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**STATE OF TEXAS
DEPARTMENT OF TRANSPORTATION**

**PLANS OF PROPOSED
STATE HIGHWAY IMPROVEMENT**

FEDERAL PROJECT: STP 2023 (639)HES
US 84
COLEMAN COUNTY

FOR THE CONSTRUCTION OF WIDENING A NON - FREEWAY FACILITY
CONSISTING OF ADDING A CONTINUOUS LEFT TURN LANE

LIMITS: .2 miles north of FM 568 to
.2 south of CR 196

US 84	0054-04-123
ROADWAY =	7408.00 FT = 1.40 MI.
BRIDGE =	0.00 FT = 0.000 MI.
TOTAL =	7408.00 FT = 1.40 MI.

THE CONSTRUCTION WORK WAS PERFORMED IN ACCORDANCE WITH THE
PLANS AND CONTRACT.

P. E.

DATE

FEDERAL AID PROJECT NO.			
STP 2023 (639)HES			
CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN		1

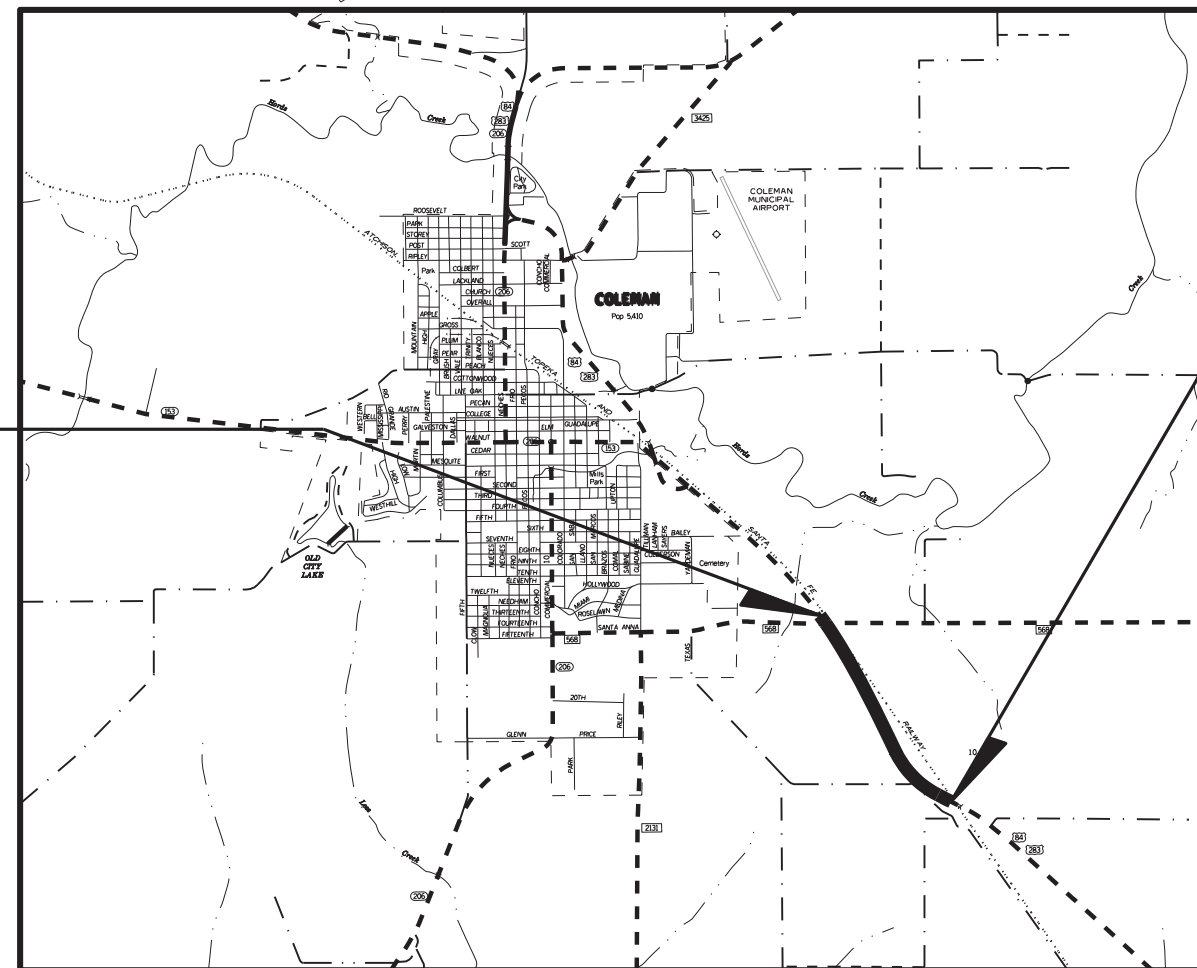
FUNCTIONAL CLASSIFICATION = PRINCIPAL ARTERIAL
DESIGN SPEED = 65
A.D.T.(2021) = 5711
A.D.T.(2041) = 7995

FINAL PLANS

LETTING DATE: _____
DATE CONTRACTOR BEGAN WORK: _____
DATE WORK WAS COMPLETED & ACCEPTED: _____
FINAL CONTRACT COST: \$ _____
CONTRACTOR: _____

REQUIRED SIGNS SHALL BE IN ACCORDANCE WITH
BC (1)-21 THRU BC (12)-21 AND THE "TEXAS
MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".

**BEGIN PROJECT
STA 284+00.00
REF MRKR 536+0.831
CSJ 0054-04-123**



**END PROJECT
STA 358+08.00
REF MRKR 538+0.18
CSJ 0054-04-123**



SUBMITTED FOR LETTING: 2/2/2023

DocuSigned by:
MAH STT, P.E.
77D14777834646F...
DISTRICT DESIGN ENGINEER

RECOMMENDED FOR LETTING: 2/2/2023

DocuSigned by:
MAH STT, P.E.
77D14777834646F...
DISTRICT DIRECTOR OF TRANSPORTATION
PLANNING AND DEVELOPMENT

RECOMMENDED FOR LETTING: 2/3/2023

DocuSigned by:
Gregory W. Cedillo, P.E.
58E2D01C26B344F...
DISTRICT ENGINEER

SCALE IN MILES



NO EXCEPTIONS
EQUATIONS: NONE
NO RAILROAD CROSSINGS

SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION,
NOVEMBER 1, 2014 AND SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS,
SHALL GOVERN ON THIS PROJECT: REQUIRED CONTRACT PROVISIONS FOR ALL
FEDERAL-AID CONSTRUCTION CONTRACTS (FORM FHWA 1273, JULY, 2022)

DATE: \$DATE\$ \$TIME\$
FILE: \$FILES\$

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THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ON THIS SHEET HAVE BEEN ISSUED BY ME AND ARE APPLICABLE TO THIS PROJECT.



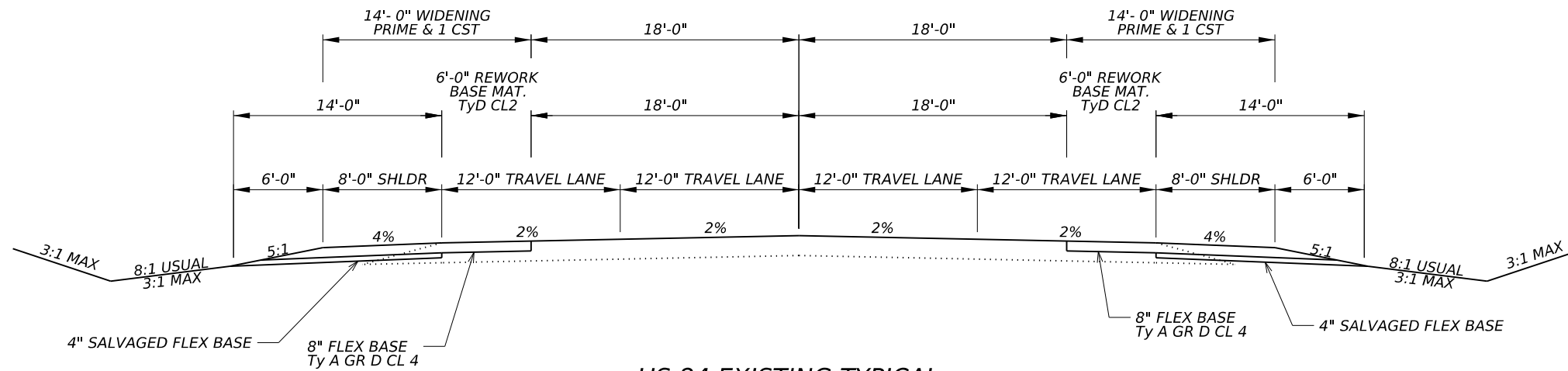
US 84
PROJECT INDEX



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
23	COLEMAN CO.		2

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DWG: CK
DWG: CK



US 84 EXISTING TYPICAL



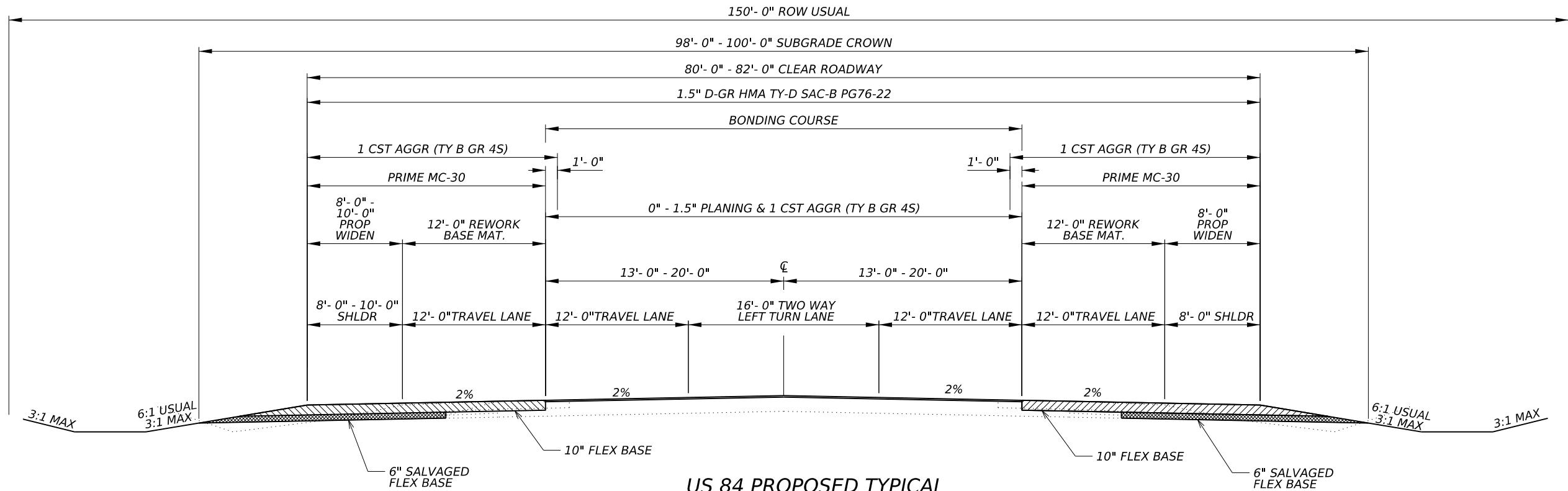
JH Scantling, P.E.

01/26/2023

**US 84
TYPICAL
SECTIONS**

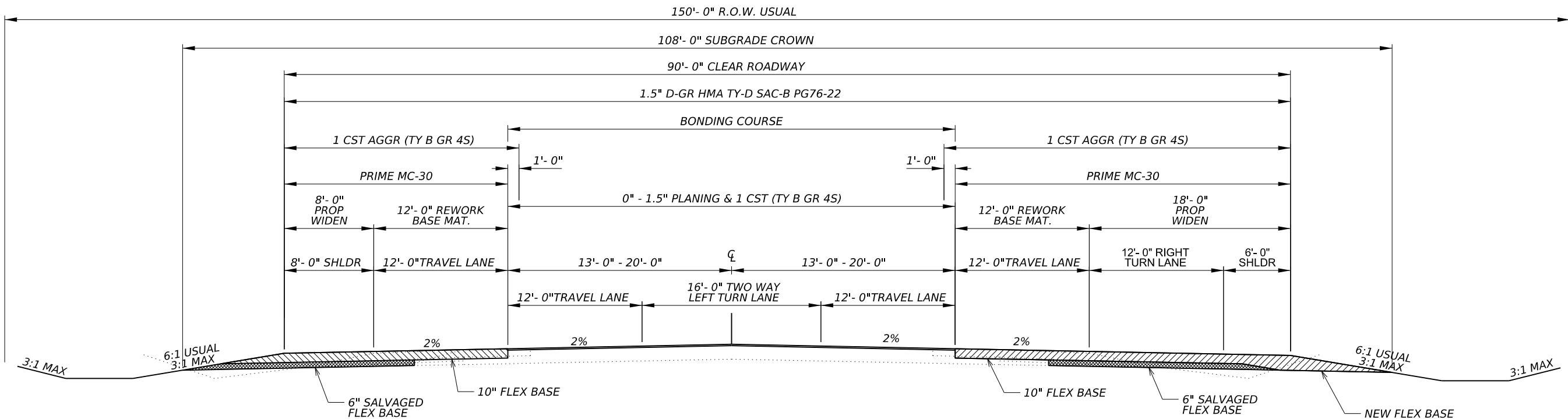


CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
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BWD	COLEMAN CO.	3	



US 84 PROPOSED TYPICAL

TRANSITION STA. 284+00 - STA. 288+80
 STA. 294+30 - STA. 353+40



**US 84 PROPOSED
 RIGHT TURN LANE TYPICAL**

TRANSITION - STA. 288+80 RT - STA. 290+30 RT
 RIGHT TURN LANE - STA. 290+30 - STA. 294+30

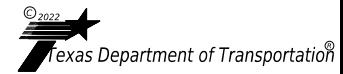
**RIGHT TURN
 LANE TRANSITION
 SURFACE AREA**

TRANSITION - 3741 SF



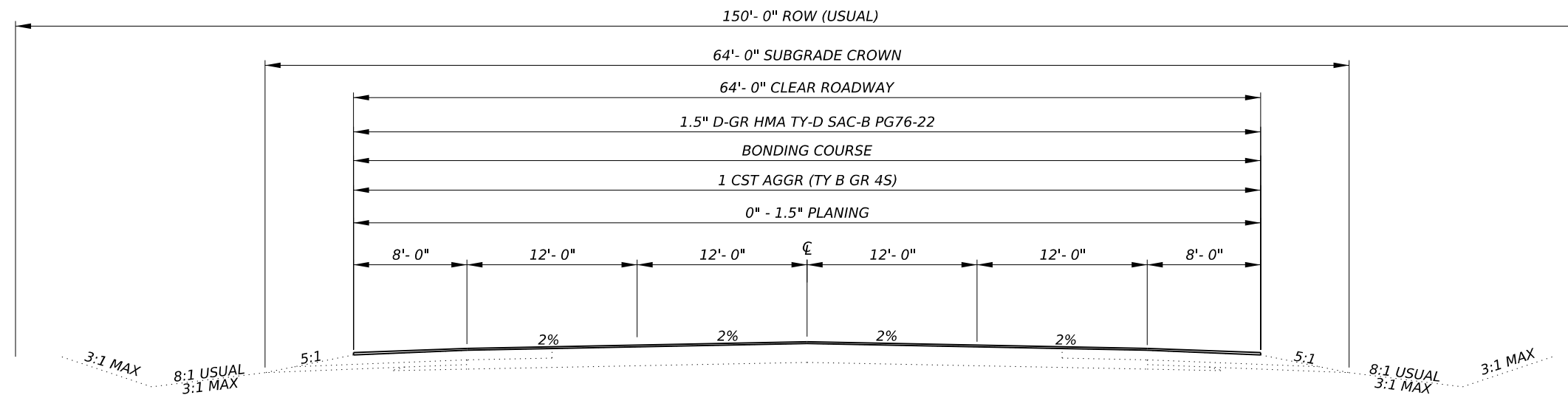
01/26/2023

**US 84
 TYPICAL
 SECTIONS**



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0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
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US 84 PROPOSED TYPICAL
STA. 353+40 - STA. 358+08



JH Scantling, P.E.

01/26/2023

**US 84
TYPICAL
SECTIONS**



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	5	

GENERAL NOTES

TEST TO BE IN ACCORDANCE WITH
TEXAS DEPARTMENT OF TRANSPORTATION
STANDARD TEST METHODS.

Item	Description	Soil Constants		
		Max LL.	Max. PI	Min. PI
* 132	Embankment (Final)(Dens Cont)(Ty C)	40	25	3
247	FI Bs (Cmp In Plc) (Ty D Gr1-2)(Fnal Pos)			3

* Applies to borrow only.

Job control samples for gradation and P.I. testing will be taken from the windrow after blade mixing.

Asphalt Surface Areas-SY

Item	Description	Course	Exist. Roadway	Widening
310	PRIME COAT (MC-30)	Prime		31520
316	ASPH (CRS2)	1 st	35106	31520
316	AGGR (TY PB) GR 4S	1 st	35106	31520
3084	BONDING COURSE	Final	33563	
3077	1.5" SP-D SAC-B PG 76-22	Final	33563	31520

Basis of Estimate

Item	Description	Course	Rate	SY	Quantity
310	PRIME COAT (MC-30)	Prime	0.20 Gal/SY	31520	6304 Gal
316	ASPH (CRS2)	1 st	0.48 Gal/SY	66626	31981 Gal
316	AGGR (TY PB) GR 4S	1 st	120 SY/CY	66626	556 CY
3084	BONDING COURSE	Final	0.14 Gal/SY	33563	4699 Gal
3077	SP-D SAC-B PG 76-22	Final	115 lbs/sy/in	65082	5614 Tons

No equipment shall be allowed within twenty five feet (25') of the railroad tracks.

The Contractor will not be allowed to store equipment, materials, incidentals, hazardous chemicals, petroleum products, concrete washouts, etc. in the Department's R.O.W. without written permission from the Engineer.

Trees that are to be trimmed and brush that is to be trimmed or removed that are not over the roadway or bridge(s), will be trimmed or removed in accordance with the Roadside Vegetation Management Manual to a height of fourteen feet. Remove limbs at the trunk with less than twenty-one feet of clearance above the pavement or bridge(s).

**See the "Environmental" section of the plans for additional information.
TEXAS ONE CALL**

Fiber optic cable systems, gas lines, underground power lines, water lines, sewer lines, and other various utilities may be buried within the project limits. Protection of these utility systems is of extreme importance since any break could disrupt service to users resulting in business interruption and loss of revenue and profits. The Contractor will telephone Texas One Call at 1-800-344-8377 (a 24-hour number), to determine if utilities are buried anywhere on the project in accordance with all UNDERGROUND FACILITY DAMAGE PREVENTION AND SAFETY laws. This action; however, will in no way be interpreted as relief of responsibilities under the terms of the Contract as set out in the plans and specifications. Coordinate the repair of all damages caused by daily operations and have facilities restored to service in a timely manner as directed at no additional cost to TxDOT.

GENERAL

Unless specifically noted as applying to only a certain project or projects, these general notes will apply to all projects associated to this contract.

Contractor questions on this project are to be addressed to the following individual(s):

<u>Name</u>	<u>Email Address</u>
Bart Fris P.E.	bart.fris@txdot.gov

Contractor questions will be accepted through email, phone, and in person by the above individuals. Questions may also be submitted via the Letting Pre-Bid Q&A web page. This webpage can be accessed from the Notice to Contractors dashboard located at the following Address:
<https://tableau.txdot.gov/views/ProjectInformationDashboard/NoticetoContractors>

All contractor questions will be reviewed by the Engineer. All questions and any corresponding responses that are generated will be posted through the same Letting Pre-Bid Q&A web page.

The Letting Pre-Bid Q&A web page for each project can be accessed by using the dashboard to navigate to the project you are interested in by scrolling or filtering the dashboard using the controls on the left. Hover over the blue hyperlink for the project you want to view the Q&A for and click on the link in the window that pops up.

The term "Article" or "Section" referred to hereon is defined in the forward of the Standard Specifications for Construction and Maintenance of Highways, Streets, And Bridges adopted by the Texas Department of Transportation November 2014.

A "Regulatory Construction Speed Zone" has been requested for this project.

Saw-Cutting with approved equipment as directed by the Engineer will be required at project limits, longitudinally, and/or at notch downs to establish clean and straight joints. This work will not be paid for directly but will be considered subsidiary to various bids.

The Contractor will establish drainage in ditches before seeding or as directed by the Engineer. Watering for dust control will be required as Directed by the Engineer and will be considered subsidiary to the various bid items.

ITEM 5 CONTROL OF WORK

The responsibility for the construction surveying on this contract will be in accordance with Section 5.9.1. "Method C".

The contractor will be required to place and maintain Blue Tops with wooden hubs for each layer of pavement structure material unless otherwise directed by the Engineer.

Prior to contract letting, bidders may obtain a computerized transfer of files (from the Engineer's office) that contains the earthwork information.

ITEM 6 CONTROL OF MATERIALS

To comply with the latest provisions of Build America, Buy America Act (BABA Act) of the Bipartisan Infrastructure Law, the contractor must submit a notarized original of the TxDOT Construction Material Buy America Certification Form for all items classified as construction materials. This form is not required for materials classified as a manufactured product.

Refer to the Buy America Material Classification Sheet for clarification on material categorization.

The Buy America Material Classification Sheet is located at the below link.
<https://www.txdot.gov/business/resources/materials/buy-america-material-classification-sheet.html>
 for clarification on material categorization.

In accordance with **Section 6.10.2**, the Contractor will dispose of all painted steel at a steel recycling or smelting facility and a receipt will be required. In lieu of this, the Contractor has the option to either show proof that the paint is lead free or show proof that the lead paint has been abated by an abatement certified company. The Department will not be obligated for the cost of paint testing and/or abatement materials, processes, personnel, incidentals, etc.

ITEM 7 LEGAL RELATIONS AND RESPONSIBILITIES

No significant traffic generator events identified.

ITEM 8 PROSECUTION AND PROGRESS

Working days will be computed and charged in accordance with Section 8.3.1.4. "Standard Workweek". Work will not be performed without time being charged unless otherwise exempted by the Section as defined above.

Working day charges will be in accordance with **SP 008---003** (90 calendar days after the date of the written authorization to begin work. Do not begin any work before the end of this period unless authorized in writing by the Engineer.) **This delay is for the production of base material.**

Construction will be completed in order, sequentially; as described in the traffic control plan phasing. Each step/phase will be completed before starting on the next step/phase unless otherwise approved by the Engineer.

PROJECT SCHEDULES

Critical Path Method (CPM) scheduling will be required to be submitted and maintained monthly by the Contractor unless otherwise directed by the Engineer. (8.5.2.)

For monthly submittals, the Contractor will provide the schedule in an Adobe Acrobat compatible format (PDF file). If the Engineer requests the schedule in an electronic format, the Contractor will submit a schedule that is fully compatible with Primavera P6 Professional Release 15.

ITEM 9 MEASUREMENT AND PAYMENT

Monthly estimates will be computed from the 28th of the previous month through the 27th of the current month unless otherwise approved in writing by the Engineer.

ITEM 100 PREPARING RIGHT OF WAY

Remove all trees, brush, and shrubs within the R.O.W., unless otherwise directed by the Engineer. Perform Preparing Right of Way in such a manner that does not disturb the native grasses unnecessarily.

ITEM 104 REMOVING CONCRETE

The Contractor will make a 1" cut to use as a guide before full depth cutting. Saw-Cut the full depth through the concrete before existing pavement removal.

ITEM 132 EMBANKMENT

Refer to Item 210 "Rolling" for additional roller requirements.

Shape the embankment, near the drainage structures, to the slope of the safety end treatment.

Embankment for the drainage structures is included in the quantities shown on the plan & profile sheets.

Embankment as shown in the plans or placed as directed will be placed before the installation of MBGF.

ITEM 150 BLADING

Blading will be used to clear the pavement edge of existing vegetation. After final surface placement, blade windrow back to edge of pavement to eliminate pavement edge drop-offs.

Within the construction limits, blade and windrow the top 8 inches of vegetative material to just outside the construction limits. Once ditch slopes and drainage have been established and approved, blade the windrow evenly over the disturbed area within the construction limits. This work is to be done as the job progresses and in conjunction with seeding. Work on the project may be suspended, if in the opinion of the Engineer, the Contractor does not make a good faith effort to stabilize loose material as the project progresses. Time will not be suspended. This work is subsidiary to Item 150.

ITEM 164 SEEDING FOR EROSION CONTROL

The Contractor should anticipate multiple mobilizations for seeding at each project location.

Additional wildflower seed will be required to be added to the seeding mixture. The wildflower seed will be provided by TxDOT and is estimated at 5 lbs/acre in addition to the required seeding as specified in Item 164. The Contractor will notify the Area Engineer a minimum of 4 weeks in advance of permanent/final seeding to ensure time for the proper seed to be acquired. The Contractor can acquire this additional seed at the County Maintenance office. The equipment, labor, tools, and incidentals to mix and apply this seed will be considered subsidiary to Item 164.

ITEM 166 FERTILIZER

Fertilize all areas of project to be seeded.

Furnish and apply fertilizer with analysis of 20-10-10 at a rate of 300 bulk pounds per acre.

ITEM 168 VEGETATIVE WATERING

Water all areas of project to be seeded or sodded.

Vegetative watering is estimated at 1 inch per week for 4 weeks.

Vegetative watering may be adjusted as directed by the Engineer to ensure saturation for vegetative establishment.

ITEM 210 ROLLING

Required Roller Type and Size for Compacted Layers

Thickness of compacted lift	Minimum Static weight of roller (tons)	Drum Type
-----------------------------	--	-----------

< 6 inches	12	Smooth
6 to 7 inches	15	Smooth or Padfoot
8 to 9 inches	18	Padfoot
10 inches or greater	20	Padfoot

ITEM 216 PROOF ROLLING

Proof Rolling will be required for right turn lane and outside traffic lanes throughout the entire project and is estimated at 2.5 hours. Calculate at 2 mph (2 passes per lane) for the full project.

ITEM 247 FLEXIBLE BASE

Refer to Item 210 for additional roller requirements.

Ride quality will be measured before the application of prime coat unless otherwise approved in writing by the Engineer.

A grader (a road grader, a blade, a maintainer, or a motor grader) will be used to process base unless otherwise approved by the Engineer.

Do not add field sand to modify the finish material to meet requirements.

Place new flexible base in lifts of approximately equal depth not to exceed 6 inches unless otherwise directed.

ITEM 251 REWORKING BASE COURSES

Refer to Item 210 for additional roller requirements.

Grade flexible base to typical section and profile to match existing grade. Contractor will establish grade to produce a smooth ride as directed.

ITEM 310 PRIME COAT

Cure prime placed with a cutback asphalt binder for 21 days before placing subsequent surface courses unless otherwise directed by the Engineer.

Finished base must be dampened before the application of a cutback asphalt binder is placed. This work will not be paid for directly but will be considered subsidiary to Item 310.

If MC-30 is used for prime, sanding may be required at intersections, drives and other areas as directed.

ITEM 316 SURFACE TREATMENTS

Furnish aggregate with a minimum B surface aggregate classification.

Warm season asphalts are not to be placed between September 1st and April 30th unless otherwise directed/approved.

CRS-2 will be used for cool season use, unless otherwise directed by the Engineer; and can be placed between September 1st and April 30th in accordance with the suppliers recommendations. A 90 day cure time may be required prior to placing 2nd course.

Protect all existing bridges, and other exposed concrete surfaces within the limits of this project(s), as much as practical, from asphalt materials by any means approved by the Engineer at the contractor's expense.

Use a medium pneumatic roller meeting the requirements of Item 210 as directed by the Engineer. This work will be subsidiary to the various bid items.

ITEM 354 PLANING AND TEXTURING PAVEMENT

The planed asphaltic material will be stockpiled at the intersection of CR 196 and CR 194 or as directed by the Engineer. This material will remain property of the Department.

Contractor will provide a 12-foot minimum milling drum. The drum will have a maximum tooth spacing of 5/8 inches and have a minimum of 3 wraps of teeth.

Milling operations will not advance faster than 30 feet per minute (fpm) or be based as a function of the RPMs of the milling drum such that the full uniform texture pattern is achieved with the speed of the milling operation in fpm limited to 30% of the drums RPMs. Any proposal to advance faster than this speed will be discussed with the Engineer and proven on a test strip of the Engineer's choosing, and will result in no repeated inconsistencies in texture during production milling. If inconsistencies are present, the machine speed will be reduced as directed by the Engineer.

ITEM 420 CONCRETE SUBSTRUCTURES

Culverts will be constructed in conjunction with roadway construction phasing, unless otherwise directed by the Engineer.

All Class C Concrete has been measured for plan quantity payment.

Unless otherwise shown on the plans, all culvert extensions and safety end treatments will conform to the existing culvert slope

ITEM 421 HYDRAULIC CEMENT CONCRETE

Furnish dome lids with 4" x 8" cylinder test molds.

Strength testing equipment is not required for Contract controlling test.

ITEM 427 SURFACE FINISHES FOR CONCRETE

Surface Area II will receive a rub finish.

ITEM 432 RIPRAP

Locations and quantities may be varied as directed by the Engineer to accommodate field conditions.

Riprap (Conc) (Cl B) is required inside all Type I safety end treatments, unless otherwise directed by the Engineer.

Limit excavation to within 1' of riprap. If excavation exceeds these limits without the Engineer's approval, riprap will be extended to the limits of the disturbance. No additional compensation will be allowed for this work.

ITEM 502 BARRICADES, SIGNS, AND TRAFFIC HANDLING

The Contractor will be required to keep all TCP devices clean. If notified by the Engineer to clean the TCP devices, the Contractor will have until the end of that daylight period to comply. Failure to comply will result in a suspension of all work until the TCP devices are clean. Time will not be suspended.

Work zone lengths will be limited to 2 mile sections unless otherwise approved by the Engineer.

Work zone lengths will be limited to a 20-minute turnaround time unless otherwise approved by the Engineer.

The Contractor Force Account "Safety Contingency" that has been established for this project is intended to be utilized for work zone enhancements, to improve the effectiveness of the Traffic Control Plan, that could not be foreseen in the project planning and design stage. These enhancements will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent traffic management reviews on the project. The Engineer may choose to use existing bid items if it does not slow the implementation of enhancement.

The Engineer will determine the locations of regulatory construction speed zone signs. The Contractor will furnish, install and remove speed zone signs at locations as directed by the Engineer.

Excavations in Intersections adjacent to travel lanes will not be exposed or open overnight. Backfilling will take place the day excavations are made.

The Contractor will be responsible for maintaining the edge of the roadway throughout the project in a traversable condition and/or as directed by the Engineer. Salvaged milling shall be used as directed by the Engineer. This work will not be paid for directly and will be considered subsidiary to Item 502 "Barricades, Signs, and Traffic Handling".

All devices shown on the TCP Standards are required and considered subsidiary to Item 502 unless specifically outlined elsewhere in the plans.

All signs will be constructed in accordance with the details shown in the current Standard Highway Sign Designs for Texas manual.

ITEM 506 TEMPORARY EROSION, SEDIMENTATION, AND ENVIRONMENTAL CONTROLS

The Contractor should anticipate multiple mobilizations for the installation of BMP's on this project.

The Engineer will determine actual time and placement locations of BMP's and temporary measures.

Contractor will not install BMPs until locations are approved by the Engineer.

Stockpile sites may be cleared of cover vegetation, but the vegetation root system will not be destroyed.

Erosion Control Logs Dam (CL-D) shall have stakes placed upstream in an alternating pattern of the downstream stakes as shown for CL-SST or CL-SSL details on the Erosion Control Standards.

ITEM 529 CONCRETE CURB, GUTTER, AND COMBINED CURB AND GUTTER

Reinforcing steel will be required in all curb and gutter.

ITEM 530 INTERSECTIONS, DRIVEWAYS, AND TURNOUTS

The Contractor will always maintain access to driveways unless otherwise coordinated with the property owner(s) and approved by the Engineer.

All intersections will be primed and receive a one course surface treatment, and ACP matching the rates as shown on the basis of estimate for "ROADWAY" unless otherwise shown on the plans or directed by the Engineer.

All driveways will be primed and receive a one course surface treatment matching the rates as shown on the basis of estimate for "ROADWAY" unless otherwise shown on the plans or directed by the Engineer.

ITEM 540 METAL BEAM GUARD FENCE

Metal beam guard fence will not be installed until the embankment, flex base, and/or one course surface treatment is complete.

ITEM 585 RIDE QUALITY FOR PAVEMENT SURFACES

Surface Test Type B will be required on this project.

Diamond grinding will not be allowed unless otherwise approved by the Engineer.

Refer to Item 247 and **SP 247-003** for ride quality requirements.

ITEM 600 ELECTRIC GENERAL

Electrical materials, wiring, and fittings not covered by the plans and specifications for this project will conform to the requirements of the current edition of the National Electrical Code as published by the National Fire Protection Association.

Contractor will maintain signals through construction with the exception of camera detection. Contractor will notify the District Director of Operations at 325-643-0417, 48 hours prior to beginning any electrical related work items and 48 hours prior to traffic switch so the district signal personnel can adjust the camera detection.

Electrical Contractor, Signal Shop personnel and Project Inspector will conduct a 'Tool Box' meeting to discuss upcoming electrical work.

All materials will be from the pre-qualified material producer list, "Roadway Illumination and Electrical Supplies" located on the TxDOT website. Electrical submittals will be required for all materials not on the pre-qualified list.

All electrical submittals will be forwarded to District Director of Operations (325-643-0417). No electrical work will be performed prior to approval of electrical materials.

ITEM 610 ROADWAY ILLUMINATION ASSEMBLIES

All luminaire poles will be steel.

For both transformer and shoe-base type illumination poles, provide double-pole breakaway fuse holder as shown on the Texas Department of Transportation (TxDOT)-Construction Division's (CST) material producers list. Category is "Roadway Illumination and Electrical Supplies". Fuse holder is shown on list under Items 610 & 620. Provide 10 amp time delay fuses.

For instructions on submitting shop drawings electronically go to TxDOT home page, Divisions (bottom left), Bridge, Shop Drawings, Electronic Submission of Shop Drawings, Guide to Electronic Shop Drawing Submittal.pdf or click on the following link:

http://ftp.dot.state.tx.us/pub/txdot-info/library/pubs/bus/bridge/e_submit_guide.pdf

For project specific shop drawings, furnish drawings of the complete assembly in accordance with Item 441, "Steel Structures". Submit shop drawings electronically.

Pre-approved shop drawing manufacturers and assembly model numbers can be found on the Texas Department of Transportation(TxDOT) – Construction Division's(CST) material producer list. Category is "Roadway Illumination and Electrical Supplies."

Provide 12 circuit Buchanan Type 112SN, Kulka Type 985-GP-10 CU, or equal terminal strip in the luminaire pole access compartment. The conductors for the line and load side of the terminal strip will be identified with a plastic label with two straps per tag. The load side will have each signal head and ped head identified on the tag.

Fabricate steel roadway illumination poles in accordance with TxDOT standards RIP (Roadway Illumination Poles -2011). Poles fabricated according to RIP require no shop drawings. Alternate designs to RIP or the use of aluminum to fabricate poles will require the submission of shop drawings electronically.

Limitations on Use of the RIP Standard

The Roadway Illumination Pole (RIP) standard details were developed for installations in locations where the 3-second gust basic maximum wind speed is 110 mph, and where the elevation of the base of the pole is less than (i.e. not more than) 25' above the elevation of surrounding terrain, in accordance with the "AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals," 4th Edition (2001) (AASHTO Design Specifications). For poles to be installed in regions where the maximum basic wind speed exceeds 110 mph or to be mounted more than 25' above the surrounding terrain, the contractor will provide poles meeting the following requirements:

- Submittals. Following the electronic shop drawing submittal process (see http://ftp.dot.state.tx.us/pub/txdot-info/library/pubs/bus/bridge/e_submit_guide.pdf) the contractor will submit to the Engineer, for approval, fabrication drawings and calculations for the poles. The drawings and calculations will be sealed by a Texas registered or licensed professional engineer (P.E.).
- Luminaire Structural Support Requirements. Lighting poles, arms, and anchor bolt assemblies will have a 25 year design life to safely resist dead loads, ice loads and the required basic wind speeds at the location of installation in accordance with the current edition of the AASHTO Design Specifications. For transformer base poles, the fabricator will include transformer base and connecting hardware in calculations and shop drawing submittals. All transformer bases will have been structurally tested to resist the theoretical plastic moment capacity of the pole. Certification of the plastic moment load test and FHWA breakaway requirement test of the model of base being furnished will be submitted with the shop drawings. Shop drawings will show breakaway base model number, and manufacturer's name and logo. Manufacturer's shop drawings will include the ASTM designations for all materials to be used.

ITEM 618 CONDUIT

All conduit will be SCH 80 PVC.

Where PVC, duct cable, and HDPE conduit 1" and larger is allowed and installed as per TxDOT standards, provide a PVC elbow at all ground boxes and foundations.

See plans & specifications regarding type of conduit. High density polyethylene (HDPE) may be substituted where PVC is called out. High density polyethylene (HDPE) may be threaded and used with

threaded PVC connectors or couplings. All couplings & connections will be tight & waterproof. Each end of every PVC pipe connection and/or coupling will be cleaned with PVC cleaner and glued thoroughly with PVC sealer. Proposed and existing conduit will be brought into a pull box and elbowed unless otherwise shown. Where a rigid metal conduit run terminates, a bushing will be provided to protect the wire from abrasion.

The conduit will be placed at a minimum depth of two 2 ft. unless otherwise shown on the plans or directed by the Engineer. If utility lines or other obstacles are at the 2 ft. minimum depth then the conduit will be routed under the utility or obstacle unless otherwise approved by the Engineer.

The conduit will be placed on a 2 in. Sand cushion and then backfilled with a minimum of six inch (6") sand fill. The remainder of the trench will be backfilled with flexible base or soil as required by location of conduit on the project.

Flexible metal will not be permitted on this project.

Do not use cast iron junction boxes in concrete traffic barriers and single slope traffic barriers. Use polymer concrete junction boxes instead of the cast iron junction boxes shown on standard sheets CTBI (3), CTBI (4), and SSCB (4). Mount the junction boxes flush (+ 0", - 1/2") with concrete surface of concrete barrier.

Use materials from prequalified material producers list as shown on the Texas Department of Transportation (TxDOT) - Construction Division's (CST) material producer list. Category is "Roadway Illumination and Electrical Supplies."

The polymer concrete barrier box will not be paid for separately, but will be considered subsidiary to ITEM 618, "CONDUIT".

ITEM 620 ELECTRICAL CONDUCTORS

Labeling conductors with label maker is acceptable.

Grounding conductors that share the same conduit, junction box, ground box or structure will be bonded together at every accessible point in accordance with the National Electrical Code.

For all grounded conductors use Bussman HET, Littlefuse LET, Ferraz-Shawmut FEBN, or equal. These breakaway connectors have a white colored marking and a permanently installed solid neutral.

ITEM 624 GROUND BOXES

All concrete used on ground boxes with aprons or cast in various slabs, will be as thick as the ground box depth within the dimensions as shown on TxDOT's ED Standard Sheets. No variance from this will be allowed.

ITEM 628 ELECTRICAL SERVICE

Any service installed by others will comply with all TxDOT standards from weather-head to fixtures.

Coordinate setting up the electrical service with the Area Engineer to insure the meter is installed under the proper account name.

Photocell enclosed in pedestal services will be orientated in a northerly direction unless otherwise directed.

The Contractor will verify conductor slack length at the weather head with the utility provider. If the utility provider requires a conductor slack length that does not meet the requirements shown on ED(7) notify the Engineer immediately for a resolution.

ITEM 644 SMALL ROADSIDE SIGN ASSEMBLIES

The Contractor will notify the Engineer 5 working days before installing any sign base. The Engineer will coordinate with the Contractor and the Maintenance office to assure proposed sign placements are in accordance with the current version of the Sign Crew Field Book and the TMUTCD. Any signs that are placed without this coordination by the Contractor that are not located correctly will be removed and relocated at the Contractor's expense.

Triangular Slip Bases will be supplied by TxDOT. All other components of the sign assembly (stubs, posts, hardware, signs, etc.) will be supplied by the Contractor. The Contractor can acquire the bases at the Mills County Maintenance office located at 130 US Hwy 84, Goldthwaite, Tx. Contact the Mills County Maintenance Supervisor (Christopher Smith) at (325) 648-3028 for further information.

For Triangular Slip Base systems use HWYCOM (3 way set screw), Southern Plains (2 bolt clamp), or approved equivalent.

Build signs not detailed in the plans according to the latest edition of the Standard Highway Sign Designs for Texas.

TxDOT will mark the locations of the SPEED LIMIT (R2-1) and REDUCED SPEED LIMIT AHEAD (W3-5) signs.

Existing roadside signs are to be removed/relocated and mounted on temporary supports and placed during construction as directed by the Engineer. The removal/relocation and temporary mounting of any existing sign (stop, yield, warning, etc.) will not be paid for directly but will be considered subsidiary to Item 644 unless otherwise directed by the Engineer.

Signs that are to be transferred to new posts must be placed upon the new supports before the end of the working day. Regulatory signs must be transferred immediately.

Conformable Retroreflective Sheeting in accordance with DMS 8300 will be required on all Warning, Stop, and Yield signs. Retroreflective sheeting wrapped around a sign support is yellow unless the sign on the support is a Stop or Yield, in which case the sheeting will be red. Retroreflective sheeting will have a height on the post of 12 inches and the bottom of the sheeting will be 4 feet above the edge of the travel lane. Retroreflective sheeting will not be paid for directly but will be considered subsidiary to Item 644 Small Roadside Sign Assemblies.

ITEM 662 WORK ZONE PAVEMENT MARKINGS

Removable work zone pavement markings will be raised pavement markers unless otherwise approved by the Engineer.

For milled surfaces, removable work zone pavement markings will be pavement tape markings unless otherwise approved by the Engineer.

Bituminous material used for raised pavement markers will be removed before the next lift of pavement material is placed.

Temporary tabs will not be placed on a road more than 24 hours prior to operations beginning on the road.

The temporary tabs will be removed by an acceptable method approved by the Engineer once final striping has been placed.

TY II Paint will be allowed for non-removable work zone pavement markings.

Short term pavement markings will be installed in accordance with WZ(STPM)-13.

ITEM 666 RETROREFLECTORIZED PAVEMENT MARKINGS

A mobile retroreflectometer is not required for this project.

Furnish a needlepoint micrometer gauge Mitutoyo - Model 342-711-30 or equivalent.

Sealed roadways will be allowed to cure for 3 days before final striping is placed unless otherwise directed by the Engineer.

Unless otherwise approved, all 6 in. longitudinal striping (centerline, edgeline, etc.) will be placed and approved before any other striping (crosswalks, stop bars, arrows, numbers, etc.) is allowed to begin.

ITEM 677 ELIMINATING EXISTING PAVEMENT MARKINGS AND MARKERS

Use "Blasting Method" in accordance with 677.4.3 for eliminating existing pavement markings. Water blasting will be the only allowable option. A vacuum recovery system will be required as approved.

ITEM 3077 SUPERPAVE MIXTURES

Binder substitution is not allowed.

RAP and RAS will not be allowed.

Superpave Mix to be placed in one lift.

Surge Volume and Remixing MTV will be required for this project.

During paving operations; proper adjustment of Surge Volume and Remixing MTV is required to ensure clean pickup of HMAC and to have residual HMAC not be in excess of 1/4" to 3/8" as approved by the Engineer. HMAC will not be dumped in a windrow that is determined by the Engineer to be an excessive distance from the paving operation.

Belly dumps will not be allowed if a spray paver is used.

See item 504 for additional structure requirements located at HMAC plant(s).

ITEM 3084 BONDING COURSE

Rates will be adjusted in the field based on the exposed surface as directed by the Engineer.

A test strip will be required.

ITEM 6185 TRUCK MOUNTED ATTENUATOR (TMA) AND TRAILER ATTENUATOR (TA)

Provide the number of vehicles with truck mounted attenuators (TMA) listed in the table below. The Contractor will be responsible for determining if one or more of these operations will be ongoing at the same time to determine the total number of TMAs needed for the project.

STANDARD / PHASE	# TMA'S REQUIRED
TCP(1-2)	1
TCP(2-1)	1
TCP(2-2)	1
TCP(2-5)	1 per workspace
TCP(3-1)	2
TCP(3-3)	2 or 3
TCP(7-1)	N/A to be used in conjunction with another TCP

Stationary shadow vehicle(s) with TMA are estimated at 185 days for this project.

Mobile shadow vehicle(s) with TMA are estimated at 176 hours for this project.



