

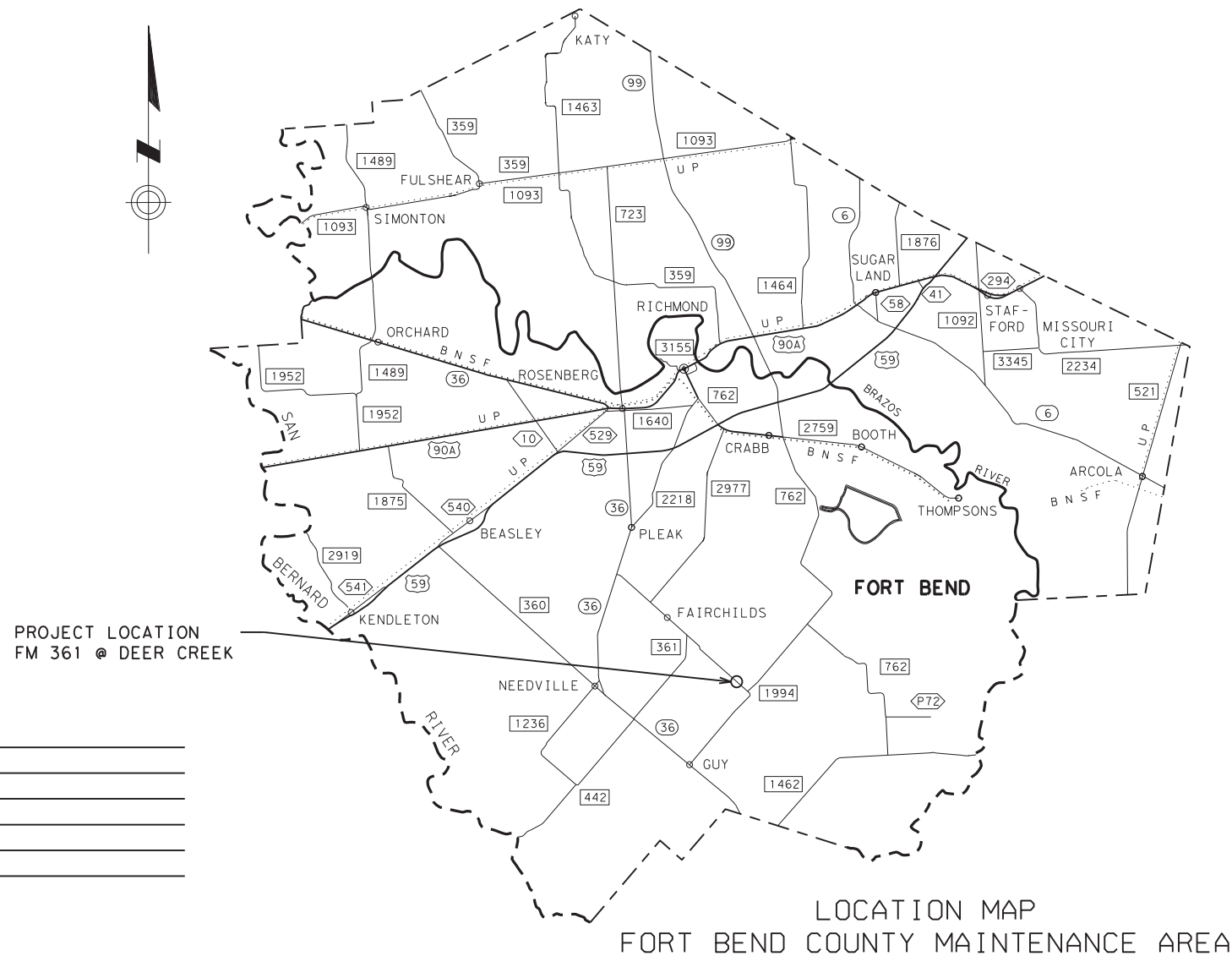
# STATE OF TEXAS

## DEPARTMENT OF TRANSPORTATION

FED. ROAD DIV. NO.	STATE	MAINTENANCE PROJECT NO.	SHEET NO.
6	TEXAS	RMC 6430-55-001	1
STATE DIST. NO.	COUNTY	STATE CONTROL NO.	HIGHWAY NO.
HOU	FORT BEND	6430-55-001	FM 361

### PLANS OF PROPOSED HIGHWAY ROUTINE MAINTENANCE CONTRACT

TYPE OF WORK  
**EROSION REPAIR**  
PROJECT NO.: RMC 6430-55-001  
HIGHWAY: FM 361  
LIMITS OF WORK: AT DEER CREEK



© 2023 By TEXAS DEPARTMENT OF TRANSPORTATION;  
ALL RIGHTS RESERVED

CONTRACTOR: \_\_\_\_\_  
 DATE OF LETTING: \_\_\_\_\_  
 DATE WORK BEGAN: \_\_\_\_\_  
 DATE OF WORK COMPLETED: \_\_\_\_\_  
 DATE WORK ACCEPTED: \_\_\_\_\_  
 FINAL CONTRACT COST: \_\_\_\_\_

SUBMITTED FOR LETTING: 12/20/2022

DocuSigned by:  
*Carlos M. Zepeda, Jr., P.E.*  
 AREA ENGINEER  
 999EB2AF5ACE472...

RECOMMENDED FOR LETTING: 2/13/2023

DocuSigned by:  
*Melody Gallardo*  
 A667165730A3459...  
 DIRECTOR OF MAINTENANCE

SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION, NOVEMBER 1, 2014, AND THE SPECIFICATION ITEMS INCLUDED IN THE CONTRACT SHALL GOVERN ON THIS PROJECT.

EXCEPTIONS: NONE  
EQUATIONS: NONE

LOCATION MAP  
FORT BEND COUNTY MAINTENANCE AREA



**County:** Fort Bend**Sheet****Highway:** FM 361**Control:** 6430-55-001**GENERAL NOTES****SUPERVISION:**

All work will be scheduled and directed by, and request for payment addressed to:

Juan Mata  
Fort Bend Area Maintenance Supervisor  
4235 SH 36 South  
Rosenberg, Texas 77471  
(281) 238-7950

**General:**

Area Engineer contact information for this project follows:

Carlos M. Zepeda, Jr., P.E.,  
Phone: (281)238-7920  
Email: [Carlos.Zepeda@txdot.gov](mailto:Carlos.Zepeda@txdot.gov)

Daniel J. Dvorak, P.E.  
Phone: (281)238-7915  
Email: [Daniel.Dvorak@txdot.gov](mailto:Daniel.Dvorak@txdot.gov)

Submit any questions about this project via the Letting Pre-Bid Q&A web page, located at:

<https://tableau.txdot.gov/views/ProjectInformationDashboard/NoticetoContractors>

The Letting Pre-Bid Q&A web page for each project can be accessed by scrolling or filtering the dashboard using the controls on the left side to navigate to the project. Hover over the blue hyperlink of the project to view the Q&A and click on the link in the window that pops up.

This is a Routine Maintenance Site-Specific contract.

Plan and execute all work in a neat manner.

Notify the Department by 7:30 a.m. when scheduled work is cancelled for any reason.

The following standard detail sheets are modified:

**Modified Standards**

*TCP One-Lane Two-Way Traffic Control TCP(2-2)-18 (MOD)*

**County:** Fort Bend**Sheet 3****Highway:** FM 361**Control:** 6430-55-001

References to manufacturer's trade name or catalog numbers are for the purpose of identification only. Similar materials from other manufacturers are permitted if they are of equal quality, comply with the specifications for this project, and are approved, except for roadway illumination, electrical, and traffic signal items.

Stencil the National Bridge Inventory (NBI) number on each existing bridge shown on these plans. The NBI number is shown above the title block for each bridge layout.

Tolls incurred by the Contractor are incidental to the various bid items.

Procure permits and licenses, which are to be issued by the City, County, or Municipal Utility District.

**General: Site Management**

Do not mix or store materials, or store or repair equipment, on top of concrete pavement or bridge decks unless authorized by the Engineer. Permission will be granted to store materials on surfaces if no damage or discoloration will result.

Personal vehicles of employees are not permitted to park within the right of way, including sections closed to public traffic. Employees may park on the right of way at the Contractor's office, equipment, and materials storage yard sites.

Assume ownership of debris and dispose of at an approved location. Do not dispose of debris on private property unless approved in writing by the District Engineer.

Control the dust caused by construction operations. For sweeping the base material in preparation for laying asphalt and for sweeping the finished concrete pavement, use one of the following types of sweepers or approved equal:

**Tricycle Type**  
Wayne Series 900  
Elgin White Wing  
Elgin Pelican

**Truck Type- 4 Wheel**  
M-B Cruiser II  
Wayne Model 945  
Mobile TE-3  
Mobile TE-4  
Murphy 4042

**General: Traffic Control and Construction**

When design details are not shown on the plans, provide signs and arrows conforming to the latest "Standard Highway Sign Designs for Texas" manual.

Existing pavement markings removed or damaged by more than 20 ft. will be replaced with temporary striping. Temporary striping shall be paint based unless otherwise directed by the engineer. This work will be considered incidental to the item of work.

**County:** Fort Bend**Sheet****Highway:** FM 361**Control:** 6430-55-001**General: Utilities**

If the Contractor damages or causes damage (breaks, leaks, nicks, dents, gouges, etc.) to the utility, contact the utility facility owner or operator immediately.

Be aware that an operational Computerized Transportation Management System (CTMS) exists within the limits of this project and that the system must remain operational throughout construction. If the Contractor damages or causes damage to this system, repair such damage within 8 hours of occurrence at no cost to the Department. In the event of system damage, notify the Director of Traffic Management Systems at 713-881-3283 within one hour of occurrence. Failure of the Contractor to repair damage to the main fiber optic cable and CCTV cable trunk lines, which convey all corridor information to TranStar, will result in the Contractor being billed for the full cost of emergency repairs.

At least 72 hours before starting work, make arrangements for locating existing Department-owned above ground and underground fiber optic, communications, power, illumination, and traffic signal cabling and conduit. Do this by calling the Department's Houston District Traffic Signal Operations Office at 713-802-5662, or by e-mailing the Department's Houston District Traffic Signal Operations Office at [HOU-LocateRequest@txdot.gov](mailto:HOU-LocateRequest@txdot.gov), to schedule marking of underground lines on the ground. Use caution if working in these areas to avoid damaging or interfering with existing facilities.

If overhead or underground power lines need to be de-energized, contact the electrical service provider to perform this work. Costs associated with de-energizing the power lines or other protective measures required are at no expense to the Department.

If working near power lines, comply with the appropriate sections of Texas State Law and Federal Regulations relating to the type of work involved.

**Item 7: Legal Relations and Responsibilities**

This project does not require a U.S. Army Corps of Engineers (USACE) Section 404 Permit before letting, but if a permit is needed during construction, assume responsibility for preparing the permit application. Submit the permit application to the Department's District Environmental Section for approval. Once the permit application is approved, the Department will submit it to the USACE. Assume responsibility for the requested revisions, in coordination with the Department's District Environmental Section.

Maintain the roadway slope stability. Maintaining slope stability is subsidiary to the various bid items.

This project is on a hurricane evacuation route. Provide at the pre-construction meeting a written plan outlining procedures to suspend work, secure the job site, and safely handle traffic through and across the project in the event of a hurricane evacuation.

During the hurricane season (June 1 through November 30), do not close any travel lanes except when the Contractor can demonstrate that he/she can provide labor, equipment, material, a work

**County:** Fort Bend**Sheet 3A****Highway:** FM 361**Control:** 6430-55-001

plan, and quality of work to satisfactorily return all lanes to an open, all-weather travel surface within 3 days of receiving written or verbal notice but no later than 3 days before the predicted hurricane landfall. Construction of temporary lanes to an all-weather surface will be paid for in accordance with Article 9.7, "Payment for Extra Work and Force Account Method."

In addition to lane closures, cease work 3 days before the predicted hurricane landfall on or near the roadway that adversely impacts the flow of traffic and reduces the capacity of the highway during an evacuation. Vehicles of the Contractor, subcontractors, or material suppliers will not be allowed to enter or exit the traffic stream, including those for the purpose of material hauling and delivery, and mobilization or demobilization of equipment. When directed, this prohibition will include a reasonable time period for the evacuees to return to their point of origin.

No significant traffic generator events have been identified.

**Item 8: Prosecution and Progress**

Working days will be computed and charged based on a standard workweek in accordance with Section 8.3.1.4.

The Lane Closure Assessment Fee is \$50. This fee applies to the Contractor for closures or obstructions that overlap into restricted hour traffic for each hour or portion thereof, per lane, regardless of the length of lane closure or obstruction. For Restricted Hours subject to Lane Assessment Fee refer to the Item, "Barricades, Signs, and Traffic Handling." The time increment for the Lane Closure Assessment fee for this project is one hour.

**Item 132: Embankment**

If salvaged base is used for the embankment material, break it into small pieces to achieve the required density and to facilitate placing in the embankment. Obtain approval of the material before placing in the embankment.

Furnish Type C material with a maximum Liquid Limit (LL) of 65, a minimum Plasticity Index (PI) of 5, and composed of suitable earth material such as loam, clay, or other materials that form a suitable embankment.

The embankment material used on the project which has a Liquid Limit exceeding 45 will be tested for Liquid Limits at the rate of one test per 20,000 cu. yd. or per total quantity less than 20,000 cu. yd., unless otherwise directed. Only use material that passes the above tests.

For unpaved areas, provide a finished grade with the top 4 in. capable of sustaining vegetation. Use fertile soil that is easily cultivated, free from objectionable material and highly resistant to erosion.

**Item 432: Riprap**

Provide class B concrete type RR8 & RR9 for concrete riprap and shoulder drains embankments at bridge ends.

**County:** Fort Bend

**Sheet**

**Highway:** FM 361

**Control:** 6430-55-001

If stone riprap is shown on the plans, use common stone riprap in accordance with Section 432.2.3.3, placed dry in accordance with Section 432.3.2.3. Do not grout. Crushed concrete may also be used.

**Items 500: Mobilization**

This contract consists of one (1) lump sum (LS) Mobilization.

**Item 502: Barricades, Signs and Traffic Handling**

Use a traffic control plan for handling traffic through the various phases of construction. Follow the phasing sequence unless otherwise agreed upon by the Area Engineer and the Project Manager. Ensure this plan conforms to the latest “Texas Manual on Uniform Traffic Control Devices” and the latest Barricade and Construction (BC) Standard Sheets.

Submit changes to the traffic control plan to the Area Engineer. Provide a layout showing the construction phasing, signs, striping, and signalizations for changes to the original traffic control plan.

Furnish and maintain the barricades and warning signs, including the necessary temporary and portable traffic control devices, during the various phases of construction. Place and construct these barricades and warning signs in accordance with the latest “Texas Manual on Uniform Traffic Control Devices” for typical construction layouts.

Cover work zone signs when work related to the signs is not in progress, or any hazard related to the signs no longer exists.

Keep the delineation devices, signs, and pavement markings clean. This work is subsidiary to the Item, “Barricades, Signs, and Traffic Handling.”

Erect temporary signs when exit ramps are closed or moved to new locations during construction.

Before detouring traffic onto the main lane shoulders, remove dirt, debris, vegetation, and other deleterious material from the surface of the shoulders. Appropriately sign the detour in an approved manner. This work is subsidiary to the various bid items.

Coordinate and schedule the work with the appropriate Metro representative if requiring access to the High Occupancy Vehicle lanes.

Do not mount signs on drums or barricades, except those listed in the latest Barricades and Construction standard sheets.

Use traffic cones for daytime work only; replace the cones with plastic drums during nighttime hours.

Use shadow vehicles with Truck Mounted Attenuators (TMAs) for lane and shoulder closures.

Do not reduce the existing number of lanes open to traffic except as shown on the following time schedule:

**County:** Fort Bend

**Sheet 3B**

**Highway:** FM 361

**Control:** 6430-55-001

**One Lane Closure/Two Lane Facility**

Day	Daytime Work Hours	Nighttime Work Hours	Restricted Hours Subject to Lane Assessment Fee
Monday Through Friday	No Restrictions	No Restrictions	No Restrictions

**Weekend One/Two Lane Closure**

Day	Daytime Work Hours	Nighttime Work Hours	Restricted Hours Subject to Lane Assessment Fee
Saturday Through Sunday	None	12:00 AM – 11:00 AM 8:00 PM - 12:00 AM	11:00 AM - 8:00 PM

The above times are approved for the traffic control conditions listed. The Area Engineer may approve other closure times if traffic counts warrant. The Area Engineer may reduce the above times for special events.

Law enforcement assistance will be required for this project and is expected to be required for major traffic control changes and lane closures. Coordinate with local law enforcement and arrange for law enforcement as directed or agreed by the Engineer. Before payment will be made, complete the “Daily Report on Law Enforcement Force Account Work” (Form 318), provided by the Department and submit daily invoices that agree with this form for any day during the month in which approved services were provided.

Provide full-time, off-duty, uniformed, certified peace officers, as part of traffic control operations. The peace officers must be able to show proof of certification by the Texas Commission on Law Enforcement Officers Standards. The cost of the officers is paid for on a force account basis.

The number of peace officers and working hours will be determined in advance of the work and approved by the Engineer.

Use Uneven Lane Signs (CW 8-11) during resurfacing operations for elevation differences between adjacent lanes of greater than 1 in.

The Contractor Force Account “Safety Contingency” that has been established for this project is intended to be utilized for work zone enhancements, to improve the effectiveness of the Traffic Control Plan, that could not be foreseen in the project planning and design stage. These enhancements will be mutually agreed upon by the Engineer and the Contractor’s Responsible Person based on weekly or more frequent traffic management reviews on the project. The Engineer may choose to use existing bid items if it does not slow the implementation of enhancement.

**County:** Fort Bend

**Sheet 3C**

**Highway:** FM 361

**Control:** 6430-55-001

All work and materials furnished with this item are subsidiary to the pertinent bid items except:

- Truck mounted attenuators payable under Item 6185-6002
- Law enforcement personnel payable under force account.

All lane closures are considered subsidiary to the various bid items.

**Item 506: Temporary Erosion, Sedimentation and Environmental Controls**

The use of hay bales is not permitted as Storm Water Pollution Prevention Plan (SWP3) measures.

The Storm Water Pollution Prevention Plan (SWP3) consists of temporary erosion control measures needed and provided for under this Item. The disturbed area is less than one acre and use of erosion control measures is not anticipated. If physical conditions encountered at the job site require necessary controls, BMP installation, maintenance, and removal will be paid as extra work on a force account basis per Articles 4.4 and 9.7. Since the disturbed area is less than 5 acres, a "Notice of Intent" (NOI) is not required.

Use appropriate measures to prevent, minimize, and control the spill of hazardous materials in the construction staging area. Remove and dispose of materials in compliance with State and Federal laws.

**Item 6185: Truck Mounted Attenuator (TMA) and Trailer Attenuator (TA)**

A shadow vehicle with Truck Mounted Attenuators (TMAs) or Trailer Attenuators (TAs) is required as shown on the appropriate Traffic Control Plan (TCP) sheets. TMAs/TAs must meet the requirements of the Compliant Work Zone Traffic Control Device List.

Level 3 Compliant TMAs/TAs are required for this project.

A total of one (1) shadow vehicle with a TMA/TA is required for the work with the exception of Pavement Marking Operations. The Contractor is responsible for determining if one or more of these operations will be ongoing at the same time to determine the total number of TMAs/TAs needed on the project.

A total of three (3) shadow vehicles with a TMA/TA are required for Pavement Marking Operations. The Contractor is responsible for determining if one or more of these operations will be ongoing at the same time to determine the total number of TMAs/TAs needed on the project.

In addition to the shadow vehicles with TMAs/TAs that are specified as being required on the TCP layout sheets for this project, provide additional shadow vehicles with TMAs/TAs as shown on the TCP Standard sheets. The Contractor is responsible for determining if one or more of these operations will be ongoing at the same time to determine the total number of TMAs/TAs needed on the project.



# Estimate & Quantity Sheet

**CONTROLLING PROJECT ID** 6430-55-001

**DISTRICT** Houston

**COUNTY** Fort Bend

**HIGHWAY** FM0361

<b>CONTROL SECTION JOB</b>				<b>6430-55-001</b>		TOTAL EST.	TOTAL FINAL
<b>PROJECT ID</b>				<b>A00192620</b>			
<b>COUNTY</b>				<b>Fort Bend</b>			
<b>HIGHWAY</b>				<b>FM0361</b>			
<b>ALT</b>	<b>BID CODE</b>	<b>DESCRIPTION</b>	<b>UNIT</b>	EST.	FINAL		
	104-6009	REMOVING CONC (RIPRAP)	SY	367.000		367.000	
	400-6005	CEM STABIL BKFL	CY	1,148.000		1,148.000	
	401-6001	FLOWABLE BACKFILL	CY	5.000		5.000	
	432-6002	RIPRAP (CONC)(5 IN)	CY	50.000		50.000	
	432-6026	RIPRAP (STONE COMMON)(DRY)(18 IN)	CY	89.000		89.000	
	500-6001	MOBILIZATION	LS	1.000		1.000	
	502-6001	BARRICADES, SIGNS AND TRAFFIC HANDLING	MO	2.000		2.000	
	552-6001	WIRE FENCE (TY A)	LF	100.000		100.000	
	6185-6002	TMA (STATIONARY)	DAY	18.000		18.000	

**SUMMARY OF QUANTITIES**

	0104-6009	0400-6005	0401-6001	0432-6002	0432-6026	0502-6001	0552-6001	6185-6002
LOCATION	REMOVING CONC (RIPRAP)	CEM STABIL BKFL	FLOWABLE BACKFILL	RIPRAP (CONC) (5 IN)	RIPRAP (STONE COMMON) (DRY)(18 IN)	BARRICADES, SIGNS AND TRAFFIC HANDLING	WIRE FENCE (TY A)	TMA (STATIONARY)
	SY	CY	CY	CY	CY	MO	LF	DAY
TOTAL	367	1,148	5	50	89	2	100	18

**SUMMARY OF QUANTITIES**

SHEET 1 OF 1



CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST	COUNTY		SHEET NO.
HOU	FORT BEND		5



**BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES:**

1. The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction pavement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
2. The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.
3. The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.
4. The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer.
5. Geometric design of lane shifts and detours should, when possible, meet the applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.
6. When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.
7. The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.
8. All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.
9. The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.
10. Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES, CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.
11. Traffic control devices should be in place only while work is actually in progress or a definite need exists.
12. The Engineer has the final decision on the location of all traffic control devices.
13. Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

**WORKER SAFETY NOTES:**


1. Workers on foot who are exposed to traffic or to construction equipment within the right-of-way shall wear high-visibility safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel," or equivalent revisions, and labeled as ANSI 107-2004 standard performance for Class 2 or 3 risk exposure. Class 3 garments should be considered for high traffic volume work areas or night time work.
2. Except in emergency situations, flagger stations shall be illuminated when flagging is used at night.

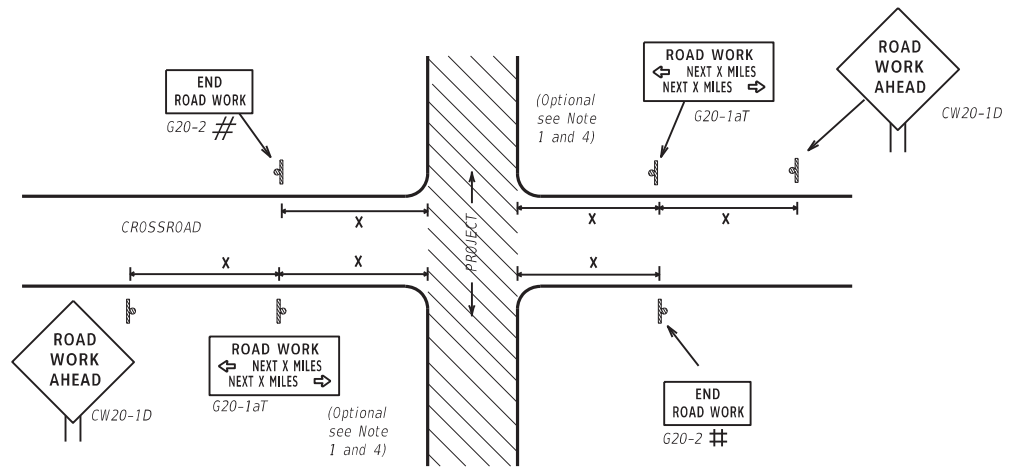
**COMPLIANT WORKZONE TRAFFIC CONTROL DEVICES**

1. Only pre-qualified products shall be used. The "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products and their sources.
2. Work zone traffic control devices shall be compliant with the Manual for Assessing safety Hardware (MASH).

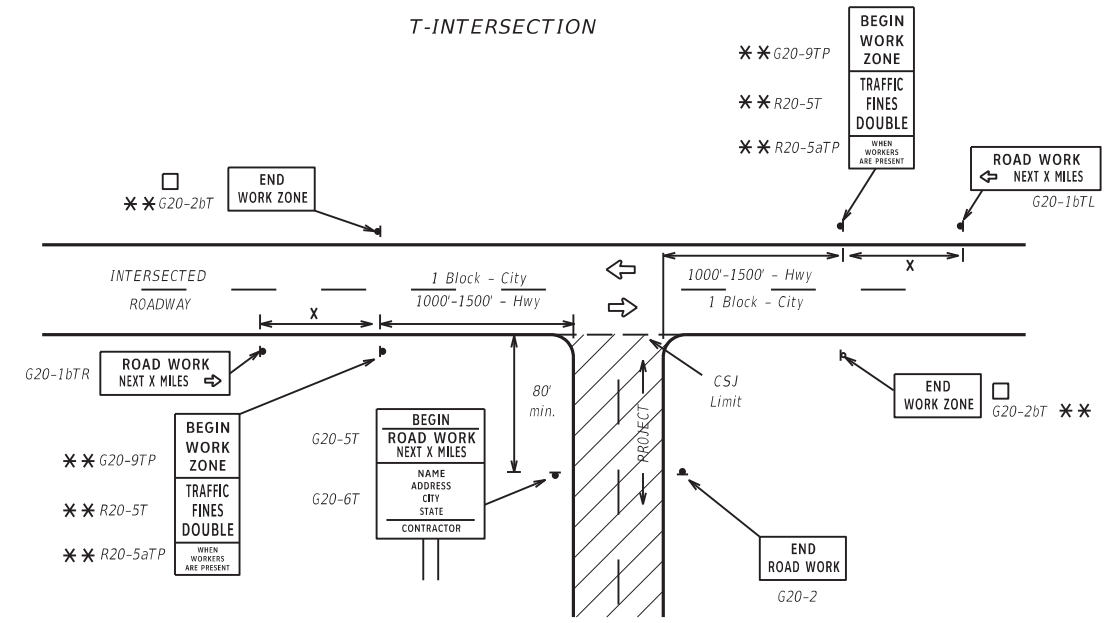
THE DOCUMENTS BELOW CAN BE FOUND ON-LINE AT <a href="http://www.txdot.gov">http://www.txdot.gov</a>
COMPLIANT WORK ZONE TRAFFIC CONTROL DEVICES LIST (CWZTCD)
DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS)
MATERIAL PRODUCER LIST (MPL)
ROADWAY DESIGN MANUAL - SEE "MANUALS (ONLINE MANUALS)"
STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS (SHSD)
TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD)
TRAFFIC ENGINEERING STANDARD SHEETS

SHEET 1 OF 12

 <b>Texas Department of Transportation</b>		<b>Traffic Safety Division Standard</b>	
<b>BARRICADE AND CONSTRUCTION GENERAL NOTES AND REQUIREMENTS</b>			
<b>BC(1)-21</b>			
FILE:	bc-21.dgn	DN: TxDOT	CK: TxDOT
©TxDOT	November 2002	CONT SECT	JOB HIGHWAY
REVISIONS		6430 55	001 FM 361
4-03	7-13	DIST	COUNTY SHEET NO.
9-07	8-14	HOU	FORT BEND 6
5-10	5-21		



- # May be mounted on back of "ROAD WORK AHEAD"(CW20-1D) sign with approval of Engineer. (See note 2 below)
- The typical minimum signing on a crossroad approach should be a "ROAD WORK AHEAD" (CW20-1D) sign and a (G20-2) "END ROAD WORK" sign, unless noted otherwise in plans.
  - The Engineer may use the reduced size 36" x 36" ROAD WORK AHEAD (CW20-1D) sign mounted back to back with the reduced size 36" x 18" "END ROAD WORK"(G20-2) sign on low volume crossroads (see Note 4 under "Typical Construction Warning Sign Size and Spacing"). See the "Standard Highway Sign Designs for Texas" manual for sign details. The Engineer may omit the advance warning signs on low volume crossroads. The Engineer will determine whether a road is low volume as per TMUTCD Part 5. This information shall be shown in the plans.
  - Based on existing field conditions, the Engineer/Inspector may require additional signs such as FLAGGER AHEAD, LOOSE GRAVEL, or other appropriate signs. When additional signs are required, these signs will be considered part of the minimum requirements. The Engineer/Inspector will determine the proper location and spacing of any sign not shown on the BC sheets, Traffic Control Plan sheets or the Work Zone Standard Sheets.
  - The "ROAD WORK NEXT X MILES"(G20-1aT) sign shall be required at high volume crossroads to advise motorists of the length of construction in either direction from the intersection. The Engineer will determine whether a roadway is considered high volume.
  - Additional traffic control devices may be shown elsewhere in the plans for higher volume crossroads.
  - When work occurs in the intersection area, appropriate traffic control devices, as shown elsewhere in the plans or as determined by the Engineer/Inspector, shall be in place.



**CSJ LIMITS AT T-INTERSECTION**

- The Engineer will determine the types and location of any additional traffic control devices, such as a flagger and accompanying signs, or other signs, that should be used when work is being performed at or near an intersection.
- If construction closes the road at a T-intersection, the Contractor shall place the "CONTRACTOR NAME"(G20-6T) sign behind the Type 3 Barricades for the road closure (see BC(10) also). The "ROAD WORK NEXT X MILES" left arrow(G20-1bTL) and "ROAD WORK NEXT X MILES" right arrow (G20-1bTR) signs shall be replaced by the detour signing called for in the plans.

**TYPICAL CONSTRUCTION WARNING SIGN SIZE AND SPACING**

Sign Number or Series	SIZE		SPACING	
	Conventional Road	Expressway/ Freeway	Posted Speed	Sign Spacing "X"
CW20 <sup>4</sup>	48" x 48"	48" x 48"	MPH	Feet (Apprx.)
CW21			30	120
CW22			35	160
CW23			40	240
CW25			45	320
CW1, CW2, CW7, CW8, CW9, CW11, CW14	36" x 36"	48" x 48"	50	400
			55	500 <sup>2</sup>
			60	600 <sup>2</sup>
			65	700 <sup>2</sup>
CW3, CW4, CW5, CW6, CW8-3, CW10, CW12	48" x 48"	48" x 48"	70	800 <sup>2</sup>
			75	900 <sup>2</sup>
			80	1000 <sup>2</sup>
			*	* <sup>3</sup>

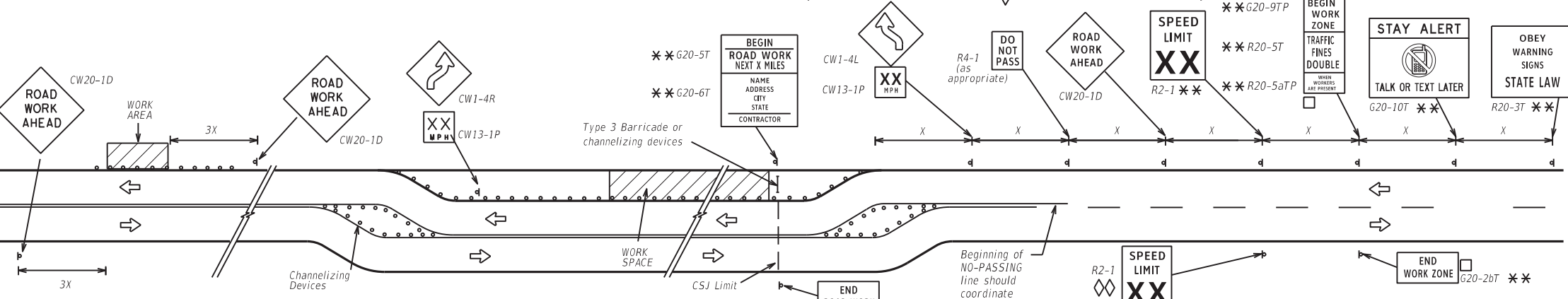
\* For typical sign spacings on divided highways, expressways and freeways, see Part 6 of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) typical application diagrams or TCP Standard Sheets.

△ Minimum distance from work area to first Advance Warning sign nearest the work area and/or distance between each additional sign.

**GENERAL NOTES**

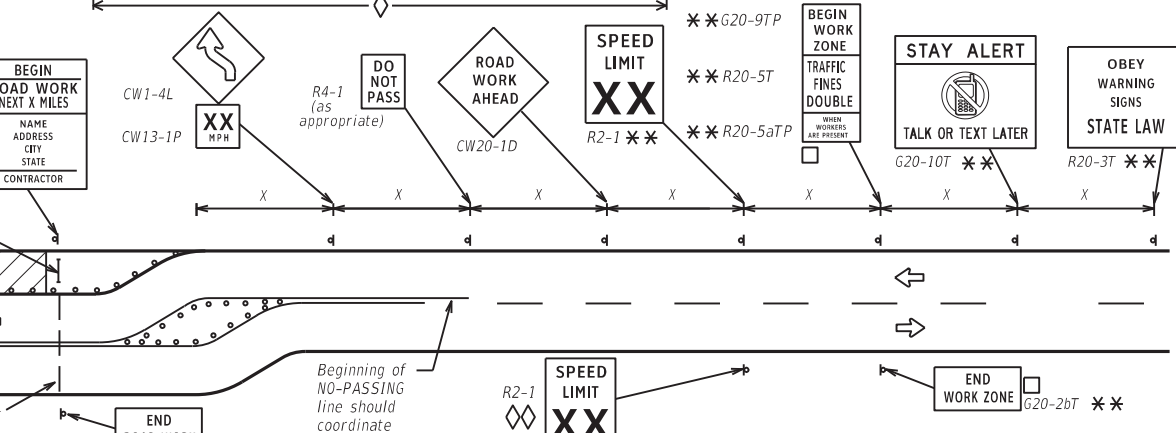
- Special or larger size signs may be used as necessary.
- Distance between signs should be increased as required to have 1500 feet advance warning.
- Distance between signs should be increased as required to have 1/2 mile or more advance warning.
- 36" x 36" "ROAD WORK AHEAD" (CW20-1D) signs may be used on low volume crossroads at the discretion of the Engineer as per TMUTCD Part 5. See Note 2 under "Typical Location of Crossroad Signs".
- Only diamond shaped warning sign sizes are indicated.
- See sign size listing in "TMUTCD", Sign Appendix or the "Standard Highway Sign Designs for Texas" manual for complete list of available sign design sizes.

