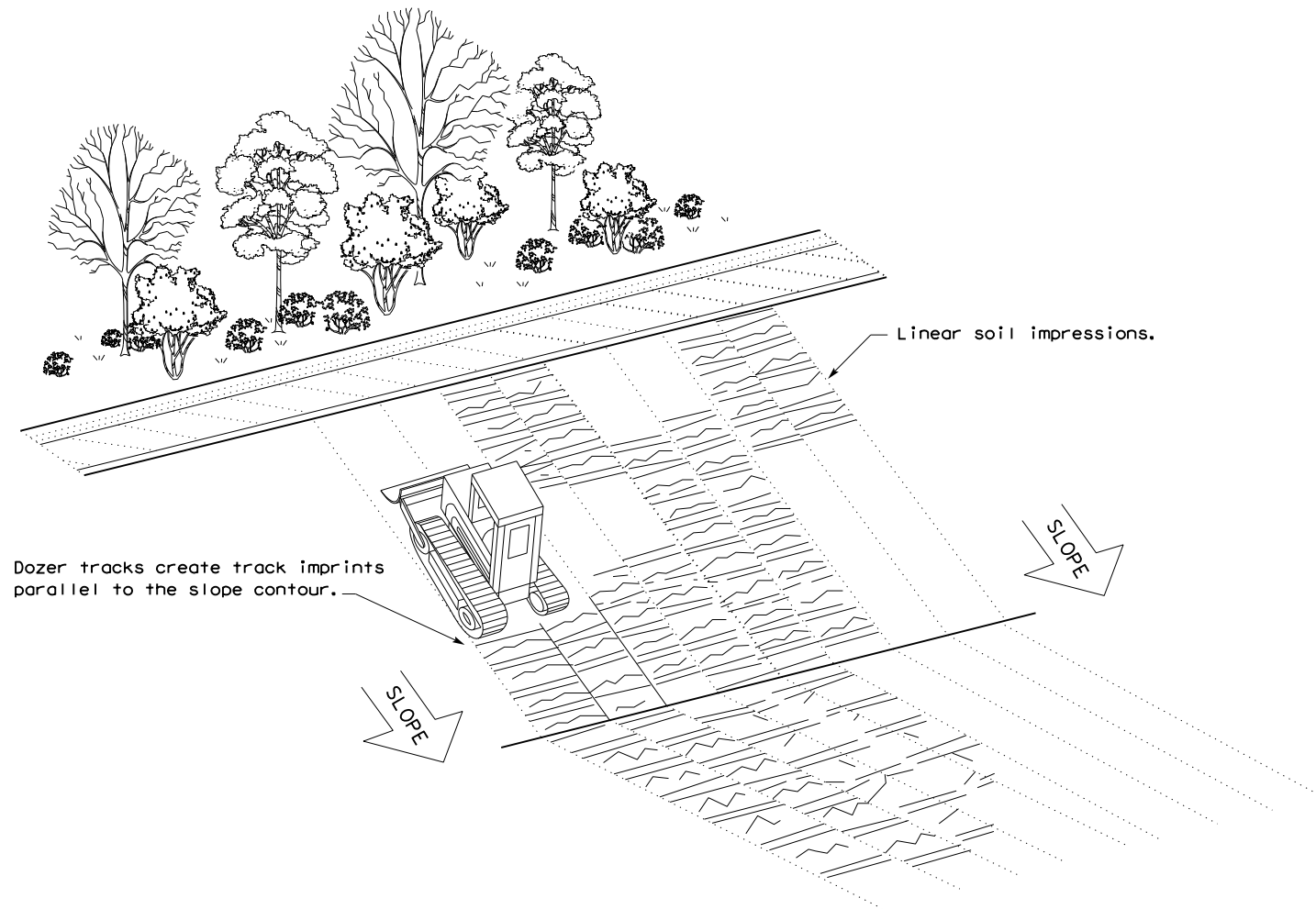
LEGEND

Sediment Control Fence

SCF

GENERAL NOTES

1. Vertical tracking is required on projects where soil distributing activities have occurred unless otherwise approved.
2. Perform vertical tracking on slopes to temporarily stabilize soil.
3. Provide equipment with a track undercarriage capable of producing linear soil impressions measuring a minimum of 12" in length by 2" to 4" in width by 1/2" to 2" in depth.
4. Do not exceed 12" between track impressions.
5. Install continuous linear track impressions where the minimum 12" length impressions are perpendicular to the slope or direction of water flow.

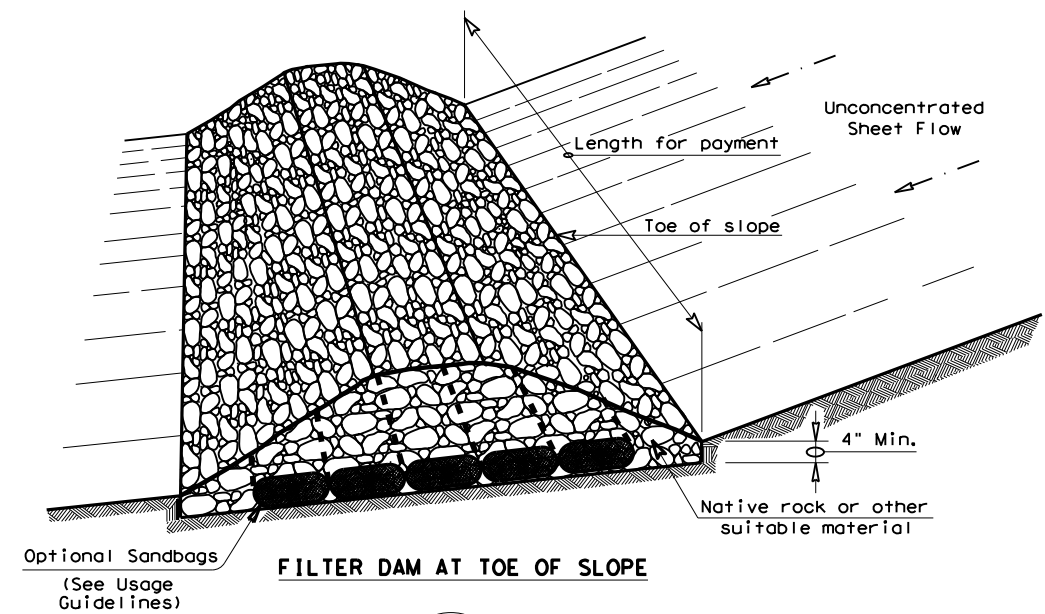


VERTICAL TRACKING

				Design Division Standard	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES FENCE & VERTICAL TRACKING EC(1) - 16					
FILE: ec116	DN: TxDOT	CK: KM	DW: VP	DN/CK: LS	
© TxDOT: JULY 2016	CONT	SECT	JOB	HIGHWAY	
REVISIONS	0482	01	036, etc.	SH 285	
	DIST	COUNTY	SHEET NO.		
	PHR	JIM HOGG	380		

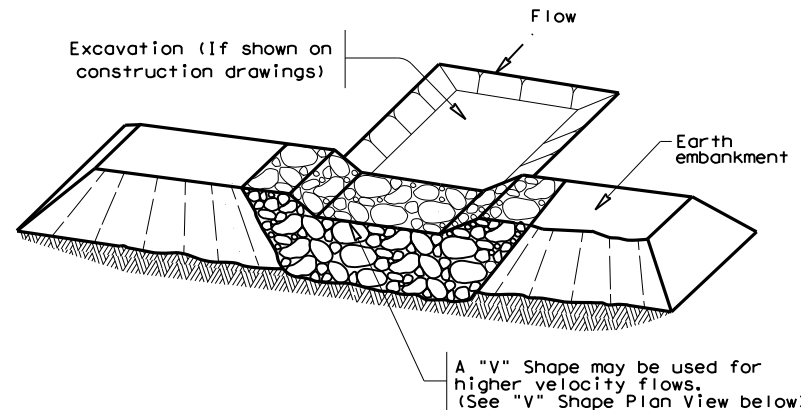
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024
 FILE: pw:\t\tdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\Environmental Standards\ec216.dgn



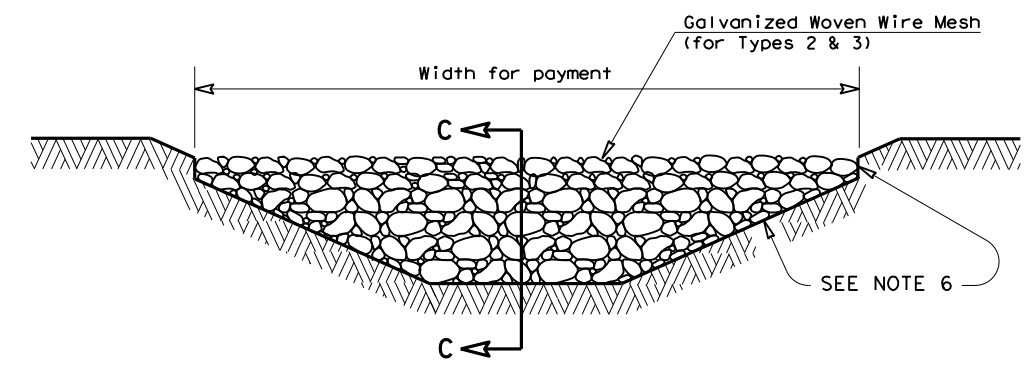
FILTER DAM AT TOE OF SLOPE

(RFD1)



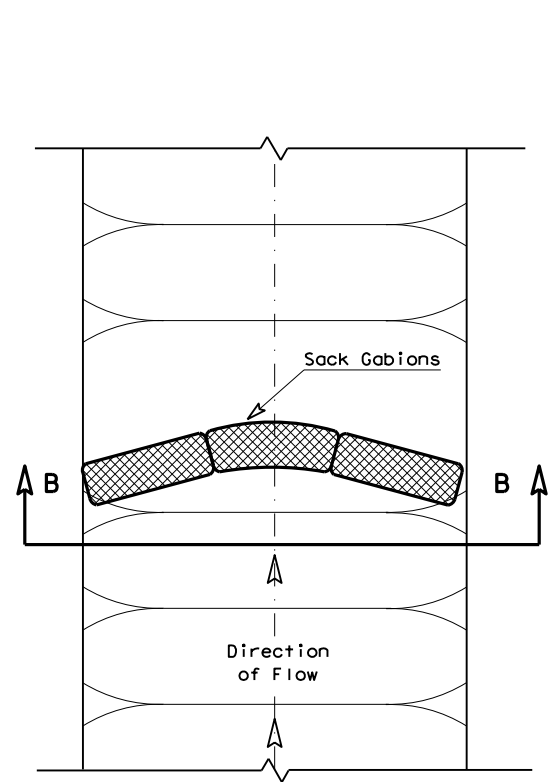
FILTER DAM AT SEDIMENT TRAP

(RFD1) OR (RFD2)

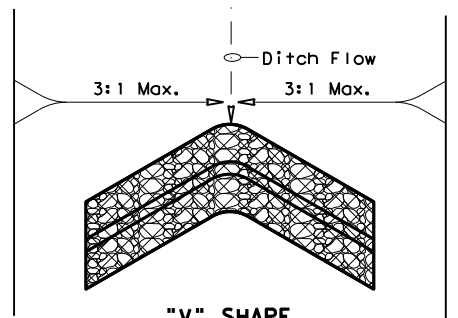


FILTER DAM AT CHANNEL SECTIONS

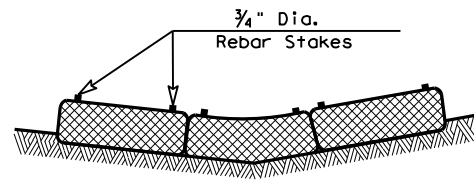
(RFD1) OR (RFD2) OR (RFD3)



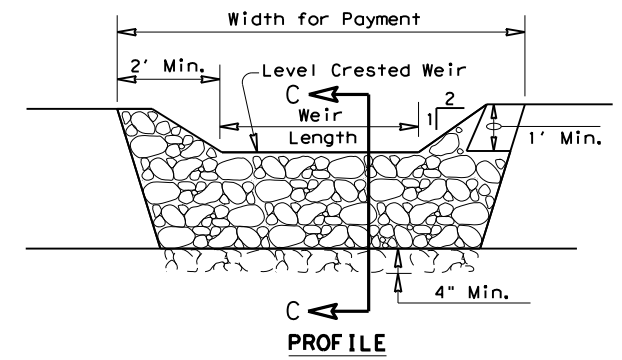
PLAN VIEW



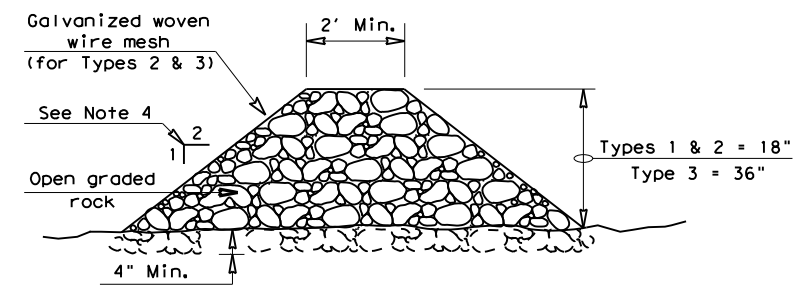
"V" SHAPE PLAN VIEW



SECTION B-B



PROFILE



SECTION C-C

ROCK FILTER DAM USAGE GUIDELINES

Rock Filter Dams should be constructed downstream from disturbed areas to intercept sediment from overland runoff and/or concentrated flow. The dams should be sized to filter a maximum flow through rate of 60 GPM/FT² of cross sectional area. A 2 year storm frequency may be used to calculate the flow rate.

Type 1 (18" high with no wire mesh) (3" to 6" aggregate): Type 1 may be used at the toe of slopes, around inlets, in small ditches, and at dike or swale outlets. This type of dam is recommended to control erosion from a drainage area of 5 acres or less. Type 1 may not be used in concentrated high velocity flows (approximately 8 Ft/Sec or more) in which aggregate wash out may occur. Sandbags may be used at the embedded foundation (4" deep min.) for better filtering efficiency of low flows if called for on the plans or directed by the Engineer.

Type 2 (18" high with wire mesh) (3" to 6" aggregate): Type 2 may be used in ditches and at dike or swale outlets.

Type 3 (36" high with wire mesh) (4" to 8" aggregate): Type 3 may be used in stream flow and should be secured to the stream bed.

Type 4 (Sack gabions) (3" to 6" aggregate): Type 4 May be used in ditches and smaller channels to form an erosion control dam.

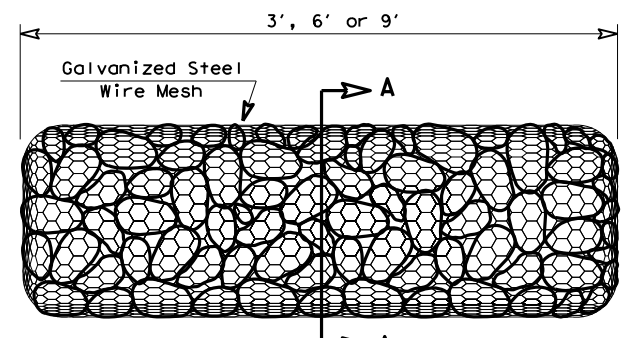
Type 5: Provide rock filter dams as shown on plans.

GENERAL NOTES

1. If shown on the plans or directed by the Engineer, filter dams should be placed near the toe of slopes where erosion is anticipated, upstream and/or downstream at drainage structures, and in roadway ditches and channels to collect sediment.
2. Materials (aggregate, wire mesh, sandbags, etc.) shall be as indicated by the specification for "Rock Filter Dams for Erosion and Sedimentation Control".
3. The rock filter dam dimensions shall be as indicated on the SW3P plans.
4. Side slopes should be 2:1 or flatter. Dams within the safety zone shall have sideslopes of 6:1 or flatter.
5. Maintain a minimum of 1' between top of rock filter dam weir and top of embankment for filter dams at sediment traps.
6. Filter dams should be embedded a minimum of 4" into existing ground.
7. The sediment trap for ponding of sediment laden runoff shall be of the dimensions shown on the plans.
8. Rock filter dam types 2 & 3 shall be secured with 20 gauge galvanized woven wire mesh with 1" diameter hexagonal openings. The aggregate shall be placed on the mesh to the height & slopes specified. The mesh shall be folded at the upstream side over the aggregate and tightly secured to itself on the downstream side using wire ties or hog rings. For in stream use, the mesh should be secured or staked to the stream bed prior to aggregate placement.
9. Sack Gabions should be staked down with 3/4" dia. rebar stakes, and have a double-twisted hexagonal weave with a nominal mesh opening of 2 1/2" x 3 1/4".
10. Flow outlet should be onto a stabilized area (vegetation, rock, etc.).
11. The guidelines shown hereon are suggestions only and may be modified by the Engineer.

