

Project Number: RMC 6463-11-001

County: WEBB

Control: 6463-11-001

Highway: IH 35

GENERAL NOTES:

The contract becomes effective upon receipt of the work authorization letter and covers one (1) year. Provide sufficient staff to concurrently pursue each contract in the event that additional mowing contracts are awarded to the same contractor.

Contractor questions on this project are to be addressed to the following individual(s):

Vanessa Rosales-Herrera, PE – Vanessa.Rosales@txdot.gov

Questions may be submitted via the Letting Pre-bid Q&A web page. This webpage can be accessed from the Notice to Contractors dashboard located at the following address:

<https://tableau.txdot.gov/views/ProjectInformationDashboard/NoticetoContractors>

All contractor questions will be reviewed by the Engineer. All questions and any corresponding responses that are generated will be posted through the same Letting Pre-Bid Q&A web page.

The Letting Pre-Bid Q&A webpage for each project can be accessed by using the dashboard to navigate to the project you are interested in by scrolling or filtering the dashboard using the controls on the left. Hover over the blue hyperlink for the project you want to view the Q&A for and click on the link in the window that pops up.

Confine all operations to daylight hours with no work performed on Sundays or State-observed holidays, unless otherwise authorized by the Engineer.

The approximate quantities determined for this project are for information only and are not to be considered as actual quantities. Contractors are hereby instructed to assure themselves of the actual conditions of the work area before bidding.

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SUPERVISION:

The engineer's representative in charge of the inspection of all work in that county is the maintenance supervisor listed below.

For this project the roadway maintenance supervisor in charge is:

Webb County
Jose Magana
1817 Bob Bullock Lp.
Laredo, Texas 78043
956.712.7713

When arrow boards are required, provide a standby unit in good working condition at the jobsite ready for immediate use.

Placement & removal of all traffic control devices will be done within the working hours listed.

HAY BAILING:

Hay bailing shall not be permitted.

WORK PROSECUTION:

Written notification will be given to begin mowing operations via work order. The work order will consist of the applicable mowing cycle, the number of working days allowed to complete the cycle, & date when the time charges for said cycle will start. Contractor will commence mowing within 10 business days of notification. Fall cut(s) should be performed between October 15 through December 20, weather permitting. Summer cut(s) should begin no sooner than May 20. Contractor request for variation(s) from these time frames will need prior approval from the Engineer. The number of working days allowed to complete the mowing cycle will be determined by the Engineer based upon the minimum amount of mowing acreage required per working day. For this contract, the mowing acreage required per normal working days is 125 acres of Strip Mowing, 125 acres of Modified Full Width Mowing, & 125 acres of Full Width Mowing. Except for Spot Mowing, additional acres may be required to be mowed while the Contractor is mowing. Additional working days for these added acres will be determined & allowed by the Engineer based on the minimum daily mowing acreage required per applicable cycle.

Charge liquidated damages as per "Schedule of Liquidated Damages" if work is not completed in the work order specified timeframe.

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ITEM 7 LEGAL RELATIONS AND RESPONSIBILITIES:

Roadway closures during the following key dates and/or special events are prohibited: January 1, the last Monday in May, July 4, the first Monday in September, the fourth Thursday in November, and December 24 or 25.

ITEM 502 BARRICADES, SIGNS AND TRAFFIC HANDLING:

Provide traffic control in accordance with plan standards and specifications & the Texas Manual on Uniform Traffic Control Devices.

Traffic control shall not be paid for directly, but shall be subsidiary to pertinent bid items.

Signs will have two safety flags attached to it at all times. It will not be permissible to hang or lean these signs on or against the State's sign posts, guardrails, bridge rail, etc. "Mowers Ahead" sign is intended for use in advance of mowing operations on the progress to keep within 2 miles or less from the work area. All sign stands & safety flags will be provided by the contractor.

ITEM 730 ROADSIDE MOWING:

SCOPE OF WORK:

Refer to the 2014 Standard Specifications for additional information.

Trimming is required around all guardrails, signs, the face of retaining walls, bridge sloped riprap embankments & around all landscape plantings within the right of way regardless of location.

Strip mowing shall be a maximum of 15 feet wide along the edge of pavement, unpaved shoulders or end of drainage structures.

Full Width Mowing Tract 1 represents a Modified 30' wide mowing along the edge of the pavement or where vegetation commences.

The state has the option of any or all tracts strip mowed from the Right of Way line inward or from the edge of pavement outward.

Mowing is not allowed during the wildflower blooming/growing season.

Trim limbs/branches to ensure visibility of traffic signs. Work is subsidiary to mowing items. Contractor to document roadway location(s) & stockpile branches by Right-of-Way fence. Provide list to Maintenance Supervisor.

Project Number: RMC 6463-11-001

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Mowing for sight distance per standard is required and shall be considered subsidiary to pertinent bid items. See sheets STRIP-MOW-D-04 for divided highways and STRIP-MOW-ND-04 for non-divided highways and Standard-Mow-ND-04 & Strip-Mow-D-04 for more detailed information.

At the time of letting for this contract, there may be locations of existing Metal Beam Guard Fence (MBGF), concrete traffic barriers and/or other types of traffic barriers in the R.O.W. within the limits of this mowing contract. The contractor is responsible for traveling the highways within this contract to determine what type of mowing equipment will be necessary for mowing these narrow areas.

Conventional batwing mowers may not be suitable or acceptable for mowing along the narrow side of existing or new traffic barrier. Encroachment of equipment onto the paved shoulder or travel lane will not be permitted for mowing this narrow width area. Therefore, the contractor must utilize mowing equipment that will not encroach into or adversely affect traffic in the travel lanes. Encroaching onto the roadway will require mobile traffic control for a mobile operation in accordance with applicable standards & specifications.

There will be no adjustment to the unit bid prices on this contract for mowing along existing or new traffic barrier, MBGF, post & cable and/or any other type of traffic barrier & any additional or alternative equipment, additional labor, or other anticipated expenses necessary to complete the work required by this contract will be paid for at the unit price bid for the items in this contract. No additional payment will be made for mowing of these narrow width areas along existing or new traffic barrier.

Divided Highway:

Mow an area approximately 15 feet wide on each side at all ramps on each side of crossing roads within the limits of the highway right-of-way. An area approximately 15 feet wide adjacent to the inside shoulder of the through lanes in locations where the unpaved width of the median exceeds 84 feet, plus the areas for transition and safety mowing. An area approximately 15 feet wide adjacent to the outside shoulder of the through lanes and 15 feet wide adjacent to the inside shoulder of the frontage roads where the outer separations exceed 84 feet, plus the areas for transition and safety mowing.

Mow the entire median area where the width is 84 feet or less. Mow the entire outer separation where the width is 84 feet or less.

Mow an area of 15 feet wide along the outside shoulder of all frontage roads, plus the area for transition and safety mowing.

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Undivided Highway:

Mow an area approximately 15 feet wide on each side at all ramps on each side of crossing roads within the limits of the highway right-of-way. An area approximately 15 feet wide adjacent to the inside shoulder of the through lanes in locations where the unpaved width of the median exceeds 84 feet, plus the areas for transition and safety mowing. An area approximately 15 feet wide adjacent to the inside shoulder of the frontage roads where the outer separations exceed 84 feet, plus the areas for transition and safety mowing.

Mow an area of 15 feet wide along the outside shoulder of all frontage roads, plus the area for transition and safety mowing.

When performing Type I mowing (divided and undivided roadway) in front of private dwellings, churches, schools, and developed areas, the contractor will mow the entire right-of-way from the pavement edge to the right-of-way line, plus the areas for transition and safety mowing. The contractor will mow an area 15 feet wide past the ends of all pipes and culverts.