**WORK AREAS IN MULTIPLE LOCATIONS WITHIN CSJ LIMITS**

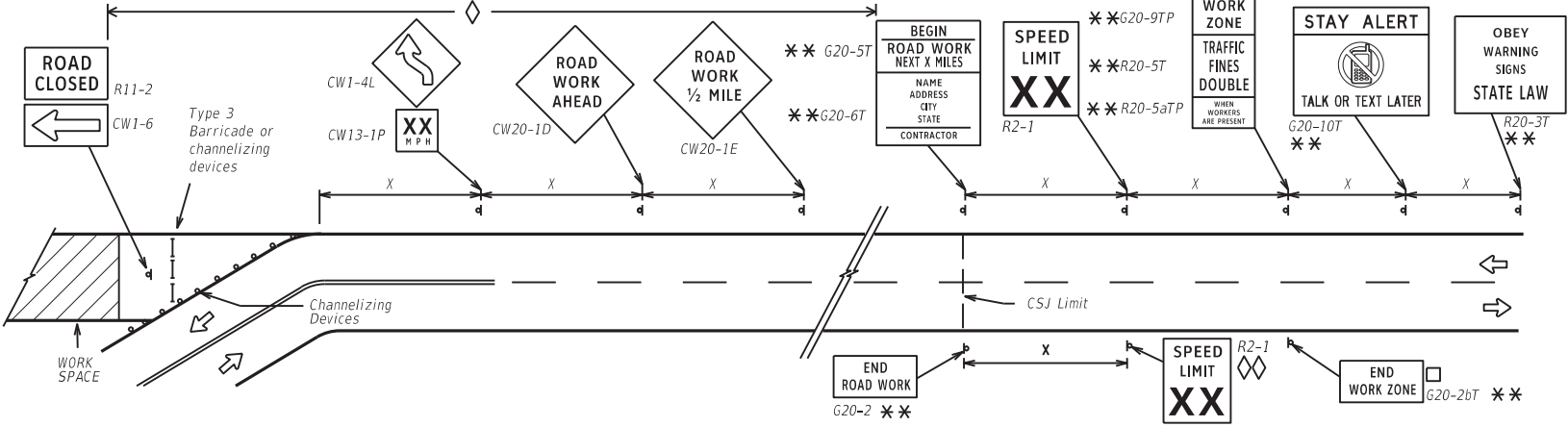


When extended distances occur between minimal work spaces, the Engineer/Inspector should ensure additional "ROAD WORK AHEAD"(CW20-1D) signs are placed in advance of these work areas to remind drivers they are still within the project limits. See the applicable TCP sheets for exact location and spacing of signs and channelizing devices.

**SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING AT THE CSJ LIMITS**



**SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING DOWNSTREAM OF THE CSJ LIMITS**



**NOTES**

- The Contractor shall determine the appropriate distance to be placed on the G20-1 series signs and "BEGIN ROAD WORK NEXT X MILES"(G20-5T) sign for each specific project. This distance shall replace the "X" and shall be rounded to the nearest whole mile with the approval of the Engineer. No decimals shall be used.
- The "BEGIN WORK ZONE"(G20-9TP) and "END WORK ZONE" (G20-2bT) shall be used as shown on the sample layout when advance signs are required outside the CSJ Limits. They inform the motorist of entering or leaving a part of the work zone lying outside the CSJ Limits where traffic fines may double if workers are present.
  - \* \* \* CSJ limit signing is required for highway construction and maintenance work, with the exception of mobile operations.
  - ◇ Area for placement of "ROAD WORK AHEAD" (CW20-1D) sign and other signs or devices as called for on the Traffic Control Plan.
  - ◇◇ Contractor will install a regulatory speed limit sign at the end of the work zone.

LEGEND	
—	Type 3 Barricade
○ ○ ○	Channelizing Devices
■	Sign
X	See Typical Construction Warning Sign Size and Spacing chart or the TMUTCD for sign spacing requirements.

SHEET 2 OF 12



**BARRICADE AND CONSTRUCTION PROJECT LIMIT**

BC(2)-21

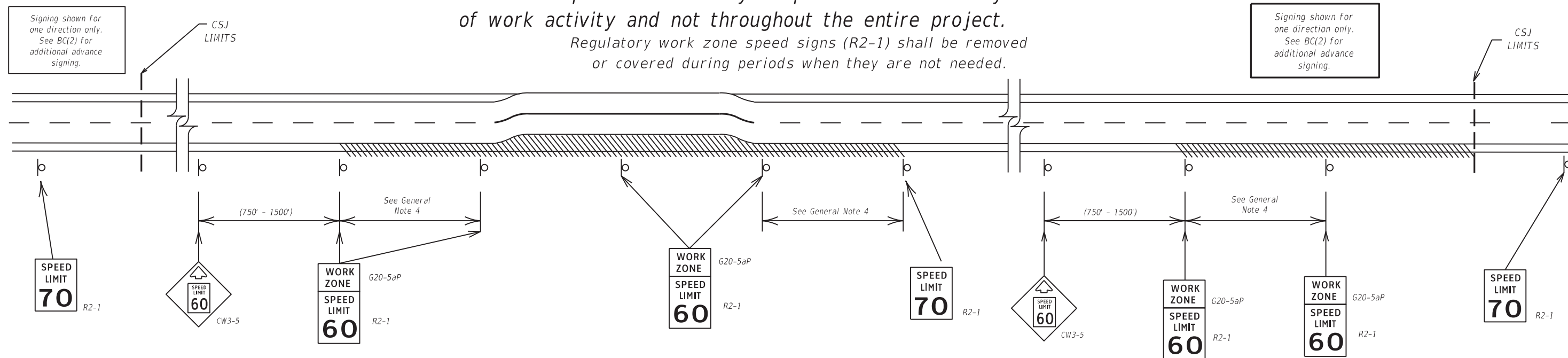
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	HOU	FORT BEND	7	

# TYPICAL APPLICATION OF WORK ZONE SPEED LIMIT SIGNS

Work zone speed limits shall be regulatory, established in accordance with the "Procedures for Establishing Speed Zones," and approved by the Texas Transportation Commission, or by City Ordinance when within Incorporated City Limits.

Reduced speeds should only be posted in the vicinity of work activity and not throughout the entire project.

Regulatory work zone speed signs (R2-1) shall be removed or covered during periods when they are not needed.



## GUIDANCE FOR USE:

### LONG/INTERMEDIATE TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit should be included on the design of the traffic control plans when restricted geometrics with a lower design speed are present in the work zone and modification of the geometrics to a higher design speed is not feasible.

Long/Intermediate Term Work Zone Speed Limit signs, when approved as described above, should be posted and visible to the motorist when work activity is present. Work activity may also be defined as a change in the roadway that requires a reduced speed for motorists to safely negotiate the work area, including:

- a) rough road or damaged pavement surface
- b) substantial alteration of roadway geometrics (diversions)
- c) construction detours
- d) grade
- e) width
- f) other conditions readily apparent to the driver

As long as any of these conditions exist, the work zone speed limit signs should remain in place.

### SHORT TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit may be included on the design of the traffic control plans when workers or equipment are not behind concrete barrier, when work activity is within 10 feet of the traveled way or actually in the traveled way.

Short Term Work Zone Speed Limit signs should be posted and visible to the motorists only when work activity is present. When work activity is not present, signs shall be removed or covered. (See Removing or Covering on BC(4)).

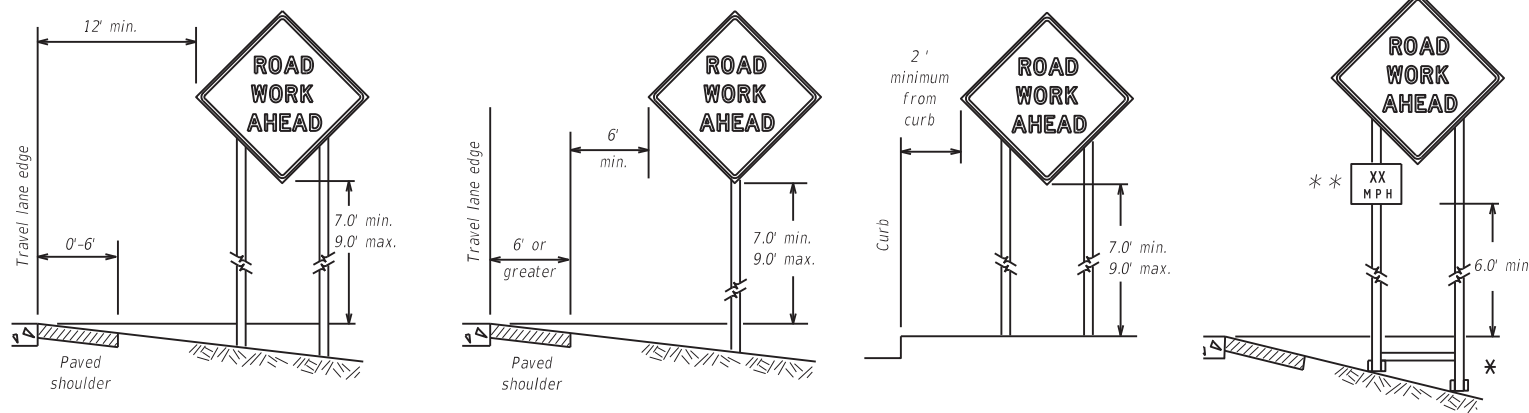
### GENERAL NOTES

1. Regulatory work zone speed limits should be used only for sections of construction projects where speed control is of major importance.
2. Regulatory work zone speed limit signs shall be placed on supports at a 7 foot minimum mounting height.
3. Speed zone signs are illustrated for one direction of travel and are normally posted for each direction of travel.
4. Frequency of work zone speed limit signs should be:
  - 40 mph and greater 0.2 to 2 miles
  - 35 mph and less 0.2 to 1 mile
5. Regulatory speed limit signs shall have black legend and border on a white reflective background (See "Reflective Sheeting" on BC(4)).
6. Fabrication, erection and maintenance of the "ADVANCE SPEED LIMIT"(CW3-5)sign, "WORK ZONE"(G20-5aP) plaque and the "SPEED LIMIT"(R2-1)signs shall not be paid for directly, but shall be considered subsidiary to Item 502.
7. Turning signs from view, laying signs over or down will not be allowed, unless as otherwise noted under "REMOVING OR COVERING" on BC(4).
8. Techniques that may help reduce traffic speeds include but are not limited to:
  - A. Law enforcement.
  - B. Flagger stationed next to sign.
  - C. Portable changeable message sign (PCMS).
  - D. Low-power (drone) radar transmitter.
  - E. Speed monitor trailers or signs.
9. Speeds shown on details above are for illustration only. Work Zone Speed Limits should only be posted as approved for each project.
10. For more specific guidance concerning the type of work, work zone conditions and factors impacting allowable regulatory construction speed zone reduction see TxDOT form #1204 in the TxDOT e-form system.

SHEET 3 OF 12

		Traffic Safety Division Standard	
<h2>BARRICADE AND CONSTRUCTION WORK ZONE SPEED LIMIT</h2>			
<h3>BC(3)-21</h3>			
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
©TxDOT November 2002	CONT	SECT	HIGHWAY
9-07 8-14	6430	55	001
7-13 5-21	DIST	COUNTY	SHEET NO.
	HOU	FORT BEND	8

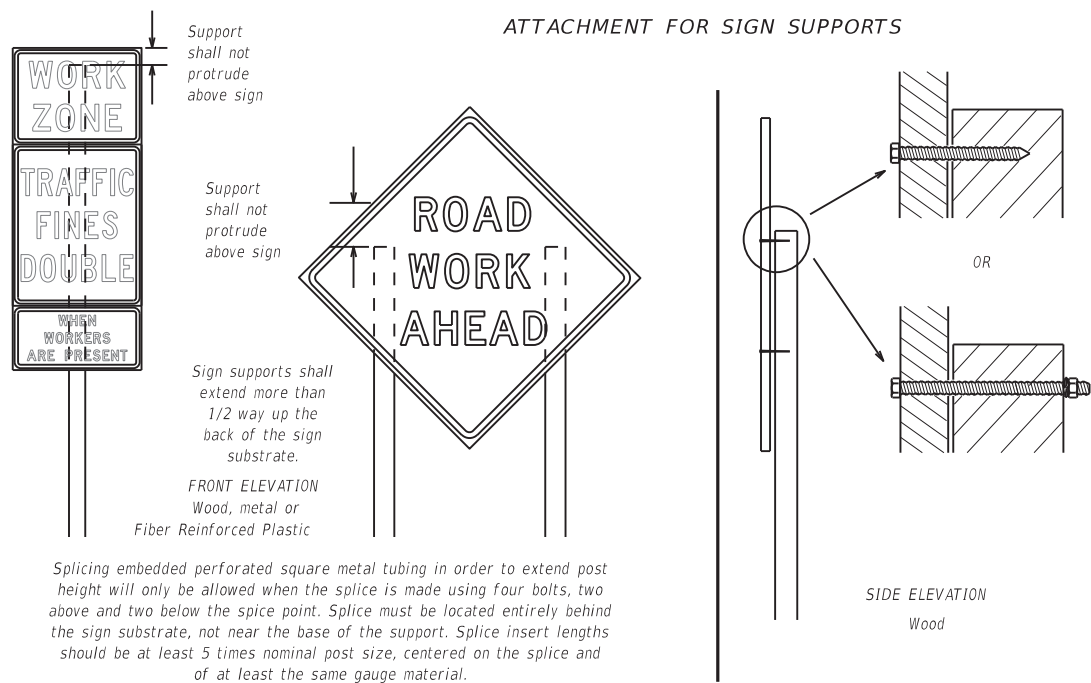
TYPICAL MINIMUM CLEARANCES FOR LONG TERM AND INTERMEDIATE TERM SIGNS



\* When placing skid supports on unlevel ground, the leg post lengths must be adjusted so the sign appears straight and plumb. Objects shall NOT be placed under skids as a means of leveling.

\*\* When plaques are placed on dual-leg supports, they should be attached to the upright nearest the travel lane. Supplemental plaques (advisory or distance) should not cover the surface of the parent sign.

ATTACHMENT FOR SIGN SUPPORTS



Splicing embedded perforated square metal tubing in order to extend post height will only be allowed when the splice is made using four bolts, two above and two below the splice point. Splice must be located entirely behind the sign substrate, not near the base of the support. Splice insert lengths should be at least 5 times nominal post size, centered on the splice and of at least the same gauge material.

GENERAL NOTES FOR WORK ZONE SIGNS

- Contractor shall install and maintain signs in a straight and plumb condition and/or as directed by the Engineer.
- Wooden sign posts shall be painted white.
- Barricades shall NOT be used as sign supports.
- All signs shall be installed in accordance with the plans or as directed by the Engineer. Signs shall be used to regulate, warn, and guide the traveling public safely through the work zone.
- The Contractor may furnish either the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD). The Engineer/Inspector may require the Contractor to furnish other work zone signs that are shown in the TMUTCD but may have been omitted from the plans. Any variation in the plans shall be documented by written agreement between the Engineer and the Contractor's Responsible Person. All changes must be documented in writing before being implemented. This can include documenting the changes in the Inspector's TxDOT diary and having both the Inspector and Contractor initial and date the agreed upon changes.
- The Contractor shall furnish sign supports listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCD) for small roadside signs. Supports for temporary large roadside signs shall meet the requirements detailed on the Temporary Large Roadside Signs (TLRS) standard sheets. The Contractor shall install the sign support in accordance with the manufacturer's recommendations. If there is a question regarding installation procedures, the Contractor shall furnish the Engineer a copy of the manufacturer's installation recommendations so the Engineer can verify the correct procedures are being followed.
- The Contractor is responsible for installing signs on approved supports and replacing signs with damaged or cracked substrates and/or damaged or marred reflective sheeting as directed by the Engineer/Inspector.
- Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1 inch.
- The Contractor shall replace damaged wood posts. New or damaged wood sign posts shall not be spliced.

DURATION OF WORK (as defined by the "Texas Manual on Uniform Traffic Control Devices" Part 6)

- The types of sign supports, sign mounting height, the size of signs, and the type of sign substrates can vary based on the type of work being performed. The Engineer is responsible for selecting the appropriate size sign for the type of work being performed. The Contractor is responsible for ensuring the sign support, sign mounting height and substrate meets manufacturer's recommendations in regard to crashworthiness and duration of work requirements.
  - Long-term stationary - work that occupies a location more than 3 days.
  - Intermediate-term stationary - work that occupies a location more than one daylight period up to 3 days, or nighttime work lasting more than one hour.
  - Short-term stationary - daytime work that occupies a location for more than 1 hour in a single daylight period.
  - Short, duration - work that occupies a location up to 1 hour.
  - Mobile - work that moves continuously or intermittently (stopping for up to approximately 15 minutes).

SIGN MOUNTING HEIGHT

- The bottom of Long-term/Intermediate-term signs shall be at least 7 feet, but not more than 9 feet, above the paved surface, except as shown for supplemental plaques mounted below other signs.
- The bottom of Short-term/Short Duration signs shall be a minimum of 1 foot above the pavement surface but no more than 2 feet above the ground.
- Long-term/Intermediate-term Signs may be used in lieu of Short-term/Short Duration signing.
- Short-term/Short Duration signs shall be used only during daylight and shall be removed at the end of the workday or raised to appropriate Long-term/Intermediate sign height.
- Regulatory signs shall be mounted at least 7 feet, but not more than 9 feet, above the paved surface regardless of work duration.

SIZE OF SIGNS

- The Contractor shall furnish the sign sizes shown on BC (2) unless otherwise shown in the plans or as directed by the Engineer.

SIGN SUBSTRATES

- The Contractor shall ensure the sign substrate is installed in accordance with the manufacturer's recommendations for the type of sign support that is being used. The CWZTCD lists each substrate that can be used on the different types and models of sign supports.
- "Mesh" type materials are NOT an approved sign substrate, regardless of the tightness of the weave.
- All wooden individual sign panels fabricated from 2 or more pieces shall have one or more plywood cleat, 1/2" thick by 6" wide, fastened to the back of the sign and extending fully across the sign. The cleat shall be attached to the back of the sign using wood screws that do not penetrate the face of the sign panel. The screws shall be placed on both sides of the splice and spaced at 6" centers. The Engineer may approve other methods of splicing the sign face.

REFLECTIVE SHEETING

- All signs shall be retroreflective and constructed of sheeting meeting the color and retro-reflectivity requirements of DMS-8300 for rigid signs or DMS-8310 for roll-up signs. The web address for DMS specifications is shown on BC(1).
- White sheeting, meeting the requirements of DMS-8300 Type A, shall be used for signs with a white background.
- Orange sheeting, meeting the requirements of DMS-8300 Type B<sub>FL</sub> or Type C<sub>FL</sub>, shall be used for rigid signs with orange backgrounds.

SIGN LETTERS

- All sign letters and numbers shall be clear, and open rounded type uppercase alphabet letters as approved by the Federal Highway Administration (FHWA) and as published in the "Standard Highway Sign Design for Texas" manual. Signs, letters and numbers shall be of first class workmanship in accordance with Department Standards and Specifications.

REMOVING OR COVERING

- When sign messages may be confusing or do not apply, the signs shall be removed or completely covered.
- Long-term stationary or intermediate stationary signs installed on square metal tubing may be turned away from traffic 90 degrees when the sign message is not applicable. This technique may not be used for signs installed in the median of divided highways or near any intersections where the sign may be seen from approaching traffic.
- Signs installed on wooden skids shall not be turned at 90 degree angles to the roadway. These signs should be removed or completely covered when not required.
- When signs are covered, the material used shall be opaque, such as heavy mil black plastic, or other materials which will cover the entire sign face and maintain their opaque properties under automobile headlights at night, without damaging the sign sheeting.
- Burlap shall NOT be used to cover signs.
- Duct tape or other adhesive material shall NOT be affixed to a sign face.
- Signs and anchor stubs shall be removed and holes backfilled upon completion of work.

SIGN SUPPORT WEIGHTS

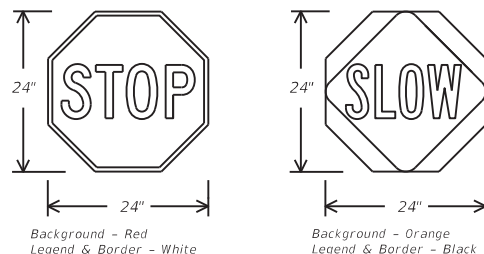
- Where sign supports require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand should be used.
- The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
- Rock, concrete, iron, steel or other solid objects shall not be permitted for use as sign support weights.
- Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
- Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall NOT be used.
- Rubber ballasts designed for channelizing devices should not be used for ballast on portable sign supports. Sign supports designed and manufactured with rubber bases may be used when shown on the CWZTCD list.
- Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign support.
- Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

FLAGS ON SIGNS

- Flags may be used to draw attention to warning signs. When used, the flag shall be 16 inches square or larger and shall be orange or fluorescent red-orange in color. Flags shall not be allowed to cover any portion of the sign face.

STOP/SLOW PADDLES

- STOP/SLOW paddles are the primary method to control traffic by flaggers. The STOP/SLOW paddle size should be 24" x 24".
- STOP/SLOW paddles shall be retroreflectorized when used at night.
- STOP/SLOW paddles may be attached to a staff with a minimum length of 6' to the bottom of the sign.
- Any lights incorporated into the STOP or SLOW paddle faces shall only be as specifically described in Section 6E.03 Hand Signaling Devices in the TMUTCD.



Background - Red Legend & Border - White

Background - Orange Legend & Border - Black

SHEETING REQUIREMENTS (WHEN USED AT NIGHT)

USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	ORANGE	TYPE B <sub>FL</sub> OR C <sub>FL</sub> SHEETING
LEGEND & BORDER	WHITE	TYPE B OR C SHEETING
LEGEND & BORDER	BLACK	ACRYLIC NON-REFLECTIVE FILM

CONTRACTOR REQUIREMENTS FOR MAINTAINING PERMANENT SIGNS WITHIN THE PROJECT LIMITS

- Permanent signs are used to give notice of traffic laws or regulations, call attention to conditions that are potentially hazardous to traffic operations, show route designations, destinations, directions, distances, services, points of interest, and other geographical, recreational, specific service (LOGO), or cultural information. Drivers proceeding through a work zone need the same, if not better route guidance as normally installed on a roadway without construction.
- When permanent regulatory or warning signs conflict with work zone conditions, remove or cover the permanent signs until the permanent sign message matches the roadway condition. For details for covering large guide signs see the TS-CD standard.
- When existing permanent signs are moved and relocated due to construction purposes, they shall be visible to motorists at all times.
- If existing signs are to be relocated on their original supports, they shall be installed on crashworthy bases as shown on the SMD Standard sheets. The signs shall meet the required mounting heights shown on the BC Sheets or the SMD Standards. This work should be paid for under the appropriate pay item for relocating existing signs.
- If permanent signs are to be removed and relocated using temporary supports, the Contractor shall use crashworthy supports as shown on the BC standard sheets, TLRS standard sheets or the CWZTCD list. The signs shall meet the required mounting heights shown on the BC, or the SMD standard sheets during construction. This work should be paid for under the appropriate pay item for relocating existing signs.
- Any sign or traffic control device that is struck or damaged by the Contractor or his/her construction equipment shall be replaced as soon as possible by the Contractor to ensure proper guidance for the motorists. This will be subsidiary to Item 502.

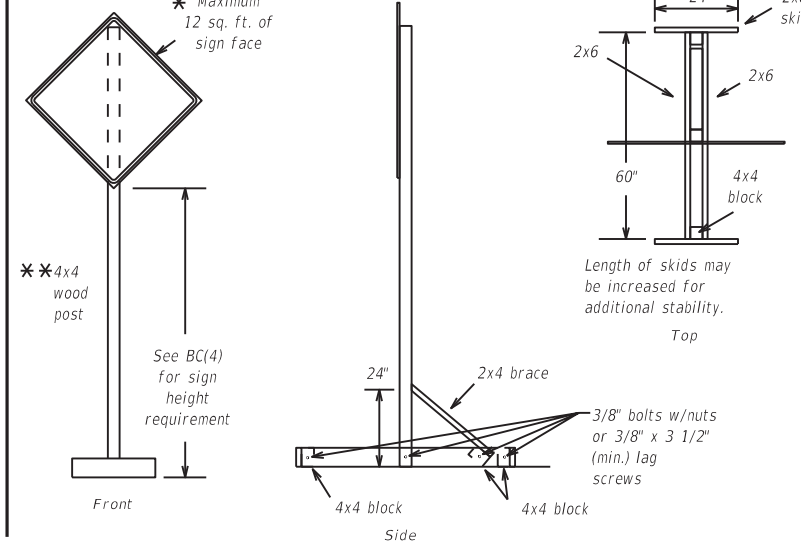
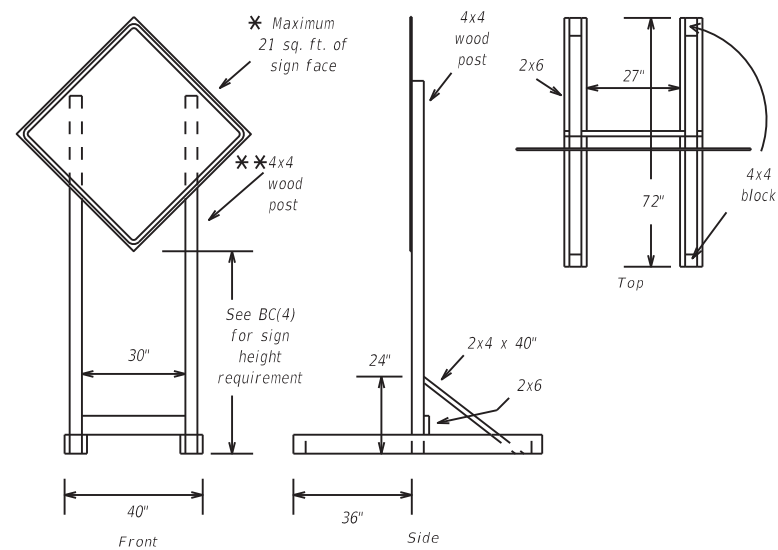
SHEET 4 OF 12



BARRICADE AND CONSTRUCTION TEMPORARY SIGN NOTES

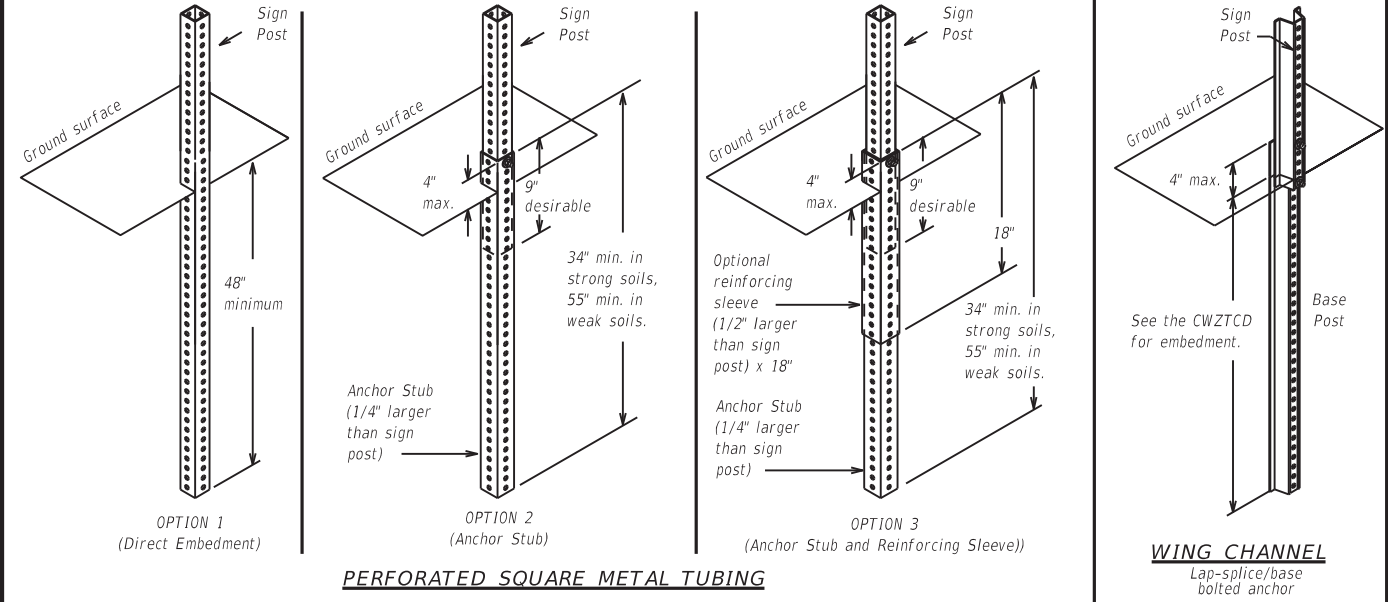
BC(4)-21

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	OW:	TxDOT	CK:	TxDOT
©TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		6430	55	001	FM 361				
9-07	8-14	DIST		COUNTY	SHEET NO.				
7-13	5-21	HOU		FORT BEND	9				



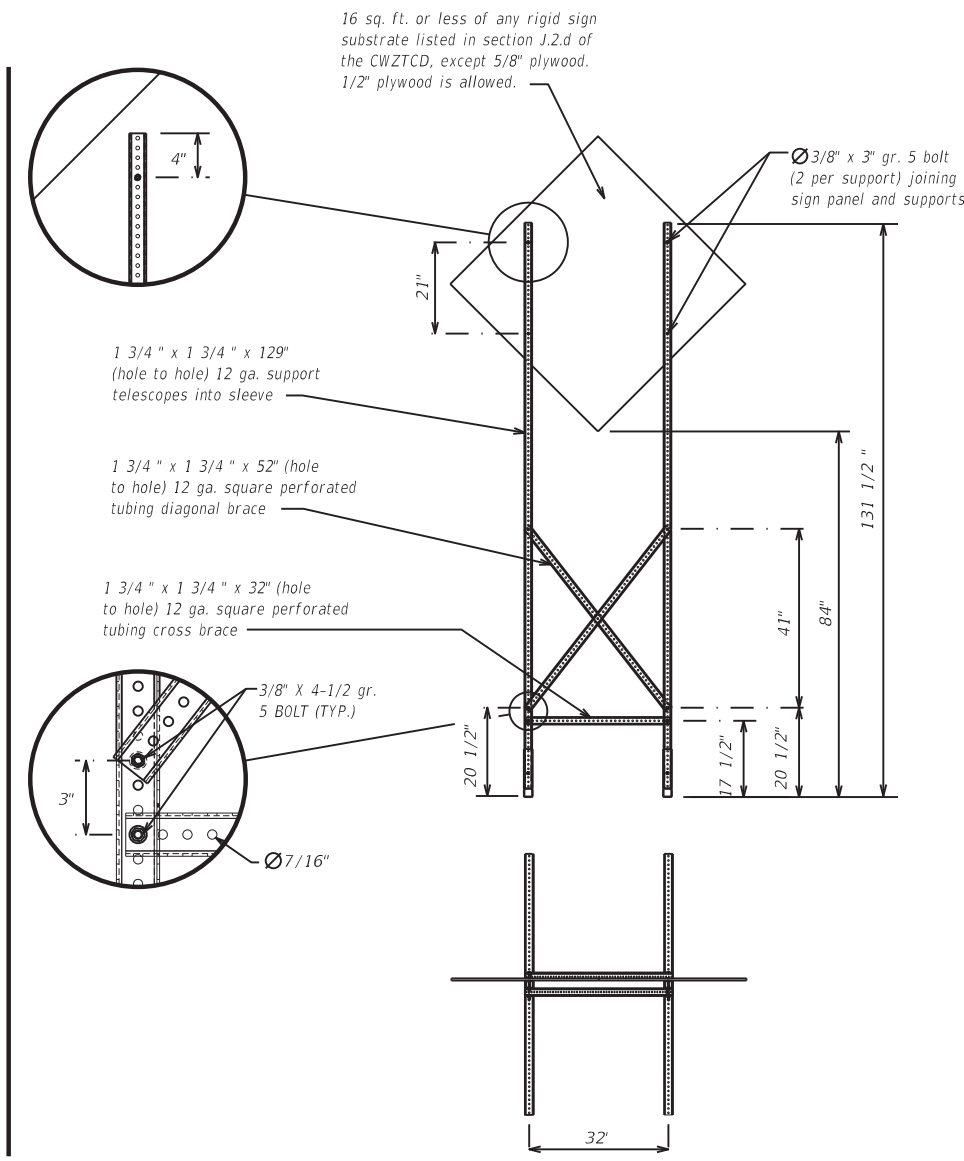
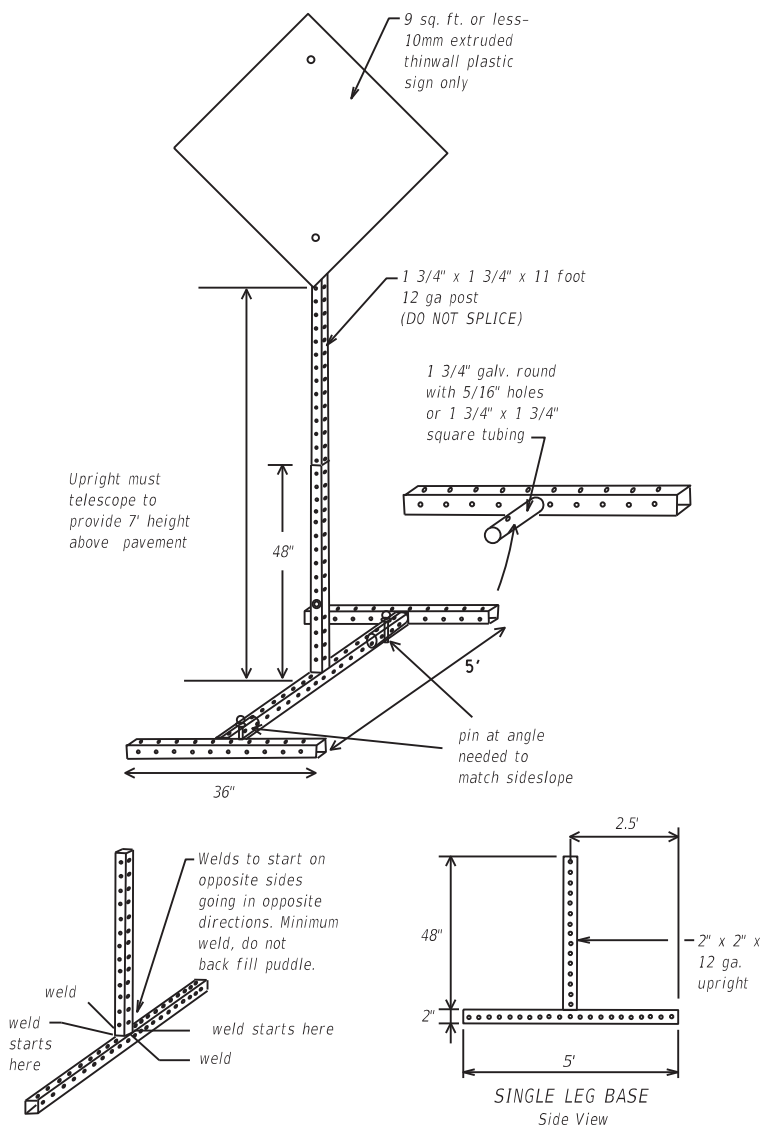
**SKID MOUNTED WOOD SIGN SUPPORTS**

\* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS



**GROUND MOUNTED SIGN SUPPORTS**

Refer to the CWZTCD and the manufacturer's installation procedure for each type sign support. The maximum sign square footage shall adhere to the manufacturer's recommendation. Two post installations can be used for larger signs.