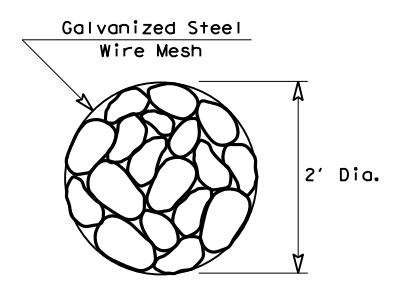
PLAN SHEET LEGEND

- Type 1 Rock Filter Dam (RFD1)
- Type 2 Rock Filter Dam (RFD2)
- Type 3 Rock Filter Dam (RFD3)
- Type 4 Rock Filter Dam (RFD4)



TYPE 4 (SACK GABIONS)

(RFD4)

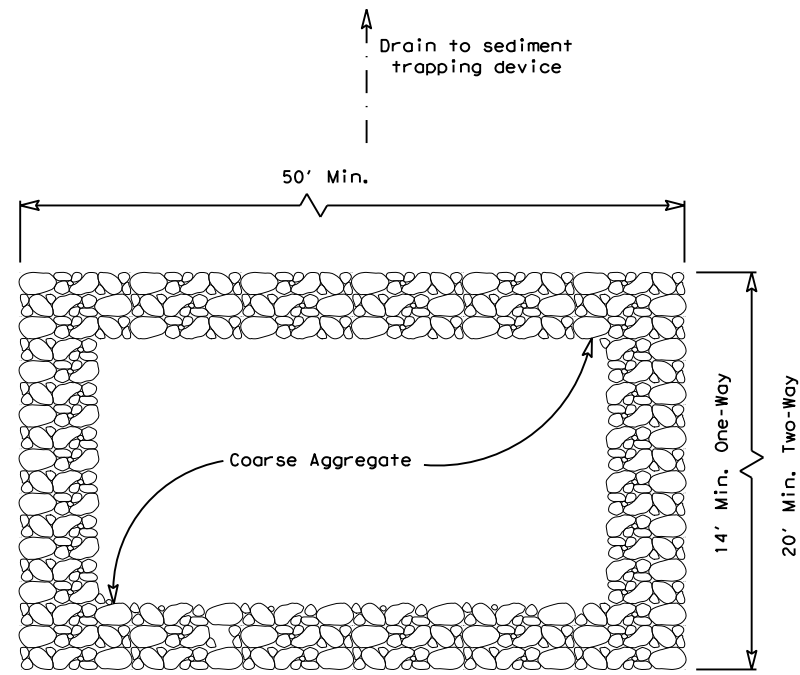


SECTION A-A

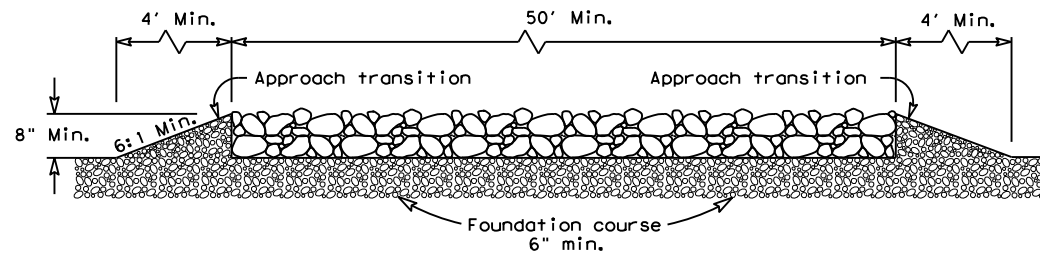
		Design Division Standard	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES			
ROCK FILTER DAMS			
EC(2) - 16			
FILE: ec216	DN: TxDOT	CK: KM	DW: VP
© TxDOT: JULY 2016	CONT	SECT	JOB
REVISIONS	0482	01	036, etc.
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	381	

DISCLAIMER:
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024
 FILE: \\txdot.projectwiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\Environmental_Standards\ec316.dgn



PLAN VIEW

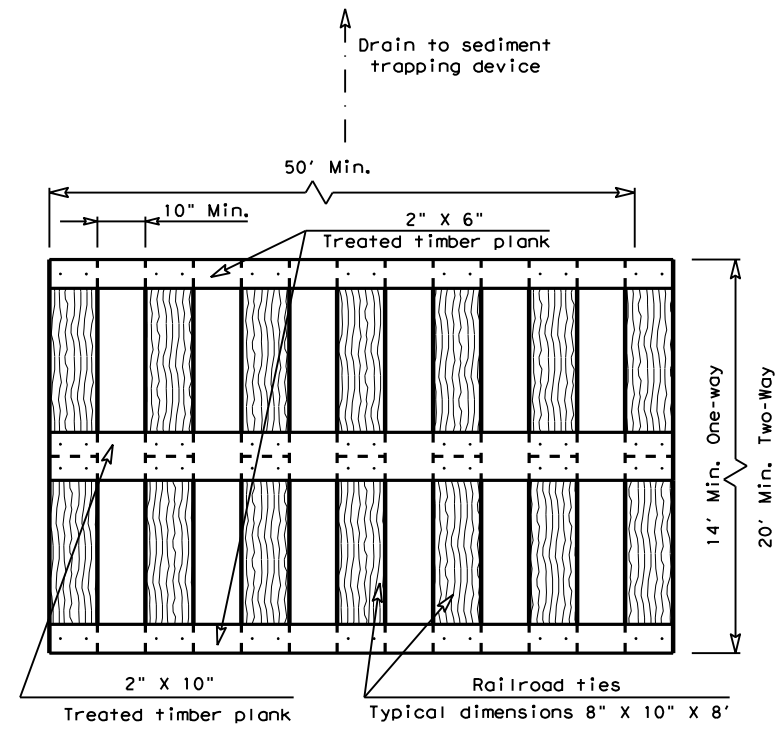


ELEVATION VIEW

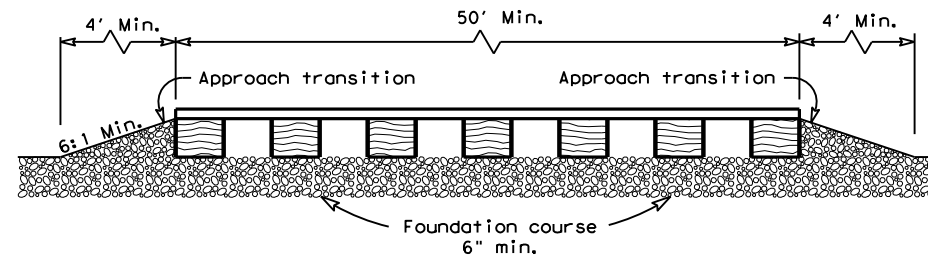
CONSTRUCTION EXIT (TYPE 1)
 ROCK CONSTRUCTION (LONG TERM)

GENERAL NOTES (TYPE 1)

- The length of the type 1 construction exit shall be as indicated on the plans, but not less than 50'.
- The coarse aggregate should be open graded with a size of 4" to 8".
- The approach transitions should be no steeper than 6:1 and constructed as directed by the Engineer.
- The construction exit foundation course shall be flexible base, bituminous concrete, portland cement concrete or other materials approved by the Engineer.
- The construction exit shall be graded to allow drainage to a sediment trapping device.
- The guidelines shown hereon are suggestions only and may be modified by the Engineer.
- Construct exits with a width of at least 14 ft. for one-way and 20 ft. for two-way traffic for the full width of the exit, or as directed by the engineer.



PLAN VIEW

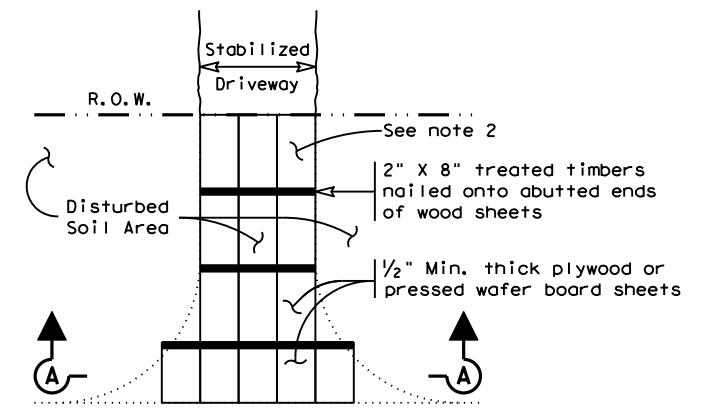


ELEVATION VIEW

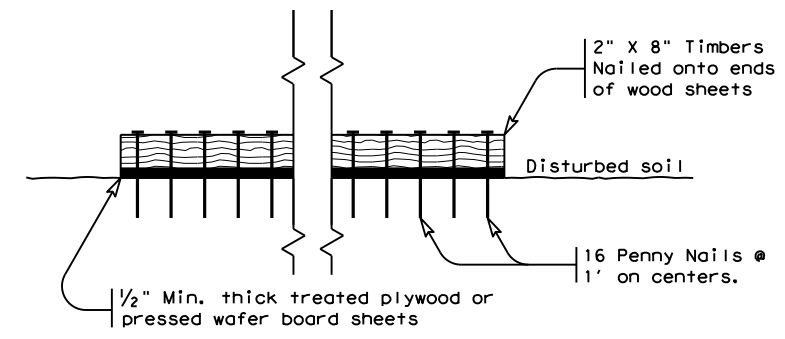
CONSTRUCTION EXIT (TYPE 2)
 TIMBER CONSTRUCTION (LONG TERM)

GENERAL NOTES (TYPE 2)

- The length of the type 2 construction exit shall be as indicated on the plans, but not less than 50'.
- The treated timber planks shall be attached to the railroad ties with 1/2" x 6" min. lag bolts. Other fasteners may be used as approved by the Engineer.
- The treated timber planks shall be #2 grade min., and should be free from large and loose knots.
- The approach transitions shall be no steeper than 6:1 and constructed as directed by the Engineer.
- The construction exit foundation course shall be flexible base, bituminous concrete, portland cement concrete or other material as approved by the Engineer.
- The construction exit should be graded to allow drainage to a sediment trapping device.
- The guidelines shown hereon are suggestions only and may be modified by the Engineer.
- Construct exits with a width of at least 14 ft. for one-way and 20 ft. for two-way traffic for the full width of the exit, or as directed by the engineer.



PLAN VIEW



SECTION A-A
 CONSTRUCTION EXIT (TYPE 3)
 SHORT TERM

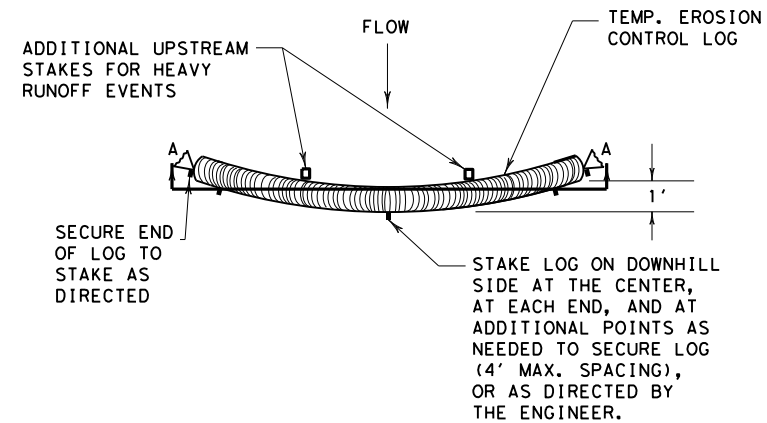
GENERAL NOTES (TYPE 3)

- The length of the type 3 construction exit shall be as shown on the plans, or as directed by the Engineer.
- The type 3 construction exit may be constructed from open graded crushed stone with a size of two to four inches spread a min. of 4" thick to the limits shown on the plans.
- The treated timber planks shall be #2 grade min., and should be free from large and loose knots.
- The guidelines shown hereon are suggestions only and may be modified by the Engineer.

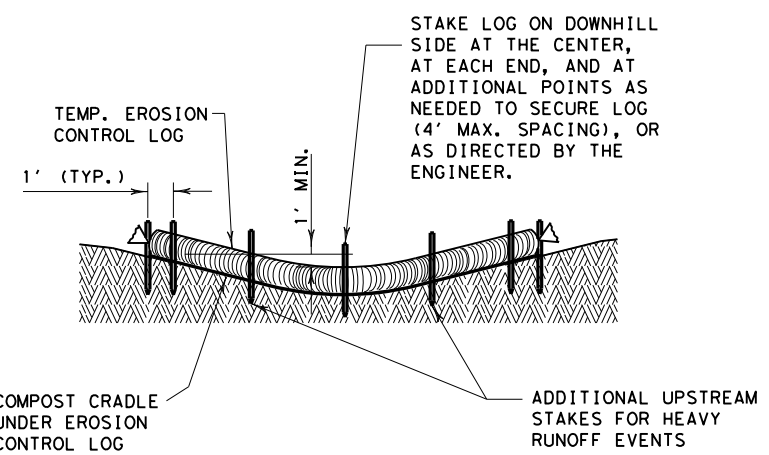
		Design Division Standard	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES CONSTRUCTION EXITS EC(3)-16			
FILE: ec316	DN: TxDOT	CK: KM	DW: VP
© TxDOT: JULY 2016	CONT SECT	JOB	HIGHWAY
REVISIONS	0482 01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	382	

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE: 2/12/2024
 FILE: p:\t\tdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\Environmental Standards\ec916.dgn

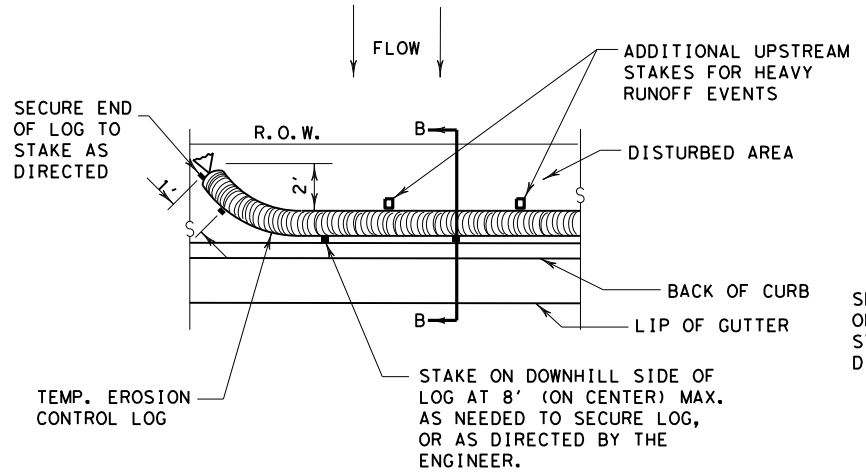


PLAN VIEW

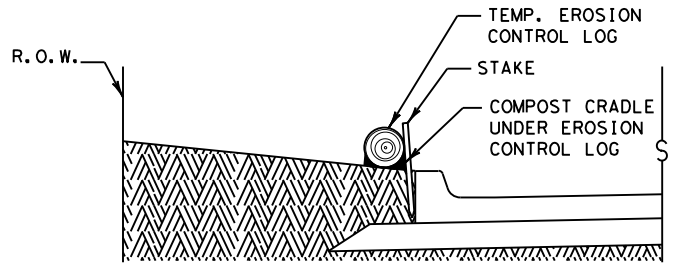


SECTION A-A
EROSION CONTROL LOG DAM

CL-D

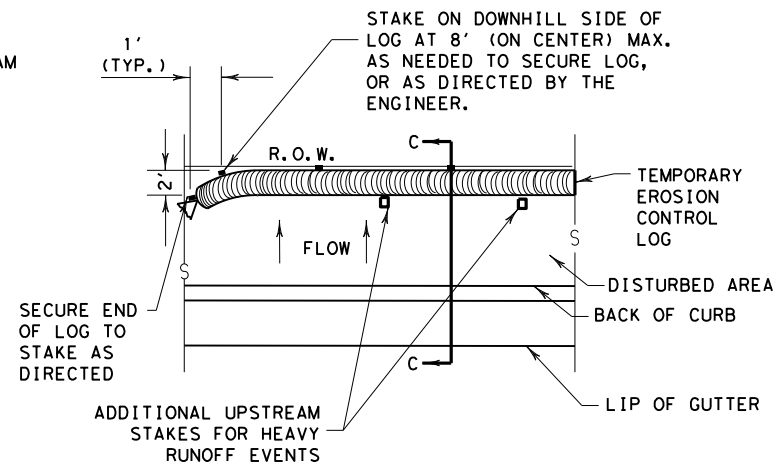


PLAN VIEW

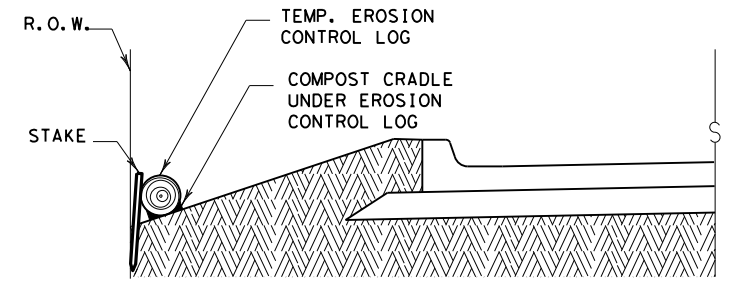


SECTION B-B
EROSION CONTROL LOG AT BACK OF CURB

CL-BOC



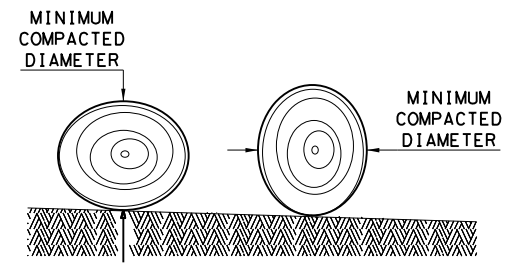
PLAN VIEW



SECTION C-C

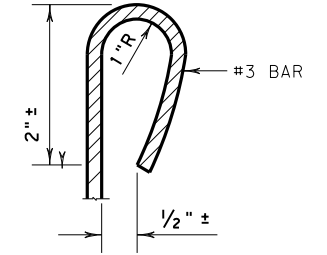
EROSION CONTROL LOG AT EDGE OF RIGHT-OF-WAY

CL-ROW



DIAMETER MEASUREMENTS OF EROSION CONTROL LOGS SPECIFIED IN PLANS

- LEGEND**
- CL-D EROSION CONTROL LOG DAM
 - CL-BOC EROSION CONTROL LOG AT BACK OF CURB
 - CL-ROW EROSION CONTROL LOG AT EDGE OF RIGHT-OF-WAY
 - CL-SST EROSION CONTROL LOGS ON SLOPES STAKE AND TRENCHING ANCHORING
 - CL-SSL EROSION CONTROL LOGS ON SLOPES STAKE AND LASHING ANCHORING
 - CL-DI EROSION CONTROL LOG AT DROP INLET
 - CL-CI EROSION CONTROL LOG AT CURB INLET
 - CL-GI EROSION CONTROL LOG AT CURB & GRATE INLET



REBAR STAKE DETAIL

SEDIMENT BASIN & TRAP USAGE GUIDELINES

An erosion control log sediment trap may be used to filter sediment out of runoff draining from an unstabilized area.

Log Traps: The drainage area for a sediment trap should not exceed 5 acres. The trap capacity should be 1800 CF/Acre (0.5" over the drainage area).

Control logs should be placed in the following locations:

1. Within drainage ditches spaced as needed or min. 500' on center
2. Immediately preceding ditch inlets or drain inlets
3. Just before the drainage enters a water course
4. Just before the drainage leaves the right of way
5. Just before the drainage leaves the construction limits where drainage flows away from the project.

The logs should be cleaned when the sediment has accumulated to a depth of 1/2 the log diameter.

Cleaning and removal of accumulated sediment deposits is incidental and will not be paid for separately.