Estimate & Quantity Sheet

CONTROLLING PROJECT ID 0054-04-123

DISTRICT Brownwood
HIGHWAY US 84

COUNTY Coleman

CONTROL SECTION JOB				0054-04-123		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00178843			
COUNTY				Coleman			
HIGHWAY				US 84			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL		
	100-6006	PREP ROW (TREE)(LESS THAN 24" DIA)	EA	2.000		2.000	
	104-6021	REMOVING CONC (CURB)	LF	340.000		340.000	
	104-6054	REMOVING CONCRETE(MOW STRIP)	LF	900.000		900.000	
	110-6001	EXCAVATION (ROADWAY)	CY	15,069.000		15,069.000	
	132-6006	EMBANKMENT (FINAL)(DENS CONT)(TY C)	CY	6,209.000		6,209.000	
	150-6001	BLADING	STA	12.000		12.000	
	164-6009	BROADCAST SEED (TEMP) (WARM)	SY	16,956.000		16,956.000	
	164-6011	BROADCAST SEED (TEMP) (COOL)	SY	16,956.000		16,956.000	
	168-6001	VEGETATIVE WATERING	MG	757.000		757.000	
	216-6001	PROOF ROLLING	HR	2.500		2.500	
	247-6053	FL BS (CMP IN PLC)(TYD GR1-2)(FNAL POS)	CY	8,856.000		8,856.000	
	251-6086	REWORK BS MTL (TY C)(10")(DENS CONT)	SY	18,027.000		18,027.000	
	310-6009	PRIME COAT (MC-30)	GAL	6,304.000		6,304.000	
	316-6022	ASPH (CRS-2)	GAL	31,981.000		31,981.000	
	316-6174	AGGR(TY-B GR-45 SAC-B)	CY	556.000		556.000	
	354-6051	PLANE ASPH CONC PAV (0" TO 1 1/2")	SY	33,563.000		33,563.000	
	416-6029	DRILL SHAFT (RDWY ILL POLE) (30 IN)	LF	10.000		10.000	
	420-6051	CL C CONC (CULV)	CY	1.500		1.500	
	432-6003	RIPRAP (CONC)(6 IN)	CY	86.000		86.000	
	432-6045	RIPRAP (MOW STRIP)(4 IN)	CY	57.000		57.000	
	460-6010	CMP AR (GAL STL DES 3)	LF	328.000		328.000	
	467-6109	SET (TY I)(S=3 FT)(HW= 3 FT)(6:1)(C)	EA	2.000		2.000	
	467-6536	SET (TY II) (DES 3) (CMP) (6: 1) (C)	EA	18.000		18.000	
	496-6007	REMOV STR (PIPE)	LF	147.000		147.000	
	500-6001	MOBILIZATION	LS	1.000		1.000	
	502-6001	BARRICADES, SIGNS AND TRAFFIC HANDLING	MO	7.000		7.000	
	506-6038	TEMP SEDMT CONT FENCE (INSTALL)	LF	1,200.000		1,200.000	
	506-6039	TEMP SEDMT CONT FENCE (REMOVE)	LF	1,200.000		1,200.000	
	506-6041	BIODEG EROSN CONT LOGS (INSTL) (12")	LF	696.000		696.000	
	506-6043	BIODEG EROSN CONT LOGS (REMOVE)	LF	696.000		696.000	
	529-6014	CONC CURB (MOD) (TYPE I)	LF	323.000		323.000	
	530-6002	INTERSECTIONS (ACP)	SY	627.000		627.000	
	530-6006	DRIVEWAYS (SURF TREAT)	SY	1,785.000		1,785.000	
	533-6001	RUMBLE STRIPS (SHOULDER)	LF	14,653.000		14,653.000	
	533-6002	RUMBLE STRIPS (CENTERLINE)	LF	14,256.000		14,256.000	
	540-6002	MTL W-BEAM GD FEN (STEEL POST)	LF	2,521.000		2,521.000	
	540-6006	MTL BEAM GD FEN TRANS (THRIE-BEAM)	EA	2.000		2.000	

DISTRICT	COUNTY	CCSJ	SHEET
Brownwood	Coleman	0054-04-123	



Estimate & Quantity Sheet

CONTROLLING PROJECT ID 0054-04-123

DISTRICT Brownwood
HIGHWAY US 84

COUNTY Coleman

CONTROL SECTION JOB				0054-04-123		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00178843			
COUNTY				Coleman			
HIGHWAY				US 84			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL		
	542-6001	REMOVE METAL BEAM GUARD FENCE	LF	2,700.000		2,700.000	
	544-6001	GUARDRAIL END TREATMENT (INSTALL)	EA	6.000		6.000	
	610-6004	RELOCATE RD IL ASM (TRANS-BASE)	EA	1.000		1.000	
	636-6001	ALUMINUM SIGNS (TY A)	SF	31.000		31.000	
	644-6001	IN SM RD SN SUP&AM TY10BWG(1)SA(P)	EA	11.000		11.000	
	644-6004	IN SM RD SN SUP&AM TY10BWG(1)SA(T)	EA	8.000		8.000	
	644-6076	REMOVE SM RD SN SUP&AM	EA	20.000		20.000	
	658-6062	INSTL DEL ASSM (D-SW)SZ 1(BRF)GF2(BI)	EA	57.000		57.000	
	658-6073	INSTL OM ASSM (OM-2Y)(WC)GND(BI)	EA	2.000		2.000	
	662-6008	WK ZN PAV MRK NON-REMOV (W)6"(SLD)	LF	19,950.000		19,950.000	
	662-6037	WK ZN PAV MRK NON-REMOV (Y)6"(SLD)	LF	10,216.000		10,216.000	
	662-6048	WK ZN PAV MRK REMOV (REFL) TY I-C	EA	19.000		19.000	
	662-6050	WK ZN PAV MRK REMOV (REFL) TY II-A-A	EA	526.000		526.000	
	662-6111	WK ZN PAV MRK SHT TERM (TAB)TY Y-2	EA	3,000.000		3,000.000	
	662-6113	WK ZN PAV MRK SHT TERM RMV (Y)(4")	LF	7,942.000		7,942.000	
	666-6030	REFL PAV MRK TY I (W)8"(DOT)(100MIL)	LF	113.000		113.000	
	666-6036	REFL PAV MRK TY I (W)8"(SLD)(100MIL)	LF	1,310.000		1,310.000	
	666-6048	REFL PAV MRK TY I (W)24"(SLD)(100MIL)	LF	65.000		65.000	
	666-6306	RE PM W/RET REQ TY I (W)6"(BRK)(100MIL)	LF	3,684.000		3,684.000	
	666-6309	RE PM W/RET REQ TY I (W)6"(SLD)(100MIL)	LF	14,653.000		14,653.000	
	666-6318	RE PM W/RET REQ TY I (Y)6"(BRK)(100MIL)	LF	2,445.000		2,445.000	
	666-6321	RE PM W/RET REQ TY I (Y)6"(SLD)(100MIL)	LF	18,660.000		18,660.000	
	668-6077	PREFAB PAV MRK TY C (W) (ARROW)	EA	10.000		10.000	
	668-6085	PREFAB PAV MRK TY C (W) (WORD)	EA	6.000		6.000	
	672-6007	REFL PAV MRKR TY I-C	EA	283.000		283.000	
	672-6009	REFL PAV MRKR TY II-A-A	EA	491.000		491.000	
	677-6001	ELIM EXT PAV MRK & MRKS (4")	LF	1,270.000		1,270.000	
	3077-6066	SP MIXESSP-DSAC-B PG76-22	TON	5,614.000		5,614.000	
	3084-6001	BONDING COURSE	GAL	4,699.000		4,699.000	
	6185-6002	TMA (STATIONARY)	DAY	185.000		185.000	
	6185-6003	TMA (MOBILE OPERATION)	HR	176.000		176.000	
18		EROSION CONTROL MAINTENANCE: CONTRACTOR FORCE ACCOUNT WORK (PART)	LS	1.000		1.000	
		SAFETY CONTINGENCY: CONTRACTOR FORCE ACCOUNT WORK (PARTICIPATING)	LS	1.000		1.000	

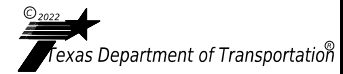
DISTRICT	COUNTY	CCSJ	SHEET
Brownwood	Coleman	0054-04-123	

Element	Point Type	Point Number	Station	Northing	Easting	Radius	Length	Delta /Theta	Rotation/Direction
Alignment Name: US 84									
Style: Alignment\Baseline									
Tangent	PC		261+51.372 R1	10627910.54	2585099.664				
Arc	PC		261+51.372 R1	10627910.54	2585099.664				
Arc	PI		277+28.347 R1	10626881.55	2586294.664	12734.7	3137.975	14°07'06.000"	Right
Arc	CC			10618260.44	2576790.145				
Arc	PT		292+89.348 R1	10625592.14	2587202.571				
Tangent	PT		292+89.348 R1	10625592.14	2587202.571				
Tangent	PC		297+85.806 R1	10625186.22	2587488.396				
Arc	PC		297+85.806 R1	10625186.22	2587488.396				
Arc	PI		311+19.855 R1	10624095.45	2588256.443	16370.924	2662.214	09°19'02.462"	Right
Arc	CC			10615761.04	2574102.84				
Arc	PT		324+48.020 R1	10622894.71	2588837.758				
Tangent	PT		324+48.020 R1	10622894.71	2588837.758				
Tangent	PC		336+70.529 R1	10621794.37	2589370.47				
Arc	PC		336+70.529 R1	10621794.37	2589370.47				
Arc	PI		346+87.286 R1	10620879.23	2589813.525	2860.995	1953.858	39°07'44.368"	Left
Arc	CC			10623041.06	2591945.555				
Arc	PT		356+24.388 R1	10620448.92	2590734.736				
Tangent	PT		356+24.388 R1	10620448.92	2590734.737				
Tangent	PC		363+69.356 R1	10620133.63	2591409.7				
Arc	PC		363+69.356 R1	10620133.63	2591409.7				
Arc	PI		367+88.224 R1	10619956.36	2591789.206	2867.01	831.852	16°37'26.907"	Right
Arc	CC			10617536.04	2590196.335				
Arc	PT		372+01.207 R1	10619677.93	2592102.135				
Tangent	PT		372+01.207 R1	10619677.93	2592102.135				
Tangent	POT		389+63.240 R1	10618506.64	2593418.514				



01/26/2023

**US 84
ALIGNMENT
DATA**



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		16

DATE: 1/11/2023 7:29:09 AM
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DATE: 1/11/2023 7:29:37 AM
 FILE: ...SheetFiles\Earthwork.dgn

Baseline Station	Station Quantities				Mass Ordinate
	Area	Volume	Cut Area	Volume	
277+63.696 R1	0	0	0	0	0
278+00.000 R1	0.04	0.027	15.781	10.609	-10.582
279+00.000 R1	0.128	0.311	34.722	93.523	-103.794
280+00.000 R1	0.326	0.84	31.664	122.936	-225.891
281+00.000 R1	0.457	1.449	43.149	138.541	-362.982
282+00.000 R1	9.352	18.165	0.353	80.558	-425.375
283+00.000 R1	0.725	18.661	0.971	2.452	-409.166
284+00.000 R1	44.065	82.944	21.362	41.358	-367.579
285+00.000 R1	42.838	160.932	28.175	91.736	-298.383
286+00.000 R1	45.19	163.016	27.317	102.763	-238.129
287+00.000 R1	45.693	168.302	27.391	101.31	-171.137
288+00.000 R1	50.093	177.38	23.32	93.91	-87.667
288+89.420 R1	50.083	184.649	23.851	88.151	0
289+00.000 R1	50.082	185.509	23.913	87.47	10.372
290+00.000 R1	60.952	205.618	12.857	68.094	147.897
291+00.000 R1	55.269	215.223	19.296	59.543	303.577
292+00.000 R1	51.668	198.032	21.275	75.132	426.477
293+00.000 R1	56.217	199.788	11.952	61.532	564.734
294+00.000 R1	65.621	225.626	4.818	31.055	759.305
295+00.000 R1	96.536	300.29	0.6	10.033	1049.563
296+00.000 R1	49.119	269.731	1.872	4.579	1314.715
297+00.000 R1	46.45	176.979	35.292	68.823	1422.87
298+00.000 R1	49.838	178.311	22.536	107.089	1494.093
299+00.000 R1	52.6	189.7	27.105	91.928	1591.865
300+00.000 R1	49.346	188.789	28.49	102.954	1677.699
301+00.000 R1	47.59	179.51	28.372	105.301	1751.909
302+00.000 R1	56.204	192.21	16.499	83.095	1861.024
303+00.000 R1	62.568	219.948	11.351	51.573	2029.399
304+00.000 R1	60.079	227.125	11.986	43.216	2213.307
305+00.000 R1	67.516	236.287	9.278	39.378	2410.217
306+00.000 R1	67.306	249.671	7.259	30.625	2629.263
307+00.000 R1	64.914	244.852	7.752	27.799	2846.315
308+00.000 R1	74.878	258.873	5.823	25.139	3080.049
309+00.000 R1	72.657	273.213	8.945	27.349	3325.914
310+00.000 R1	69.205	262.707	8.784	32.831	3555.79
311+00.000 R1	60.032	239.328	12.343	39.123	3755.994
312+00.000 R1	51.871	207.228	14.981	50.599	3912.623
313+00.000 R1	57.32	202.206	12.513	50.914	4063.914
314+00.000 R1	83.223	260.266	4.769	32.002	4292.178
315+00.000 R1	56.122	258.047	13.55	33.924	4516.302
316+00.000 R1	53.465	202.94	17.58	57.648	4661.593
317+00.000 R1	51.318	194.043	17.322	64.632	4791.004
318+00.000 R1	47.941	183.812	9.954	50.51	4924.306

Baseline Station	Station Quantities				Mass Ordinate
	Fill Area	Volume	Cut Area	Volume	
319+00.000 R1	41.689	165.982	20.301	56.026	5034.262
320+00.000 R1	40.226	151.695	23.985	82.01	5103.947
321+00.000 R1	40.724	149.908	23.129	87.248	5166.606
322+00.000 R1	46.756	161.999	17.035	74.378	5254.227
323+00.000 R1	56.476	191.169	13.106	55.818	5389.578
324+00.000 R1	66.861	228.401	6.053	35.481	5582.499
325+00.000 R1	43.49	204.354	11.737	32.946	5753.907
326+00.000 R1	43.951	161.927	10.333	40.872	5874.962
327+00.000 R1	42.327	159.774	20.633	57.345	5977.391
328+00.000 R1	40.951	154.22	21.013	77.122	6054.488
329+00.000 R1	39.529	149.037	21.982	79.62	6123.905
330+00.000 R1	42.81	152.479	17.93	73.91	6202.473
331+00.000 R1	49.878	171.644	13.47	58.148	6315.969
332+00.000 R1	61.749	206.716	7.758	39.31	6483.375
333+00.000 R1	58.596	222.86	5.34	24.254	6681.981
334+00.000 R1	51.073	203.09	16.378	40.218	6844.853
335+00.000 R1	46.754	181.16	28.388	82.901	6943.112
336+00.000 R1	58.037	194.058	20.417	90.381	7046.789
337+00.000 R1	62.859	223.883	17.257	69.767	7200.904
338+00.000 R1	62.203	231.597	19.843	68.704	7363.797
339+00.000 R1	65.396	236.295	16.16	66.672	7533.421
340+00.000 R1	65.858	243.063	14.99	57.685	7718.798
341+00.000 R1	66.987	246.008	6.555	39.898	7924.908
342+00.000 R1	63.976	242.524	10.219	31.063	8136.369
343+00.000 R1	78.088	263.082	8.567	34.789	8364.662
344+00.000 R1	92.852	316.555	5.876	26.746	8654.471
345+00.000 R1	72.423	306.064	8.207	26.079	8934.457
346+00.000 R1	64.663	253.863	4.831	24.144	9164.176
347+00.000 R1	98.876	302.849	1.937	12.533	9454.492
348+00.000 R1	40.641	258.364	16.489	34.122	9678.733
349+00.000 R1	39.432	148.283	102.19	219.775	9607.24
350+00.000 R1	159.315	368.05	22.74	231.351	9743.939
351+00.000 R1	46.712	381.532	144.874	310.397	9815.075
352+00.000 R1	47.356	174.2	169.573	582.309	9406.966
353+00.000 R1	45.159	171.324	183.922	654.619	8923.671
353+39.868 R1	52.946	72.43	0.075	135.843	8860.258
Grand Total:		15068.328		6208.07	



JH Scantling, P.E.

01/26/2023

**US 84
 EARTHWORK
 SUMMARY**



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		17

TxDOT - Coleman County US 84 - from SH 153 to CR 196	Texas State Plane	Texas Central Zone 4203	Project Vertical Datum	CSF- 1.0001	US Survey Feet
Coleman County	NAD83(2011)	NAVD88	Geiod 12B	TxDOT VRS	
0054-04-123					

Monument/Target Number	Surface Northing	Surface Easting	Elevation	Description	Grid Northing	Grid Easting	*Latitude (N)	*Longitude (W)
CP1	10628832.2650	2584123.4510	1673.3160	ALC	10627769.4881	2583865.0645	314920.73043	99 2430.31008
CP2	10628112.4180	2584949.1850	1678.8190	ALC	10627049.7130	2584690.7159	314913.53979	99 2420.81052
CP3	10627332.4010	2585636.8220	1675.8920	ALC	10626269.7740	2585378.2842	314905.76501	99 2412.91745
CP4	10626199.4310	2586833.4070	1674.3960	ALC	10625136.9173	2586574.7495	314854.45552	99 2359.16147
CP5	10625156.6630	2587429.6570	1686.0160	ALC	10624094.2536	2587170.9399	314844.08814	99 2352.35389
CP6	10624275.5410	2588006.5790	1699.4510	ALC	10623213.2197	2587747.8042	314835.32168	99 2345.75490
CP7	10623323.7440	2588552.0330	1709.0550	ALC	10622261.5179	2588293.2037	314825.85845	99 2339.52777
CP8	10622455.6130	2589127.4720	1719.3780	ALC	10621393.4737	2588868.5851	314817.22047	99 2332.94538
CP9	10621825.1550	2589432.3170	1728.3400	ALC	10620763.0787	2589173.3997	314810.95677	99 2329.47514
CP10	10621294.9920	2589767.7500	1730.3930	ALC	10620232.9687	2589508.7991	314805.68286	99 2325.64080

ALC-5/8" IRON ROD W/ TxDOT 3-1/4" ALUMINUM CONTROL CAP
Surveyed September 2021
*Lat/*Long conversion from NOAA NGSCoordinate Conversion Tool (NCAT) (input/output) TX East HPGN/HARN
TxDOT Brownwood District Chet M. Glasscock, RPLS Travis Jordan George Trott

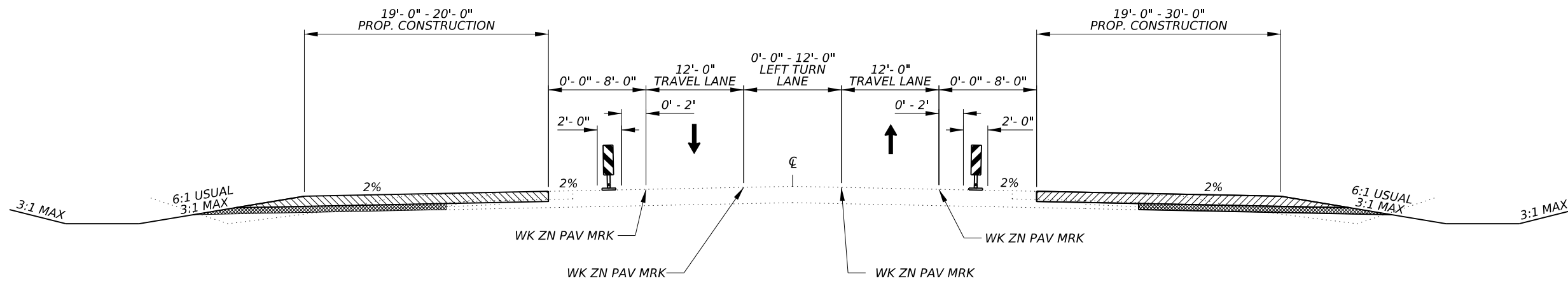


01/26/2023

US 84
CONTROL
SHEET



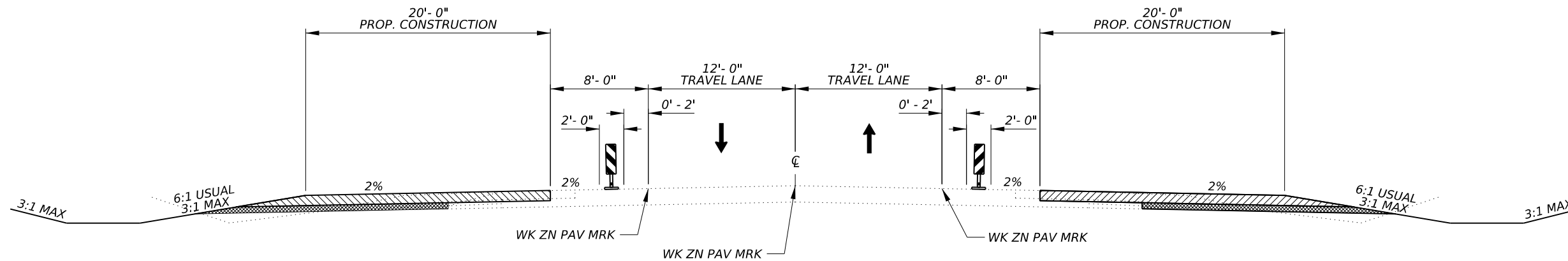
CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	18	



US 84 DETOUR TYPICAL #1

STA. 284+00 - STA. 306+76

*Tape shall be placed after milling,
 tabs shall be placed after surface treat,
 and paint shall be placed after ACP



US 84 DETOUR TYPICAL #2

STA. 306+76 - STA. 353+40

*Tape shall be placed after milling,
 tabs shall be placed after surface treat,
 and paint shall be placed after ACP

*TAPE WILL BE USED FOR SHORT TERM PAVEMENT MARKINGS

ITEM CODE	DESCRIPTION	QUANT.	UNIT
662 6008	WK ZN PAV MRK NON-REMOV (W) 6" (SLD)	19950.0	LF
662 6037	WK ZN PAV MRK NON-REMOV (Y) 6" (SLD)	10216.0	LF
662 6048	WK ZN PAV MRK REMOV (REFL) TY I-C	19.0	EA
662 6050	WK ZN PAV MRK REMOV (REFL) TY II-A-A	526.0	EA
662 6111	WK ZN PAV MRK SHT TERM (TAB) TY Y-2	3000.0	EA
662 6113	WK ZN PAV MRK SHT TERM RMV (Y)(4")	7942.0	LF
677 6001	ELIM EXT PAV MRK & MRKS (4")	1270.0	LF



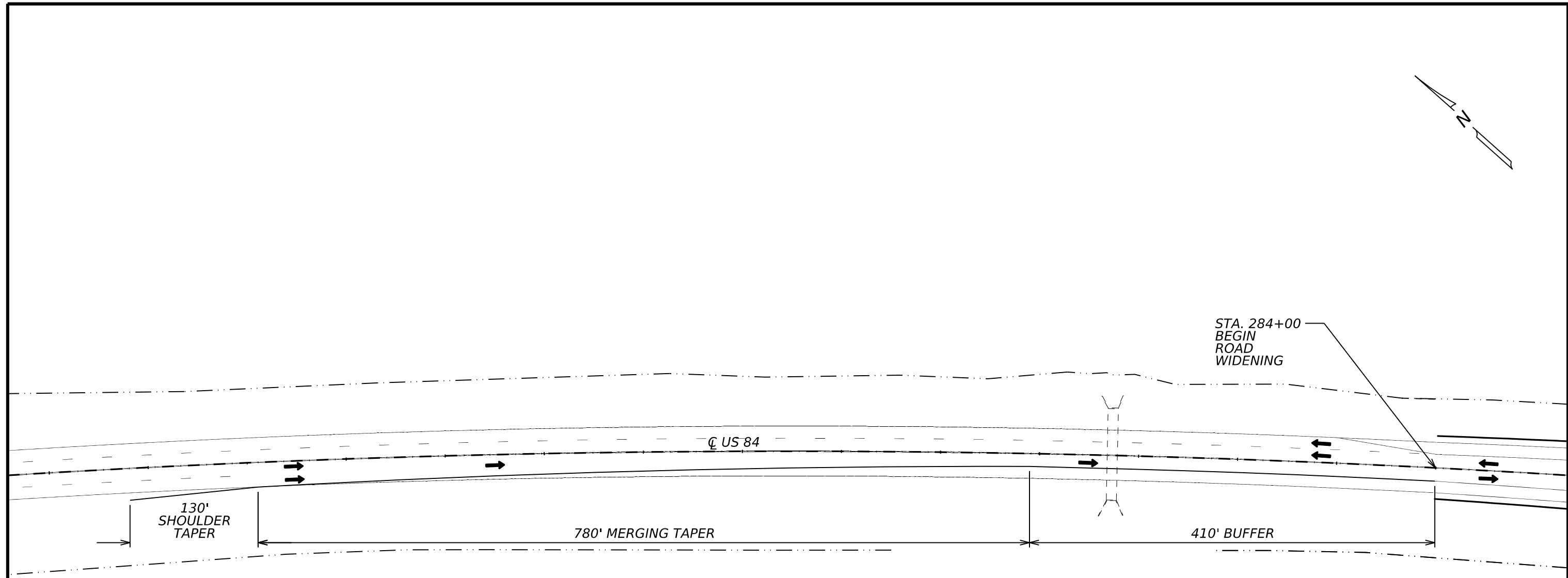
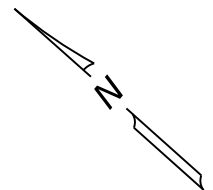
JH Scantling, P.E.

01/26/2023

**US 84
 DETOUR
 TYPICALS**



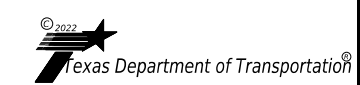
CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		19



JH Scantling, P.E.

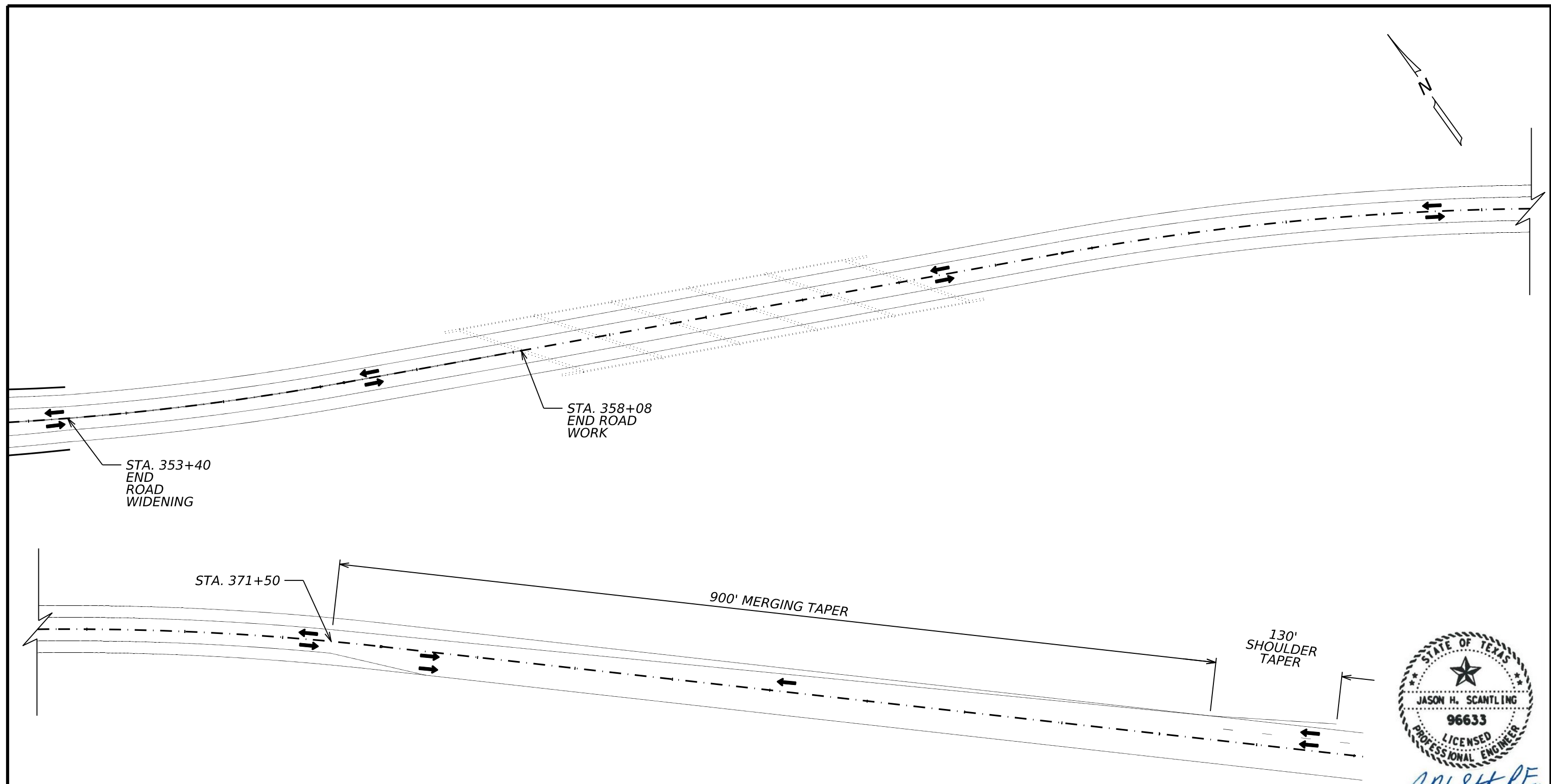
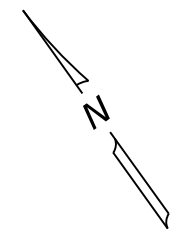
01/26/2023

**US 84
DETOUR
LAYOUT**



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	20	

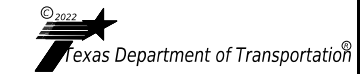
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JH Scantling, P.E.

01/26/2023

**US 84
DETOUR
LAYOUT**



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		21

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BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES:

1. The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction pavement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
2. The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.
3. The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.
4. The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer.
5. Geometric design of lane shifts and detours should, when possible, meet the applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.
6. When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.
7. The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.
8. All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.
9. The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.
10. Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES, CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.
11. Traffic control devices should be in place only while work is actually in progress or a definite need exists.
12. The Engineer has the final decision on the location of all traffic control devices.
13. Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

WORKER SAFETY NOTES:


1. Workers on foot who are exposed to traffic or to construction equipment within the right-of-way shall wear high-visibility safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel," or equivalent revisions, and labeled as ANSI 107-2004 standard performance for Class 2 or 3 risk exposure. Class 3 garments should be considered for high traffic volume work areas or night time work.
2. Except in emergency situations, flagger stations shall be illuminated when flagging is used at night.

COMPLIANT WORKZONE TRAFFIC CONTROL DEVICES

1. Only pre-qualified products shall be used. The "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products and their sources.
2. Work zone traffic control devices shall be compliant with the Manual for Assessing safety Hardware (MASH).

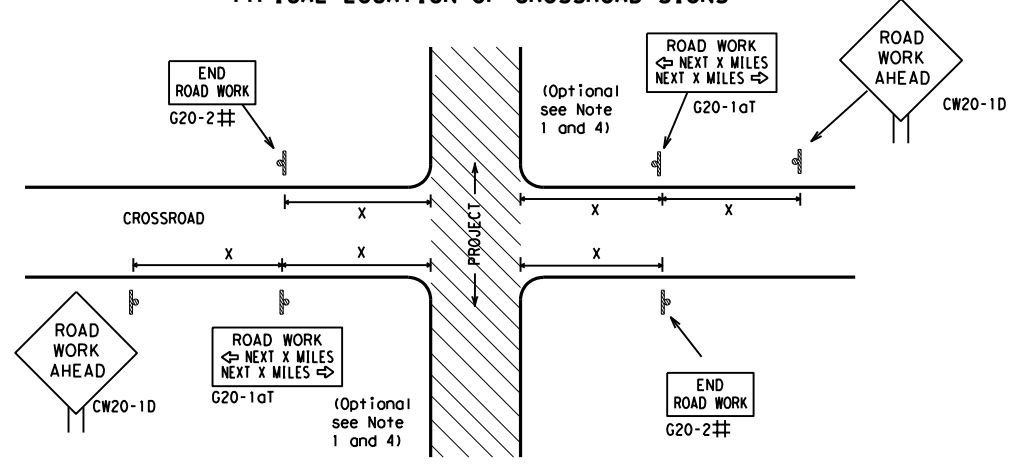
THE DOCUMENTS BELOW CAN BE FOUND ON-LINE AT http://www.txdot.gov
COMPLIANT WORK ZONE TRAFFIC CONTROL DEVICES LIST (CWZTCD)
DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS)
MATERIAL PRODUCER LIST (MPL)
ROADWAY DESIGN MANUAL - SEE "MANUALS (ONLINE MANUALS) "
STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS (SHSD)
TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD)
TRAFFIC ENGINEERING STANDARD SHEETS

SHEET 1 OF 12

		<i>Texas Department of Transportation</i> Traffic Safety Division Standard
<h2 style="margin: 0;">BARRICADE AND CONSTRUCTION</h2> <h3 style="margin: 0;">GENERAL NOTES AND REQUIREMENTS</h3> <h4 style="margin: 0;">BC (1) - 21</h4>		
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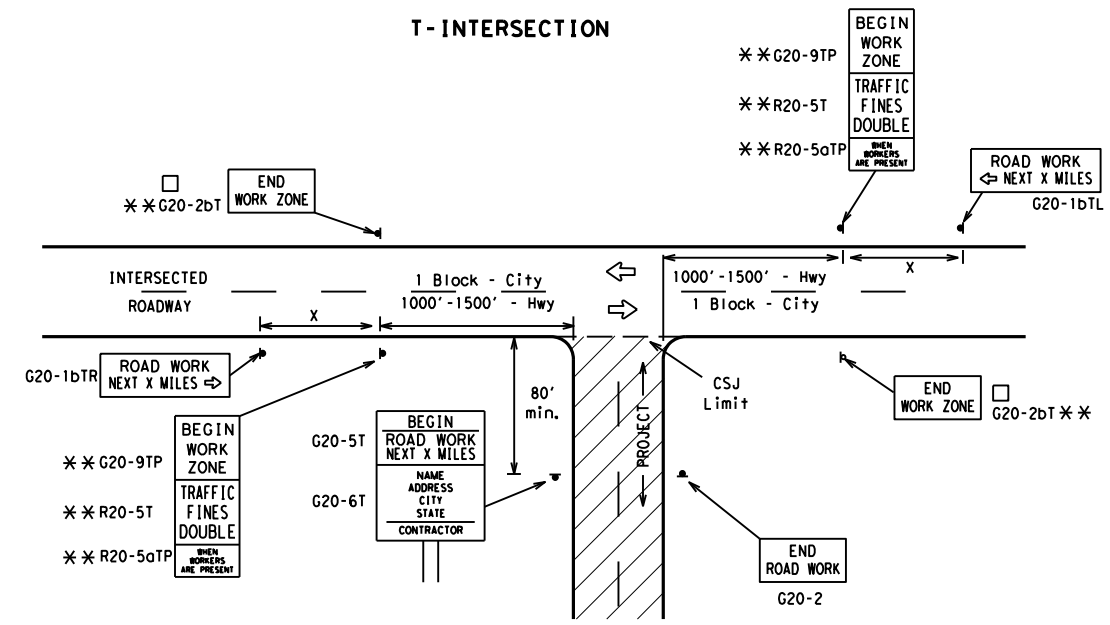
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TYPICAL LOCATION OF CROSSROAD SIGNS



- ## May be mounted on back of "ROAD WORK AHEAD" (CW20-1D) sign with approval of Engineer. (See note 2 below)
- The typical minimum signing on a crossroad approach should be a "ROAD WORK AHEAD" (CW20-1D) sign and a (G20-2) "END ROAD WORK" sign, unless noted otherwise in plans.
 - The Engineer may use the reduced size 36" x 36" ROAD WORK AHEAD (CW20-1D) sign mounted back to back with the reduced size 36" x 18" "ROAD WORK NEXT X MILES" (G20-2) sign on low volume crossroads (see Note 4 under "Typical Construction Warning Sign Size and Spacing"). See the "Standard Highway Sign Designs for Texas" manual for sign details. The Engineer may omit the advance warning signs on low volume crossroads. The Engineer will determine whether a road is low volume as per TMUTCD Part 5. This information shall be shown in the plans.
 - Based on existing field conditions, the Engineer/Inspector may require additional signs such as FLAGGER AHEAD, LOOSE GRAVEL, or other appropriate signs. When additional signs are required, these signs will be considered part of the minimum requirements. The Engineer/Inspector will determine the proper location and spacing of any sign not shown on the BC sheets, Traffic Control Plan sheets or the Work Zone Standard Sheets.
 - The "ROAD WORK NEXT X MILES" (G20-1aT) sign shall be required at high volume crossroads to advise motorists of the length of construction in either direction from the intersection. The Engineer will determine whether a roadway is considered high volume.
 - Additional traffic control devices may be shown elsewhere in the plans for higher volume crossroads.
 - When work occurs in the intersection area, appropriate traffic control devices, as shown elsewhere in the plans or as determined by the Engineer/Inspector, shall be in place.

T-INTERSECTION



CSJ LIMITS AT T-INTERSECTION

- The Engineer will determine the types and location of any additional traffic control devices, such as a flagger and accompanying signs, or other signs, that should be used when work is being performed at or near an intersection.
- If construction closes the road at a T-intersection, the Contractor shall place the "CONTRACTOR NAME" (G20-6T) sign behind the Type 3 Barricades for the road closure (see BC(10) also). The "ROAD WORK NEXT X MILES" left arrow (G20-1bTL) and "ROAD WORK NEXT X MILES" right arrow (G20-1bTR) signs shall be replaced by the detour signing called for in the plans.

TYPICAL CONSTRUCTION WARNING SIGN SIZE AND SPACING^{1,5,6}

Sign Number or Series	SIZE		SPACING	
	Conventional Road	Expressway/Freeway	Posted Speed MPH	Sign Δ Spacing "x" Feet (Apprx.)
CW20 ⁴	48" x 48"	48" x 48"	30	120
CW21			35	160
CW22			40	240
CW23			45	320
CW25			50	400
CW1, CW2, CW7, CW8, CW9, CW11, CW14	36" x 36"	48" x 48"	55	500 ²
CW3, CW4, CW5, CW6, CW8-3, CW10, CW12	48" x 48"	48" x 48"	60	600 ²
			65	700 ²
			70	800 ²
			75	900 ²
			80	1000 ²
			*	* ³

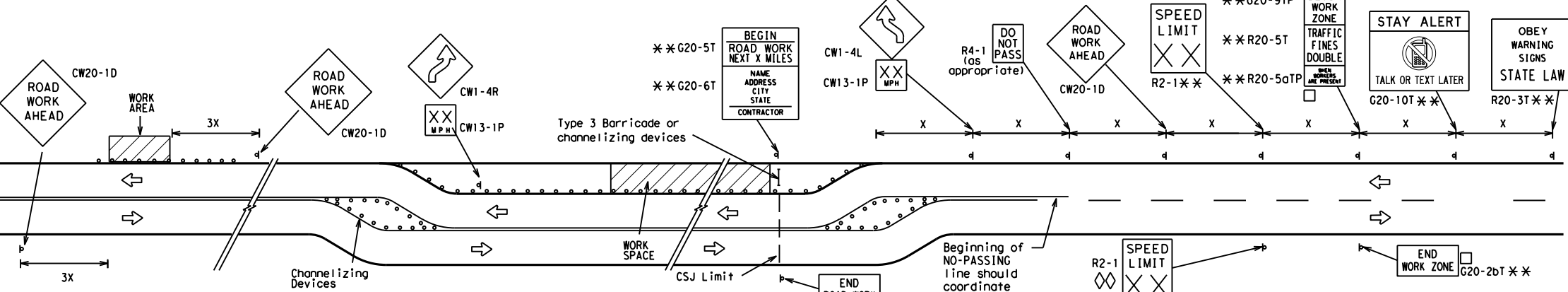
* For typical sign spacings on divided highways, expressways and freeways, see Part 6 of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) typical application diagrams or TCP Standard Sheets.

Δ Minimum distance from work area to first Advance Warning sign nearest the work area and/or distance between each additional sign.

GENERAL NOTES

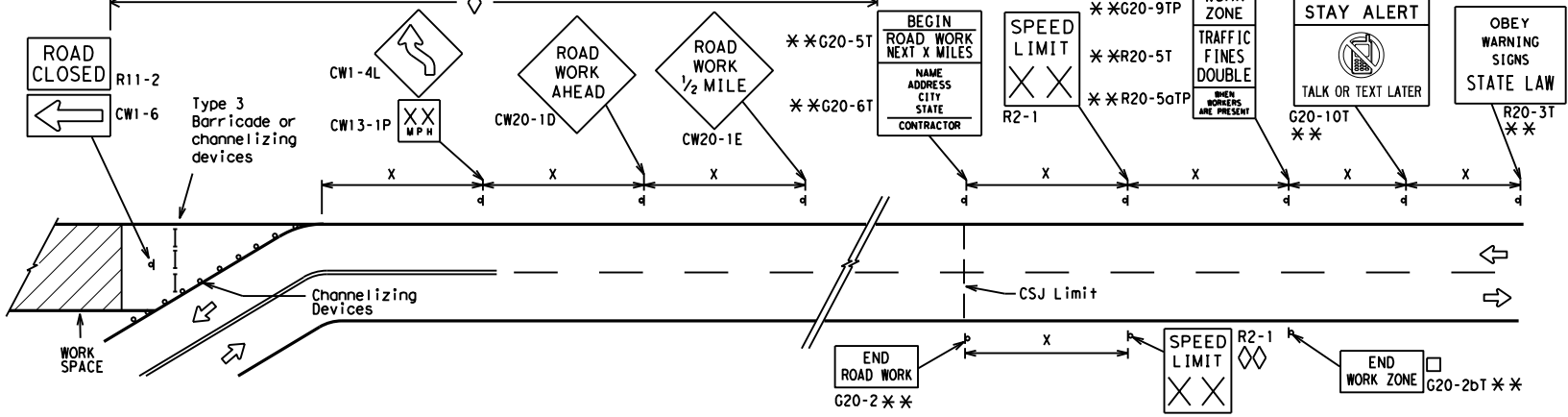
- Special or larger size signs may be used as necessary.
- Distance between signs should be increased as required to have 1500 feet advance warning.
- Distance between signs should be increased as required to have 1/2 mile or more advance warning.
- 36" x 36" "ROAD WORK AHEAD" (CW20-1D) signs may be used on low volume crossroads at the discretion of the Engineer as per TMUTCD Part 5. See Note 2 under "Typical Location of Crossroad Signs".
- Only diamond shaped warning sign sizes are indicated.
- See sign size listing in "TMUTCD", Sign Appendix or the "Standard Highway Sign Designs for Texas" manual for complete list of available sign design sizes.

WORK AREAS IN MULTIPLE LOCATIONS WITHIN CSJ LIMITS

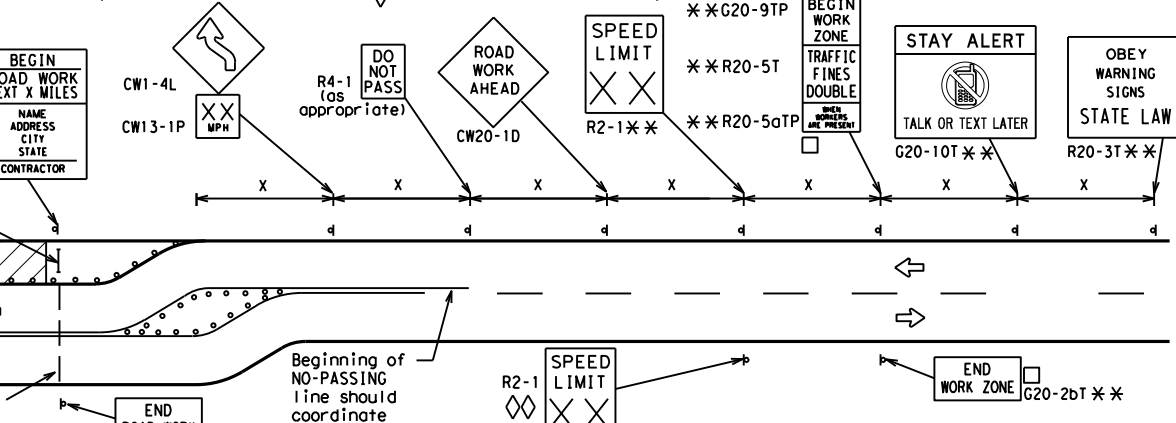


When extended distances occur between minimal work spaces, the Engineer/Inspector should ensure additional "ROAD WORK AHEAD" (CW20-1D) signs are placed in advance of these work areas to remind drivers they are still within the project limits. See the applicable TCP sheets for exact location and spacing of signs and channelizing devices.

SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING DOWNSTREAM OF THE CSJ LIMITS



SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING AT THE CSJ LIMITS



NOTES

- The Contractor shall determine the appropriate distance to be placed on the G20-1 series signs and "BEGIN ROAD WORK NEXT X MILES" (G20-5T) sign for each specific project. This distance shall replace the "x" and shall be rounded to the nearest whole mile with the approval of the Engineer. No decimals shall be used.
- The "BEGIN WORK ZONE" (G20-9TP) and "END WORK ZONE" (G20-2bT) shall be used as shown on the sample layout when advance signs are required outside the CSJ Limits. They inform the motorist of entering or leaving a part of the work zone lying outside the CSJ Limits where traffic fines may double if workers are present.
 - CSJ limit signing is required for highway construction and maintenance work, with the exception of mobile operations.
 - Area for placement of "ROAD WORK AHEAD" (CW20-1D) sign and other signs or devices as called for on the Traffic Control Plan.
 - Contractor will install a regulatory speed limit sign at the end of the work zone.

LEGEND

—	Type 3 Barricade
○ ○ ○	Channelizing Devices
■	Sign
X	See Typical Construction Warning Sign Size and Spacing chart or the TMUTCD for sign spacing requirements.