The rate of transition between the designated strip width and other areas is 6:1 parallel to the roadway for every one (1) foot increase or decrease in width.

Equipment:

Equipment must have four daytime fluorescent orange flags in good condition, 2 mounted on the rear of the equipment & 2 mounted on the front of the equipment. Provide highly visible omnidirectional amber flashing warning lights on tractors & work trucks. Ensure that all workers have personal equipment as per Item 7.2.1 "Safety".

ITEM 6185 TRUCK MOUNTED ATTENUATOR (TMA) AND TRAILER:

The total number of truck mounted attenuators (TMA) required when utilizing the traffic control standards are shown in the tables below.

TCP 3 Series	Scenario	Required TMA
(3-1)-13	AII	2
(3-2)-13	AII	3

The Contractor will be responsible for determining if one or more of these operations will be ongoing at the same time to determine the total number of TMA needed for the project for those times per plan requirements. Additional TMAs used that are not specified in the plans in which the Contractor expects compensation will require prior approval from Engineer.



Estimate & Quantity Sheet

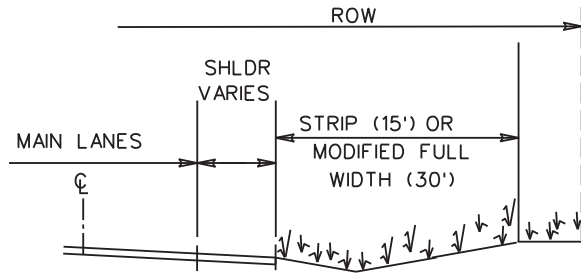
CONTROLLING PROJECT ID 6463-11-001

DISTRICT Laredo
HIGHWAY IH0035

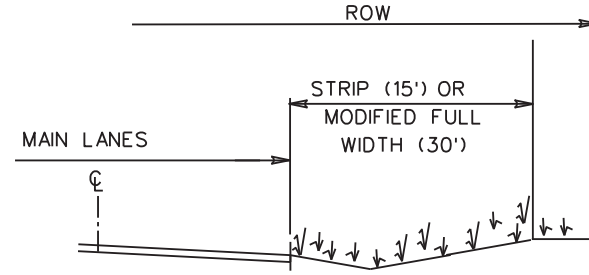
COUNTY Webb

CONTROL SECTION JOB				6463-11-001		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00207059			
COUNTY				Webb			
HIGHWAY				IH0035			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL		
	730-6001	STRIP MOWING	AC	936.200		936.200	
	730-6002	FULL - WIDTH MOWING	AC	2,042.000		2,042.000	
	730-6054	FULL - WIDTH MOWING - TRACT (1)	AC	3,621.600		3,621.600	
	6185-6005	TMA (MOBILE OPERATION)	DAY	12.000		12.000	

DISTRICT	COUNTY	CCSJ	SHEET
Laredo	Webb	6463-11-001	



TYPICAL SECTION A
SYMMETRICAL TO CENTER LINE

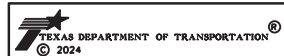


TYPICAL SECTION B
SYMMETRICAL TO CENTER LINE
(DRAG STRIP)

GENERAL NOTES

- A. MOW FROM EDGE OF PAVEMENT APPLIES TO BOTH STRIP AND MODIFIED FULL WIDTH MOWING.
- B. MOW FROM EDGE OF DRAG STRIP. DRAG STRIP IS BARREN GROUND (CALICHE, GRAVEL, OR UNVEGETATED GROUND).
- C. THE STATE HAS THE OPTION OF ANY OR ALL TRACTS STRIP MOWED FROM THE THE RIGHT-OF-WAY LINE INWARD OR FROM THE EDGE OF PAVEMENT OUTWARD.

1/18/2024 esern3 T:\LRD05\TMT\FY 2024\MNT Contract (F24)\RMC\MOWING\Webb (23 to 24)\CAD\Typ Sec.dgn



TYPICAL SECTIONS
MOWING


DR	A.S.	DR	A.S.	STATE	SHEET NUMBER	SHEET NO.	
DR	V.R.	DR	V.R.	TEXAS	SHEET 1 OF 1	6	
PROJECT NO.	22	COUNTY	WEBB	SECTION	11	JOB NO.	001
DATE	11/30/24	SECTION	11	JOB	001	ROADWAY NO.	35, Etc.