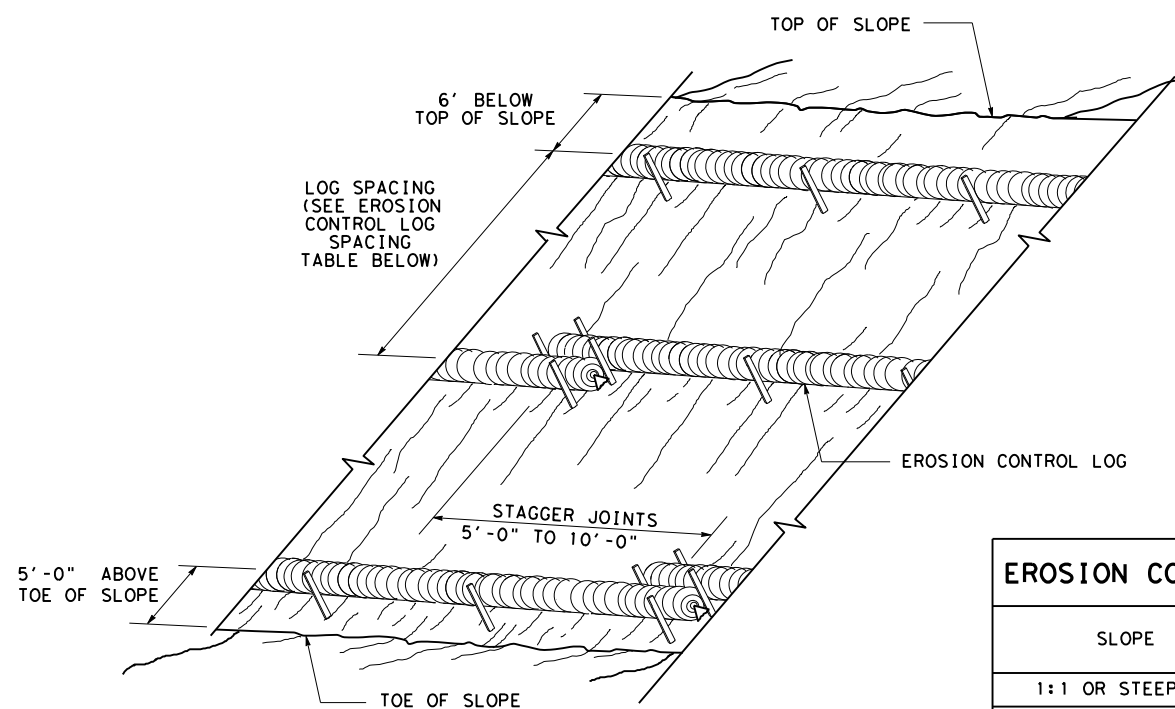
GENERAL NOTES:

1. EROSION CONTROL LOGS SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS, OR AS DIRECTED BY THE ENGINEER.
2. LENGTHS OF EROSION CONTROL LOGS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND AS REQUIRED FOR THE PURPOSE INTENDED.
3. UNLESS OTHERWISE DIRECTED, USE BIODEGRADABLE OR PHOTODEGRADABLE CONTAINMENT MESH ONLY WHERE LOG WILL REMAIN IN PLACE AS PART OF A VEGETATIVE SYSTEM. FOR TEMPORARY INSTALLATIONS, USE RECYCLABLE CONTAINMENT MESH.
4. FILL LOGS WITH SUFFICIENT FILTER MATERIAL TO ACHIEVE THE MINIMUM COMPACTED DIAMETER SPECIFIED IN THE PLANS WITHOUT EXCESSIVE DEFORMATION.
5. STAKES SHALL BE 2" X 2" WOOD OR #3 REBAR, 2'-4' LONG, EMBEDDED SUCH THAT 2" PROTRUDES ABOVE LOG, OR AS DIRECTED BY THE ENGINEER.
6. DO NOT PLACE STAKES THROUGH CONTAINMENT MESH.
7. COMPOST CRADLE MATERIAL IS INCIDENTAL & WILL NOT BE PAID FOR SEPARATELY.
8. SANDBAGS USED AS ANCHORS SHALL BE PLACED ON TOP OF LOGS & SHALL BE OF SUFFICIENT SIZE TO HOLD LOGS IN PLACE.
9. TURN THE ENDS OF EACH ROW OF LOGS UPSLOPE TO PREVENT RUNOFF FROM FLOWING AROUND THE LOG.
10. FOR HEAVY RUNOFF EVENTS, ADDITIONAL UPSTREAM STAKES MAY BE NECESSARY TO KEEP LOG FROM FOLDING IN ON ITSELF.

SHEET 1 OF 3

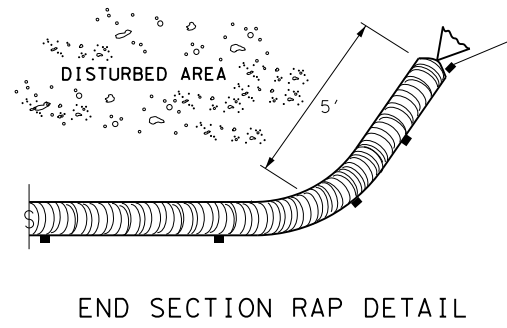
		<i>Design Division Standard</i>	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec916	DN: TxDOT	CK: KM	DW: LS/PT
© TxDOT: JULY 2016	CONT SECT	JOB	HIGHWAY
REVISIONS	0482 01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.
	PHR	JIM HOGG	383

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.
 DATE: 2/12/2024
 FILE: \\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\Environmental Standards\ec916.dgn



**EROSION CONTROL LOGS ON SLOPES
STAKE AND TRENCHING ANCHORING**

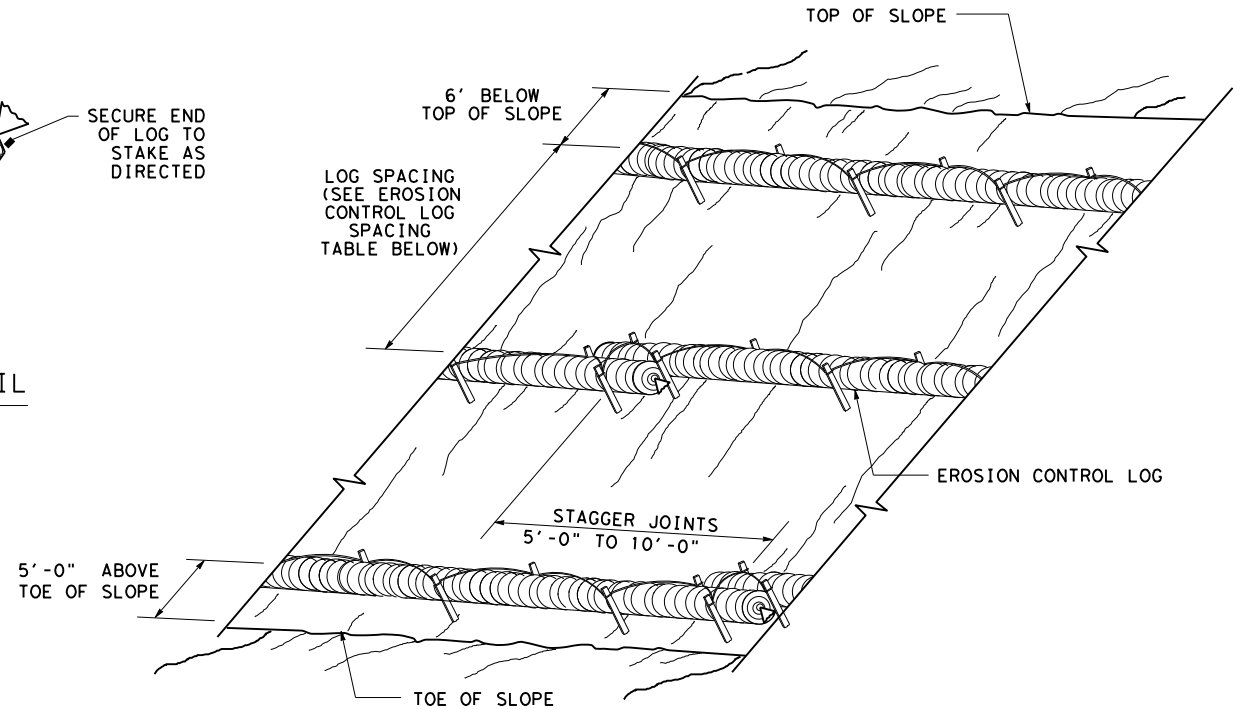
CL-SST



END SECTION RAP DETAIL

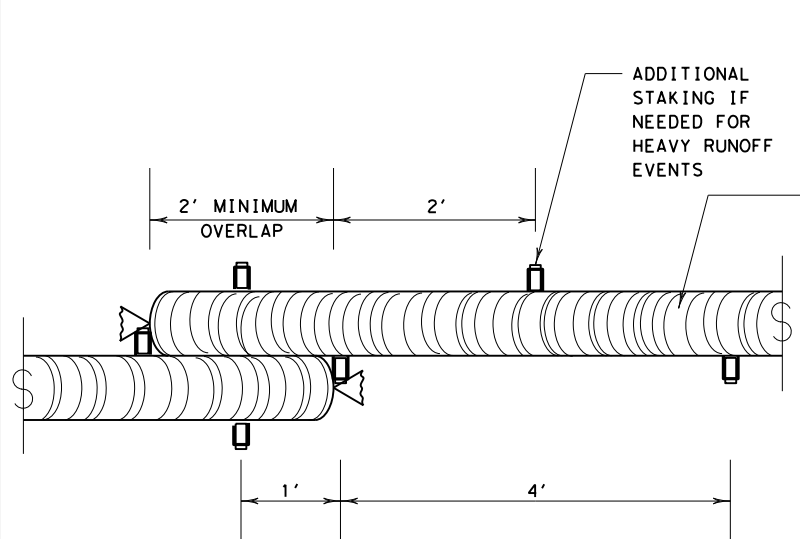
EROSION CONTROL LOG SPACING TABLE				
SLOPE	LOG DIAMETER			
	6"	8"	12"	18"
1:1 OR STEEPER	5'	10'	15'	20'
2:1	10'	20'	30'	40'
3:1	15'	30'	45'	60'
4:1 OR FLATTER	20'	40'	60'	80'

* ADJUSTMENTS CAN BE MADE FOR SOIL TYPE:
 SOFT, LOAMY SOILS-ADJUST ROWS CLOSER TOGETHER;
 HARD, ROCKY SOILS- ADJUST ROWS FARTHER APART



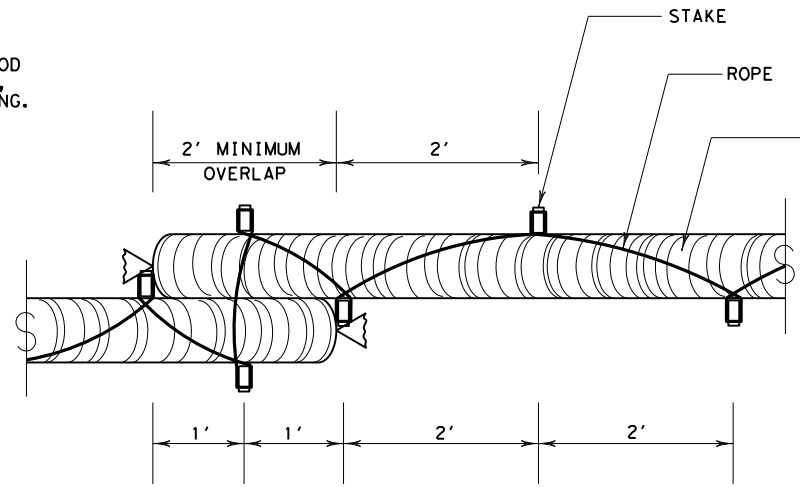
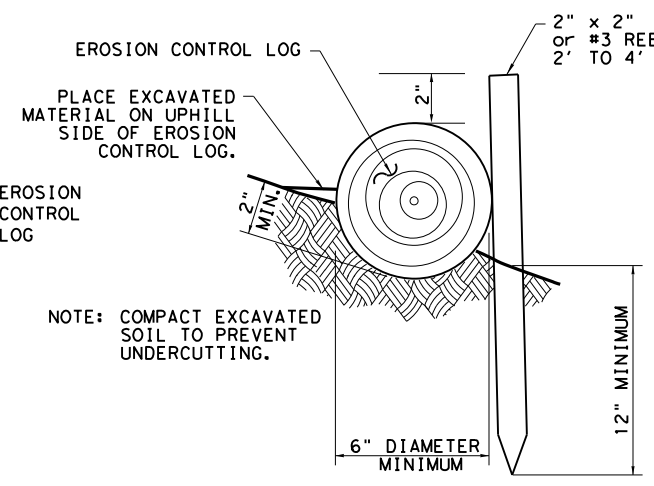
**EROSION CONTROL LOGS ON SLOPES
STAKE AND LASHING ANCHORING**

CL-SSL



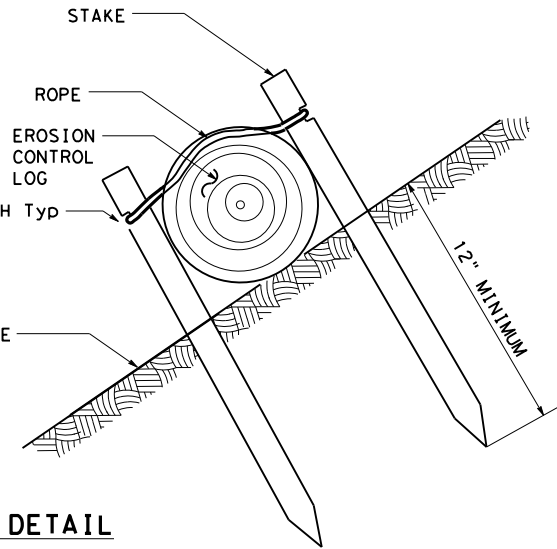
STAKE AND TRENCHING ANCHORING DETAIL

CL-SST

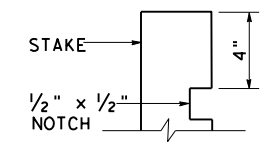


STAKE AND LASHING ANCHORING DETAIL

CL-SSL



TRENCH DEPTH TABLE	
LOG DIAMETER	DEPTH
6"	2"
8"	3"
12"	4"
18"	5"

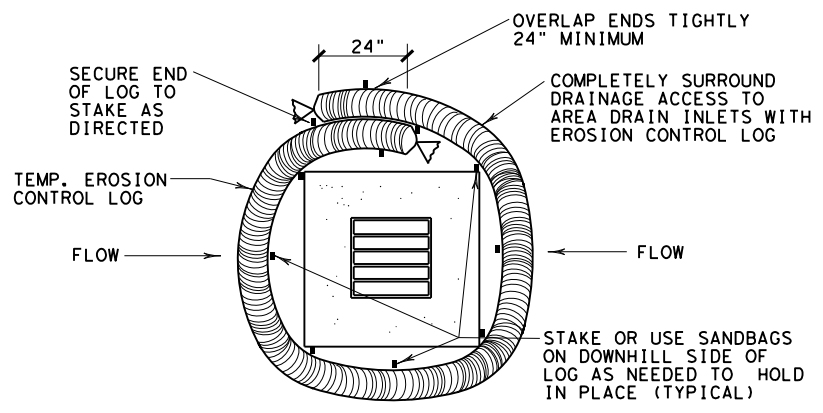


STAKE NOTCH DETAIL

SHEET 2 OF 3

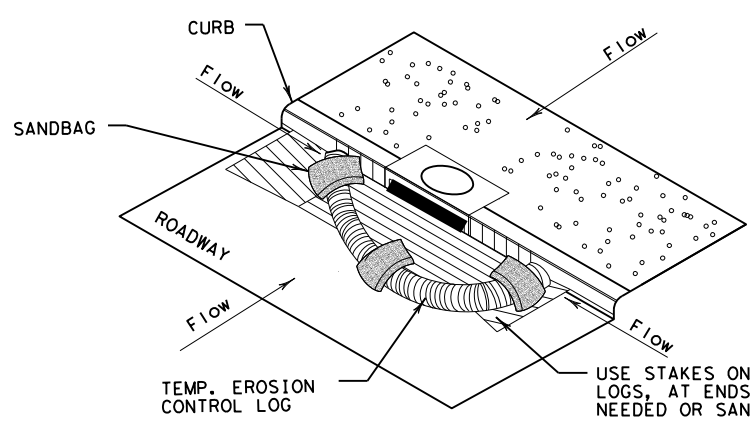
		Design Division Standard	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec116	DN: TxDOT	CK: KM	DW: LS/PT
© TxDOT: JULY 2016	CONT SECT	JOB	HIGHWAY
REVISIONS	0482 01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	384	

DATE: 2/12/2024
 FILE: p:\txdot\projectwise\online.com\TXDOT\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\14. SH 285 Standards\Environmental Standards\ec916.dgn
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.



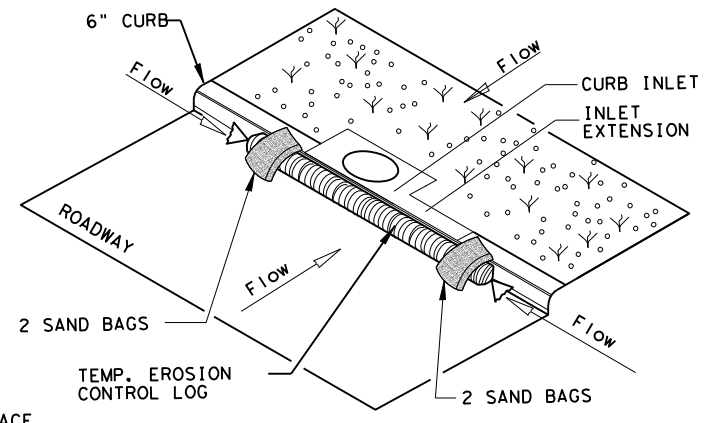
EROSION CONTROL LOG AT DROP INLET

CL-DI



EROSION CONTROL LOG AT CURB INLET

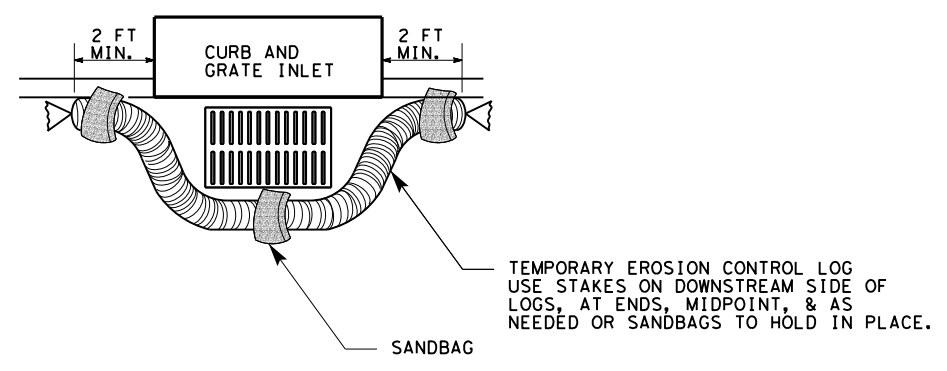
CL-CI



EROSION CONTROL LOG AT CURB INLET

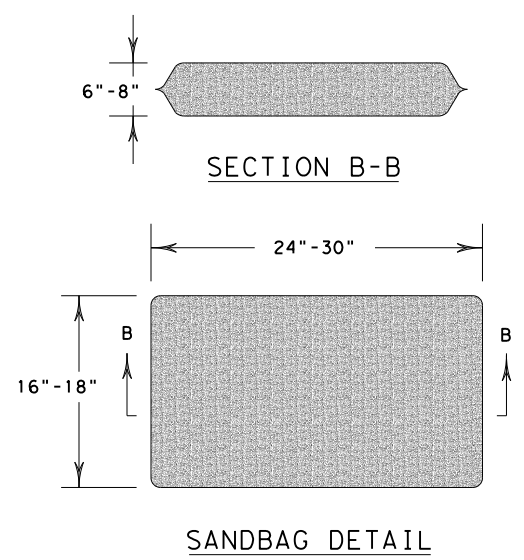
CL-CI

NOTE:
 EROSION CONTROL LOGS USED AT CURB INLETS SHOULD ONLY BE USED IF THEY WILL NOT IMPEDE TRAFFIC OR FLOOD THE ROADWAY OR WHEN THE STORM SEWER SYSTEM IS NOT FULLY FUNCTIONAL.