SHEET 2 OF 12



BARRICADE AND CONSTRUCTION PROJECT LIMIT

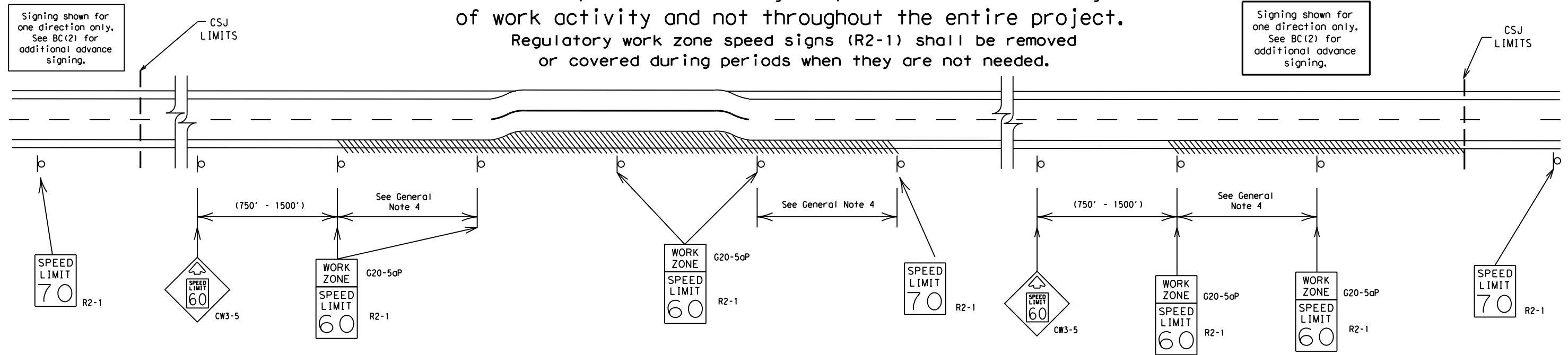
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© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	BWD	COLEMAN CO.	23	

TYPICAL APPLICATION OF WORK ZONE SPEED LIMIT SIGNS

Work zone speed limits shall be regulatory, established in accordance with the "Procedures for Establishing Speed Zones," and approved by the Texas Transportation Commission, or by City Ordinance when within Incorporated City Limits.

Reduced speeds should only be posted in the vicinity of work activity and not throughout the entire project. Regulatory work zone speed signs (R2-1) shall be removed or covered during periods when they are not needed.



GUIDANCE FOR USE:

LONG/INTERMEDIATE TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit should be included on the design of the traffic control plans when restricted geometrics with a lower design speed are present in the work zone and modification of the geometrics to a higher design speed is not feasible.

Long/Intermediate Term Work Zone Speed Limit signs, when approved as described above, should be posted and visible to the motorist when work activity is present. Work activity may also be defined as a change in the roadway that requires a reduced speed for motorists to safely negotiate the work area, including:

- rough road or damaged pavement surface
- substantial alteration of roadway geometrics (diversions)
- construction detours
- grade
- width
- other conditions readily apparent to the driver

As long as any of these conditions exist, the work zone speed limit signs should remain in place.

SHORT TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit may be included on the design of the traffic control plans when workers or equipment are not behind concrete barrier, when work activity is within 10 feet of the traveled way or actually in the traveled way.

Short Term Work Zone Speed Limit signs should be posted and visible to the motorists only when work activity is present. When work activity is not present, signs shall be removed or covered. (See Removing or Covering on BC(4)).

GENERAL NOTES

- Regulatory work zone speed limits should be used only for sections of construction projects where speed control is of major importance.
- Regulatory work zone speed limit signs shall be placed on supports at a 7 foot minimum mounting height.
- Speed zone signs are illustrated for one direction of travel and are normally posted for each direction of travel.
- Frequency of work zone speed limit signs should be:

40 mph and greater	0.2 to 2 miles
35 mph and less	0.2 to 1 mile
- Regulatory speed limit signs shall have black legend and border on a white reflective background (See "Reflective Sheeting" on BC(4)).
- Fabrication, erection and maintenance of the "ADVANCE SPEED LIMIT" (CW3-5) sign, "WORK ZONE" (G20-5aP) plaque and the "SPEED LIMIT" (R2-1) signs shall not be paid for directly, but shall be considered subsidiary to Item 502.
- Turning signs from view, laying signs over or down will not be allowed, unless as otherwise noted under "REMOVING OR COVERING" on BC(4).
- Techniques that may help reduce traffic speeds include but are not limited to:
 - Law enforcement.
 - Flagger stationed next to sign.
 - Portable changeable message sign (PCMS).
 - Low-power (drone) radar transmitter.
 - Speed monitor trailers or signs.
- Speeds shown on details above are for illustration only. Work Zone Speed Limits should only be posted as approved for each project.
- For more specific guidance concerning the type of work, work zone conditions and factors impacting allowable regulatory construction speed zone reduction see TxDOT form #1204 in the TxDOT e-form system.

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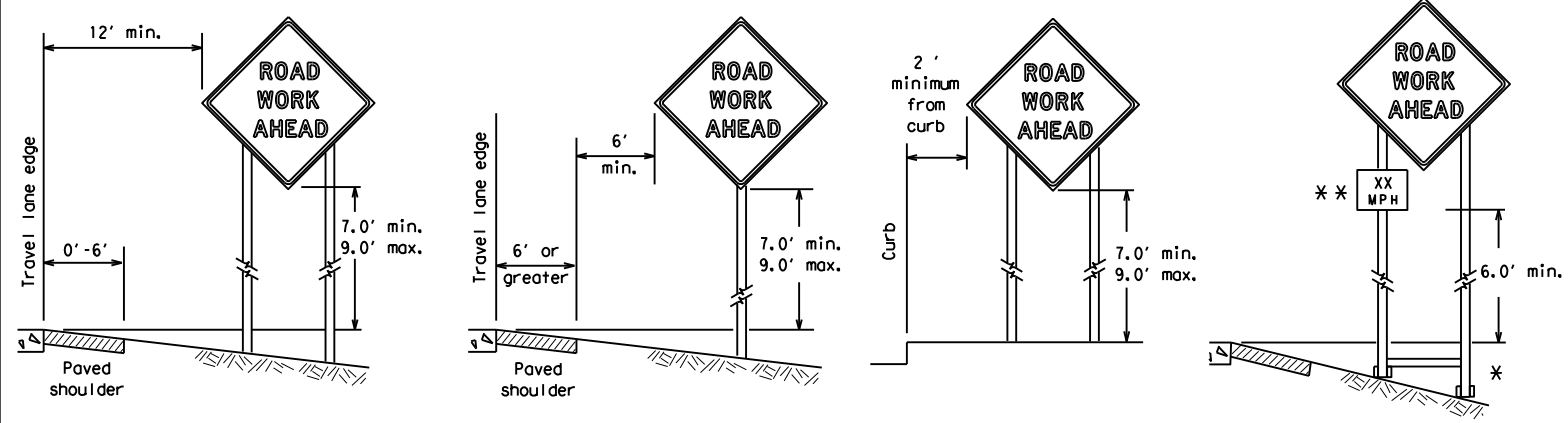
SHEET 3 OF 12

		Traffic Safety Division Standard	
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© TxDOT	November 2002	CONT SECT	JOB HIGHWAY
REVISIONS	0054 04	123	US 84
9-07 8-14			
7-13 5-21			
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	24	

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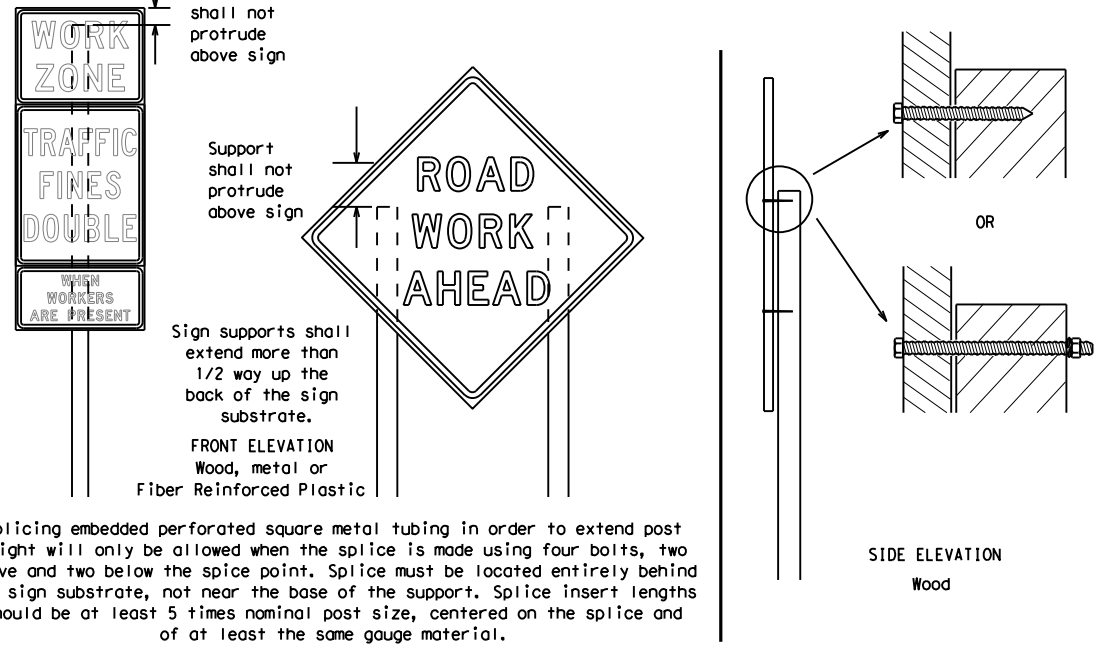
TYPICAL MINIMUM CLEARANCES FOR LONG TERM AND INTERMEDIATE TERM SIGNS



* When placing skid supports on unlevel ground, the leg post lengths must be adjusted so the sign appears straight and plumb. Objects shall NOT be placed under skids as a means of leveling.

** When plaques are placed on dual-leg supports, they should be attached to the upright nearest the travel lane. Supplemental plaques (advisory or distance) should not cover the surface of the parent sign.

ATTACHMENT FOR SIGN SUPPORTS



Splicing embedded perforated square metal tubing in order to extend post height will only be allowed when the splice is made using four bolts, two above and two below the splice point. Splice must be located entirely behind the sign substrate, not near the base of the support. Splice insert lengths should be at least 5 times nominal post size, centered on the splice and of at least the same gauge material.

GENERAL NOTES FOR WORK ZONE SIGNS

- Contractor shall install and maintain signs in a straight and plumb condition and/or as directed by the Engineer.
- Wooden sign posts shall be painted white.
- Barricades shall NOT be used as sign supports.
- All signs shall be installed in accordance with the plans or as directed by the Engineer. Signs shall be used to regulate, warn, and guide the traveling public safely through the work zone.
- The Contractor may furnish either the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD). The Engineer/Inspector may require the Contractor to furnish other work zone signs that are shown in the TMUTCD but may have been omitted from the plans. Any variation in the plans shall be documented by written agreement between the Engineer and the Contractor's Responsible Person. All changes must be documented in writing before being implemented. This can include documenting the changes in the Inspector's TxDOT diary and having both the Inspector and Contractor initial and date the agreed upon changes.
- The Contractor shall furnish sign supports listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCD) for small roadside signs. Supports for temporary large roadside signs shall meet the requirements detailed on the Temporary Large Roadside Signs (TLRS) standard sheets. The Contractor shall install the sign support in accordance with the manufacturer's recommendations. If there is a question regarding installation procedures, the Contractor shall furnish the Engineer a copy of the manufacturer's installation recommendations so the Engineer can verify the correct procedures are being followed.
- The Contractor is responsible for installing signs on approved supports and replacing signs with damaged or cracked substrates and/or damaged or marred reflective sheeting as directed by the Engineer/Inspector.
- Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1 inch.
- The Contractor shall replace damaged wood posts. New or damaged wood sign posts shall not be spliced.

DURATION OF WORK (as defined by the "Texas Manual on Uniform Traffic Control Devices" Part 6)

- The types of sign supports, sign mounting height, the size of signs, and the type of sign substrates can vary based on the type of work being performed. The Engineer is responsible for selecting the appropriate size sign for the type of work being performed. The Contractor is responsible for ensuring the sign support, sign mounting height and substrate meets manufacturer's recommendations in regard to crashworthiness and duration of work requirements.
 - Long-term stationary - work that occupies a location more than 3 days.
 - Intermediate-term stationary - work that occupies a location more than one daylight period up to 3 days, or nighttime work lasting more than one hour.
 - Short-term stationary - daytime work that occupies a location for more than 1 hour in a single daylight period.
 - Short, duration - work that occupies a location up to 1 hour.
 - Mobile - work that moves continuously or intermittently (stopping for up to approximately 15 minutes.)

SIGN MOUNTING HEIGHT

- The bottom of Long-term/Intermediate-term signs shall be at least 7 feet, but not more than 9 feet, above the paved surface, except as shown for supplemental plaques mounted below other signs.
- The bottom of Short-term/Short Duration signs shall be a minimum of 1 foot above the pavement surface but no more than 2 feet above the ground.
- Long-term/Intermediate-term Signs may be used in lieu of Short-term/Short Duration signing.
- Short-term/Short Duration signs shall be used only during daylight and shall be removed at the end of the workday or raised to appropriate Long-term/Intermediate sign height.
- Regulatory signs shall be mounted at least 7 feet, but not more than 9 feet, above the paved surface regardless of work duration.

SIZE OF SIGNS

- The Contractor shall furnish the sign sizes shown on BC (2) unless otherwise shown in the plans or as directed by the Engineer.

SIGN SUBSTRATES

- The Contractor shall ensure the sign substrate is installed in accordance with the manufacturer's recommendations for the type of sign support that is being used. The CWZTCD lists each substrate that can be used on the different types and models of sign supports.
- "Mesh" type materials are NOT an approved sign substrate, regardless of the tightness of the weave.
- All wooden individual sign panels fabricated from 2 or more pieces shall have one or more plywood cleat, 1/2" thick by 6" wide, fastened to the back of the sign and extending fully across the sign. The cleat shall be attached to the back of the sign using wood screws that do not penetrate the face of the sign panel. The screws shall be placed on both sides of the splice and spaced at 6" centers. The Engineer may approve other methods of splicing the sign face.

REFLECTIVE SHEETING

- All signs shall be retroreflective and constructed of sheeting meeting the color and retro-reflectivity requirements of DMS-8300 for rigid signs or DMS-8310 for roll-up signs. The web address for DMS specifications is shown on BC(1).
- White sheeting, meeting the requirements of DMS-8300 Type A, shall be used for signs with a white background.
- Orange sheeting, meeting the requirements of DMS-8300 Type B_{FL} or Type C_{FL}, shall be used for rigid signs with orange backgrounds.

SIGN LETTERS

- All sign letters and numbers shall be clear, and open rounded type uppercase alphabet letters as approved by the Federal Highway Administration (FHWA) and as published in the "Standard Highway Sign Design for Texas" manual. Signs, letters and numbers shall be of first class workmanship in accordance with Department Standards and Specifications.

REMOVING OR COVERING

- When sign messages may be confusing or do not apply, the signs shall be removed or completely covered.
- Long-term stationary or intermediate stationary signs installed on square metal tubing may be turned away from traffic 90 degrees when the sign message is not applicable. This technique may not be used for signs installed in the median of divided highways or near any intersections where the sign may be seen from approaching traffic.
- Signs installed on wooden skids shall not be turned at 90 degree angles to the roadway. These signs should be removed or completely covered when not required.
- When signs are covered, the material used shall be opaque, such as heavy mil black plastic, or other materials which will cover the entire sign face and maintain their opaque properties under automobile headlights at night, without damaging the sign sheeting.
- Burlap shall NOT be used to cover signs.
- Duct tape or other adhesive material shall NOT be affixed to a sign face.
- Signs and anchor stubs shall be removed and holes backfilled upon completion of work.

SIGN SUPPORT WEIGHTS

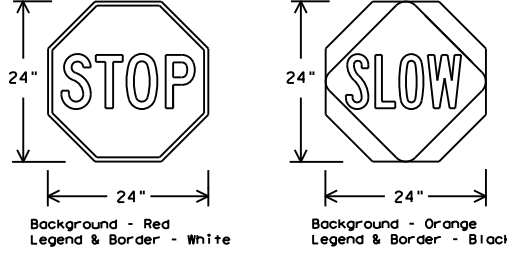
- Where sign supports require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand should be used.
- The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
- Rock, concrete, iron, steel or other solid objects shall not be permitted for use as sign support weights.
- Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
- Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall NOT be used.
- Rubber ballasts designed for channelizing devices should not be used for ballast on portable sign supports. Sign supports designed and manufactured with rubber bases may be used when shown on the CWZTCD list.
- Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign support.
- Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

FLAGS ON SIGNS

- Flags may be used to draw attention to warning signs. When used, the flag shall be 16 inches square or larger and shall be orange or fluorescent red-orange in color. Flags shall not be allowed to cover any portion of the sign face.

STOP/SLOW PADDLES

- STOP/SLOW paddles are the primary method to control traffic by flaggers. The STOP/SLOW paddle size should be 24" x 24".
- STOP/SLOW paddles shall be retroreflective when used at night.
- STOP/SLOW paddles may be attached to a staff with a minimum length of 6' to the bottom of the sign.
- Any lights incorporated into the STOP or SLOW paddle faces shall only be as specifically described in Section 6E.03 Hand Signaling Devices in the TMUTCD.



SHEETING REQUIREMENTS (WHEN USED AT NIGHT)		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	ORANGE	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND & BORDER	WHITE	TYPE B OR C SHEETING
LEGEND & BORDER	BLACK	ACRYLIC NON-REFLECTIVE FILM

CONTRACTOR REQUIREMENTS FOR MAINTAINING PERMANENT SIGNS WITHIN THE PROJECT LIMITS

- Permanent signs are used to give notice of traffic laws or regulations, call attention to conditions that are potentially hazardous to traffic operations, show route designations, destinations, directions, distances, services, points of interest, and other geographical, recreational, specific service (LOGO), or cultural information. Drivers proceeding through a work zone need the same, if not better route guidance as normally installed on a roadway without construction.
- When permanent regulatory or warning signs conflict with work zone conditions, remove or cover the permanent signs until the permanent sign message matches the roadway condition. For details for covering large guide signs see the TS-CD standard.
- When existing permanent signs are moved and relocated due to construction purposes, they shall be visible to motorists at all times.
- If existing signs are to be relocated on their original supports, they shall be installed on crashworthy bases as shown on the SMD Standard sheets. The signs shall meet the required mounting heights shown on the BC Sheets or the SMD Standards. This work should be paid for under the appropriate pay item for relocating existing signs.
- If permanent signs are to be removed and relocated using temporary supports, the Contractor shall use crashworthy supports as shown on the BC standard sheets, TLRS standard sheets or the CWZTCD list. The signs shall meet the required mounting heights shown on the BC, or the SMD standard sheets during construction. This work should be paid for under the appropriate pay item for relocating existing signs.
- Any sign or traffic control device that is struck or damaged by the Contractor or his/her construction equipment shall be replaced as soon as possible by the Contractor to ensure proper guidance for the motorists. This will be subsidiary to Item 502.

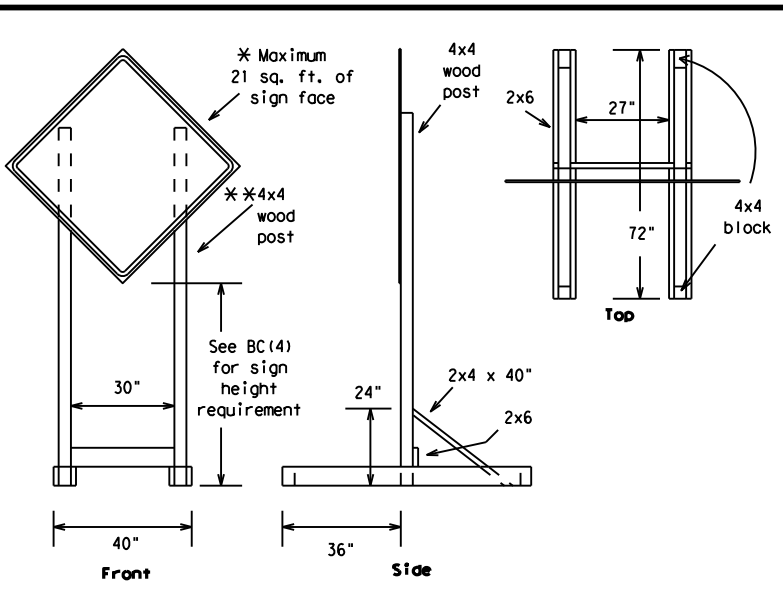
Texas Department of Transportation
 Traffic Safety Division Standard

BARRICADE AND CONSTRUCTION TEMPORARY SIGN NOTES

BC (4) - 21

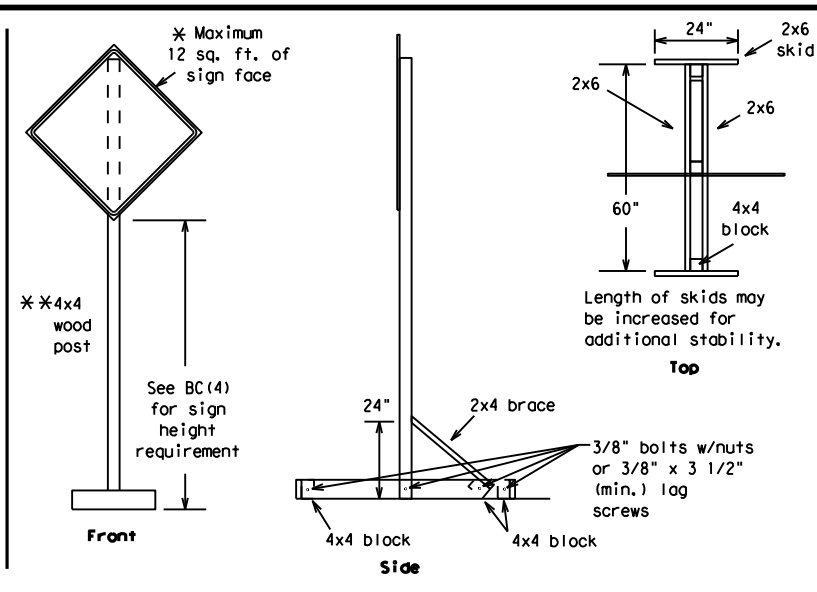
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REVISIONS		0054	04	123	US 84				
9-07	8-14	DIST	COUNTY	SHEET NO.					
7-13	5-21	BWD	COLEMAN CO.	25					

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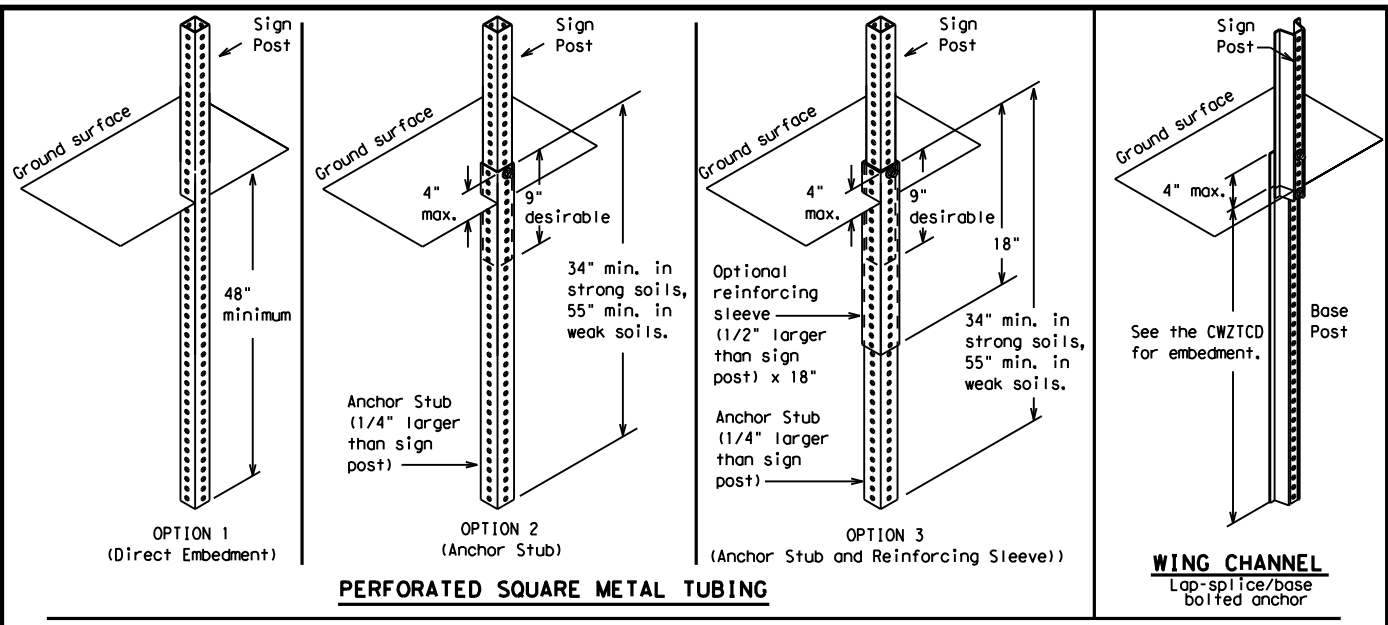
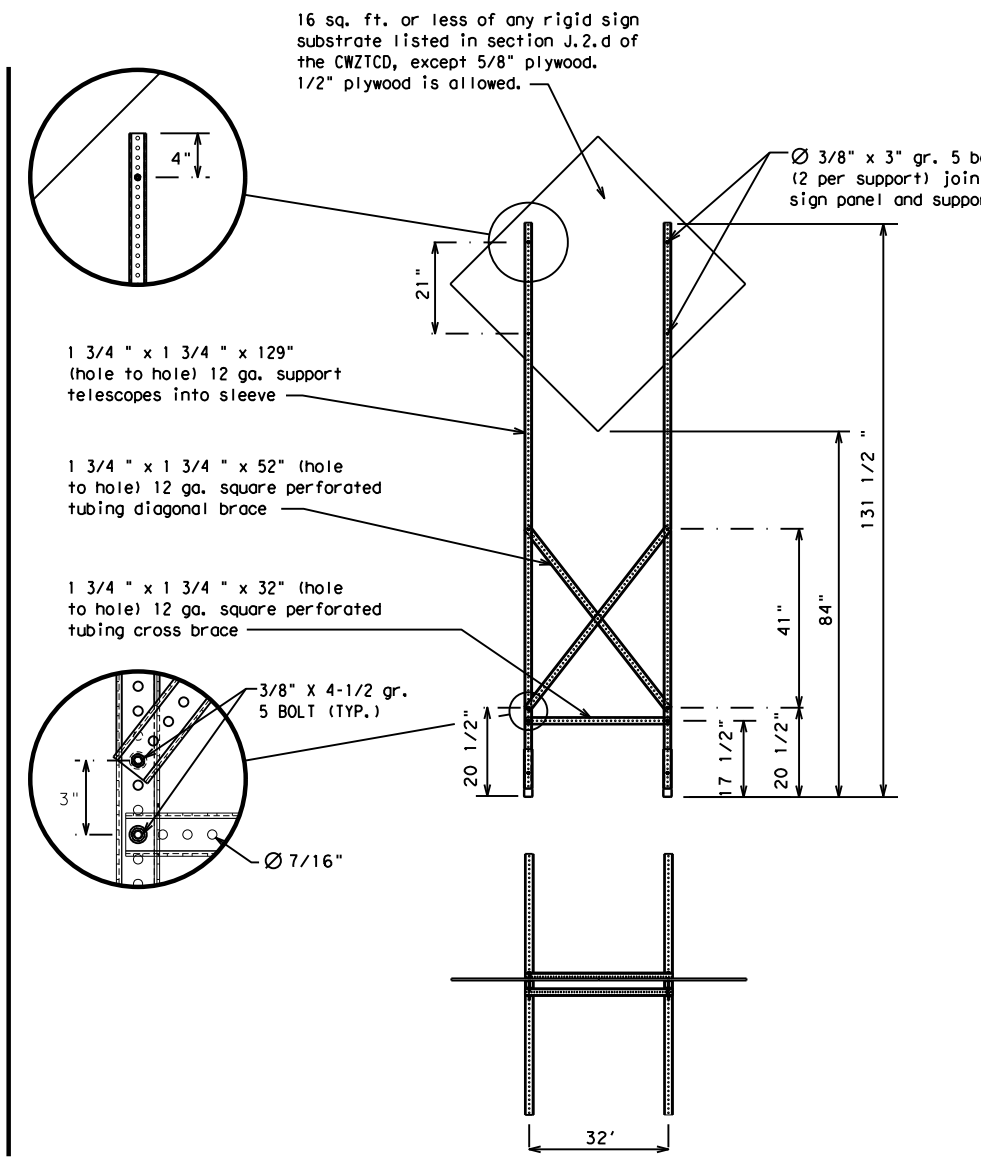
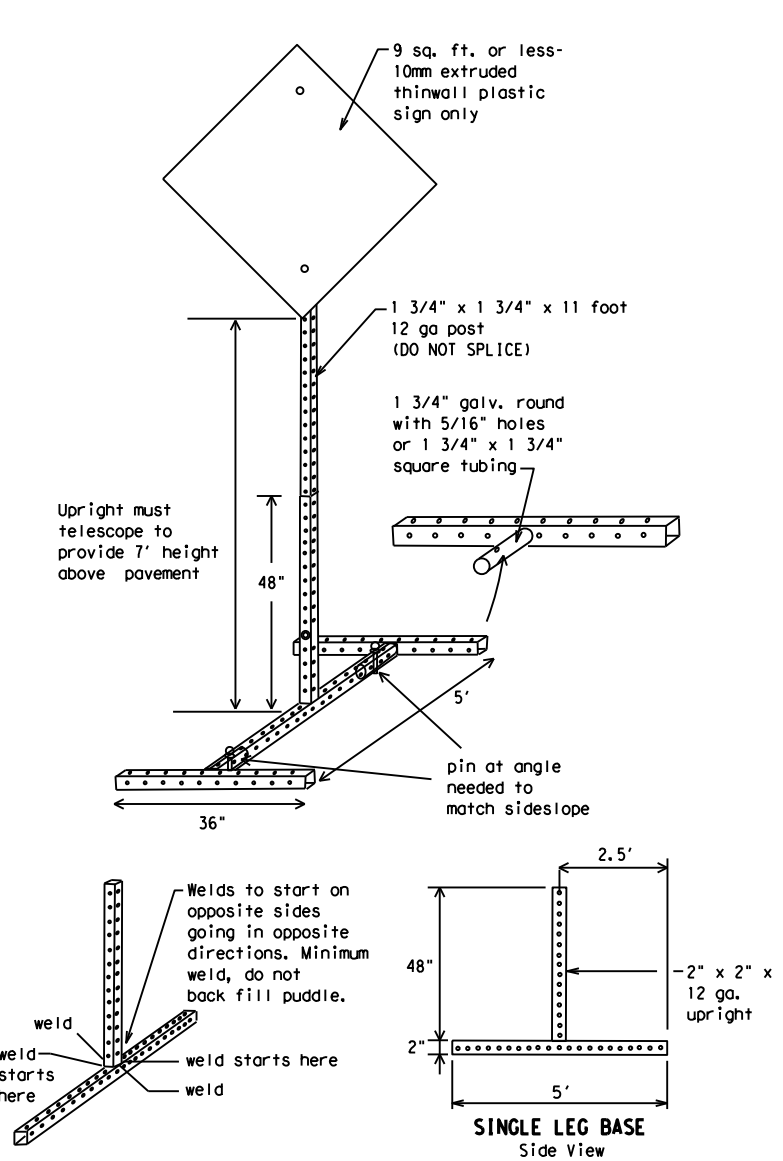
SKID MOUNTED WOOD SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS



SKID MOUNTED PERFORATED SQUARE STEEL TUBING SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS



GROUND MOUNTED SIGN SUPPORTS

Refer to the CWZTCD and the manufacturer's installation procedure for each type sign support. The maximum sign square footage shall adhere to the manufacturer's recommendation. Two post installations can be used for larger signs.

WEDGE ANCHORS
 Both steel and plastic Wedge Anchor Systems as shown on the SMD Standard Sheets may be used as temporary sign supports for signs up to 10 square feet of sign face. They may be set in concrete or in sturdy soils if approved by the Engineer. (See web address for "Traffic Engineering Standard Sheets" on BC(1)).

OTHER DESIGNS
 MORE DETAILS OF APPROVED LONG/INTERMEDIATE AND SHORT TERM SUPPORTS CAN BE FOUND ON THE CWZTCD LIST. SEE WEBSITE LOCATION.

- GENERAL NOTES**
- Nails may be used in the assembly of wooden sign supports, but 3/8" bolts with nuts or 3/8" x 3 1/2" lag screws must be used on every joint for final connection.
 - No more than 2 sign posts shall be placed within a 7 ft. circle, except for specific materials noted on the CWZTCD List.
 - When project is completed, all sign supports and foundations shall be removed from the project site. This will be considered subsidiary to Item 502.
- * See BC(4) for definition of "Work Duration."
 ** Wood sign posts MUST be one piece. Splicing will NOT be allowed. Posts shall be painted white.
 See the CWZTCD for the type of sign substrate that can be used for each approved sign support.

BARRICADE AND CONSTRUCTION TYPICAL SIGN SUPPORT

BC(5) - 21

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© TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
REVISIONS	0054	04	123	US 84					
9-07	8-14	DIST	COUNTY	SHEET NO.					
7-13	5-21	BWD	COLEMAN CO.	26					

WHEN NOT IN USE, REMOVE THE PCMS FROM THE RIGHT-OF-WAY OR PLACE THE PCMS BEHIND BARRIER OR GUARDRAIL WITH SIGN PANEL TURNED PARALLEL TO TRAFFIC

RECOMMENDED PHASES AND FORMATS FOR PCMS MESSAGES DURING ROADWORK ACTIVITIES

(The Engineer may approve other messages not specifically covered here.)

PORTABLE CHANGEABLE MESSAGE SIGNS

- The Engineer/Inspector shall approve all messages used on portable changeable message signs (PCMS).
- Messages on PCMS should contain no more than 8 words (about four to eight characters per word), not including simple words such as "TO," "FOR," "AT," etc.
- Messages should consist of a single phase, or two phases that alternate. Three-phase messages are not allowed. Each phase of the message should convey a single thought, and must be understood by itself.
- Use the word "EXIT" to refer to an exit ramp on a freeway; i.e., "EXIT CLOSED." Do not use the term "RAMP."
- Always use the route or interstate designation (IH, US, SH, FM) along with the number when referring to a roadway.
- When in use, the bottom of a stationary PCMS message panel should be a minimum 7 feet above the roadway, where possible.
- The message term "WEEKEND" should be used only if the work is to start on Saturday morning and end by Sunday evening at midnight. Actual days and hours of work should be displayed on the PCMS if work is to begin on Friday evening and/or continue into Monday morning.
- The Engineer/Inspector may select one of two options which are available for displaying a two-phase message on a PCMS. Each phase may be displayed for either four seconds each or for three seconds each.
- Do not "flash" messages or words included in a message. The message should be steady burn or continuous while displayed.
- Do not present redundant information on a two-phase message; i.e., keeping two lines of the message the same and changing the third line.
- Do not use the word "Danger" in message.
- Do not display the message "LANES SHIFT LEFT" or "LANES SHIFT RIGHT" on a PCMS. Drivers do not understand the message.
- Do not display messages that scroll horizontally or vertically across the face of the sign.
- The following table lists abbreviated words and two-word phrases that are acceptable for use on a PCMS. Both words in a phrase must be displayed together. Words or phrases not on this list should not be abbreviated, unless shown in the TMUTCD.
- PCMS character height should be at least 18 inches for trailer mounted units. They should be visible from at least 1/2 (.5) mile and the text should be legible from at least 600 feet at night and 800 feet in daylight. Truck mounted units must have a character height of 10 inches and must be legible from at least 400 feet.
- Each line of text should be centered on the message board rather than left or right justified.
- If disabled, the PCMS should default to an illegible display that will not alarm motorists and will only be used to alert workers that the PCMS has malfunctioned. A pattern such as a series of horizontal solid bars is appropriate.

Phase 1: Condition Lists

Road/Lane/Ramp Closure List

FREEWAY CLOSED X MILE	FRONTAGE ROAD CLOSED
ROAD CLOSED AT SH XXX	SHOULDER CLOSED XXX FT
ROAD CLSD AT FM XXXX	RIGHT LN CLOSED XXX FT
RIGHT X LANES CLOSED	RIGHT X LANES OPEN
CENTER LANE CLOSED	DAYTIME LANE CLOSURES
NIGHT LANE CLOSURES	I-XX SOUTH EXIT CLOSED
VARIOUS LANES CLOSED	EXIT XXX CLOSED X MILE
EXIT CLOSED	RIGHT LN TO BE CLOSED
MALL DRIVEWAY CLOSED	X LANES CLOSED TUE - FRI
XXXXXXXX BLVD CLOSED	

Other Condition List

ROADWORK XXX FT	ROAD REPAIRS XXXX FT
FLAGGER XXXX FT	LANE NARROWS XXXX FT
RIGHT LN NARROWS XXXX FT	TWO-WAY TRAFFIC XX MILE
MERGING TRAFFIC XXXX FT	CONST TRAFFIC XXX FT
LOOSE GRAVEL XXXX FT	UNEVEN LANES XXXX FT
DETOUR X MILE	ROUGH ROAD XXXX FT
ROADWORK PAST SH XXXX	ROADWORK NEXT FRI-SUN
BUMP XXXX FT	US XXX EXIT X MILES
TRAFFIC SIGNAL XXXX FT	LANES SHIFT *

* LANES SHIFT in Phase 1 must be used with STAY IN LANE in Phase 2.

Phase 2: Possible Component Lists

Action to Take/Effect on Travel List

MERGE RIGHT	FORM X LINES RIGHT
DETOUR NEXT X EXITS	USE XXXXX RD EXIT
USE EXIT XXX	USE EXIT I-XX NORTH
STAY ON US XXX SOUTH	USE I-XX E TO I-XX N
TRUCKS USE US XXX N	WATCH FOR TRUCKS
WATCH FOR TRUCKS	EXPECT DELAYS
EXPECT DELAYS	PREPARE TO STOP
REDUCE SPEED XXX FT	END SHOULDER USE
USE OTHER ROUTES	WATCH FOR WORKERS
STAY IN LANE *	

Location List

AT FM XXXX
BEFORE RAILROAD CROSSING
NEXT X MILES
PAST US XXX EXIT
XXXXXXXX TO XXXXXX
US XXX TO FM XXXX

Warning List

SPEED LIMIT XX MPH
MAXIMUM SPEED XX MPH
MINIMUM SPEED XX MPH
ADVISORY SPEED XX MPH
RIGHT LANE EXIT
USE CAUTION
DRIVE SAFELY
DRIVE WITH CARE

** Advance Notice List

TUE-FRI XX AM-X PM
APR XX-XX X PM-X AM
BEGINS MONDAY
BEGINS MAY XX
MAY X-X XX PM - XX AM
NEXT FRI-SUN
XX AM TO XX PM
NEXT TUE AUG XX
TONIGHT XX PM-XX AM

** See Application Guidelines Note 6.

APPLICATION GUIDELINES

- Only 1 or 2 phases are to be used on a PCMS.
- The 1st phase (or both) should be selected from the "Road/Lane/Ramp Closure List" and the "Other Condition List".
- A 2nd phase can be selected from the "Action to Take/Effect on Travel, Location, General Warning, or Advance Notice Phase Lists".
- A Location Phase is necessary only if a distance or location is not included in the first phase selected.
- If two PCMS are used in sequence, they must be separated by a minimum of 1000 ft. Each PCMS shall be limited to two phases, and should be understandable by themselves.
- For advance notice, when the current date is within seven days of the actual work date, calendar days should be replaced with days of the week. Advance notification should typically be for no more than one week prior to the work.

WORDING ALTERNATIVES

- The words RIGHT, LEFT and ALL can be interchanged as appropriate.
- Roadway designations IH, US, SH, FM and LP can be interchanged as appropriate.
- EAST, WEST, NORTH and SOUTH (or abbreviations E, W, N and S) can be interchanged as appropriate.
- Highway names and numbers replaced as appropriate.
- ROAD, HIGHWAY and FREEWAY can be interchanged as needed.
- AHEAD may be used instead of distances if necessary.
- FT and MI, MILE and MILES interchanged as appropriate.
- AT, BEFORE and PAST interchanged as needed.
- Distances or AHEAD can be eliminated from the message if a location phase is used.

PCMS SIGNS WITHIN THE R.O.W. SHALL BE BEHIND GUARDRAIL OR CONCRETE BARRIER OR SHALL HAVE A MINIMUM OF FOUR (4) PLASTIC DRUMS PLACED PERPENDICULAR TO TRAFFIC ON THE UPSTREAM SIDE OF THE PCMS, WHEN EXPOSED TO ONE DIRECTION OF TRAFFIC. WHEN EXPOSED TO TWO WAY TRAFFIC, THE FOUR DRUMS SHOULD BE PLACED WITH ONE DRUM AT EACH OF THE FOUR CORNERS OF THE UNIT.

FULL MATRIX PCMS SIGNS

- When Full Matrix PCMS signs are used, the character height and legibility/visibility requirements shall be maintained as listed in Note 15 under "PORTABLE CHANGEABLE MESSAGE SIGNS" above.
- When symbol signs, such as the "Flagger Symbol" (CW20-7) are represented graphically on the Full Matrix PCMS sign and, with the approval of the Engineer, it shall maintain the legibility/visibility requirement listed above.
- When symbol signs are represented graphically on the Full Matrix PCMS, they shall only supplement the use of the static sign represented, and shall not substitute for, or replace that sign.
- A full matrix PCMS may be used to simulate a flashing arrow board provided it meets the visibility, flash rate and dimming requirements on BC(7), for the same size arrow.

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WORD OR PHRASE	ABBREVIATION	WORD OR PHRASE	ABBREVIATION
Access Road	ACCS RD	Major	MAJ
Alternate	ALT	Miles	MI
Avenue	AVE	Miles Per Hour	MPH
Best Route	BEST RTE	Minor	MNR
Boulevard	BLVD	Monday	MON
Bridge	BRDG	Normal	NORM
Canal	CANT	North	N
Center	CTR	Northbound	(route) N
Construction Ahead	CONST AHD	Parking	PKING
CROSSING	XING	Road	RD
Detour Route	DETOUR RTE	Right Lane	RT LN
Do Not	DONT	Saturday	SAT
East	E	Service Road	SERV RD
Eastbound	(route) E	Shoulder	SHLDR
Emergency	EMER	Slippery	SLIP
Emergency Vehicle	EMER VEH	South	S
Entrance, Enter	ENT	Southbound	(route) S
Express Lane	EXP LN	Speed	SPD
Expressway	EXPWY	Street	ST
XXXX Feet	XXXX FT	Sunday	SUN
Fog Ahead	FOG AHD	Telephone	PHONE
Freeway	FRWY, FWY	Temporary	TEMP
Freeway Blocked	FWY BLKD	Thursday	THURS
Friday	FRI	To Downtown	TO DWNTN
Hazardous Driving	HAZ DRIVING	Traffic	TRAF
Hazardous Material	HAZMAT	Travelers	TRVLR
High-Occupancy Vehicle	HOV	Tuesday	TUES
Highway	HWY	Time Minutes	TIME MIN
Hour(s)	HR, HRS	Upper Level	UPR LEVEL
Information	INFO	Vehicles (s)	VEH, VEHS
It Is	ITS	Warning	WARN
Junction	JCT	Wednesday	WED
Left	LFT	Weight Limit	WT LIMIT
Left Lane	LFT LN	West	W
Lane Closed	LN CLOSED	Westbound	(route) W
Lower Level	LWR LEVEL	Wet Pavement	WET PVMT
Maintenance	MAINT	Will Not	WONT

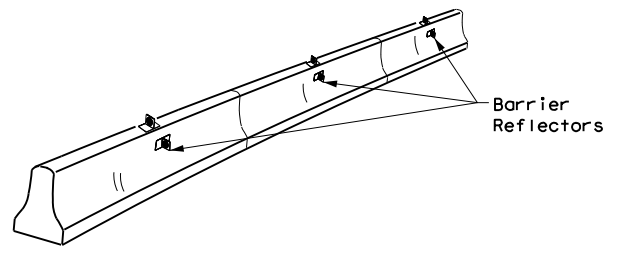
Roadway designation # IH-number, US-number, SH-number, FM-number

<h3>BARRICADE AND CONSTRUCTION PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)</h3>			
<h2>BC (6) - 21</h2>			
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© TxDOT	November 2002	CONT:	SECT:
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7-13	5-21	BWD	COLEMAN CO.
		SHEET NO.:	27

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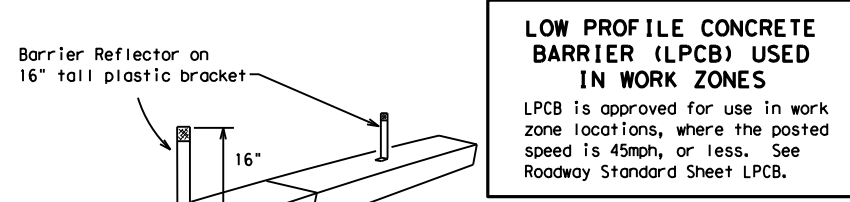
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- Barrier Reflectors shall be pre-qualified, and conform to the color and reflectivity requirements of DMS-8600. A list of prequalified Barrier Reflectors can be found at the Material Producer List web address shown on BC(1).
- Color of Barrier Reflectors shall be as specified in the TMUTCD. The cost of the reflectors shall be considered subsidiary to Item 512.