WEBB COUNTY							ITEM 730-6001 TYPE I			ITEM 730-6002 TYPE II			ITEM 730-6054 TYPE III		
HIGHWAY	TRACT NO.	LIMITS	FROM RMN	TO RMN	LENGTH (Miles)	CYCLES	STRIP MOWING			FULL WIDTH MOWING			FULL WIDTH MOWING-TRACT1		
							QTY	AC	TOTAL	QTY	AC	TOTAL	QTY	AC	TOTAL
IH 35*	1	FROM U. PACIFIC RR UNDERPASS N TO MP 14	7	14	7.2	2	0	78.2	0.0	2	104.3	208.6	0	104.3	0.0
IH 35*	2	FROM MP14 N TO MP18	14	18	4.0	2	0	77.6	0.0	2	85.3	170.7	0	82.4	0.0
IH 35*	3	FROM MP18 N TO MP22	18	22	4.0	2	0	67.9	0.0	2	85.3	170.7	0	82.4	0.0
IH 35*	4	FROM MP22 N TO MP26	22	26	4.0	2	0	67.9	0.0	2	85.3	170.7	0	82.4	0.0
IH 35*	5	FROM MP26 N TO MP30	26	30	4.0	2	0	67.9	0.0	2	85.3	170.7	0	82.4	0.0
IH 35*	6	FROM MP30 N TO MP34	30	34	4.0	2	0	67.9	0.0	2	85.3	170.7	0	82.4	0.0
IH 35*	7	FROM MP34 N TO LASALLE/WEBB CO. LINE	34	39	4.3	2	0	72.8	0.0	2	91.5	183.0	0	88.4	0.0
US 59	8	FROM FM2895 SW TO RMN806	797	806	8.2	3	1	29.9	29.9	0	106.1	0.0	2	59.9	119.7
US 59	9	FROM RMN806 SW TO RMN814	806	814	8.0	3	1	29.1	29.1	0	102.5	0.0	2	58.2	116.4
US 59	10	FROM RMN814 SW TO RMN820	814	820	6.0	3	1	21.8	21.8	0	74.3	0.0	2	43.6	87.3
US 59	11	FROM RMN820 SW TO RMN824	820	824	4.0	3	1	14.5	14.5	0	51.5	0.0	2	29.1	58.2
US 59	12	FROM WEBB/DUVAL CO. LINE TO RMN790	782	790	8.0	3	1	29.1	29.1	0	102.8	0.0	2	58.2	116.4
US 59	13	FROM RMN790 SW TO FM2895	790	798	7.8	3	1	28.3	28.3	0	97.4	0.0	2	56.5	113.0
US 83 N	14	FROM WEBB/DIMMIT CO. LINE S TO RMN680	666	682	16.0	3	1	58.2	58.2	0	150.8	0.0	2	116.4	232.7
US 83 N	15	FROM RMN680 S TO IH35	682	699	16.9	3	1	61.4	61.4	0	89.2	0.0	2	89.2	178.4
US 83 S*	16	(REST AREA) 9 MILES SOUTH OF LAREDO	724	724	0.1	2	0	0.5	0.0	2	5.6	11.2	0	1.1	0.0
US 83 S*	17	FROM LCC CAMPUS TO 1 MILE S OF RIO BRAVO ENTRANCE	724	730	6.0	2	0	65.5	0.0	2	152.2	304.5	0	94.5	0.0
US 83 S*	18	FROM RMN30 TO WEBB/ZAPATA CO. LINE	730	736	4.9	2	0	65.5	0.0	2	116.4	232.7	0	87.3	0.0
SH 44	19	FROM US83 E TO LASALLE/WEBB CO. LINE	428	438	9.6	3	1	35.0	35.0	0	102.7	0.0	2	70.0	140.1
SH 359	20	FROM END OF 5-LANE RD TO RMN444	438	444	6.0	3	1	21.8	21.8	0	70.1	0.0	2	43.6	87.3
SH 359	21	FROM RMN444 E TO FM2895	444	457	12.8	3	1	46.6	46.6	0	102.8	0.0	2	93.2	186.5
SH 359	22	FROM FM2895 E TO WEBB/DUVAL CO. LINE	457	476	19.2	3	1	69.9	69.9	0	124.6	0.0	2	126.6	253.2
SH 359	23	FROM WEBB/DUVAL CO. LINE SE TO DUVAL/JIM HOGG CO. LIN	476	480	4.0	3	1	14.5	14.5	0	36.7	0.0	2	28.9	57.9
FM 1472 ^A	24	FROM 3.30 MI NW OF IH35 TO 17.50 MI W	424	438	14.0	3	1	101.8	101.8	0	407.3	0.0	2	208.8	417.6
FM 1472	25	FROM 17.50 MI W OF IH35 W TO END OF PAVEMENT	408	424	15.9	3	1	57.7	57.7	0	173.2	0.0	2	115.5	231.0
FM 3338	26	FROM SH255 TO FM1472	616	624	7.8	3	1	28.5	28.5	0	68.7	0.0	2	56.9	113.9
FM 255	27	FROM FM1472 TO GSA COMPLEX	620	621	1.1	3	1	4.0	4.0	0	20.9	0.0	2	8.0	16.0
FM 255T	28	FROM INT OF IH35 TO INT OF FM1472	417	439	22.0	3	1	80.0	80.0	0	846.1	0.0	2	160.0	320.0
FM 649	29	FROM SH359 S TO JIM HOGG CO. LINE	636	650	13.7	3	1	49.8	49.8	0	79.6	0.0	2	79.6	159.2
FM 2895	30	FROM US59 S TO SH359	620	638	17.4	3	1	63.4	63.4	0	156.7	0.0	2	126.8	253.7
FM 2050	31	FROM US59 TO RMN626	614	626	12.0	3	1	43.6	43.6	0	99.8	0.0	2	87.3	174.5
FM 2050	32	FROM RMN626 TO SH359	626	639	12.6	3	1	45.7	45.7	0	107.4	0.0	2	91.4	182.8
SL 20	33	FROM SH359 INT. (SOUTH OF BRIDGE) TO SIERRA VISTA INT.	432	436	3.8	2	0	13.6	0.0	2	45.5	90.9	0	27.3	0.0
SL 20	34	FROM SIERRA VISTA INT. TO MANGANA HEIN RD	436	439	3.3	2	0	12.0	0.0	2	78.9	157.7	0	24.0	0.0
SS 259	35	FROM INT SH359 TO SL20 (EXIT RAMPS)	638	638.5	0.4	3	1	1.5	1.5	0	4.5	0.0	2	3.0	6.0
TOTAL					293			936.2		2,042.0		3,621.6			

Δ ACREAGE FOR CROSSVER AT FM 1472/ FM 3338 BETWEEN RM 432-434-0.04 IS 5.28 AC. WHICH IS INCLUDED ON QUANTITY SPECIFIED FOR FM 1472.

NOTES:

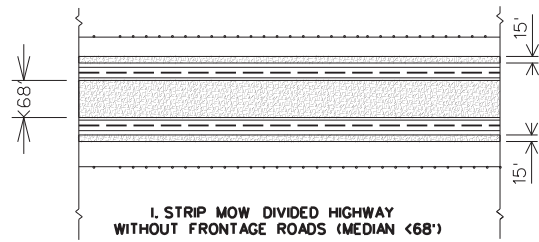
- "MOWERS AHEAD" SIGNS ARE INTENDED FOR USE IN ADVANCE OF MOWING OPERATIONS ON THE RIGHT-OF-WAY.
- THE WARNING SIGNS WILL BE ADJUSTED AS THE WORK PROGRESSES TO KEEP WITHIN TWO (2) MILES OR LESS FROM THE WORK AREA.
- STRIP MOWING FOR DIVIDED HIGHWAYS SHALL BE IN ACCORDANCE WITH STANDARD STRIP-MOW-D-04. STRIP MOWING FOR UNDIVIDED HIGHWAYS SHALL BE IN ACCORDANCE WITH STANDARD STRIP-MOW-ND-04, WHERE "STRIP MOWING WIDTH"(Ws) IS 15 FEET.
- THE STATE HAS THE OPTION OF ANY OR ALL TRACTS STRIP MOWED FROM THE RIGHT OF WAY LINE INWARD OR FROM THE EDGE OF PAVEMENT OUTWARD.
- MOWING IS NOT ALLOWED DURING THE WILDFLOWER BLOOMING/GROWING SEASON.
- FULL WIDTH MOWING TRACT 1 REPRESENTS A MODIFIED 30 FOOT WIDTH MOWING STRIP ALONG THE EDGE OF THE PAVEMENT, OR WHERE VEGETATION COMMENCES. REFER TO TYPICAL SECTIONS MOWING PLAN SET.
- TRIM LIMBS/BRANCHES THAT BLOCK TRAFFIC SIGNS. WORK IS SUBSIDIARY TO MOWING ITEMS. CONTRACTOR TO DOCUMENT ROADWAY LOCATION(S) AND STOCKPILE BRANCHES BY RIGHT-OF-WAY FENCE. PROVIDE LIST TO MAINTENANCE SUPERVISOR.
- MOWING FOR SIGHT DISTANCE PER STANDARD REQUIRED. CONSIDER WORK SUBSIDIARY TO PERTINENT BID ITEMS.



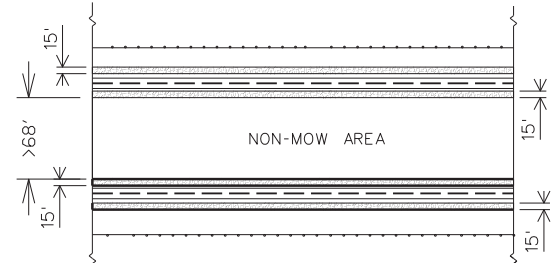
**SUMMARY OF TRACTS
AND QUANTITIES**

DATE: 1/18/2024	BY: esern3	PROJECT: T-1LRD05TMTXVY 2024 MNT Contract (F24) RMC MOWING-Webb (23 to 24) CAD Sum of Tris and Quantities.dgn	STATE: TEXAS	SHEET NUMBER: SHEET 1 OF 1	SHEET NO: 7
DR: V.R.	DE: V.R.	COUNTY: WEBB	CONTROL: 6463	SECTION: 11	JOB: 001
ISSUE DATE: 22	ISSUE NO: 22	ISSUE BY: WEBB	ISSUE NO: 6463	ISSUE DATE: 11	ISSUE NO: 001