**SKID MOUNTED PERFORATED SQUARE STEEL TUBING SIGN SUPPORTS**

\* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS

**WEDGE ANCHORS**  
Both steel and plastic Wedge Anchor Systems as shown on the SMD Standard Sheets may be used as temporary sign supports for signs up to 10 square feet of sign face. They may be set in concrete or in sturdy soils if approved by the Engineer. (See web address for "Traffic Engineering Standard Sheets" on BC(1)).

**OTHER DESIGNS**  
MORE DETAILS OF APPROVED LONG/INTERMEDIATE AND SHORT TERM SUPPORTS CAN BE FOUND ON THE CWZTCD LIST. SEE BC(1) FOR WEBSITE LOCATION.

- GENERAL NOTES**
1. Nails may be used in the assembly of wooden sign supports, but 3/8" bolts with nuts or 3/8" x 3 1/2" lag screws must be used on every joint for final connection.
  2. No more than 2 sign posts shall be placed within a 7 ft. circle, except for specific materials noted on the CWZTCD List.
  3. When project is completed, all sign supports and foundations shall be removed from the project site. This will be considered subsidiary to Item 502.

- \* See BC(4) for definition of "Work Duration."
- \*\* Wood sign posts MUST be one piece. Splicing will NOT be allowed. Posts shall be painted white.
- See the CWZTCD for the type of sign substrate that can be used for each approved sign support.

SHEET 5 OF 12



**BARRICADE AND CONSTRUCTION TYPICAL SIGN SUPPORT**

BC(5)-21

FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	HOU	FORT BEND	10	

WHEN NOT IN USE, REMOVE THE PCMS FROM THE RIGHT-OF-WAY OR PLACE THE PCMS BEHIND BARRIER OR GUARDRAIL WITH SIGN PANEL TURNED PARALLEL TO TRAFFIC

**PORTABLE CHANGEABLE MESSAGE SIGNS**

- The Engineer/Inspector shall approve all messages used on portable changeable message signs (PCMS).
- Messages on PCMS should contain no more than 8 words (about four to eight characters per word), not including simple words such as "TO," "FOR," "AT," etc.
- Messages should consist of a single phase, or two phases that alternate. Three-phase messages are not allowed. Each phase of the message should convey a single thought, and must be understood by itself.
- Use the word "EXIT" to refer to an exit ramp on a freeway; i.e., "EXIT CLOSED." Do not use the term "RAMP."
- Always use the route or interstate designation (IH, US, SH, FM) along with the number when referring to a roadway.
- When in use, the bottom of a stationary PCMS message panel should be a minimum 7 feet above the roadway, where possible.
- The message term "WEEKEND" should be used only if the work is to start on Saturday morning and end by Sunday evening at midnight. Actual days and hours of work should be displayed on the PCMS if work is to begin on Friday evening and/or continue into Monday morning.
- The Engineer/Inspector may select one of two options which are available for displaying a two-phase message on a PCMS. Each phase may be displayed for either four seconds each or for three seconds each.
- Do not "flash" messages or words included in a message. The message should be steady burn or continuous while displayed.
- Do not present redundant information on a two-phase message; i.e., keeping two lines of the message the same and changing the third line.
- Do not use the word "Danger" in message.
- Do not display the message "LANES SHIFT LEFT" or "LANES SHIFT RIGHT" on a PCMS. Drivers do not understand the message.
- Do not display messages that scroll horizontally or vertically across the face of the sign.
- The following table lists abbreviated words and two-word phrases that are acceptable for use on a PCMS. Both words in a phrase must be displayed together. Words or phrases not on this list should not be abbreviated, unless shown in the TMUTCD.

WORD OR PHRASE	ABBREVIATION	WORD OR PHRASE	ABBREVIATION
Access Road	ACCS RD	Major	MAJ
Alternate	ALT	Miles	MI
Avenue	AVE	Miles Per Hour	MPH
Best Route	BEST RTE	Minor	MNR
Boulevard	BLVD	Monday	MON
Bridge	BRDG	Normal	NORM
Cannot	CANT	North	N
Center	CTR	Northbound	(route) N
Construction Ahead	CONST AHD	Parking	PKING
CROSSING	XING	Road	RD
Detour Route	DETOUR RTE	Right Lane	RT LN
Do Not	DONT	Saturday	SAT
East	E	Service Road	SERV RD
Eastbound	(route) E	Shoulder	SHLDR
Emergency	EMER	Slippery	SLIP
Emergency Vehicle	EMER VEH	South	S
Entrance, Enter	ENT	Southbound	(route) S
Express Lane	EXP LN	Speed	SPD
Expressway	EXPWY	Street	ST
XXXX Feet	XXXX FT	Sunday	SUN
Fog Ahead	FOG AHD	Telephone	PHONE
Freeway	FRWY, FWY	Temporary	TEMP
Freeway Blocked	FWY BLKD	Thursday	THURS
Friday	FRI	To Downtown	TO DWNTN
Hazardous Driving	HAZ DRIVING	Traffic	TRAF
Hazardous Material	HAZMAT	Travelers	TRVLRs
High-Occupancy Vehicle	HOV	Tuesday	TUES
Highway	HWY	Time Minutes	TIME MIN
Hour(s)	HR, HRS	Upper Level	UPR LEVEL
Information	INFO	Vehicles (s)	VEH, VEHS
It Is	ITS	Warning	WARN
Junction	JCT	Wednesday	WED
Left	LFT	Weight Limit	WT LIMIT
Left Lane	LFT LN	West	W
Lane Closed	LN CLOSED	Westbound	(route) W
Lower Level	LWR LEVEL	Wet Pavement	WET PVMT
Maintenance	MAINT	Will Not	WONT

Roadway designation # IH-number, US-number, SH-number, FM-number

**RECOMMENDED PHASES AND FORMATS FOR PCMS MESSAGES DURING ROADWORK ACTIVITIES**

(The Engineer may approve other messages not specifically covered here.)

**Phase 1: Condition Lists**

Road/Lane/Ramp Closure List		Other Condition List	
FREEWAY CLOSED X MILE	FRONTAGE ROAD CLOSED	ROADWORK XXX FT	ROAD REPAIRS XXXX FT
ROAD CLOSED AT SH XXX	SHOULDER CLOSED XXX FT	FLAGGER XXXX FT	LANE NARROWS XXXX FT
ROAD CLSD AT FM XXXX	RIGHT LN CLOSED XXX FT	RIGHT LN NARROWS XXXX FT	TWO-WAY TRAFFIC XX MILE
RIGHT X LANES CLOSED	RIGHT X LANES OPEN	MERGING TRAFFIC XXXX FT	CONST TRAFFIC XXX FT
CENTER LANE CLOSED	DAYTIME LANE CLOSURES	LOOSE GRAVEL XXXX FT	UNEVEN LANES XXXX FT
NIGHT LANE CLOSURES	I-XX SOUTH EXIT CLOSED	DETOUR X MILE	ROUGH ROAD XXXX FT
VARIOUS LANES CLOSED	EXIT XXX CLOSED X MILE	ROADWORK PAST SH XXXX	ROADWORK NEXT FRI-SUN
EXIT CLOSED	RIGHT LN TO BE CLOSED	BUMP XXXX FT	US XXX EXIT X MILES
MALL DRIVEWAY CLOSED	X LANES CLOSED TUE - FRI	TRAFFIC SIGNAL XXXX FT	LANES SHIFT *
XXXXXXXX BLVD CLOSED			

\* LANES SHIFT in Phase 1 must be used with STAY IN LANE in Phase 2.

**Phase 2: Possible Component Lists**

Action to Take/Effect on Travel List	Location List	Warning List	** Advance Notice List
MERGE RIGHT	FORM X LINES RIGHT	AT FM XXXX	SPEED LIMIT XX MPH
DETOUR NEXT X EXITS	USE XXXXX RD EXIT	BEFORE RAILROAD CROSSING	MAXIMUM SPEED XX MPH
USE EXIT XXX	USE EXIT I-XX NORTH	NEXT X MILES	MINIMUM SPEED XX MPH
STAY ON US XXX SOUTH	USE I-XX E TO I-XX N	PAST US XXX EXIT	ADVISORY SPEED XX MPH
TRUCKS USE US XXX N	WATCH FOR TRUCKS	XXXXXXXX TO XXXXXXX	RIGHT LANE EXIT
WATCH FOR TRUCKS	EXPECT DELAYS	US XXX TO FM XXXX	USE CAUTION
EXPECT DELAYS	PREPARE TO STOP		DRIVE SAFELY
REDUCE SPEED XXX FT	END SHOULDER USE		DRIVE WITH CARE
USE OTHER ROUTES	WATCH FOR WORKERS		
STAY IN LANE *			

\*\* See Application Guidelines Note 6.

**APPLICATION GUIDELINES**

- Only 1 or 2 phases are to be used on a PCMS.
- The 1st phase (or both) should be selected from the "Road/Lane/Ramp Closure List" and the "Other Condition List".
- A 2nd phase can be selected from the "Action to Take/Effect on Travel, Location, General Warning, or Advance Notice Phase Lists".
- A Location Phase is necessary only if a distance or location is not included in the first phase selected.
- If two PCMS are used in sequence, they must be separated by a minimum of 1000 ft. Each PCMS shall be limited to two phases, and should be understandable by themselves.
- For advance notice, when the current date is within seven days of the actual work date, calendar days should be replaced with days of the week. Advance notification should typically be for no more than one week prior to the work.

**WORDING ALTERNATIVES**

- The words RIGHT, LEFT and ALL can be interchanged as appropriate.
- Roadway designations IH, US, SH, FM and LP can be interchanged as appropriate.
- EAST, WEST, NORTH and SOUTH (or abbreviations E, W, N and S) can be interchanged as appropriate.
- Highway names and numbers replaced as appropriate.
- ROAD, HIGHWAY and FREEWAY can be interchanged as needed.
- AHEAD may be used instead of distances if necessary.
- FT and MI, MILE and MILES interchanged as appropriate.
- AT, BEFORE and PAST interchanged as needed.
- Distances or AHEAD can be eliminated from the message if a location phase is used.

PCMS SIGNS WITHIN THE R.O.W. SHALL BE BEHIND GUARDRAIL OR CONCRETE BARRIER OR SHALL HAVE A MINIMUM OF FOUR (4) PLASTIC DRUMS PLACED PERPENDICULAR TO TRAFFIC ON THE UPSTREAM SIDE OF THE PCMS, WHEN EXPOSED TO ONE DIRECTION OF TRAFFIC. WHEN EXPOSED TO TWO WAY TRAFFIC, THE FOUR DRUMS SHOULD BE PLACED WITH ONE DRUM AT EACH OF THE FOUR CORNERS OF THE UNIT.

**FULL MATRIX PCMS SIGNS**

- When Full Matrix PCMS signs are used, the character height and legibility/visibility requirements shall be maintained as listed in Note 15 under "PORTABLE CHANGEABLE MESSAGE SIGNS" above.
- When symbol signs, such as the "Flagger Symbol"(CW20-7) are represented graphically on the Full Matrix PCMS sign and, with the approval of the Engineer, it shall maintain the legibility/visibility requirement listed above.
- When symbol signs are represented graphically on the Full Matrix PCMS, they shall only supplement the use of the static sign represented, and shall not substitute for, or replace that sign.
- A full matrix PCMS may be used to simulate a flashing arrow board provided it meets the visibility, flash rate and dimming requirements on BC(7), for the same size arrow.

SHEET 6 OF 12

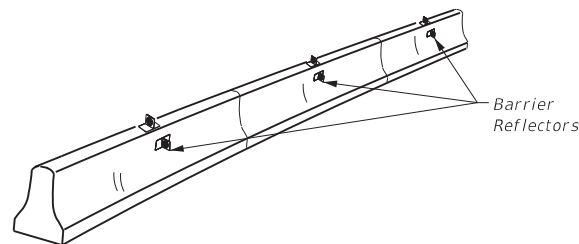


**BARRICADE AND CONSTRUCTION PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)**

**BC(6)-21**

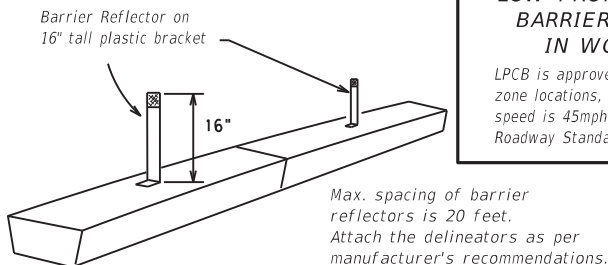
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	HOU	FORT BEND	11	

- Barrier Reflectors shall be pre-qualified, and conform to the color and reflectivity requirements of DMS-8600. A list of prequalified Barrier Reflectors can be found at the Material Producer List web address shown on BC(1).
- Color of Barrier Reflectors shall be as specified in the TMUTCD. The cost of the reflectors shall be considered subsidiary to Item 512.



**CONCRETE TRAFFIC BARRIER (CTB)**

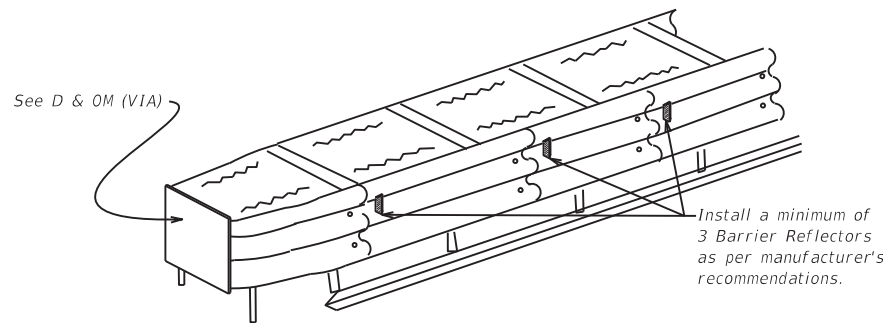
- Where traffic is on one side of the CTB, two (2) Barrier Reflectors shall be mounted in approximately the midsection of each section of CTB. An alternate mounting location is uniformly spaced at one end of each CTB. This will allow for attachment of a barrier grapple without damaging the reflector. The Barrier Reflector mounted on the side of the CTB shall be located directly below the reflector mounted on top of the barrier, as shown in the detail above.
- Where CTB separates two-way traffic, three barrier reflectors shall be mounted on each section of CTB. The reflector unit on top shall have two yellow reflective faces (Bi-Directional) while the reflectors on each side of the barrier shall have one yellow reflective face, as shown in the detail above.
- When CTB separates traffic traveling in the same direction, no barrier reflectors will be required on top of the CTB.
- Barrier Reflector units shall be yellow or white in color to match the edgeline being supplemented.
- Maximum spacing of Barrier Reflectors is forty (40) feet.
- Pavement markers or temporary flexible-reflective roadway marker tabs shall NOT be used as CTB delineation.
- Attachment of Barrier Reflectors to CTB shall be per manufacturer's recommendations.
- Missing or damaged Barrier Reflectors shall be replaced as directed by the Engineer.
- Single slope barriers shall be delineated as shown on the above detail.



**LOW PROFILE CONCRETE BARRIER (LPCB)**

**LOW PROFILE CONCRETE BARRIER (LPCB) USED IN WORK ZONES**

LPCB is approved for use in work zone locations, where the posted speed is 45mph, or less. See Roadway Standard Sheet LPCB.



**DELINEATION OF END TREATMENTS**

**END TREATMENTS FOR CTB'S USED IN WORK ZONES**

End treatments used on CTB's in work zones shall meet the appropriate crashworthy standards as defined in the Manual for Assessing Safety Hardware (MASH). Refer to the CWZTCD List for approved end treatments and manufacturers.

**BARRIER REFLECTORS FOR CONCRETE TRAFFIC BARRIER AND ATTENUATORS**

**WARNING LIGHTS**

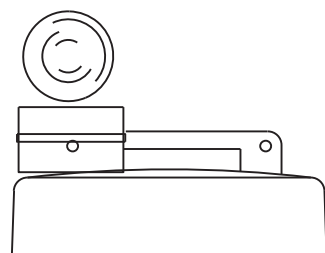
- Warning lights shall meet the requirements of the TMUTCD.
- Warning lights shall NOT be installed on barricades.
- Type A-Low Intensity Flashing Warning Lights are commonly used with drums. They are intended to warn of or mark a potentially hazardous area. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "FL". The Type A Warning Lights shall not be used with signs manufactured with Type B<sub>1</sub> or C<sub>1</sub> Sheeting meeting the requirements of Departmental Material Specification DMS-8300.
- Type-C and Type D 360 degree Steady Burn Lights are intended to be used in a series for delineation to supplement other traffic control devices. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "SB".
- The Engineer/Inspector or the plans shall specify the location and type of warning lights to be installed on the traffic control devices.
- When required by the Engineer, the Contractor shall furnish a copy of the warning lights certification. The warning light manufacturer will certify the warning lights meet the requirements of the latest ITE Purchase Specifications for Flashing and Steady-Burn Warning Lights.
- When used to delineate curves, Type-C and Type D Steady Burn Lights should only be placed on the outside of the curve, not the inside.
- The location of warning lights and warning reflectors on drums shall be as shown elsewhere in the plans.

**WARNING LIGHTS MOUNTED ON PLASTIC DRUMS**

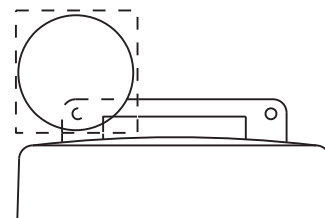
- Type A flashing warning lights are intended to warn drivers that they are approaching or are in a potentially hazardous area.
- Type A random flashing warning lights are not intended for delineation and shall not be used in a series.
- A series of sequential flashing warning lights placed on channelizing devices to form a merging taper may be used for delineation. If used, the successive flashing of the sequential warning lights should occur from the beginning of the taper to the end of the merging taper in order to identify the desired vehicle path. The rate of flashing for each light shall be 65 flashes per minute, plus or minus 10 flashes.
- Type C and D steady-burn warning lights are intended to be used in a series to delineate the edge of the travel lane on detours, on lane changes, on lane closures, and on other similar conditions.
- Type A, Type C and Type D warning lights shall be installed at locations as detailed on other sheets in the plans.
- Warning lights shall not be installed on a drum that has a sign, chevron or vertical panel.
- The maximum spacing for warning lights on drums should be identical to the channelizing device spacing.

**WARNING REFLECTORS MOUNTED ON PLASTIC DRUMS AS A SUBSTITUTE FOR TYPE C (STEADY BURN) WARNING LIGHTS**

- A warning reflector or approved substitute may be mounted on a plastic drum as a substitute for a Type C, steady burn warning light at the discretion of the Contractor unless otherwise noted in the plans.
- The warning reflector shall be yellow in color and shall be manufactured using a sign substrate approved for use with plastic drums listed on the CWZTCD.
- The warning reflector shall have a minimum retroreflective surface area (one-side) of 30 square inches.
- Round reflectors shall be fully reflectorized, including the area where attached to the drum.
- Square substrates must have a minimum of 30 square inches of reflectorized sheeting. They do not have to be reflectorized where it attaches to the drum.
- The side of the warning reflector facing approaching traffic shall have sheeting meeting the color and retroreflectivity requirements for DMS 8300-Type B or Type C.
- When used near two-way traffic, both sides of the warning reflector shall be reflectorized.
- The warning reflector should be mounted on the side of the handle nearest approaching traffic.
- The maximum spacing for warning reflectors should be identical to the channelizing device spacing requirements.



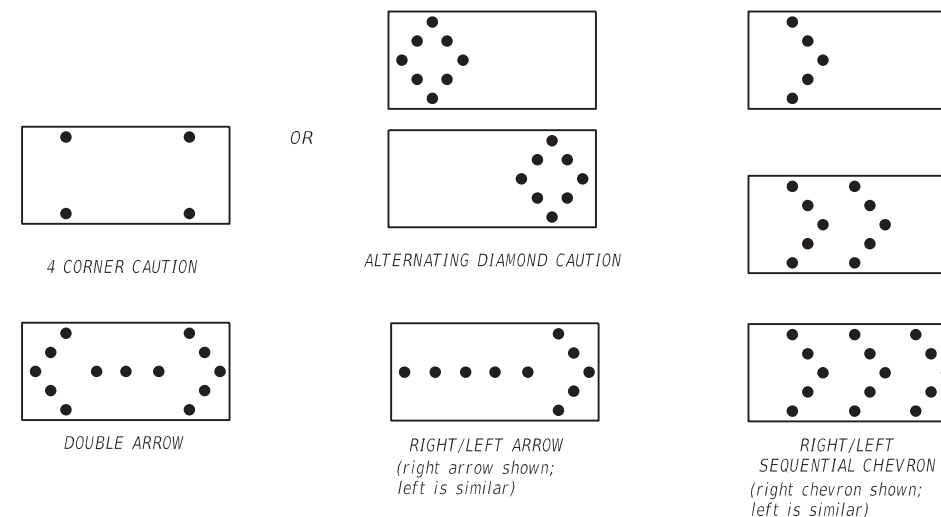
Type C Warning Light or approved substitute mounted on a drum adjacent to the travel way.



Warning reflector may be round or square. Must have a yellow reflective surface area of at least 30 square inches

Arrow Boards may be located behind channelizing devices in place for a shoulder taper or merging taper, otherwise they shall be delineated with four (4) channelizing devices placed perpendicular to traffic on the upstream side of traffic.

- The Flashing Arrow Board should be used for all lane closures on multi-lane roadways, or slow moving maintenance or construction activities on the travel lanes.
- Flashing Arrow Boards should not be used on two-lane, two-way roadways, detours, diversions or work on shoulders unless the "CAUTION" display (see detail below) is used.
- The Engineer/Inspector shall choose all appropriate signs, barricades and/or other traffic control devices that should be used in conjunction with the Flashing Arrow Board.
- The Flashing Arrow Board should be able to display the following symbols:



- The "CAUTION" display consists of four corner lamps flashing simultaneously, or the Alternating Diamond Caution mode as shown.
- The straight line caution display is NOT ALLOWED.
- The Flashing Arrow Board shall be capable of minimum 50 percent dimming from rated lamp voltage. The flashing rate of the lamps shall not be less than 25 nor more than 40 flashes per minute.
- Minimum lamp "on time" shall be approximately 50 percent for the flashing arrow and equal intervals of 25 percent for each sequential phase of the flashing chevron.
- The sequential arrow display is NOT ALLOWED.
- The flashing arrow display is the TxDOT standard; however, the sequential chevron display may be used during daylight operations.
- The Flashing Arrow Board shall be mounted on a vehicle, trailer or other suitable support.
- A Flashing Arrow Board SHALL NOT BE USED to laterally shift traffic.
- A full matrix PCMS may be used to simulate a Flashing Arrow Board provided it meets visibility, flash rate and dimming requirements on this sheet for the same size arrow.
- Minimum mounting height of trailer mounted Arrow Boards should be 7 feet from roadway to bottom of panel.

REQUIREMENTS			
TYPE	MINIMUM SIZE	MINIMUM NUMBER OF PANEL LAMPS	MINIMUM VISIBILITY DISTANCE
B	30 x 60	13	3/4 mile
C	48 x 96	15	1 mile

**ATTENTION**  
Flashing Arrow Boards shall be equipped with automatic dimming devices.

WHEN NOT IN USE, REMOVE THE ARROW BOARD FROM THE RIGHT-OF-WAY OR PLACE THE ARROW BOARD BEHIND CONCRETE TRAFFIC BARRIER OR GUARDRAIL.

**FLASHING ARROW BOARDS**

SHEET 7 OF 12

**TRUCK-MOUNTED ATTENUATORS**

- Truck-mounted attenuators (TMA) used on TxDOT facilities must meet the requirements outlined in the Manual for Assessing Safety Hardware (MASH).
- Refer to the CWZTCD for the requirements of Level 2 or Level 3 TMAs.
- Refer to the CWZTCD for a list of approved TMAs.
- TMAs are required on freeways unless otherwise noted in the plans.
- A TMA should be used anytime that it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the work performance.
- The only reason a TMA should not be required is when a work area is spread down the roadway and the work crew is an extended distance from the TMA.



**BARRICADE AND CONSTRUCTION ARROW PANEL, REFLECTORS, WARNING LIGHTS & ATTENUATOR**

BC(7)-21

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CK:	TxDOT
©TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		6430	55	001	FM 361				
9-07	8-14								
7-13	5-21	DIST	COUNTY	SHEET NO.					
		HOU	FORT BEND	12					

**GENERAL NOTES**

- For long term stationary work zones on freeways, drums shall be used as the primary channelizing device.
- For intermediate term stationary work zones on freeways, drums should be used as the primary channelizing device but may be replaced in tangent sections by vertical panels, or 42" two-piece cones. In tangent sections, one-piece cones may be used with the approval of the Engineer but only if personnel are present on the project at all times to maintain the cones in proper position and location.
- For short term stationary work zones on freeways, drums are the preferred channelizing device but may be replaced in tapers, transitions and tangent sections by vertical panels, two-piece cones or one-piece cones as approved by the Engineer.
- Drums and all related items shall comply with the requirements of the current version of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Drums, bases, and related materials shall exhibit good workmanship and shall be free from objectionable marks or defects that would adversely affect their appearance or serviceability.
- The Contractor shall have a maximum of 24 hours to replace any plastic drums identified for replacement by the Engineer/Inspector. The replacement device must be an approved device.

**GENERAL DESIGN REQUIREMENTS**

Pre-qualified plastic drums shall meet the following requirements:

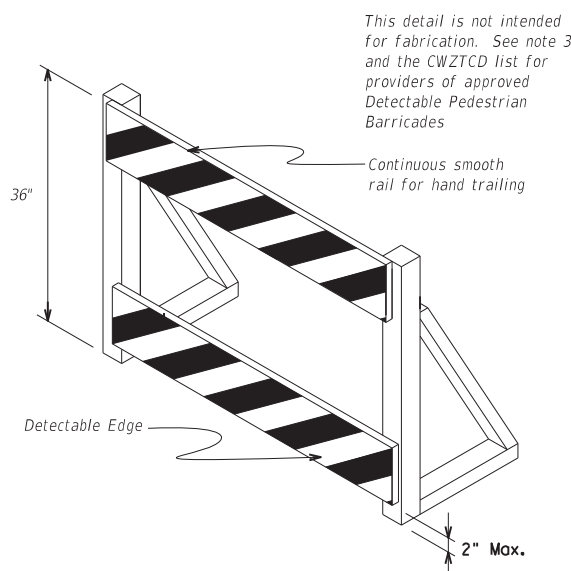
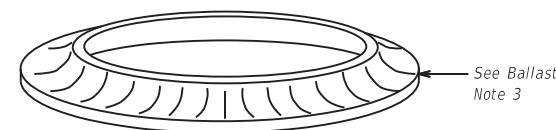
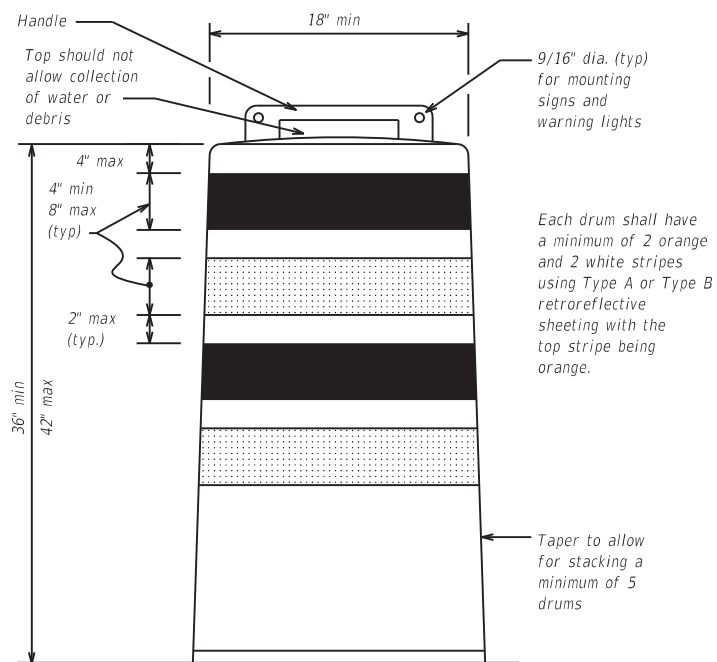
- Plastic drums shall be a two-piece design; the "body" of the drum shall be the top portion and the "base" shall be the bottom.
- The body and base shall lock together in such a manner that the body separates from the base when impacted by a vehicle traveling at a speed of 20 MPH or greater but prevents accidental separation due to normal handling and/or air turbulence created by passing vehicles.
- Plastic drums shall be constructed of lightweight flexible, and deformable materials. The Contractor shall NOT use metal drums or single piece plastic drums as channelization devices or sign supports.
- Drums shall present a profile that is a minimum of 18 inches in width at the 36 inch height when viewed from any direction. The height of drum unit (body installed on base) shall be a minimum of 36 inches and a maximum of 42 inches.
- The top of the drum shall have a built-in handle for easy pickup and shall be designed to drain water and not collect debris. The handle shall have a minimum of two widely spaced 9/16 inch diameter holes to allow attachment of a warning light, warning reflector unit or approved compliant sign.
- The exterior of the drum body shall have a minimum of four alternating orange and white retroreflective circumferential stripes not less than 4 inches nor greater than 8 inches in width. Any non-reflectORIZED space between any two adjacent stripes shall not exceed 2 inches in width.
- Bases shall have a maximum width of 36 inches, a maximum height of 4 inches, and a minimum of two footholds of sufficient size to allow base to be held down while separating the drum body from the base.
- Plastic drums shall be constructed of ultra-violet stabilized, orange, high-density polyethylene (HDPE) or other approved material.
- Drum body shall have a maximum unballasted weight of 11 lbs.
- Drum and base shall be marked with manufacturer's name and model number.

**RETROREFLECTIVE SHEETING**

- The stripes used on drums shall be constructed of sheeting meeting the color and retroreflectivity requirements of Departmental Materials Specification DMS-8300, "Sign Face Materials." Type A or Type B reflective sheeting shall be supplied unless otherwise specified in the plans.
- The sheeting shall be suitable for use on and shall adhere to the drum surface such that, upon vehicular impact, the sheeting shall remain adhered in-place and exhibit no delaminating, cracking, or loss of retroreflectivity other than that loss due to abrasion of the sheeting surface.

**BALLAST**

- Unballasted bases shall be large enough to hold up to 50 lbs. of sand. This base, when filled with the ballast material, should weigh between 35 lbs (minimum) and 50 lbs (maximum). The ballast may be sand in one to three sandbags separate from the base, sand in a sand-filled plastic base, or other ballasting devices as approved by the Engineer. Stacking of sandbags will be allowed, however height of sandbags above pavement surface may not exceed 12 inches.
- Bases with built-in ballast shall weigh between 40 lbs. and 50 lbs. Built-in ballast can be constructed of an integral crumb rubber base or a solid rubber base.
- Recycled truck tire sidewalls may be used for ballast on drums approved for this type of ballast on the CWZTCD list.
- The ballast shall not be heavy objects, water, or any material that would become hazardous to motorists, pedestrians, or workers when the drum is struck by a vehicle.
- When used in regions susceptible to freezing, drums shall have drainage holes in the bottoms so that water will not collect and freeze becoming a hazard when struck by a vehicle.
- Ballast shall not be placed on top of drums.
- Adhesives may be used to secure base of drums to pavement.



**DETECTABLE PEDESTRIAN BARRICADES**

- When existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility. Refer to WZ(BTS-2) for Pedestrian Control requirements for Sidewalk Diversions, Sidewalk Detours and Crosswalk Closures.
- Where pedestrians with visual disabilities normally use the closed sidewalk, a Detectable Pedestrian Barricade shall be placed across the full width of the closed sidewalk instead of a Type 3 Barricade.
- Detectable pedestrian barricades similar to the one pictured above, longitudinal channelizing devices, some concrete barriers, and wood or chain link fencing with a continuous detectable edging can satisfactorily delineate a pedestrian path.
- Tape, rope, or plastic chain strung between devices are not detectable, do not comply with the design standards in the "Americans with Disabilities Act Accessibility Guidelines (ADAAG)" and should not be used as a control for pedestrian movements.
- Warning lights shall not be attached to detectable pedestrian barricades.
- Detectable pedestrian barricades should use 8" nominal barricade rails as shown on BC(10) provided that the top rail provides a smooth continuous rail suitable for hand trailing with no splinters, burrs, or sharp edges.