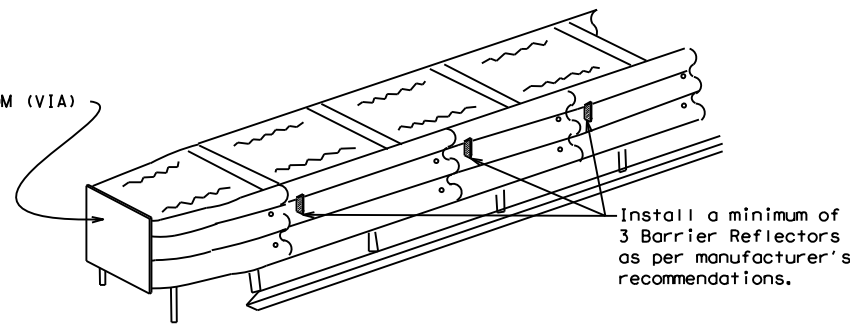
CONCRETE TRAFFIC BARRIER (CTB)

- Where traffic is on one side of the CTB, two (2) Barrier Reflectors shall be mounted in approximately the midsection of each section of CTB. An alternate mounting location is uniformly spaced at one end of each CTB. This will allow for attachment of a barrier grapple without damaging the reflector. The Barrier Reflector mounted on the side of the CTB shall be located directly below the reflector mounted on top of the barrier, as shown in the detail above.
- Where CTB separates two-way traffic, three barrier reflectors shall be mounted on each section of CTB. The reflector unit on top shall have two yellow reflective faces (Bi-Directional) while the reflectors on each side of the barrier shall have one yellow reflective face, as shown in the detail above.
- When CTB separates traffic traveling in the same direction, no barrier reflectors will be required on top of the CTB.
- Barrier Reflector units shall be yellow or white in color to match the edgeline being supplemented.
- Maximum spacing of Barrier Reflectors is forty (40) feet.
- Pavement markers or temporary flexible-reflective roadway marker tabs shall NOT be used as CTB delineation.
- Attachment of Barrier Reflectors to CTB shall be per manufacturer's recommendations.
- Missing or damaged Barrier Reflectors shall be replaced as directed by the Engineer.
- Single slope barriers shall be delineated as shown on the above detail.



LOW PROFILE CONCRETE BARRIER (LPCB) USED IN WORK ZONES
 LPCB is approved for use in work zone locations, where the posted speed is 45mph, or less. See Roadway Standard Sheet LPCB.

LOW PROFILE CONCRETE BARRIER (LPCB)



DELINEATION OF END TREATMENTS

END TREATMENTS FOR CTB'S USED IN WORK ZONES
 End treatments used on CTB's in work zones shall meet the appropriate crashworthy standards as defined in the Manual for Assessing Safety Hardware (MASH). Refer to the CWZTCD List for approved end treatments and manufacturers.

BARRIER REFLECTORS FOR CONCRETE TRAFFIC BARRIER AND ATTENUATORS

WARNING LIGHTS

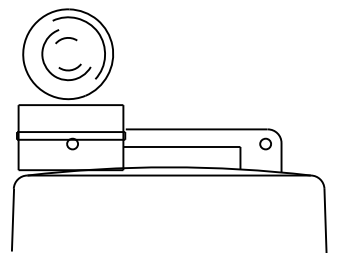
- Warning lights shall meet the requirements of the TMUTCD.
- Warning lights shall NOT be installed on barricades.
- Type A-Low Intensity Flashing Warning Lights are commonly used with drums. They are intended to warn of or mark a potentially hazardous area. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "FL". The Type A Warning Lights shall not be used with signs manufactured with Type B_{FL} or C_{FL} Sheeting meeting the requirements of Departmental Material Specification DMS-8300.
- Type-C and Type D 360 degree Steady Burn Lights are intended to be used in a series for delineation to supplement other traffic control devices. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "SB".
- The Engineer/Inspector or the plans shall specify the location and type of warning lights to be installed on the traffic control devices.
- When required by the Engineer, the Contractor shall furnish a copy of the warning lights certification. The warning light manufacturer will certify the warning lights meet the requirements of the latest ITE Purchase Specifications for Flashing and Steady-Burn Warning Lights.
- When used to delineate curves, Type-C and Type D Steady Burn Lights should only be placed on the outside of the curve, not the inside.
- The location of warning lights and warning reflectors on drums shall be as shown elsewhere in the plans.

WARNING LIGHTS MOUNTED ON PLASTIC DRUMS

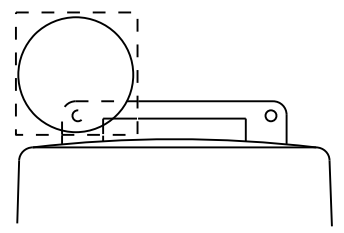
- Type A flashing warning lights are intended to warn drivers that they are approaching or are in a potentially hazardous area.
- Type A random flashing warning lights are not intended for delineation and shall not be used in a series.
- A series of sequential flashing warning lights placed on channelizing devices to form a merging taper may be used for delineation. If used, the successive flashing of the sequential warning lights should occur from the beginning of the taper to the end of the merging taper in order to identify the desired vehicle path. The rate of flashing for each light shall be 65 flashes per minute, plus or minus 10 flashes.
- Type C and D steady-burn warning lights are intended to be used in a series to delineate the edge of the travel lane on detours, on lane changes, on lane closures, and on other similar conditions.
- Type A, Type C and Type D warning lights shall be installed at locations as detailed on other sheets in the plans.
- Warning lights shall not be installed on a drum that has a sign, chevron or vertical panel.
- The maximum spacing for warning lights on drums should be identical to the channelizing device spacing.

WARNING REFLECTORS MOUNTED ON PLASTIC DRUMS AS A SUBSTITUTE FOR TYPE C (STEADY BURN) WARNING LIGHTS

- A warning reflector or approved substitute may be mounted on a plastic drum as a substitute for a Type C, steady burn warning light at the discretion of the Contractor unless otherwise noted in the plans.
- The warning reflector shall be yellow in color and shall be manufactured using a sign substrate approved for use with plastic drums listed on the CWZTCD.
- The warning reflector shall have a minimum retroreflective surface area (one-side) of 30 square inches.
- Round reflectors shall be fully reflectorized, including the area where attached to the drum.
- Square substrates must have a minimum of 30 square inches of reflectorized sheeting. They do not have to be reflectorized where it attaches to the drum.
- The side of the warning reflector facing approaching traffic shall have sheeting meeting the color and retroreflectivity requirements for DMS 8300-Type B or Type C.
- When used near two-way traffic, both sides of the warning reflector shall be reflectorized.
- The warning reflector should be mounted on the side of the handle nearest approaching traffic.
- The maximum spacing for warning reflectors should be identical to the channelizing device spacing requirements.



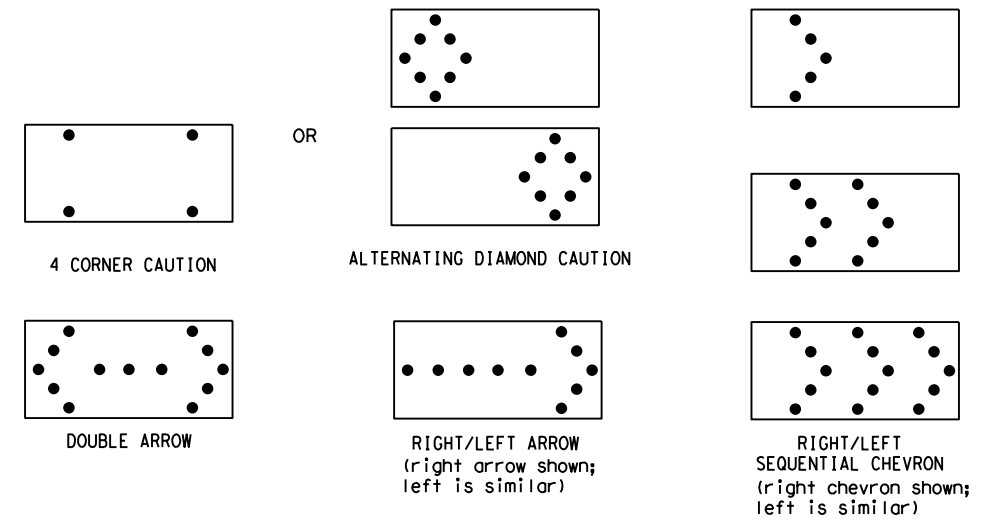
Type C Warning Light or approved substitute mounted on a drum adjacent to the travel way.



Warning reflector may be round or square. Must have a yellow reflective surface area of at least 30 square inches

Arrow Boards may be located behind channelizing devices in place for a shoulder taper or merging taper, otherwise they shall be delineated with four (4) channelizing devices placed perpendicular to traffic on the upstream side of traffic.

- The Flashing Arrow Board should be used for all lane closures on multi-lane roadways, or slow moving maintenance or construction activities on the travel lanes.
- Flashing Arrow Boards should not be used on two-lane, two-way roadways, detours, diversions or work on shoulders unless the "CAUTION" display (see detail below) is used.
- The Engineer/Inspector shall choose all appropriate signs, barricades and/or other traffic control devices that should be used in conjunction with the Flashing Arrow Board.
- The Flashing Arrow Board should be able to display the following symbols:



- The "CAUTION" display consists of four corner lamps flashing simultaneously, or the Alternating Diamond Caution mode as shown.
- The straight line caution display is NOT ALLOWED.
- The Flashing Arrow Board shall be capable of minimum 50 percent dimming from rated lamp voltage. The flashing rate of the lamps shall not be less than 25 nor more than 40 flashes per minute.
- Minimum lamp "on time" shall be approximately 50 percent for the flashing arrow and equal intervals of 25 percent for each sequential phase of the flashing chevron.
- The sequential arrow display is NOT ALLOWED.
- The flashing arrow display is the TxDOT standard; however, the sequential chevron display may be used during daylight operations.
- The Flashing Arrow Board shall be mounted on a vehicle, trailer or other suitable support.
- A Flashing Arrow Board SHALL NOT BE USED to laterally shift traffic.
- A full matrix PCMS may be used to simulate a Flashing Arrow Board provided it meets visibility, flash rate and dimming requirements on this sheet for the same size arrow.
- Minimum mounting height of trailer mounted Arrow Boards should be 7 feet from roadway to bottom of panel.

REQUIREMENTS			
TYPE	MINIMUM SIZE	MINIMUM NUMBER OF PANEL LAMPS	MINIMUM VISIBILITY DISTANCE
B	30 x 60	13	3/4 mile
C	48 x 96	15	1 mile

ATTENTION
 Flashing Arrow Boards shall be equipped with automatic dimming devices.

WHEN NOT IN USE, REMOVE THE ARROW BOARD FROM THE RIGHT-OF-WAY OR PLACE THE ARROW BOARD BEHIND CONCRETE TRAFFIC BARRIER OR GUARDRAIL.

FLASHING ARROW BOARDS

SHEET 7 OF 12

TRUCK-MOUNTED ATTENUATORS

- Truck-mounted attenuators (TMA) used on TxDOT facilities must meet the requirements outlined in the Manual for Assessing Safety Hardware (MASH).
- Refer to the CWZTCD for the requirements of Level 2 or Level 3 TMAs.
- Refer to the CWZTCD for a list of approved TMAs.
- TMAs are required on freeways unless otherwise noted in the plans.
- A TMA should be used anytime that it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the work performance.
- The only reason a TMA should not be required is when a work area is spread down the roadway and the work crew is an extended distance from the TMA.



BARRICADE AND CONSTRUCTION ARROW PANEL, REFLECTORS, WARNING LIGHTS & ATTENUATOR

BC (7) - 21

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REVISIONS		0054	04	123	US 84				
9-07	8-14	DIST	COUNTY	SHEET NO.					
7-13	5-21	BWD	COLEMAN CO.	28					

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GENERAL NOTES

- For long term stationary work zones on freeways, drums shall be used as the primary channelizing device.
- For intermediate term stationary work zones on freeways, drums should be used as the primary channelizing device but may be replaced in tangent sections by vertical panels, or 42" two-piece cones. In tangent sections, one-piece cones may be used with the approval of the Engineer but only if personnel are present on the project at all times to maintain the cones in proper position and location.
- For short term stationary work zones on freeways, drums are the preferred channelizing device but may be replaced in tapers, transitions and tangent sections by vertical panels, two-piece cones or one-piece cones as approved by the Engineer.
- Drums and all related items shall comply with the requirements of the current version of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Drums, bases, and related materials shall exhibit good workmanship and shall be free from objectionable marks or defects that would adversely affect their appearance or serviceability.
- The Contractor shall have a maximum of 24 hours to replace any plastic drums identified for replacement by the Engineer/Inspector. The replacement device must be an approved device.

GENERAL DESIGN REQUIREMENTS

Pre-qualified plastic drums shall meet the following requirements:

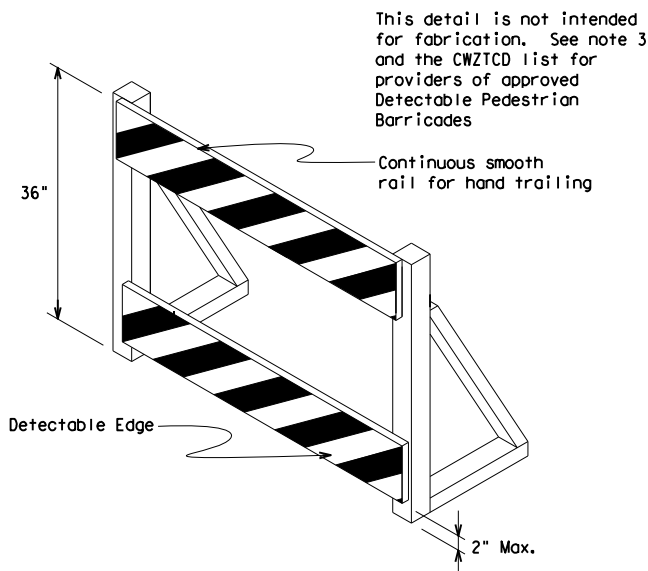
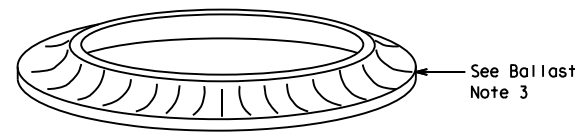
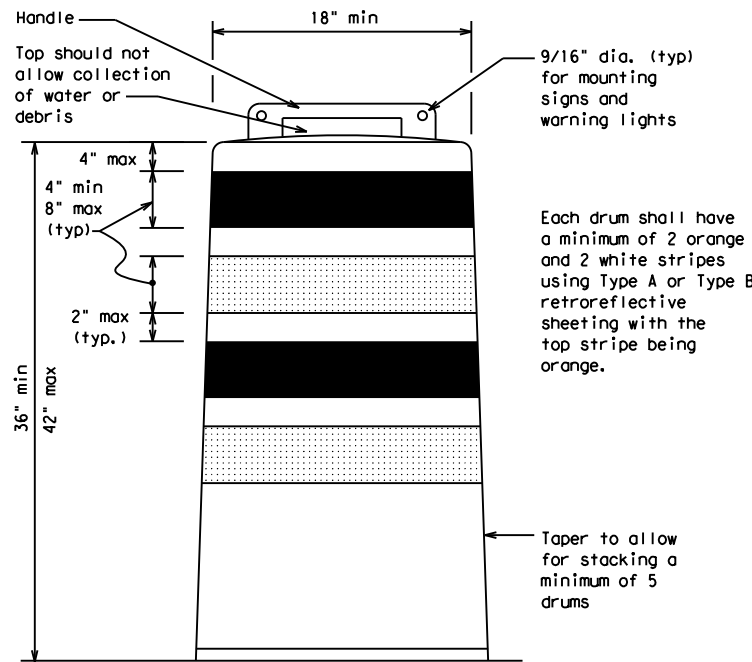
- Plastic drums shall be a two-piece design; the "body" of the drum shall be the top portion and the "base" shall be the bottom.
- The body and base shall lock together in such a manner that the body separates from the base when impacted by a vehicle traveling at a speed of 20 MPH or greater but prevents accidental separation due to normal handling and/or air turbulence created by passing vehicles.
- Plastic drums shall be constructed of lightweight flexible, and deformable materials. The Contractor shall NOT use metal drums or single piece plastic drums as channelization devices or sign supports.
- Drums shall present a profile that is a minimum of 18 inches in width at the 36 inch height when viewed from any direction. The height of drum unit (body installed on base) shall be a minimum of 36 inches and a maximum of 42 inches.
- The top of the drum shall have a built-in handle for easy pickup and shall be designed to drain water and not collect debris. The handle shall have a minimum of two widely spaced 9/16 inch diameter holes to allow attachment of a warning light, warning reflector unit or approved compliant sign.
- The exterior of the drum body shall have a minimum of four alternating orange and white retroreflective circumferential stripes not less than 4 inches nor greater than 8 inches in width. Any non-reflectorized space between any two adjacent stripes shall not exceed 2 inches in width.
- Bases shall have a maximum width of 36 inches, a maximum height of 4 inches, and a minimum of two footholds of sufficient size to allow base to be held down while separating the drum body from the base.
- Plastic drums shall be constructed of ultra-violet stabilized, orange, high-density polyethylene (HDPE) or other approved material.
- Drum body shall have a maximum unballasted weight of 11 lbs.
- Drum and base shall be marked with manufacturer's name and model number.

RETROREFLECTIVE SHEETING

- The stripes used on drums shall be constructed of sheeting meeting the color and retroreflectivity requirements of Departmental Materials Specification DMS-8300, "Sign Face Materials." Type A or Type B reflective sheeting shall be supplied unless otherwise specified in the plans.
- The sheeting shall be suitable for use on and shall adhere to the drum surface such that, upon vehicular impact, the sheeting shall remain adhered in-place and exhibit no delaminating, cracking, or loss of retroreflectivity other than that loss due to abrasion of the sheeting surface.

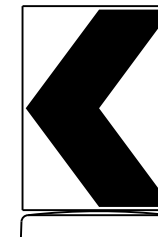
BALLAST

- Unballasted bases shall be large enough to hold up to 50 lbs. of sand. This base, when filled with the ballast material, should weigh between 35 lbs (minimum) and 50 lbs (maximum). The ballast may be sand in one to three sandbags separate from the base, sand in a sand-filled plastic base, or other ballasting devices as approved by the Engineer. Stacking of sandbags will be allowed, however height of sandbags above pavement surface may not exceed 12 inches.
- Bases with built-in ballast shall weigh between 40 lbs. and 50 lbs. Built-in ballast can be constructed of an integral crumb rubber base or a solid rubber base.
- Recycled truck tire sidewalls may be used for ballast on drums approved for this type of ballast on the CWZTCD list.
- The ballast shall not be heavy objects, water, or any material that would become hazardous to motorists, pedestrians, or workers when the drum is struck by a vehicle.
- When used in regions susceptible to freezing, drums shall have drainage holes in the bottoms so that water will not collect and freeze becoming a hazard when struck by a vehicle.
- Ballast shall not be placed on top of drums.
- Adhesives may be used to secure base of drums to pavement.

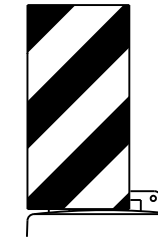


DETECTABLE PEDESTRIAN BARRICADES

- When existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility. Refer to WZ(BTS-2) for Pedestrian Control requirements for Sidewalk Diversions, Sidewalk Detours and Crosswalk Closures.
- Where pedestrians with visual disabilities normally use the closed sidewalk, a Detectable Pedestrian Barricade shall be placed across the full width of the closed sidewalk instead of a Type 3 Barricade.
- Detectable pedestrian barricades similar to the one pictured above, longitudinal channelizing devices, some concrete barriers, and wood or chain link fencing with a continuous detectable edging can satisfactorily delineate a pedestrian path.
- Tape, rope, or plastic chain strung between devices are not detectable, do not comply with the design standards in the "Americans with Disabilities Act Accessibility Guidelines (ADAAG)" and should not be used as a control for pedestrian movements.
- Warning lights shall not be attached to detectable pedestrian barricades.
- Detectable pedestrian barricades should use 8" nominal barricade rails as shown on BC(10) provided that the top rail provides a smooth continuous rail suitable for hand trailing with no splinters, burrs, or sharp edges.



18" x 24" Sign
(Maximum Sign Dimension)
Chevron CW1-8, Opposing Traffic Lane
Divider, Driveway sign D70a, Keep Right
R4 series or other signs as approved
by Engineer



12" x 24"
Vertical Panel
mount with diagonals
sloping down towards
travel way

Plywood, Aluminum or Metal sign
substrates shall NOT be used on
plastic drums

SIGNS, CHEVRONS, AND VERTICAL PANELS MOUNTED ON PLASTIC DRUMS

- Signs used on plastic drums shall be manufactured using substrates listed on the CWZTCD.
- Chevrons and other work zone signs with an orange background shall be manufactured with Type B_{FL} or Type C_{FL} Orange sheeting meeting the color and retroreflectivity requirements of DMS-8300, "Sign Face Material," unless otherwise specified in the plans.
- Vertical Panels shall be manufactured with orange and white sheeting meeting the requirements of DMS-8300 Type A or Type B. Diagonal stripes on Vertical Panels shall slope down toward the intended traveled lane.
- Other sign messages (text or symbolic) may be used as approved by the Engineer. Sign dimensions shall not exceed 18 inches in width or 24 inches in height, except for the R9 series signs discussed in note 8 below.
- Signs shall be installed using a 1/2 inch bolt (nominal) and nut, two washers, and one locking washer for each connection.
- Mounting bolts and nuts shall be fully engaged and adequately torqued. Bolts should not extend more than 1/2 inch beyond nuts.
- Chevrons may be placed on drums on the outside of curves, on merging tapers or on shifting tapers. When used in these locations, they may be placed on every drum or spaced not more than on every third drum. A minimum of three (3) should be used at each location called for in the plans.
- R9-9, R9-10, R9-11 and R9-11a Sidewalk Closed signs which are 24 inches wide may be mounted on plastic drums, with approval of the Engineer.

SHEET 8 OF 12



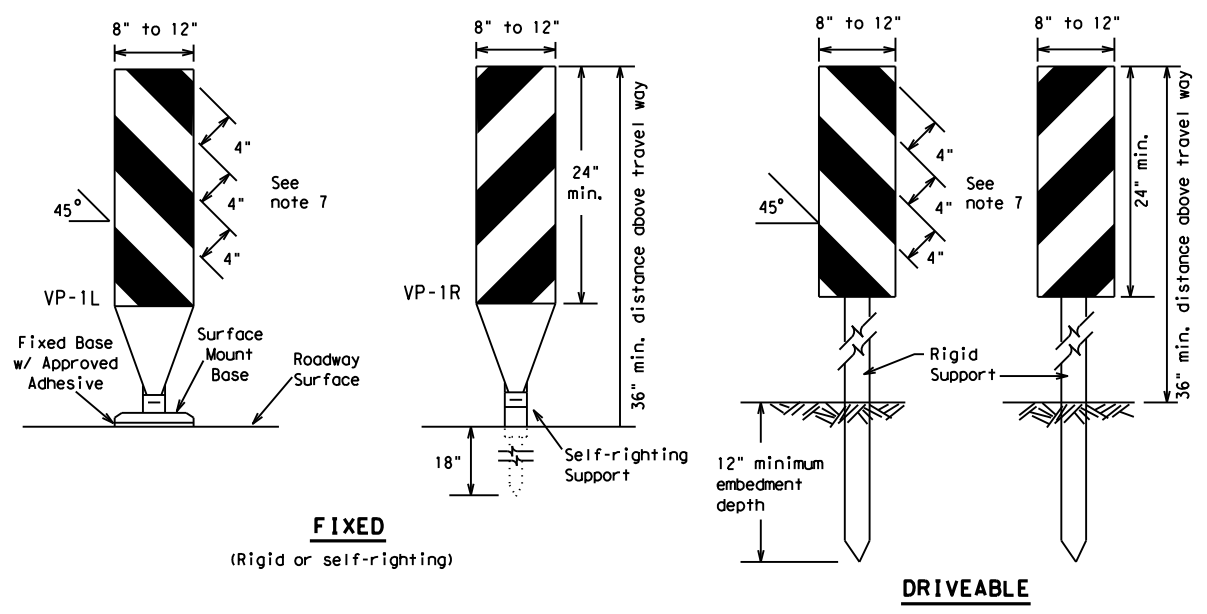
BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (8) - 21

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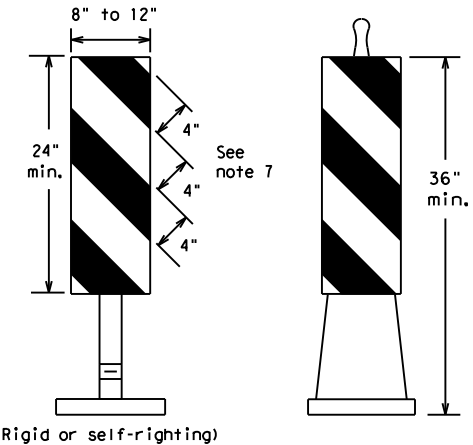
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FIXED
(Rigid or self-righting)

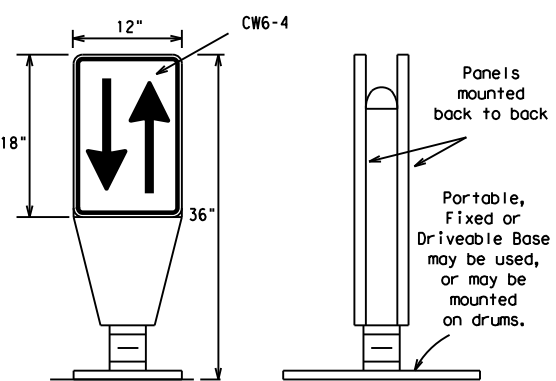
DRIVEABLE



PORTABLE

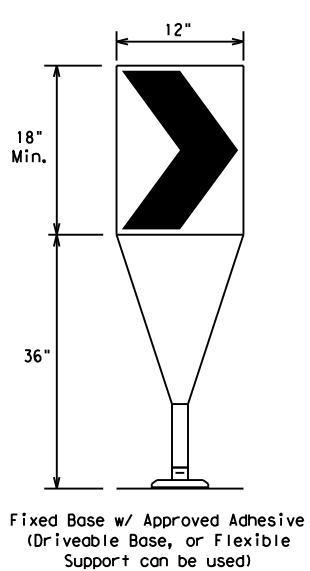
VERTICAL PANELS (VPs)

- Vertical Panels (VP's) are normally used to channelize traffic or divide opposing lanes of traffic.
- VP's may be used in daytime or nighttime situations. They may be used at the edge of shoulder drop-offs and other areas such as lane transitions where positive daytime and nighttime delineation is required. The Engineer/Inspector shall refer to the Roadway Design Manual for additional requirements on the use VP's for drop-offs.
- VP's should be mounted back to back if used at the edge of cuts adjacent to two-way two lane roadways. Stripes are to be reflective orange and reflective white and should always slope downward toward the travel lane.
- VP's used on expressways and freeways or other high speed roadways, may have more than 270 square inches of retroreflective area facing traffic.
- Self-righting supports are available with portable base. See "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Sheeting for the VP's shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300, unless noted otherwise.
- Where the height of reflective material on the vertical panel is 36 inches or greater, a panel stripe of 6 inches shall be used.



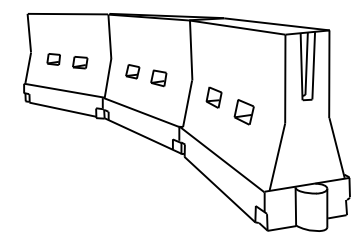
OPPOSING TRAFFIC LANE DIVIDERS (OTLD)

- Opposing Traffic Lane Dividers (OTLD) are delineation devices designed to convert a normal one-way roadway section to two-way operation. OTLD's are used on temporary centerlines. The upward and downward arrows on the sign's face indicate the direction of traffic on either side of the divider. The base is secured to the pavement with an adhesive or rubber weight to minimize movement caused by a vehicle impact or wind gust.
- The OTLD may be used in combination with 42" cones or VPs.
- Spacing between the OTLD shall not exceed 500 feet. 42" cones or VPs placed between the OTLD's should not exceed 100 foot spacing.
- The OTLD shall be orange with a black non-reflective legend. Sheeting for the OTLD shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.



- The chevron shall be a vertical rectangle with a minimum size of 12 by 18 inches.
- Chevrons are intended to give notice of a sharp change of alignment with the direction of travel and provide additional emphasis and guidance for vehicle operators with regard to changes in horizontal alignment of the roadway.
- Chevrons, when used, shall be erected on the outside of a sharp curve or turn, or on the far side of an intersection. They shall be in line with and at right angles to approaching traffic. Spacing should be such that the motorist always has three in view, until the change in alignment eliminates its need.
- To be effective, the chevron should be visible for at least 500 feet.
- Chevrons shall be orange with a black nonreflective legend. Sheeting for the chevron shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.
- For Long Term Stationary use on tapers or transitions on freeways and divided highways, self-righting chevrons may be used to supplement plastic drums but not to replace plastic drums.

CHEVRONS



LONGITUDINAL CHANNELIZING DEVICES (LCD)

- LCDs are crashworthy, lightweight, deformable devices that are highly visible, have good target value and can be connected together. They are not designed to contain or redirect a vehicle on impact.
- LCDs may be used instead of a line of cones or drums.
- LCDs shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- LCDs should not be used to provide positive protection for obstacles, pedestrians or workers.
- LCDs shall be supplemented with retroreflective delineation as required for temporary barriers on BC(7) when placed roughly parallel to the travel lanes.
- LCDs used as barricades placed perpendicular to traffic should have at least one row of reflective sheeting meeting the requirements for barricade rails as shown on BC(10). Place reflective sheeting near the top of the LCD along the full length of the device.

WATER BALLASTED SYSTEMS USED AS BARRIERS

- Water ballasted systems used as barriers shall not be used solely to channelize road users, but also to protect the work space per the appropriate Manual for Assessing Safety Hardware (MASH) crashworthiness requirements based on roadway speed and barrier application.
- Water ballasted systems used to channelize vehicular traffic shall be supplemented with retroreflective delineation or channelizing devices to improve daytime/nighttime visibility. They may also be supplemented with pavement markings.
- Water ballasted systems used as barriers shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- Water ballasted systems used as barriers should not be used for a merging taper except in low speed (less than 45 MPH) urban areas. When used on a taper in a low speed urban area, the taper shall be delineated and the taper length should be designed to optimize road user operations considering the available geometric conditions.
- When water ballasted systems used as barriers have blunt ends exposed to traffic, they should be attenuated as per manufacturer recommendations or flared to a point outside the clear zone.

If used to channelize pedestrians, longitudinal channelizing devices or water ballasted systems must have a continuous detectable bottom for users of long canes and the top of the unit shall not be less than 32 inches in height.

HOLLOW OR WATER BALLASTED SYSTEMS USED AS LONGITUDINAL CHANNELIZING DEVICES OR BARRIERS

GENERAL NOTES

- Work Zone channelizing devices illustrated on this sheet may be installed in close proximity to traffic and are suitable for use on high or low speed roadways. The Engineer/Inspector shall ensure that spacing and placement is uniform and in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Channelizing devices shown on this sheet may have a driveable, fixed or portable base. The requirement for self-righting channelizing devices must be specified in the General Notes or other plan sheets.
- Channelizing devices on self-righting supports should be used in work zone areas where channelizing devices are frequently impacted by errant vehicles or vehicle related wind gusts making alignment of the channelizing devices difficult to maintain. Locations of these devices shall be detailed elsewhere in the plans. These devices shall conform to the TMUTCD and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- The Contractor shall maintain devices in a clean condition and replace damaged, nonreflective, faded, or broken devices and bases as required by the Engineer/Inspector. The Contractor shall be required to maintain proper device spacing and alignment.
- Portable bases shall be fabricated from virgin and/or recycled rubber. The portable bases shall weigh a minimum of 30 lbs.
- Pavement surfaces shall be prepared in a manner that ensures proper bonding between the adhesives, the fixed mount bases and the pavement surface. Adhesives shall be prepared and applied according to the manufacturer's recommendations.
- The installation and removal of channelizing devices shall not cause detrimental effects to the final pavement surfaces, including pavement surface discoloration or surface integrity. Driveable bases shall not be permitted on final pavement surfaces. The Engineer/Inspector shall approve all application and removal procedures of fixed bases.

Posted Speed	Formula	Minimum Desirable Taper Lengths * * *			Suggested Maximum Spacing of Channelizing Devices	
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent
30	L = WS ² / 60	150'	165'	180'	30'	60'
35		205'	225'	245'	35'	70'
40		265'	295'	320'	40'	80'
45	L = WS	450'	495'	540'	45'	90'
50		500'	550'	600'	50'	100'
55		550'	605'	660'	55'	110'
60		600'	660'	720'	60'	120'
65		650'	715'	780'	65'	130'
70		700'	770'	840'	70'	140'
75		750'	825'	900'	75'	150'
80		800'	880'	960'	80'	160'

* * *Taper lengths have been rounded off.
 L=Length of Taper (FT.) W=Width of Offset (FT.)
 S=Posted Speed (MPH)

SUGGESTED MAXIMUM SPACING OF CHANNELIZING DEVICES AND MINIMUM DESIRABLE TAPER LENGTHS

SHEET 9 OF 12



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (9) - 21

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TYPE 3 BARRICADES

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTCD) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road, striping should slope downward in both directions toward the center of roadway.
4. Striping of rails, for the right side of the roadway, should slope downward to the left. For the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rails. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand bags shall not be stacked in a manner that covers any portion of a barricade rails reflective sheeting. Rock, concrete, iron, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed along or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheeting for barricades shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300 unless otherwise noted.

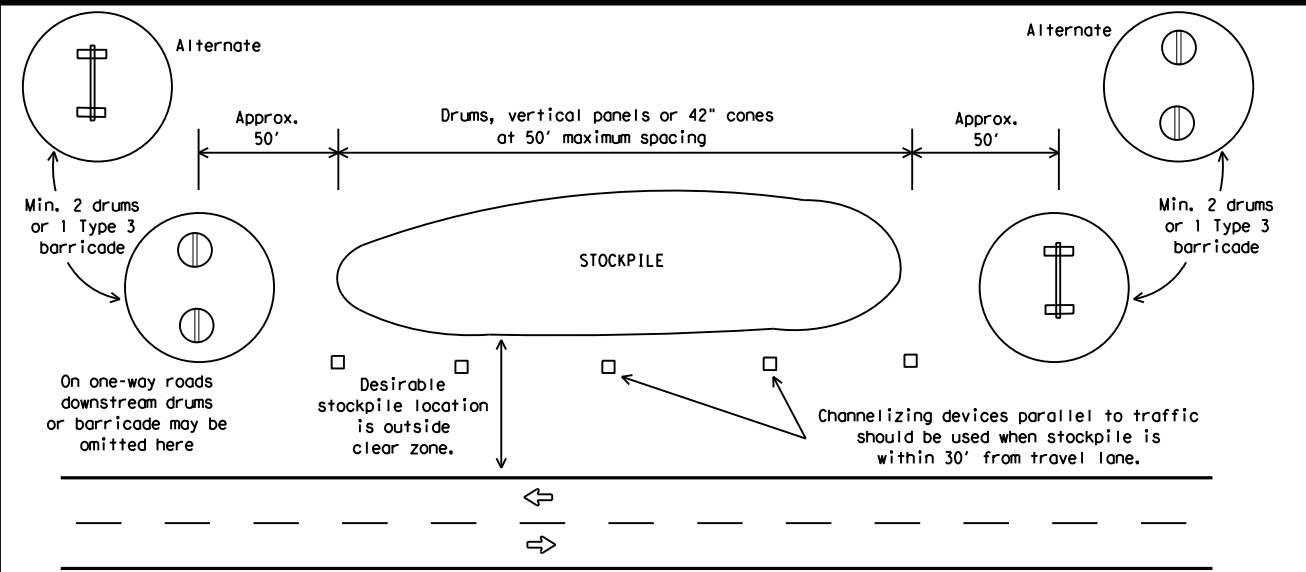
Barricades shall NOT be used as a sign support.



TYPICAL STRIPING DETAIL FOR BARRICADE RAIL

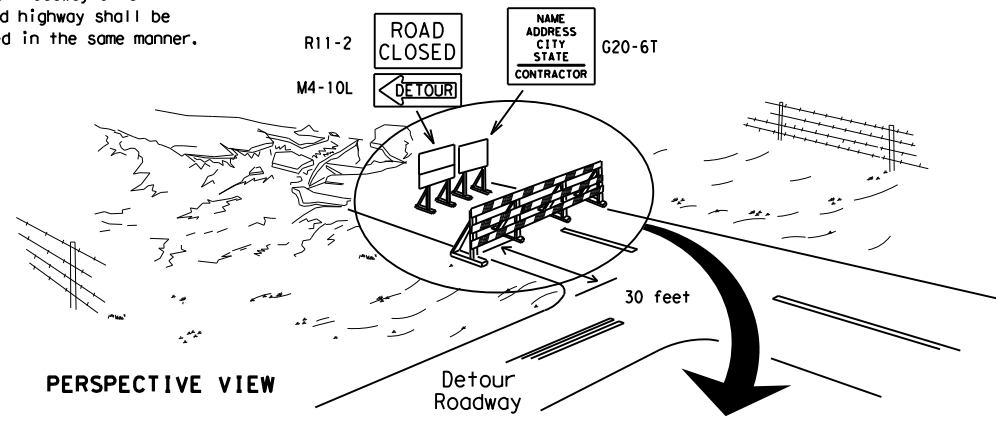


TYPICAL PANEL DETAIL FOR SKID OR POST TYPE BARRICADES



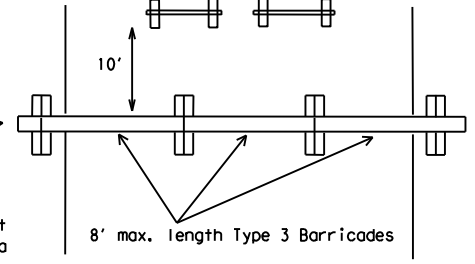
TRAFFIC CONTROL FOR MATERIAL STOCKPILES

Each roadway of a divided highway shall be barricaded in the same manner.



PERSPECTIVE VIEW

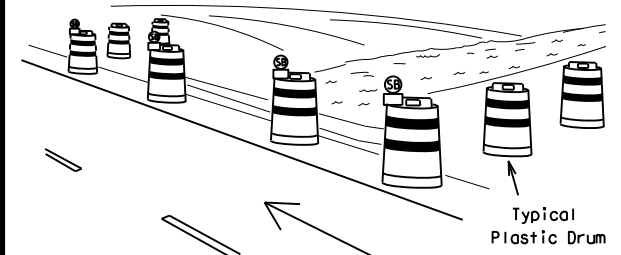
The three rails on Type 3 barricades shall be reflectorized orange and reflective white stripes on one side facing one-way traffic and both sides for two-way traffic. Barricade striping should slant downward in the direction of detour.



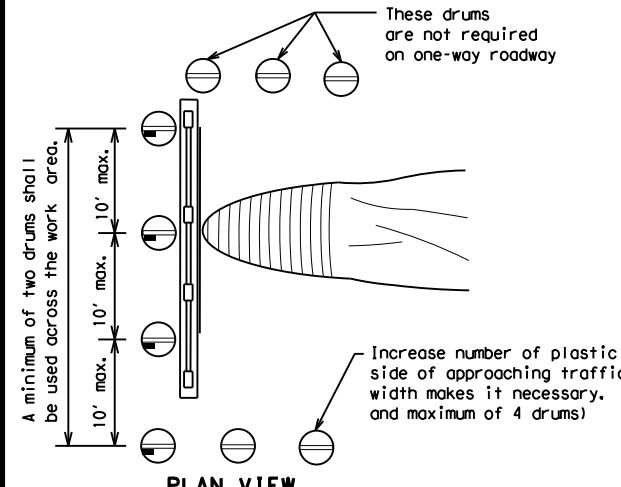
PLAN VIEW

1. Signs should be mounted on independent supports at a 7 foot mounting height in center of roadway. The signs should be a minimum of 10 feet behind Type 3 Barricades.
2. Advance signing shall be as specified elsewhere in the plans.

TYPE 3 BARRICADE (POST AND SKID) TYPICAL APPLICATION



PERSPECTIVE VIEW

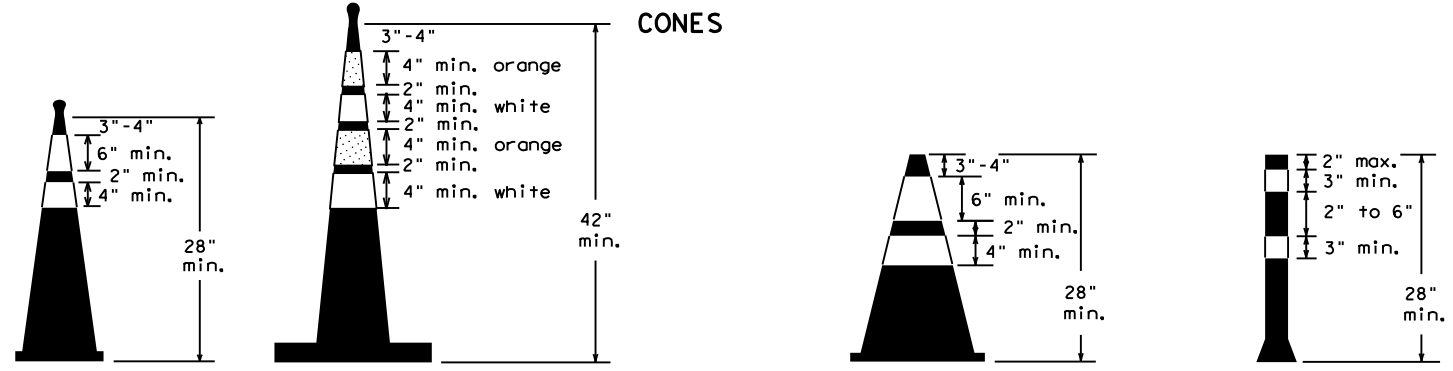


PLAN VIEW

CULVERT WIDENING OR OTHER ISOLATED WORK WITHIN THE PROJECT LIMITS

1. Where positive redirection capability is provided, drums may be omitted.
2. Plastic construction fencing may be used with drums for safety as required in the plans.
3. Vertical Panels on flexible support may be substituted for drums when the shoulder width is less than 4 feet.
4. When the shoulder width is greater than 12 feet, steady-burn lights may be omitted if drums are used.
5. Drums must extend the length of the culvert widening.

LEGEND	
	Plastic drum
	Plastic drum with steady burn light or yellow warning reflector
	Steady burn warning light or yellow warning reflector



Two-Piece cones

One-Piece cones

Tubular Marker

28" Cones shall have a minimum weight of 9 1/2 lbs.
 42" 2-piece cones shall have a minimum weight of 30 lbs. including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A or Type B.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined on BC(4). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone durations.
7. Cones or tubular markers used on each project should be of the same size and shape.



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (10) - 21

FILE: bc-21.dgn	DN: TxDOT	CR: TxDOT	OW: TxDOT	CK: TxDOT
© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	BWD	COLEMAN CO.	31	

WORK ZONE PAVEMENT MARKINGS

GENERAL

- The Contractor shall be responsible for maintaining work zone and existing pavement markings, in accordance with the standard specifications and special provisions, on all roadways open to traffic within the CSJ limits unless otherwise stated in the plans.
- Color, patterns and dimensions shall be in conformance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Additional supplemental pavement marking details may be found in the plans or specifications.
- Pavement markings shall be installed in accordance with the TMUTCD and as shown on the plans.
- When short term markings are required on the plans, short term markings shall conform with the TMUTCD, the plans and details as shown on the Standard Plan Sheet WZ(STPM).
- When standard pavement markings are not in place and the roadway is opened to traffic, DO NOT PASS signs shall be erected to mark the beginning of the sections where passing is prohibited and PASS WITH CARE signs at the beginning of sections where passing is permitted.
- All work zone pavement markings shall be installed in accordance with Item 662, "Work Zone Pavement Markings."

RAISED PAVEMENT MARKERS

- Raised pavement markers are to be placed according to the patterns on BC(12).
- All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and Departmental Material Specification DMS-4200 or DMS-4300.

PREFABRICATED PAVEMENT MARKINGS

- Removable prefabricated pavement markings shall meet the requirements of DMS-8241.
- Non-removable prefabricated pavement markings (foil back) shall meet the requirements of DMS-8240.

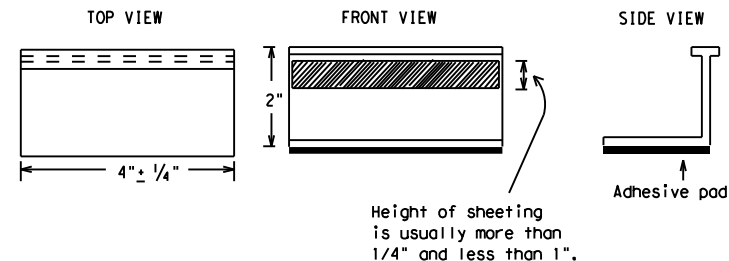
MAINTAINING WORK ZONE PAVEMENT MARKINGS

- The Contractor will be responsible for maintaining work zone pavement markings within the work limits.
- Work zone pavement markings shall be inspected in accordance with the frequency and reporting requirements of work zone traffic control device inspections as required by Form 599.
- The markings should provide a visible reference for a minimum distance of 300 feet during normal daylight hours and 160 feet when illuminated by automobile low-beam headlights at night, unless sight distance is restricted by roadway geometrics.
- Markings failing to meet this criteria within the first 30 days after placement shall be replaced at the expense of the Contractor as per Specification Item 662.