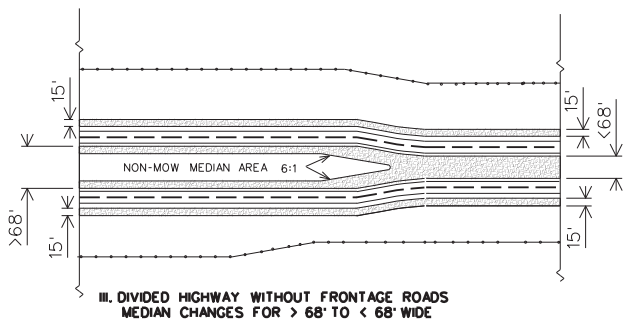
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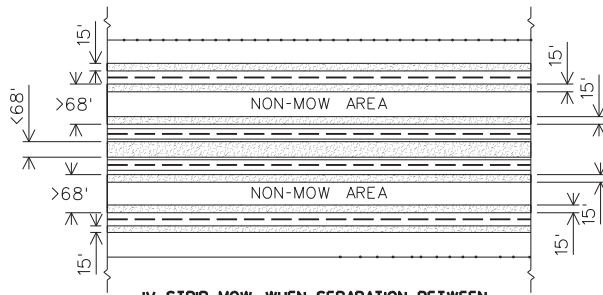
I. STRIP MOW DIVIDED HIGHWAY WITHOUT FRONTAGE ROADS (MEDIAN <68')



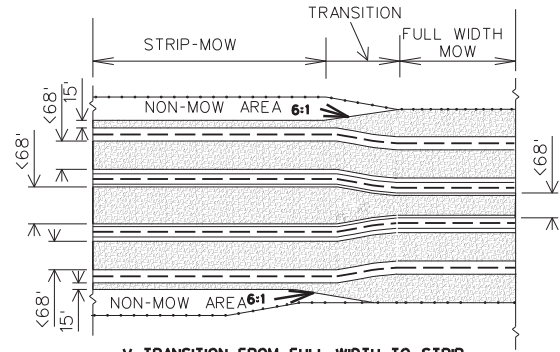
II. STRIP MOW DIVIDED HIGHWAY WITHOUT FRONTAGE ROADS (MEDIAN >68')



III. DIVIDED HIGHWAY WITHOUT FRONTAGE ROADS MEDIAN CHANGES FOR > 68' TO < 68' WIDE



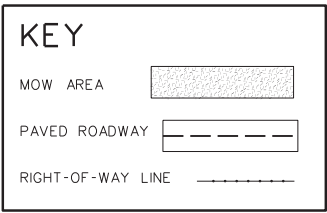
IV. STRIP MOW WHEN SEPARATION BETWEEN MAIN LANE AND FRONTAGE ROADS IS WIDER THAN 68'



V. TRANSITION FROM FULL WIDTH TO STRIP MOWING DIVIDED HIGHWAY WITH FRONTAGE ROADS (MEDIAN <68' WIDE)

GENERAL NOTES:

1. MOW THE ENTIRE WIDTH OF MEDIANS AND OUTER SEPARATIONS (AREAS BETWEEN MAIN LANES, RAMPS, AND FRONTAGE ROAD) EXCEPT FOR NON-MOW AREAS.
2. MOW FULL-WIDTH ALL MEDIANS AND OUTER SEPARATIONS 68' OR LESS FROM PAVEMENT EDGE TO PAVEMENT EDGE.
3. FOR MEDIANS AND OUTER SEPARATIONS GREATER THAN 68' MOW A 15' ALONG EACH PAVEMENT EDGE.
4. NON-MOW AREAS IN MEDIANS & OUTER SEPARATIONS WILL BE CONSIDERED THE AREA IN MEDIANS AND OUTER SEPARATIONS GREATER THAN 68' BETWEEN THE 15' STRIP MOW AREAS.
5. OTHER NON-MOW AREA'S WILL BE SHOWN ELSEWHERE ON PLANS OR MARKED ON THE RIGHT OF WAY.



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	00
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Texas Department of Transportation
 Maintenance Division
 Standard Plans

STRIP MOWING
 (DIVIDED HIGHWAYS)
 STRIP-MOW-D-04

FILE: SMOWD04.DGN

DN:

CK:

DW:

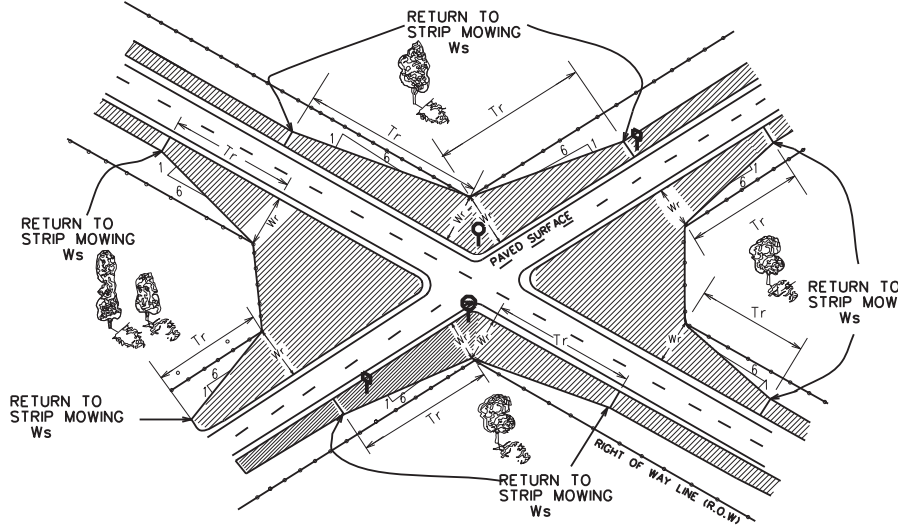
CK-:

NEG NO.:

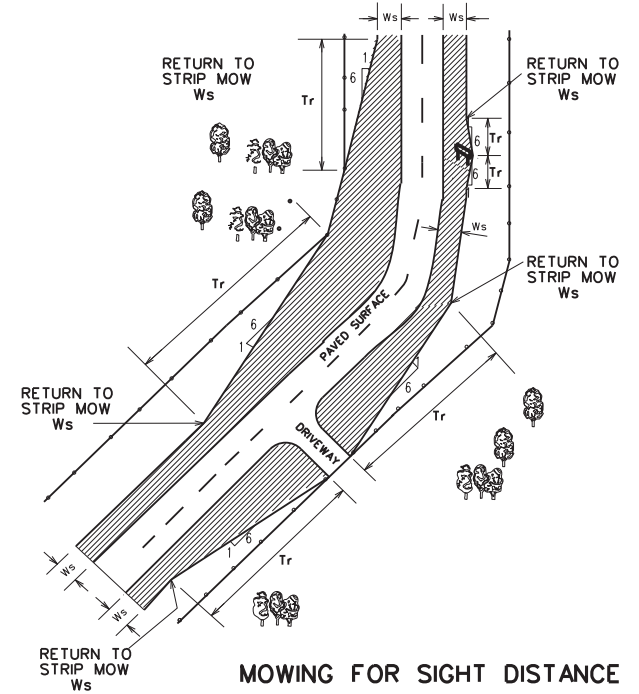
© TxDOT JUNE 2004

STATE DISTRICT	FEDERAL REGION	FEDERAL AD PROJECT	SHEET
22	6	RMC-6463-11 - 001	8
REVISED: 6/03/2004	COUNTY	CONTROL SECTION	JOB HIGHWAY
	WEBB	6463 11	001 IH 35, Etc.