EROSION CONTROL LOG AT CURB & GRADE INLET

CL-GI



		<i>Design Division Standard</i>	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec916	DN: TxDOT	CK: KM	DW: LS/PT
© TxDOT: JULY 2016	CONT	SECT	JOB
REVISIONS	0482	01	036, etc.
	DIST	COUNTY	SHEET NO.
	PHR	JIM HOGG	385

DATE: 2/12/2024 8:38:42 AM
FILE: p:\txdot\projectwiseonline.com\TXDOT\5\Documents\21 - PHR\Design Projects\0482010364 - Design\Plan Set\10_Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 MISCELLANEOUS COVER.dgn

MISCELLANEOUS COVER SHEET

Pharr District Central Design



**SH 285
MISCELLANEOUS
COVER SHEET**

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036,etc.	SH 285
	DIST	COUNTY		SHEET NO.
	PHR	JIM HOGG		386

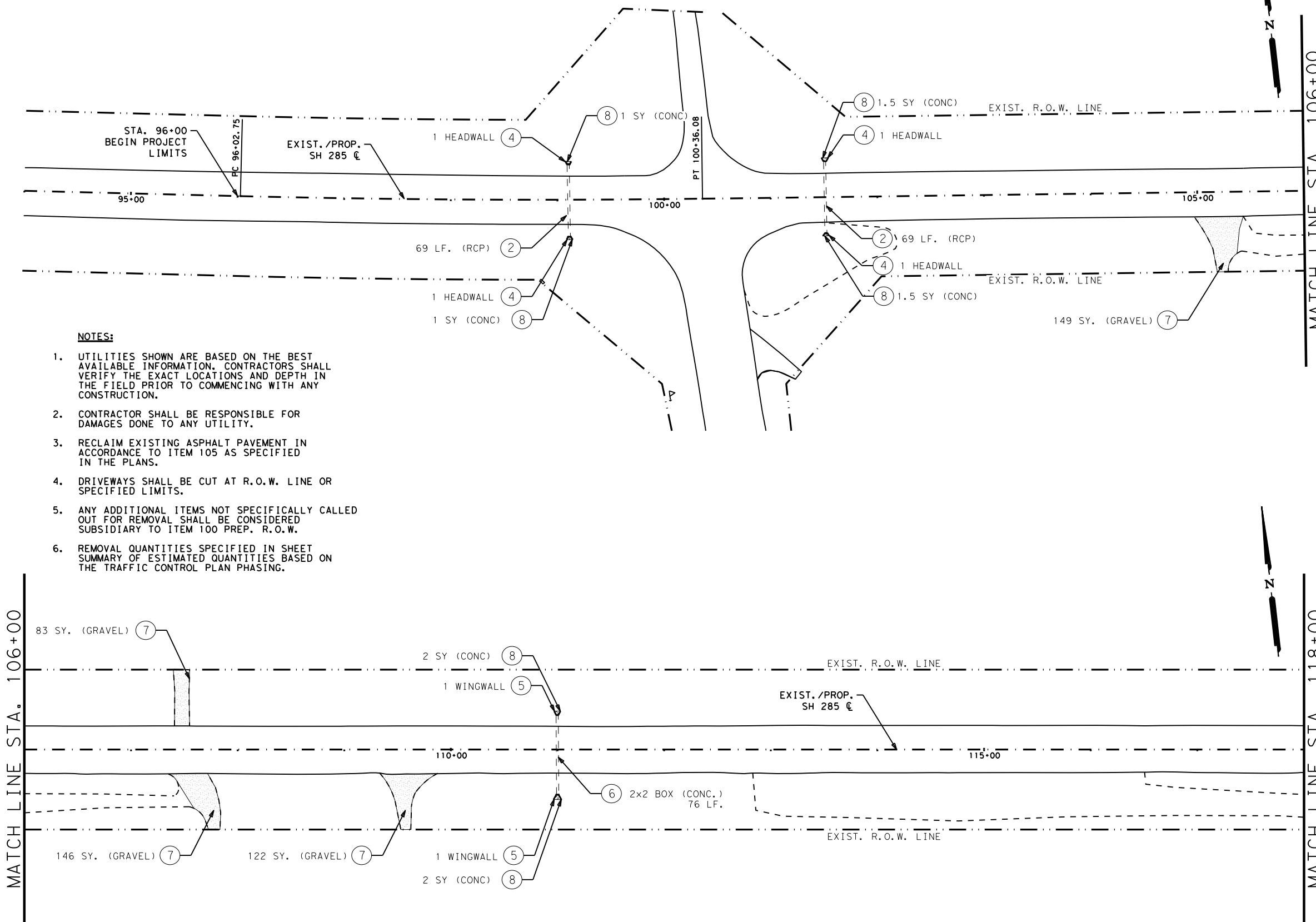
DATE: 2/7/2024 11:10:37 AM
 FILE: D:\projects\phr\design\sh 285 removal layouts\sh 285 removal 1.dgn

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	9
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6006	REMOVE STR (HEADWALL)	EA	4
0496 6007	REMOVE STR (PIPE)	LF	138
0496 6008	REMOVE STR (BOX CULVERT)	LF	76
*	REMOVE GRAVEL DRIVEWAY	SY	500

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



NOTES:

- UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
- RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
- DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
- ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
- REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

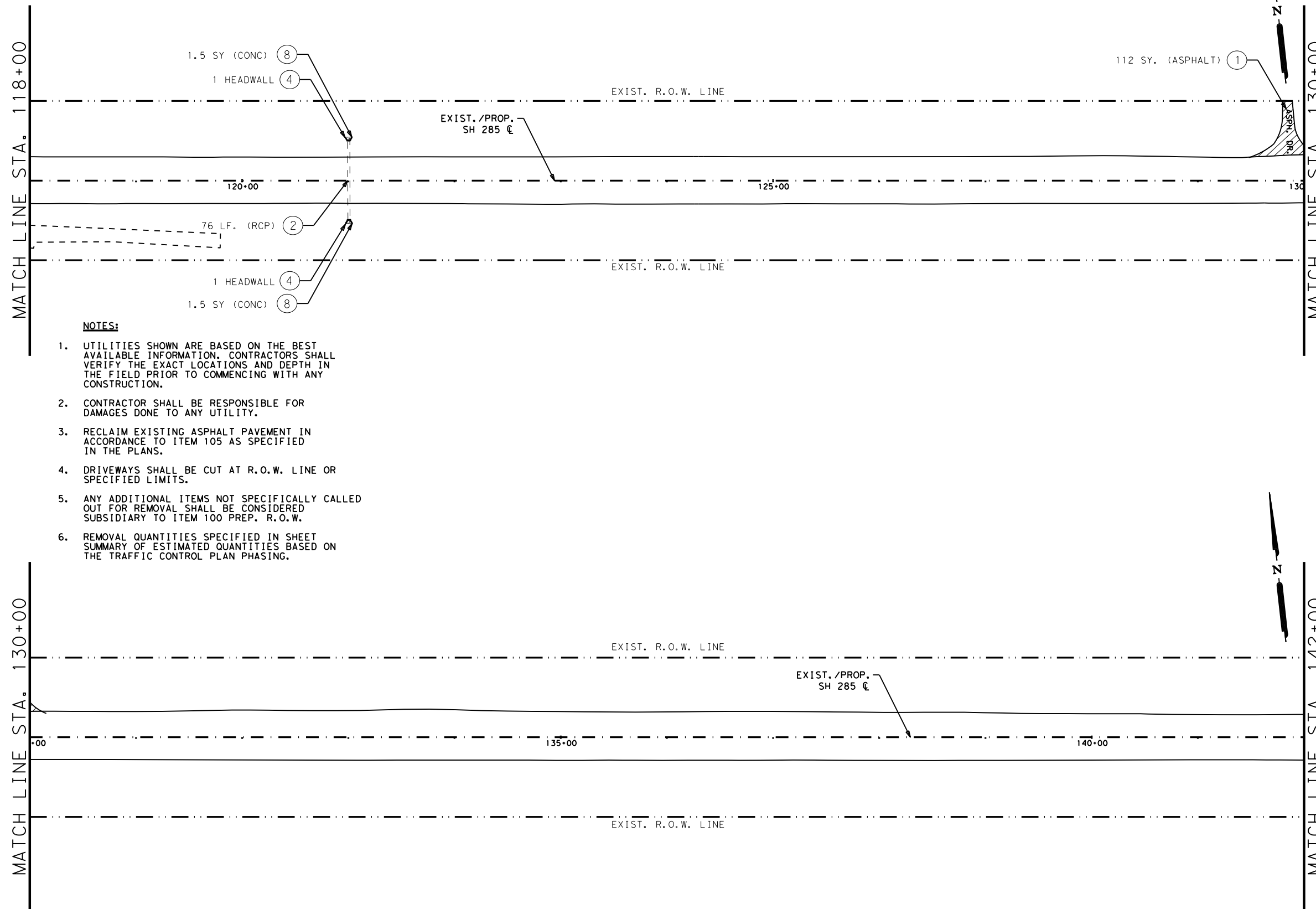
Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 1 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	387

DATE: 2/7/2024 11:10:42 AM
 FILE: P:\t\tdot.project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal.2.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	3
0496 6006	REMOVE STR (HEADWALL)	EA	2
0496 6007	REMOVE STR (PIPE)	LF	76
*	REMOVE ASPHALT DRIVEWAY	SY	112

- LEGEND:**
- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
 REMOVAL LAYOUTS**

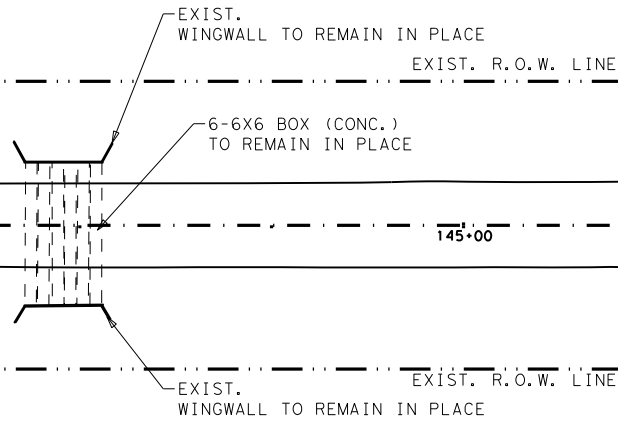
SCALE: 1"=100' SHEET 2 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	388	

DATE: 2/7/2024 11:10:49 AM
 FILE: P:\tdot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL.3.dgn

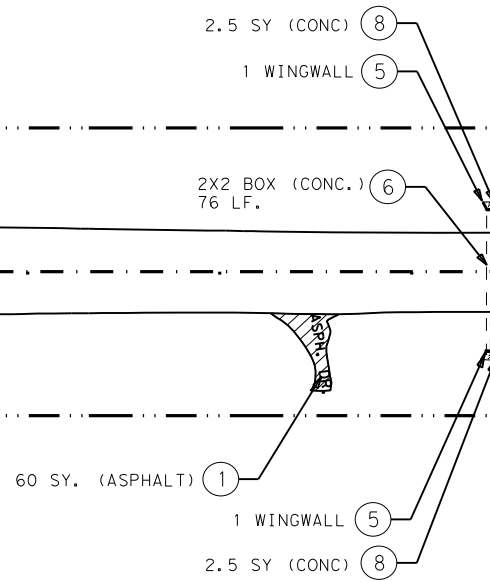
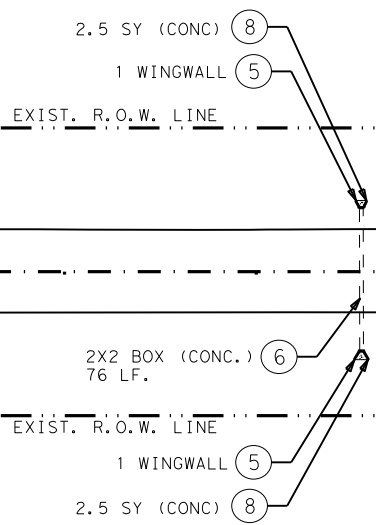
MATCH LINE STA. 142+00

MATCH LINE STA. 154+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.



MATCH LINE STA. 154+00

MATCH LINE STA. 166+00

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	10
0496 6005	REMOVE STR (WINGWALL)	EA	4
0496 6008	REMOVE STR (BOX CULVERT)	LF	152
*	REMOVE ASPHALT DRIVEWAY	SY	60

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

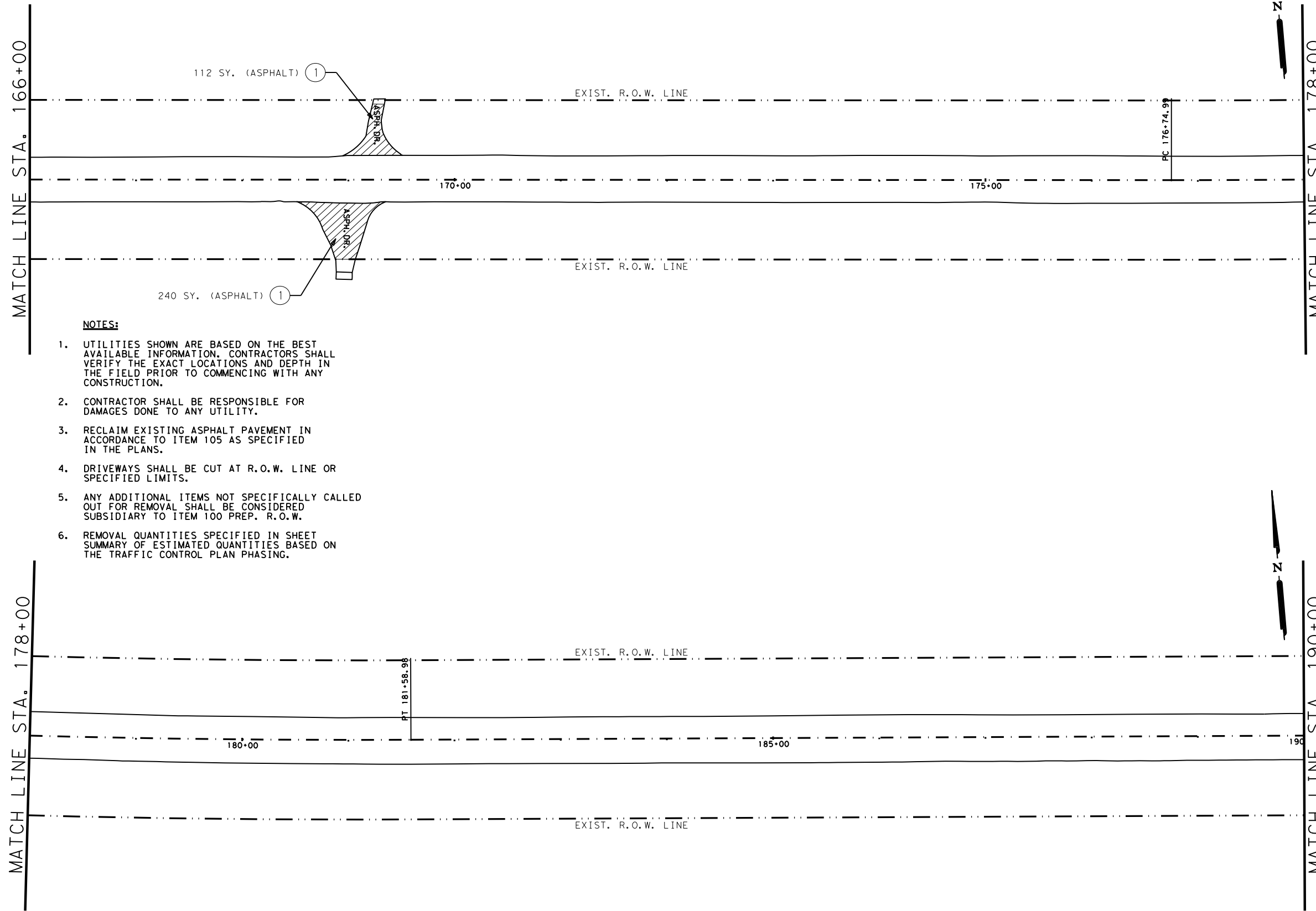
Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 3 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	389	

DATE: 2/7/2024 11:10:58 AM
 FILE: P:\xdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL 4.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
*	REMOVE ASPHALT DRIVEWAY	SY	352