18" x 24" Sign  
(Maximum Sign Dimension)  
Chevron CW1-8, Opposing Traffic Lane Divider, Driveway sign D70a, Keep Right R4 series or other signs as approved by Engineer



12" x 24" Vertical Panel  
mount with diagonals sloping down towards travel way

Plywood, Aluminum or Metal sign substrates shall NOT be used on plastic drums

**SIGNS, CHEVRONS, AND VERTICAL PANELS MOUNTED ON PLASTIC DRUMS**

- Signs used on plastic drums shall be manufactured using substrates listed on the CWZTCD.
- Chevrons and other work zone signs with an orange background shall be manufactured with Type B<sub>FL</sub> or Type C<sub>FL</sub> Orange sheeting meeting the color and retroreflectivity requirements of DMS-8300, "Sign Face Material," unless otherwise specified in the plans.
- Vertical Panels shall be manufactured with orange and white sheeting meeting the requirements of DMS-8300 Type A or Type B. Diagonal stripes on Vertical Panels shall slope down toward the intended traveled lane.
- Other sign messages (text or symbolic) may be used as approved by the Engineer. Sign dimensions shall not exceed 18 inches in width or 24 inches in height, except for the R9 series signs discussed in note 8 below.
- Signs shall be installed using a 1/2 inch bolt (nominal) and nut, two washers, and one locking washer for each connection.
- Mounting bolts and nuts shall be fully engaged and adequately torqued. Bolts should not extend more than 1/2 inch beyond nuts.
- Chevrons may be placed on drums on the outside of curves, on merging tapers or on shifting tapers. When used in these locations, they may be placed on every drum or spaced not more than on every third drum. A minimum of three (3) should be used at each location called for in the plans.
- R9-9, R9-10, R9-11 and R9-11a Sidewalk Closed signs which are 24 inches wide may be mounted on plastic drums, with approval of the Engineer.

SHEET 8 OF 12

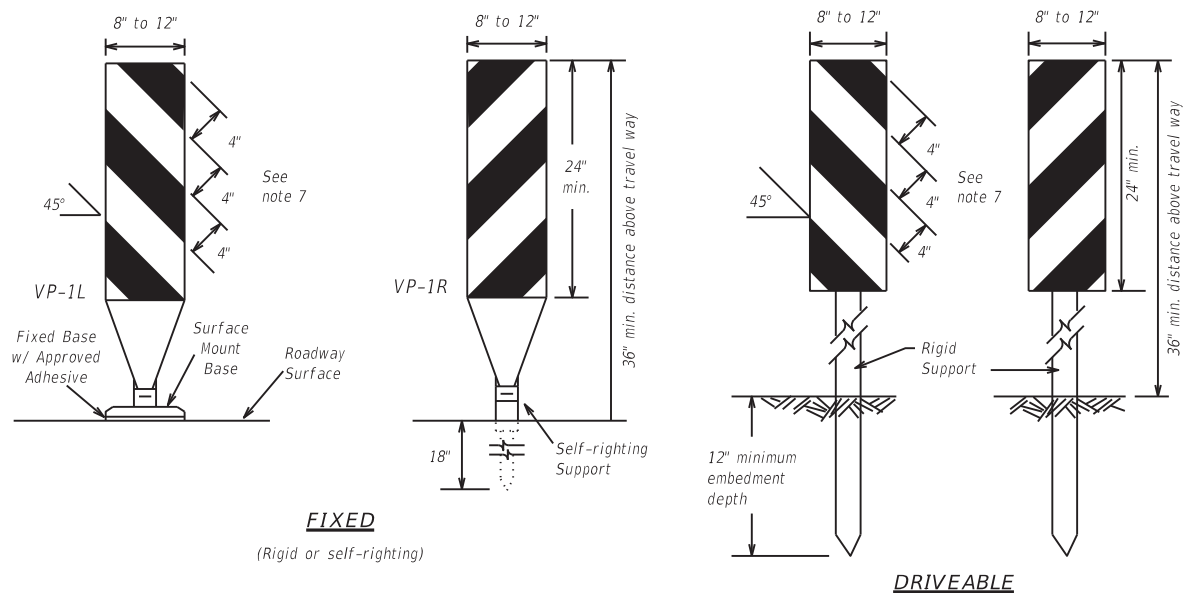


**BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES**

BC(8)-21

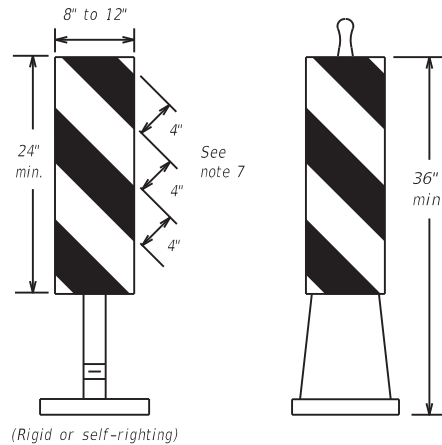
FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CK:	TxDOT
©TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		6430	55	001	FM 361				
4-03	8-14	DIST	COUNTY	SHEET NO.					
9-07	5-21	HOU	FORT BEND	13					
7-13									





**FIXED**  
(Rigid or self-righting)

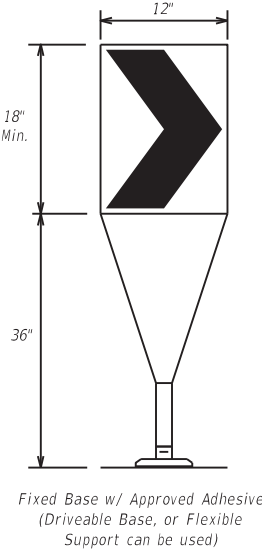
**DRIVEABLE**



**PORTABLE**  
(Rigid or self-righting)

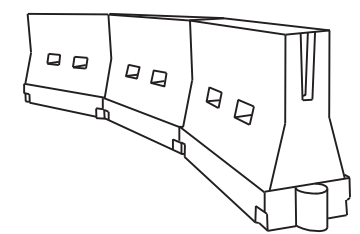
**VERTICAL PANELS (VPs)**

1. Vertical Panels (VPs) are normally used to channelize traffic or divide opposing lanes of traffic.
2. VP's may be used in daytime or nighttime situations. They may be used at the edge of shoulder drop-offs and other areas such as lane transitions where positive daytime and nighttime delineation is required. The Engineer/Inspector shall refer to the Roadway Design Manual for additional requirements on the use VP's for drop-offs.
3. VP's should be mounted back to back if used at the edge of cuts adjacent to two-way two lane roadways. Stripes are to be reflective orange and reflective white and should always slope downward toward the travel lane.
4. VP's used on expressways and freeways or other high speed roadways, may have more than 270 square inches of retroreflective area facing traffic.
5. Self-righting supports are available with portable base. See "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
6. Sheeting for the VP's shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300, unless noted otherwise.
7. Where the height of reflective material on the vertical panel is 36 inches or greater, a panel stripe of 6 inches shall be used.



1. The chevron shall be a vertical rectangle with a minimum size of 12 by 18 inches.
2. Chevrons are intended to give notice of a sharp change of alignment with the direction of travel and provide additional emphasis and guidance for vehicle operators with regard to changes in horizontal alignment of the roadway.
3. Chevrons, when used, shall be erected on the outside of a sharp curve or turn, or on the far side of an intersection. They shall be in line with and at right angles to approaching traffic. Spacing should be such that the motorist always has three in view, until the change in alignment eliminates its need.
4. To be effective, the chevron should be visible for at least 500 feet.
5. Chevrons shall be orange with a black nonreflective legend. Sheeting for the chevron shall be retroreflective Type B<sub>FL</sub> or Type C<sub>FL</sub> conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.
6. For Long Term Stationary use on tapers or transitions on freeways and divided highways, self-righting chevrons may be used to supplement plastic drums but not to replace plastic drums.

**CHEVRONS**



**LONGITUDINAL CHANNELIZING DEVICES (LCD)**

1. LCDs are crashworthy, lightweight, deformable devices that are highly visible, have good target value and can be connected together. They are not designed to contain or redirect a vehicle on impact.
2. LCDs may be used instead of a line of cones or drums.
3. LCDs shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
4. LCDs should not be used to provide positive protection for obstacles, pedestrians or workers.
5. LCDs shall be supplemented with retroreflective delineation as required for temporary barriers on BC(7) when placed roughly parallel to the travel lanes.
6. LCDs used as barricades placed perpendicular to traffic should have at least one row of reflective sheeting meeting the requirements for barricade rails as shown on BC(10). Place reflective sheeting near the top of the LCD along the full length of the device.

**WATER BALLASTED SYSTEMS USED AS BARRIERS**

1. Water ballasted systems used as barriers shall not be used solely to channelize road users, but also to protect the work space per the appropriate Manual for Assessing Safety Hardware (MASH) crashworthiness requirements based on roadway speed and barrier application.
2. Water ballasted systems used to channelize vehicular traffic shall be supplemented with retroreflective delineation or channelizing devices to improve daytime/nighttime visibility. They may also be supplemented with pavement markings.
3. Water ballasted systems used as barriers shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
4. Water ballasted systems used as barriers should not be used for a merging taper except in low speed (less than 45 MPH) urban areas. When used on a taper in a low speed urban area, the taper shall be delineated and the taper length should be designed to optimize road user operations considering the available geometric conditions.
5. When water ballasted systems used as barriers have blunt ends exposed to traffic, they should be attenuated as per manufacturer recommendations or flared to a point outside the clear zone.

If used to channelize pedestrians, longitudinal channelizing devices or water ballasted systems must have a continuous detectable bottom for users of long canes and the top of the unit shall not be less than 32 inches in height.

**HOLLOW OR WATER BALLASTED SYSTEMS USED AS LONGITUDINAL CHANNELIZING DEVICES OR BARRIERS**

**GENERAL NOTES**

1. Work Zone channelizing devices illustrated on this sheet may be installed in close proximity to traffic and are suitable for use on high or low speed roadways. The Engineer/Inspector shall ensure that spacing and placement is uniform and in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
2. Channelizing devices shown on this sheet may have a driveable, fixed or portable base. The requirement for self-righting channelizing devices must be specified in the General Notes or other plan sheets.
3. Channelizing devices on self-righting supports should be used in work zone areas where channelizing devices are frequently impacted by errant vehicles or vehicle related wind gusts making alignment of the channelizing devices difficult to maintain. Locations of these devices shall be detailed elsewhere in the plans. These devices shall conform to the TMUTCD and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
4. The Contractor shall maintain devices in a clean condition and replace damaged, nonreflective, faded, or broken devices and bases as required by the Engineer/Inspector. The Contractor shall be required to maintain proper device spacing and alignment.
5. Portable bases shall be fabricated from virgin and/or recycled rubber. The portable bases shall weigh a minimum of 30 lbs.
6. Pavement surfaces shall be prepared in a manner that ensures proper bonding between the adhesives, the fixed mount bases and the pavement surface. Adhesives shall be prepared and applied according to the manufacturer's recommendations.
7. The installation and removal of channelizing devices shall not cause detrimental effects to the final pavement surfaces, including pavement surface discoloration or surface integrity. Driveable bases shall not be permitted on final pavement surfaces. The Engineer/Inspector shall approve all application and removal procedures of fixed bases.

Posted Speed	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices	
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'
35		205'	225'	245'	35'	70'
40		265'	295'	320'	40'	80'
45	L=WS	450'	495'	540'	45'	90'
50		500'	550'	600'	50'	100'
55		550'	605'	660'	55'	110'
60		600'	660'	720'	60'	120'
65		650'	715'	780'	65'	130'
70		700'	770'	840'	70'	140'
75		750'	825'	900'	75'	150'
80		800'	880'	960'	80'	160'

\*\* Taper lengths have been rounded off.  
L=Length of Taper (FT.) W=Width of Offset (FT.)  
S=Posted Speed (MPH)

**SUGGESTED MAXIMUM SPACING OF CHANNELIZING DEVICES AND MINIMUM DESIRABLE TAPER LENGTHS**

SHEET 9 OF 12



**BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES**

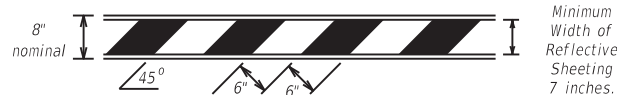
**BC(9)-21**

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CK:	TxDOT
©TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS		6430	55	001	FM 361				
9-07	8-14	DIST		COUNTY	SHEET NO.				
7-13	5-21	HOU		FORT BEND	14				

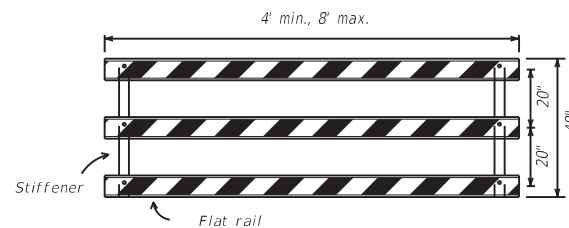
**TYPE 3 BARRICADES**

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTCD) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road, striping should slope downward in both directions toward the center of roadway.
4. Striping of rails, for the right side of the roadway, should slope downward to the left. For the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rails. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand bags shall not be stacked in a manner that covers any portion of a barricade rails reflective sheeting. Rock, concrete, iron, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed along or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheeting for barricades shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300 unless otherwise noted.

Barricades shall NOT be used as a sign support.

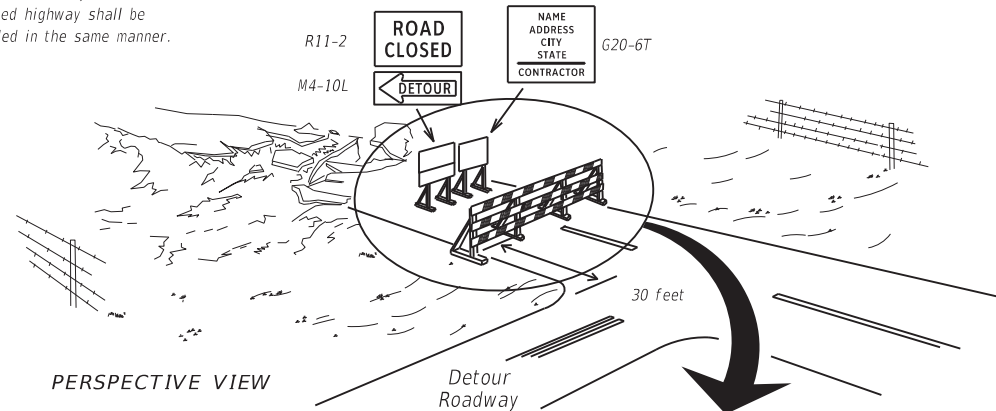


**TYPICAL STRIPING DETAIL FOR BARRICADE RAIL**



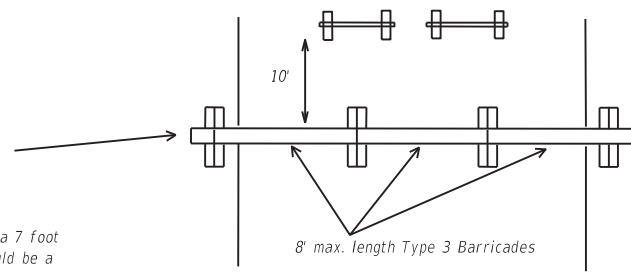
**TYPICAL PANEL DETAIL FOR SKID OR POST TYPE BARRICADES**

Each roadway of a divided highway shall be barricaded in the same manner.



**PERSPECTIVE VIEW**

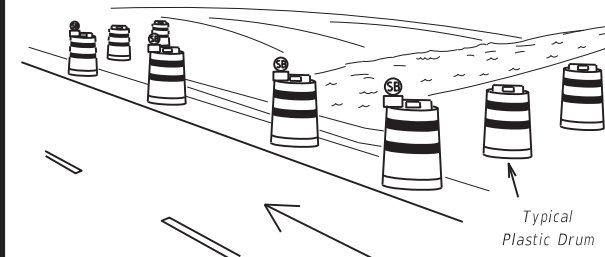
The three rails on Type 3 barricades shall be reflectorized orange and reflective white stripes on one side facing one-way traffic and both sides for two-way traffic. Barricade striping should slant downward in the direction of detour.



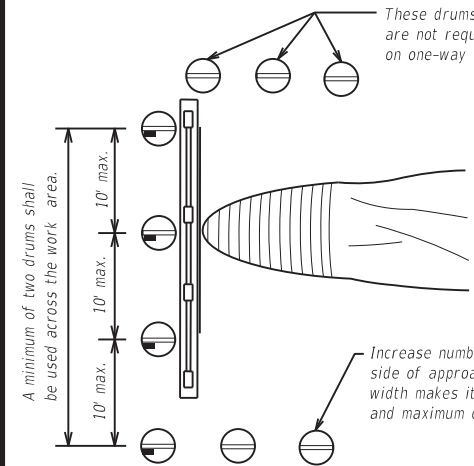
**PLAN VIEW**

1. Signs should be mounted on independent supports at a 7 foot mounting height in center of roadway. The signs should be a minimum of 10 feet behind Type 3 Barricades.
2. Advance signing shall be as specified elsewhere in the plans.

**TYPE 3 BARRICADE (POST AND SKID) TYPICAL APPLICATION**



**PERSPECTIVE VIEW**

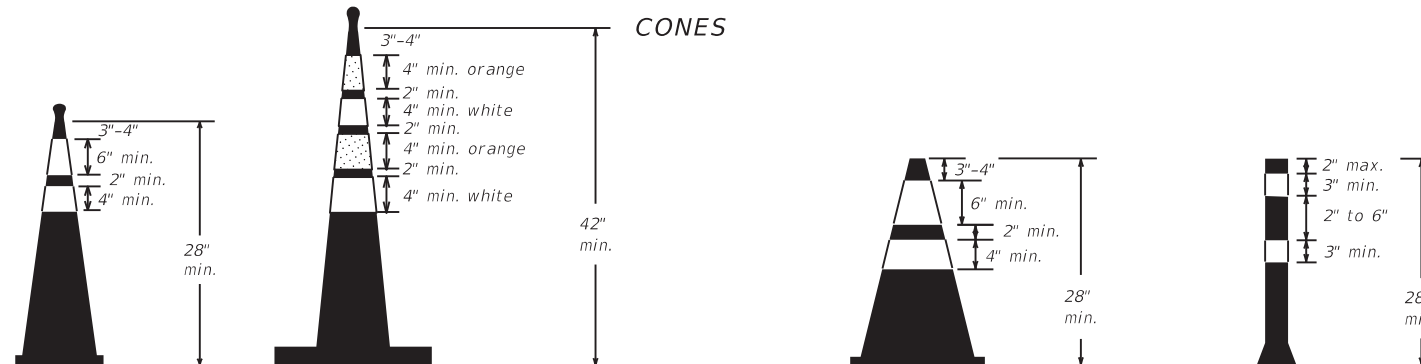


**PLAN VIEW**

**CULVERT WIDENING OR OTHER ISOLATED WORK WITHIN THE PROJECT LIMITS**

1. Where positive redirection capability is provided, drums may be omitted.
2. Plastic construction fencing may be used with drums for safety as required in the plans.
3. Vertical Panels on flexible support may be substituted for drums when the shoulder width is less than 4 feet.
4. When the shoulder width is greater than 12 feet, steady-burn lights may be omitted if drums are used.
5. Drums must extend the length of the culvert widening.

LEGEND	
	Plastic drum
	Plastic drum with steady burn light or yellow warning reflector
	Steady burn warning light or yellow warning reflector



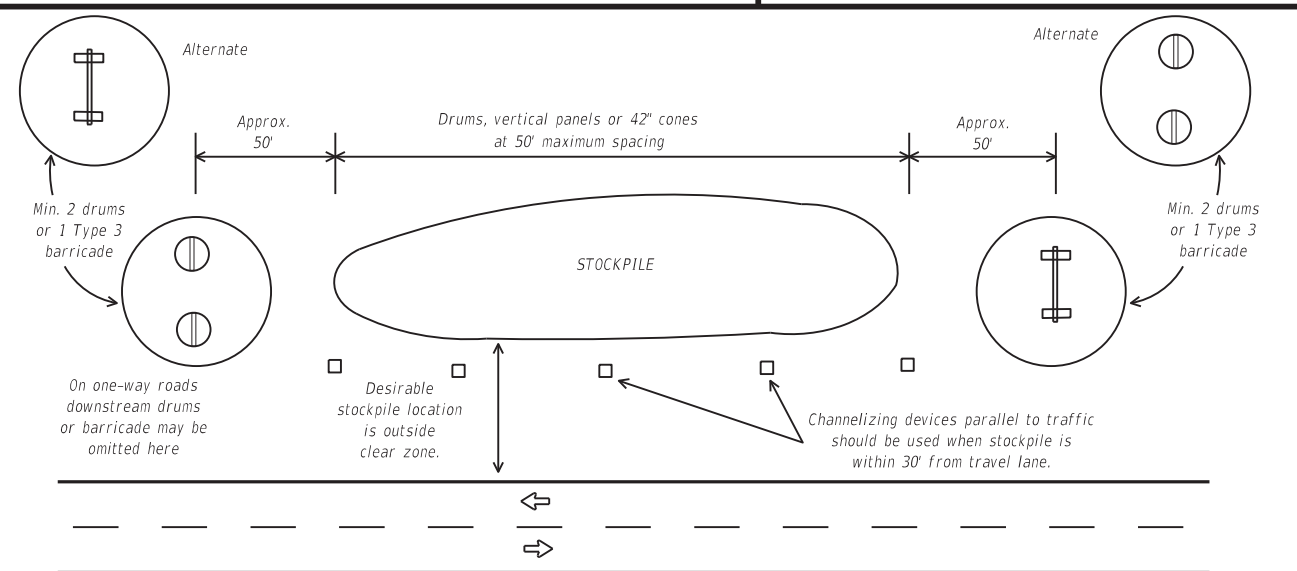
**Two-Piece cones**

**One-Piece cones**

**Tubular Marker**

28" Cones shall have a minimum weight of 9 1/2 lbs.  
42" 2-piece cones shall have a minimum weight of 30 lbs. including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A or Type B.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined in BC(4). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone durations.
7. Cones or tubular markers used on each project should be of the same size and shape.



**TRAFFIC CONTROL FOR MATERIAL STOCKPILES**



**BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES**

**BC(10)-21**

FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	HOU	FORT BEND	15	

**WORK ZONE PAVEMENT MARKINGS**

**GENERAL**

1. The Contractor shall be responsible for maintaining work zone and existing pavement markings, in accordance with the standard specifications and special provisions, on all roadways open to traffic within the CSJ limits unless otherwise stated in the plans.
2. Color, patterns and dimensions shall be in conformance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
3. Additional supplemental pavement marking details may be found in the plans or specifications.
4. Pavement markings shall be installed in accordance with the TMUTCD and as shown on the plans.
5. When short term markings are required on the plans, short term markings shall conform with the TMUTCD, the plans and details as shown on the Standard Plan Sheet WZ(STPM).
6. When standard pavement markings are not in place and the roadway is opened to traffic, DO NOT PASS signs shall be erected to mark the beginning of the sections where passing is prohibited and PASS WITH CARE signs at the beginning of sections where passing is permitted.
7. All work zone pavement markings shall be installed in accordance with Item 662, "Work Zone Pavement Markings."

**RAISED PAVEMENT MARKERS**

1. Raised pavement markers are to be placed according to the patterns on BC(12).
2. All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and Departmental Material Specification DMS-4200 or DMS-4300.

**PREFABRICATED PAVEMENT MARKINGS**

1. Removable prefabricated pavement markings shall meet the requirements of DMS-8241.
2. Non-removable prefabricated pavement markings (foil back) shall meet the requirements of DMS-8240.

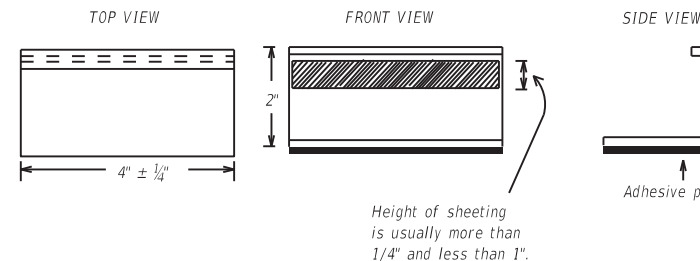
**MAINTAINING WORK ZONE PAVEMENT MARKINGS**

1. The Contractor will be responsible for maintaining work zone pavement markings within the work limits.
2. Work zone pavement markings shall be inspected in accordance with the frequency and reporting requirements of work zone traffic control device inspections as required by Form 599.
3. The markings should provide a visible reference for a minimum distance of 300 feet during normal daylight hours and 160 feet when illuminated by automobile low-beam headlights at night, unless sight distance is restricted by roadway geometrics.
4. Markings failing to meet this criteria within the first 30 days after placement shall be replaced at the expense of the Contractor as per Specification Item 662.

**REMOVAL OF PAVEMENT MARKINGS**

1. Pavement markings that are no longer applicable, could create confusion or direct a motorist toward or into the closed portion of the roadway shall be removed or obliterated before the roadway is opened to traffic.
2. The above shall not apply to detours in place for less than three days, where flaggers and/or sufficient channelizing devices are used in lieu of markings to outline the detour route.
3. Pavement markings shall be removed to the fullest extent possible, so as not to leave a discernable marking. This shall be by any method approved by TxDOT Specification Item 677 for "Eliminating Existing Pavement Markings and Markers".
4. The removal of pavement markings may require resurfacing or seal coating portions of the roadway as described in Item 677.
5. Subject to the approval of the Engineer, any method that proves to be successful on a particular type pavement may be used.
6. Blast cleaning may be used but will not be required unless specifically shown in the plans.
7. Over-painting of the markings SHALL NOT BE permitted.
8. Removal of raised pavement markers shall be as directed by the Engineer.
9. Removal of existing pavement markings and markers will be paid for directly in accordance with Item 677, "ELIMINATING EXISTING PAVEMENT MARKINGS AND MARKERS," unless otherwise stated in the plans.
10. Black-out marking tape may be used to cover conflicting existing markings for periods less than two weeks when approved by the Engineer.

**Temporary Flexible-Reflective Roadway Marker Tabs**



**STAPLES OR NAILS SHALL NOT BE USED TO SECURE TEMPORARY FLEXIBLE-REFLECTIVE ROADWAY MARKER TABS TO THE PAVEMENT SURFACE**

1. Temporary flexible-reflective roadway marker tabs used as guidemarks shall meet the requirements of DMS-8242.
2. Tabs detailed on this sheet are to be inspected and accepted by the Engineer or designated representative. Sampling and testing is not normally required, however at the option of the Engineer, either "A" or "B" below may be imposed to assure quality before placement on the roadway.
  - A. Select five (5) or more tabs at random from each lot or shipment and submit to the Construction Division, Materials and Pavement Section to determine specification compliance.
  - B. Select five (5) tabs and perform the following test. Affix five (5) tabs at 24 inch intervals on an asphaltic pavement in a straight line. Using a medium size passenger vehicle or pickup, run over the markers with the front and rear tires at a speed of 35 to 40 miles per hour, four (4) times in each direction. No more than one (1) out of the five (5) reflective surfaces shall be lost or displaced as a result of this test.
3. Small design variances may be noted between tab manufacturers.
4. See Standard Sheet WZ(STPM) for tab placement on new pavements. See Standard Sheet TCP(7-1) for tab placement on seal coat work.

**RAISED PAVEMENT MARKERS USED AS GUIDEMARKS**

1. Raised pavement markers used as guidemarks shall be from the approved product list, and meet the requirements of DMS-4200.
2. All temporary construction raised pavement markers provided on a project shall be of the same manufacturer.
3. Adhesive for guidemarks shall be bituminous material hot applied or butyl rubber pad for all surfaces, or thermoplastic for concrete surfaces.

Guidemarks shall be designated as:  
 YELLOW - (two amber reflective surfaces with yellow body).  
 WHITE - (one silver reflective surface with white body).

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
TRAFFIC BUTTONS	DMS-4300
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240
TEMPORARY REMOVABLE, PREFABRICATED PAVEMENT MARKINGS	DMS-8241
TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS	DMS-8242

A list of prequalified reflective raised pavement markers, non-reflective traffic buttons, roadway marker tabs and other pavement markings can be found at the Material Producer List web address shown on BC(1).

SHEET 11 OF 12

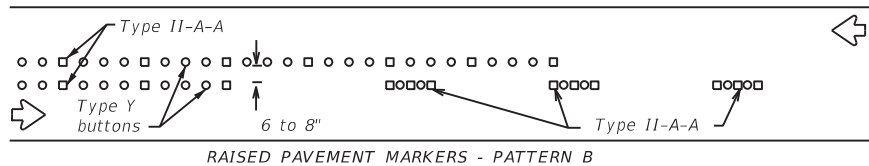
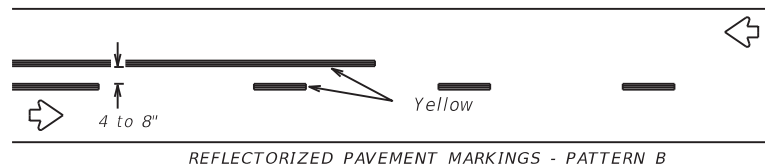
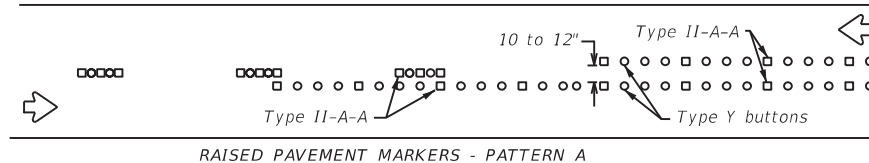


**BARRICADE AND CONSTRUCTION PAVEMENT MARKINGS**

**BC(11)-21**

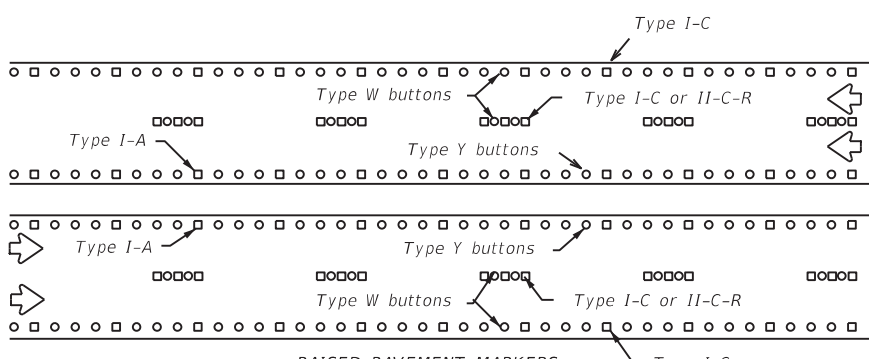
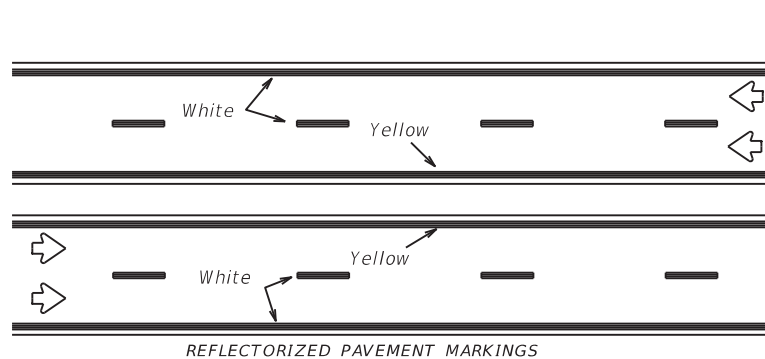
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
	6430	55	001	FM 361
2-98 9-07 5-21	REVISIONS		DIST	COUNTY
1-02 7-13			HOU	FORT BEND
11-02 8-14				SHEET NO.
				16

### PAVEMENT MARKING PATTERNS



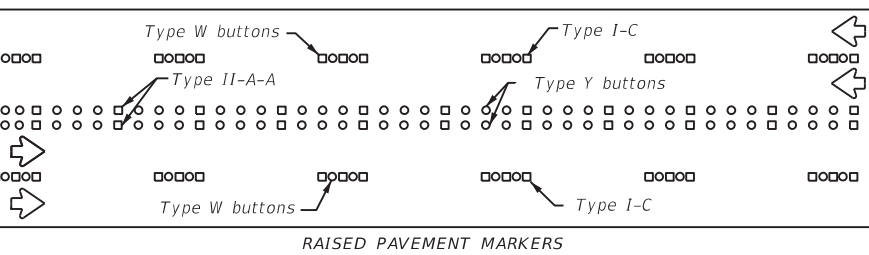
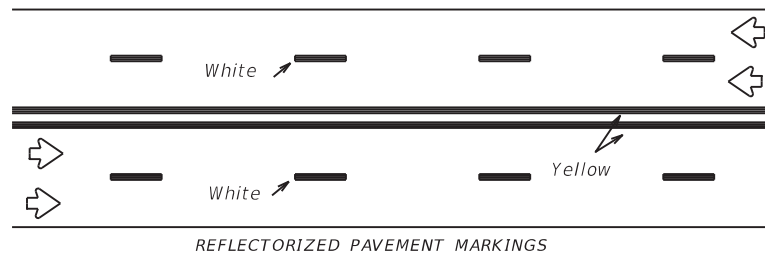
Pattern A is the TXDOT Standard, however Pattern B may be used if approved by the Engineer. Prefabricated markings may be substituted for reflectorized pavement markings.