SHEET 1 OF 1 NOT TO SCALE



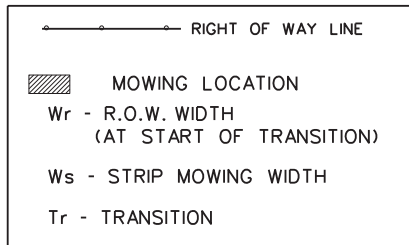
MOWING FOR SIGHT DISTANCE
WITH TRANSITION FROM INTERSECTION
BACK TO STRIP MOWING



MOWING FOR SIGHT DISTANCE
TRANSITIONS AT DRIVEWAYS,
SIGNS, AND CURVES

GENERAL NOTES:

1. THE NORMAL WIDTH FOR STRIP MOWING IS 15' UNLESS OTHERWISE SHOWN ON THE PLANS.
2. MOW TO THE R.O.W. LINE IN FRONT OF BUSINESSES, RESIDENCES, CHURCHES, OR CULTIVATED FIELDS UNLESS OTHERWISE SHOWN ON THE PLANS.
3. TRANSITION FOR SIGHT DISTANCE TO R.O.W LINE OR AROUND SIGNS AS SHOWN ON THIS SHEET UNLESS OTHERWISE SHOWN ON THE PLANS.



 *Texas Department of Transportation*
Maintenance Division
Standard Plans

STRIP MOWING NON-DIVIDED HIGHWAYS

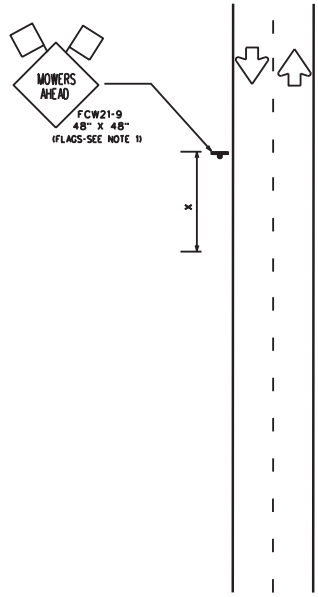
STRIP-MOW-ND-04

SHEET 1 OF 1 NOT TO SCALE

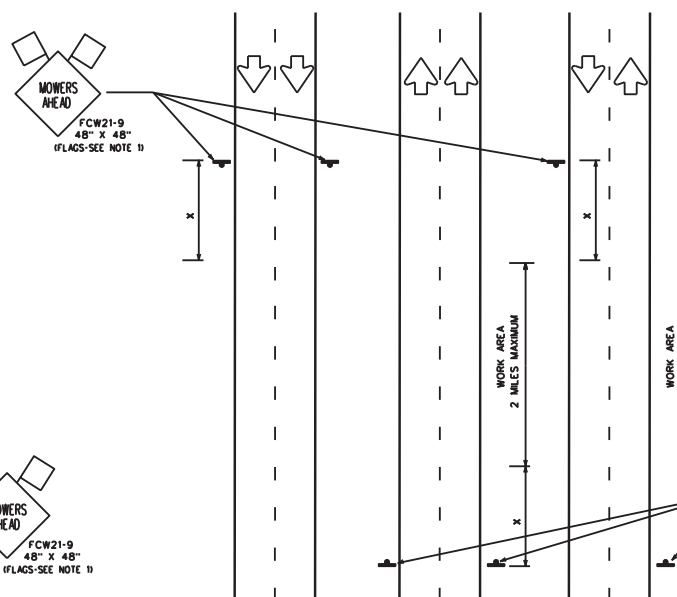
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©	TxDOT 2004	STATE DISTRICT	FEDERAL REGION	FEDERAL AID PROJECT	SHEET	
REVISED:	5/18/2004	22	6	RMC:6463-11-001	9	
REVISED:	COUNTY			CONTROL SECTION	JOB	HIGHWAY
REVISED:	WEBB			6463	11	001 IH 35, Etc.

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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32
33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48



TCP (LRD-MWGa)
Two-Lane Highway



TCP (LRD-MWGb)
Multi-Lane Divided Highway

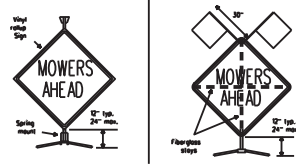
LEGEND

□ Flag
 Sign Post

Posted Speed	Minimum Sign Spacing x Distance
30	120'
35	160'
40	240'
45	320'
50	400'
55	500'
60	600'
65	700'
70	800'

GENERAL NOTES:

- Flags attached to signs are REQUIRED.
- All traffic control devices illustrated are REQUIRED.
- The warning signs shall be adjusted as the work progresses to keep at least two (2) miles or less from the work area.
- All signs shall be mounted in portable stands. It is not permissible to hang or lean these signs on or against the State's sign post, guardrail, bridge rail, etc.



Short Term Stationary/ Short Duration
Portable Sign Supports
1 Foot Mounting Height
(SEE NOTE 4)

TEXAS DEPARTMENT OF TRANSPORTATION
 © 2024

**TRAFFIC CONTROL PLAN
MOWING RIGHT-OF-WAY**

DN: A.S.	DR: A.S.	STATE: TEXAS	SHEET NUMBER: SHEET 1 OF 1	SHEET NO:
DD: V.R.	DC: V.R.	COUNTY: WEBB	SECTION: 11	JOB: 001
DD: 22	DD: 22	DD: 22	DD: 22	DD: 22

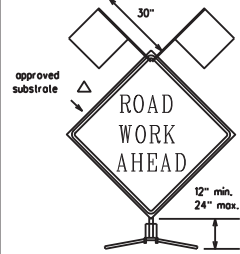
I:\18\2024 esern3 T:\LRD05T\MNT\FY 2024\MNT Contract (FY24)\RMC\MOWING\Webb (23 to 24)\CAD\Standards\MOWROW.dgn

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LEVELS DISPLAYED

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32
33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48
49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	

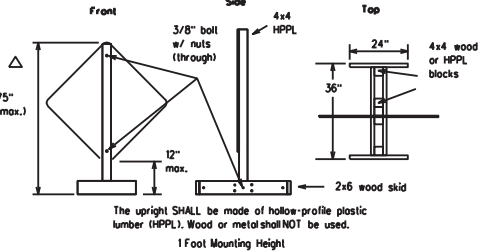
See the CWZTCO for the type of sign substrate that can be used for each approved sign support.



Flags as required by Engineer or as shown on plans

EXAMPLES OF SIGN SUPPORTS

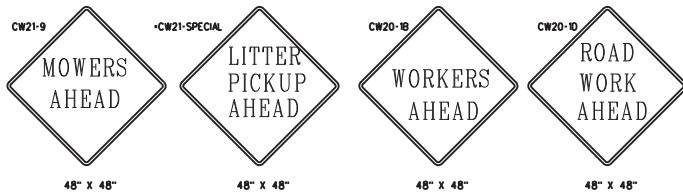
SHORT TERM DURATION, DAYTIME USE ONLY PORTABLE SIGN SUPPORTS



Attachment to wooden supports will be by bolts and nuts or screws. Use TxDOT's or manufacturer's recommended procedures for attaching sign substrates to other types of sign supports.

Nails will NOT be allowed.

The upright SHALL be made of hollow-profile plastic lumber (HPPL). Wood or metal shall NOT be used.
1 Foot Mounting Height



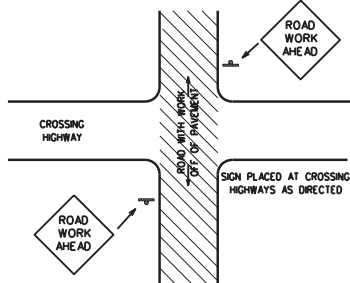
SIGN IN ACCORDANCE WITH THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS

MOWERS AHEAD SIGNS ARE USED FOR MOWING OPERATIONS.