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

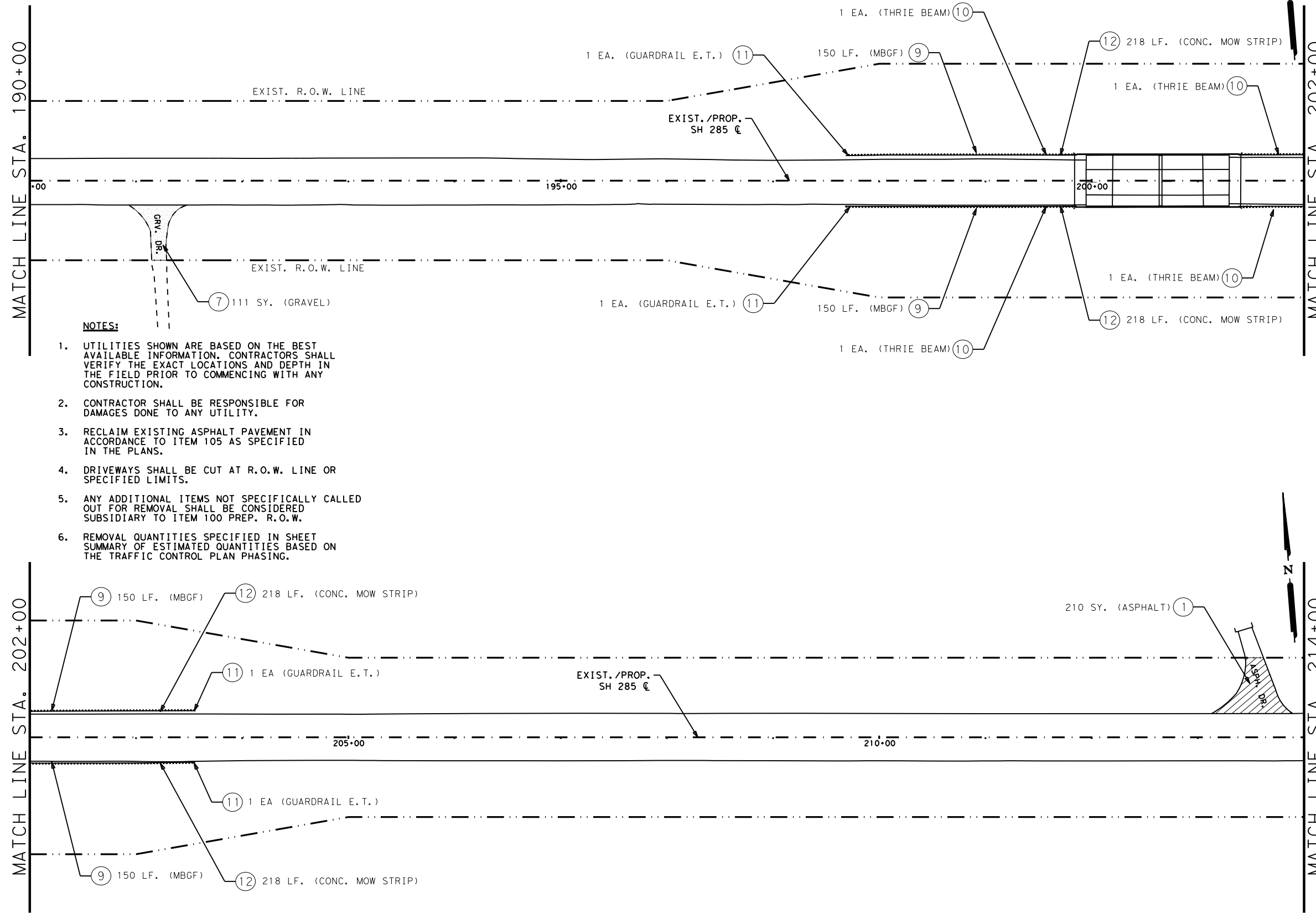


**SH 285
REMOVAL LAYOUTS**

SCALE: 1" = 100' SHEET 4 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	390

DATE: 2/7/2024 11:11:03 AM
 FILE: D:\xtdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL.5.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT
0542 6001	REMOVE MBGF	LF	600
0542 6004	REMOVE THRIE BEAM	EA	4
0544 6003	REMOVE GUARDRAIL E.T.	EA	4
0104 6054	REMOVE CONC (MOW STRIP)	LF	872
*	REMOVE ASPHALT DRIVEWAY	SY	210
*	REMOVE GRAVEL DRIVEWAY	SY	111

- LEGEND:**
- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E.T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 5 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	391	

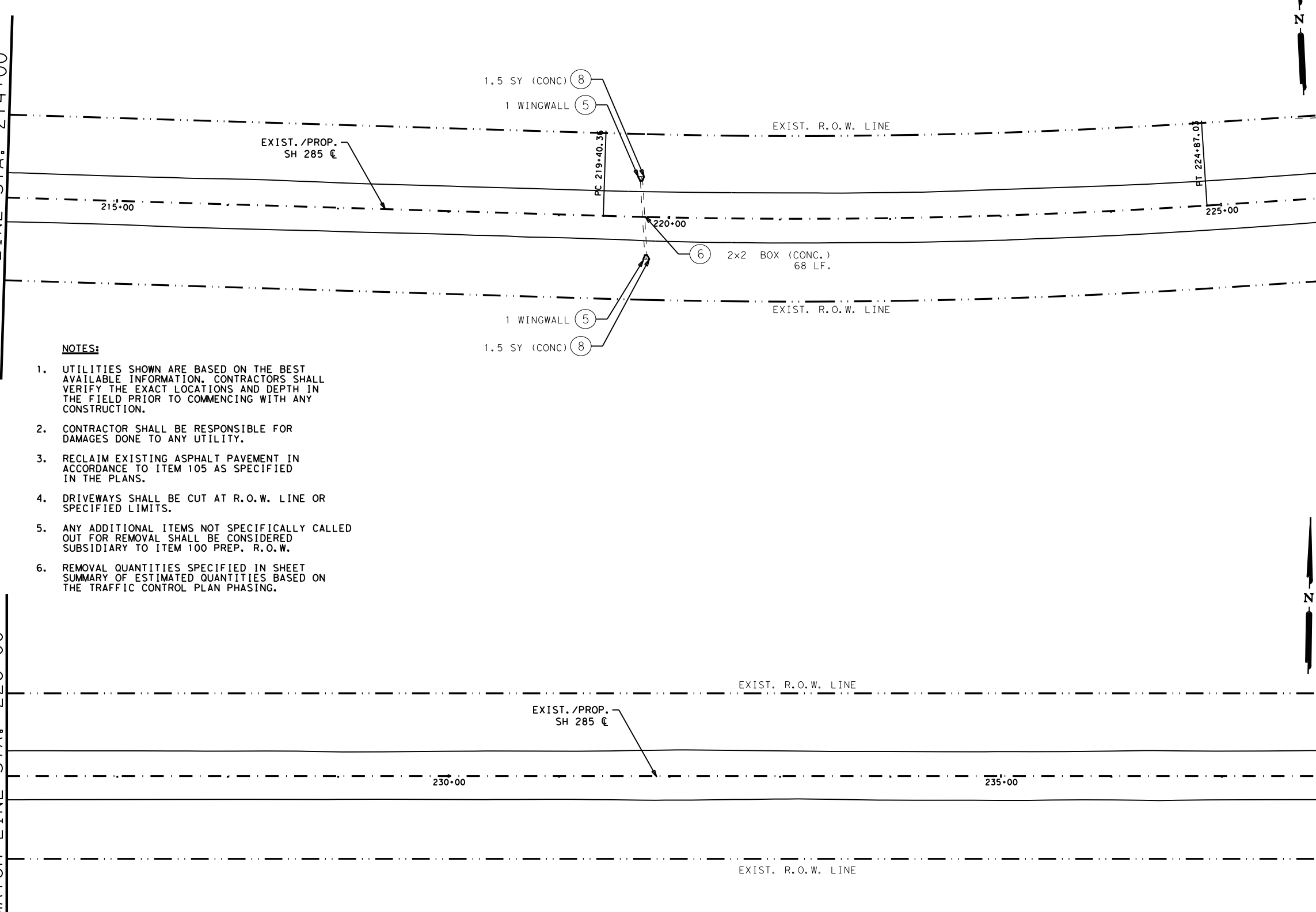
DATE: 2/7/2024 11:11:10 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL.6.dgn

MATCH LINE STA. 214+00

MATCH LINE STA. 226+00

MATCH LINE STA. 226+00

MATCH LINE STA. 238+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	3
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	68

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia
 02.13.2024

Pharr District Central Design
 Texas Department of Transportation

**SH 285
 REMOVAL LAYOUTS**

SCALE: 1" = 100' SHEET 6 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	392	

DATE: 2/7/2024 11:11:18 AM
 FILE: P:\dot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL 7.dgn

MATCH LINE STA. 238+00

MATCH LINE STA. 250+00

MATCH LINE STA. 250+00

MATCH LINE STA. 262+00

1056 SY (ASPHALT) ①

EXIST. R.O.W. LINE

EXIST./PROP.
SH 285 €

240+00

245+00

250+00

EXIST. R.O.W. LINE

NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

EXIST. R.O.W. LINE

EXIST./PROP.
SH 285 €

255+00

260+00

EXIST. R.O.W. LINE

1.5 SY (CONC) ⑧

1 WINGWALL ⑤

⑥ 2x2 BOX (CONC.)
76 LF.

1 WINGWALL ⑤

1.5 SY (CONC) ⑧

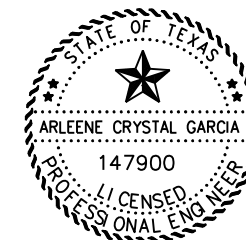
SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	3
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	76
*	REMOVE ASPHALT (DRIVEWAY)	SY	1056

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
- ② REMOVE STR (PIPE)
- ③ REMOVE STR (SET)
- ④ REMOVE STR (HEADWALL)
- ⑤ REMOVE STR (WINGWALL)
- ⑥ REMOVE STR (BOX CULVERT)
- ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
- ⑧ REMOVE CONC (MISC)
- ⑨ REMOVE MBGF
- ⑩ REMOVE THRIE BEAM
- ⑪ REMOVE GUARDRAIL E. T.
- ⑫ REMOVE CONC (MOW STRIP)

* FOR CONTRACTOR INFORMATION ONLY
(SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



SH 285
REMOVAL LAYOUTS

SCALE: 1"=100' SHEET 7 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	393	

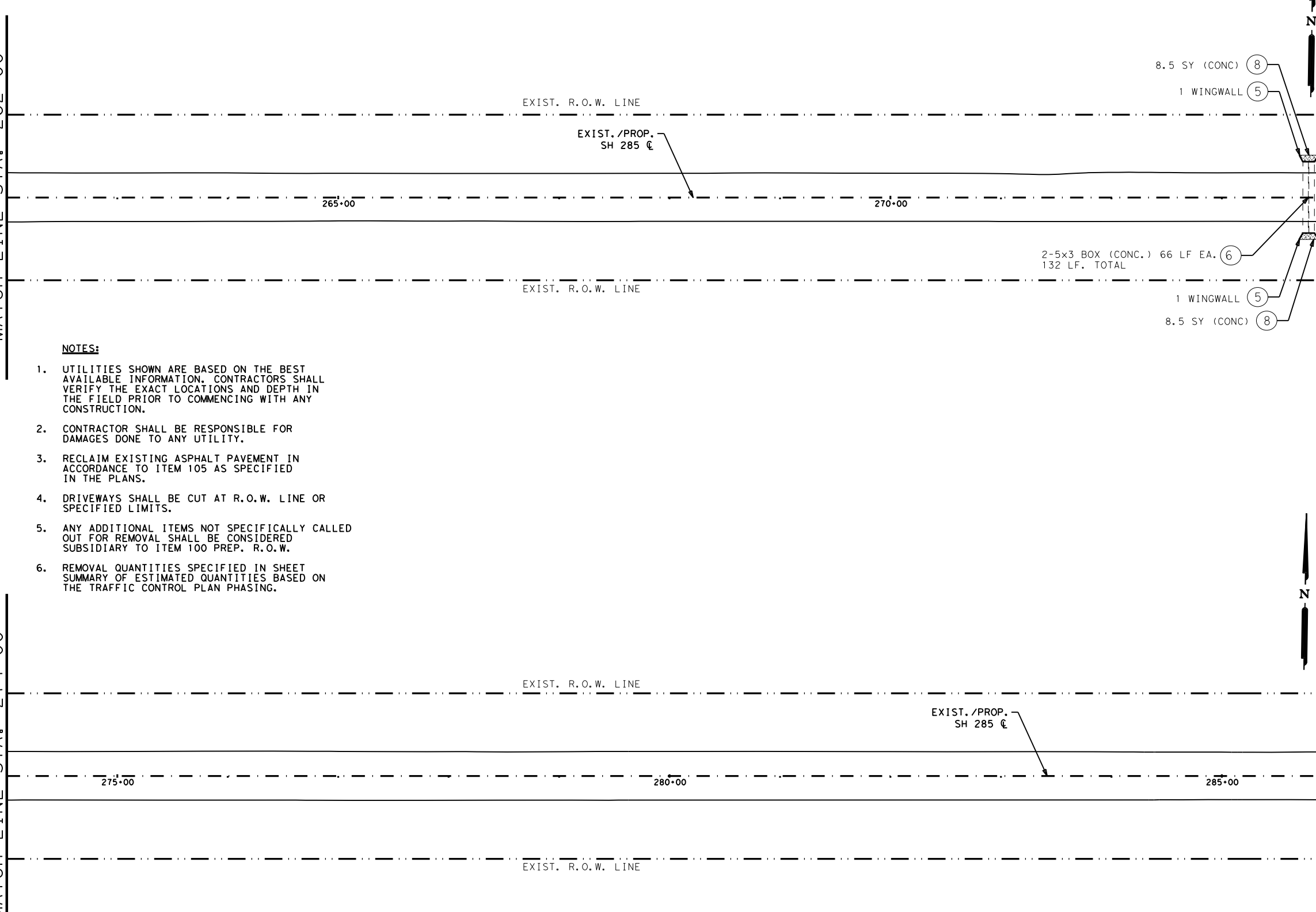
DATE: 2/7/2024 11:11:23 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal 8.dgn

MATCH LINE STA. 262+00

MATCH LINE STA. 274+00

MATCH LINE STA. 274+00

MATCH LINE STA. 286+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	17
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	132

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 8 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	394	

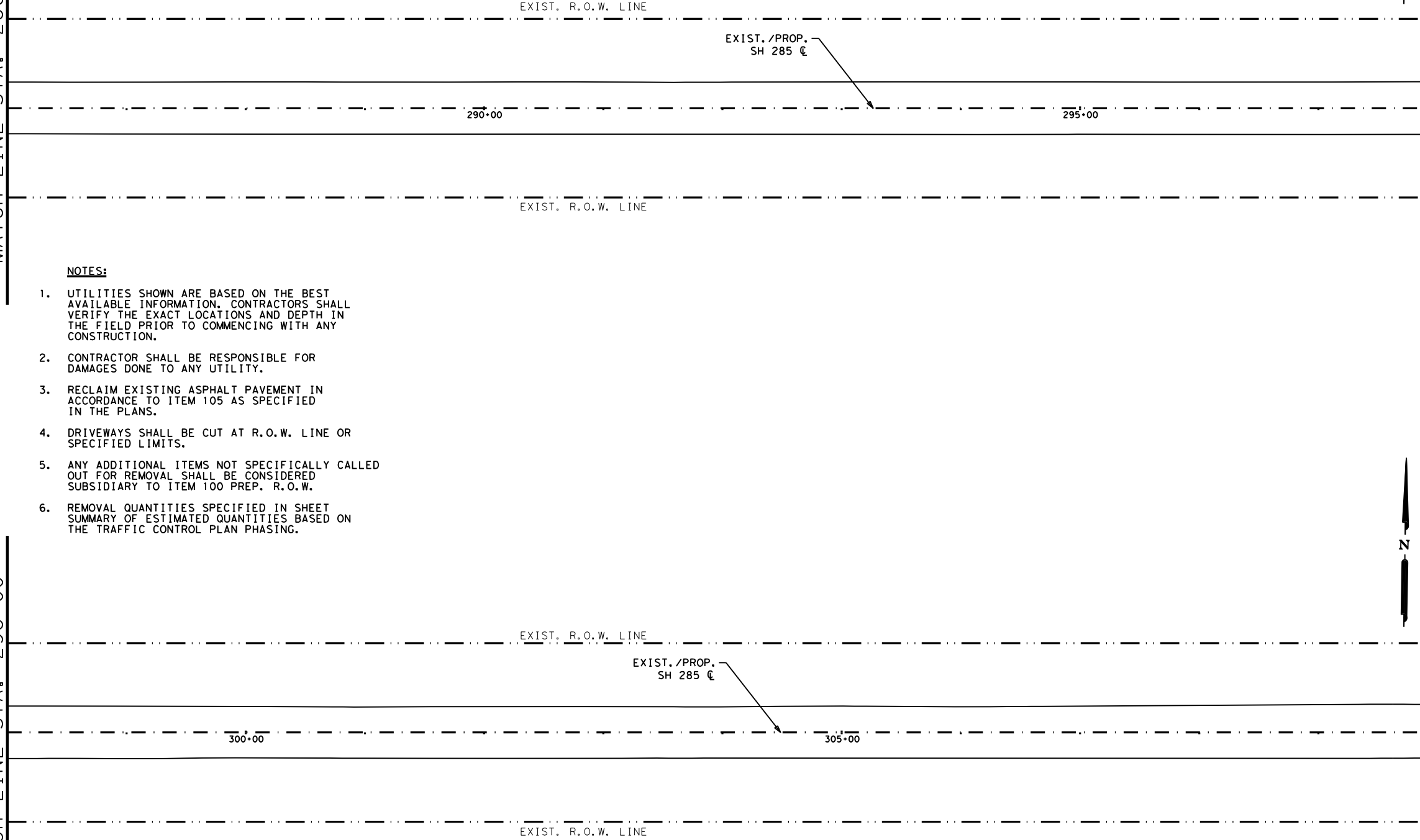
DATE: 2/7/2024 11:11:29 AM
 FILE: \\phr\dot\project\wiseonline.com:TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal 9.dgn

MATCH LINE STA. 286+00

MATCH LINE STA. 298+00

MATCH LINE STA. 298+00

MATCH LINE STA. 310+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
- ② REMOVE STR (PIPE)
- ③ REMOVE STR (SET)
- ④ REMOVE STR (HEADWALL)
- ⑤ REMOVE STR (WINGWALL)
- ⑥ REMOVE STR (BOX CULVERT)
- ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
- ⑧ REMOVE CONC (MISC)
- ⑨ REMOVE MBGF
- ⑩ REMOVE THRIE BEAM
- ⑪ REMOVE GUARDRAIL E. T.
- ⑫ REMOVE CONC (MOW STRIP)

* FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

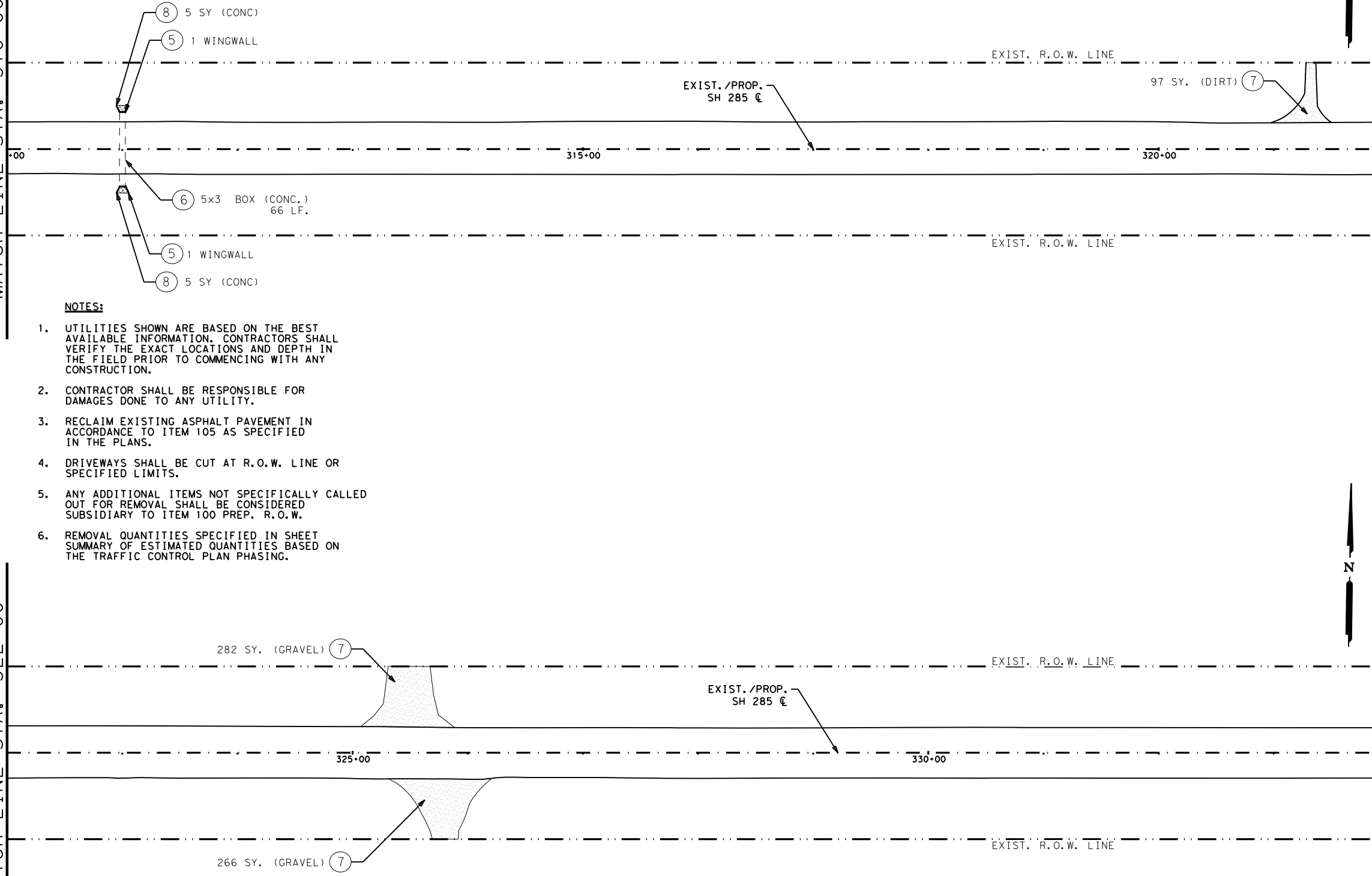
SCALE: 1" = 100' SHEET 9 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY		SHEET NO.
PHR	JIM HOGG		395

DATE: 2/7/2024 11:11:34 AM
 FILE: D:\projects\pharr\sh285\sh285removal\10.dgn
 PROJECT: TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal 10.dgn

MATCH LINE STA. 310+00

MATCH LINE STA. 322+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	10
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	66
*	REMOVE DIRT DRIVEWAY	SY	97
*	REMOVE GRAVEL DRIVEWAY	SY	548

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 10 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	396	

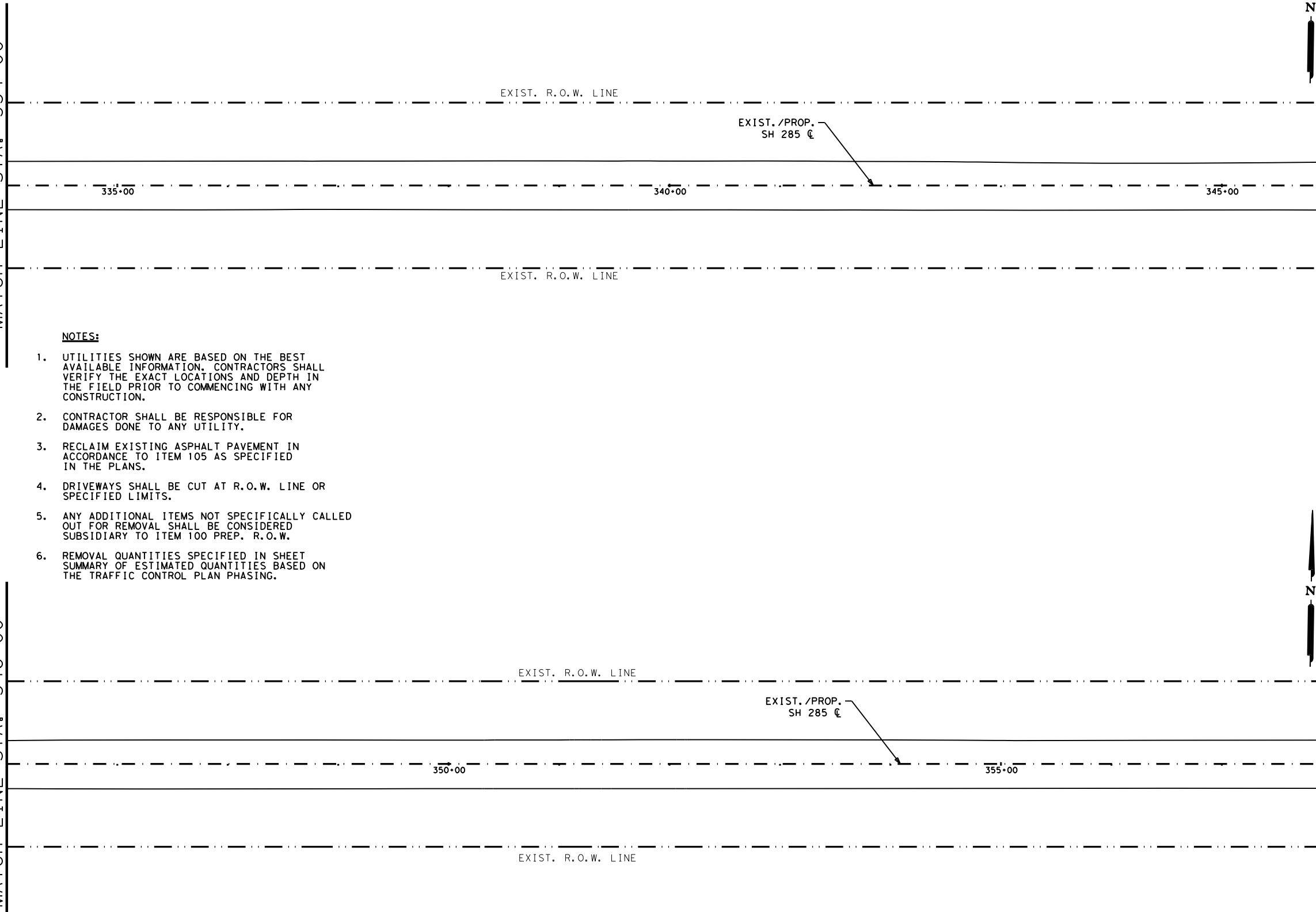
DATE: 2/7/2024 11:11:38 AM
 FILE: P:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL 11.dgn

MATCH LINE STA. 334+00

MATCH LINE STA. 346+00

MATCH LINE STA. 346+00

MATCH LINE STA. 358+00



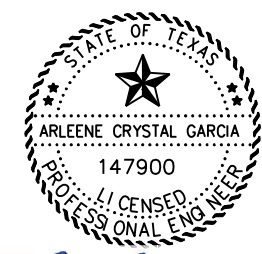
NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

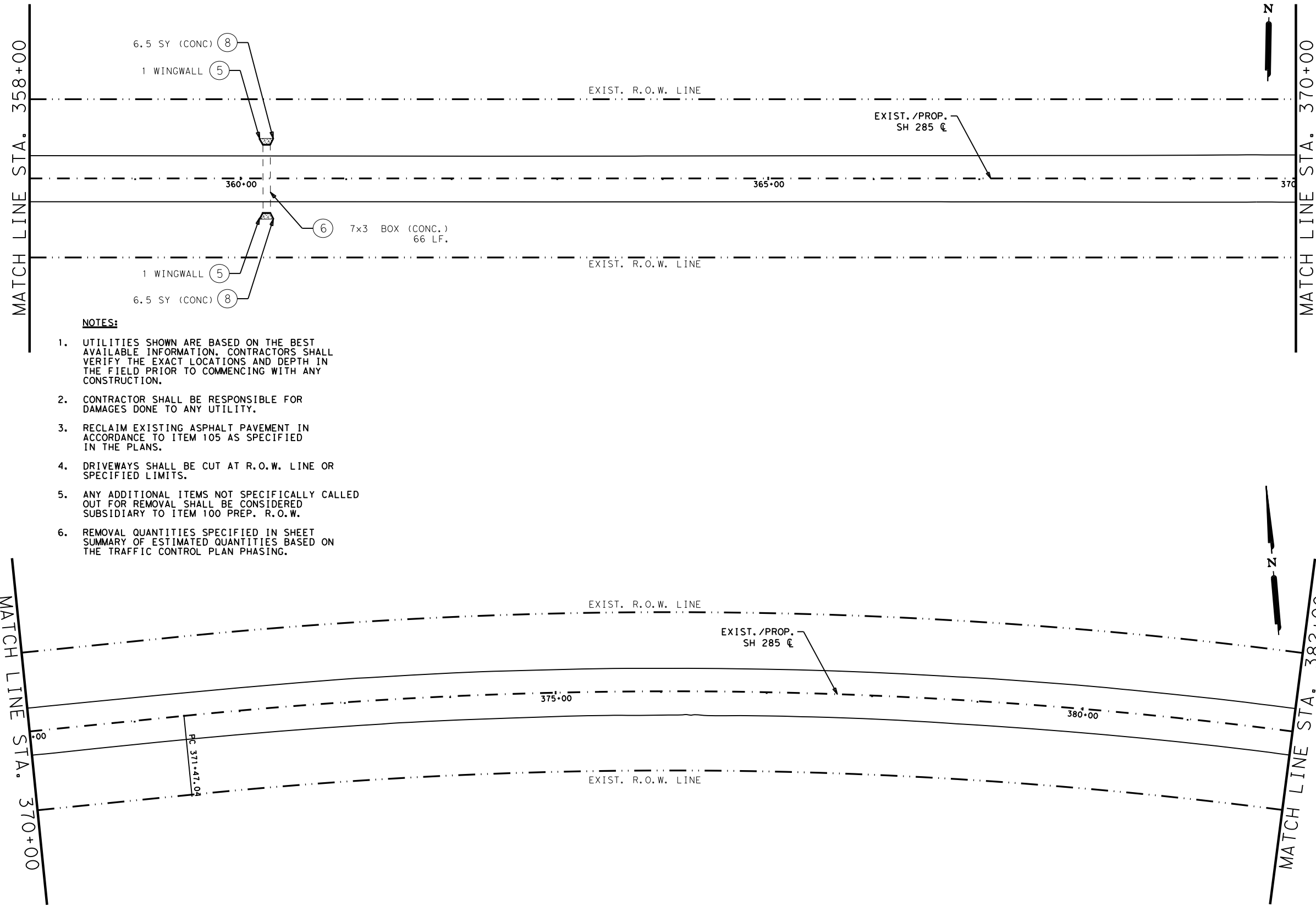


**SH 285
REMOVAL LAYOUTS**

SCALE: 1" = 100' SHEET 11 OF 22

© 2024	CONT	SECT	JOB	HIGHWAY
	0482	01	036, etc.	SH 285
	DIST	COUNTY	SHEET NO.	
	PHR	JIM HOGG	397	

DATE: 2/7/2024 11:11:43 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL.12.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	13
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	66

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

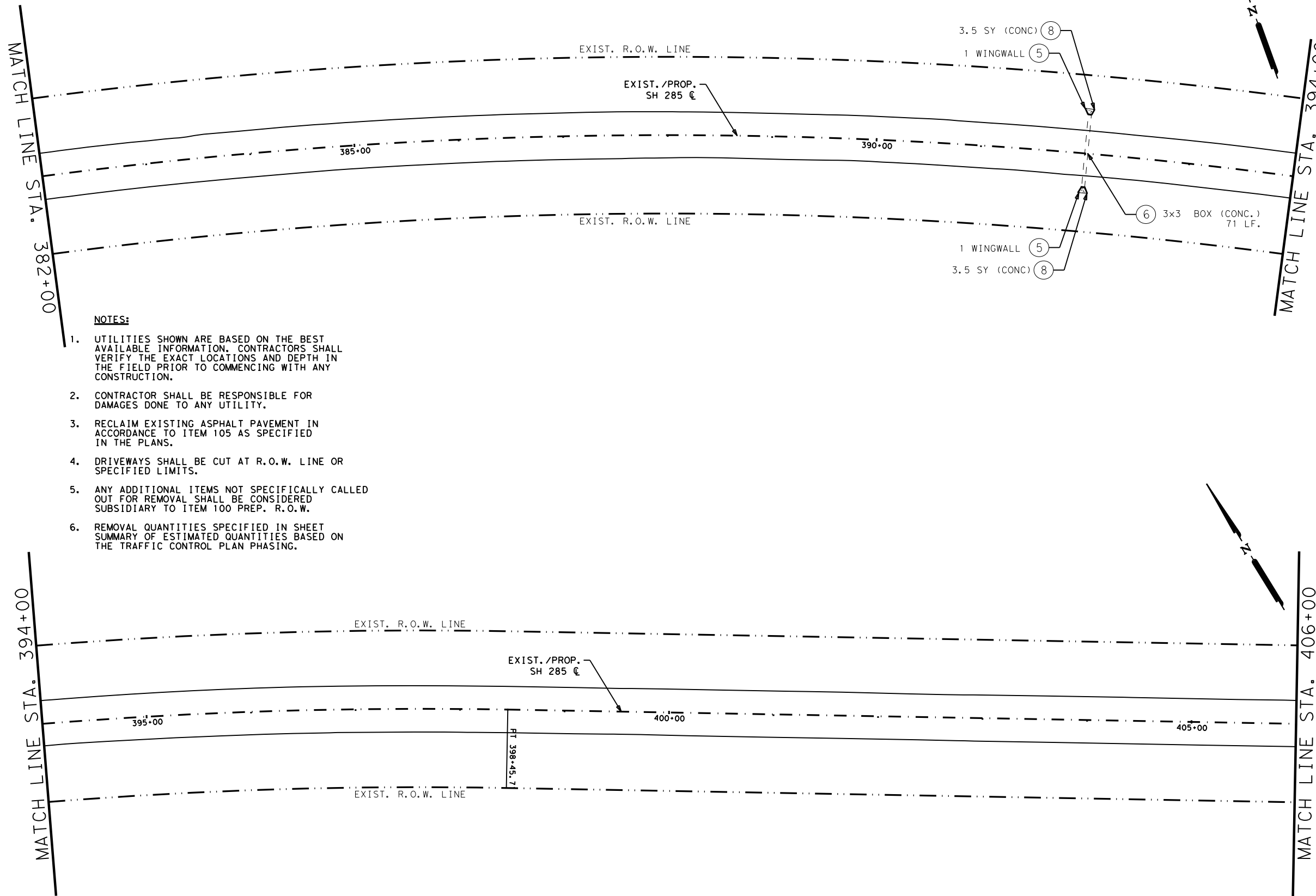
Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 12 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	398	

DATE: 2/7/2024 11:11:49 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL.13.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	7
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	71

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

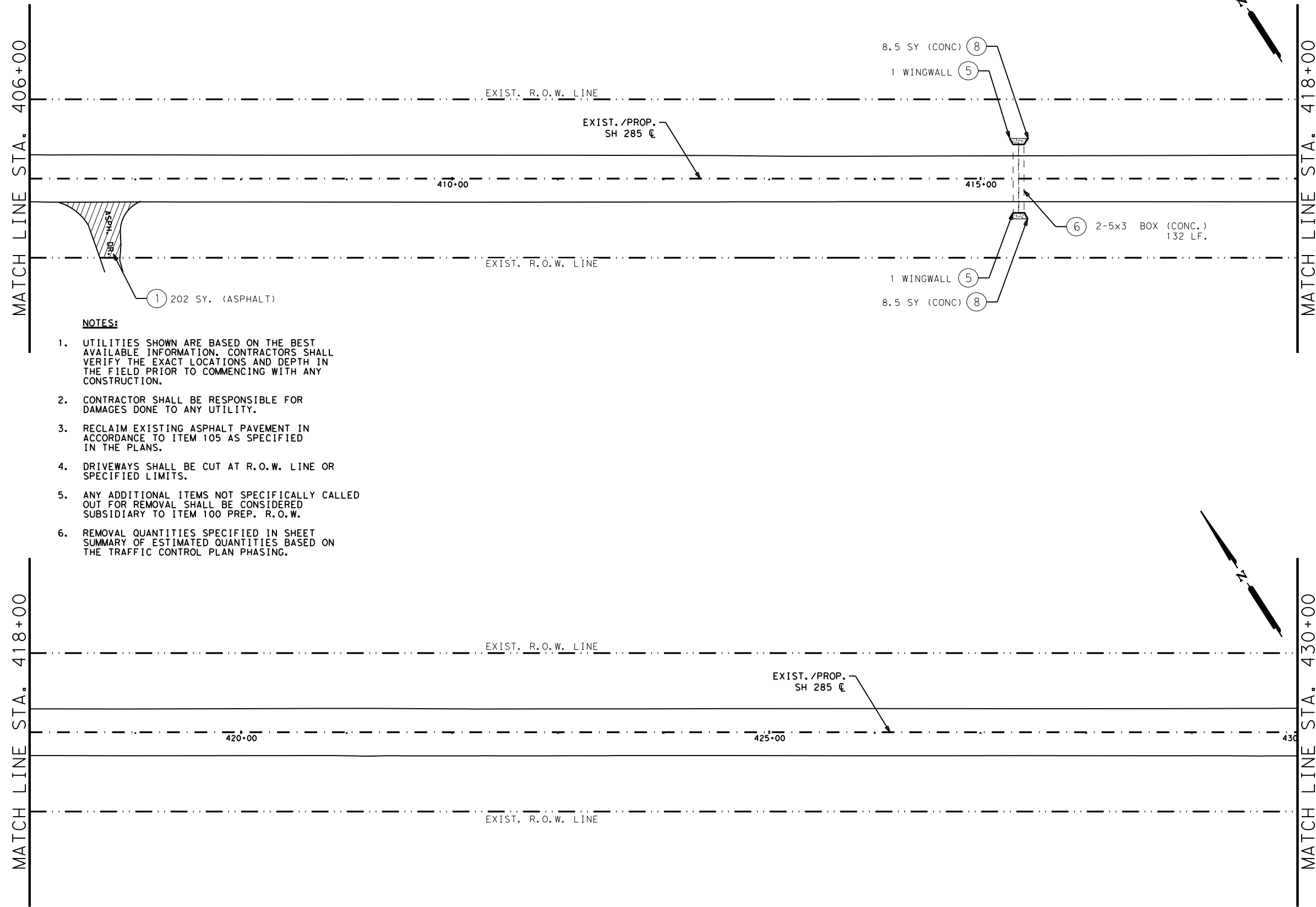


**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 13 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	399

DATE: 2/7/2024 11:11:54 AM
 FILE: D:\projects\projectwiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10 - Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL 14.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	17
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	132
*	REMOVE ASPHALT DRIVEWAY	SY	202