REMOVAL OF PAVEMENT MARKINGS

- Pavement markings that are no longer applicable, could create confusion or direct a motorist toward or into the closed portion of the roadway shall be removed or obliterated before the roadway is opened to traffic.
- The above shall not apply to detours in place for less than three days, where flaggers and/or sufficient channelizing devices are used in lieu of markings to outline the detour route.
- Pavement markings shall be removed to the fullest extent possible, so as not to leave a discernable marking. This shall be by any method approved by TxDOT Specification Item 677 for "Eliminating Existing Pavement Markings and Markers".
- The removal of pavement markings may require resurfacing or seal coating portions of the roadway as described in Item 677.
- Subject to the approval of the Engineer, any method that proves to be successful on a particular type pavement may be used.
- Blast cleaning may be used but will not be required unless specifically shown in the plans.
- Over-painting of the markings SHALL NOT BE permitted.
- Removal of raised pavement markers shall be as directed by the Engineer.
- Removal of existing pavement markings and markers will be paid for directly in accordance with Item 677, "ELIMINATING EXISTING PAVEMENT MARKINGS AND MARKERS," unless otherwise stated in the plans.
- Black-out marking tape may be used to cover conflicting existing markings for periods less than two weeks when approved by the Engineer.

Temporary Flexible-Reflective Roadway Marker Tabs



**STAPLES OR NAILS SHALL NOT BE USED TO SECURE
TEMPORARY FLEXIBLE-REFLECTIVE ROADWAY MARKER
TABS TO THE PAVEMENT SURFACE**

- Temporary flexible-reflective roadway marker tabs used as guidemarks shall meet the requirements of DMS-8242.
- Tabs detailed on this sheet are to be inspected and accepted by the Engineer or designated representative. Sampling and testing is not normally required, however at the option of the Engineer, either "A" or "B" below may be imposed to assure quality before placement on the roadway.
 - Select five (5) or more tabs at random from each lot or shipment and submit to the Construction Division, Materials and Pavement Section to determine specification compliance.
 - Select five (5) tabs and perform the following test. Affix five (5) tabs at 24 inch intervals on an asphaltic pavement in a straight line. Using a medium size passenger vehicle or pickup, run over the markers with the front and rear tires at a speed of 35 to 40 miles per hour, four (4) times in each direction. No more than one (1) out of the five (5) reflective surfaces shall be lost or displaced as a result of this test.
- Small design variances may be noted between tab manufacturers.
- See Standard Sheet WZ(STPM) for tab placement on new pavements. See Standard Sheet TCP(7-1) for tab placement on seal coat work.

RAISED PAVEMENT MARKERS USED AS GUIDEMARKS

- Raised pavement markers used as guidemarks shall be from the approved product list, and meet the requirements of DMS-4200.
- All temporary construction raised pavement markers provided on a project shall be of the same manufacturer.
- Adhesive for guidemarks shall be bituminous material hot applied or butyl rubber pad for all surfaces, or thermoplastic for concrete surfaces.

Guidemarks shall be designated as:
 YELLOW - (two amber reflective surfaces with yellow body).
 WHITE - (one silver reflective surface with white body).

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
TRAFFIC BUTTONS	DMS-4300
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240
TEMPORARY REMOVABLE, PREFABRICATED PAVEMENT MARKINGS	DMS-8241
TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS	DMS-8242

A list of prequalified reflective raised pavement markers, non-reflective traffic buttons, roadway marker tabs and other pavement markings can be found at the Material Producer List web address shown on BC(1).

SHEET 11 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKINGS

BC(11)-21

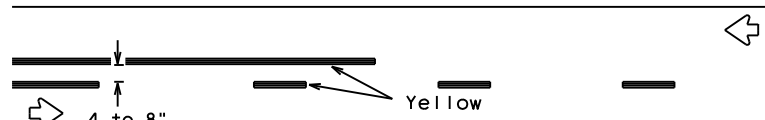
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©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
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2-98 9-07 5-21	DIST	COUNTY	SHEET NO.	
1-02 7-13	BWD	COLEMAN CO.	32	
11-02 8-14				

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 DATE: 1/11/2023 7:36:40 AM
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PAVEMENT MARKING PATTERNS

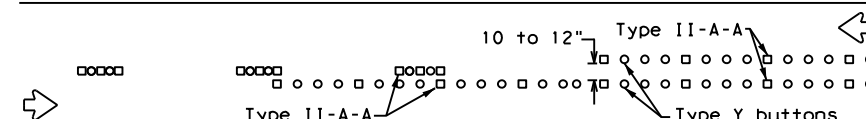


REFLECTORIZED PAVEMENT MARKINGS - PATTERN A

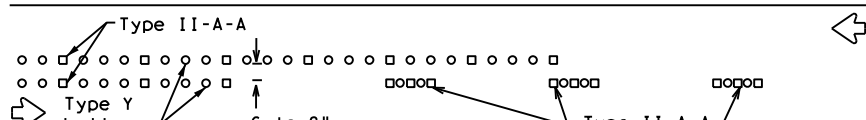


REFLECTORIZED PAVEMENT MARKINGS - PATTERN B

Pattern A is the TXDOT Standard, however Pattern B may be used if approved by the Engineer. Prefabricated markings may be substituted for reflectORIZED pavement markings.



RAISED PAVEMENT MARKERS - PATTERN A



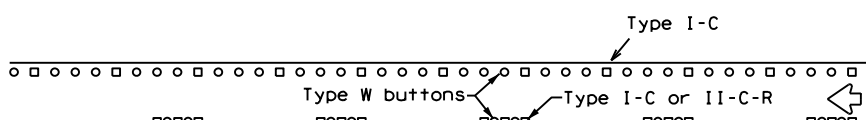
RAISED PAVEMENT MARKERS - PATTERN B

CENTER LINE & NO-PASSING ZONE BARRIER LINES FOR TWO-LANE, TWO-WAY HIGHWAYS



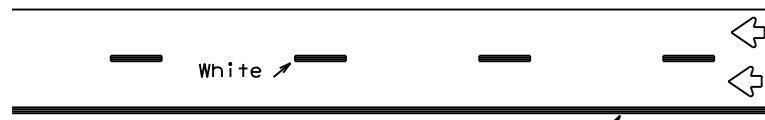
REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectORIZED pavement markings.



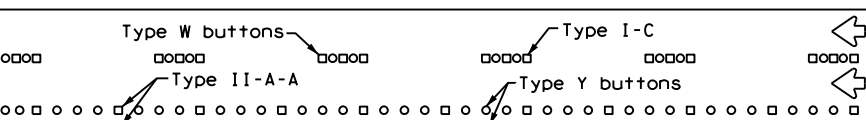
RAISED PAVEMENT MARKERS

EDGE & LANE LINES FOR DIVIDED HIGHWAY



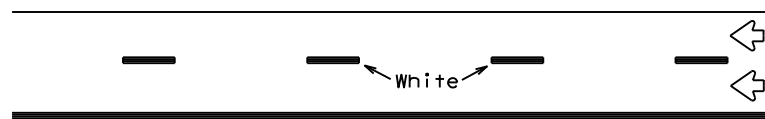
REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectORIZED pavement markings.



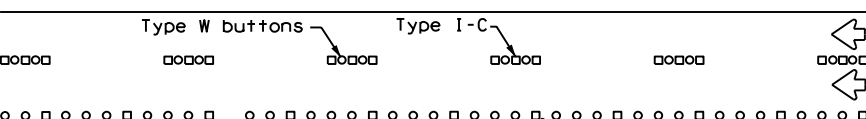
RAISED PAVEMENT MARKERS

LANE & CENTER LINES FOR MULTILANE UNDIVIDED HIGHWAYS



REFLECTORIZED PAVEMENT MARKINGS

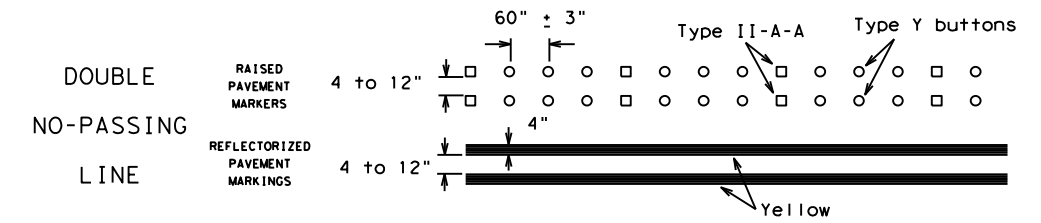
Prefabricated markings may be substituted for reflectORIZED pavement markings.



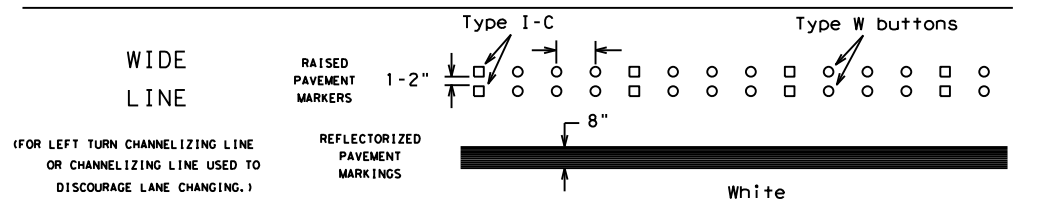
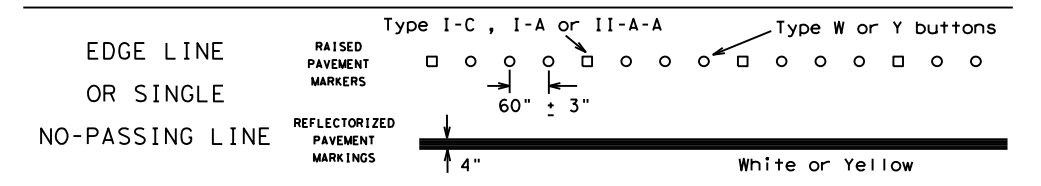
RAISED PAVEMENT MARKERS

TWO-WAY LEFT TURN LANE

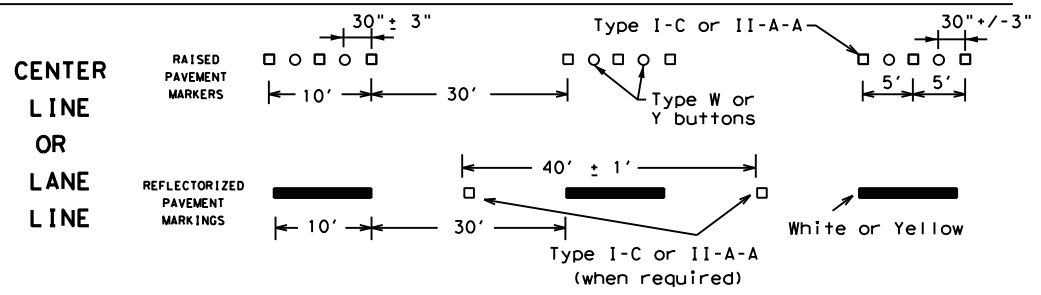
STANDARD WORK ZONE PAVEMENT MARKINGS DETAILS



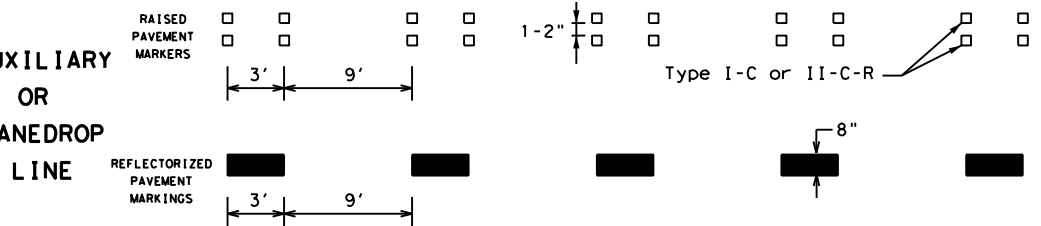
SOLID LINES



BROKEN LINES

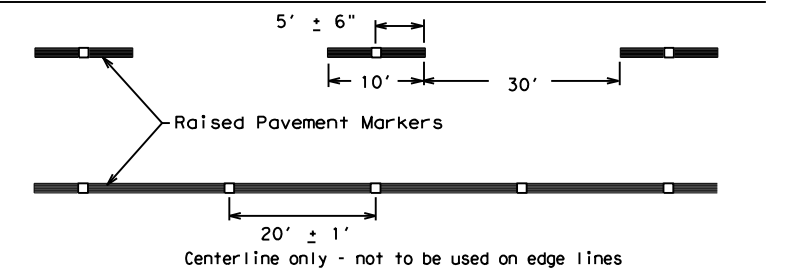


AUXILIARY OR LANEDROP LINE



REMOVABLE MARKINGS WITH RAISED PAVEMENT MARKERS

If raised pavement markers are used to supplement REMOVABLE markings, the markers shall be applied to the top of the tape at the approximate mid length of tape used for broken lines or at 20 foot spacing for solid lines. This allows an easier removal of raised pavement markers and tape.



SHEET 12 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKING PATTERNS

BC(12)-21

Raised pavement markers used as standard pavement markings shall be from the approved products list and meet the requirements of Item 672 "RAISED PAVEMENT MARKERS."

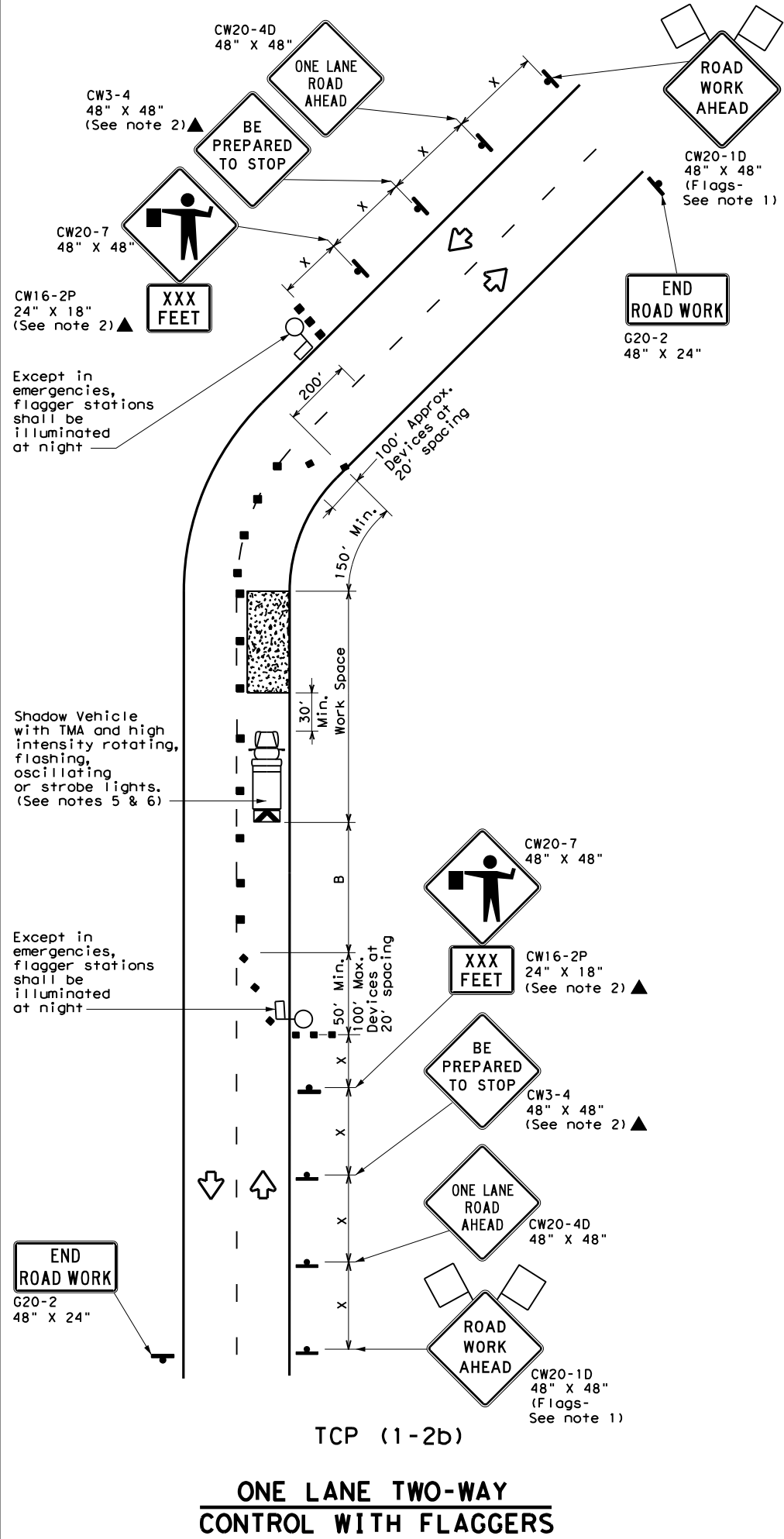
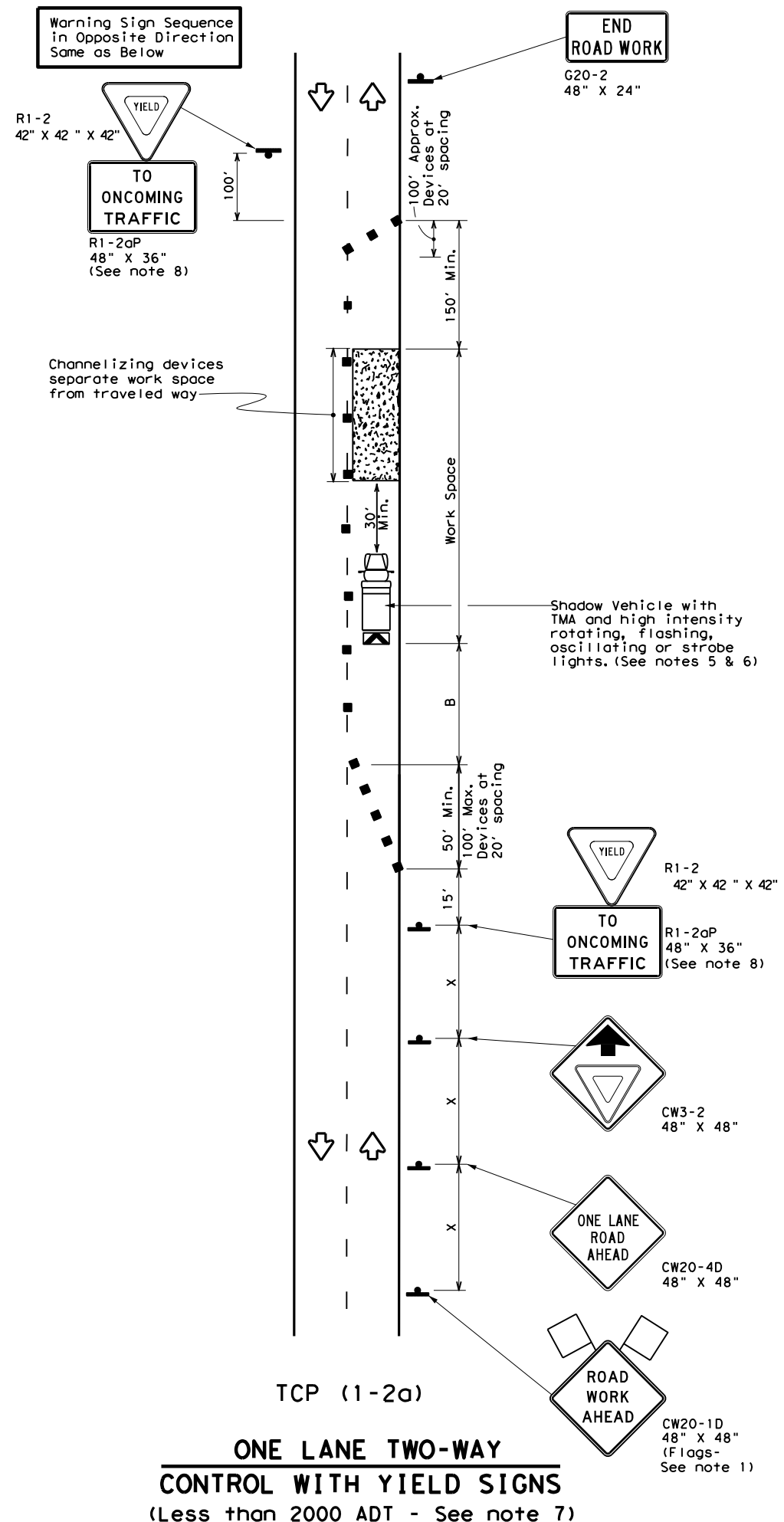
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©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
1-97 9-07 5-21	DIST	COUNTY	SHEET NO.	
2-98 7-13	BWD	COLEMAN CO.	33	
11-02 8-14				

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DATE: 1/11/2023 7:38:12 AM
 FILE: T:\BWD\SGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to off the road for a bridge project.dgn



LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed * X	Formula L = WS ² / 60	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"	Stopping Sight Distance
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent			
30	L = WS ² / 60	150'	165'	180'	30'	60'	120'	90'	200'
35		205'	225'	245'	35'	70'	160'	120'	250'
40		265'	295'	320'	40'	80'	240'	155'	305'
45		450'	495'	540'	45'	90'	320'	195'	360'
50	L = WS	500'	550'	600'	50'	100'	400'	240'	425'
55		550'	605'	660'	55'	110'	500'	295'	495'
60		600'	660'	720'	60'	120'	600'	350'	570'
65		650'	715'	780'	65'	130'	700'	410'	645'
70		700'	770'	840'	70'	140'	800'	475'	730'
75		750'	825'	900'	75'	150'	900'	540'	820'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

GENERAL NOTES

- Flags attached to signs where shown are REQUIRED.
- All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
- The CW3-4 "BE PREPARED TO STOP" sign may be installed after the CW20-4D "ONE LANE ROAD AHEAD" sign, but proper sign spacing shall be maintained.
- Sign spacing may be increased or an additional CW20-1D "ROAD WORK AHEAD" sign may be used if advance warning ahead of the flagger or R1-2 "YIELD" sign is less than 1500 feet.
- A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
- Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.

TC (1-2a)

- R1-2 "YIELD" sign traffic control may be used on projects with approaches that have adequate sight distance. For projects in urban areas, work spaces should be no longer than one half city block. In rural areas on roadways with less than 2000 ADT, work spaces should be no longer than 400 feet.
- R1-2 "YIELD" sign with R1-2aP "TO ONCOMING TRAFFIC" plaque shall be placed on a support at a 7 foot minimum mounting height.

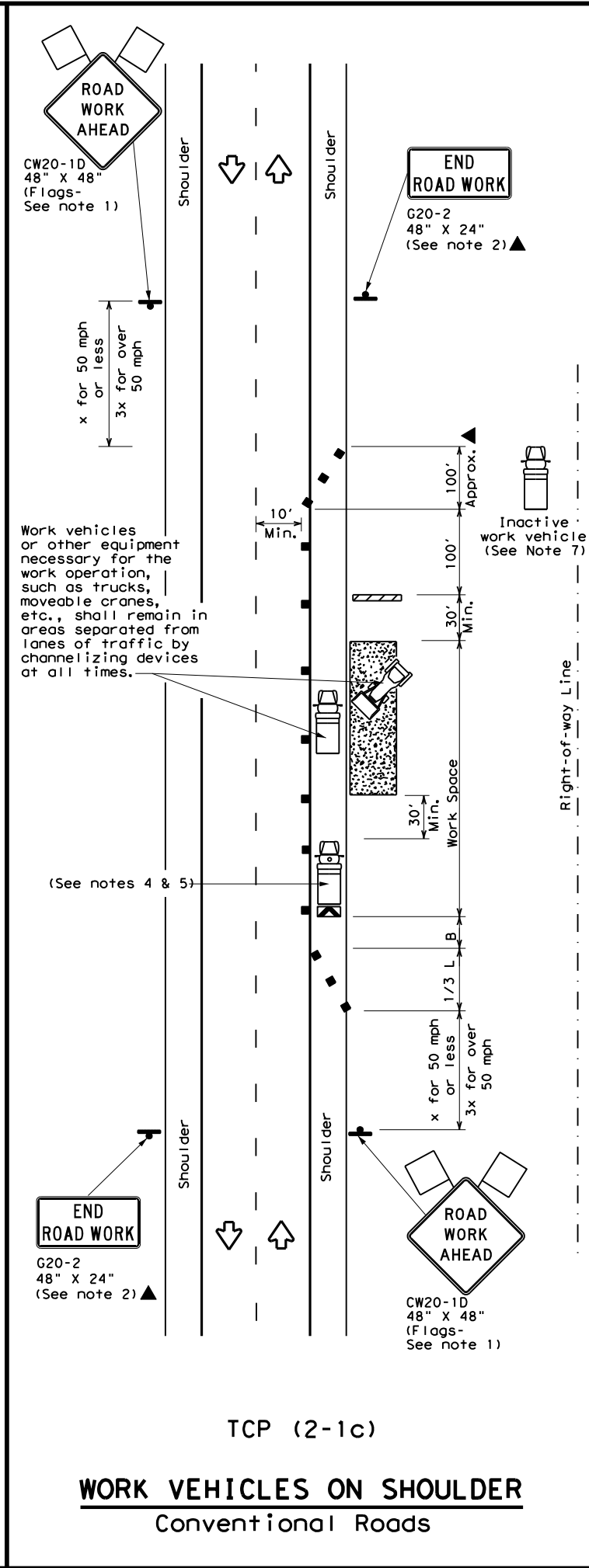
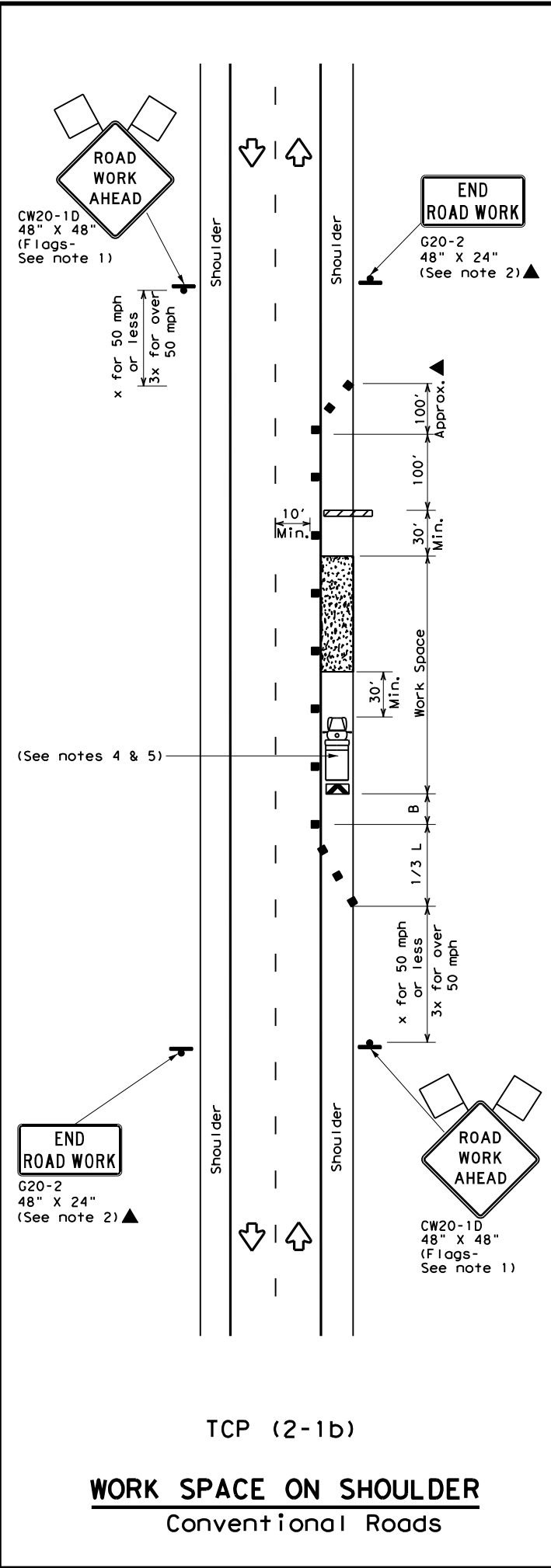
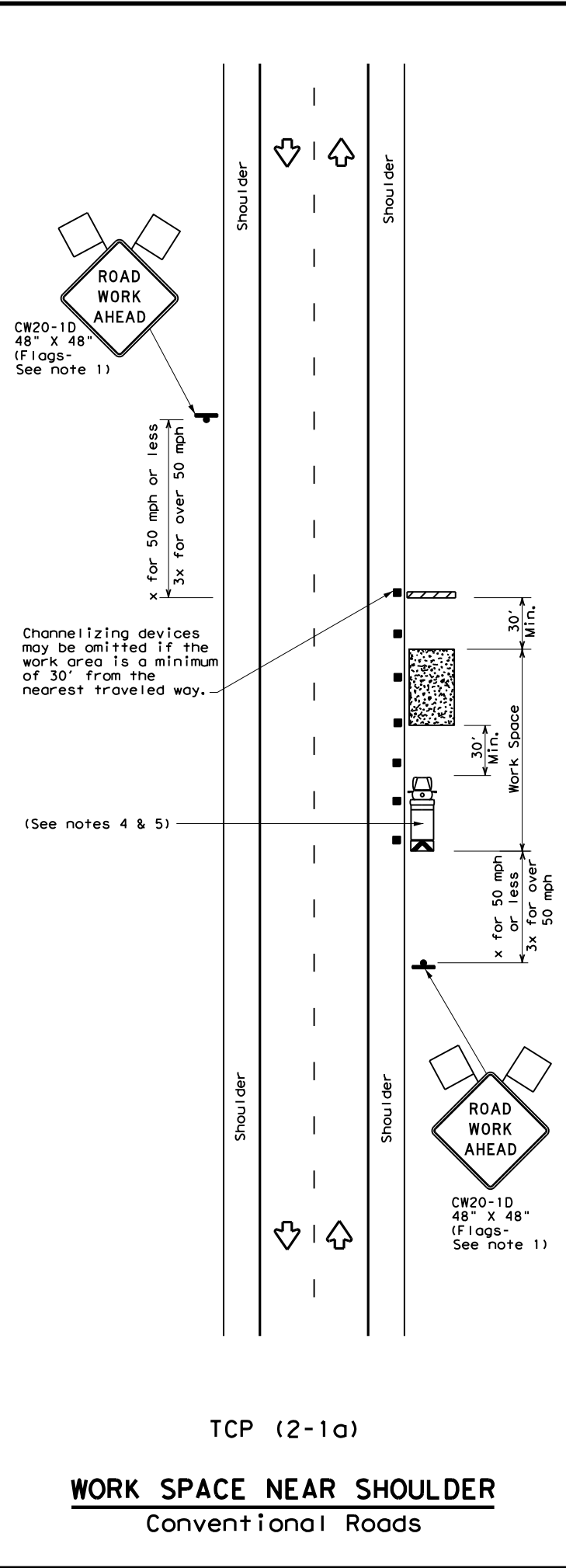
TC (1-2b)

- Flaggers should use two-way radios or other methods of communication to control traffic.
- Length of work space should be based on the ability of flaggers to communicate.
- If the work space is located near a horizontal or vertical curve, the buffer distances should be increased in order to maintain adequate stopping sight distance to the flagger and a queue of stopped vehicles (see table above).
- Channelizing devices on the center-line may be omitted when a pilot car is leading traffic and approved by the Engineer.
- Flaggers should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.

		Traffic Operations Division Standard	
TRAFFIC CONTROL PLAN ONE-LANE TWO-WAY TRAFFIC CONTROL			
TC (1-2) - 18			
FILE: tcp1-2-18.dgn	CON: 0054	CK: 04	DW: 123
© TxDOT December 1985	REVISIONS	REVISIONS	REVISIONS
4-90 4-98	2-94 2-12	1-97 2-18	
DIST: BWD		COUNTY: COLEMAN CO.	SHEET NO.: 34

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DATE: 1/11/2023 7:38:46 AM
 FILE: T:\BWD\SGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to off the shoulder work space



LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	✓

- GENERAL NOTES**
- Flags attached to signs where shown, are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated in the plans, or for routine maintenance work, when approved by the Engineer.
 - Stockpiled material should be placed a minimum of 30 feet from nearest traveled way.
 - Shadow Vehicle with TMA and high intensity rotating, flashing, oscillating or strobe lights. A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
 - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.
 - See TCP(5-1) for shoulder work on divided highways, expressways and freeways.
 - Additional work vehicles or other equipment should be parked near the right-of-way line and not parked on the paved shoulder.
 - CW21-5 "SHOULDER WORK" signs may be used in place of CW20-1D "ROAD WORK AHEAD" signs for shoulder work on conventional roadways.

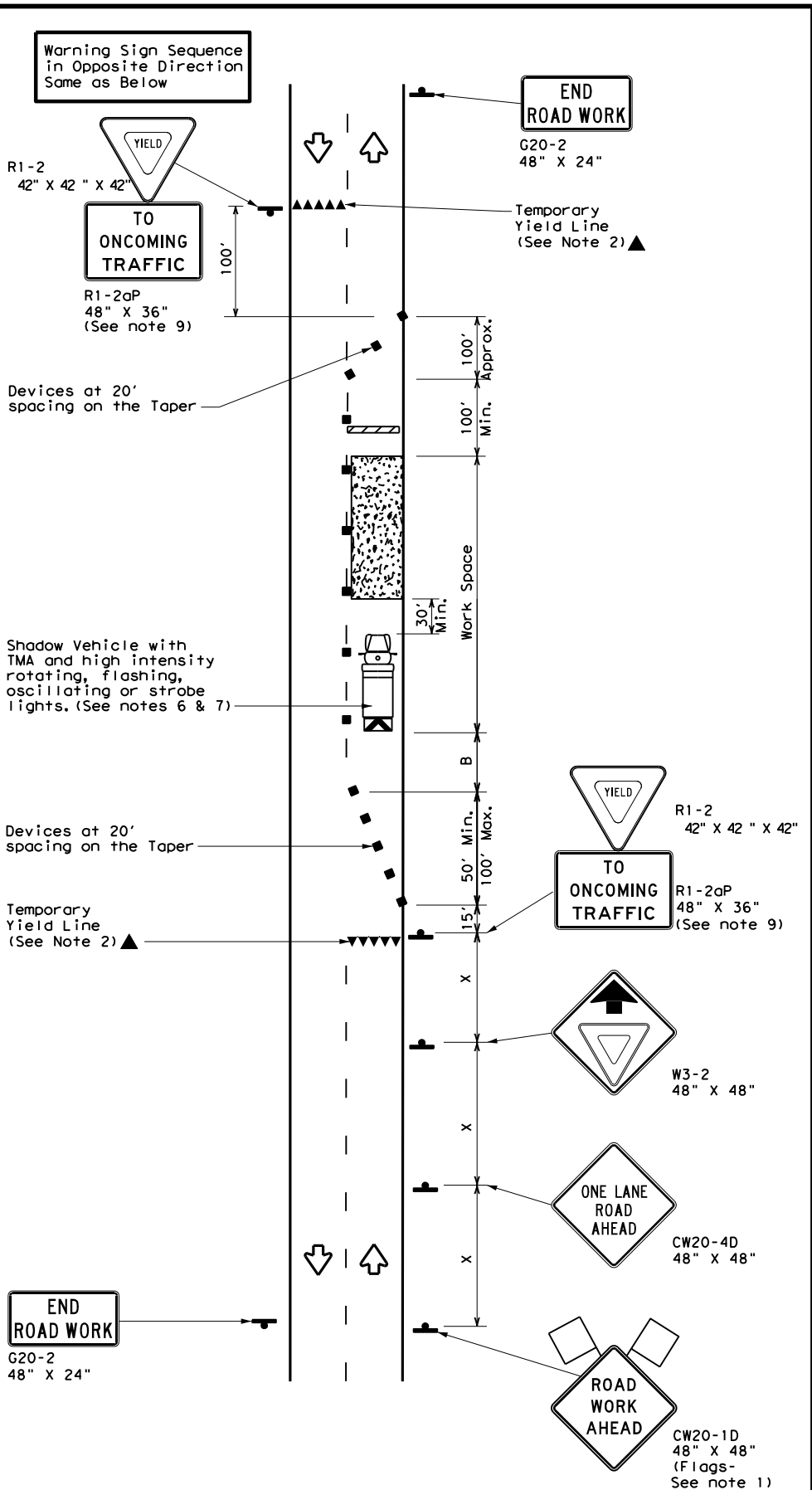
Texas Department of Transportation
 Traffic Operations Division Standard

TRAFFIC CONTROL PLAN
CONVENTIONAL ROAD
SHOULDER WORK

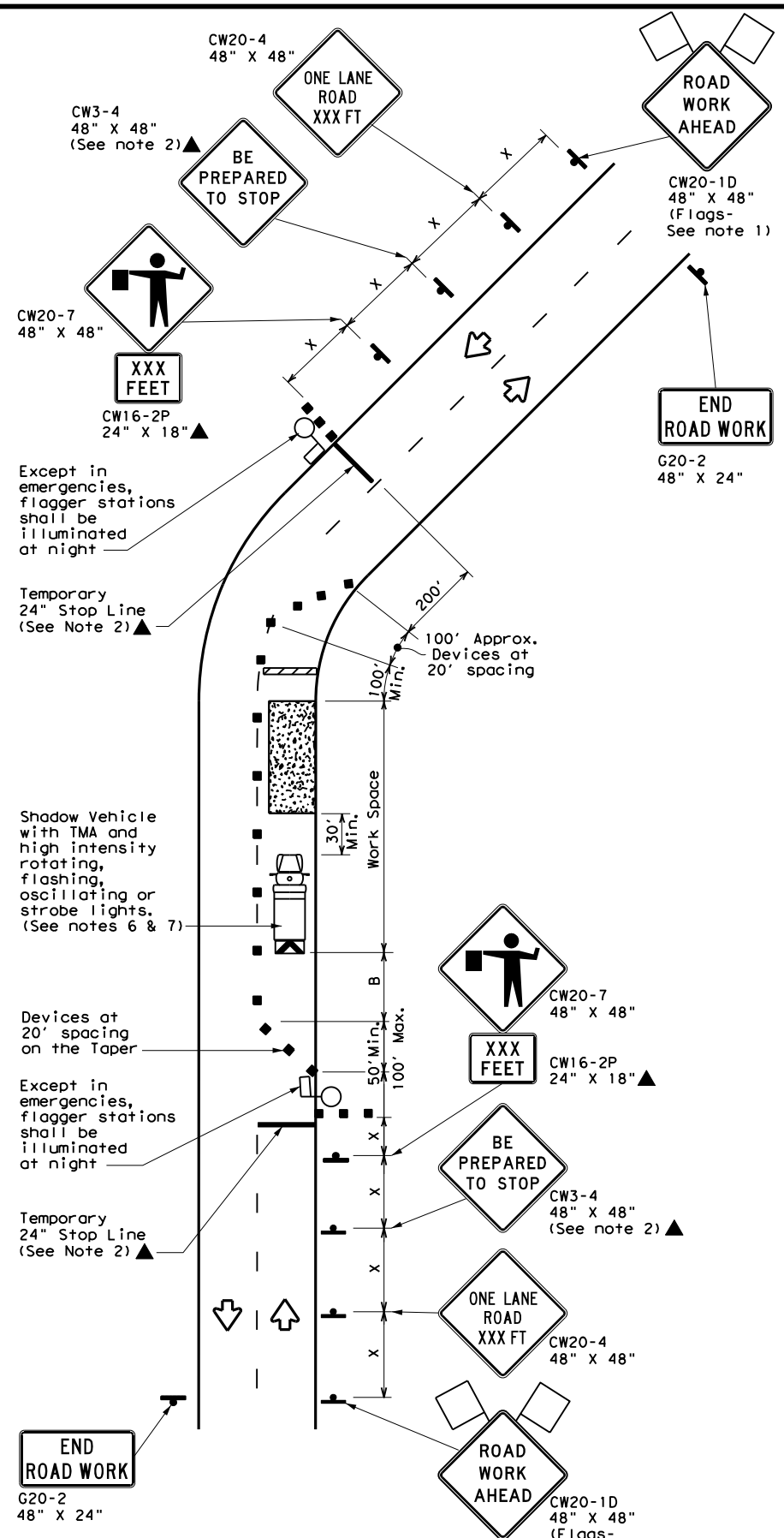
TCP (2-1) - 18

FILE: tcp2-1-18.dgn	DN:	CK:	DW:	CK:
© TxDOT December 1985	CON:	SECT:	JOB:	HIGHWAY:
REVISIONS	0054	04	123	US 84
2-94 4-98	DIST:	COUNTY:	SHEET NO.:	
8-95 2-12	BWD	COLEMAN CO.	35	
1-97 2-18				

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TCP (2-2a)
 2-LANE ROADWAY WITHOUT PAVED SHOULDERS
 ONE LANE TWO-WAY
 CONTROL WITH YIELD SIGNS
 (Less than 2000 ADT - See Note 9)



TCP (2-2b)
 2-LANE ROADWAY WITHOUT PAVED SHOULDERS
 ONE LANE TWO-WAY
 CONTROL WITH FLAGGERS

LEGEND

	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"	Stopping Sight Distance
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent			
30	L = WS ² / 60	150'	165'	180'	30'	60'	120'	90'	200'
35		205'	225'	245'	35'	70'	160'	120'	250'
40		265'	295'	320'	40'	80'	240'	155'	305'
45	L = WS	450'	495'	540'	45'	90'	320'	195'	360'
50		500'	550'	600'	50'	100'	400'	240'	425'
55		550'	605'	660'	55'	110'	500'	295'	495'
60		600'	660'	720'	60'	120'	600'	350'	570'
65		650'	715'	780'	65'	130'	700'	410'	645'
70		700'	770'	840'	70'	140'	800'	475'	730'
75		750'	825'	900'	75'	150'	900'	540'	820'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE

MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	

GENERAL NOTES

- Flags attached to signs where shown, are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
 - The CW3-4 "BE PREPARED TO STOP" sign may be installed after the CW20-4 "ONE LANE ROAD XXX FT" sign, but proper sign spacing shall be maintained.
 - Flaggers should use two-way radios or other methods of communication to control traffic.
 - Length of work space should be based on the ability of flaggers to communicate.
 - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
 - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.
- TCP (2-2a)**
- The R1-2 "YIELD" sign traffic control may be used on projects with approaches that have adequate sight distance. For projects in urban areas, work space should be no longer than one half city block. In rural areas, roadways with less than 2000 ADT, work space should be no longer than 400 feet.
 - The R1-2aP "YIELD TO ONCOMING TRAFFIC" sign shall be placed on a support at a 7 foot minimum mounting height.
- TCP (2-2b)**
- Channelizing devices on the center line may be omitted when a pilot car is leading traffic and approved by the Engineer.
 - If the work space is located near a horizontal or vertical curve, the buffer distances should be increased in order to maintain stopping sight distance to the flagger and a queue of stopped vehicles. (See table above).
 - Flaggers should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.