LITTER PICKUP AHEAD, ROAD WORK AHEAD AND WORKER AHEAD SIGNS ARE USED AS DIRECTED FOR OTHER MAINTENANCE OPERATIONS WHEN ALL WORK OCCURS OFF OF THE PAVED HIGHWAY SURFACE.

ROLL-UP SIGNS CONFORMING TO DMS-8310 AND THE CWZTCO ALLOWED

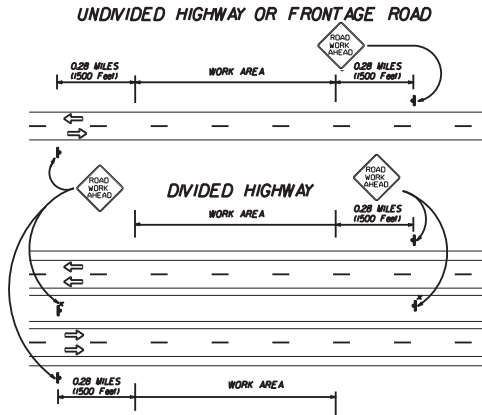
*Letter dimensions and spacing for "CW21-SPECIAL" is the same as C20-1D



TYPICAL LOCATION OF SIGNS AT HIGHWAY CROSSING

WORK AREA IS A MAXIMUM OF 2.0 MILES UNLESS OTHERWISE DIRECTED. SIGNS MAY REMAIN IN PLACE ONLY DURING DAYLIGHT HOURS. SIGNS ARE TO BE PLACED 6 TO 12 FEET OFF OF THE PAVED SURFACE UNLESS OTHERWISE DIRECTED. ROAD WORK AHEAD SIGNS SHOWN AS EXAMPLES, ONE OF THE FOUR TYPE SIGNS WILL BE USED AS DIRECTED.

* SIGNS IN THE MEDIAN ARE REQUIRED WHEN WORK OCCURS IN MEDIAN



TRAFFIC CONTROL PLAN FOR WORK OFF OF THE PAVED SURFACE.

GENERAL NOTES FOR WORK ZONE SIGNS

- Contractor shall install and maintain signs in a straight and plumb condition and/or as directed by the Engineer.
 - Wooden sign posts shall be painted white.
 - Barricades shall NOT be used as sign supports.
 - Nails shall NOT be used to attach signs to any support.
 - All signs shall be installed in accordance with the plans or as directed by the Engineer. Signs shall be used to regulate, warn, and guide the traveling public safely through the work zone.
 - The Contractor may furnish either the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD). The Engineer/Inspector may require the Contractor to furnish other work zone signs that are shown in the TMUTCD but may have been omitted from the plans. Any variation in the plans shall be documented by written agreement between the Engineer and the Contractor's Responsible Person. All changes must be documented in writing before being implemented. This can include documenting the changes in the Inspector's TxDOT diary and having both the Inspector and Contractor initial and date the agreed upon changes. The additional signs requested by the Engineer/Inspector shall not be subsidiary.
 - The Contractor shall furnish sign supports listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCO). The Contractor shall install the sign support in accordance with the manufacturer's recommendations, if there is a question regarding installation procedures, the Contractor shall furnish the Engineer a copy of the manufacturer's installation recommendations so that the Engineer can verify the correct procedures are being followed.
 - The Contractor is responsible for sign installations and replacing signs with damaged or cracked substrates and/or damaged or marred reflective sheeting as directed by the Engineer/Inspector.
 - Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1".
 - The Contractor shall replace damaged wood posts. New or damaged wood sign posts shall not be spliced.
- Duration of Work (as defined by the "Texas Manual on Uniform Traffic Control Devices" Part VI)
- The Contractor is responsible for ensuring the sign support and substrate meets crashworthiness. For mowing operation all signs and supports are Short-term Duration for daytime work.
 - The Contractor shall furnish the sign sizes shown on this sheet or as directed by the Engineer.

SIGN SUBSTRATES

- The Contractor shall ensure that the sign substrate is allowed for the type of sign support that is being used. The CWZTCO lists each substrate that can be used on the different types and models of sign supports.
- "Mesh" type materials are NOT an approved sign substrate.
- All wooden individual sign panels fabricated from 2 or more pieces shall have one or more plywood cleat, 1/2" thick by 6" wide, fastened to the back of the sign and extending fully across the sign. The cleat shall be attached to the back of the sign using wood screws that do not penetrate the face of the sign panel. The screws shall be placed on both sides of the splice and spaced at 6" centers. The Engineer may approve other methods of splicing the sign faces.

REFLECTIVE SHEETING

- Reflectorized signs shall be constructed of sheeting meeting the color and retro-reflectivity requirements of DMS-8300 or DMS-8310. The DMS specifications can be accessed from the following web address:
http://manuals.dot.state.tx.us/80/dynweb/colnotes/#Generic_Collection/Venics/default
 - White sheeting, meeting the requirements of DMS-8300 Type C (High Specific Intensity), shall be used for signs with white background and chevron devices.
 - Orange sheeting, meeting the requirements of DMS-8300 Type E (Fluorescent Prismatic), shall be used for signs with orange backgrounds.
- #### SIGN LETTERS
- All sign letters and numbers shall be clear, and open rounded type uppercase alphabet letters as approved by the Federal Highway Administration (FHWA) and as published in the "Standard Highway Sign Design for Texas" manual. Signs, letters and numbers shall be of first class workmanship in accordance with Department Standards and Specifications.

REMOVING OR COVERING

- Signs should be removed or completely covered when not mowing.
- Duct tape or other adhesive material shall NOT be affixed to a sign face.
- Signs and supports shall be removed by the end of the day.

SIGN SUPPORT WEIGHTS

- Where sign supports require the use of weights to keep from turning over, the use of sandbags with dry cohesionless sand is recommended.
- The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
- Rock, concrete, iron, steel or other solid objects will not be permitted for use as sign support weights.
- Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
- Sandbags shall be made of a durable material that tears upon vehicular impact.
- Rubber (such as tire inner tubes) shall NOT be used for sandbags.
- Rubber ballasts (such as those used with cones or edge-line channelizers) shall NOT be used as sign support weights.
- Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign supports.
- Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

CONTRACTOR REQUIREMENTS FOR MAINTAINING PERMANENT SIGNS WITHIN THE PROJECT LIMITS

Any sign, sign support or traffic control device that is struck or damaged by the Contractor or his/her construction equipment shall be replaced or repaired as soon as possible by the Contractor at the Contractor's expense.

Only pre-qualified products shall be used. A copy of the "Compliant Work Zone Traffic Control Device List" (CWZTCO) describes pre-qualified products and their sources and may be obtained by contacting:

Standards Engineer
 Traffic Operations Division - TE
 Texas Department of Transportation
 125 East 11th Street
 Austin, Texas 78701-2483
 Phone (512) 416-3100
 Fax (512) 416-3299

Instructions to locate the "CWZTCO" on TxDOT website are:

Start at website - www.dot.state.tx.us
 Click on "About TxDOT".
 Click on "Organizational Chart".
 Click on Traffic Operations Box.
 Click on "Compliant Work Zone Traffic Control Devices".
 Click on "View PDF".
 This site is printable.