### CENTER LINE & NO-PASSING ZONE BARRIER LINES FOR TWO-LANE, TWO-WAY HIGHWAYS



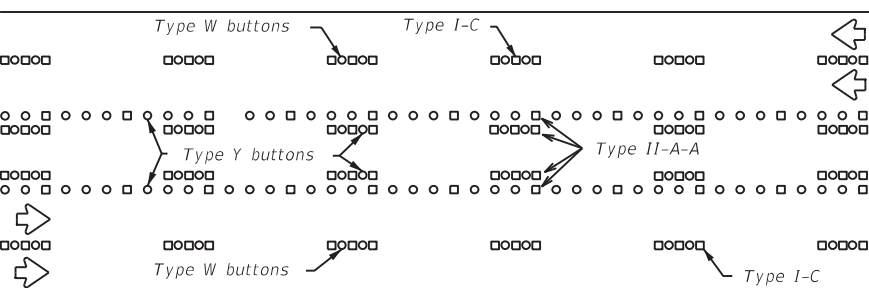
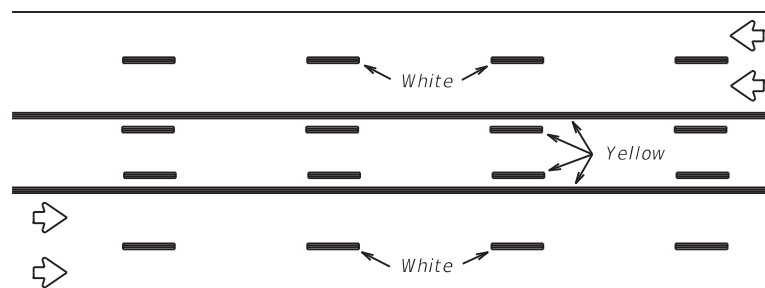
Prefabricated markings may be substituted for reflectorized pavement markings.

### EDGE & LANE LINES FOR DIVIDED HIGHWAY



Prefabricated markings may be substituted for reflectorized pavement markings.

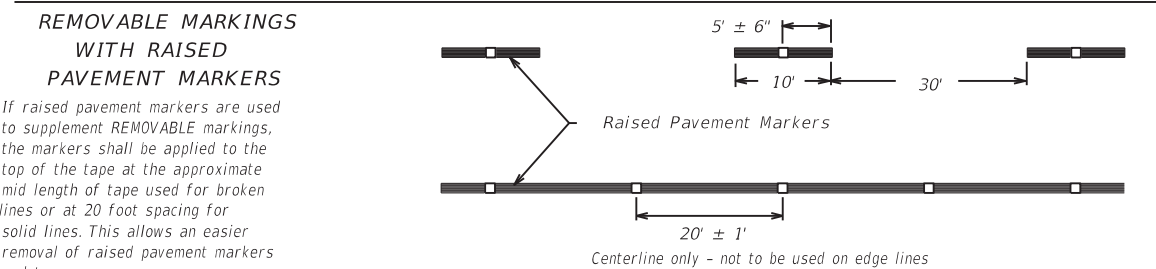
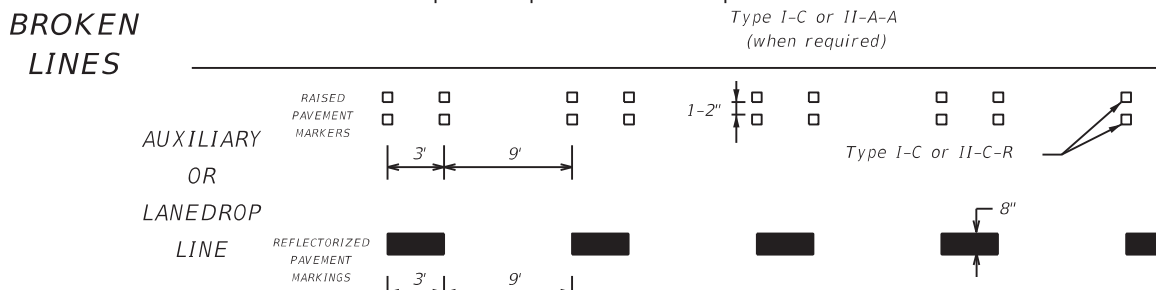
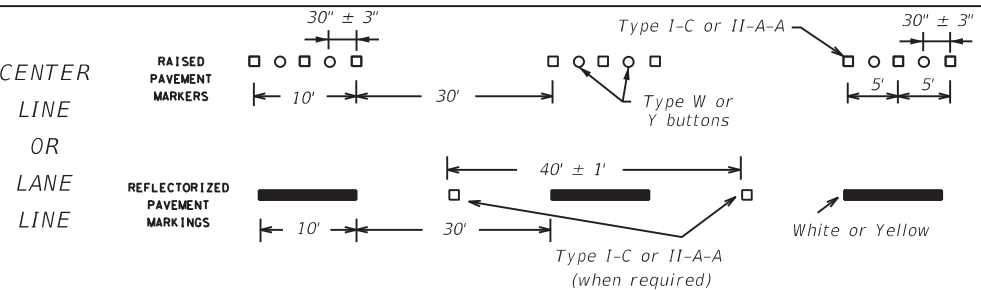
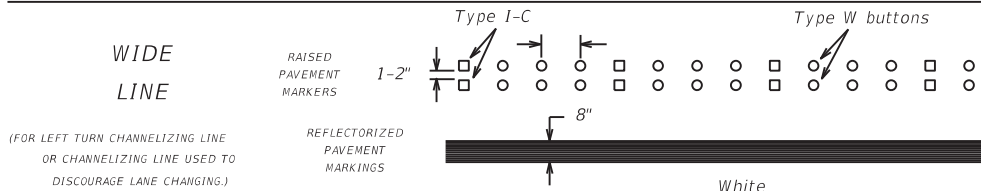
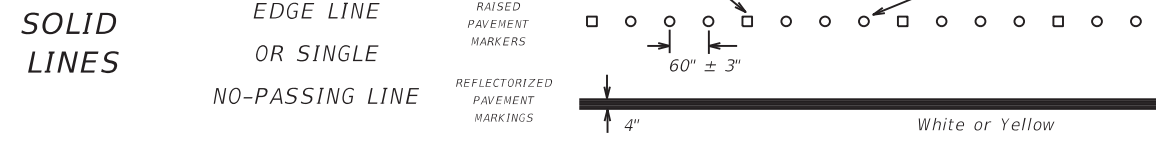
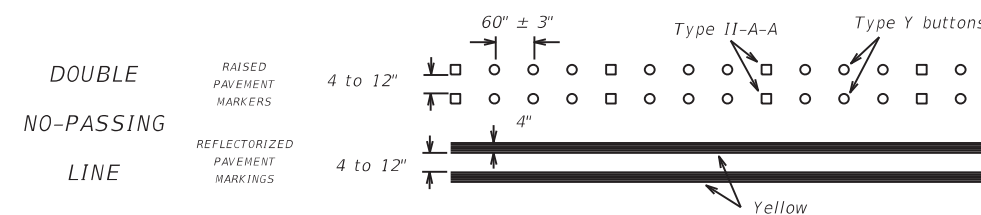
### LANE & CENTER LINES FOR MULTILANE UNDIVIDED HIGHWAYS



Prefabricated markings may be substituted for reflectorized pavement markings.

### TWO-WAY LEFT TURN LANE

### STANDARD WORK ZONE PAVEMENT MARKINGS DETAILS



If raised pavement markers are used to supplement REMOVABLE markings, the markers shall be applied to the top of the tape at the approximate mid length of tape used for broken lines or at 20 foot spacing for solid lines. This allows an easier removal of raised pavement markers and tape.

SHEET 12 OF 12

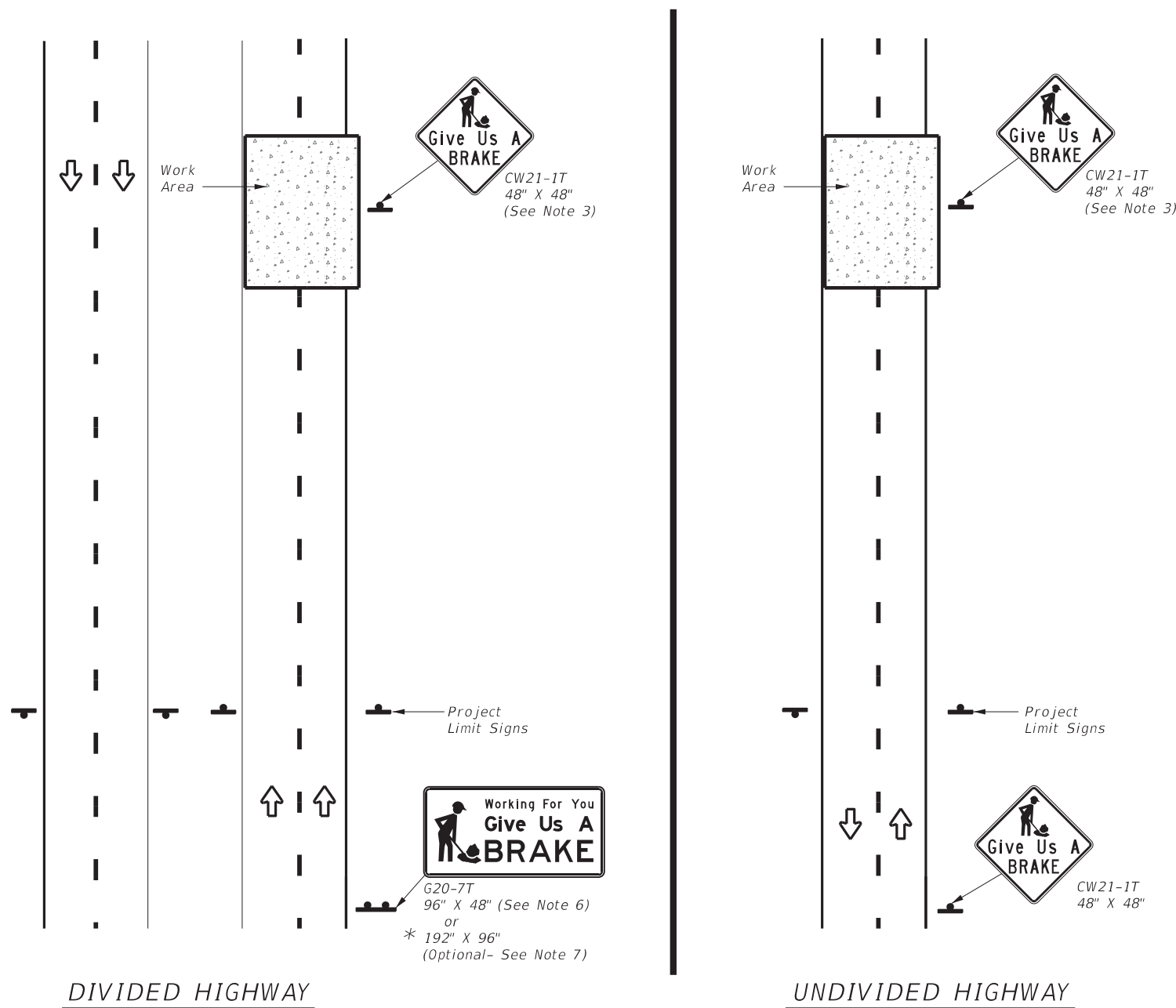


### BARRICADE AND CONSTRUCTION PAVEMENT MARKING PATTERNS

BC(12)-21

Raised pavement markers used as standard pavement markings shall be from the approved products list and meet the requirements of Item 672 "RAISED PAVEMENT MARKERS."

FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
1-97 9-07 5-21	DIST	COUNTY	SHEET NO.	
2-98 7-13	HOU	FORT BEND	17	
11-02 8-14				



SIGNS ARE SHOWN FOR ONE DIRECTION OF TRAVEL

\* When the optional larger WORKING FOR YOU GIVE US A BRAKE (G20-7T) 192" x 96" sign is required, the locations shall be noted elsewhere in the plans.

SUMMARY OF LARGE SIGNS									
BACKGROUND COLOR	SIGN DESIGNATION	SIGN	SIGN DIMENSIONS	REFLECTIVE SHEETING	SQ FT	GALVANIZED STRUCTURAL STEEL		DRILLED SHAFT	
						Size	(LF)		24" DIA. (LF)
Orange	G20-7T		96" X 48"	Type B <sub>FL</sub> or C <sub>FL</sub>	32	▲	▲	▲	▲
Orange	G20-7T		192" X 96"	Type B <sub>FL</sub> or C <sub>FL</sub>	128	W8x18	16	17	12

▲ See Note 6 Below

LEGEND	
	Sign
	Large Sign
	Traffic Flow

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PLYWOOD SIGN BLANKS	DMS-7100
ALUMINUM SIGN BLANKS	DMS-7110
SIGN FACE MATERIALS	DMS-8300

COLOR	USAGE	SHEETING MATERIAL
ORANGE	BACKGROUND	TYPE B <sub>FL</sub> OR TYPE C <sub>FL</sub>
BLACK	LEGEND & BORDERS	NON-REFLECTIVE ACRYLIC FILM

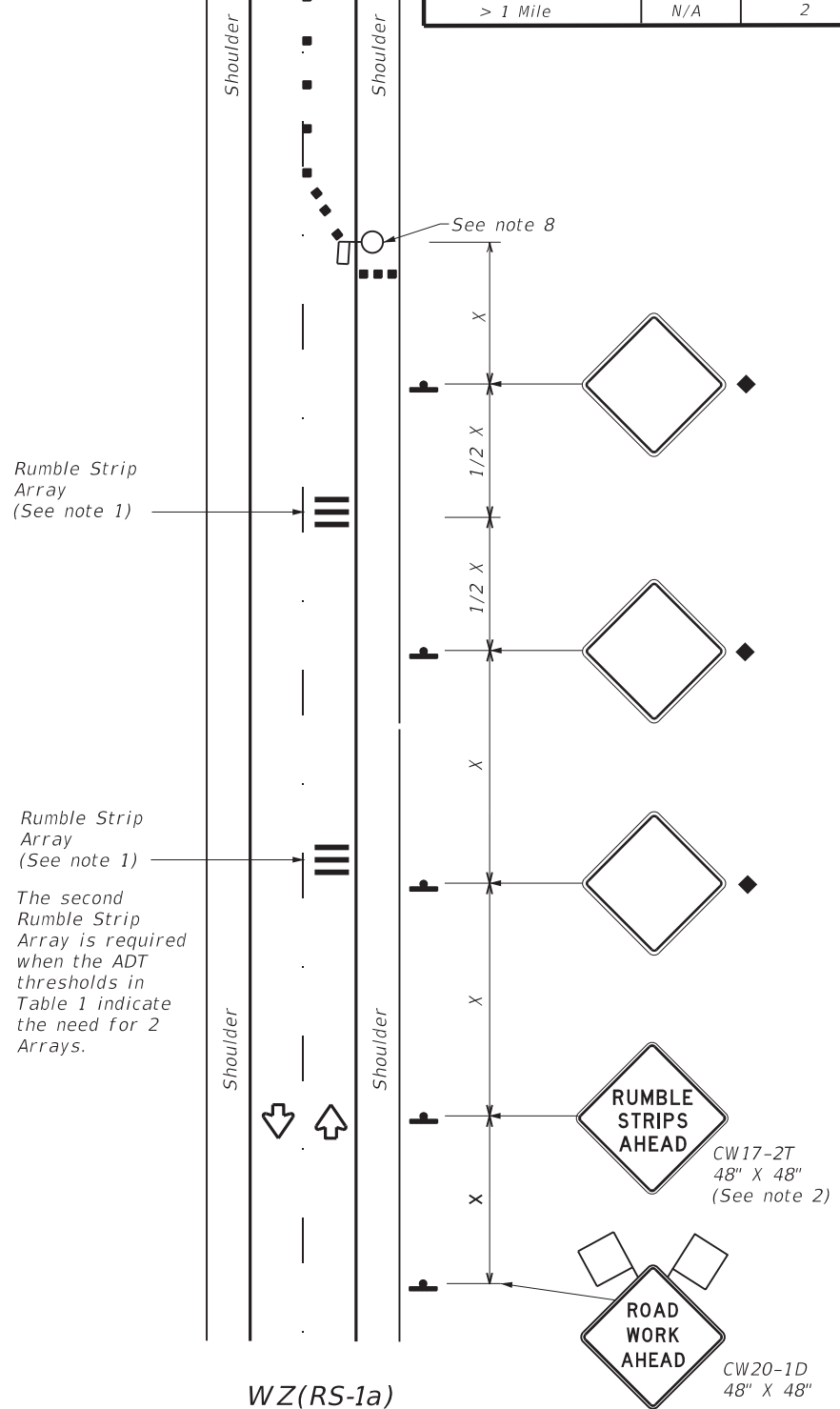
GENERAL NOTES

1. See BC and SMD sheets for additional sign support details.
2. Sign locations shall be approved by the Engineer.
3. For projects more than two miles in length, Give Us a BRAKE signs should be repeated halfway through the project. The Give Us a Brake (CW21-1T) may be used for this purpose.
4. Work zone speed limits are sometimes used in conjunction with GIVE US A BRAKE signing. See BC(3) for location and spacing of construction speed zone signing when required.
5. Give Us a Brake (CW21-1T) signs and supports shall be considered subsidiary to Item 502, "Barricades, Signs and Traffic Handling."
6. The 96" X 48" Working For You Give Us A BRAKE (G20-7T) may use a 1/2" or 5/8" plywood substrate or 0.125" aluminum sheeting substrate and may be supported by two 4" x 6" wood posts with drilled holes for breakaway as per BC(5) and will be subsidiary to Item 502.
7. The Working For You Give Us A BRAKE (G20-7T) 192" X 96" sign shall be paid for under the following specification items:  
 Item 636 - Aluminum Signs  
 Item 647 - Large Roadside Sign Supports and Assemblies.  
 Item 416 - Drilled Shaft Foundations
8. All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.

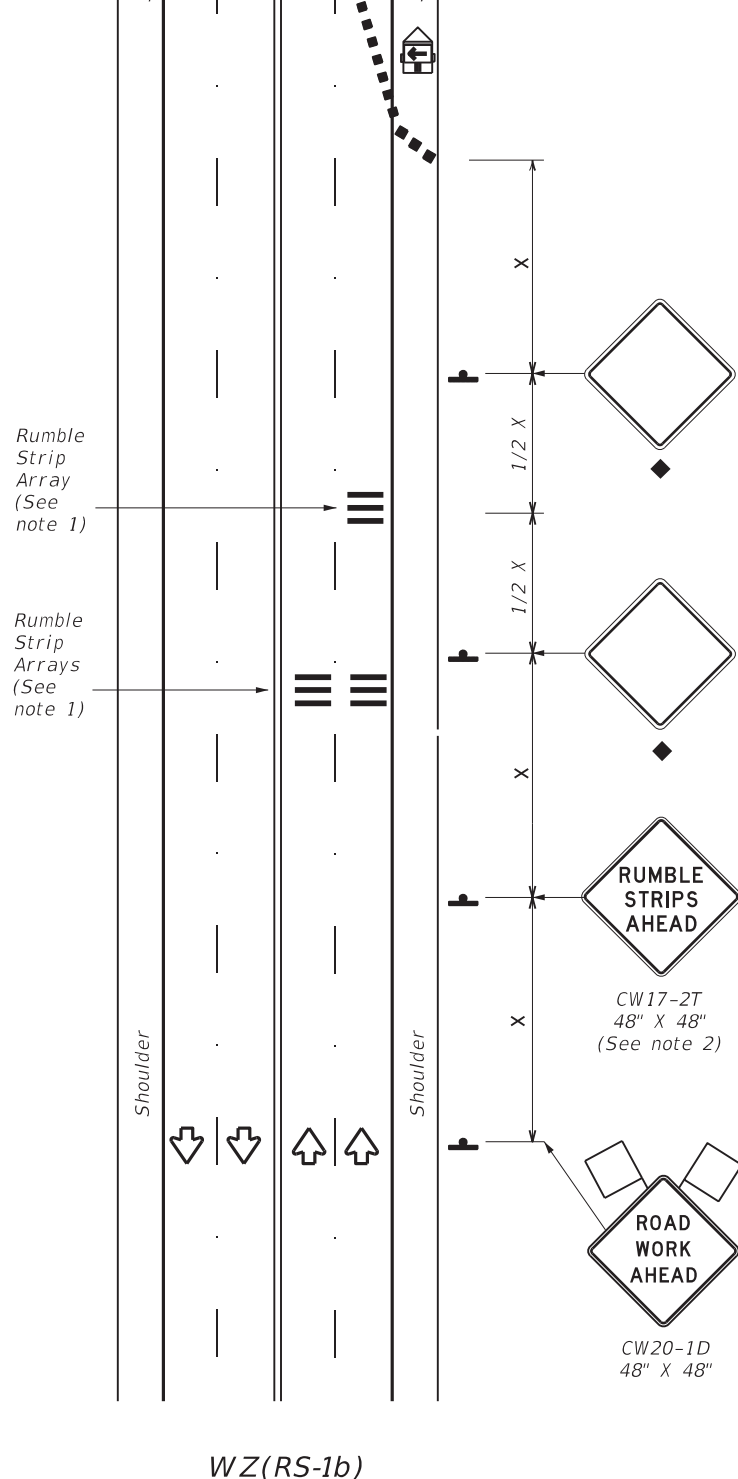
				Traffic Operations Division Standard	
<b>WORK ZONE "GIVE US A BRAKE" SIGNS</b>					
<b>WZ(BRK)-13</b>					
FILE:	wzbrk-13.dgn	DN:	TxDOT	CK:	TxDOT
©TxDOT	August 1995	CONT:	6430	SECT:	55
REVISIONS		JOB:	001		HIGHWAY:
		DIST:	COUNTY		SHEET NO.
6-96	5-98	7-13			
8-96	3-03		HOU	FORT BEND	18

Warning sign and rumble strip sequence in opposite direction is same as below.

Flagger to Flagger (Length of Work Area)	ADT	# of Rumble Strip Arrays
1/8 Mile	< 4,500	1
	≥ 4,500	2
1/4 Mile	< 3,500	1
	≥ 3,500	2
1/2 Mile	< 2,600	1
	≥ 2,600	2
1 Mile	< 1,600	1
	≥ 1,600	2
> 1 Mile	N/A	2



**RUMBLE STRIPS ON ONE-LANE TWO-WAY APPLICATION**



**RUMBLE STRIPS FOR LANE CLOSURE ON CONVENTIONAL ROADWAY**

**GENERAL NOTES**

- Each Rumble Strip Array should consist of three rumble strips spaced center to center at the spacing shown in Table 2, placed transverse across the lane at locations shown.
- The CW17-2T "RUMBLE STRIPS AHEAD" sign should be located after the CW20-1D "ROAD WORK AHEAD" sign and spaced as shown. If traffic is observed to be queuing, or is expected to queue beyond the Rumble Strips, the CW17-2T sign and the first Rumble Strip Array may be located upstream of the CW20-1D sign as necessary to provide needed warning.
- Temporary Rumble Strips will be considered subsidiary to Item 502, and shall be a product listed on the Compliant Work Zone Traffic Control Devices.
- Remove Temporary Rumble Strips before removing the advanced warning signs.
- Temporary Rumble Strips should not be used on horizontal curves, loose gravel, soft or bleeding asphalt, heavily rutted pavements or unpaved surfaces.
- Temporary Rumble Strips shall be installed and maintained as per manufacturer's recommendations.
- This standard sheet shall be used in conjunction with other appropriate TCP standard, TMUTCD typical application or project specific detail for the project.
- The one-lane two-way application may utilize a flagger, an Automated Flagger Assistance Device (AFAD) or a Portable Traffic Signal (PTS).
- Replace defective Temporary Rumble Strips as directed by the Engineer.
- Temporary Rumble Strips may be used on freeways or expressways based on engineering judgment and written direction from the Engineer.

Speed	Approximate distance between strips in an array
≤ 40 MPH	10'
> 40 MPH & ≤ 55 MPH	15'
= 60 MPH	20'
≥ 65 MPH	* 35'+

	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Panel		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L=WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

\* Conventional Roads Only  
 \*\* Taper lengths have been rounded off.  
 L=Length of Taper(FT) W=Width of Offset(FT)  
 S=Posted Speed(MPH)

MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

◆ Signs are for illustrative purposes only. Signs required may vary depending on the TCP, TMUTCD Typical Application, or project specific details for the project.

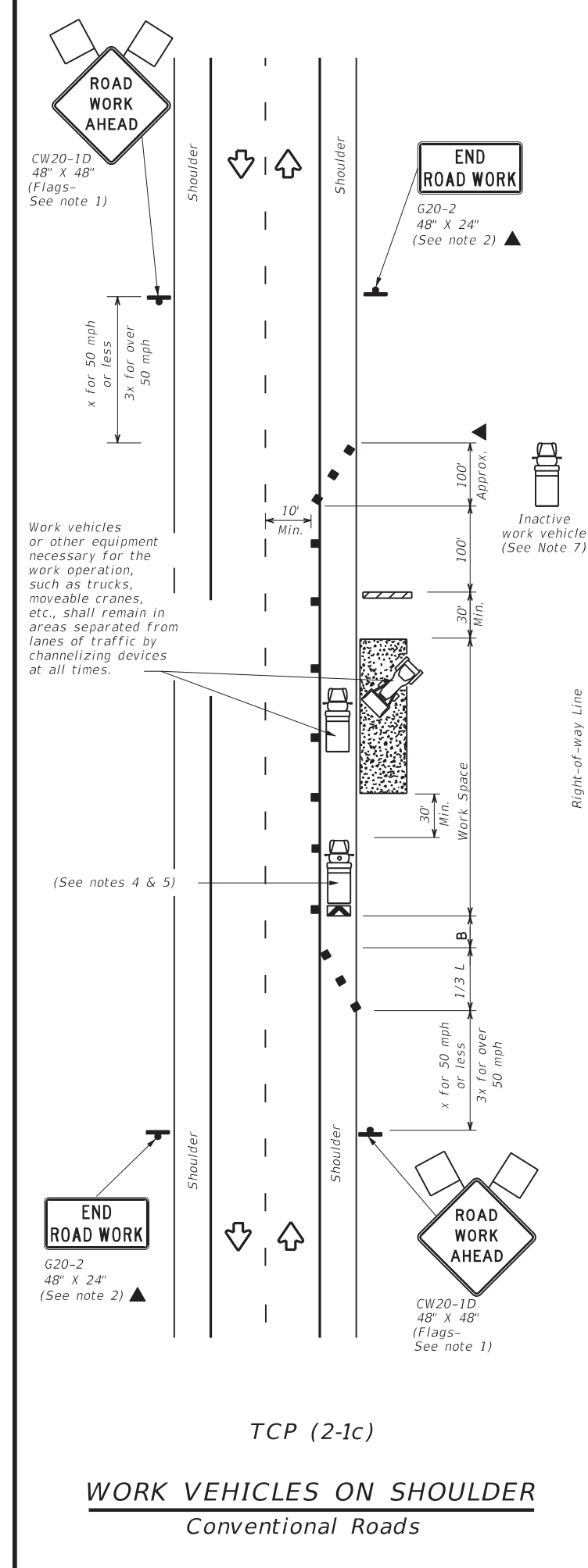
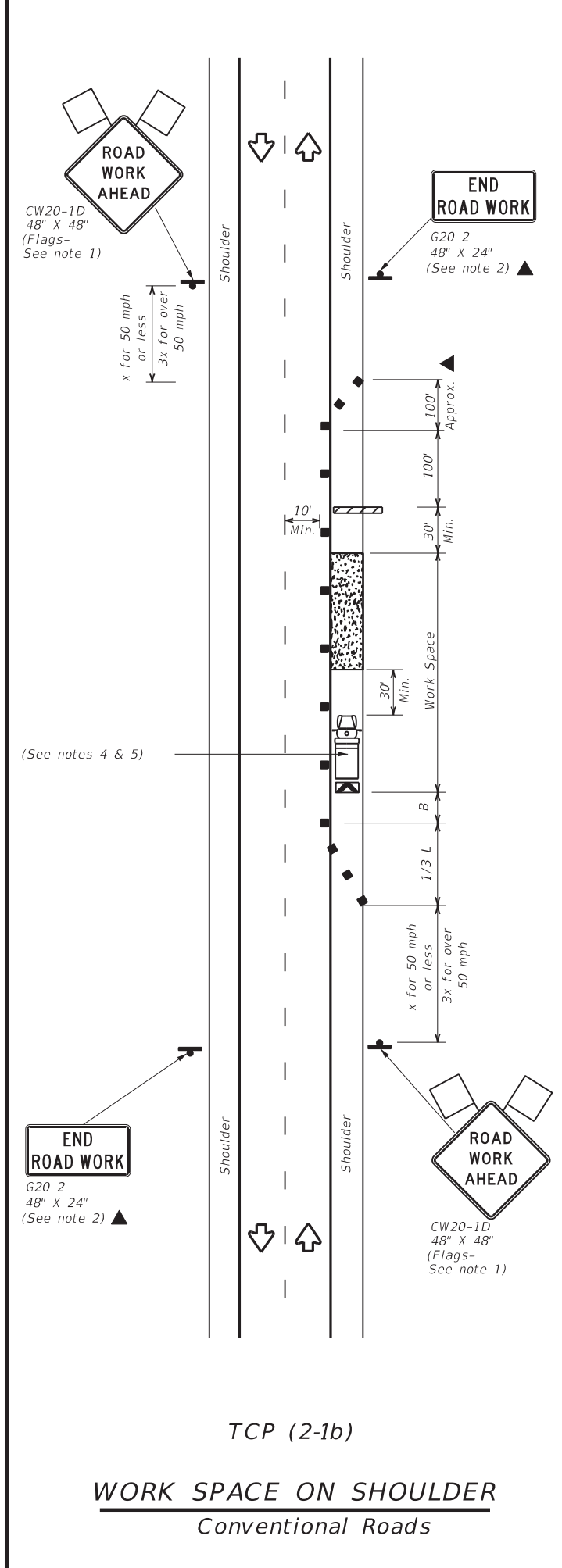
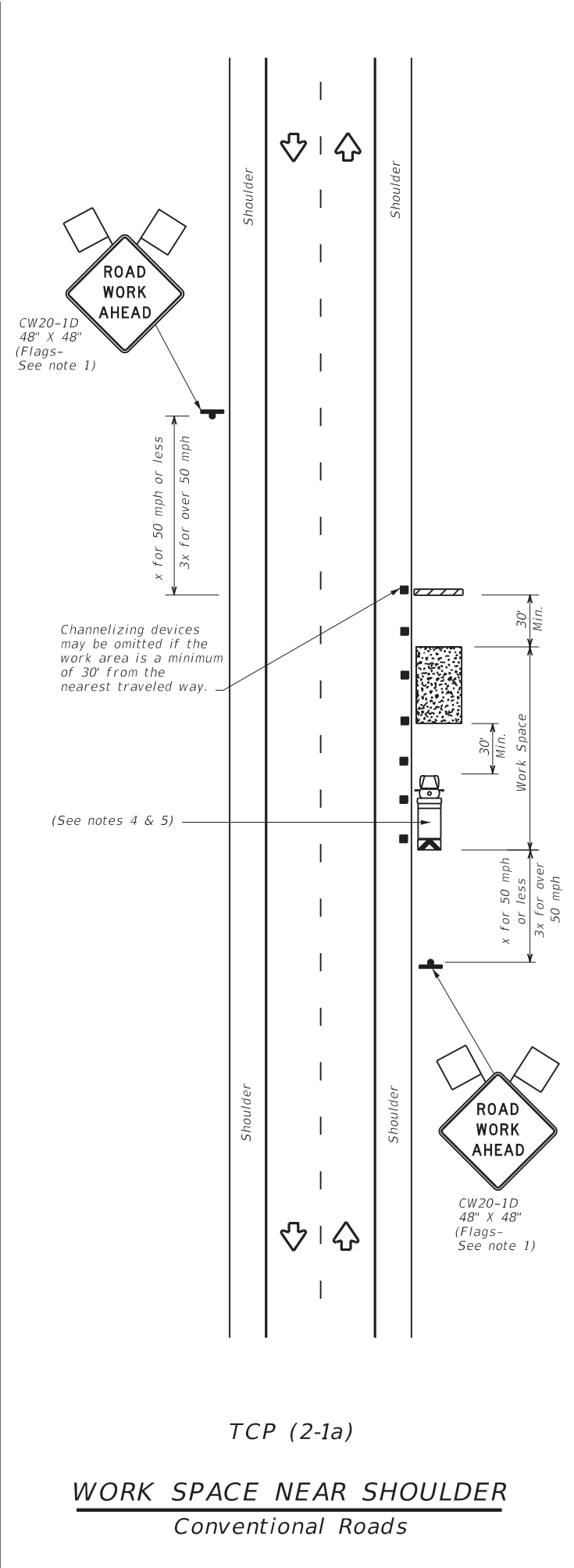
\* For posted speeds in excess of 65 MPH, it is recommended that spacing is increased as speed limits increase. Increasing space between rumble strips will improve effectiveness.