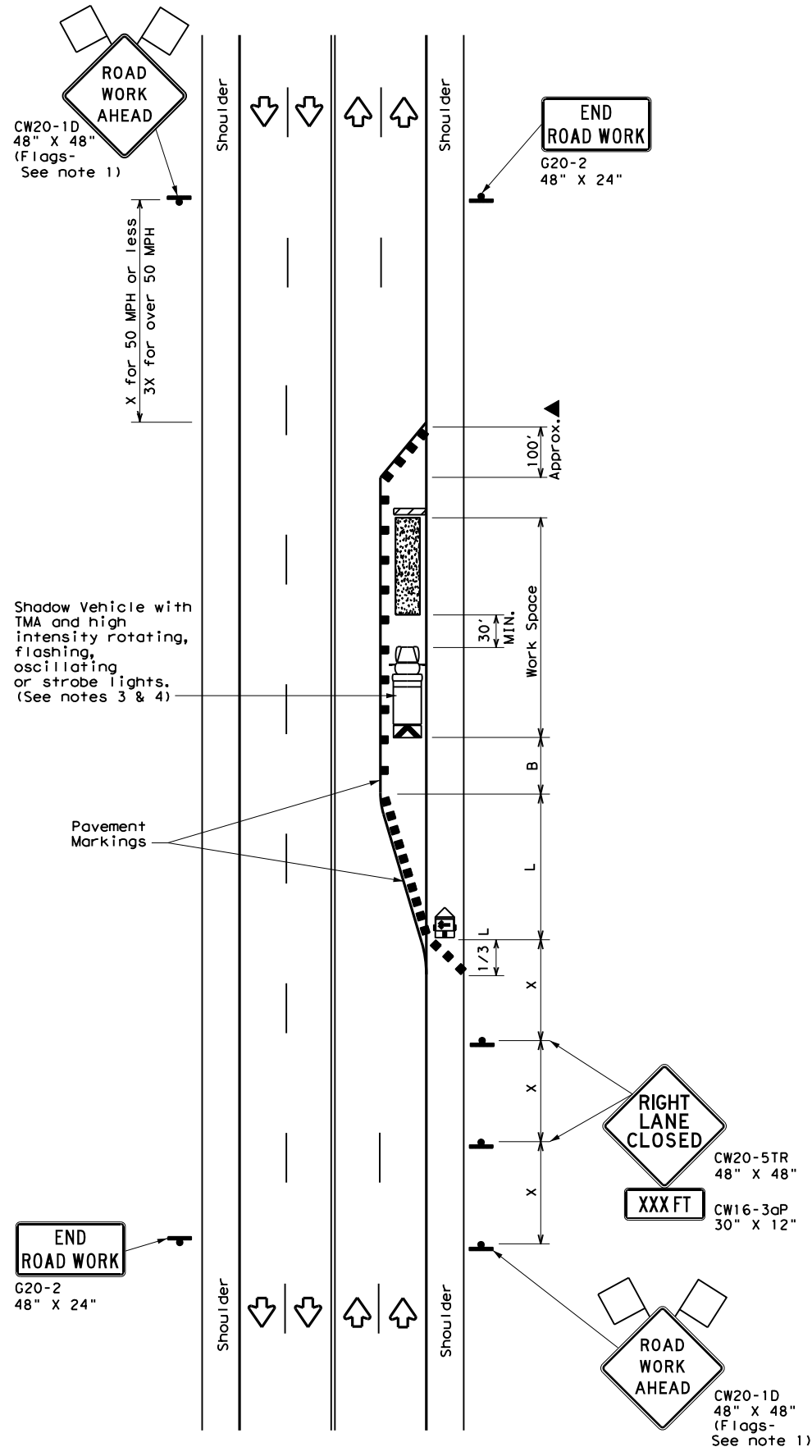
Texas Department of Transportation
 Traffic Operations Division Standard

**TRAFFIC CONTROL PLAN
 ONE-LANE TWO-WAY
 TRAFFIC CONTROL**

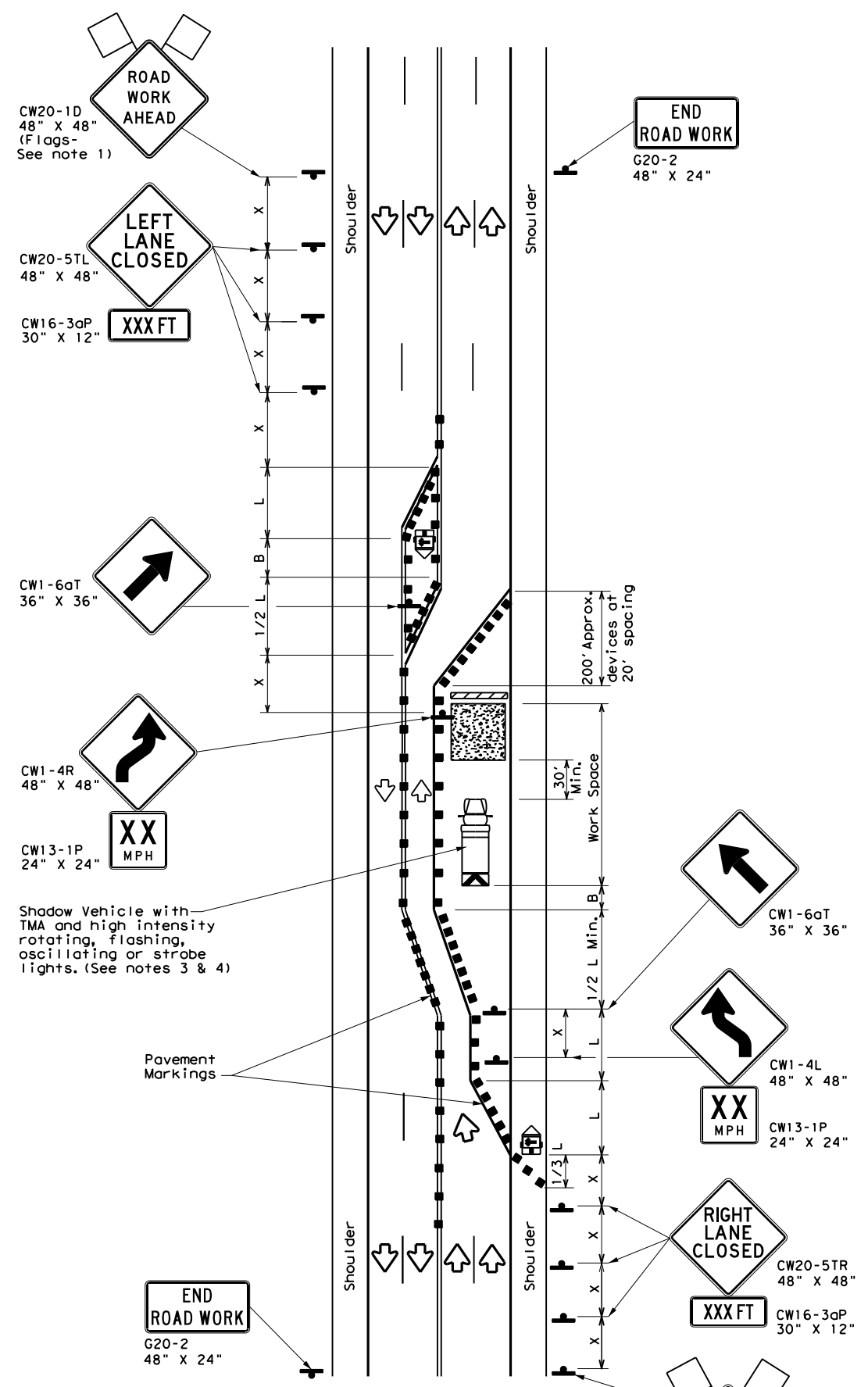
TCP (2-2) - 18

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© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
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8-95 3-03	DIST	COUNTY	SHEET NO.	
1-97 2-12	BWD	COLEMAN CO.	36	
4-98 2-18				

DATE: 1/11/2023 7:39:53 AM
 FILE: T:\BWDSD\TEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CFR the use of a Shadow Vehicle with TMA and high intensity rotating, flashing, oscillating or strobe lights.
 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information provided.



TCP (2-5a)
ONE LANE CLOSED



TCP (2-5b)
TWO LANES CLOSED

LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths X X			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	L = WS ² / 60	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
			✓	✓

- GENERAL NOTES**
- Flags attached to signs where shown, are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
 - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
 - Additional Shadow Vehicles with TMAs may be positioned in each closed lane, on the shoulder or off the paved surface, next to those shown in order to protect a wider work space.
 - The downstream taper is optional. When used, it should be 100 feet approximately per lane, with channelizing devices spaced at 20 feet.
- TCP (2-5a)**
- If this TCP is used for a left lane closure, CW20-5TL "LEFT LANE CLOSED" signs shall be used and channelizing devices shall be placed on the centerline to protect the work space from opposing traffic, with the arrow board placed in the closed lane near the end of the merging taper.
- TCP (2-5b)**
- Conflicting pavement markings shall be removed for long-term projects.

Traffic Operations Division Standard

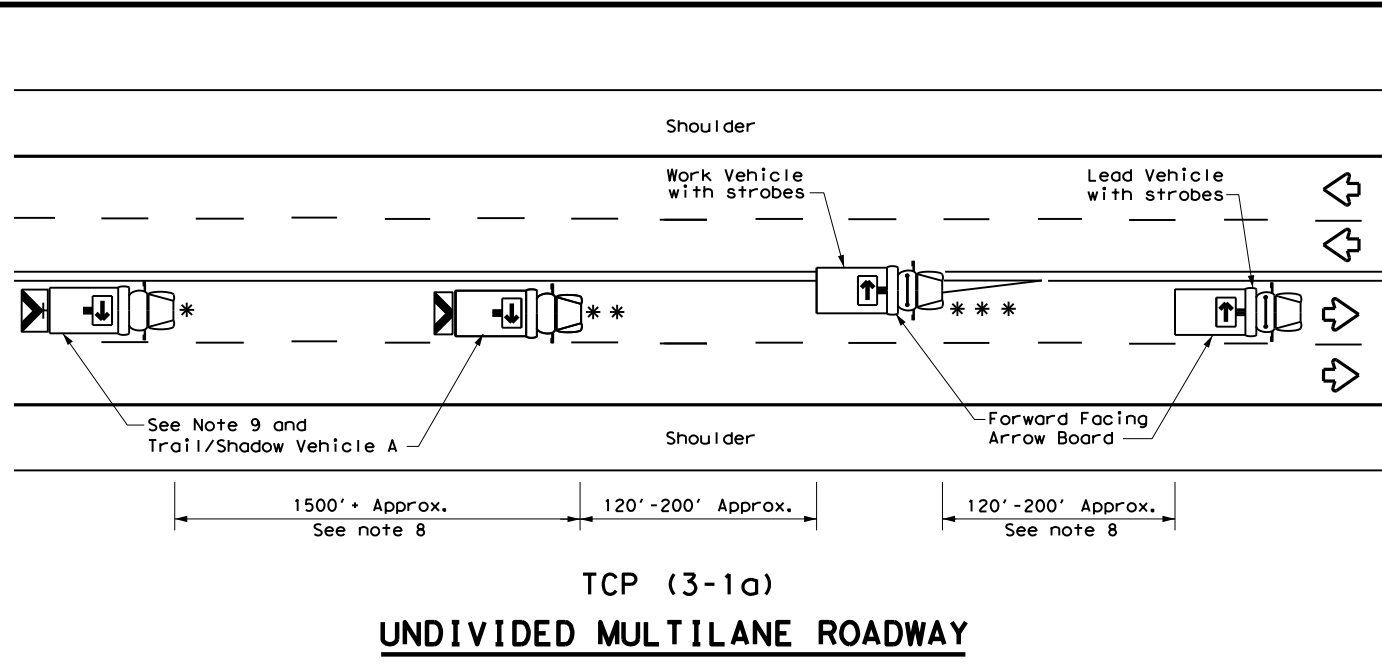
**TRAFFIC CONTROL PLAN
 LONG TERM LANE CLOSURES
 MULTILANE CONVENTIONAL RDS.**

TCP (2-5) - 18

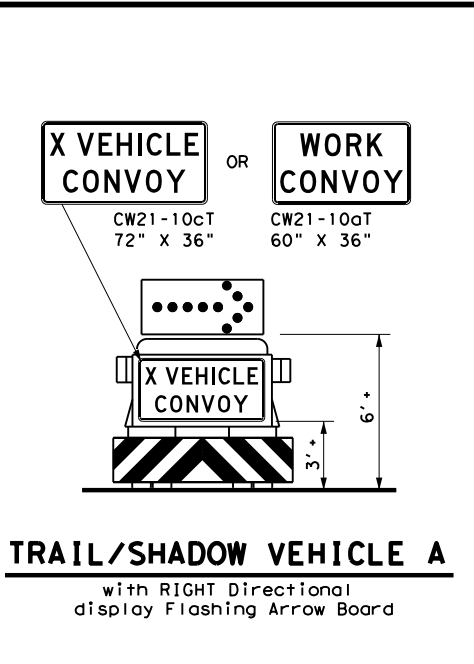
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© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
8-95 2-12 REVISIONS	0054	04	123	US 84
1-97 3-03	DIST	COUNTY	SHEET NO.	
4-98 2-18	BWD	COLEMAN CO.	37	

165

DATE: 1/11/2023 7:40:24 AM
 FILE: T:\BWDSDTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to off the road for a period of 30 days
 The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information provided.



TCP (3-1a)
UNDIVIDED MULTILANE ROADWAY



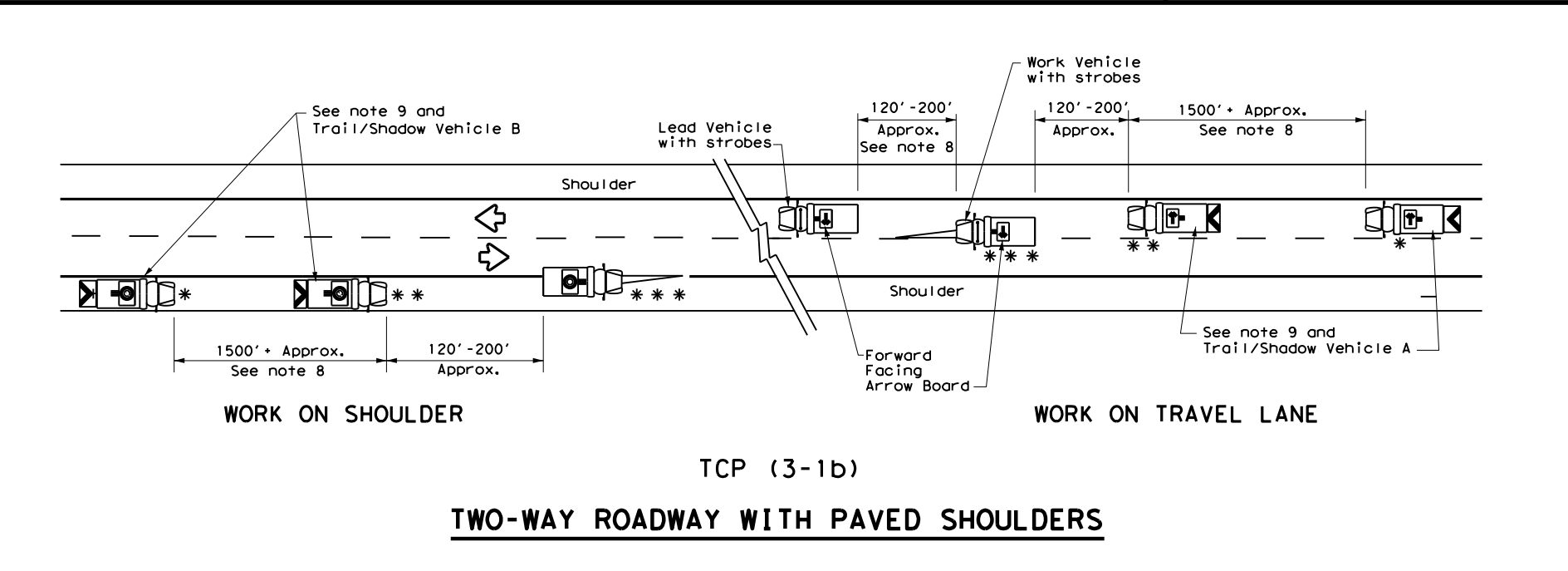
TRAIL/SHADOW VEHICLE A
with RIGHT Directional display Flashing Arrow Board

LEGEND			
*	Trail Vehicle	ARROW BOARD DISPLAY	
**	Shadow Vehicle		
***	Work Vehicle		RIGHT Directional
	Heavy Work Vehicle		LEFT Directional
	Truck Mounted Attenuator (TMA)		Double Arrow
	Traffic Flow		CAUTION (Alternating Diamond or 4 Corner Flash)

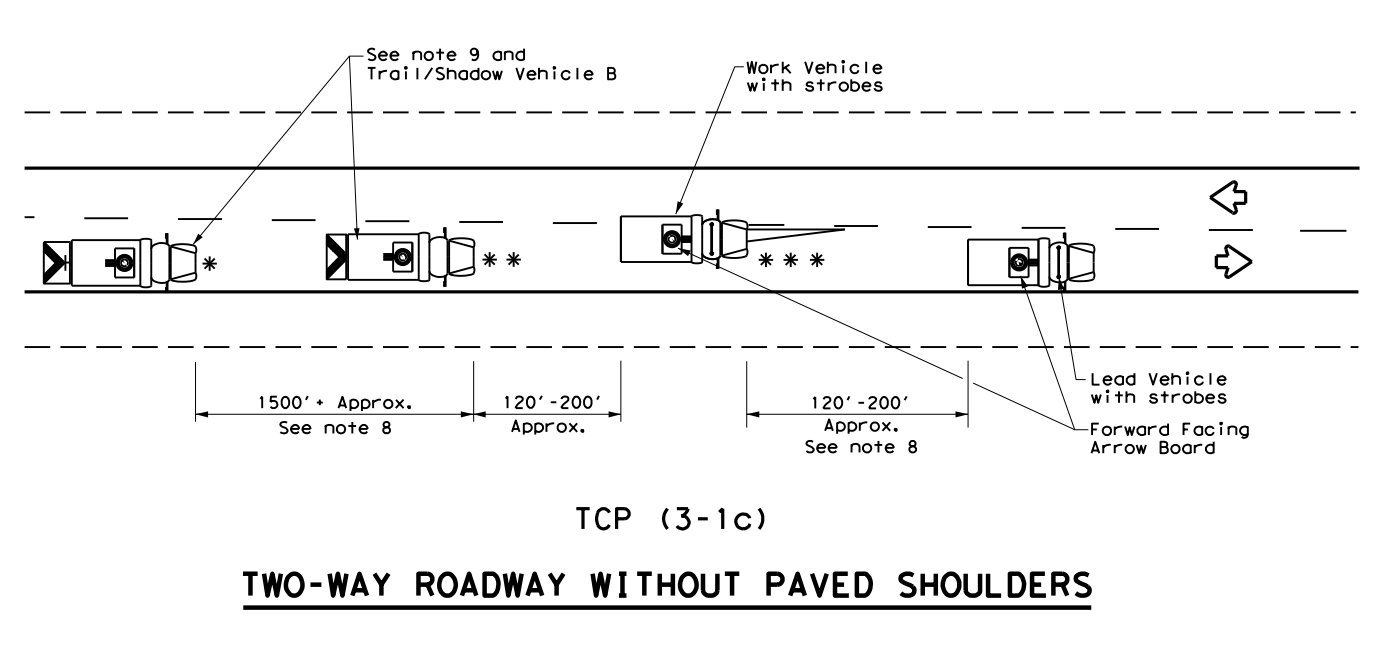
TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
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GENERAL NOTES

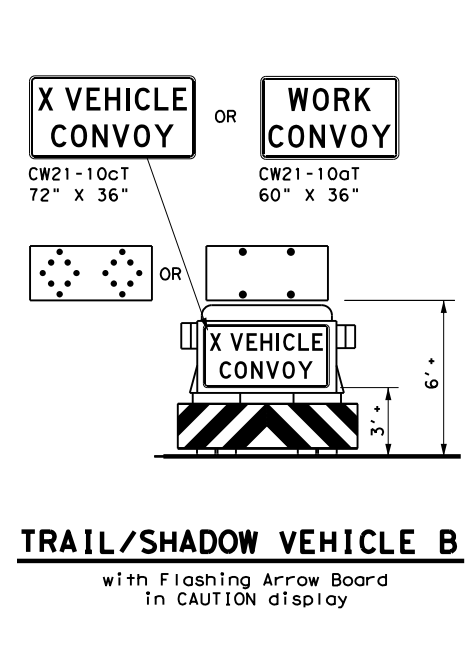
1. TRAIL, SHADOW, and LEAD vehicles shall be equipped with arrow boards as illustrated. When a LEAD vehicle is not used the WORK vehicle must be equipped with an arrow board. The Engineer will determine if the LEAD VEHICLE and/or TRAIL VEHICLE are required based on prevailing roadway conditions, traffic volume, and sight distance restrictions.
2. The use of amber high intensity rotating, flashing, oscillating, or strobe lights on vehicles are required. Blue high intensity rotating, flashing, oscillating or strobe lights when mounted on the driver's side of the vehicle may be operated simultaneously with the amber beacons or strobe lights.
3. The use of truck mounted attenuators (TMA) on the SHADOW VEHICLE and TRAIL VEHICLE are required.
4. Reflective sheeting on the rear of the TMA shall meet or exceed the reflectivity and color requirements of DEPARTMENTAL MATERIAL SPECIFICATION DMS 8300, Type A.
5. Flashing arrow boards shall be Type B or Type C as per the Barricade and Construction (BC) standards. The board shall be controlled from inside the vehicle.
6. Each vehicle shall have two-way radio communication capability.
7. When work convoys must change lanes, the TRAIL VEHICLE should change lanes first to shadow the other convoy vehicles.
8. Vehicle spacing between the TRAIL VEHICLE and the SHADOW VEHICLE will vary depending on sight distance restrictions. Motorists approaching the work convoy should be able to see the TRAIL VEHICLE in time to slow down and/or change lanes as they approach the TRAIL VEHICLE. Vehicle spacing between the WORK VEHICLE and SHADOW VEHICLE and vehicle spacing between WORK VEHICLE and LEAD VEHICLE may vary according to terrain, work activity and other factors.
9. "X VEHICLE CONVOY" (CW21-10cT) or "WORK CONVOY" (CW21-10aT) signs shall be used on TRAIL VEHICLES and SHADOW VEHICLES as shown. As an option 48" X 48" diamond shaped "WORK CONVOY" (CW21-10T) or "X VEHICLE CONVOY" (CW21-10bT) signs may be used where adequate mounting space exists. When used, the X VEHICLE CONVOY sign shall have the number of the convoy vehicles displayed on the sign in the number designation "X" location. The "X VEHICLE CONVOY" sign shall not be used on the SHADOW VEHICLE if a TRAIL VEHICLE is used.
10. On two-lane two-way roadways, the work and protection vehicles should pull over periodically to allow motor vehicle traffic to pass. If motorists are not allowed to pass the work convoy, a "DO NOT PASS" (R4-1) sign should be placed on the back of the rearmost protection vehicle.



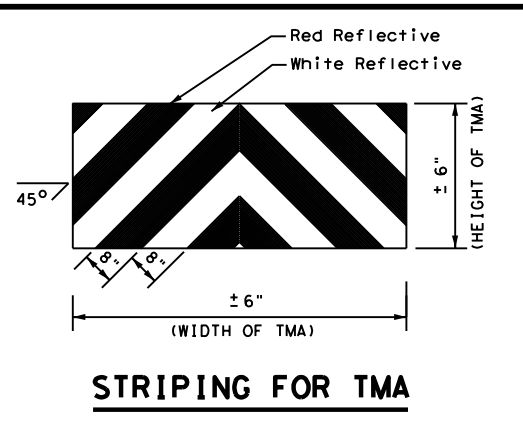
TCP (3-1b)
TWO-WAY ROADWAY WITH PAVED SHOULDERS



TCP (3-1c)
TWO-WAY ROADWAY WITHOUT PAVED SHOULDERS



TRAIL/SHADOW VEHICLE B
with Flashing Arrow Board in CAUTION display



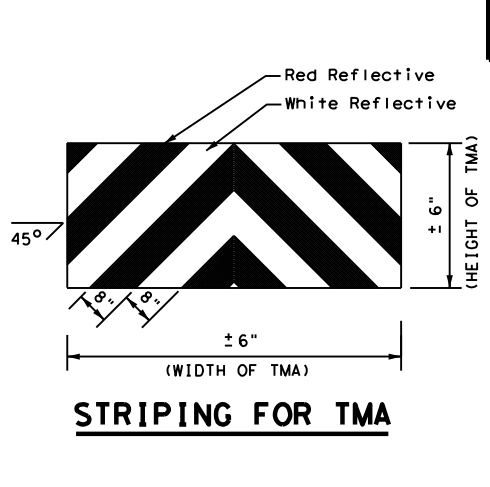
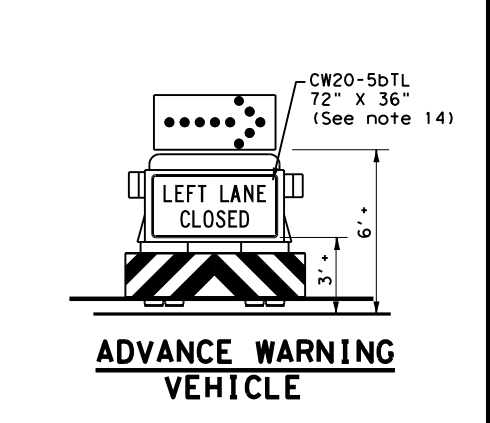
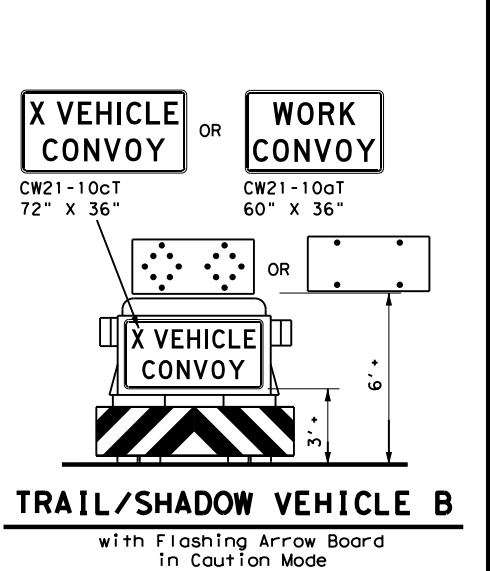
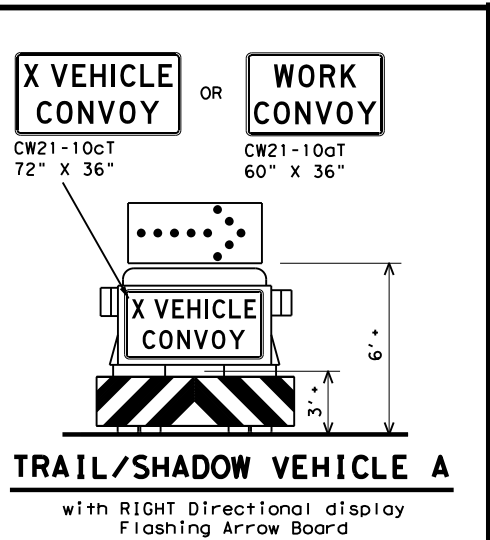
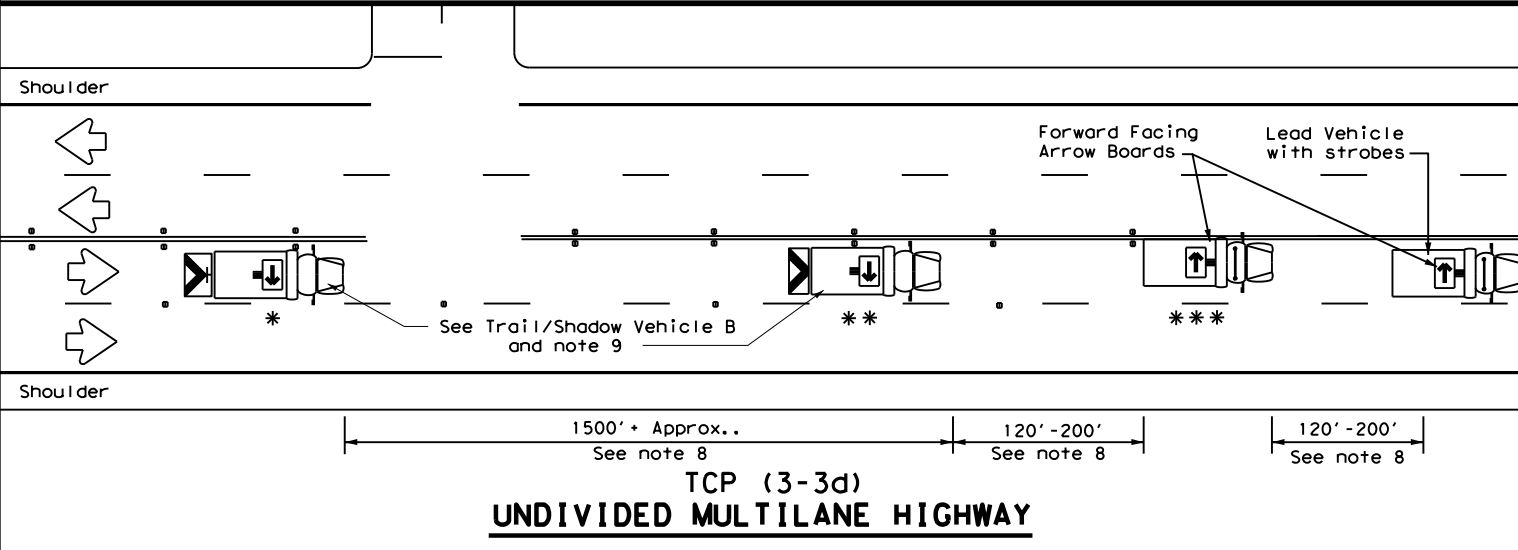
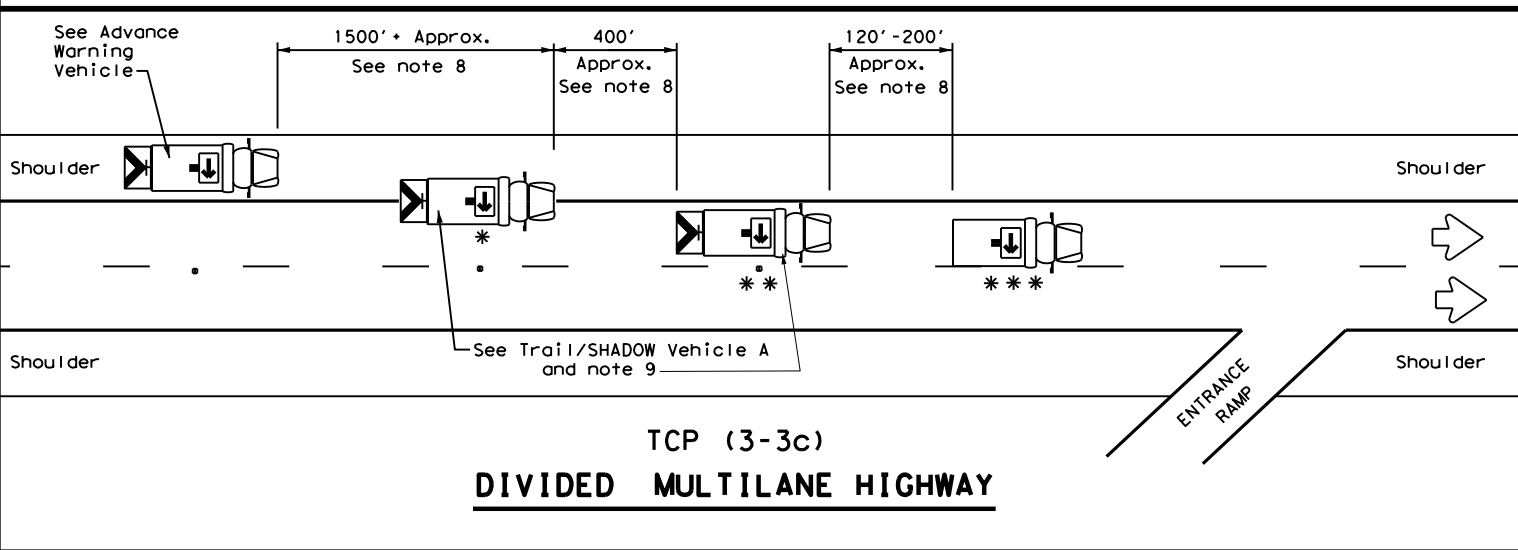
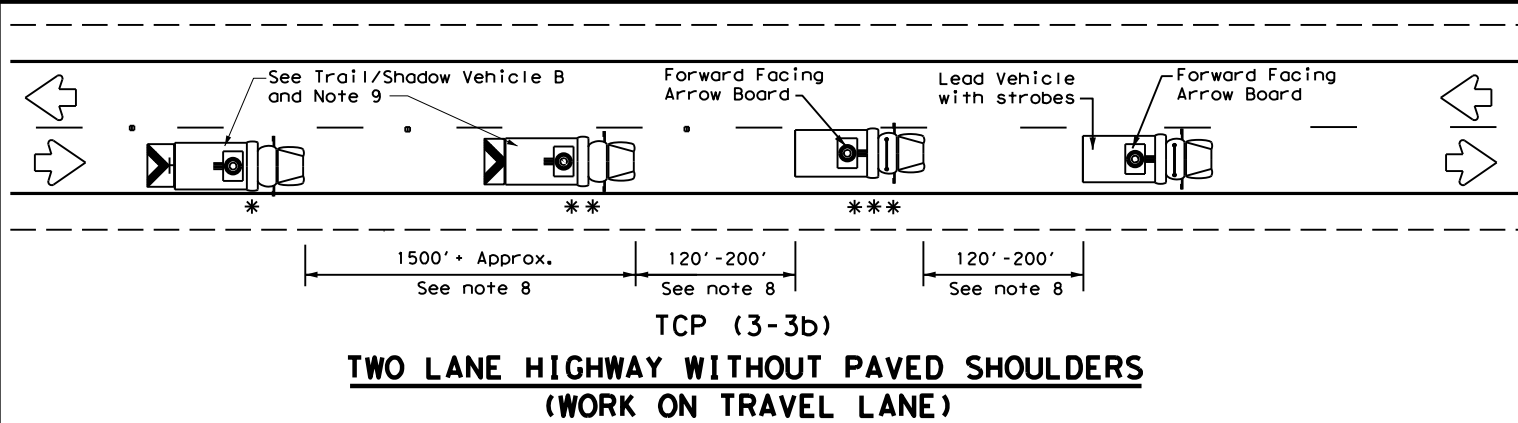
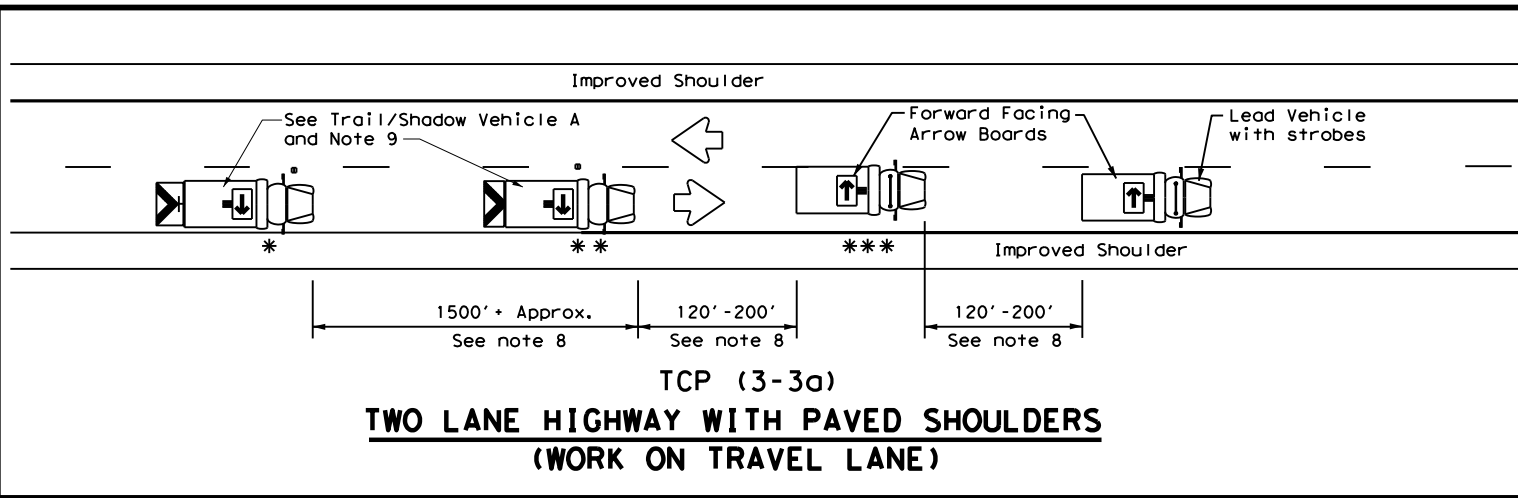
Traffic Operations Division Standard

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
UNDIVIDED HIGHWAYS

TCP(3-1)-13

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© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
2-94 4-98	DIST	COUNTY	SHEET NO.	
8-95 7-13	BWD	COLEMAN CO.	38	
1-97				

DATE: 1/11/2023 7:41:12 AM
 FILE: T:\BWDSDTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to cfr the use of raised pavement markers for the conversion of a two-lane road to a four-lane road
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LEGEND		
* Trail Vehicle	ARROW BOARD DISPLAY	
** Shadow Vehicle		
*** Work Vehicle		RIGHT Directional
		LEFT Directional
		Double Arrow
		CAUTION (Alternating Diamond or 4 Corner Flash)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
✓				

GENERAL NOTES

- TRAIL, SHADOW, and LEAD vehicles shall be equipped with arrow boards as illustrated. When a LEAD vehicle is not used on two way roads the WORK vehicle must have an arrow board. For divided roadways, the arrow board on the WORK vehicle is optional based on the type of work being performed. The Engineer will determine if the LEAD vehicle and/or TRAIL vehicle are required based on prevailing roadway conditions, traffic volume, and sight distance restrictions.
- The use of amber high intensity rotating, flashing, oscillating, or strobe lights on vehicles are required. Blue high intensity rotating, flashing, oscillating, or strobe lights when mounted on the driver's side of the vehicle may be operated simultaneously with the amber beacons or strobe lights.
- The use of truck mounted attenuators (TMA) on the SHADOW VEHICLE, ADVANCE WARNING and TRAIL VEHICLE are required.
- Reflective sheeting on the rear of the TMA shall meet or exceed the reflectivity and color requirements of DEPARTMENTAL MATERIAL SPECIFICATION DMS 8300, Type A.
- Flashing arrow boards shall be Type B or Type C as per the Barricade and Construction (BC) standards. The board shall be controlled from inside the vehicle.
- Each vehicle shall have two-way radio communication capability.
- When work convoys must change lanes, the TRAIL VEHICLE should change lanes first to shadow the other convoy vehicles.
- Vehicle spacing between the TRAIL VEHICLE and the SHADOW VEHICLE will vary depending on sight distance restrictions. Motorists approaching the convoy should be able to see the TRAIL VEHICLE in time to slow down and/or change lanes as they approach the TRAIL VEHICLE. Vehicle spacing between the WORK VEHICLE and SHADOW VEHICLE and vehicle spacing between WORK VEHICLE and LEAD VEHICLE may vary according to terrain, work activity and other factors.
- X VEHICLE CONVOY (CW21-10cT) or WORK CONVOY (CW21-10aT) signs shall be used on TRAIL VEHICLES and SHADOW VEHICLES as shown. As an option 48" x 48" diamond shaped WORK CONVOY (CW21-10T) or X VEHICLE CONVOY (CW21-10bT) signs may be used where adequate mounting space exists. When used, the X VEHICLE CONVOY sign shall have the number of the convoy vehicles displayed on the sign in the number designation "X" location. The X VEHICLE CONVOY sign shall not be used on the SHADOW VEHICLE if a TRAIL VEHICLE is used.
- For divided highways with two or three lanes in one direction, the appropriate LEFT LANE CLOSED (CW20-5bTL), RIGHT LANE CLOSED (CW20-5bTR), or CENTER LANE CLOSED (CW20-5dT) sign should be used on the Advance Warning Vehicle. As an option, a portable changeable message sign (PCMS) or truck mounted changeable message sign (TMCMS) with a minimum character height of 12", and displaying the same legend may be substituted for these signs. An appropriate directional arrow display, simulating the size and legibility of the flashing arrow board may be used in the second phase of the PCMS/TMCMS message. When this is done, the arrow board will not be required on the Advance Warning Vehicle.
- A double arrow shall not be displayed on the arrow board on the Advance Warning Vehicle.
- For divided highways with three or four lanes in each direction, use TCP(3-2).
- Standard diamond shape versions of the CW20-5 series signs may be used as an option if the rectangular signs shown are not available.
- The Advance Warning Vehicle may straddle the edgeline when Shoulder width makes it necessary.
- On two-lane two-way roadways, the work and protection vehicles should pull over periodically to allow motor vehicle traffic to pass. If motorists are not allowed to pass the work convoy, a DO NOT PASS (R4-1) sign should be placed on the back of the rearmost protection vehicle.

Texas Department of Transportation

Traffic Operations Division Standard

TRAFFIC CONTROL PLAN

MOBILE OPERATIONS

RAISED PAVEMENT

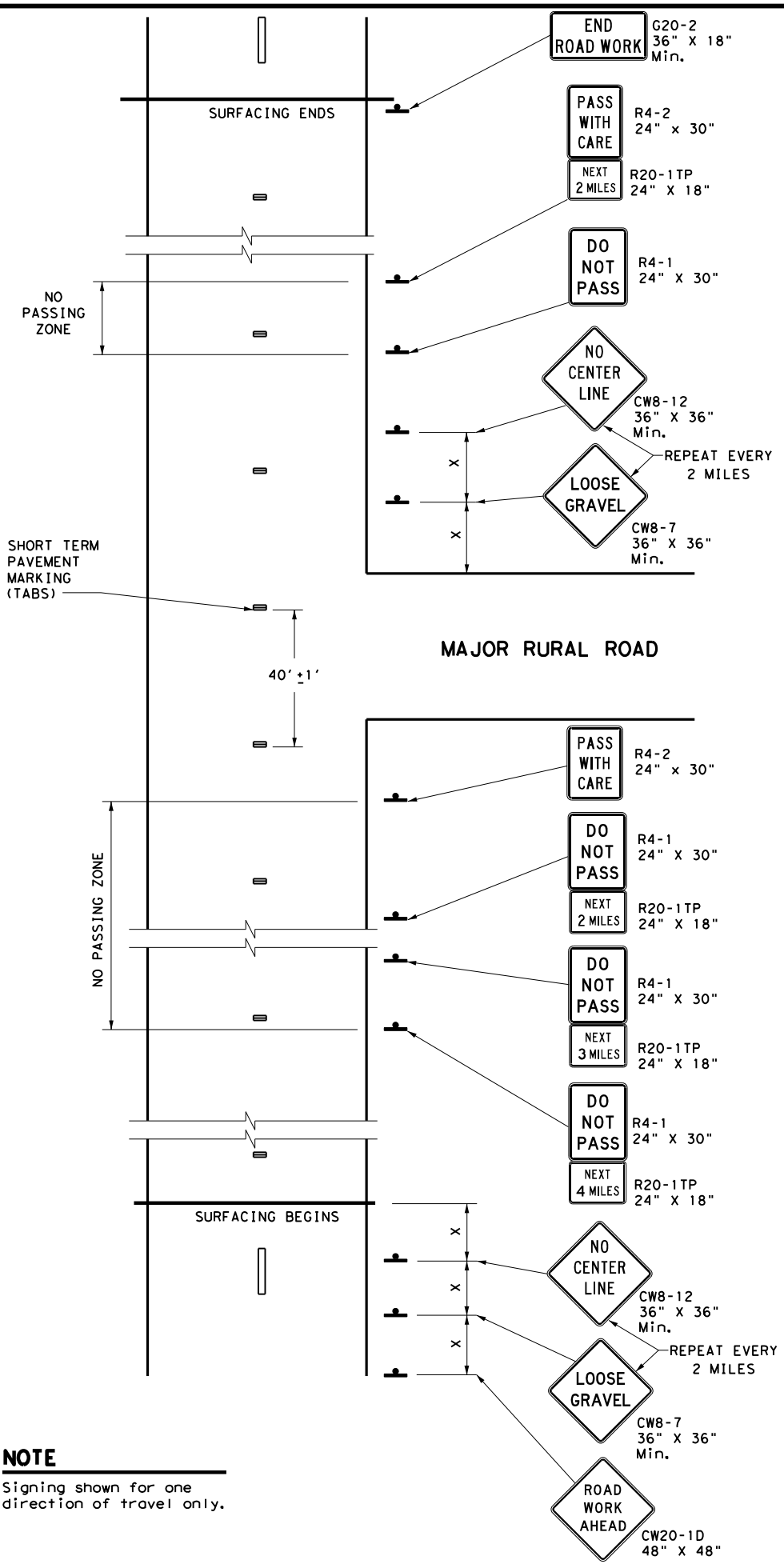
MARKER INSTALLATION/REMOVAL

TCP (3-3) - 14

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© TxDOT September 1987	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
2-94 4-98	DIST	COUNTY	SHEET NO.	
8-95 7-13	BWD	COLEMAN CO.	39	
1-97 7-14				

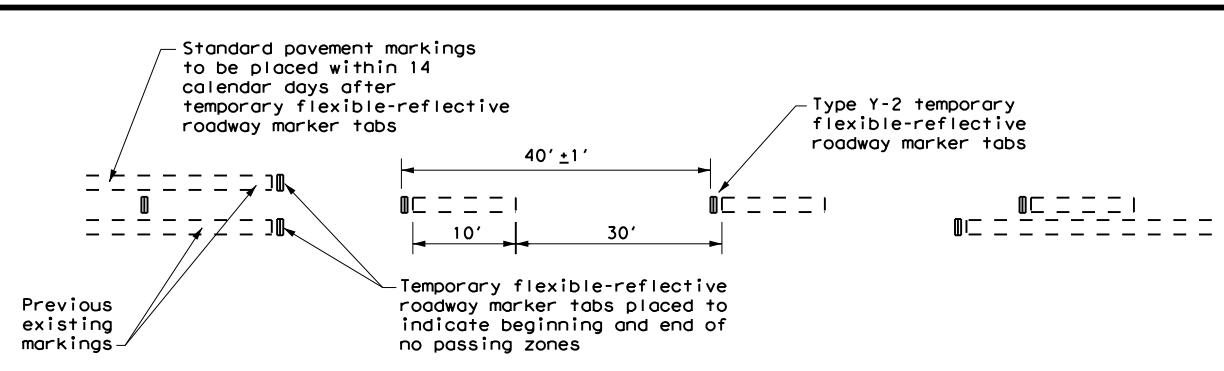
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NOTE
 Signing shown for one direction of travel only.