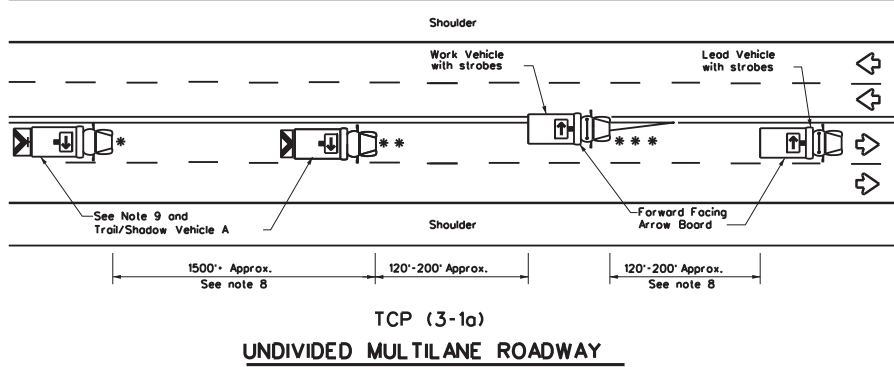
Texas Department of Transportation
 Maintenance Division
 Standard Plans

ROADSIDE
 TRAFFIC CONTROL PLAN

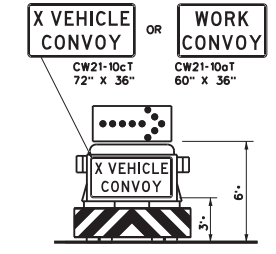
SHEET 1 OF 1 RS-TCP-05 NOT TO SCALE

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REVISED:	SEPTEMBER 17, 2004	DATE:	22	STATE:	TX	FEDERAL AID PROJECT:	RMC:6463-11-001	SHEET:	11
REVISED:	FEBRUARY 2, 2005	DATE:		COUNTY:	WEBB	CONTROL SECTION:	6463	JOB:	11
REVISED:		DATE:		COUNTY:		CONTROL SECTION:		JOB:	H 35, ETC.

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TCP (3-1a)
UNDIVIDED MULTILANE ROADWAY



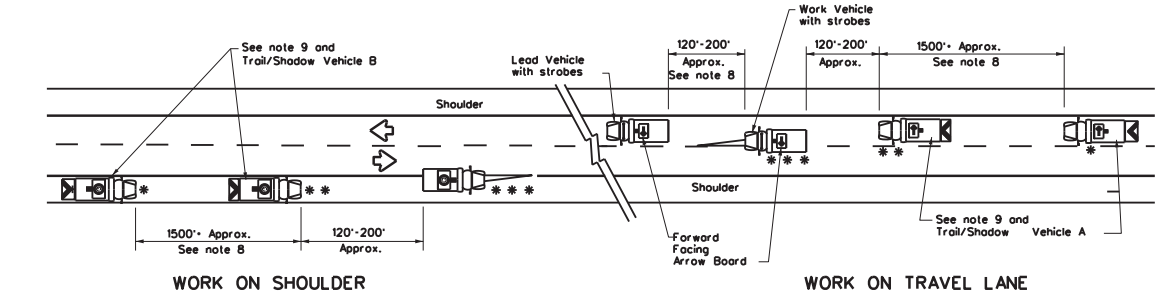
TRAIL/SHADOW VEHICLE A
with RIGHT Directional display Flashing Arrow Board

LEGEND		ARROW BOARD DISPLAY	
*	Trail Vehicle		
**	Shadow Vehicle		
***	Work Vehicle		RIGHT Directional
	Heavy Work Vehicle		LEFT Directional
	Truck Mounted Attenuator (TMA)		Double Arrow
	Traffic Flow		CAUTION (Alternating Diamond or 4 Corner Flash)

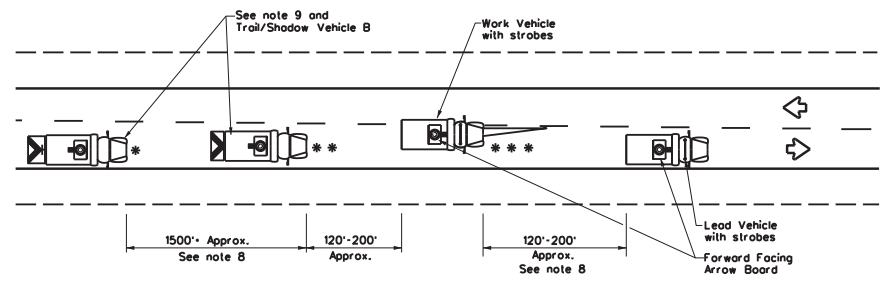
TYPICAL USAGE			
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	LONG TERM STATIONARY
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

GENERAL NOTES

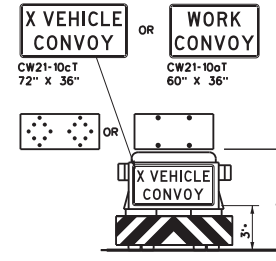
1. TRAIL, SHADOW, and LEAD vehicles shall be equipped with arrow boards as illustrated. When a LEAD vehicle is not used the WORK vehicle must be equipped with an arrow board. The Engineer will determine if the LEAD VEHICLE and/or TRAIL VEHICLE are required based on prevailing roadway conditions, traffic volume, and sight distance restrictions.
2. The use of amber high intensity rotating, flashing, oscillating, or strobe lights on vehicles are required. Blue high intensity rotating, flashing, oscillating or strobe lights when mounted on the driver's side of the vehicle may be operated simultaneously with the amber beacons or strobe lights.
3. The use of truck mounted attenuators (TMA) on the SHADOW VEHICLE and TRAIL VEHICLE are required.
4. Reflective sheeting on the rear of the TMA shall meet or exceed the reflectivity and color requirements of DEPARTMENTAL MATERIAL SPECIFICATION DMS 8300, Type A.
5. Flashing arrow boards shall be Type B or Type C as per the Barricade and Construction (BC) standards. The board shall be controlled from inside the vehicle.
6. Each vehicle shall have two-way radio communication capability.
7. When work convoys must change lanes, the TRAIL VEHICLE should change lanes first to shadow the other convoy vehicles.
8. Vehicle spacing between the TRAIL VEHICLE and the SHADOW VEHICLE will vary depending on sight distance restrictions. Motorists approaching the work convoy should be able to see the TRAIL VEHICLE in time to slow down and/or change lanes as they approach the TRAIL VEHICLE. Vehicle spacing between the WORK VEHICLE and SHADOW VEHICLE and vehicle spacing between WORK VEHICLE and LEAD VEHICLE may vary according to terrain, work activity and other factors.
9. "X VEHICLE CONVOY" (CW21-10cT) or "WORK CONVOY" (CW21-10aT) signs shall be used on TRAIL VEHICLES and SHADOW VEHICLES as shown. As an option 48" X 48" diamond shaped "WORK CONVOY" (CW21-10T1) or "X VEHICLE CONVOY" (CW21-10bT) signs may be used where adequate mounting space exists. When used, the X VEHICLE CONVOY sign shall have the number of the convoy vehicles displayed on the sign in the number designation "X" location. The "X VEHICLE CONVOY" sign shall not be used on the SHADOW VEHICLE if a TRAIL VEHICLE is used.
10. On two-lane two-way roadways, the work and protection vehicles should pull over periodically to allow motor vehicle traffic to pass. If motorists are not allowed to pass the work convoy, a "DO NOT PASS" (R4-1) sign should be placed on the back of the rearmost protection vehicle.