Texas Department of Transportation  
 Traffic Safety Division Standard

**TEMPORARY RUMBLE STRIPS**

**WZ(RS)-22**

FILE: wzrs22.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT November 2012	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
2-14 1-22	DIST	COUNTY	SHEET NO.	
4-16	HOU	FORT BEND	19	



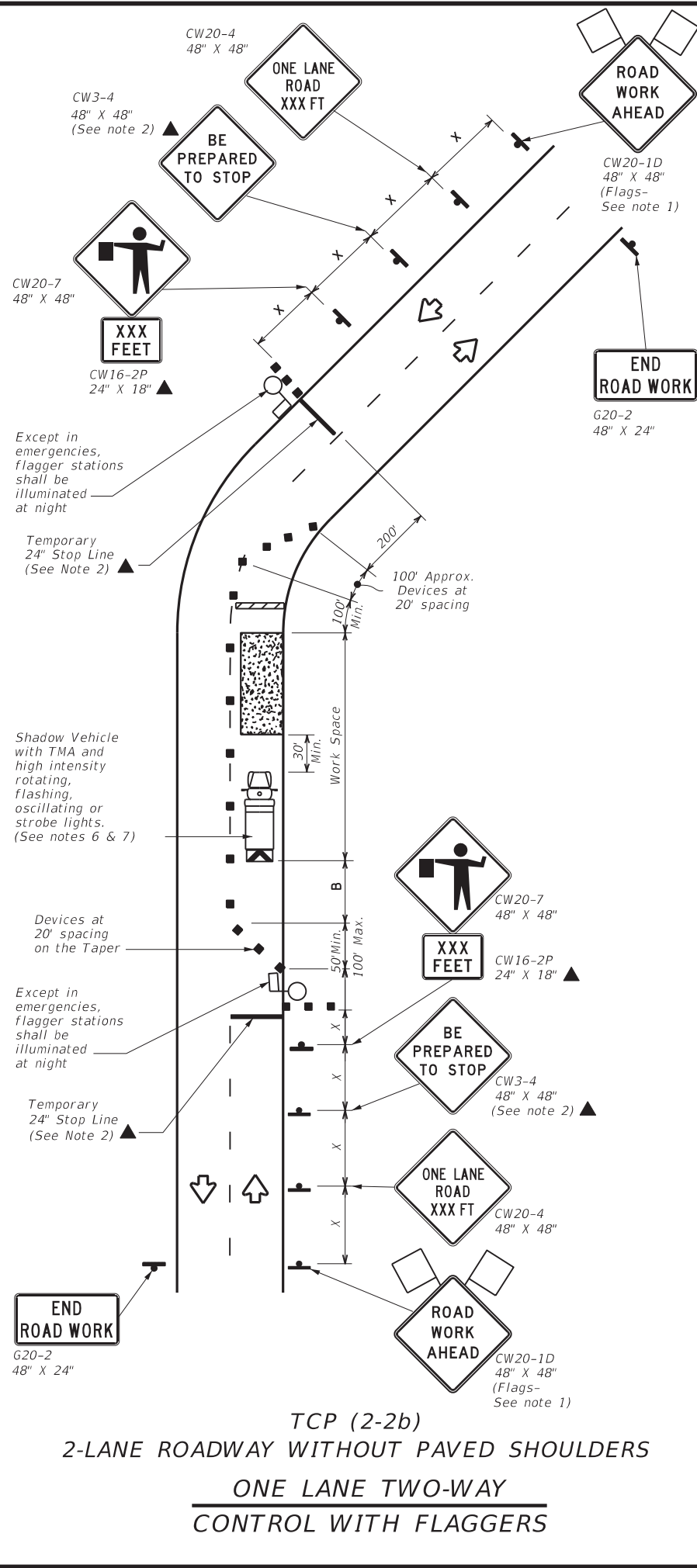
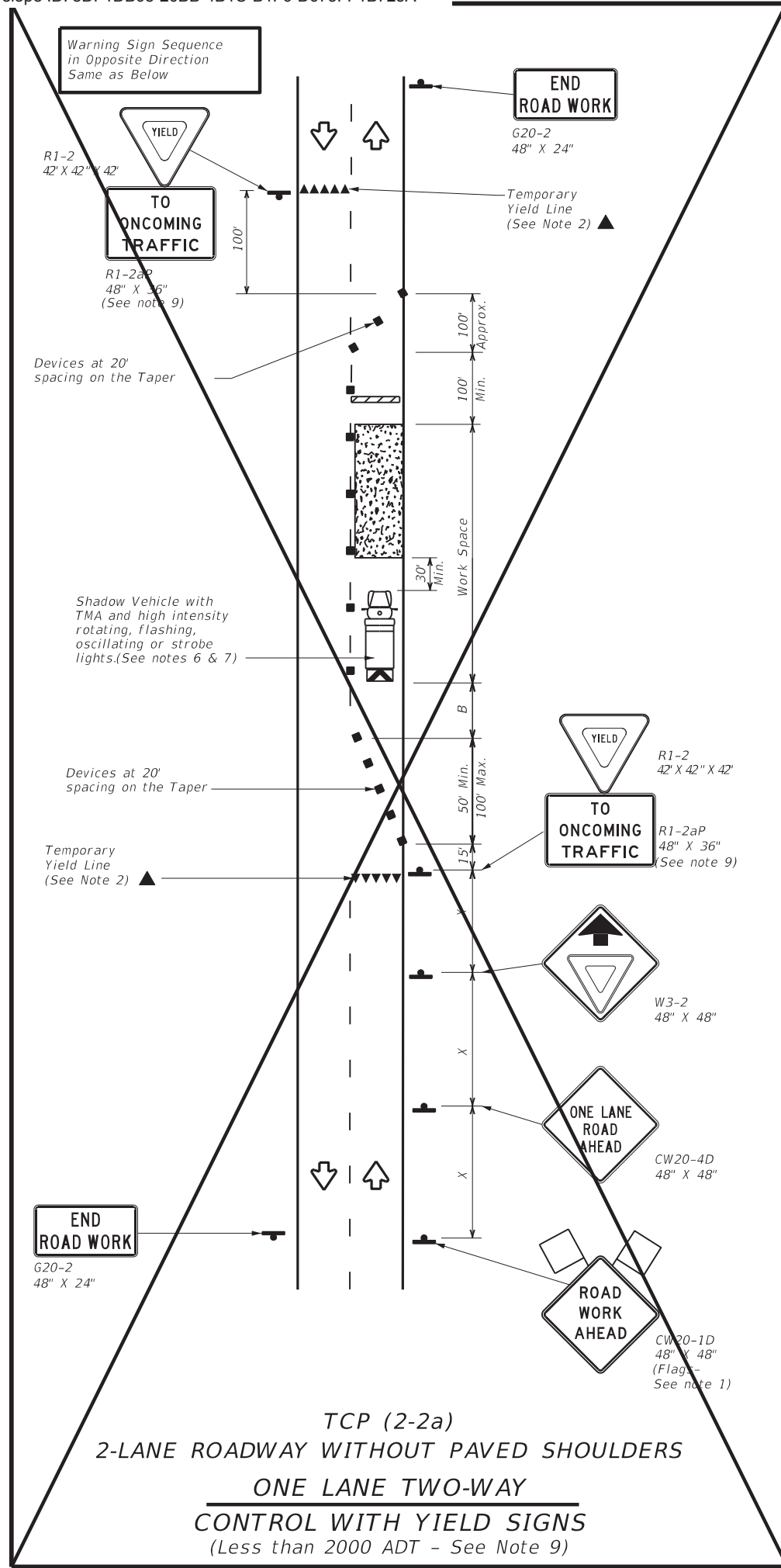
LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths * *			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40	$L = WS$	265'	295'	320'	40'	80'	240'	155'
45		450'	495'	540'	45'	90'	320'	195'
50	$L = WS$	500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60	$L = WS$	600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70	$L = WS$	700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

\* Conventional Roads Only  
 \*\* Taper lengths have been rounded off.  
 L=Length of Taper(FT) W=Width of Offset(FT) S=Posted Speed(MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	✓

- GENERAL NOTES**
- Flags attached to signs where shown, are REQUIRED.
  - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated in the plans, or for routine maintenance work, when approved by the Engineer.
  - Stockpiled material should be placed a minimum of 30 feet from nearest traveled way.
  - Shadow Vehicle with TMA and high intensity rotating, flashing, oscillating or strobe lights. A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
  - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.
  - See TCP(5-1) for shoulder work on divided highways, expressways and freeways.
  - Inactive work vehicles or other equipment should be parked near the right-of-way line and not parked on the paved shoulder.
  - CW21-5 "SHOULDER WORK" signs may be used in place of CW21-1D "ROAD WORK AHEAD" signs for shoulder work on conventional roadways.



LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"	Stopping Sight Distance
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent			
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'	200'
35		205'	225'	245'	35'	70'	160'	120'	250'
40		265'	295'	320'	40'	80'	240'	155'	305'
45	L=WS	450'	495'	540'	45'	90'	320'	195'	360'
50		500'	550'	600'	50'	100'	400'	240'	425'
55		550'	605'	660'	55'	110'	500'	295'	495'
60		600'	660'	720'	60'	120'	600'	350'	570'
65		650'	715'	780'	65'	130'	700'	410'	645'
70		700'	770'	840'	70'	140'	800'	475'	730'
75		750'	825'	900'	75'	150'	900'	540'	820'

\* Conventional Roads Only  
 \*\* Taper lengths have been rounded off.  
 L=Length of Taper(FT) W=Width of Offset(FT) S=Posted Speed(MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	

### GENERAL NOTES

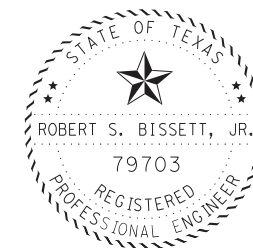
- Flags attached to signs where shown, are REQUIRED.
- All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
- The CW3-4 "BE PREPARED TO STOP" sign may be installed after the CW20-4 "ONE LANE ROAD XXX FT" sign, but proper sign spacing shall be maintained.
- Flaggers should use two-way radios or other methods of communication to control traffic.
- Length of work space should be based on the ability of flaggers to communicate.
- A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
- Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.

### TCP (2-2a)

- The R1-2 "YIELD" sign traffic control may be used on projects with approaches that have adequate sight distance. For projects in urban areas, work space should be no longer than one half city block. In rural areas, roadways with less than 2000 ADT, work space should be no longer than 400 feet.
- The R1-2aP "YIELD TO ONCOMING TRAFFIC" sign shall be placed on a support at a 7 foot minimum mounting height.

### TCP (2-2b)

- Channelizing devices on the center line may be omitted when a pilot car is leading traffic and approved by the Engineer.
- If the work space is located near a horizontal or vertical curve, the buffer distances should be increased in order to maintain stopping sight distance to the flagger and a queue of stopped vehicles. (See table above).
- Flaggers should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.



Robert S. Bissett, Jr.

01/10/23

			<b>Traffic Operations Division Standard</b>	
<b>TRAFFIC CONTROL PLAN</b> <b>ONE-LANE TWO-WAY</b> <b>TRAFFIC CONTROL</b>				
<b>TCP(2-2)-18 (MOD)</b>				
FILE: tcp2-2-18.dgn	DN:	CK:	DW:	CK:
©TxDOT December 1985	CONV	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
8-95 3-03 1-97 2-12 4-98 2-18	DIST	COUNTY	SHEET NO.	
	HOU	FORT BEND	21	

162



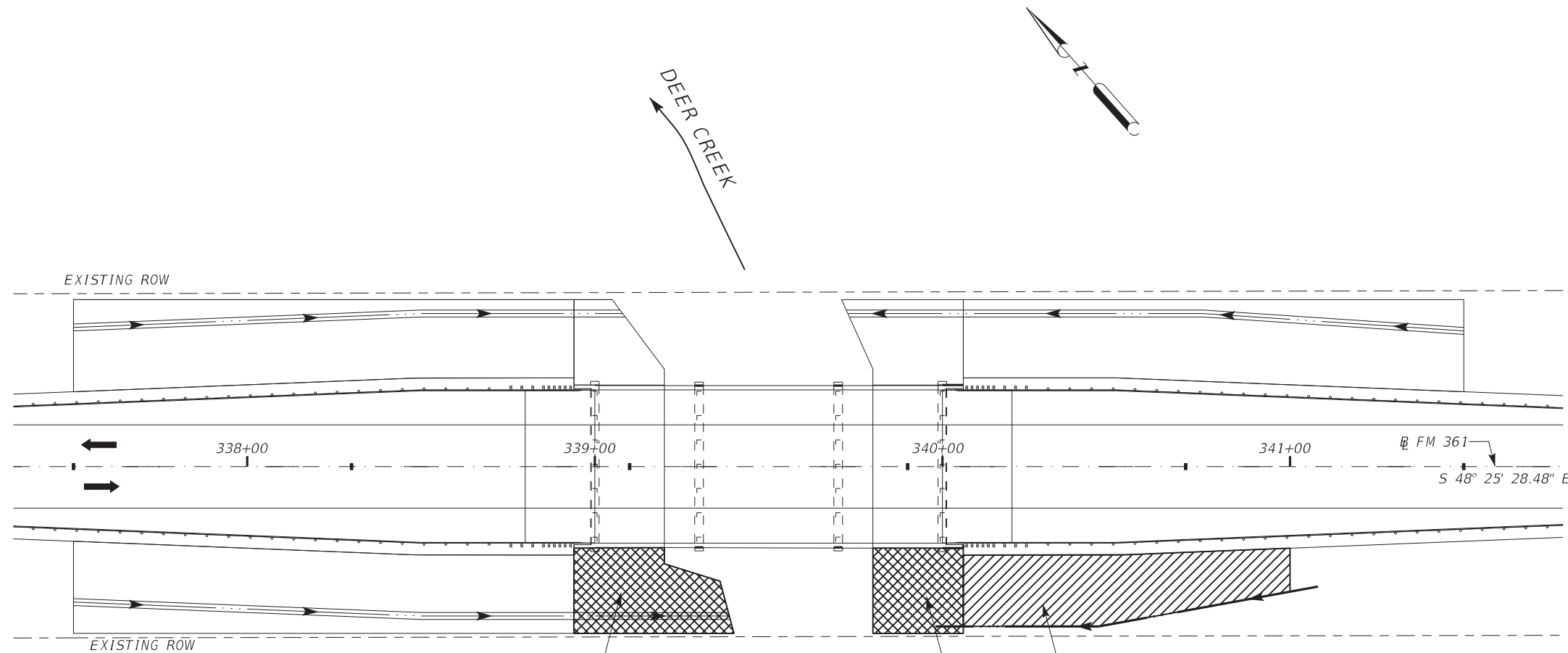
**LEGEND:**

ITEM 104-6009 REMOVING CONC (RIPRAP)



**NOTES:**

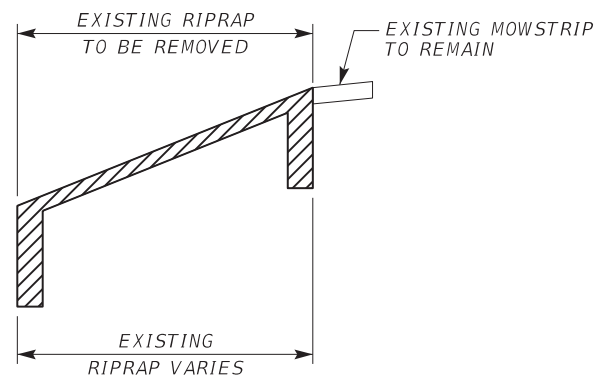
1. STEP 1 IS REMOVING THE DAMAGED CONCRETE RIPRAP (5") AND ROCK RIPRAP.
2. PUTTING THE ROCK RIPRAP BACK IN STEP 4 WILL BE INCIDENTAL TO ITS REMOVAL IN THIS STEP.



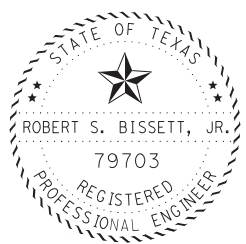
REMOVE ROCK RIPRAP  
ESTIMATED 106 SY  
TO BE PUT BACK AFTER PLACING  
CEMENT STABILIZED BASE  
(SEE NOTE 2)

REMOVE DAMAGED RIPRAP (5") AND TOEWALL  
ESTIMATED 190 SY

REMOVE ROCK RIPRAP  
ESTIMATED 71 SY  
TO BE PUT BACK AFTER PLACING  
CEMENT STABILIZED BASE  
(SEE NOTE 2)



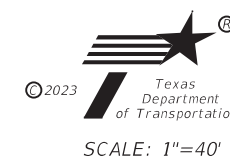
**5" CONCRETE RIPRAP TOEWALL DETAIL**  
NTS



*Robert S. Bissett, Jr.*  
12/20/22

**FM 361 AT  
DEER CREEK  
EROSION REPAIR  
STEP 1**

NBI: 12-080-0-0838-03-022



CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST COUNTY			SHEET NO.
HOU FORT BEND			22

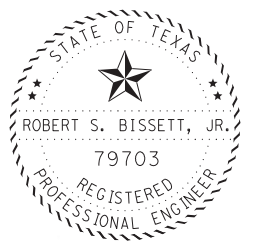
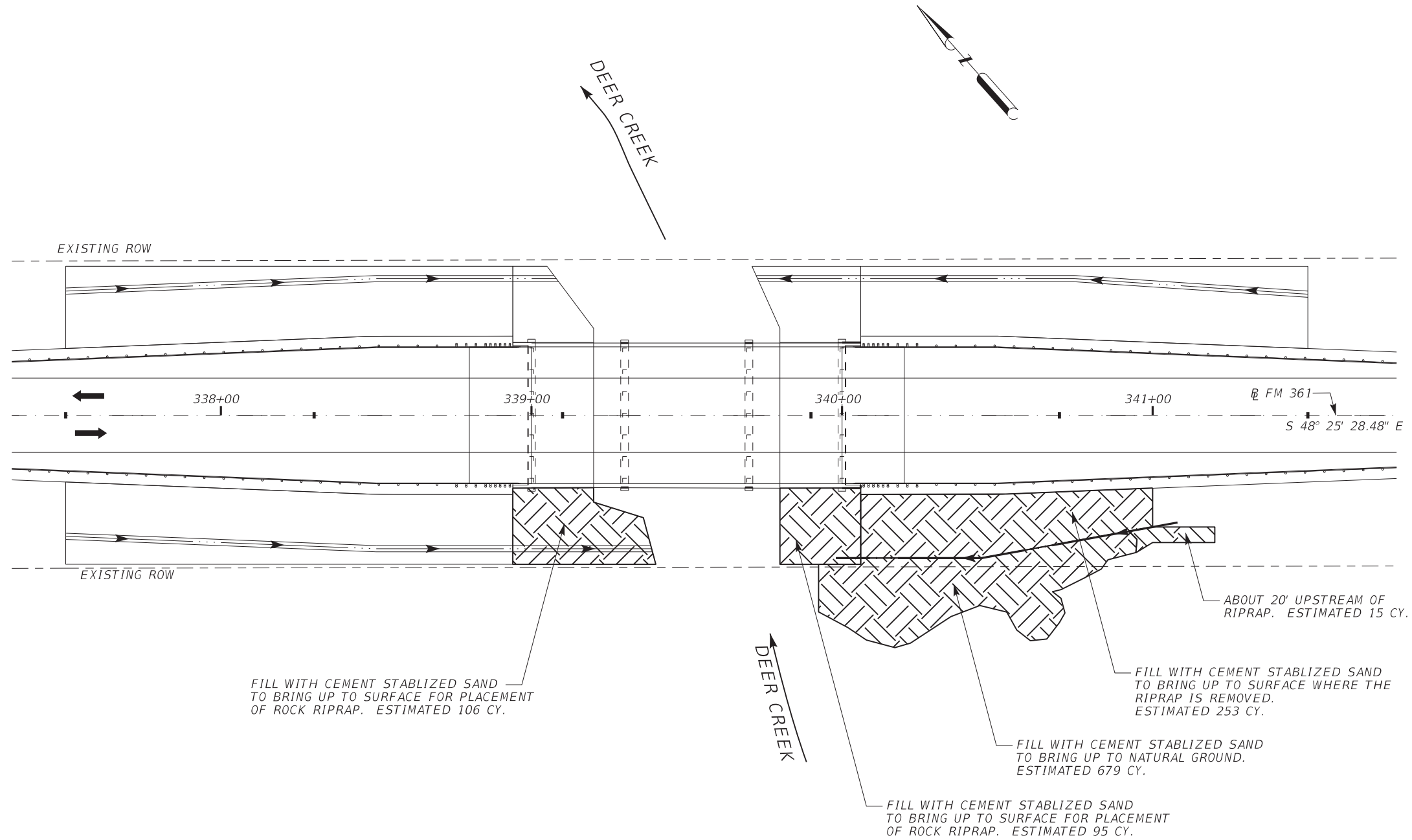
**LEGEND:**

ITEM 400-6005 CEM STABIL BKFL (CY)



**NOTES:**

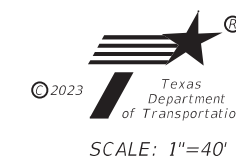
- STEP 2 IS FILLING IN THE ERODED AREA WITH CEMENT STABILIZED SAND.



*Robert S. Bissett, Jr.*  
12/20/22

**FM 361 AT  
DEER CREEK  
EROSION REPAIR  
STEP 2**

NBI: 12-080-0-0838-03-022



CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST			COUNTY
HOU			FORT BEND
			SHEET NO.
			23

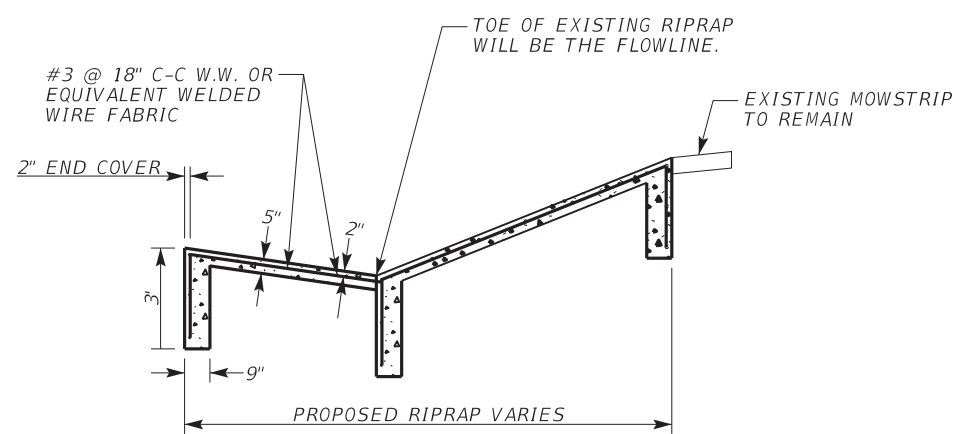
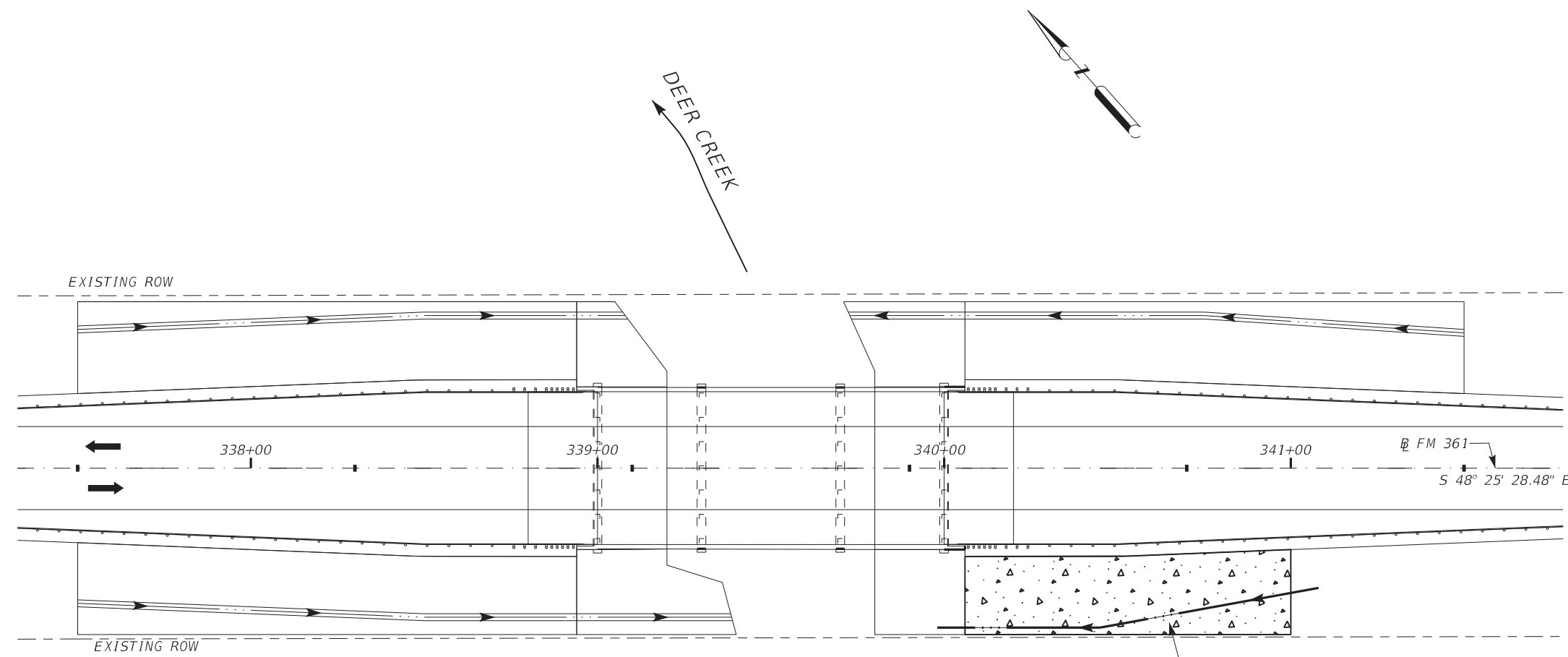
**LEGEND:**

ITEM 432-6002 RIPRAP (CONC) (5 IN)



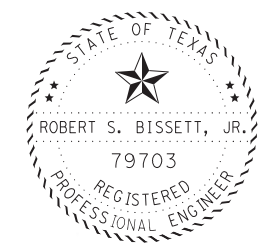
**NOTES:**

1. STEP 3 IS PLACING THE CONCRETE RIPRAP.



**5" CONCRETE RIPRAP TOEWALL DETAIL**  
NTS

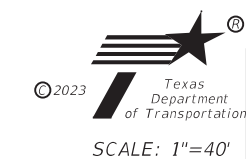
CONCRETE RIPRAP (5 IN)  
ESTIMATED 50 CY  
(INCLUDES TOE WALLS)



*Robert S. Bissett, Jr.*  
12/20/22

**FM 361 AT  
DEER CREEK  
EROSION REPAIR  
STEP 3**

NBI: 12-080-0-0838-03-022



CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST COUNTY			SHEET NO.
HOU FORT BEND			24

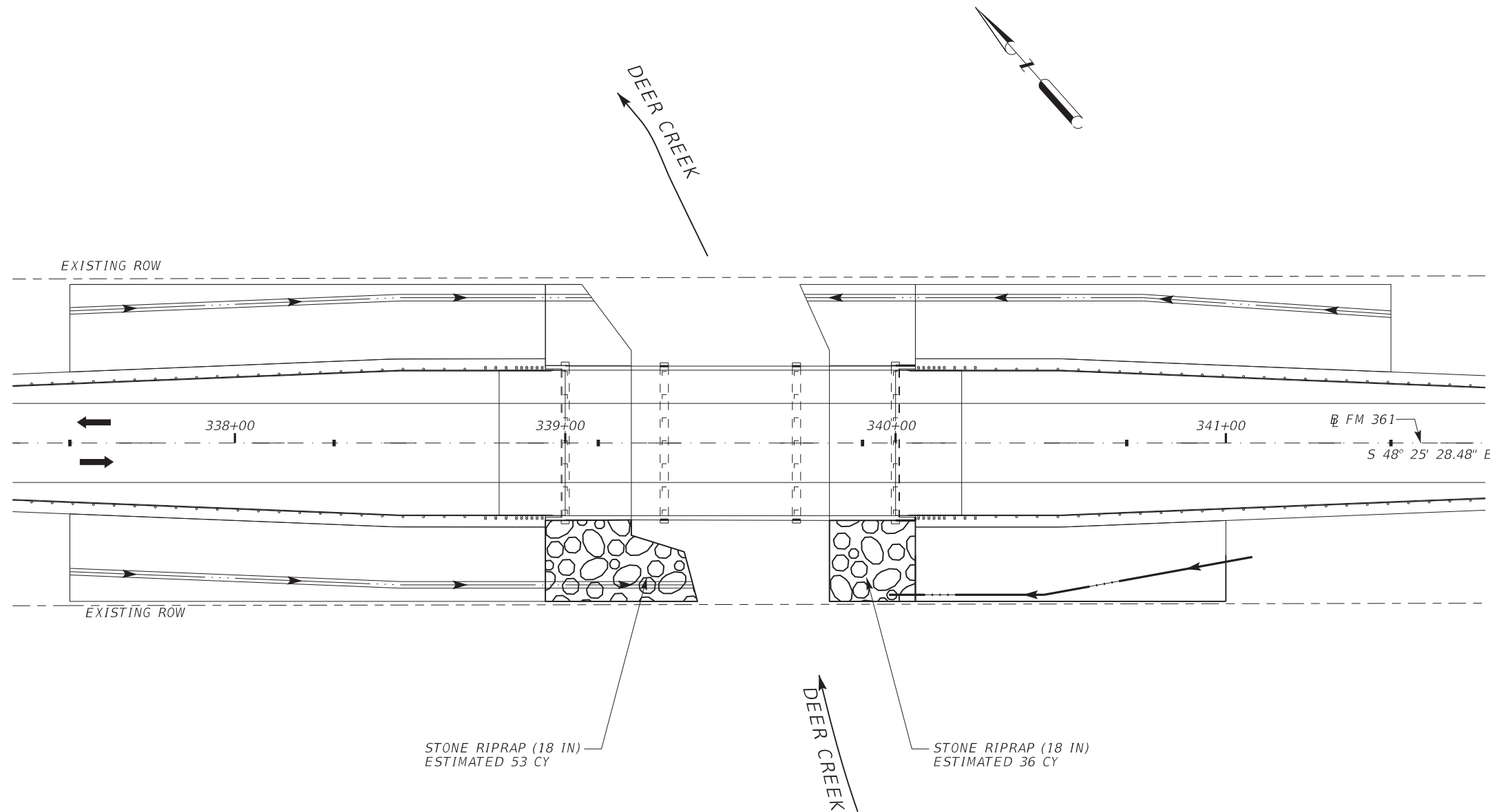
**LEGEND:**

ITEM 432-6026 RIPRAP (STONE COMMON)  
(DRY)(18 IN) (CY)



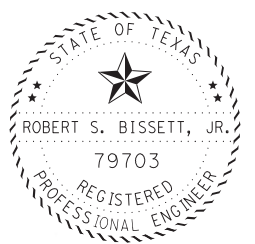
**NOTES:**

1. STEP 4 IS PLACING THE 18 IN STONE RIPRAP.



STONE RIPRAP (18 IN)  
ESTIMATED 53 CY

STONE RIPRAP (18 IN)  
ESTIMATED 36 CY

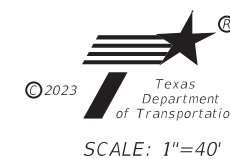


*Robert S. Bissett, Jr.*

12/20/22

**FM 361 AT  
DEER CREEK  
EROSION REPAIR  
STEP 4**

NBI: 12-080-0-0838-03-022

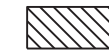


SCALE: 1"=40'

CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST COUNTY			SHEET NO.
HOU FORT BEND			25

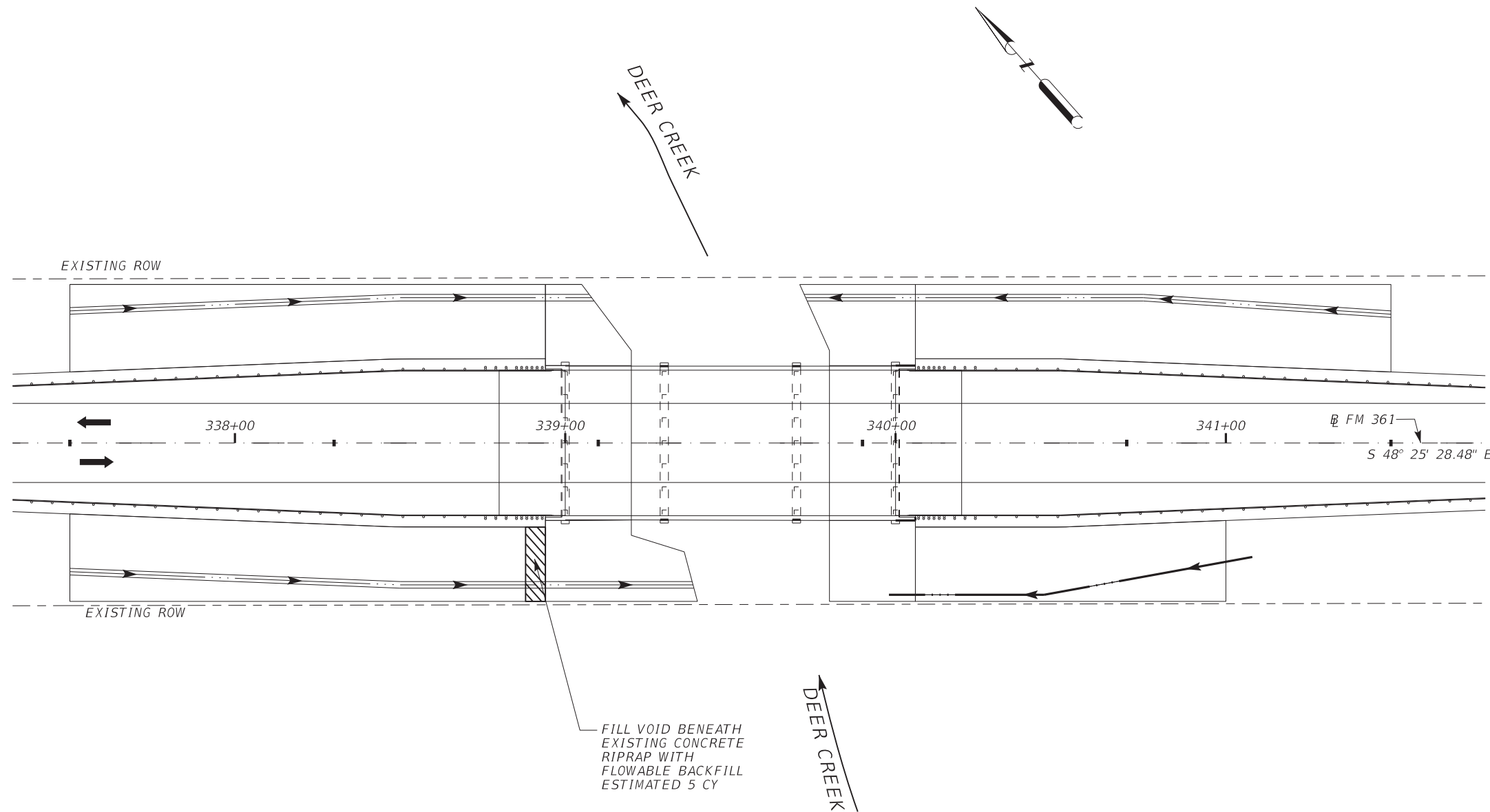
**LEGEND:**

ITEM 401-6001 FLOWABLE BACKFILL (CY)

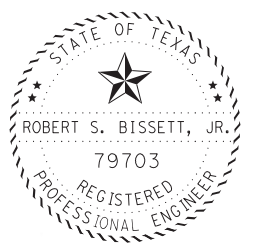


**NOTES:**

- STEP 5 IS FILLING THE VOIDS UNDER THE EXISTING CONCRETE RIPRAP WITH FLOWABLE BACKFILL.