NO PASSING ZONES ON TWO-LANE TWO-WAY ROADS



TABS ON CENTERLINES OF TWO-LANE TWO-WAY ROADS
 For seal coat, micro-surface or similar operations

"DO NOT PASS" SIGN (R4-1) and NO-PASSING ZONES

- A. Prior to the beginning of construction, all currently striped no-passing zones shall be signed with the DO NOT PASS (R4-1) signs and PASS WITH CARE (R4-2) signs placed at the beginning and end of each zone for each direction of travel except as otherwise provided herein. Signs marking these individual no-passing zones need not be covered prior to construction if the signs supplement the existing pavement markings.
- B. At the discretion of the Engineer, in areas of numerous no-passing zones, several zones may be combined as a single zone. If passing is to be prohibited over one or more lengthy sections, a DO NOT PASS sign and a NEXT XX MILES (R20-1TP) plaque may be used at the beginning of such zones. The DO NOT PASS sign and the NEXT XX MILES plaque should be repeated every mile to the end of the no-passing zone. In areas where there is considerable distance between no-passing zones, the end of the no-passing zone may be signed with a PASS WITH CARE sign and a NEXT XX MILES plaque.
- C. Depending on traffic volumes and length of sections, it may be desirable to prohibit passing throughout the project to prevent damage to windshield and lights. The DO NOT PASS sign and NEXT XX MILES plaque should be used and repeated as often as necessary for this purpose. Where several existing zones are to be combined into one individual no-passing zone, the sign at the beginning of the zone should be covered until the surfacing operation has passed this location so as not to have the DO NOT PASS sign conflict with the existing pavement markings. Also, unless one days operation completes the entire length of such combined zones, appropriate DO NOT PASS and PASS WITH CARE signs should be placed at the beginning and end of the no-passing zones where the surfacing operation has stopped for the day.
- D. R4-1 and R4-2 are to remain in place until standard pavement markings are installed.

"NO CENTER LINE" SIGN (CW8-12)

- A. Center line markings are yellow pavement markings that delineate the separation of travel lanes that have opposite directions of travel on a roadway. Divided highways do not typically have center line markings.
- B. At the time construction activity obliterates the existing center line markings (low volume roads may not have an existing centerline), a NO CENTER LINE (CW8-12) sign should be erected at the beginning of the work area, at approximately 2 mile intervals within the work area, beyond major intersections and other locations deemed necessary by the Engineer.
- C. The NO CENTER LINE signs are to remain in place until standard pavement markings are installed.

"LOOSE GRAVEL" SIGN (CW8-7)

- A. When construction begins, a LOOSE GRAVEL (CW8-7) sign should be erected at each end of the work area and repeated at intervals of approximately 2 miles in rural areas and closer in urban areas.
- B. The LOOSE GRAVEL signs are to remain in place until the condition no longer exists.

PAVEMENT MARKINGS

- A. Temporary markings for surfacing projects shall be Temporary Flexible-reflective Roadway Marker Tabs unless otherwise approved by the Engineer. Tabs are to be installed to provide true alignment for striping crews or as directed by the Engineer. Tabs will be placed at the spacing indicated. Tabs should be applied to the pavement no more than two (2) days before the surfacing is applied. After the surfacing is rolled and swept, the cover over the reflective strip shall be removed.
- B. Tabs shall not be used to simulate edge lines.
- C. Tab placement for overlay/inlay operations shall be as shown on the WZ(STPM) standard sheet.

COORDINATION OF SIGN LOCATIONS

- A. The location of warning signs at the beginning and end of a work area are to be coordinated with other signing typically shown on the Barricade and Construction Standards for project limits to ensure adequate sign spacing.
- B. Where possible the ROAD WORK AHEAD (CW20-1D), LOOSE GRAVEL (CW8-7), and NO CENTER LINE (CW8-12) signs should be placed in the sequence shown following the OBEY WARNING SIGNS STATE LAW (R20-3T) and the TRAFFIC FINES DOUBLE (R20-5T) sign, and one "X" sign spacing prior to the CONTRACTOR (G20-6T) sign typically located at or near the limits of surfacing. LOOSE GRAVEL and NO CENTER LINE signs will then be repeated as described above.

Posted Speed *	Minimum Sign Spacing "X" Distance
30	120'
35	160'
40	240'
45	320'
50	400'
55	500'
60	600'
65	700'
70	800'
75	900'

* Conventional Roads Only

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
			✓	✓

GENERAL NOTES

1. The traffic control devices detailed on this sheet will be furnished and erected as directed by the Engineer on sections of roadway where tabs must be placed prior to the surfacing operation which will cover or obliterate the existing pavement markings.
2. The devices shown on this sheet are to be used to supplement those required by the BC Standards or others required elsewhere in the plans.
3. Signs shall be erected as detailed on the BC Standards or the Compliant Work Zone Traffic Control Devices List (CWZTCD) on supports approved for Long-Term / Intermediate-Term Work Zone Sign Supports.
4. When surfacing operations take place on divided highways, freeways or expressways, the size of diamond shaped construction warning signs shall be 48" x 48".
5. Signs on divided highways, freeways and expressways will be placed on both right and left sides of the roadway based on roadway conditions as directed by the Engineer.



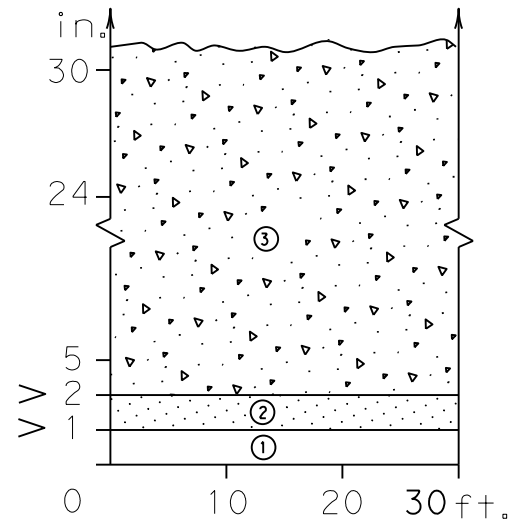
TRAFFIC CONTROL DETAILS FOR SURFACING OPERATIONS

TCP (7-1) - 13

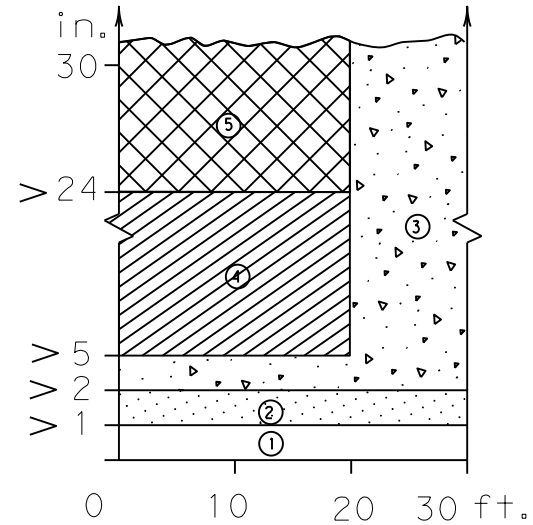
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4-92	4-98	DIST:		COUNTY:		SHEET NO.:			
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DEFINITION OF TREATMENT ZONES FOR VARIOUS EDGE CONDITIONS

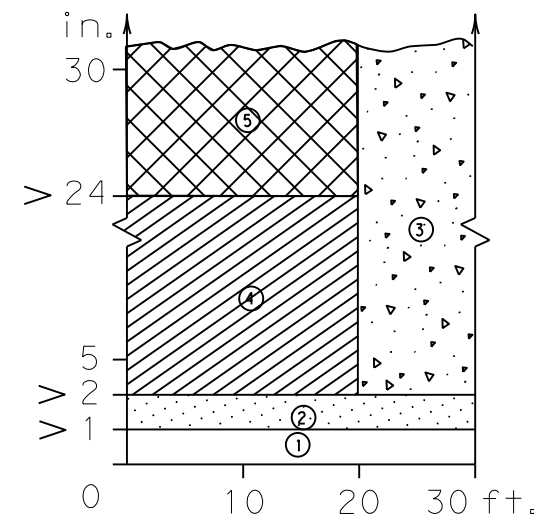
Edge Height (D) in Inches versus Lateral Clearance (Y) in Feet



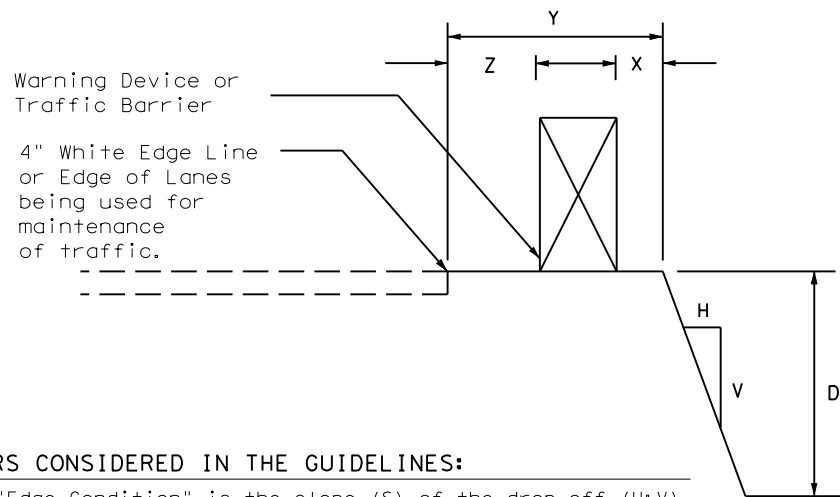
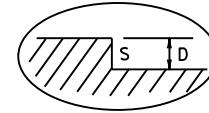
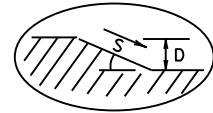
Edge Condition I
S = (3:1) (or flatter)



Edge Condition II
S = ((2.99):1) to (1:1)



Edge Condition III
S is steeper than (1:1)



FACTORS CONSIDERED IN THE GUIDELINES:

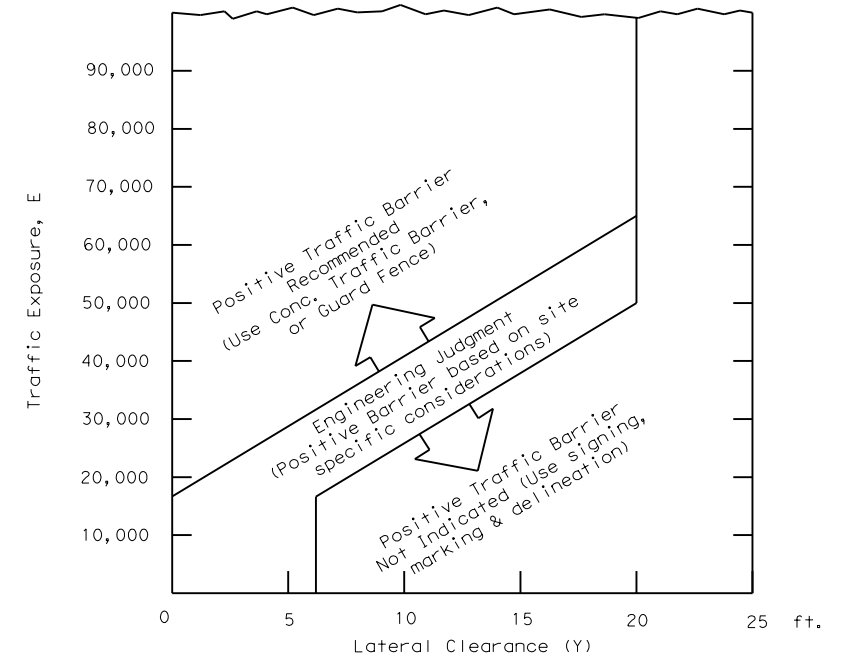
- The "Edge Condition" is the slope (S) of the drop-off (H:V). The "Edge Height" is the depth of the drop-off "D".
- Distance "X" is to be the maximum practical under job conditions. Two feet minimum for high speed conditions. Distance "Y" is the lateral clearance from edge of travel lane to edge of dropoff. Distance "Z" does not have a minimum.
- In addition to the factors considered in the guidelines, each construction zone drop-off situation should be analyzed individually, taking into account other variables, such as: traffic mix, posted speed in the construction zone, horizontal curvature, and the practicality of the treatment options.
- The conditions for indicating the use of positive or protective barriers are given by Zone-5 and Figure-1. Traffic barriers are primarily applicable for high speed conditions. Urban areas with speeds of 30 mph or less may have a lesser need for signing, delineation, and barriers. Right-angled edges, however, with "D" greater than 2 inches and located within a lateral offset of 6 feet, may indicate a higher level of treatment.
- If the distance "Y" must be less than 3 feet, the use of a positive barrier may not be feasible. In such a case, consider either: 1) narrowing the lanes to a desired 11 to 12 feet or 10 foot minimum (see CW20-8 sign), or 2) provide an edge slope such as Edge Condition I.

Zone	Treatment Types Guidelines:
①	No treatment
②	CW 8-11 "Uneven Lanes" signs.
③	CW 8-9a Shoulder Drop-Off" or CW 8-11 signs plus vertical panels.
④	CW8-9a or CW 8-11, signs plus drums. Where restricted space precludes the use of drums, use vertical panels. An edge slope to that of the proferred Edge Condition I.
⑤	Check indications (Figure-1) for positive barrier. Where positive barrier is not indicated, the treatment shown above for Zone-4 may be used after consideration of other applicable factors.

Edge Condition Notes:

- Edge Condition I: Most vehicles are able to traverse an edge condition with a slope rate of (3 to 1) or flatter. The slope must be constructed with a compacted material capable of supporting vehicles.
- Edge Condition II: Most vehicles are able to traverse an edge condition with a slope between (2.99 to 1) and (1 to 1) so long as "D" does not exceed 5 inches. Under-carriage drag on most automobiles will occur when "D" exceeds 6 inches. As "D" exceeds 24 inches, the possibility for rollover is greater in most vehicles.
- Edge Condition III: When slopes are greater than (1 to 1) and where "D" is greater than 2 inches, a more difficult control factor may exist for some vehicles, if not properly treated. For example, where "D" is greater than 2 inches and up to 24 inches different types of vehicles may experience different steering control at different edge heights. Automobiles might experience more steering control differential when "D" is greater than 2 inches and up to 5 inches. Trucks, particularly those with high loads, have more steering control differential when "D" is greater than 5 inches and up to 24 inches. When "D" exceeds 24 inches, the possibility of rollover is greater for most vehicles.
- Milling or overlay operations that result in Edge Condition III should not be in place without appropriate warning treatments, and these conditions should not be left in place for extended periods of time.

FIGURE-1: CONDITIONS INDICATING USE OF POSITIVE BARRIER FOR ZONE 5 ([Cross-hatch])



- $E = ADT \times T$
Where ADT is that portion of the average daily traffic volume traveling within 20 feet (generally two adjacent lanes) of the edge dropoff condition; and, T is the duration time in years of the dropoff condition.
- Figure-1 provides a practical approach to the use of positive barriers for the protection of vehicles from pavement drop-offs. Other factors, such as the presence of heavy machinery, construction workers, or the mix and volume of traffic may make the use of positive barriers appropriate, even when the edge condition alone may not justify the use of a barrier.
- An approved end treatment should be provided for any positive barrier end located within the clear zone.

These guidelines apply to temporary traffic control areas or work zones where continuous pavement edges or drop-offs exists parallel and adjacent to a lane used by traffic. The edge conditions may be present between shoulders and travel lanes, between adjacent or opposing travel lanes, or at intermediate points across the width of the paved surface. Due to the variability in construction operations, tolerances in the variables may be allowed by the engineer. These guidelines do not apply to short term operations. These guidelines do not constitute a rigid standard or policy; rather, they are guidance to be used in conjunction with engineering judgement. These guidelines may be updated on the Design Division's on-line manuals.

Engineer's Seal

Date: 1/11/2023

01/26/2023

Texas Department of Transportation

Traffic Safety Division Standard

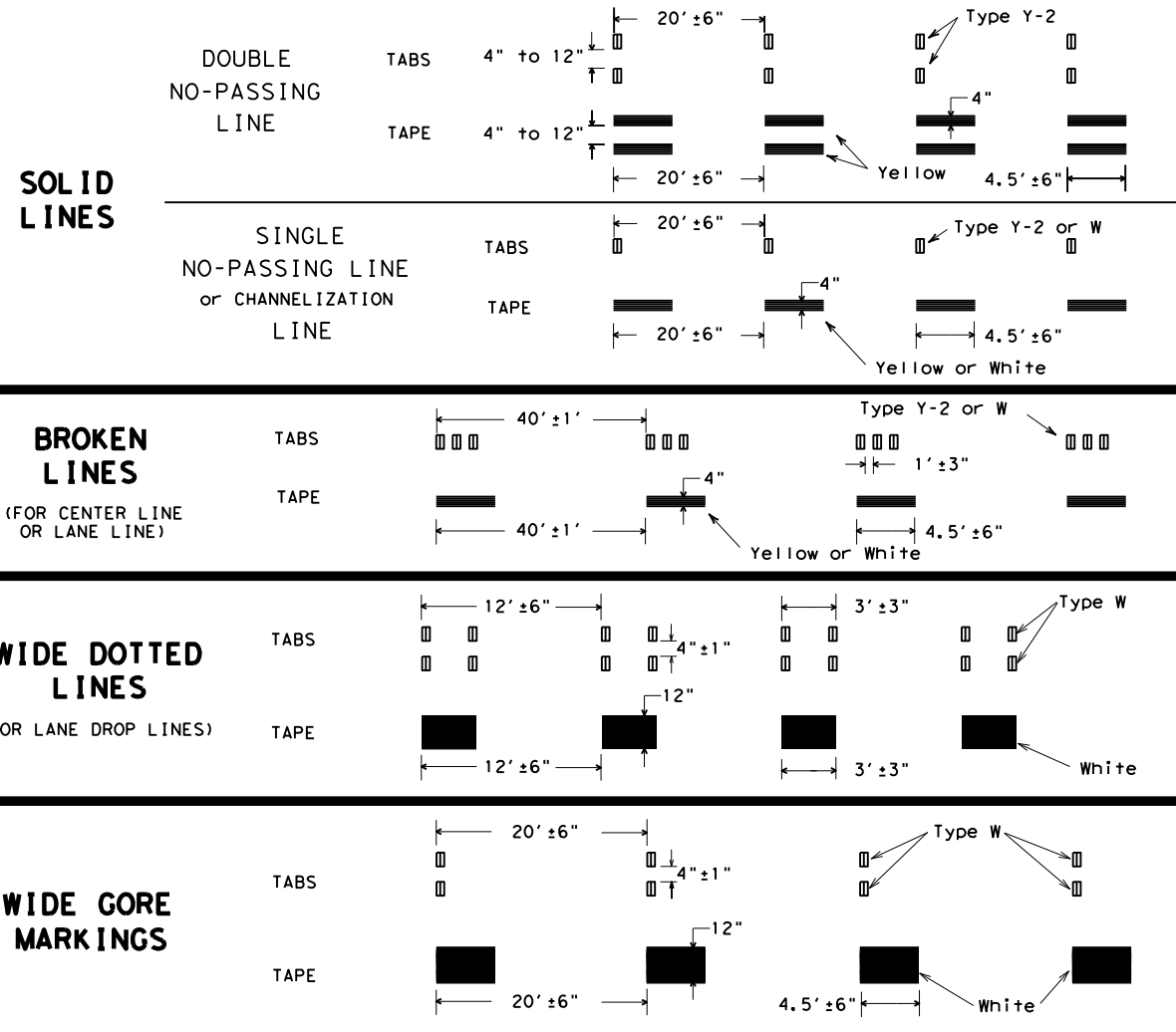
TREATMENT FOR VARIOUS EDGE CONDITIONS

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9-21				

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 DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the accuracy of the information contained herein.

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WORK ZONE SHORT TERM PAVEMENT MARKINGS DETAILS



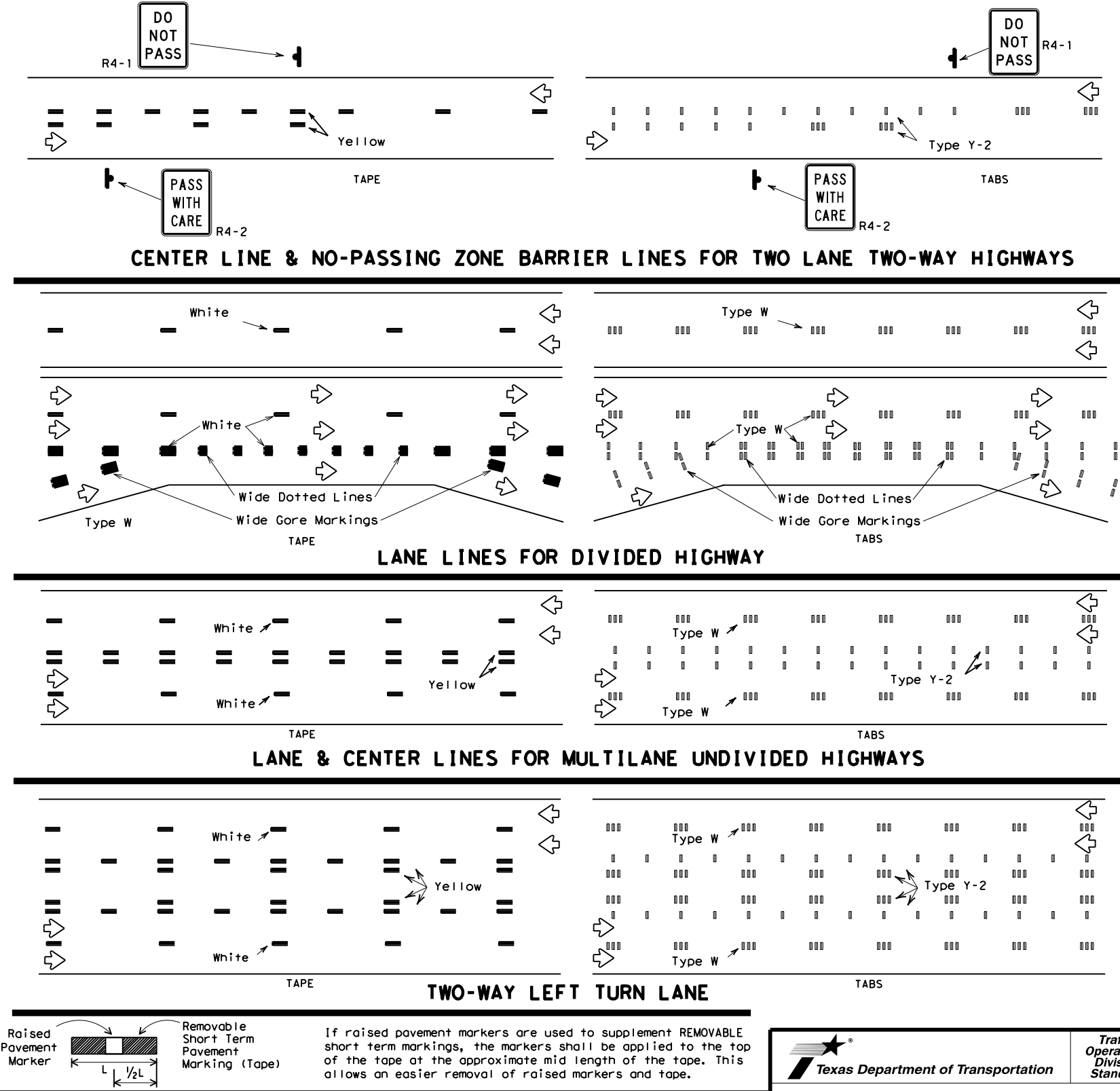
NOTES:

- Short term pavement markings may be prefabricated markings (stick down tape) or temporary flexible-reflective roadway marker tabs unless otherwise specified elsewhere in plans.
- Short term pavement markings shall NOT be used to simulate edge lines.
- Dimensions indicated on this sheet are typical and approximate. Variations in size and height may occur between markers or devices made by manufacturers, by as much as 1/4 inch, unless otherwise noted.
- Temporary flexible-reflective roadway marker tabs will require normal maintenance replacement when used on roadways with an ADT per lane of up to 7500 vehicles with no more than 10% truck mix. When roadways exceed these values, additional maintenance replacement of devices should be planned.
- No segment of roadway open to traffic shall remain without permanent pavement markings for a period greater than 14 calendar days. The Contractor will be responsible for maintaining short term pavement markings until permanent pavement markings are in place. When the Contractor is responsible for placement of permanent pavement markings, no segment of roadway shall remain without permanent pavement markings for a period greater than 14 calendar days unless weather conditions prohibit placement. Permanent pavement markings shall be placed as soon as weather permits.
- For two lane, two-way roadways, DO NOT PASS signs shall be erected to mark the beginning of sections where passing is prohibited and PASS WITH CARE signs shall be erected to mark the beginning of sections where passing is permitted. Signs shall be in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and may be used to indicate the limits of no-passing zones for up to 14 calendar days. Permanent pavement markings should then be placed.
- For low volume two lane, two-way roadways of 4000 ADT or less, no-passing lines may be omitted when approved by the Engineer. DO NOT PASS and PASS WITH CARE signs shall be erected (see note 6).
- For exit gores where a lane is being dropped place wide gore markings or retroreflective channelizing devices to guide motorist through the exit. If channelizing devices are to be used it should be noted elsewhere in the plans. One piece cones are not allowed for this purpose.

TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS (TABS)

- Temporary flexible-reflective roadway marker tabs detailed on this sheet will be designated Type Y-2 (two amber reflective surfaces with yellow body); Type Y (one amber reflective surface with yellow body); and Type W (one white or silver reflective surface with white body). Additional details may be found on BC(11).
- Tabs shall meet requirements of Departmental Material Specification DMS-8242.
- When dry, tabs shall be visible for a minimum distance of 200 feet during normal daylight hours and when illuminated by automobile low-beam head light at night, unless sight distance is restricted by roadway geometrics.
- No two consecutive tabs nor four tabs per 1000 feet of line shall be missing or fail to meet the visual performance requirements of Note 3.

WORK ZONE SHORT TERM PAVEMENT MARKINGS PATTERNS



PREFABRICATED PAVEMENT MARKINGS

- Temporary Removable Prefabricated Pavement Markings shall meet the requirements of DMS-8241.
- Non-removable Prefabricated Pavement Markings shall meet the requirements of either DMS-8240 "Permanent Prefabricated Pavement Markings" or DMS-8243 "Temporary Construction-Grade Prefabricated Pavement Markings."

RAISED PAVEMENT MARKERS

- All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and DMS-4200.

DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS) & MATERIAL PRODUCER LISTS (MPL)

- DMSs referenced above can be found along with embedded links to their respective MPLs at the following website:
http://www.txdot.gov/business/contractors_consultants/material_specifications/default.htm

Texas Department of Transportation
 Traffic Operations Division Standard

WORK ZONE SHORT TERM PAVEMENT MARKINGS

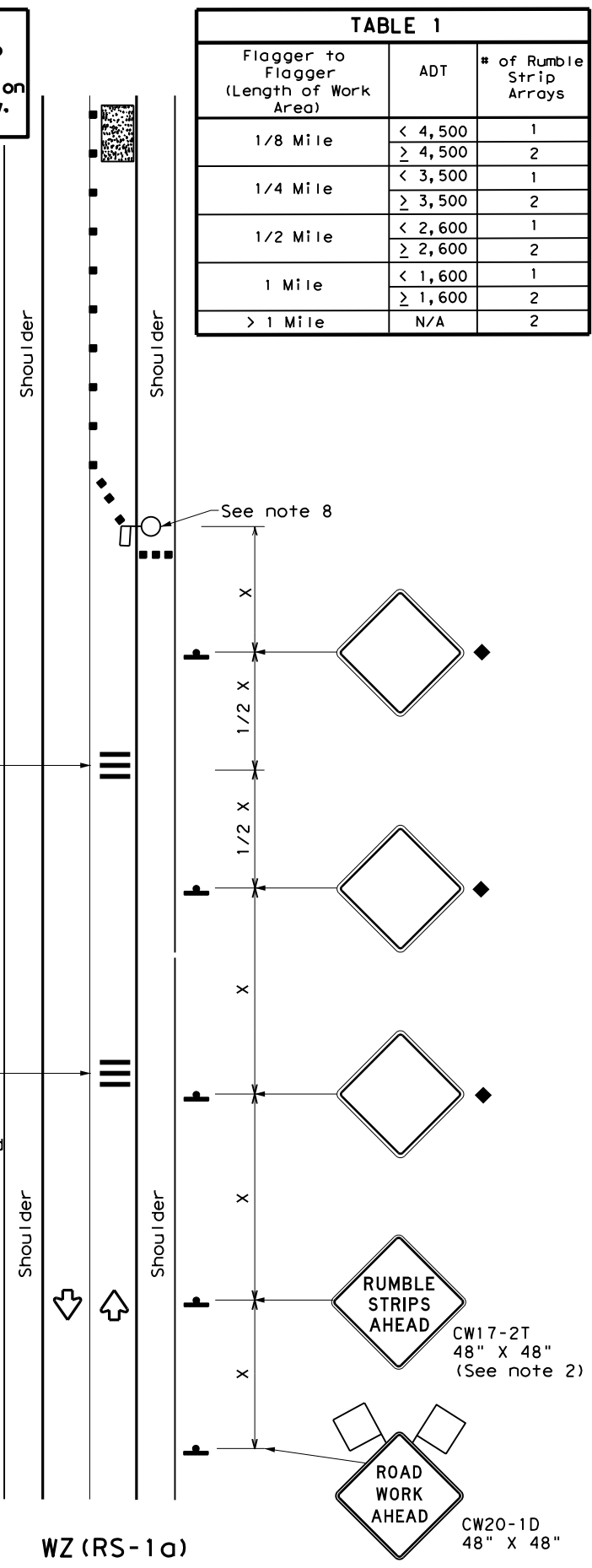
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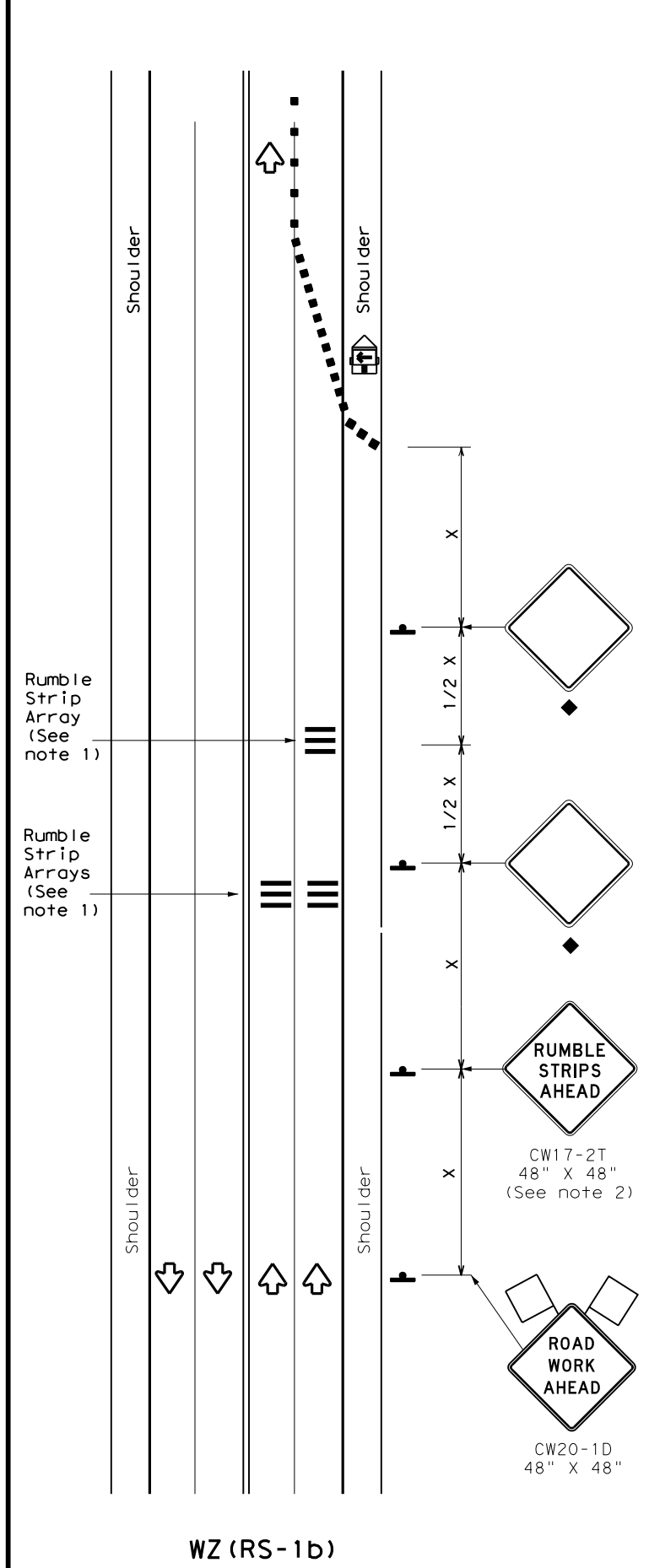
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Warning sign and rumble strip sequence in opposite direction is same as below.

Flagger to Flagger (Length of Work Area)	ADT	# of Rumble Strip Arrays
1/8 Mile	< 4,500	1
	≥ 4,500	2
1/4 Mile	< 3,500	1
	≥ 3,500	2
1/2 Mile	< 2,600	1
	≥ 2,600	2
1 Mile	< 1,600	1
	≥ 1,600	2
> 1 Mile	N/A	2



RUMBLE STRIPS ON ONE-LANE TWO-WAY APPLICATION



RUMBLE STRIPS FOR LANE CLOSURE ON CONVENTIONAL ROADWAY

GENERAL NOTES

- Each Rumble Strip Array should consist of three rumble strips spaced center to center at the spacing shown in Table 2, placed transverse across the lane at locations shown.
- The CW17-2T "RUMBLE STRIPS AHEAD" sign should be located after the CW20-1D "ROAD WORK AHEAD" sign and spaced as shown. If traffic is observed to be queuing, or is expected to queue beyond the Rumble Strips, the CW17-2T sign and the first Rumble Strip Array may be located upstream of the CW20-1D sign as necessary to provide needed warning.
- Temporary Rumble Strips will be considered subsidiary to Item 502, and shall be a product listed on the Compliant Work Zone Traffic Control Devices.
- Remove Temporary Rumble Strips before removing the advanced warning signs.
- Temporary Rumble Strips should not be used on horizontal curves, loose gravel, soft or bleeding asphalt, heavily rutted pavements or unpaved surfaces.
- Temporary Rumble Strips shall be installed and maintained as per manufacturer's recommendations.
- This standard sheet shall be used in conjunction with other appropriate TCP standard, TMUTCD typical application or project specific detail for the project.
- The one-lane two-way application may utilize a flagger, an Automated Flagger Assistance Device (AFAD) or a Portable Traffic Signal (PTS).
- Replace defective Temporary Rumble Strips as directed by the Engineer.
- Temporary Rumble Strips may be used on freeways or expressways based on engineering judgment and written direction from the Engineer.

Speed	Approximate distance between strips in an array
≤ 40 MPH	10'
> 40 MPH & ≤ 55 MPH	15'
= 60 MPH	20'
≥ 65 MPH	* 35' +

	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Panel		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	L = WS/60	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT)
 S=Posted Speed (MPH)

MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

◆ Signs are for illustrative purposes only. Signs required may vary depending on the TCP, TMUTCD Typical Application, or project specific details for the project.
 * For posted speeds in excess of 65 MPH, it is recommended that spacing is increased as speed limits increase. Increasing space between rumble strips will improve effectiveness.

Texas Department of Transportation
 Traffic Safety Division Standard

TEMPORARY RUMBLE STRIPS





WZ (RS) - 22

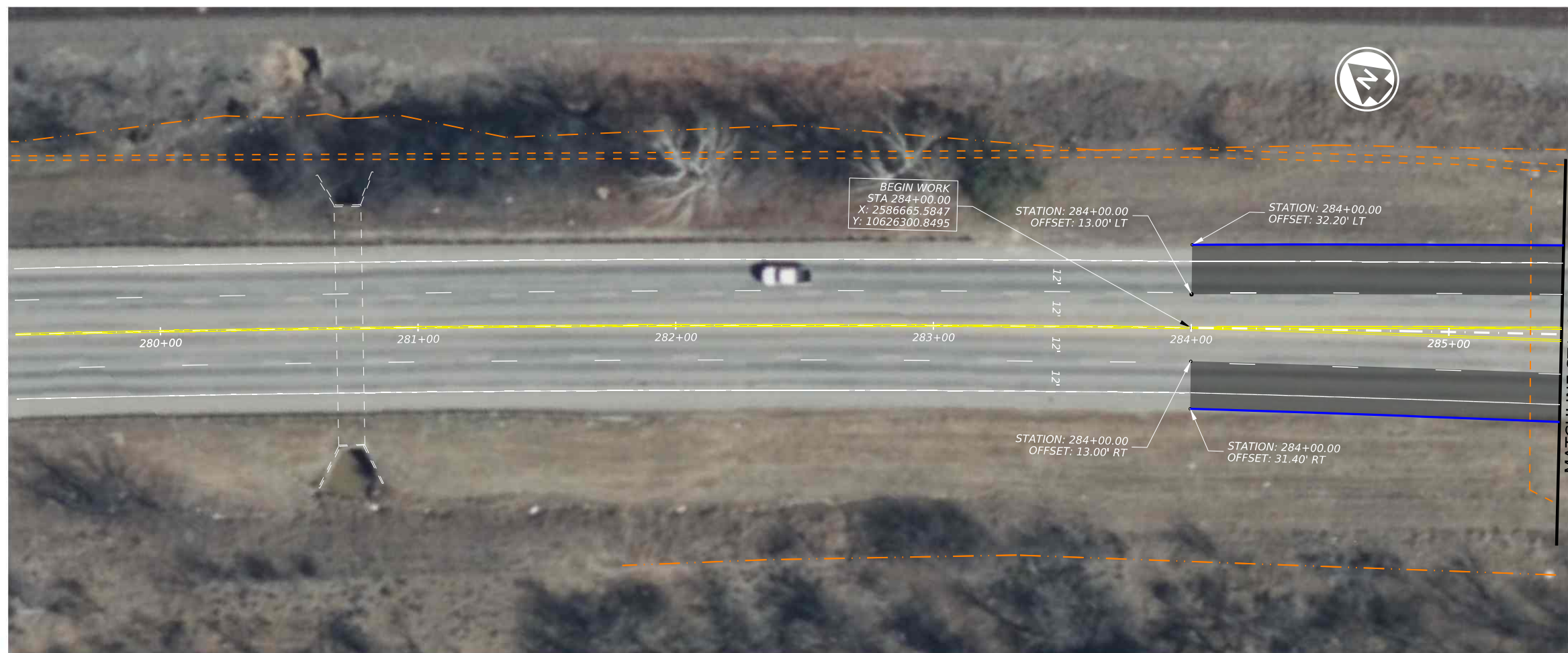
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LEGEND

- RIGHT OF WAY 
- UNDERGROUND TELEPHONE 
- WATER LINE 
- GAS LINE 



01/26/2023

 Texas Department of Transportation

US 84

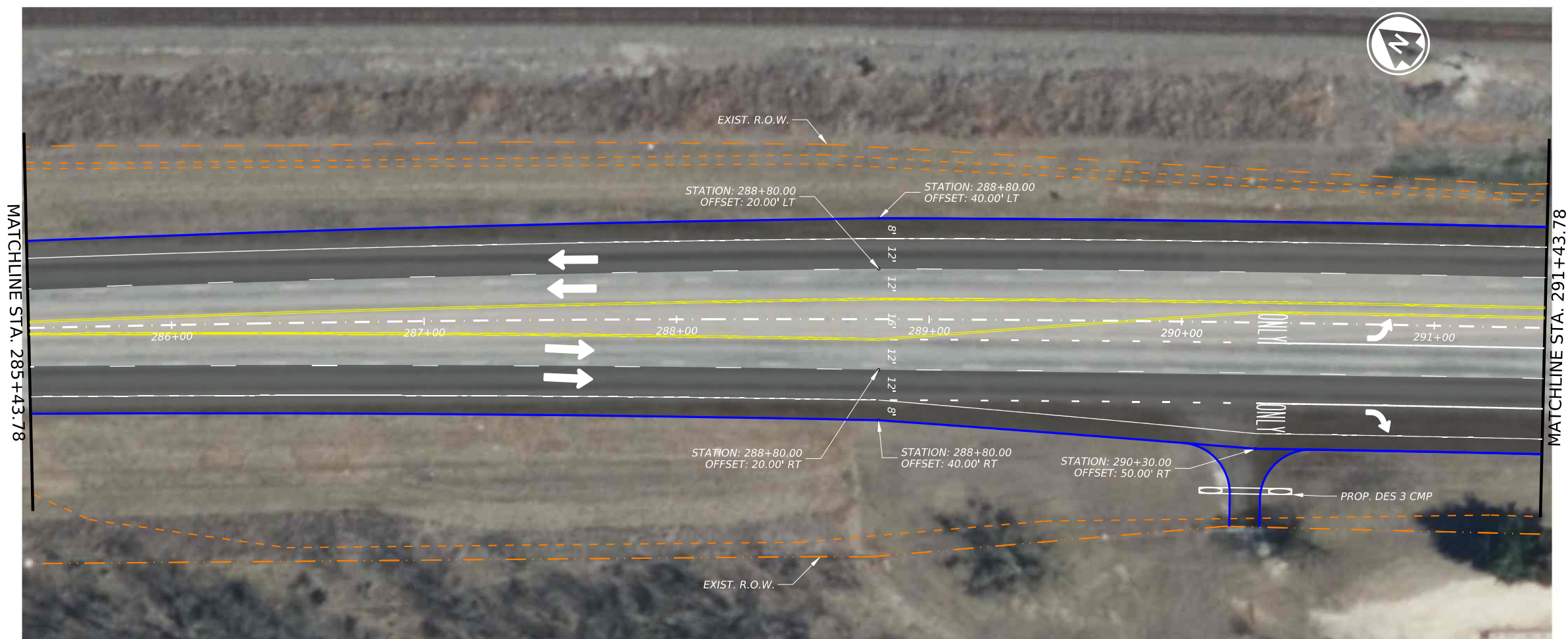
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SHEET 1 OF 8

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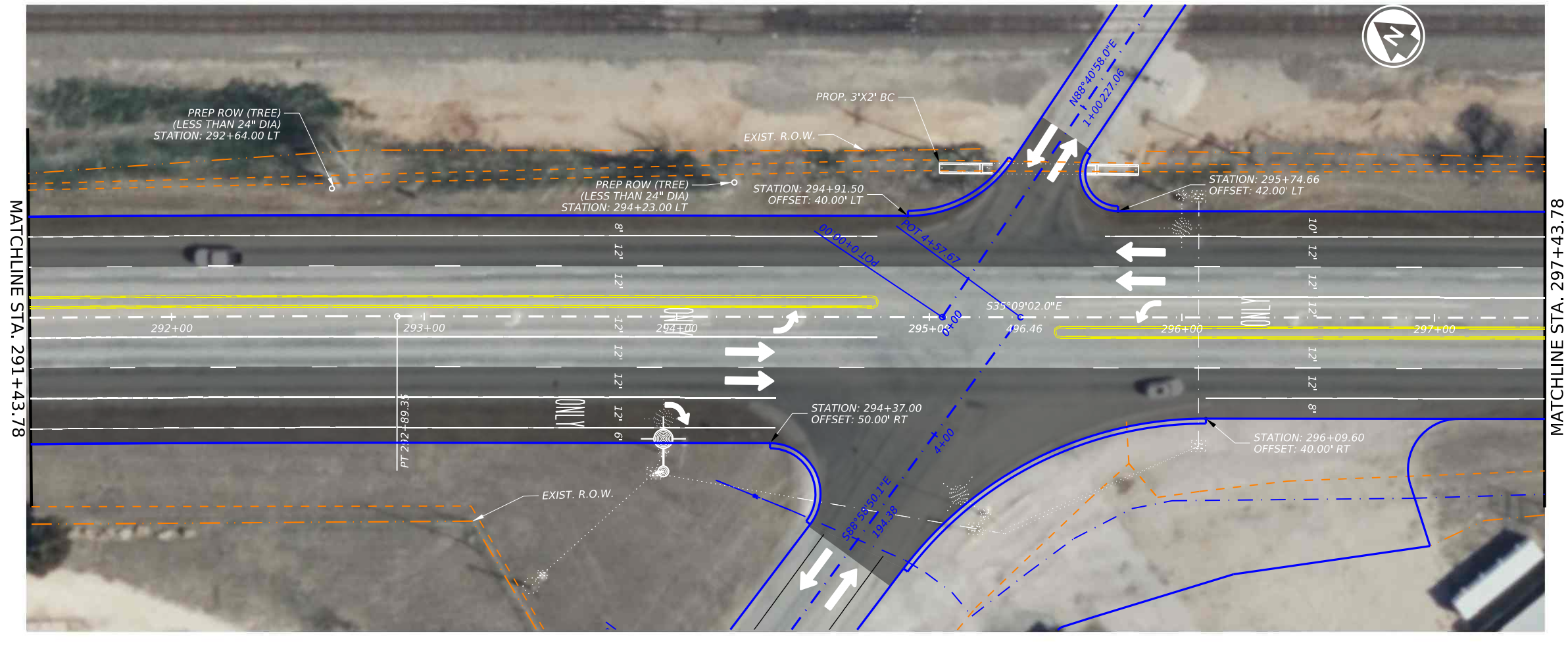
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LEGEND

- RIGHT OF WAY - - - - -
- UNDERGROUND TELEPHONE - - - - -
- WATER LINE - - - - -
- GAS LINE - - - - -



STATE OF TEXAS
 JASON H. SCANTLING
 96633
 LICENSED PROFESSIONAL ENGINEER
JH Scantling, P.E.