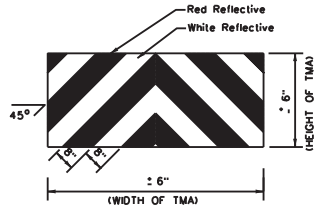
TCP (3-1b)
TWO-WAY ROADWAY WITH PAVED SHOULDERS



TCP (3-1c)
TWO-WAY ROADWAY WITHOUT PAVED SHOULDERS



TRAIL/SHADOW VEHICLE B
with Flashing Arrow Board in CAUTION display



STRIPING FOR TMA



TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
UNDIVIDED HIGHWAYS

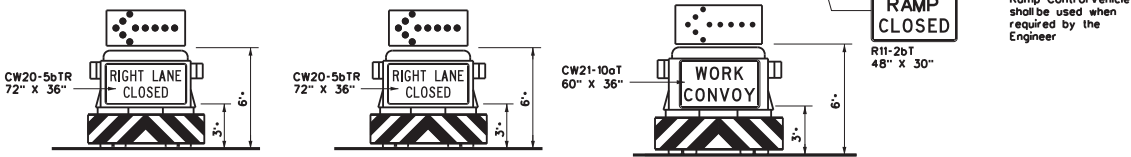
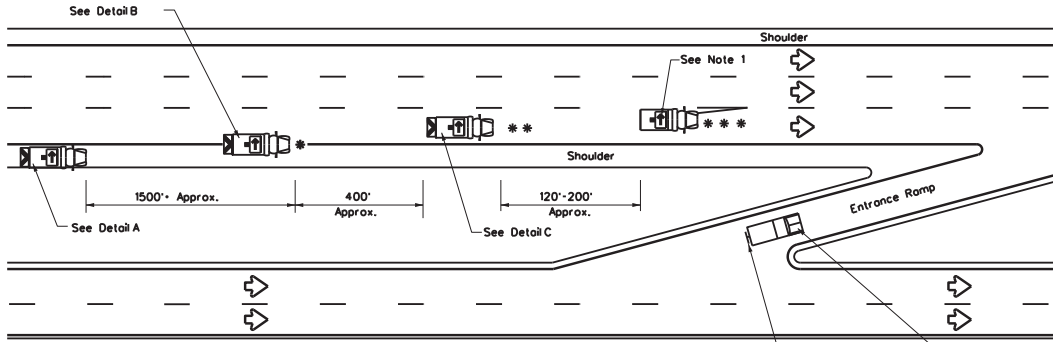
TCP(3-1)-13

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© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
2-94 4-98 8-95 7-13 1-97	6463	11	001	Rt 35, Etc.
	DIST	COUNTY		SHEET NO.
	22	WEBB		12

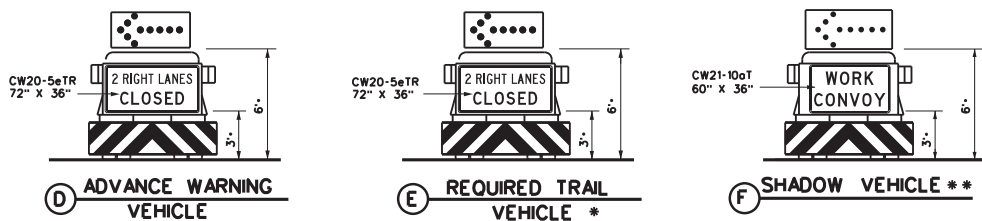
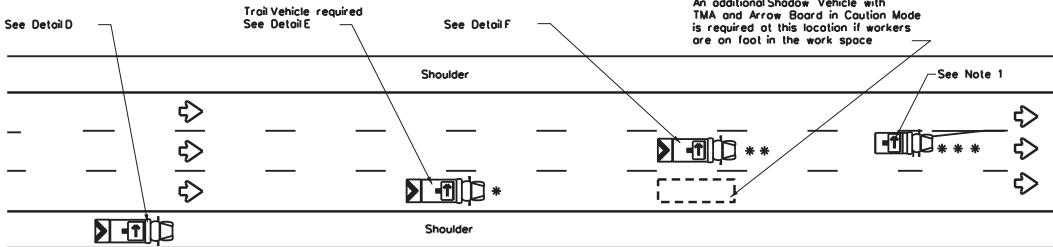
DATE: FILE:

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DATE: FILE:



RIGHT LANE CLOSURE ON DIVIDED HIGHWAY - TCP(3-2a)



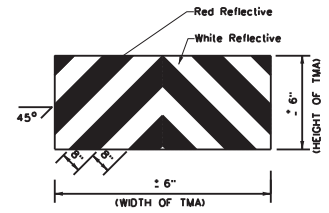
INTERIOR LANE CLOSURE ON MULTI-LANE DIVIDED HIGHWAY - TCP(3-2b)

LEGEND	
* Trail Vehicle	ARROW BOARD DISPLAY
** Shadow Vehicle	
*** Work Vehicle	RIGHT Directional
Heavy Work Vehicle	LEFT Directional
Truck Mounted Attenuator (TMA)	Double Arrow
Traffic Flow	CAUTION (Alternating Diamond or 4 Corner Flash)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
✓				

GENERAL NOTES

- ADVANCE WARNING, TRAIL and SHADOW vehicles shall be equipped with Type B or Type C flashing arrow boards as per the Barricade and Construction (BC) standards. Arrow boards on WORK vehicles will be optional based on the type of work being performed. The arrow boards shall be operated from inside the vehicle.
- For TCP(3-2a) the Engineer will determine if the TRAIL VEHICLE is required based on prevailing roadway conditions, traffic volume, and sight distance restrictions. All other vehicles shown for both TCP(3-2a) and TCP(3-2b) are required.
- The use of amber high intensity rotating, flashing, oscillating, or strobe lights on vehicles are required. Blue high intensity rotating, flashing, oscillating or strobe lights when mounted on the driver's side of the vehicle may be operated simultaneously with the amber beacons or strobe lights.
- The use of truck mounted attenuators (TMA) on the ADVANCE WARNING, SHADOW, and TRAIL vehicles are required.
- Reflective sheeting on the rear of the TMA shall meet or exceed the reflectivity and color requirements of DMS 8300, Type A.
- Each vehicle shall have two-way radio communication capability.
- When work convoys must change lanes, the TRAIL VEHICLE should change lanes first to shadow the other convoy vehicles.
- Vehicle spacing between the TRAIL VEHICLE and the SHADOW VEHICLE will vary depending on sight distance restrictions. Motorists approaching the work convoy should be able to see the TRAIL VEHICLE in time to slow down and/or change lanes as they approach the TRAIL VEHICLE. Vehicle spacing between the WORK VEHICLE and SHADOW VEHICLE may vary according to terrain, work activity and other factors.
- Standard 48" X 48" diamond shaped warning signs with the same message as those shown may be used where adequate mounting space exists.
- The signs shown should be used on the Advance Warning Vehicle. As an option, a portable changeable message sign (PCMS) or a truck mounted changeable message sign (TMCMS) with a minimum character height of 12", and displaying the same legend may be substituted for these signs. An appropriate directional arrow display, simulating the size and legibility of the flashing arrow board, must be used in the second phase of the PCMS/TMCMS message. When this is done, the arrow board will not be required on the Advance Warning Vehicle.
- Standard diamond shape versions of the CW20-5 series signs may be used as an option if the rectangular signs shown are not available.
- The principles on this sheet may be used to close lanes from the left side of the roadway considering the number of lanes, shoulder width, sight distance, and ramp frequency.
- Signs and flashing arrow board modes shall be appropriately altered when implementing left lane closures or interior closures which close the left lanes.
- The Advance Warning Vehicle may straddle the edgeline when shoulder width makes it necessary.



STRIPING FOR TMA

		Traffic Operations Division Standard	
TRAFFIC CONTROL PLAN MOBILE OPERATIONS DIVIDED HIGHWAYS			
TCP(3-2)-13			
FILE: tcp3-2.dgn	DN: TxDOT	CK: TxDOT	DN: TxDOT
© TxDOT December 1985	CONT SECT	JOB	HIGHWAY
2-94 4-96 8-95 7-13 1-97	6463 II	001	IN 35, Etc.
	DIST	COUNTY	SHEET NO.
	22	WEBB	13