FILL VOID BENEATH EXISTING CONCRETE RIPRAP WITH FLOWABLE BACKFILL ESTIMATED 5 CY

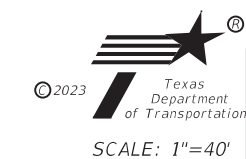


*Robert S. Bissett, Jr.*

12/20/22

**FM 361 AT DEER CREEK EROSION REPAIR STEP 5**

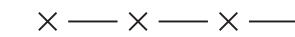
NBI: 12-080-0-0838-03-022



CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST COUNTY			SHEET NO.
HOU FORT BEND			26

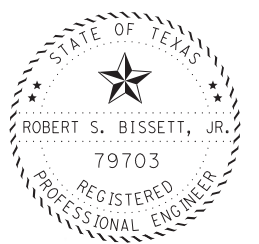
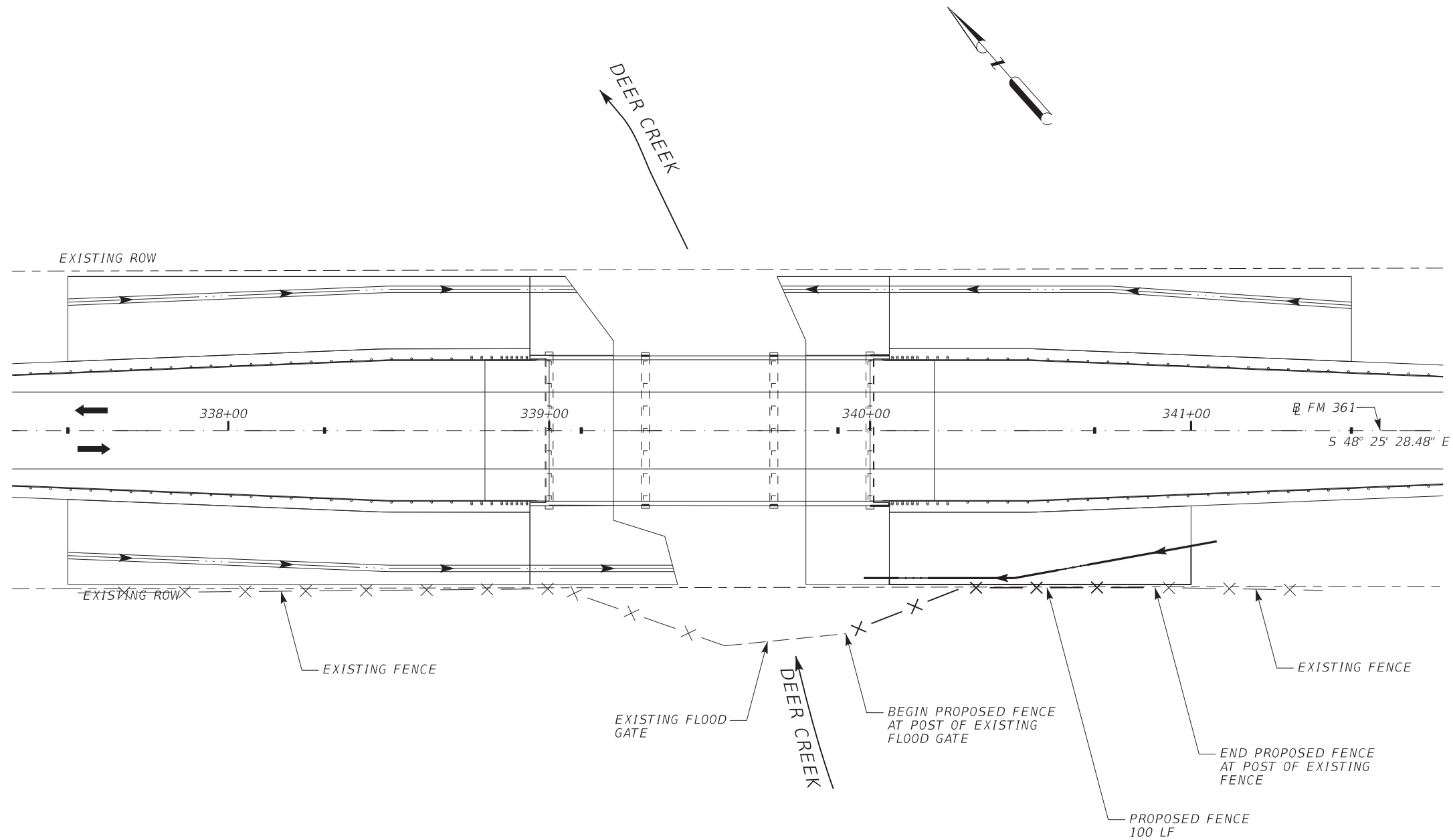
**LEGEND:**

ITEM 552-6001 WIRE FENCE (TY A) (LF)



**NOTES:**

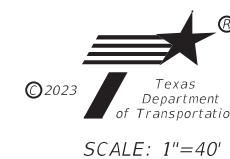
1. STEP 6 IS INSTALLING THE NEW FENCE.



*Robert S. Bissett, Jr.*  
12/20/22

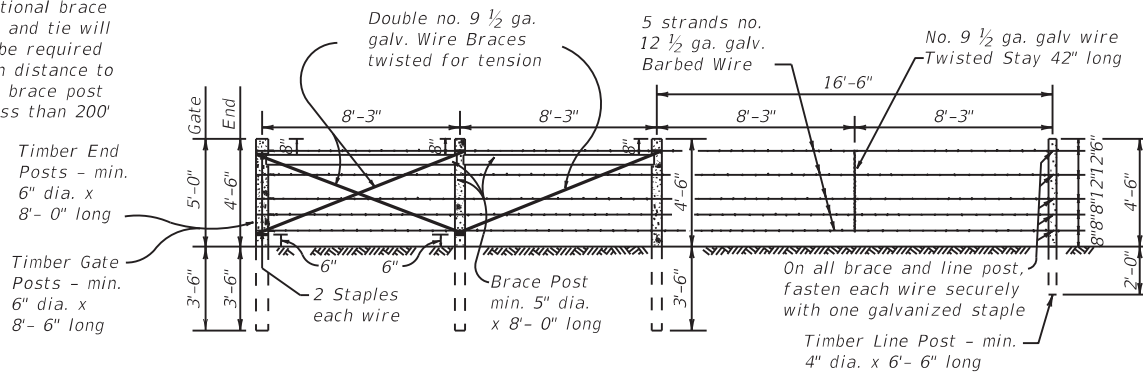
**FM 361 AT  
DEER CREEK  
EROSION REPAIR  
STEP 6**

NBI: 12-080-0-0838-03-022



CONT	SECT	JOB	HIGHWAY
6430	55	001	FM 361
DIST COUNTY			SHEET NO.
HOU FORT BEND			27

Additional brace post and tie will not be required when distance to next brace post is less than 200'



**SECTION GALVANIZED BARBED WIRE FENCE WITH WOOD POSTS**

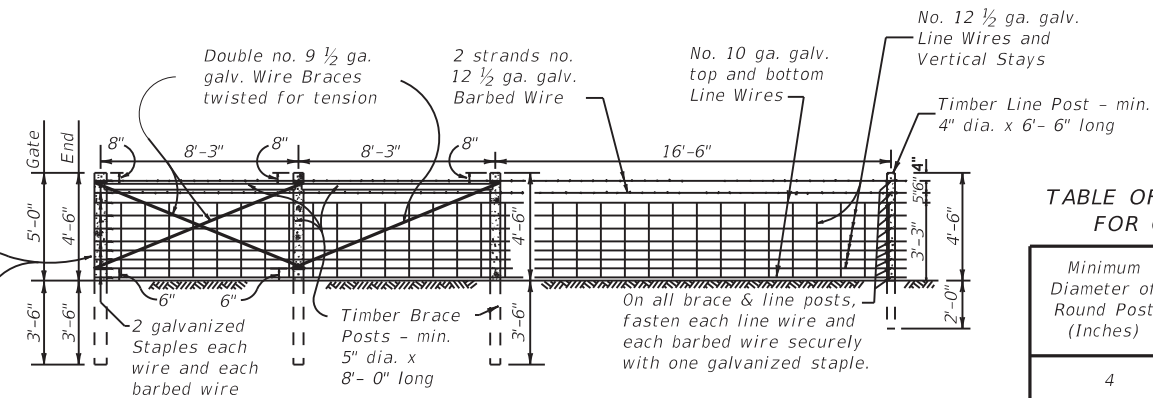
Bracing Detail Used at Ends and Gates

**TYPE "A" FENCE**

(See General Note 6)

Additional brace post and tie will not be required when distance to next brace post is less than 200'.

Timber End Posts - min. 6" dia. x 8'-0" long  
Timber Gate Posts - min. 6" dia. x 8'-6" long



**SECTION GALVANIZED WOVEN WIRE FENCE WITH WOOD POSTS**

Bracing Detail Used at Ends and Gates

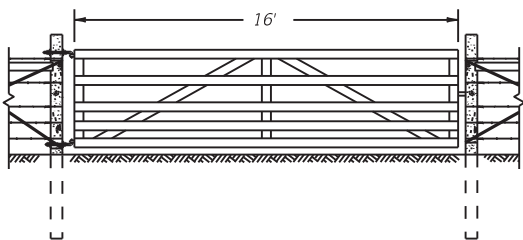
**TYPE "B" FENCE**

(See General Note 6)

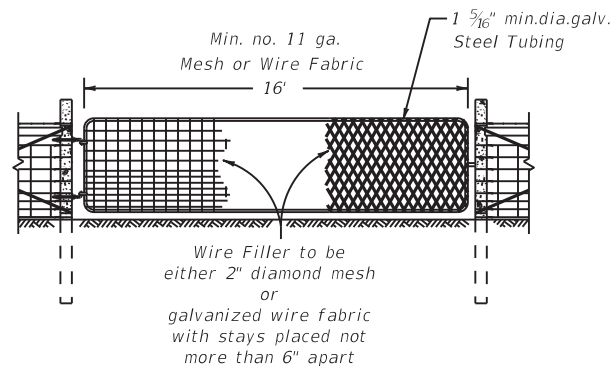
**TABLE OF EQUIVALENT SIZES FOR OPTIONAL SHAPE**

Minimum Diameter of Round Post (Inches)	Minimum Equivalent Dimension for Each Side of Square Post (Inches)
4	3 1/2
5	4 1/2
6	5 1/4

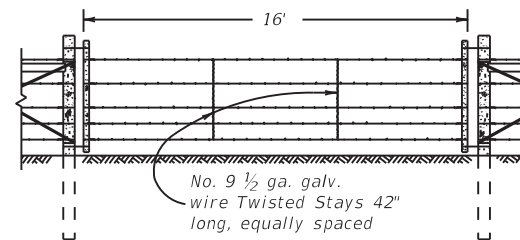
Metal gate shall consist of 5 panels not less than 4'-4" high and shall be aluminum or galvanized metal and of good quality. Gate and hardware shall meet the approval of the Engineer.



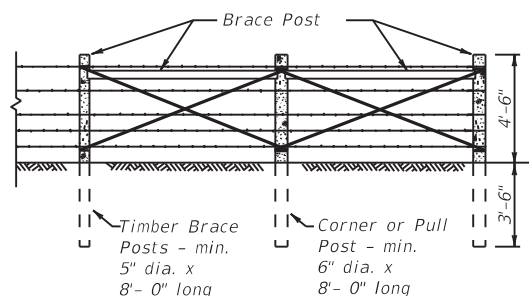
**DETAIL TYPE 1 GATE**



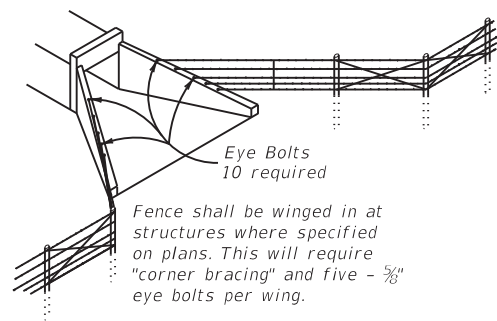
**DETAIL TYPE 2 GATE**



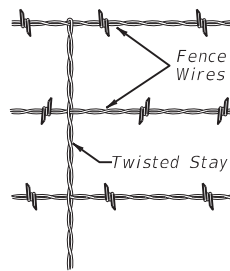
**DETAIL TYPE 3 GATE**



**CORNER OR PULL POST ASSEMBLY**

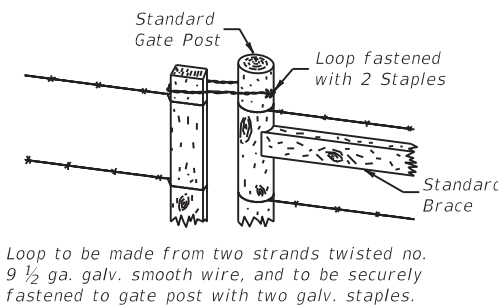


**DETAIL OF FENCE TREATMENT AT STRUCTURES**

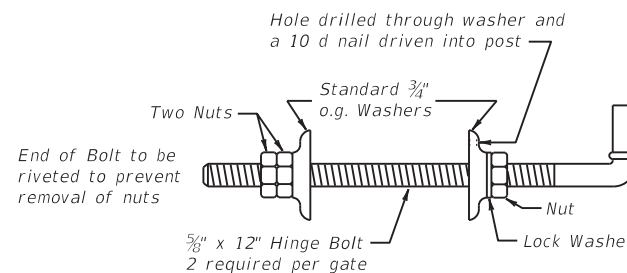


**DETAIL OF STAY**

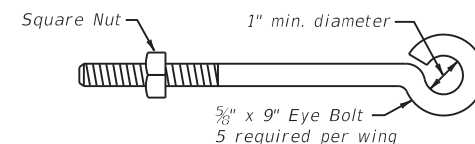
(Barbed wire fence)



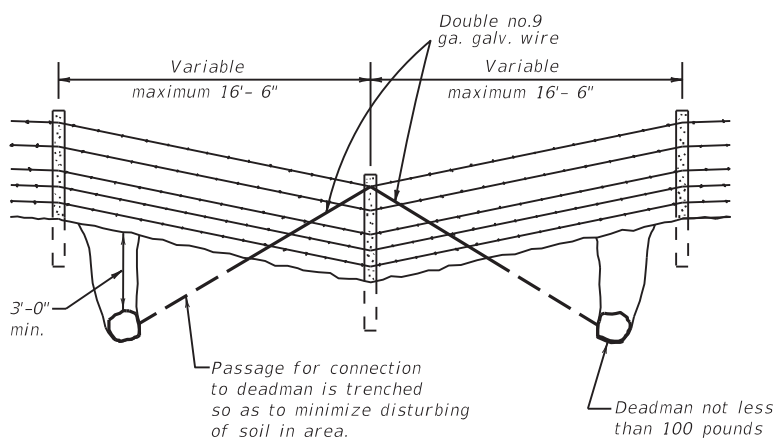
**DETAIL FASTENER TYPE 3 GATE**



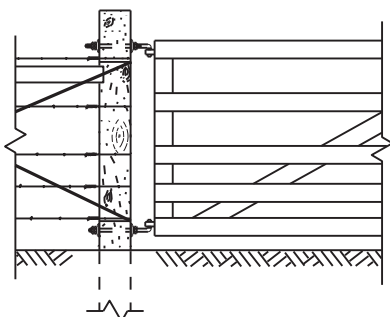
**DETAIL OF GATE HINGE BOLT ASSEMBLY**



**DETAIL OF EYE BOLT**



**DETAIL OF FENCE SAG (Single Line Connection)**



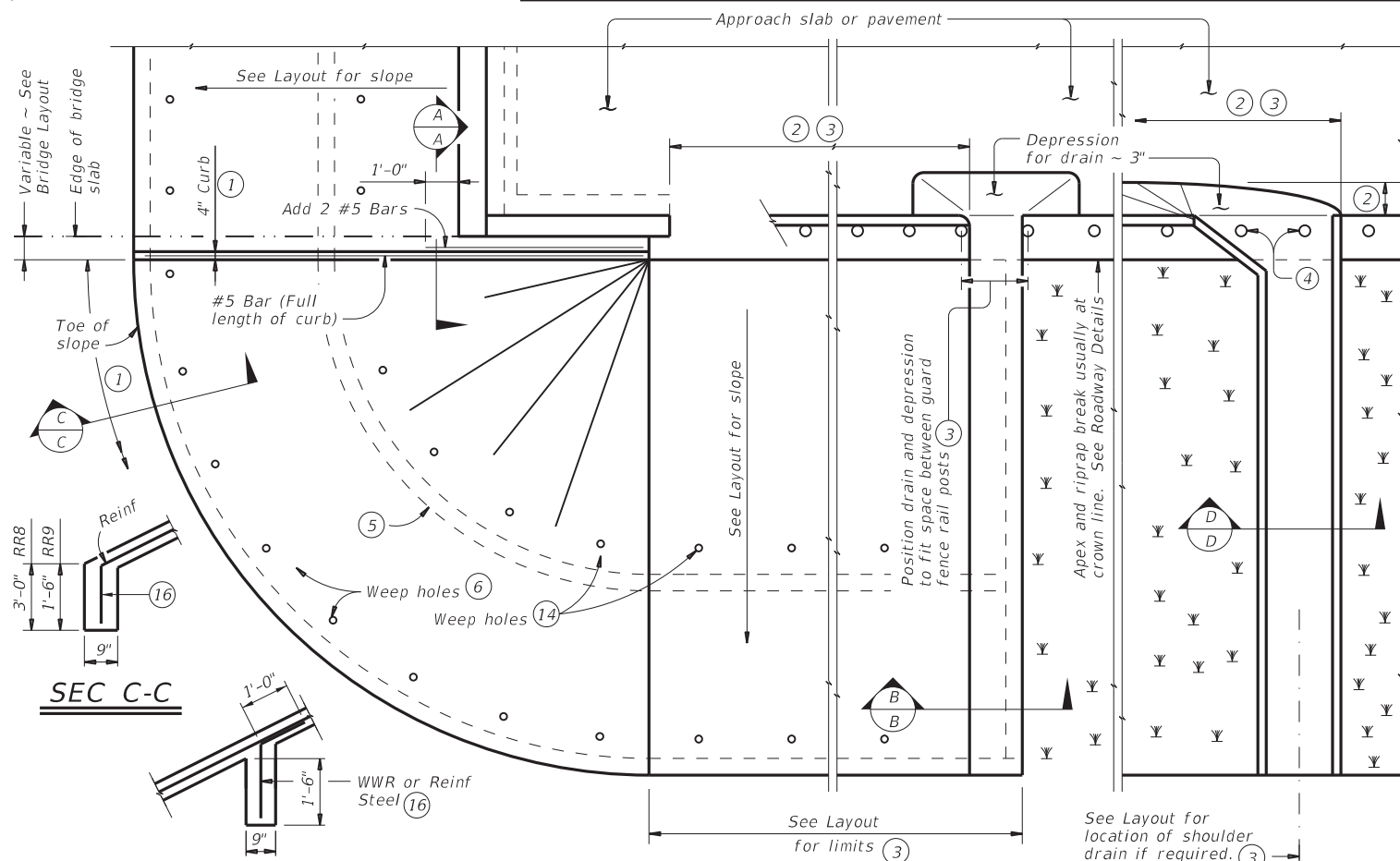
**DETAIL SHOWING INSTALLATION OF HINGES OF TYPE 1 & 2 GATE**

**GENERAL NOTES**

- Any high point which interferes with the placing of wire mesh shall be excavated to provide 2" clearance.
- Latches for Type 1 and Type 2 gates shall be good commercial quality and design latches of the spring, fork or chain type. All latches shall be suitable for the gate and shall be approved by the Engineer.
- Hinges for Type 2 gates shall be commercial design approved by the Engineer suitable for post and gate.
- Concrete shall be of the design and consistency approved by the Engineer and shall contain not less than 4 sacks of cement per cubic yard. Concrete footings are to be crowned at the top to shed water.
- If rock is encountered at a depth less than the embedded depth required, a 15" or larger diameter hole shall be drilled for the post and the post shall be set in concrete. If rock is encountered at a depth of 1'-6" or more below the ground surface, the hole shall be drilled to the required depth. If rock is encountered at a depth less than 1'-6" below the ground surface, the holes shall be drilled a minimum of 2'-0" into the rock or to the depth whichever is the lesser depth.
- Barbed Wire shall be in accordance with ASTM A 121 (Class 1) Design designation 12-2-4-1 4R or 12-2-5-1 4R, or as approved by the Engineer.  
Woven Wire Fence (Type B) shall be in accordance with ASTM A 116 (Class 1) No. 12-1/2 Grade 60 (See Table 1 ASTM A 116) to the height and design shown on the plans, or as approved by the Engineer.
- The location of gates and corner posts will be as indicated elsewhere on these plans.
- Square wood posts may be used in lieu of round posts provided minimum equivalent size requirements, as shown are met. All wood posts shall be in accordance with Item 552, "Wire Fence."

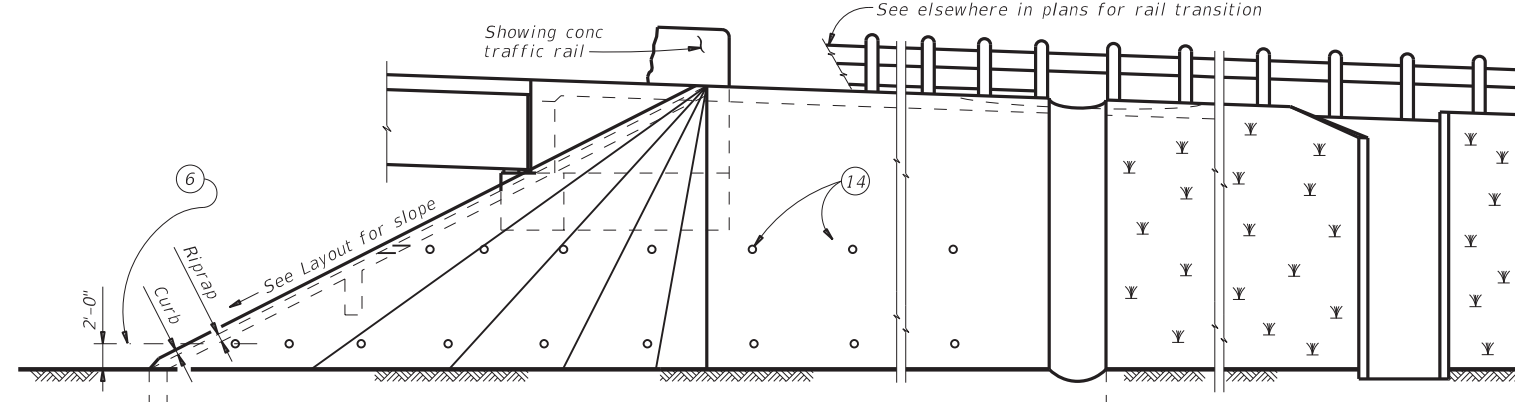
**BARBED WIRE AND WOVEN WIRE FENCE (WOOD POSTS) WF(1)-10**

FILE: wf110.dgn	DN: TxDOT	CK: AM	DW: VP	CK:
©TxDOT 1994	CONT	SECT	JOB	HIGHWAY
REVISIONS	6430	55	001	FM 361
	DIST	COUNTY	SHEET NO.	
	HOU	FORT BEND	28	

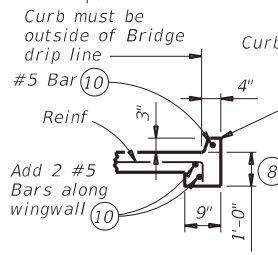


**INTERMEDIATE TOEWALL**

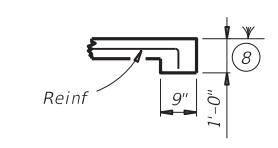
**PLAN**



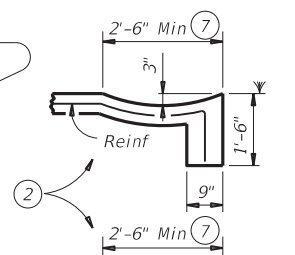
**ELEVATION**



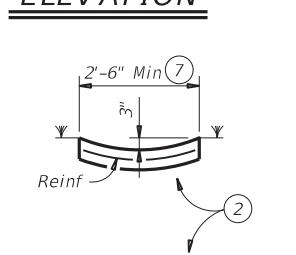
**SEC A-A**



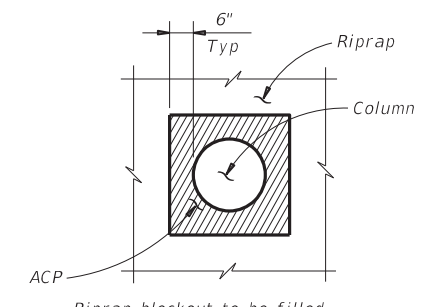
**SEC B-B**  
(No drain)



**SEC B-B**  
(Shoulder drain integral with riprap)

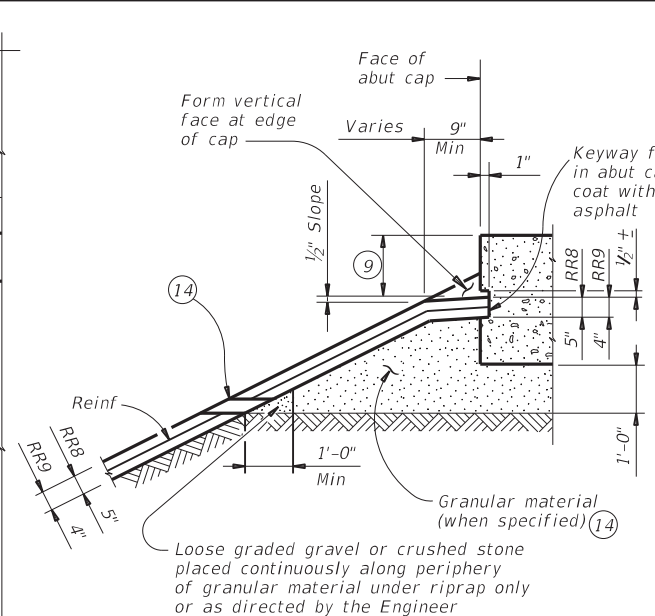


**SEC D-D**  
(Shoulder drain)

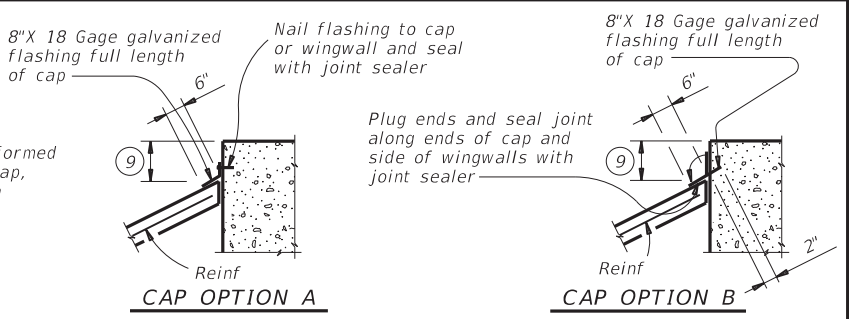


**RIPRAP DETAIL AT COLUMNS**

(As directed by the Engineer)

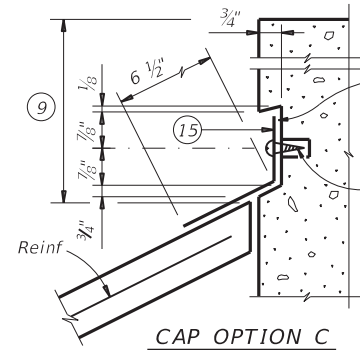


**SHOWING KEYWAY OPTION**

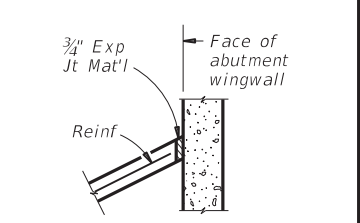


**CAP OPTION A**

**CAP OPTION B**

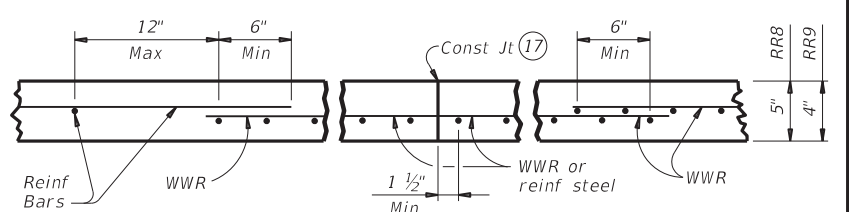


**CAP OPTION C**



**SECT THRU RIPRAP AT WINGWALL**

**SECTIONS THRU RIPRAP AT CAP**



**REINFORCEMENT DETAILS**

See General Notes for optional synthetic fiber reinforcement.

- 1 When riprap is shown extended around header on layout, extend slab and toewall as shown and eliminate 4" curb.
- 2 Limits and configuration of drains and depressions are as shown elsewhere in plans or as directed by the Engineer.
- 3 Location of shoulder drain must consider limitations imposed by rail transition. Do not locate shoulder drains at expansion joints between approach slab and concrete pavement.
- 4 See details elsewhere in plans for installation of guard fence posts through concrete riprap.
- 5 Provide intermediate toewall only when designated elsewhere in the plans or included in the specifications.
- 6 Provide lower level of 2" Dia weep holes at 10' c-c backed by 1 CF packet of gravel and galvanized hardware cloth at all locations unless directed by the Engineer to eliminate.
- 7 Use wider or other drain configurations if shown elsewhere in plans or if directed by the Engineer.
- 8 Wall extension may be reduced or modified if approved by the Engineer. Increase wall extension to 1'-6" whenever the optional intermediate toewall is called for in the plans.
- 9 Top of cap to top of riprap dimension varies as directed by the Engineer. Should be 9" Min for beam/slab type bridges and 1'-6" for slab span, box beam, or slab beam bridges.
- 10 #5 bars shown are required even when synthetic fiber reinforcing option is selected.
- 11 Provide sealing option for joint between the face of cap and riprap as designated by the Engineer or as shown elsewhere on plans.
- 12 Flashing (shown in Cap Option A) may be used at wingwall in addition to Exp Jt Mat'l if shown on plans or directed by the Engineer.
- 13 Provide #3 reinforcing bars at 18" Spa c-c. Provide Welded Wire Reinforcement (WWR) as 6x6-D2.9xD2.9 or D3xD3. Combinations of WWR and reinforcing bars may be used if both are permitted. Use lap splices of a minimum 6 inches, measured from the transverse wire of WWR, and the ends of reinforcing bars.
- 14 If granular material is specified, provide upper level of 2" Dia weep holes at 10' c-c backed by galvanized hardware cloth.
- 15 8" x 18 Gage Galv Sheet Metal
- 16 Provide WWR or #3 bars, with 1'-0" extension into slope.
- 17 WWR or reinforcing steel is continuous through riprap construction joints. Provide WWR or reinforcing steel that extends 1'-1" minimum into adjacent riprap on each side of construction joint even if synthetic reinforcing fiber is utilized.

**GENERAL NOTES:**

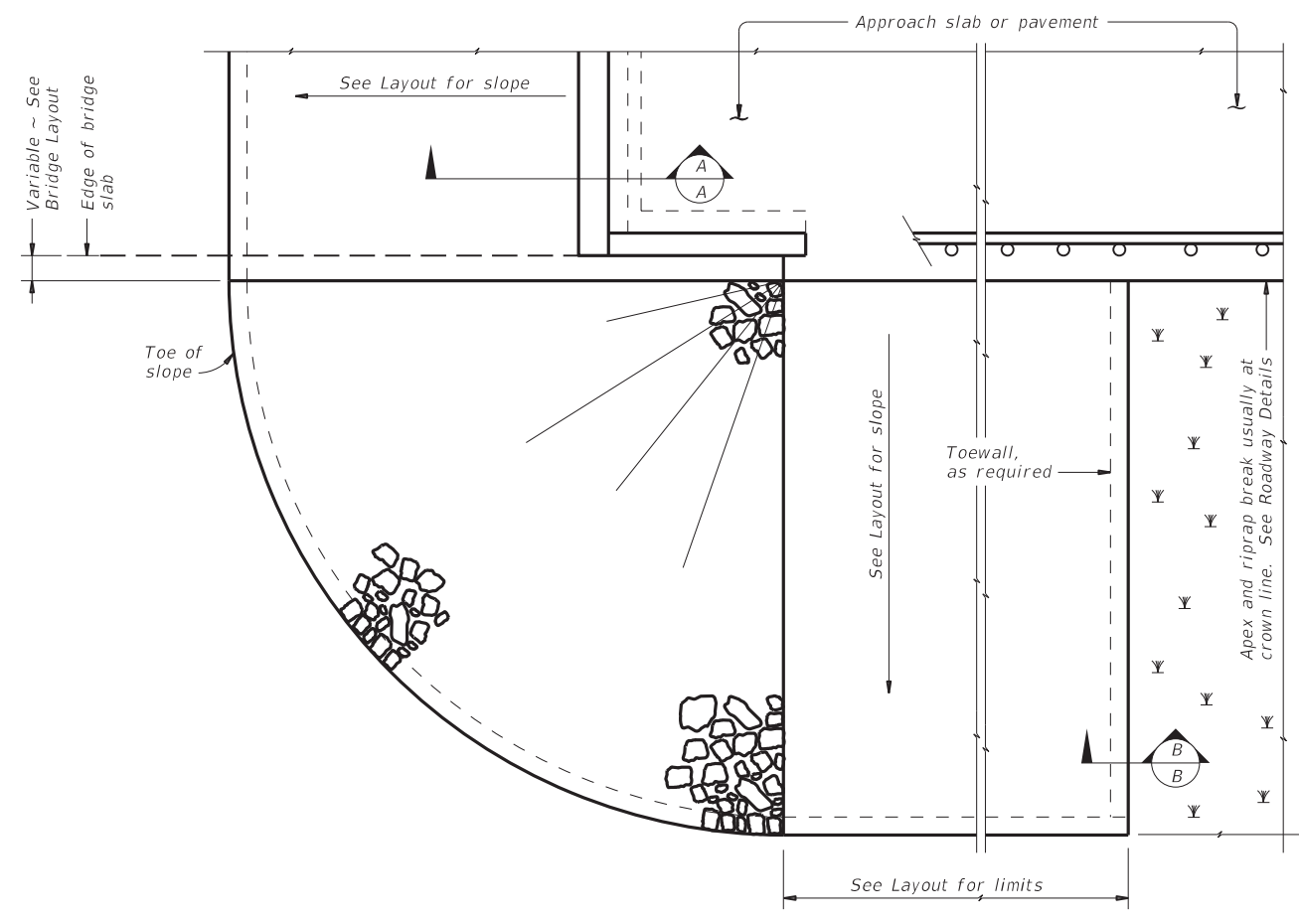
Provide Class "B" concrete (f'c = 2,000 psi) unless noted elsewhere in plans.  
 Provide Grade 60 reinforcing steel.  
 Provide deformed welded wire reinforcement (WWR) meeting ASTM A1064, unless otherwise shown.  
 Provide reinforcing bars, deformed WWR, or any suitable combination of both types for riprap reinforcing, unless specified elsewhere in the plans.  
 Optionally synthetic fibers may be used if approved by the Engineer. Provide synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) in lieu of steel reinforcing in riprap concrete.  
 Install construction joints or grooved joints extending the full slant slope height at intervals of approximately 20 feet unless otherwise directed by the Engineer.  
 Hardware cloth, loose grade stone behind weep holes, flashing, or other sealing material are subsidiary to the bid item "Riprap".  
 See Layout for limits of riprap.  
 RR8 is to be used on stream crossings.  
 RR9 is to be used on other embankments.