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arleene Crystal Garcia

02.13.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 14 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	400

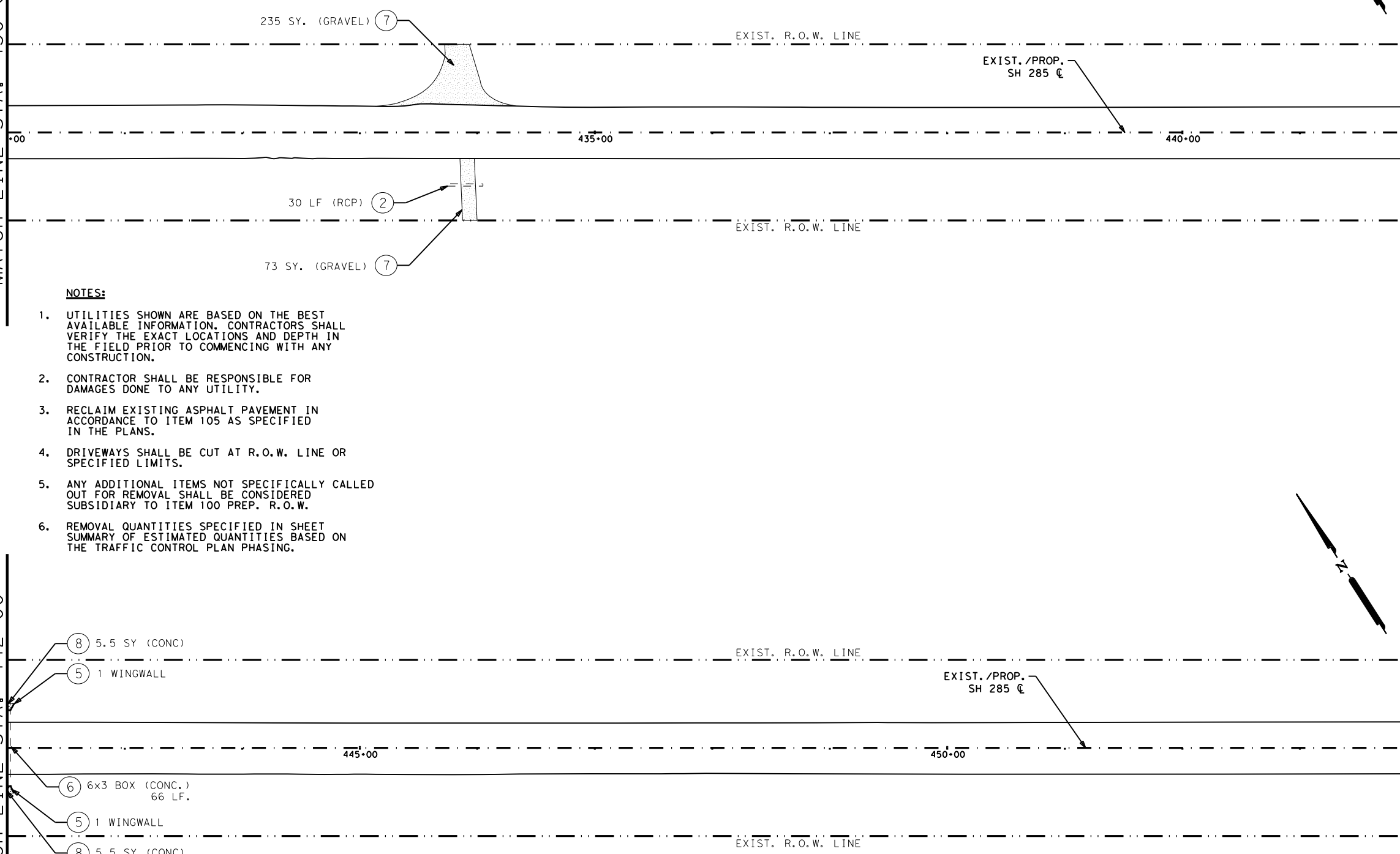
DATE: 2/7/2024 11:11:59 AM
 FILE: P:\t\tdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal 15.dgn

MATCH LINE STA. 430+00

MATCH LINE STA. 442+00

MATCH LINE STA. 442+00

MATCH LINE STA. 454+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	11
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6007	REMOVE STR (PIPE)	LF	30
0496 6008	REMOVE STR (BOX CULVERT)	LF	66
*	REMOVE GRAVEL DRIVEWAY	SY	308

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

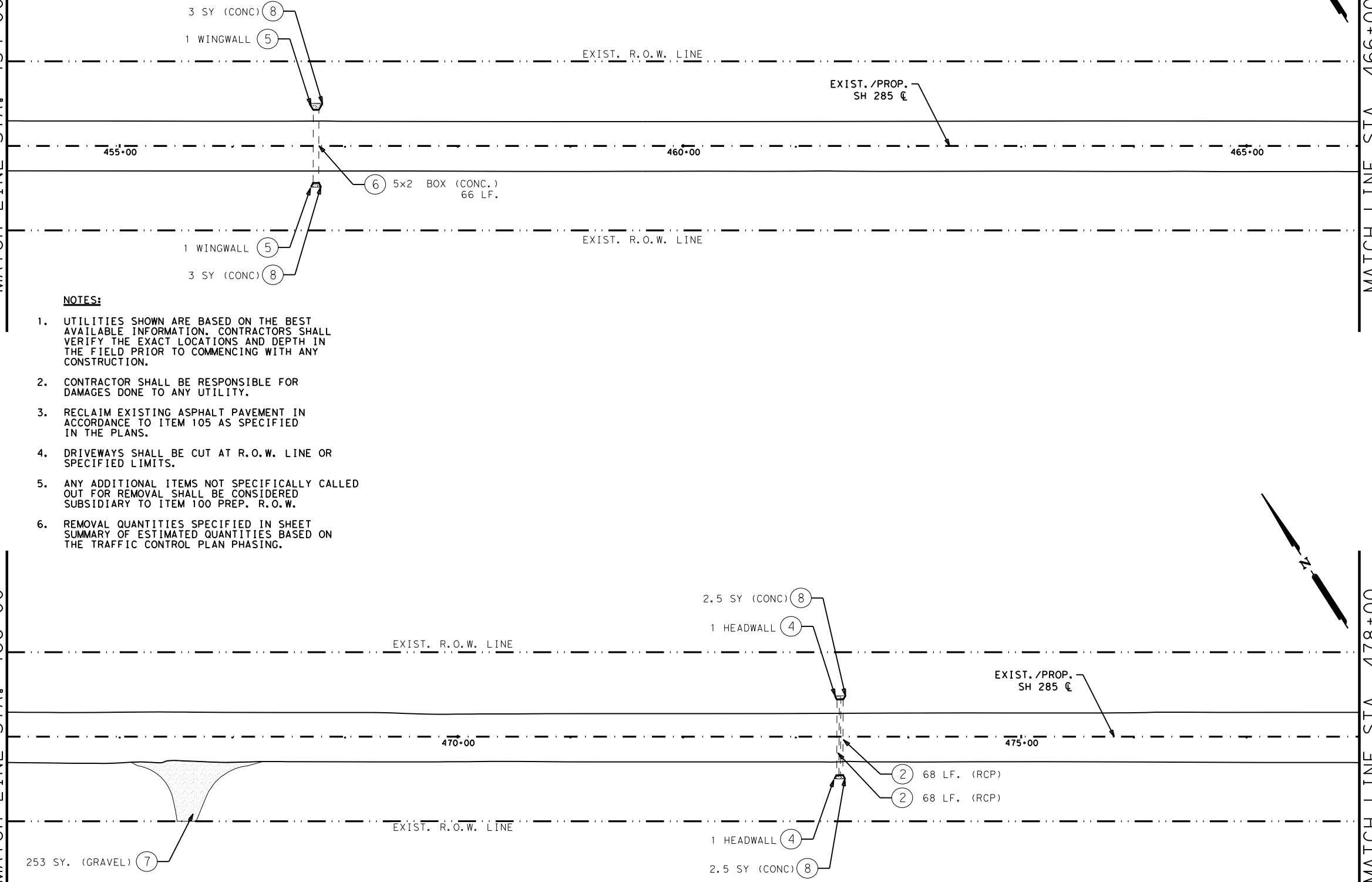
SCALE: 1" = 100' SHEET 15 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	401	

DATE: 2/7/2024 11:12:03 AM
 FILE: P:\txdot\projectwiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal 16.dgn

MATCH LINE STA. 454+00

MATCH LINE STA. 466+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	11
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6006	REMOVE STR (HEADWALL)	EA	2
0496 6007	REMOVE STR (PIPE)	LF	136
0496 6008	REMOVE STR (BOX CULVERT)	LF	66
*	REMOVE GRAVEL DRIVEWAY	SY	253

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 16 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	402	

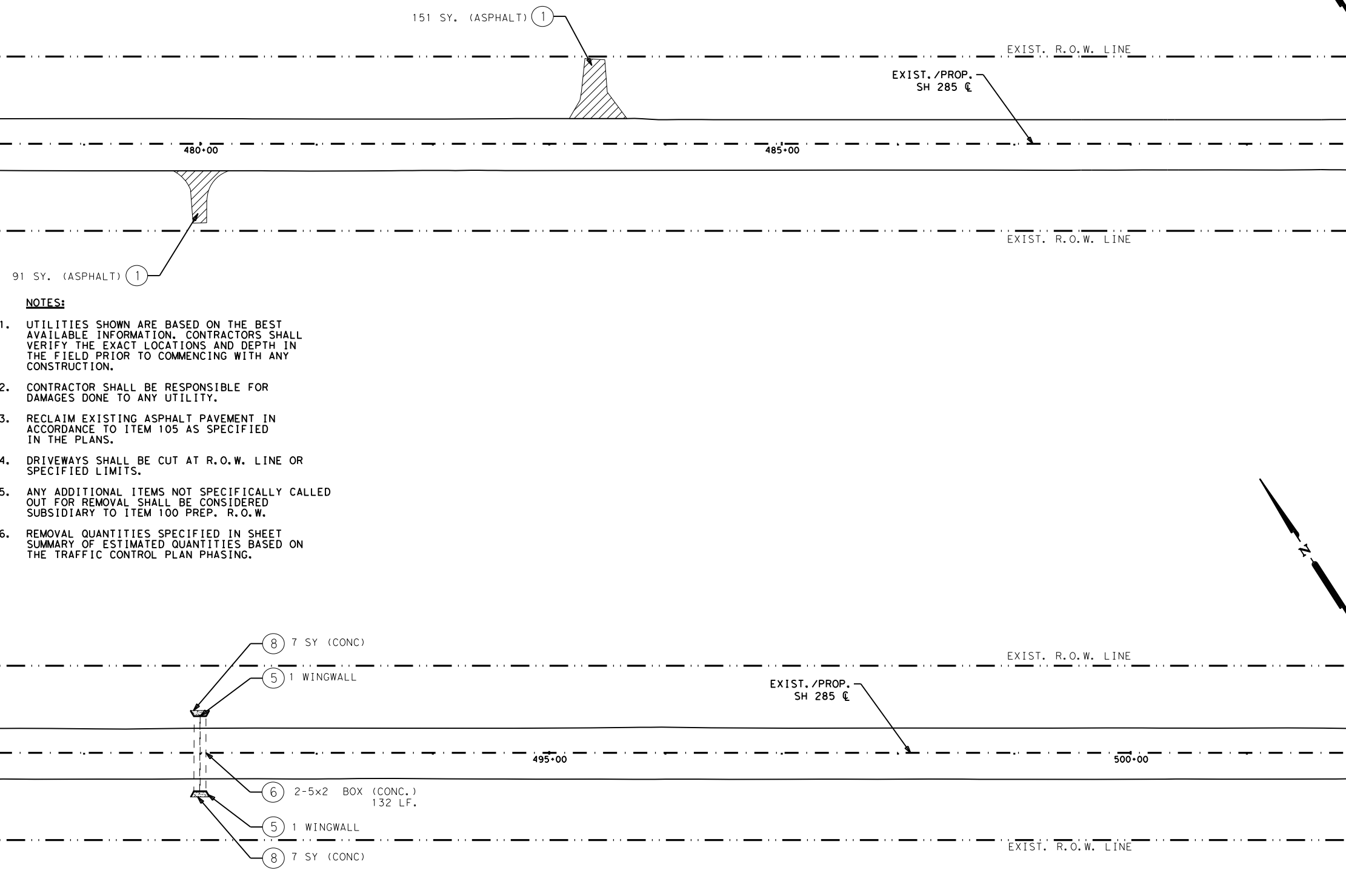
DATE: 2/7/2024 11:12:09 AM
 FILE: P:\dot\project\wiseonline.com\TXDOT5\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal 17.dgn

MATCH LINE STA. 478+00

MATCH LINE STA. 490+00

MATCH LINE STA. 490+00

MATCH LINE STA. 502+00



- NOTES:**
1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
 2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
 3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
 4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
 5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
 6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	14
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	132
*	REMOVE ASPHALT DRIVEWAY	SY	242

- LEGEND:
- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

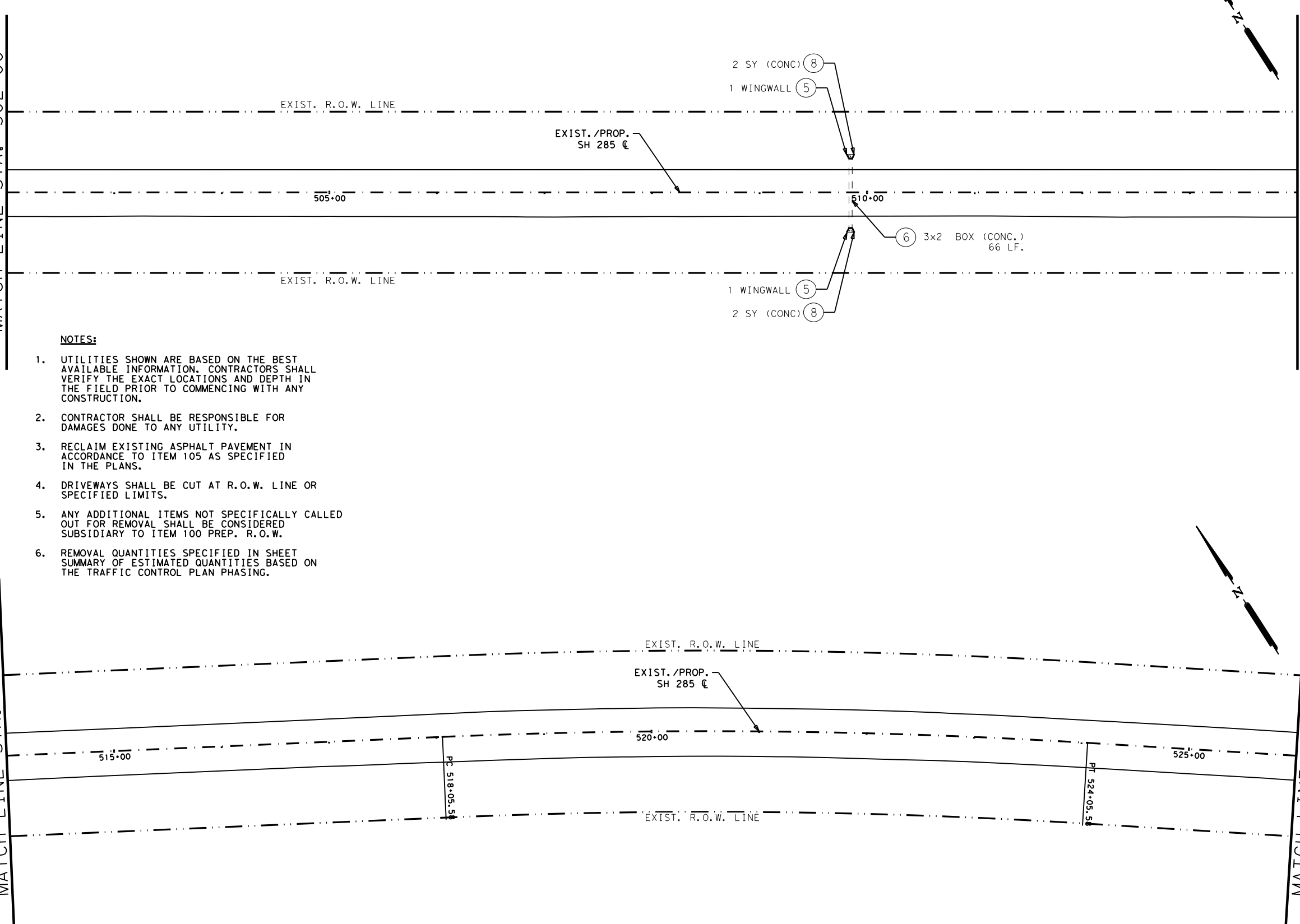
SCALE: 1"=100' SHEET 17 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	403	

DATE: 2/7/2024 11:12:14 AM
 FILE: D:\txdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL 18.dgn

MATCH LINE STA. 502+00

MATCH LINE STA. 514+00



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	4
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	66

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 18 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	404