**FOR CONTRACTOR'S INFORMATION ONLY:**

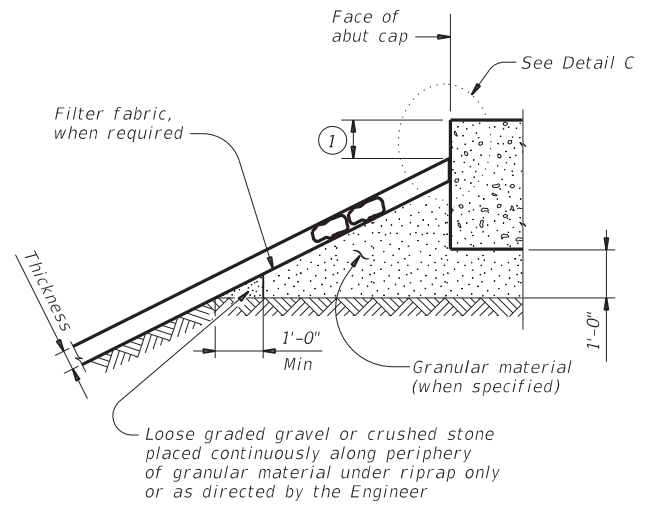
5" of RR8	= 0.015 CY/SF
4" of RR9	= 0.012 CY/SF
#3 Reinf at 18" c-c	= 0.501 Lbs/SF
6x6-D3xD3	= 0.408 Lbs/SF

		<b>Bridge Division Standard</b>	
<b>CONCRETE RIPRAP AND SHOULDER DRAINS EMBANKMENTS AT BRIDGE ENDS (TYPES RR8 &amp; RR9)</b>			
<b>CRR</b>			
FILE: crrstd1-19.dgn	DN: TxDOT	CK: TxDOT	OW: TxDOT
©TxDOT April 2019	CON: 6430	SECT: 55	JOB: 001
REVISIONS			HIGHWAY: FM 361
	DIST: HOU	COUNTY: FORT BEND	SHEET NO: 29

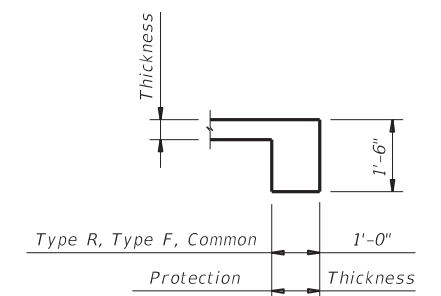




**PLAN**

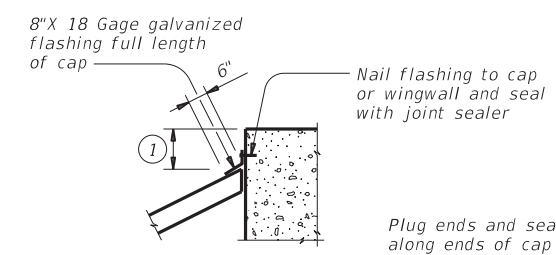


**SECTION A-A AT CAP**

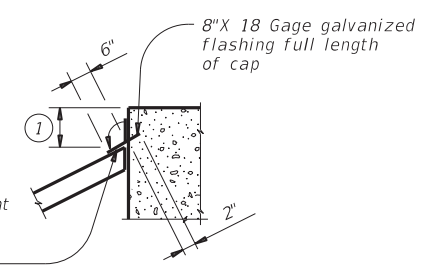


**SECTION B-B**

Provide toewall when shoulder drain is located adjacent to limits of stone riprap. Omit toewall when thickness of protection riprap is greater than 18".

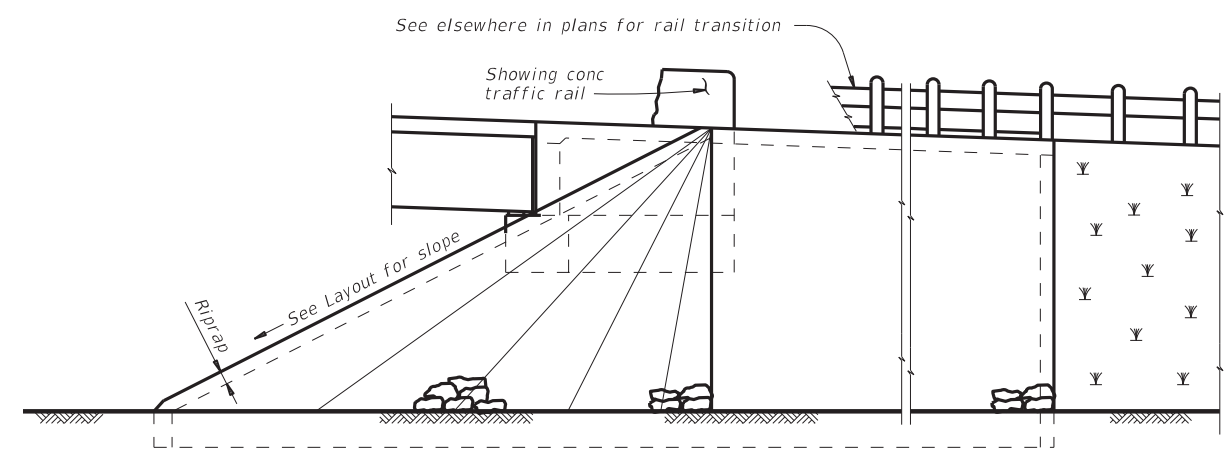


**CAP OPTION A**



**CAP OPTION B**

**DETAIL C**



**ELEVATION**

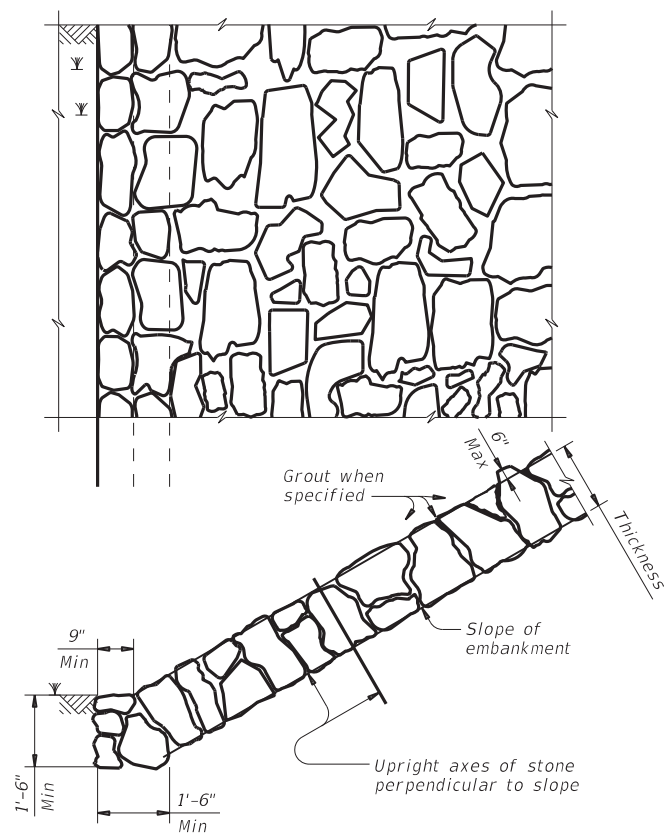
① Top of cap to top of riprap dimension varies as directed by the Engineer. Provide 9" Min for beam/slab type bridges and 1'-6" for slab span, box beam, or slab beam bridges.

**GENERAL NOTES:**

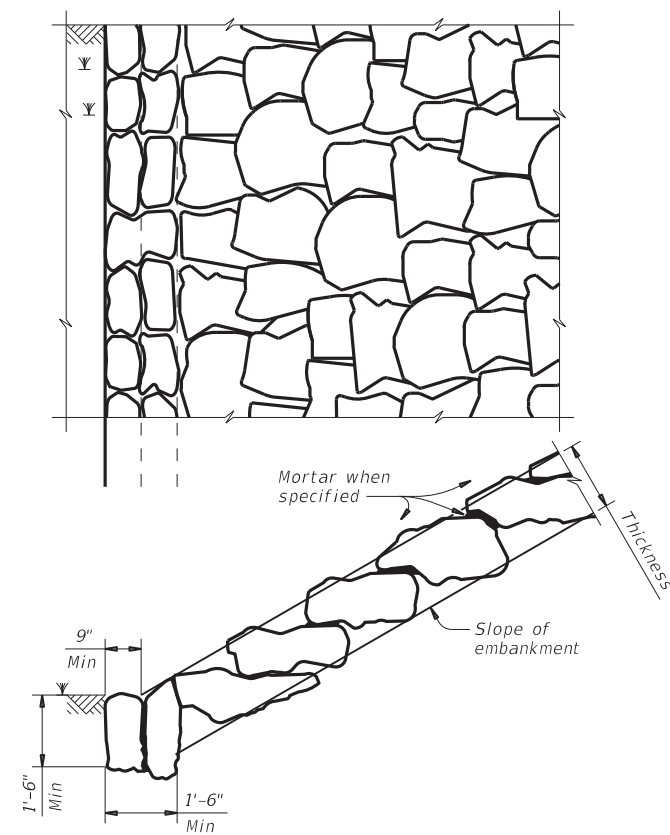
Refer to Item 432, "Riprap" for stone size and gradation, and construction details. See Layout for limits and thickness of riprap specified.  
See elsewhere in plans for locations and details of shoulder drains.

SHEET 1 OF 2

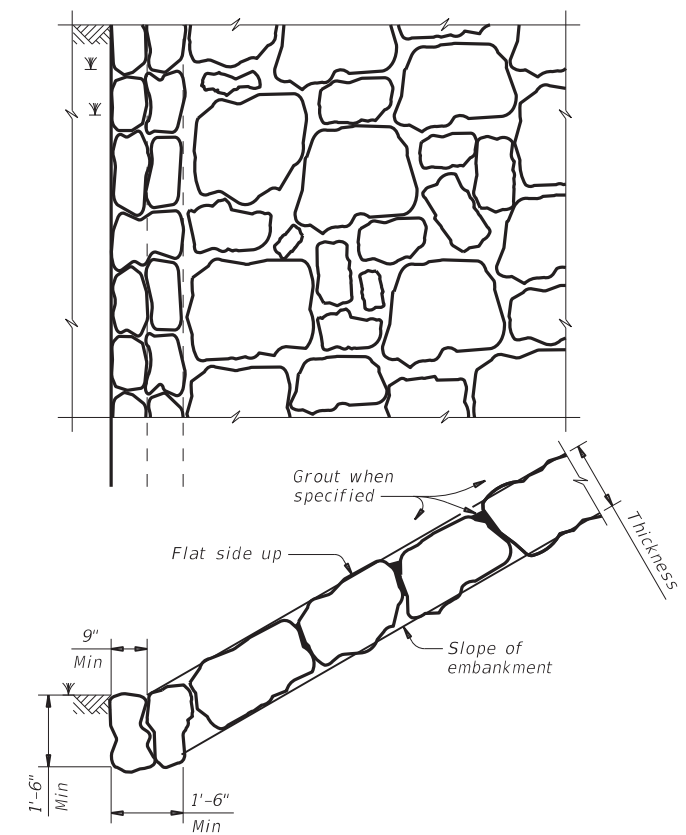
		<b>Bridge Division Standard</b>	
<h2>STONE RIPRAP</h2>			
<h3>SRR</h3>			
FILE: srrstde1-19.dgn	DN: AES	CK: JGD	DW: BWH
©TxDOT April 2019	CONT SECT	JOB	HIGHWAY
REVISIONS	6430 55	001	FM 361
	DIST	COUNTY	SHEET NO.
	HOU	FORT BEND	30



**FIGURE 1 ~ TYPE R STONE RIPRAP**  
dry or grouted

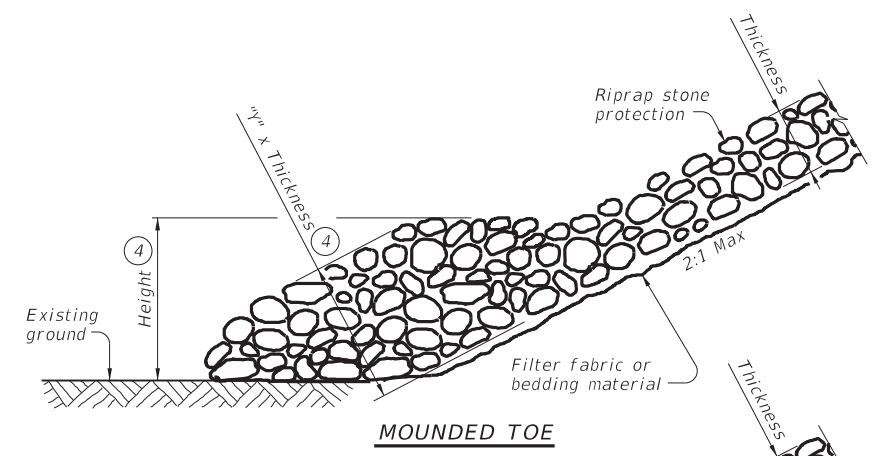


**FIGURE 2 ~ TYPE F STONE RIPRAP**  
dry or mortared

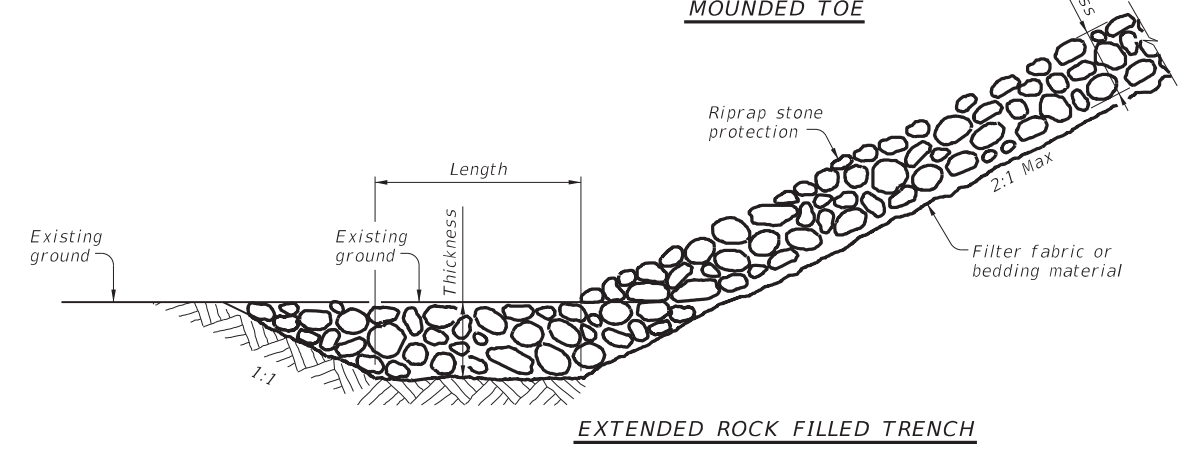


**FIGURE 3 ~ TYPE F STONE RIPRAP**  
grouted

- ② Provide bedding material instead of filter fabric if shown elsewhere in plans. See Layout for thickness of bedding material.
- ③ Minimum toe depth is the larger of the maximum scour depth or 2 times the riprap thickness.
- ④ "Y" and Height need to be defined. See layout or detail sheet for values if this option is used.
- ⑤ List Stone Protection as size (XX inch) and thickness (YY inch) on the layout.  
Example: Riprap (Stone Protection) XX inch, Thickness = YY inch.

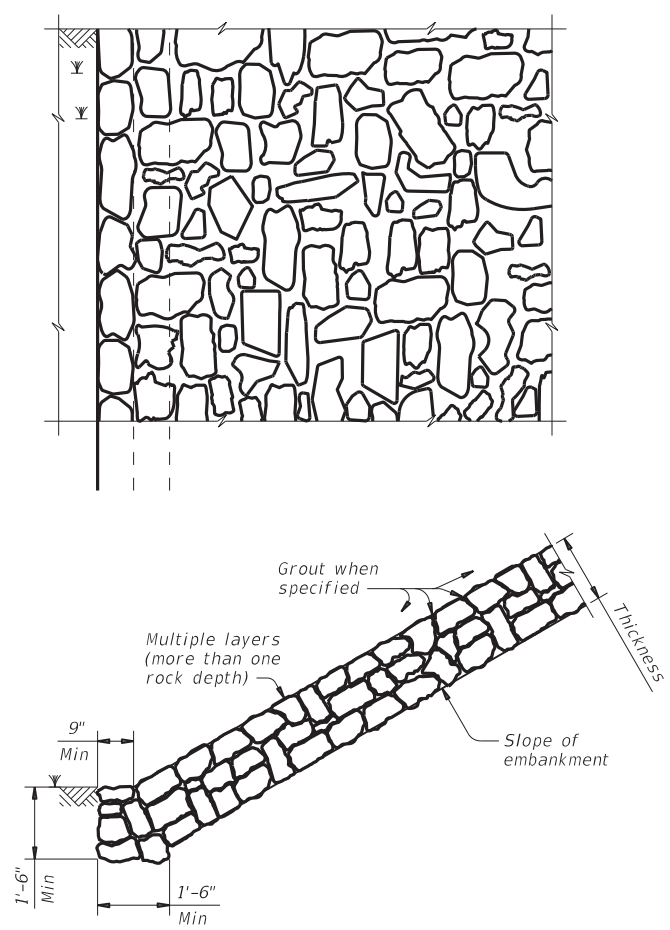


**MOUNDED TOE**

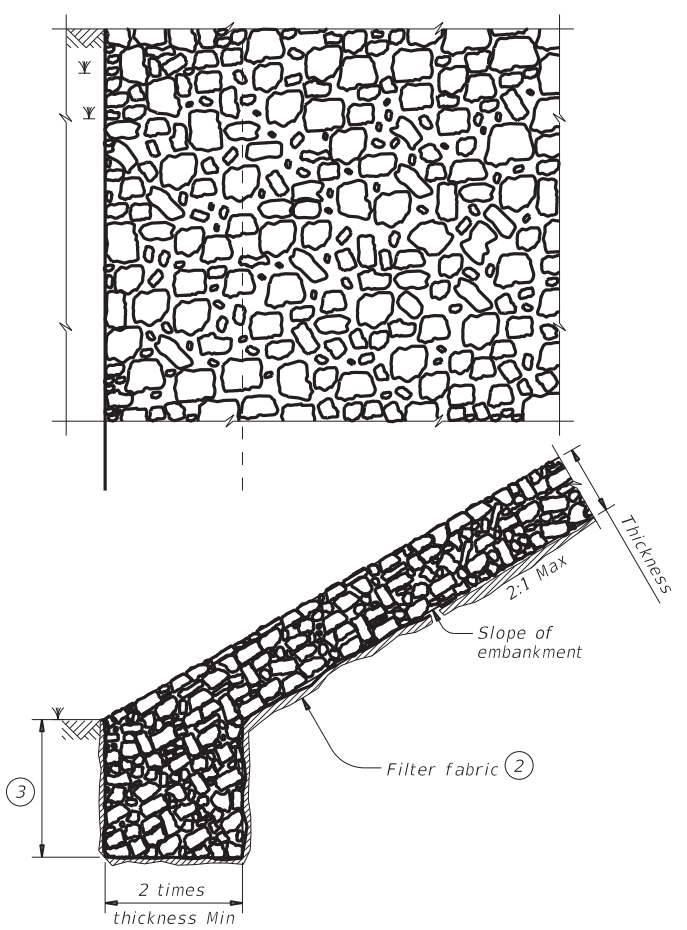


**EXTENDED ROCK FILLED TRENCH**

**PROTECTION STONE RIPRAP TOE OPTIONS ⑤**



**FIGURE 4 ~ COMMON STONE RIPRAP**  
dry or grouted

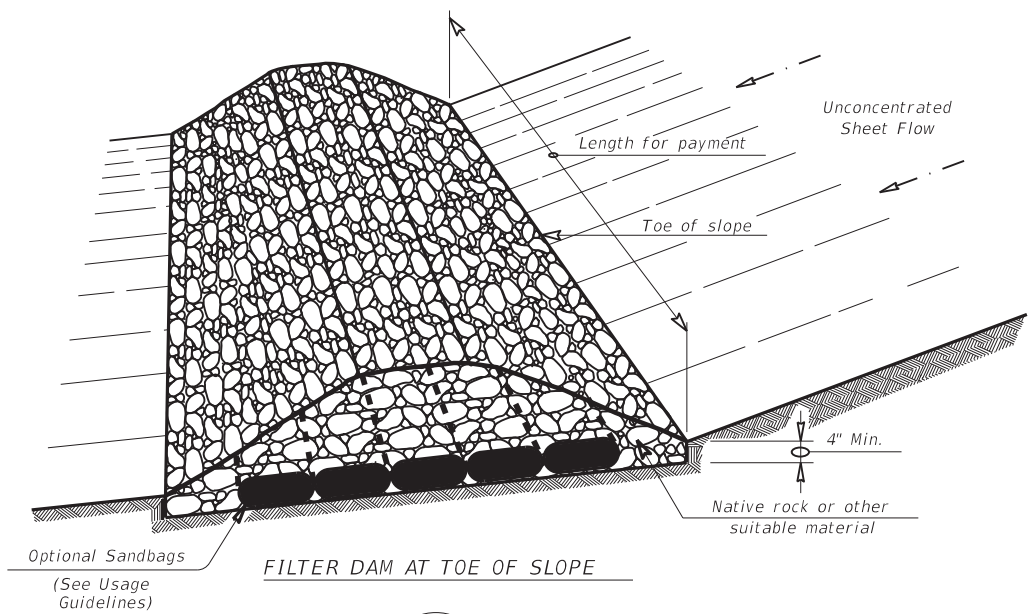


**FIGURE 5 ~ PROTECTION STONE RIPRAP ⑤**

**STONE RIPRAP**

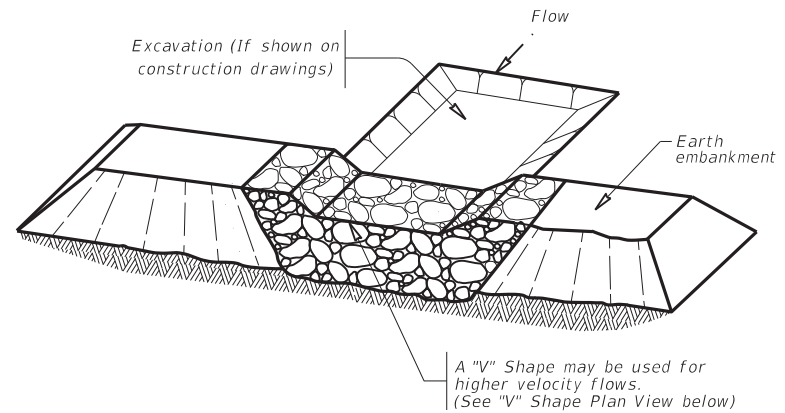
**SRR**

FILE: srrside1-19.dgn	DN: AES	CK: JGD	DW: BWH	CK: AES
©TxDOT April 2019 REVISIONS	CONT 6430	SECT 55	JOB 001	HIGHWAY FM 361
	DIST HOU	COUNTY FORT BEND	SHEET NO. 31	



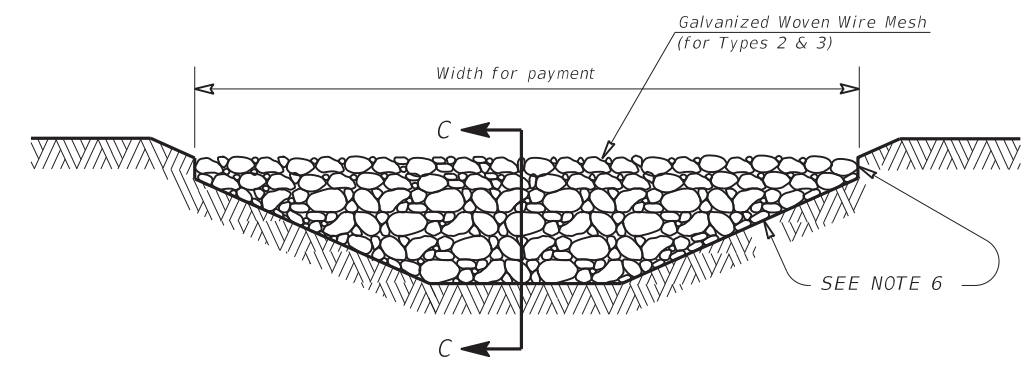
FILTER DAM AT TOE OF SLOPE

(RFD1)



FILTER DAM AT SEDIMENT TRAP

(RFD1) OR (RFD2)

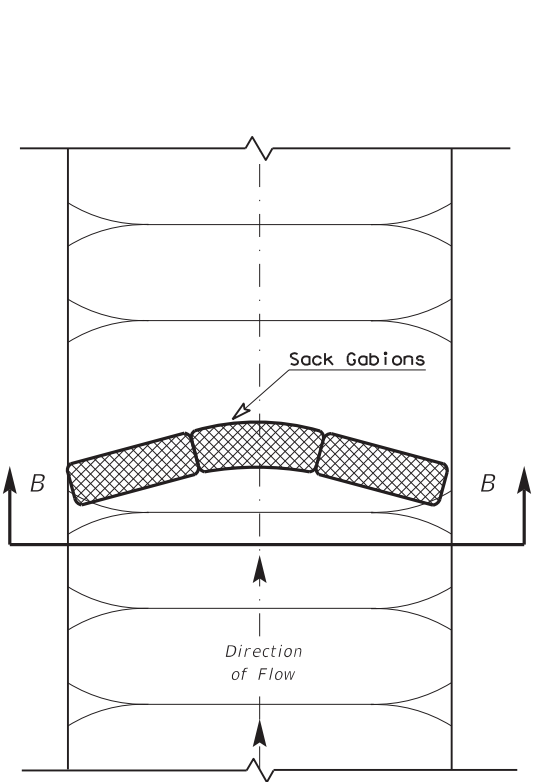


FILTER DAM AT CHANNEL SECTIONS

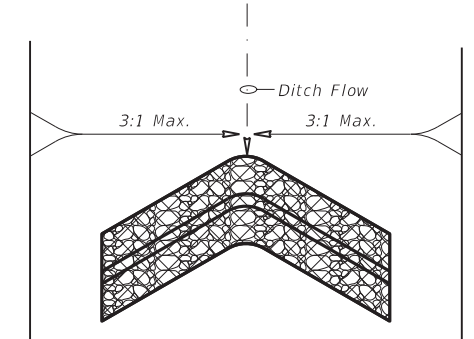
(RFD1) OR (RFD2) OR (RFD3)

GENERAL NOTES

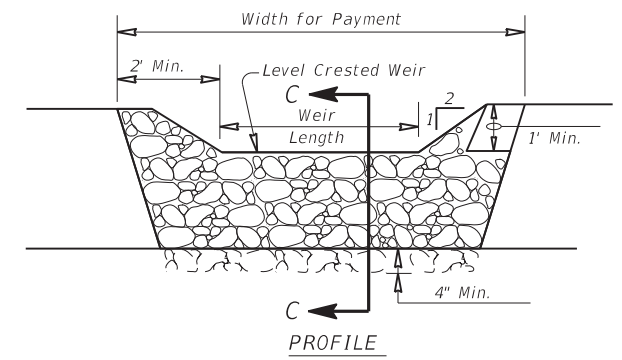
1. If shown on the plans or directed by the Engineer, filter dams should be placed near the toe of slopes where erosion is anticipated, upstream and/or downstream at drainage structures, and in roadway ditches and channels to collect sediment.
2. Materials (aggregate, wire mesh, sandbags, etc.) shall be as indicated by the specification for "Rock Filter Dams for Erosion and Sedimentation Control".
3. The rock filter dam dimensions shall be as indicated on the SW3P plans.
4. Side slopes should be 2:1 or flatter. Dams within the safety zone shall have sideslopes of 6:1 or flatter.
5. Maintain a minimum of 1' between top of rock filter dam weir and top of embankment for filter dams at sediment traps.
6. Filter dams should be embedded a minimum of 4" into existing ground.
7. The sediment trap for ponding of sediment laden runoff shall be of the dimensions shown on the plans.
8. Rock filter dam types 2 & 3 shall be secured with 20 gauge galvanized woven wire mesh with 1" diameter hexagonal openings. The aggregate shall be placed on the mesh to the height & slopes specified. The mesh shall be folded at the upstream side over the aggregate and tightly secured to itself on the downstream side using wire ties or hog rings. For in stream use, the mesh should be secured or staked to the stream bed prior to aggregate placement.
9. Sack Gabions should be staked down with 3/4" dia. rebar stakes, and have a double-twisted hexagonal weave with a nominal mesh opening of 2 1/2" x 3 1/4".
10. Flow outlet should be onto a stabilized area (vegetation, rock, etc.).
11. The guidelines shown hereon are suggestions only and may be modified by the Engineer.



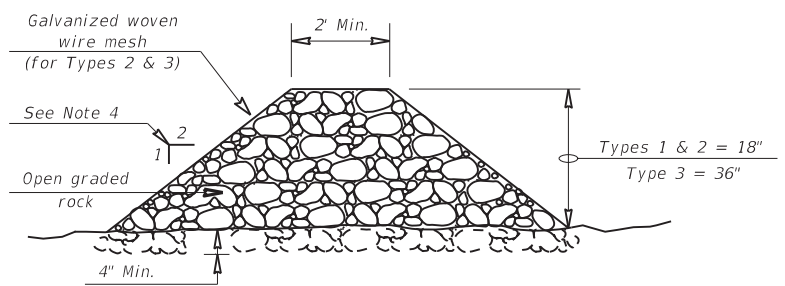
PLAN VIEW



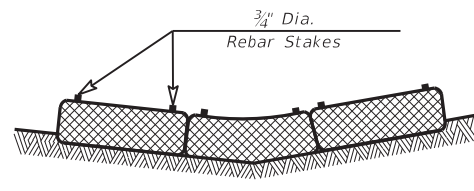
"V" SHAPE PLAN VIEW



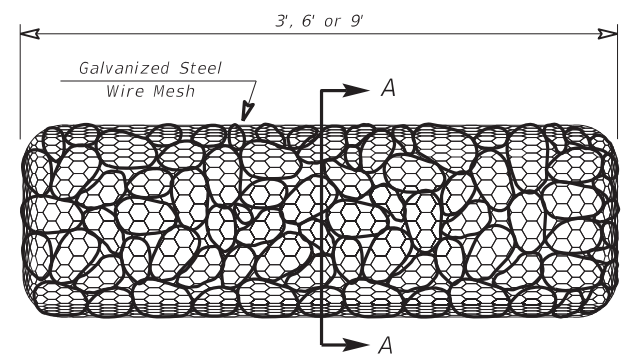
PROFILE



SECTION C-C

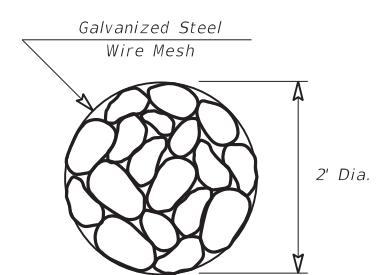


SECTION B-B



TYPE 4 (SACK GABIONS)

(RFD4)



SECTION A-A

ROCK FILTER DAM USAGE GUIDELINES

Rock Filter Dams should be constructed downstream from disturbed areas to intercept sediment from overland runoff and/or concentrated flow. The dams should be sized to filter a maximum flow through rate of 60 GPM/FT<sup>2</sup> of cross sectional area. A 2 year storm frequency may be used to calculate the flow rate.

**Type 1 (18" high with no wire mesh) (3" to 6" aggregate):** Type 1 may be used at the toe of slopes, around inlets, in small ditches, and at dike or swale outlets. This type of dam is recommended to control erosion from a drainage area of 5 acres or less. Type 1 may not be used in concentrated high velocity flows (approximately 8 Ft/Sec or more) in which aggregate wash out may occur. Sandbags may be used at the embedded foundation (4" deep min.) for better filtering efficiency of low flows if called for on the plans or directed by the Engineer.

**Type 2 (18" high with wire mesh) (3" to 6" aggregate):** Type 2 may be used in ditches and at dike or swale outlets.

**Type 3 (36" high with wire mesh) (4" to 8" aggregate):** Type 3 may be used in stream flow and should be secured to the stream bed.

**Type 4 (Sack gabions) (3" to 6" aggregate):** Type 4 May be used in ditches and smaller channels to form an erosion control dam.

**Type 5:** Provide rock filter dams as shown on plans.

PLAN SHEET LEGEND

- Type 1 Rock Filter Dam (RFD1)
- Type 2 Rock Filter Dam (RFD2)
- Type 3 Rock Filter Dam (RFD3)
- Type 4 Rock Filter Dam (RFD4)

		<b>Design Division Standard</b>	
<b>TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES</b> <b>ROCK FILTER DAMS</b> <b>EC(2)-16</b>			
FILE: ec216	DN: TxDOT	CK: KM	DW: VP
©TxDOT: JULY 2016	CONT: 6430	SECT: 55	JOB: 001
REVISIONS	DIST: HOU		COUNTY: FORT BEND
			SHEET NO: 32