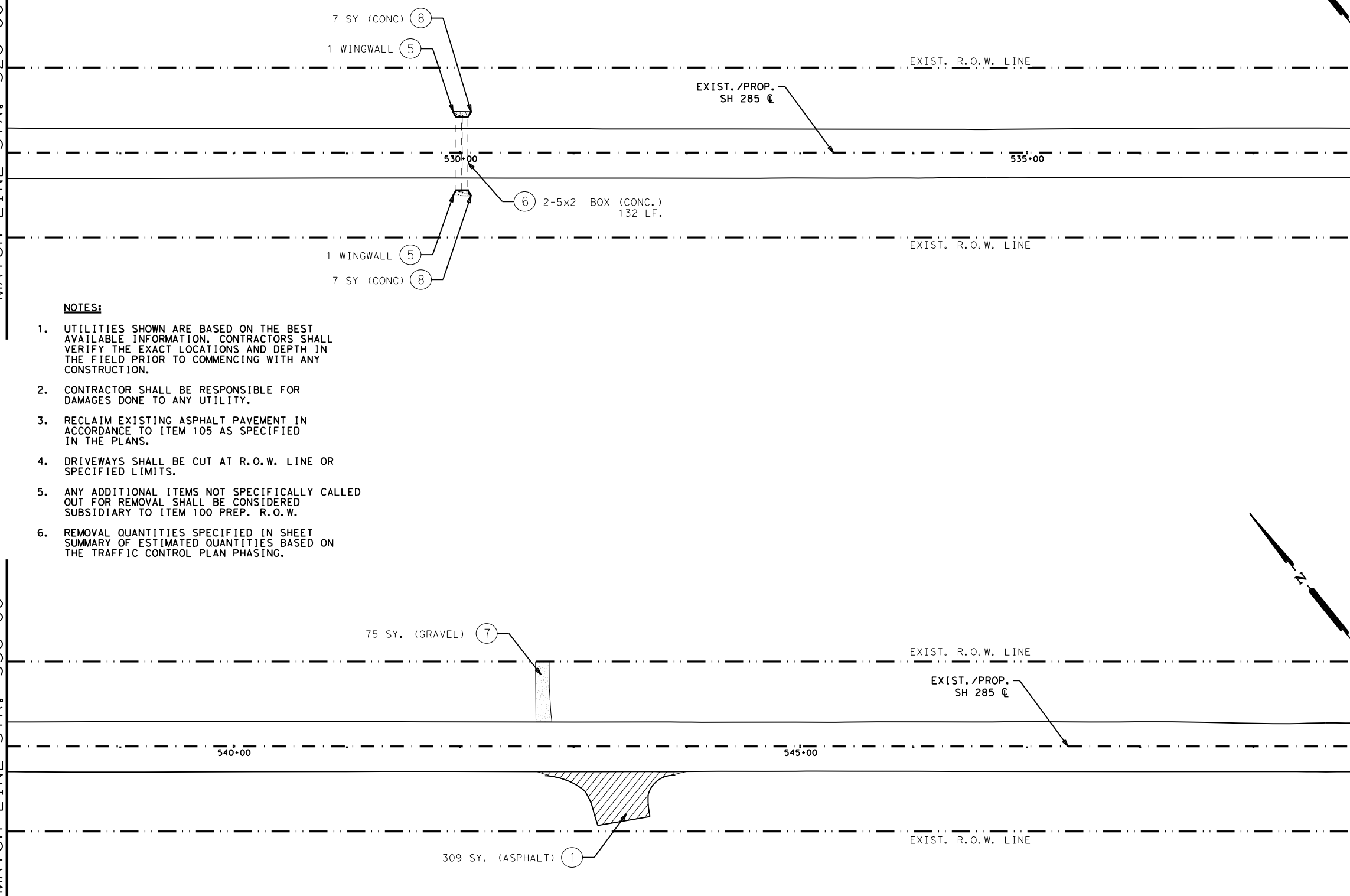
DATE: 2/7/2024 11:12:19 AM
 FILE: D:\projects\pharr\sh285\sh285removal\19.dgn

MATCH LINE STA. 526+00

MATCH LINE STA. 538+00

MATCH LINE STA. 538+00

MATCH LINE STA. 550+00



NOTES:

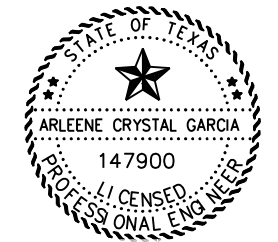
1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	14
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	132
*	REMOVE ASPHALT DRIVEWAY	SY	309
*	REMOVE GRAVEL DRIVEWAY	SY	75

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

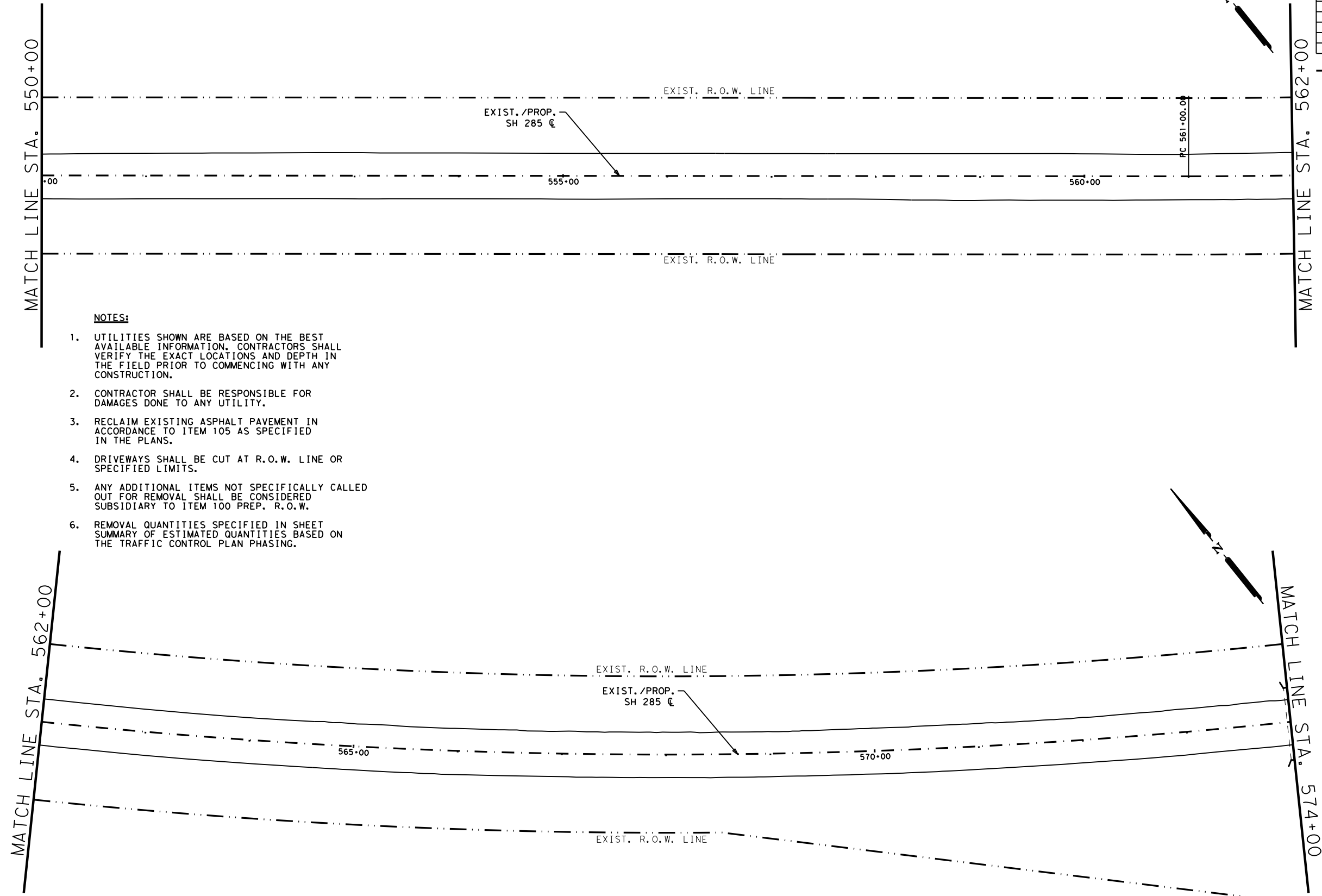


**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 19 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	405

DATE: 2/7/2024 11:12:24 AM
 FILE: D:\xtdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL20.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS			
ITEM	DESCRIPTION	UNIT	QUANT

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC) *
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design

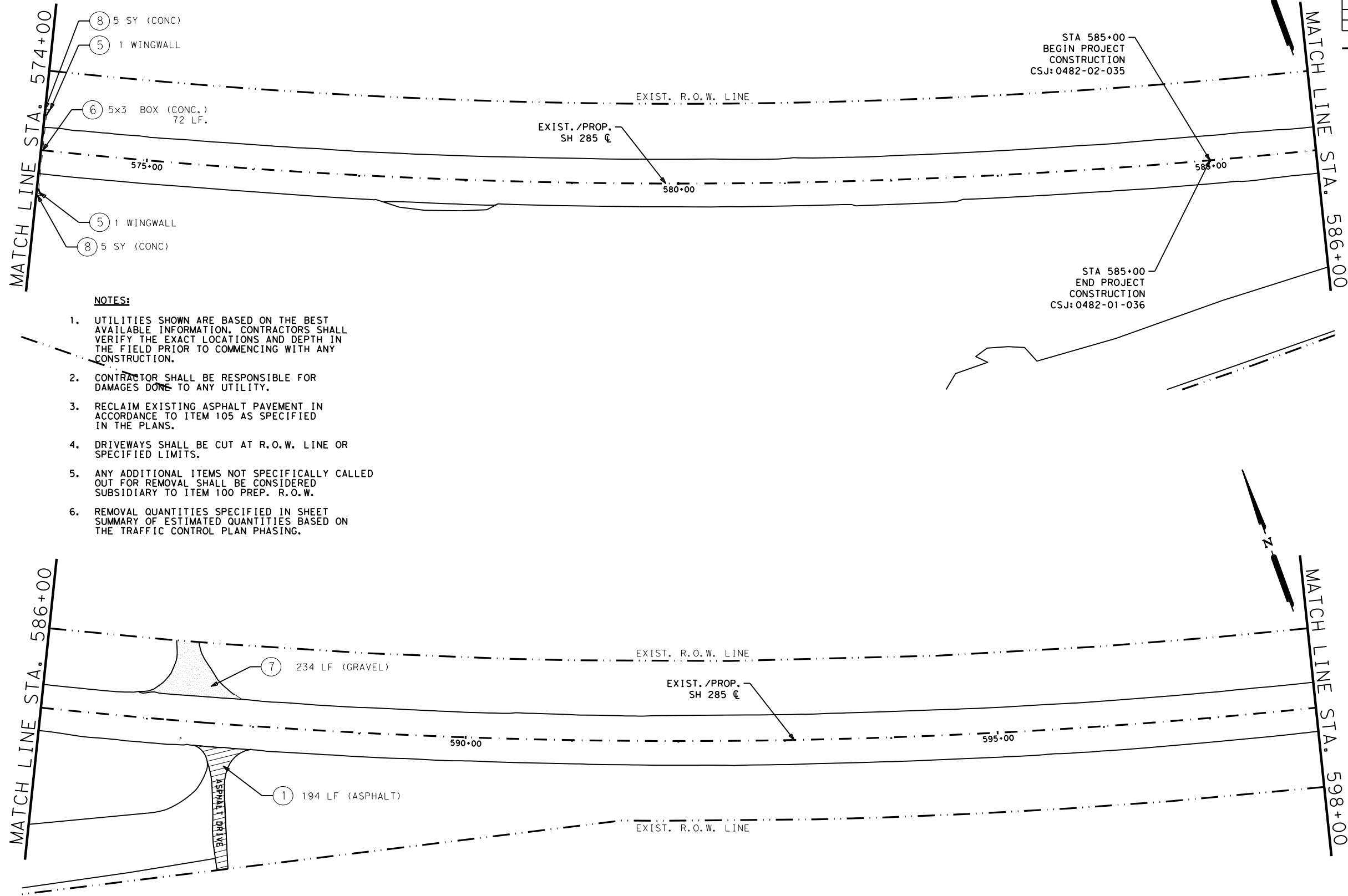
Texas Department of Transportation

**SH 285
REMOVAL LAYOUTS**

SCALE: 1"=100' SHEET 20 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	406	

DATE: 2/7/2024 11:12:28 AM
 FILE: D:\xtdot\project\wiseonline.com\TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 Removal Layouts\SH 285 Removal\21.dgn



NOTES:

1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0104 6028	REMOVING CONC (MISC)	SY	10
0496 6005	REMOVE STR (WINGWALL)	EA	2
0496 6008	REMOVE STR (BOX CULVERT)	LF	72
*	REMOVE ASPHALT DRIVEWAY	SY	194
*	REMOVE GRAVEL DRIVEWAY	SY	234

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

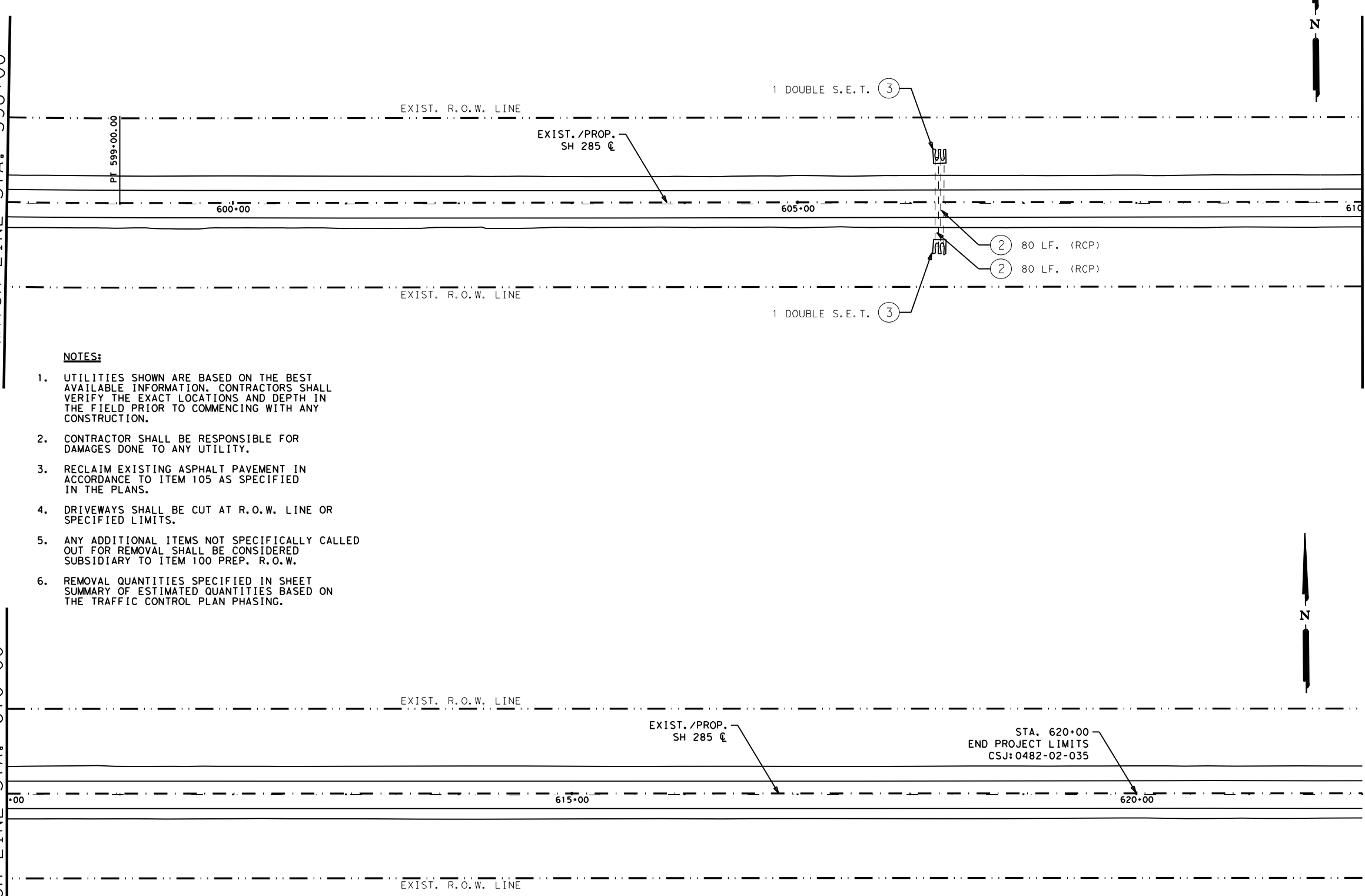
SCALE: 1"=100' SHEET 21 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST	COUNTY	SHEET NO.	
PHR	JIM HOGG	407	

DATE: 2/7/2024 11:12:32 AM
 FILE: P:\tdot\project\wiseonline.com:TXDOTS\Documents\21 - PHR\Design Projects\048201036\4 - Design\Plan Set\10. Miscellaneous\SH 285 REMOVAL LAYOUTS\SH 285 REMOVAL22.dgn

MATCH LINE STA. 598+00

MATCH LINE STA. 610+00



NOTES:

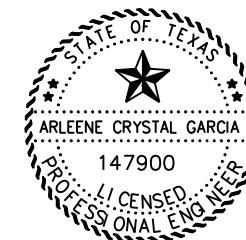
1. UTILITIES SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION. CONTRACTORS SHALL VERIFY THE EXACT LOCATIONS AND DEPTH IN THE FIELD PRIOR TO COMMENCING WITH ANY CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES DONE TO ANY UTILITY.
3. RECLAIM EXISTING ASPHALT PAVEMENT IN ACCORDANCE TO ITEM 105 AS SPECIFIED IN THE PLANS.
4. DRIVEWAYS SHALL BE CUT AT R.O.W. LINE OR SPECIFIED LIMITS.
5. ANY ADDITIONAL ITEMS NOT SPECIFICALLY CALLED OUT FOR REMOVAL SHALL BE CONSIDERED SUBSIDIARY TO ITEM 100 PREP. R.O.W.
6. REMOVAL QUANTITIES SPECIFIED IN SHEET SUMMARY OF ESTIMATED QUANTITIES BASED ON THE TRAFFIC CONTROL PLAN PHASING.

SHEET TOTALS

ITEM	DESCRIPTION	UNIT	QUANT
0496 6004	REMOVE STR (SET)	EA	2
0496 6007	REMOVE STR (PIPE)	EA	160

LEGEND:

- ① REMOVE ASPHALT (DRIVEWAY) *
 - ② REMOVE STR (PIPE)
 - ③ REMOVE STR (SET)
 - ④ REMOVE STR (HEADWALL)
 - ⑤ REMOVE STR (WINGWALL)
 - ⑥ REMOVE STR (BOX CULVERT)
 - ⑦ REMOVE GRAVEL/CALICHE/DIRT (DRIVEWAY) *
 - ⑧ REMOVE CONC (MISC)
 - ⑨ REMOVE MBGF
 - ⑩ REMOVE THRIE BEAM
 - ⑪ REMOVE GUARDRAIL E. T.
 - ⑫ REMOVE CONC (MOW STRIP)
- * FOR CONTRACTOR INFORMATION ONLY (SUBSIDIARY TO ITEM 100 PREP. R.O.W.)



Arlene Crystal Garcia

02.13.2024

Pharr District Central Design



**SH 285
REMOVAL LAYOUTS**

SCALE: 1" = 100' SHEET 22 OF 22

CONT	SECT	JOB	HIGHWAY
0482	01	036, etc.	SH 285
DIST		COUNTY	SHEET NO.
PHR		JIM HOGG	408