01/28/2023

Texas Department of Transportation

US 84

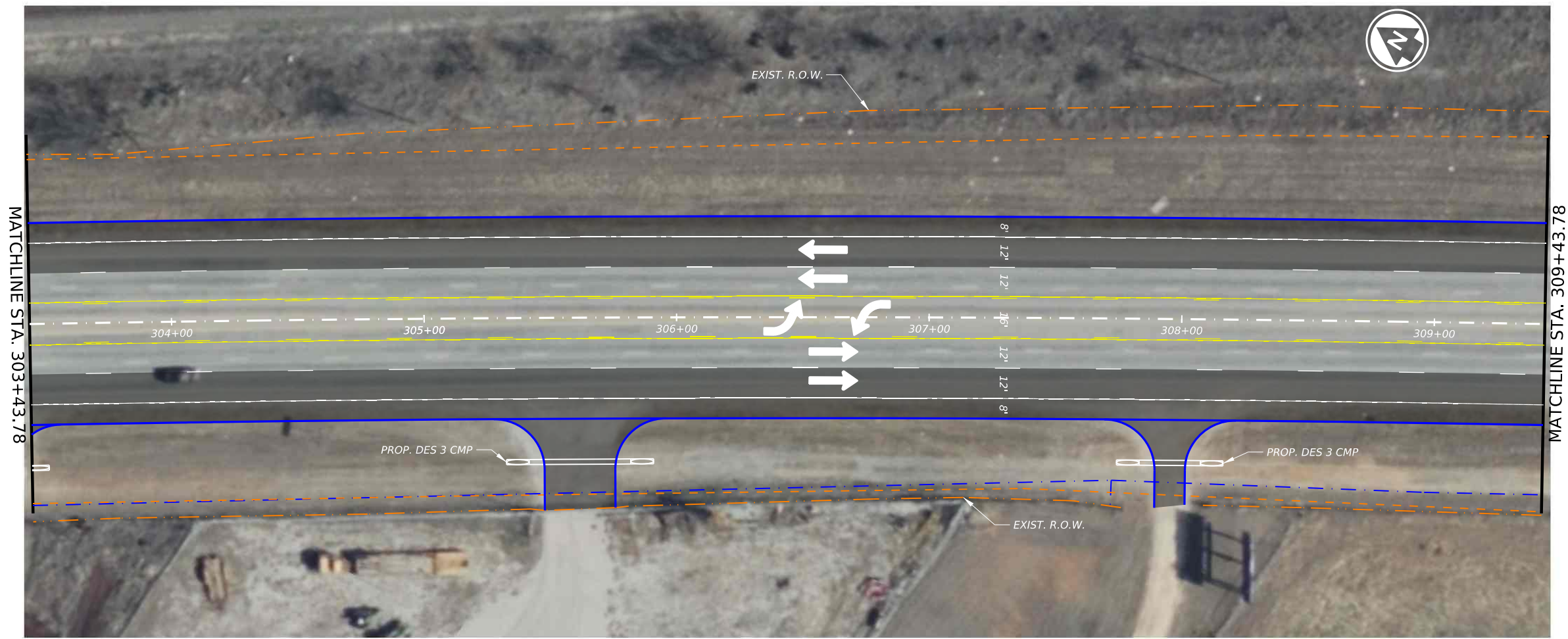
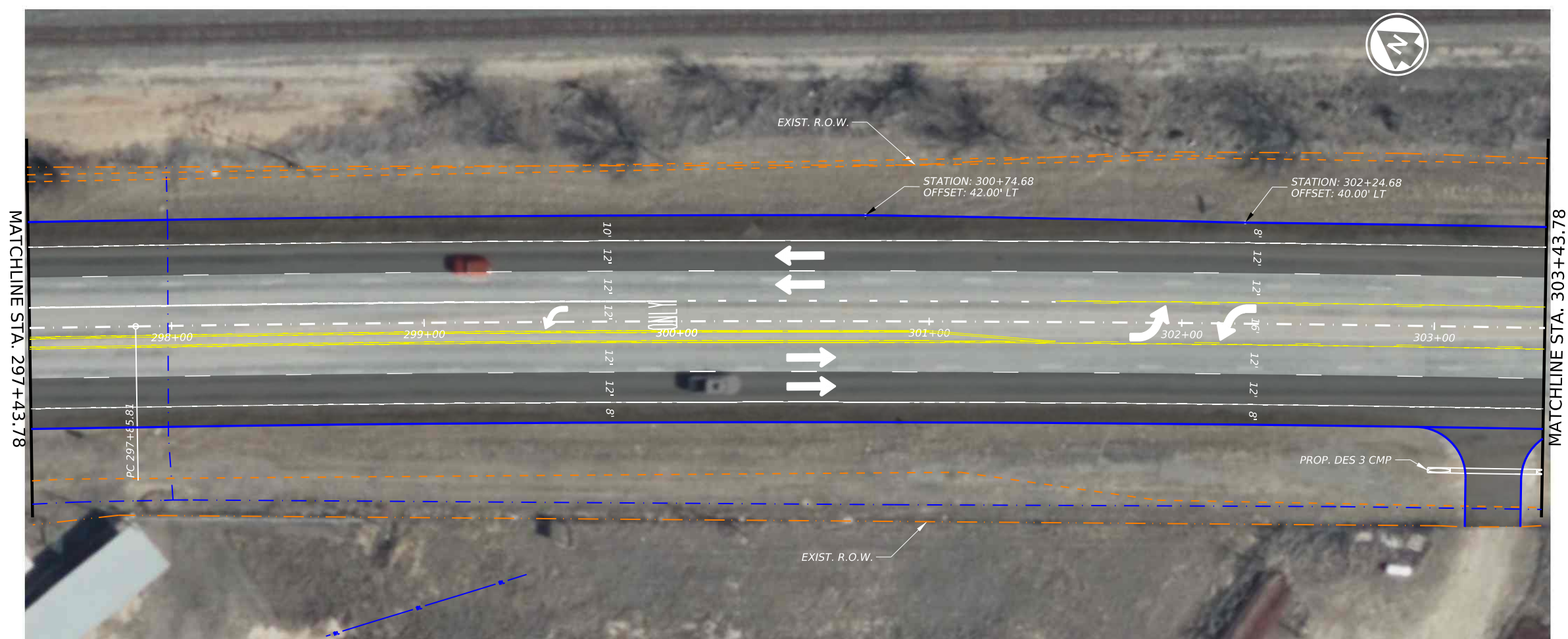
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SHEET 2 OF 8

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LEGEND

RIGHT OF WAY	
UNDERGROUND TELEPHONE	
WATER LINE	
GAS LINE	



01/28/2023

Texas Department of Transportation

US 84

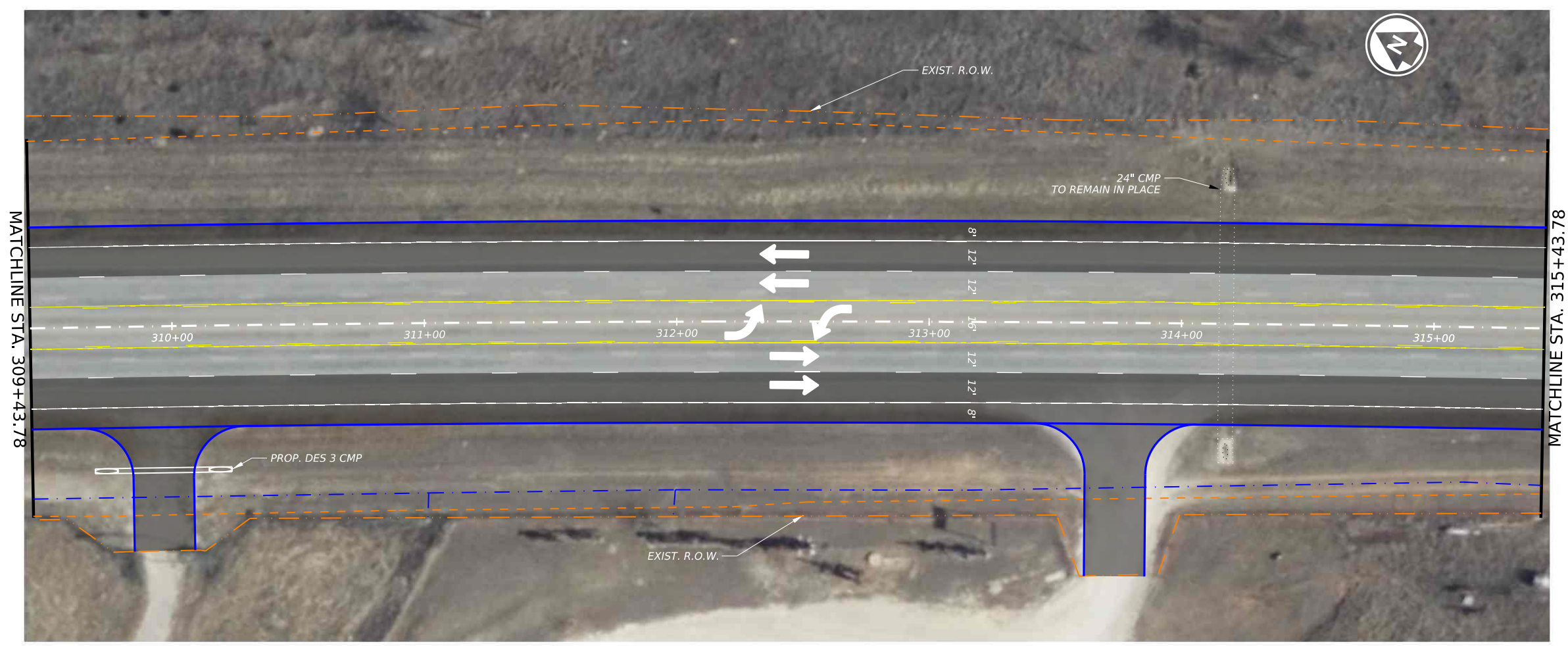
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LEGEND

- RIGHT OF WAY ———
- UNDERGROUND TELEPHONE ———
- WATER LINE ———
- GAS LINE ———

STATE OF TEXAS
 JASON H. SCANTLING
 96633
 LICENSED PROFESSIONAL ENGINEER
 JH Scantling, P.E.

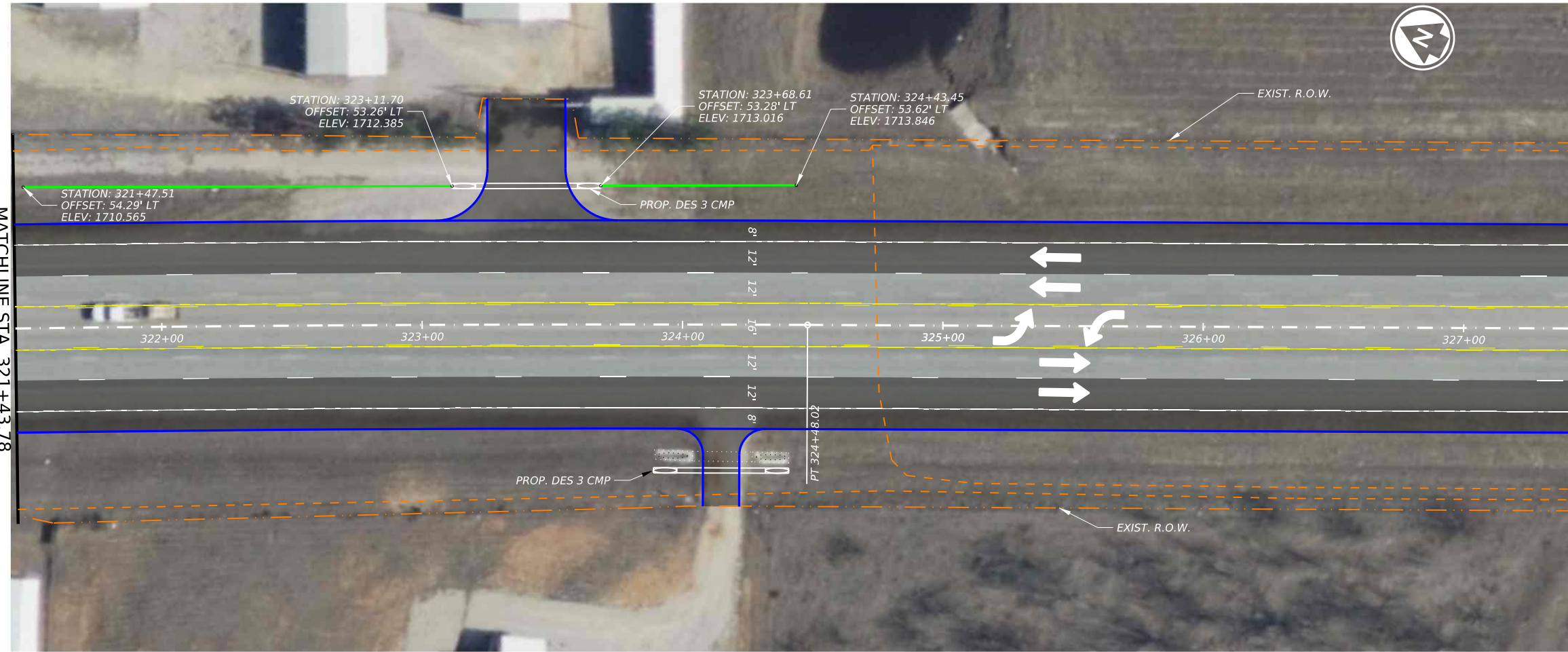
01/26/2023
 Texas Department of Transportation
 US 84
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 SHEET 4 OF 8

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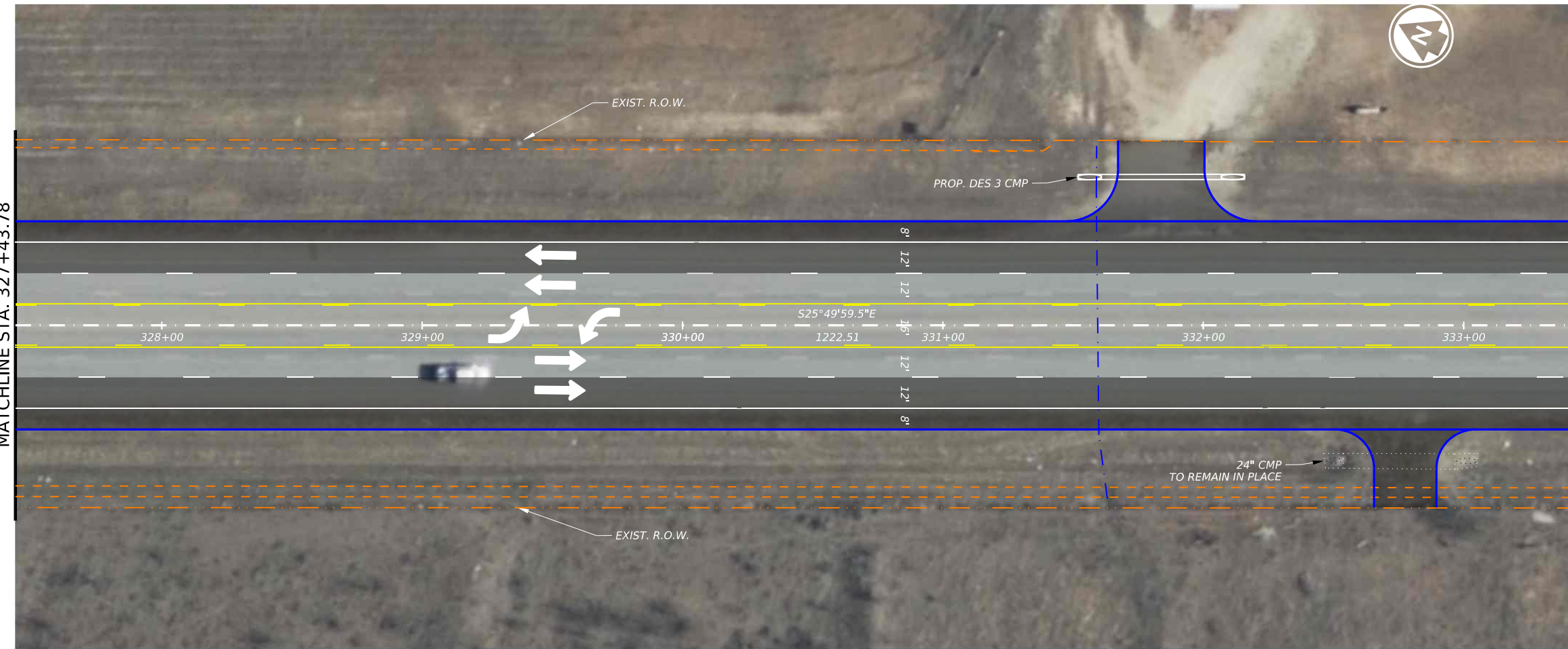
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MATCHLINE STA. 321+43.78



MATCHLINE STA. 327+43.78

MATCHLINE STA. 327+43.78



MATCHLINE STA. 333+00

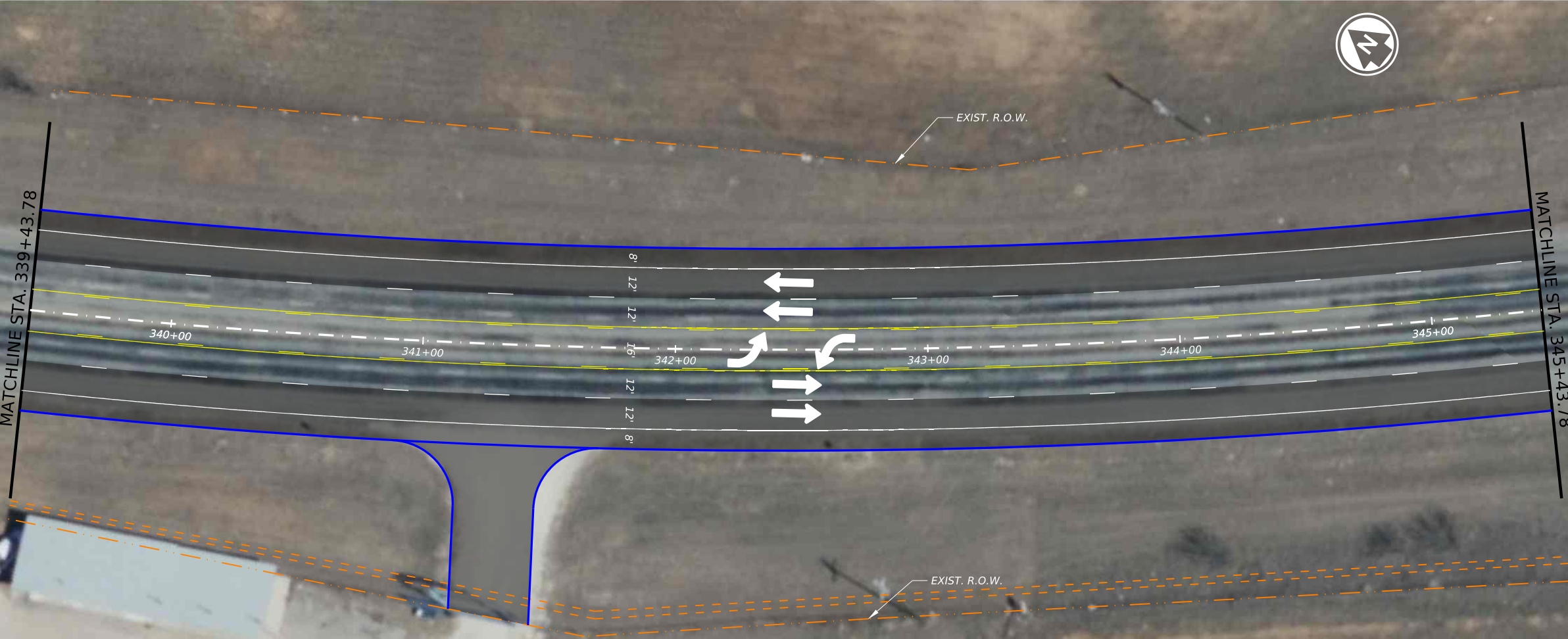
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UNDERGROUND TELEPHONE	Red dashed line
WATER LINE	Blue dashed line
GAS LINE	Yellow dashed line



01/26/2023			
US 84			
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LEGEND

- RIGHT OF WAY - - - - -
- UNDERGROUND TELEPHONE - - - - -
- WATER LINE - - - - -
- GAS LINE - - - - -



01/26/2023

Texas Department of Transportation

US 84

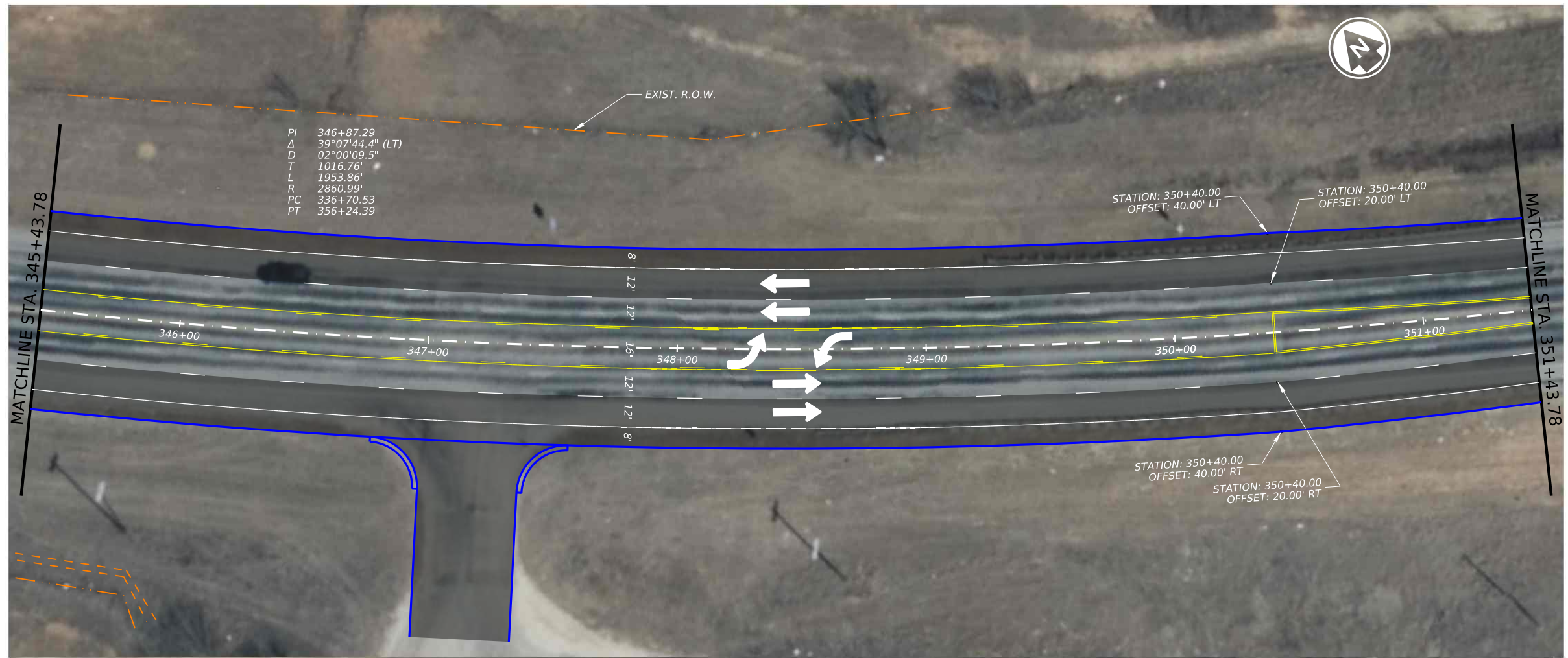
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LEGEND

- RIGHT OF WAY - - - - -
- UNDERGROUND TELEPHONE - - - - -
- WATER LINE - - - - -
- GAS LINE - - - - -



01/26/2023

Texas Department of Transportation

US 84





PLAN LAYOUT

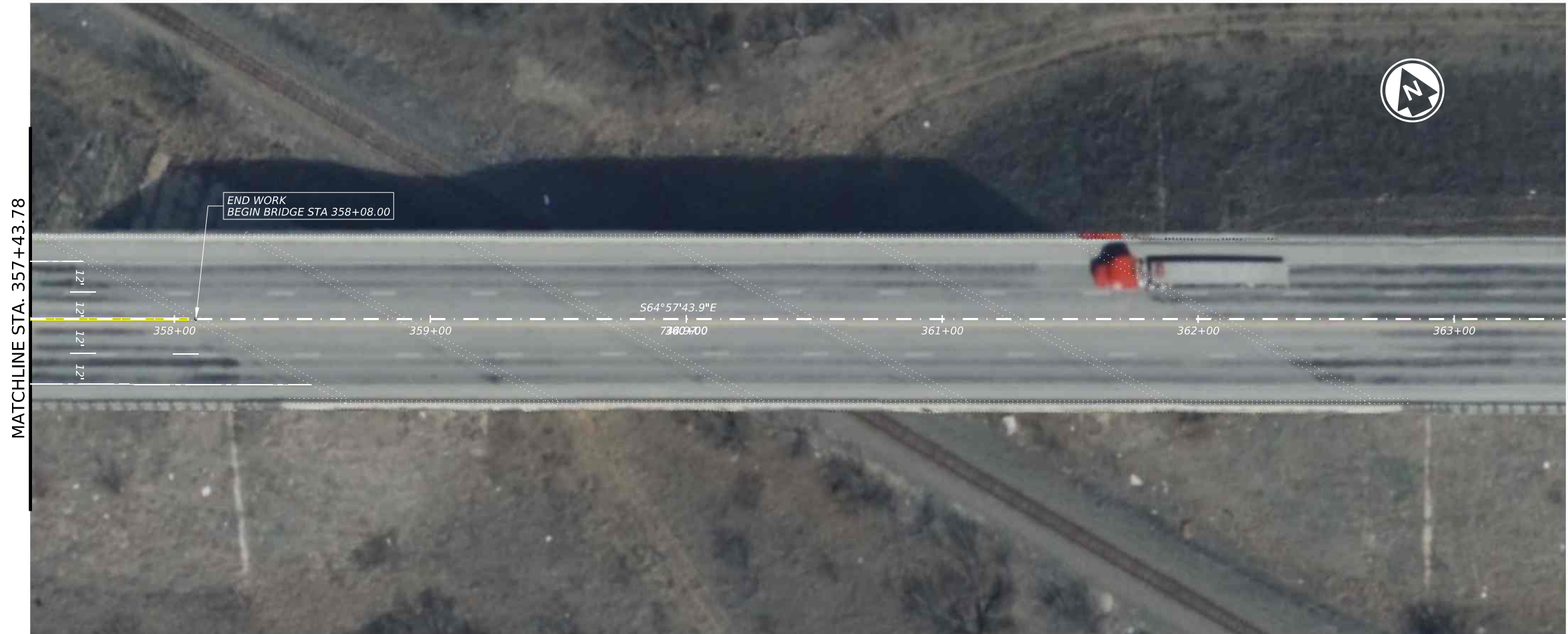
SHEET 7 OF 8

CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST		COUNTY	SHEET NO.
BWD		COLEMAN CO.	50

CK: DW: CK: DW:

LEGEND

- RIGHT OF WAY 
- UNDERGROUND TELEPHONE 
- WATER LINE 
- GAS LINE 




MATCHLINE STA. 357+43.78

STATE OF TEXAS
 JASON H. SCANTLING
 96633
 LICENSED PROFESSIONAL ENGINEER
JH Scantling, P.E.

DATE: 1/12/2023 9:12:15 AM
 FILE: T:\BWD\SGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\4. Design\Master Design\SheetFiles\Plan_Layout.dgn

01/26/2023

 Texas Department of Transportation

US 84

PLAN LAYOUT

SHEET 8 OF 8

CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	51	

DATE: 1/11/2023 8:23:06 AM
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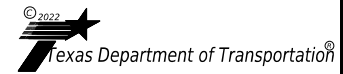
DRIVEWAY SUMMARY								
STATION	RADIUS FT	WIDTH FT	LENGTH FT	PIPE OFFSET FT	ITEM 460-6010 CMP AR (GAL STL DES 3) LF	ITEM 467-6536 SET (TY II) (DES 3)(CMP) (6:1)(C) EA	ITEM 496-6007 REMOVE STR (PIPE) LF	ITEM 530-6006 DRIVEWAYS (SURF TREAT) SY
290+26 RT	20	12	31	16	19	2		60
303+38 RT	20	22	39	17	34	2		115
305+61 RT	20	28	36	17	40	2		131
307+96 RT	20	12	34	17	24	2		65
309+96 RT	20	24	49	17	36	2		150
313+74 RT	20	24	60					179
317+62 RT	15	44	26	17	56	2	95	150
323+41 LT	20	30	47	13	39	2		176
324+15 RT	10	14	30	16	34	2	52	52
331+84 LT	20	33	31	17	46	2		134
332+77 RT	15	24	30					91
336+52 RT	15	32	30					118
337+39 RT	15	12	59					90
341+30 RT	25	32	70					274
TOTALS					328	18	147	1785

NOTES:
 STATIONS ARE APPROXIMATE AND MAY BE CHANGED IN THE FIELD
 AS DIRECTED BY THE ENGINEER

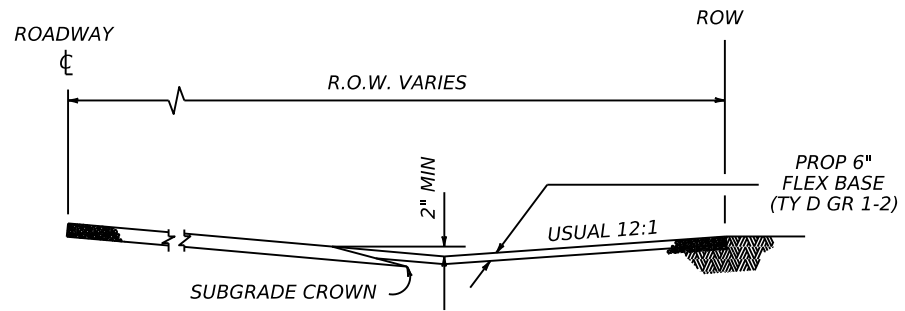


01/26/2023

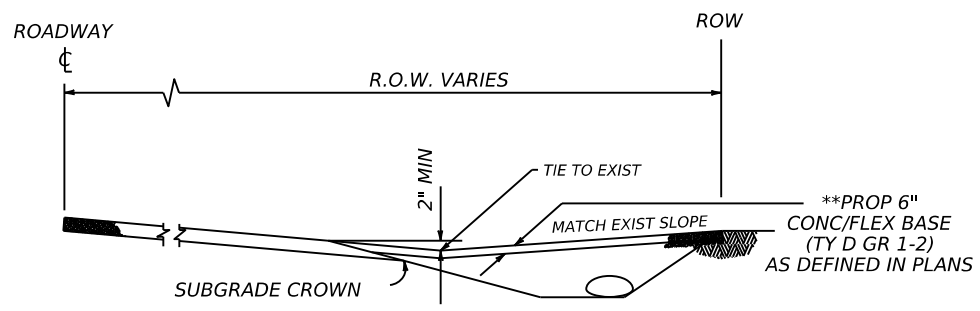
US 84
 DRIVEWAY
 SUMMARY



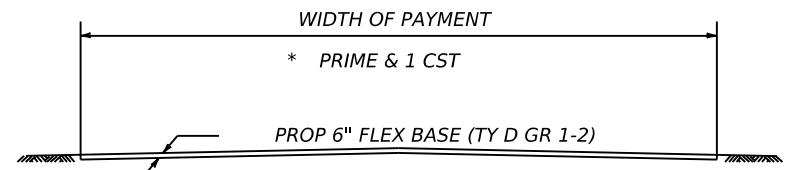
CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	52	



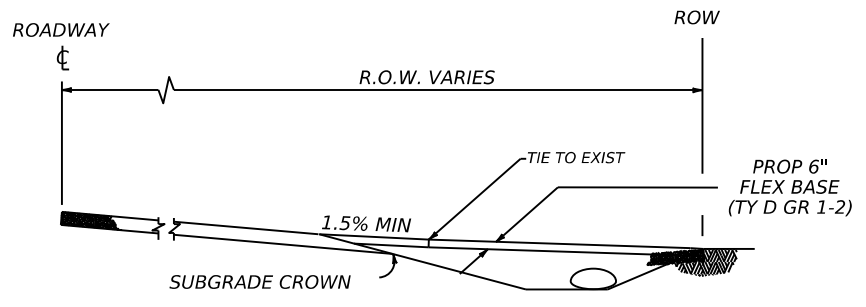
DIP SECTIONS



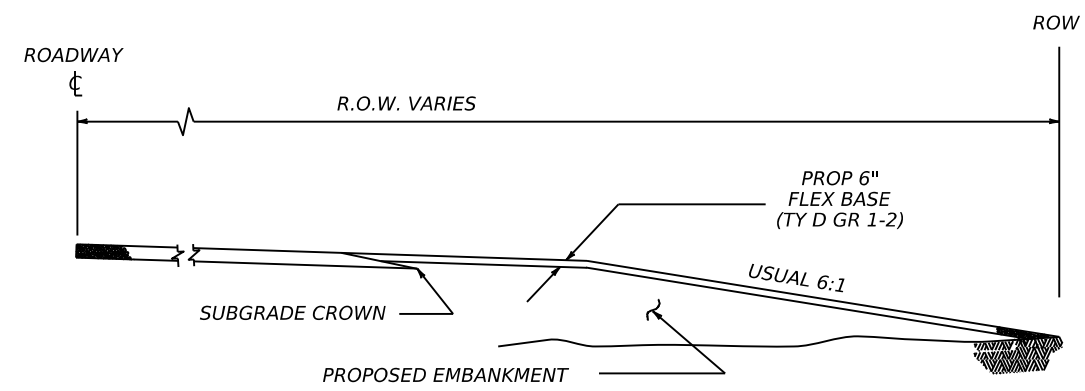
PIPE SECTIONS



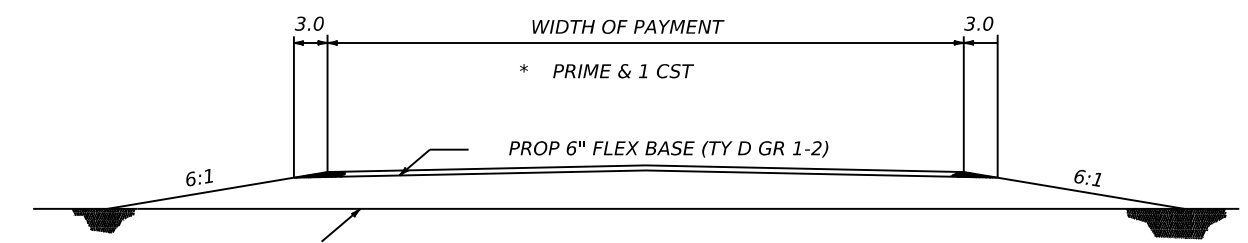
TYPICAL SECTION FOR DIP DRIVES AND COUNTY ROADS



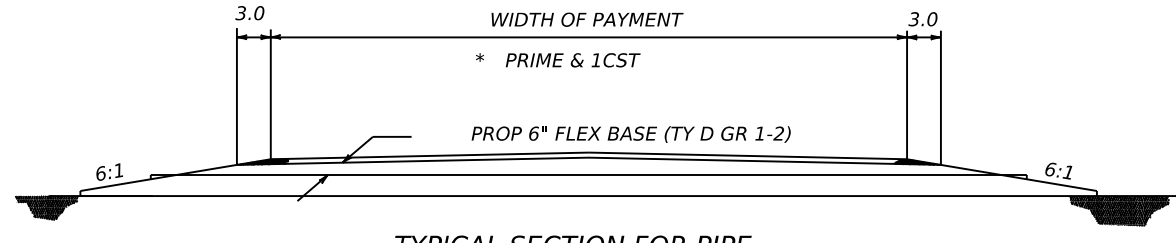
PIPE SECTIONS



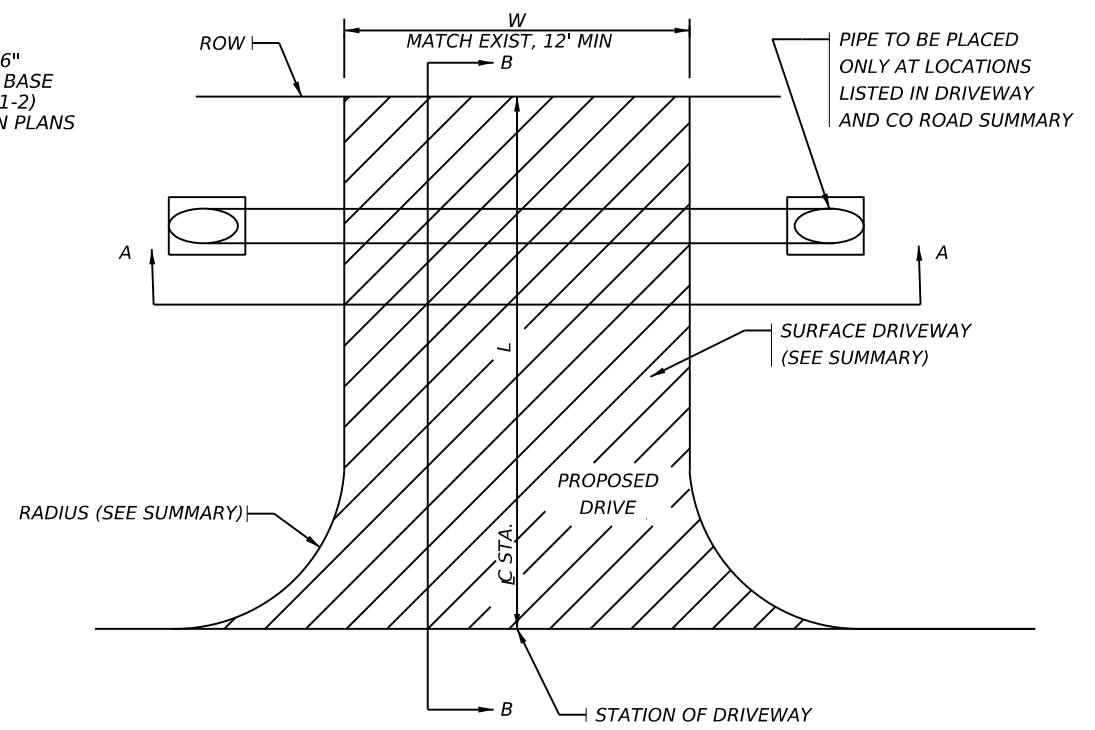
GRADE BREAK OR NO ROADSIDE DITCH SECTION



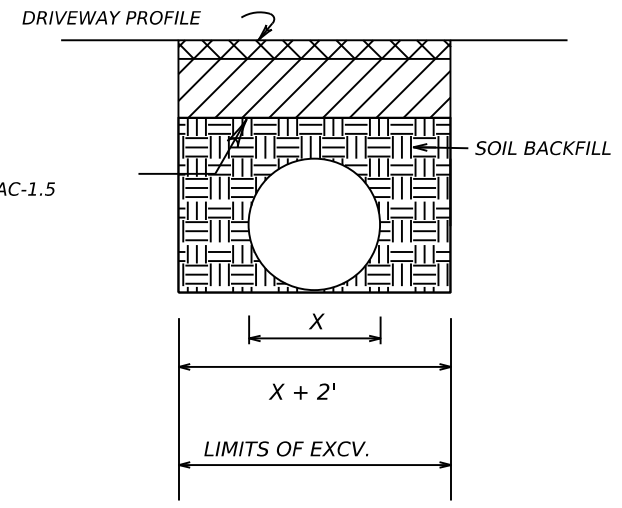
TYPICAL SECTION FOR GRADE BREAK OR NO ROADSIDE DITCH SECTION



TYPICAL SECTION FOR PIPE DRIVES AND COUNTY ROADS



TYPICAL DRIVEWAY & COUNTY ROAD



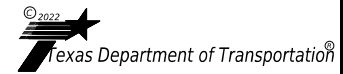
TYPICAL BACKFILL TREATMENT FOR PIPE TRENCHES ON EXISTING DRIVEWAYS

* CONTRACTORS INFORMATION
PRIME COAT (MC-30) 0.20 Gal/SY
AGGR (TY-PB GR-4S)(SAC-B) 120SY/CY



01/26/2023

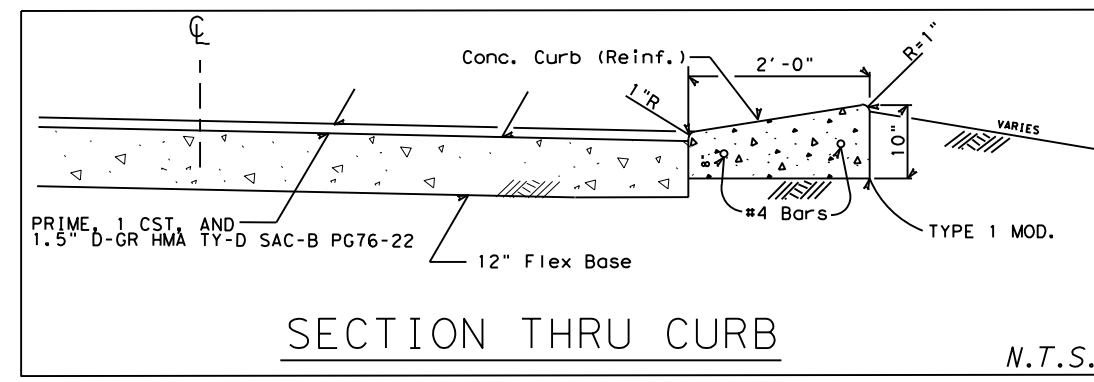
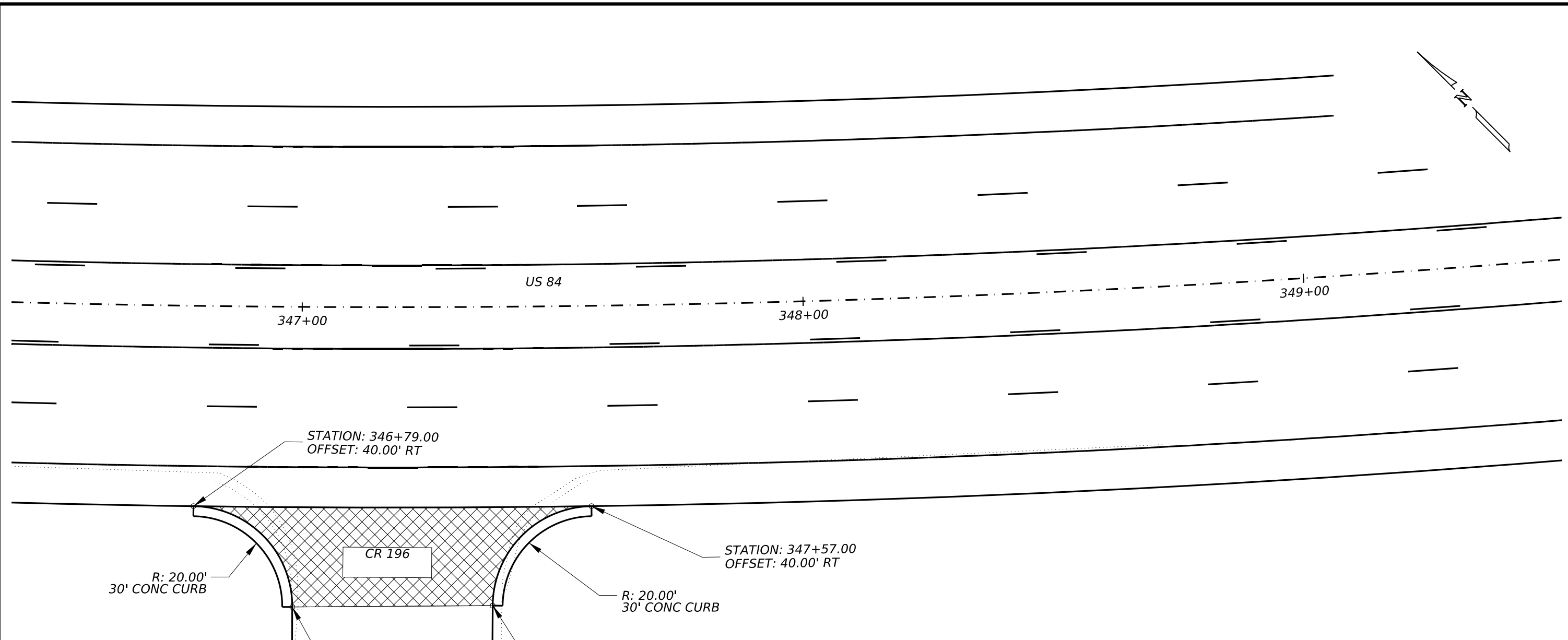
US 84 DRIVEWAY DETAILS



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		53

DATE: 1/11/2023 8:23:26 AM
FILE: ...Driveway_Summary1.dgn

DATE: 2/9/2023 2:08:44 PM
 FILE: ...CR 196 Intersection_Layout.dgn



ITEM	DESCRIPTION	QUANTITY	UNIT
104-6021	REMOVING CONC (CURB)	65	LF
529-6014	CONC CURB (TY 1 MOD)	60	LF
530-6002	INTERSECTION (ACP)	107	SY

NOTES:
 ASPHALT & AGGREGATE TYPES AND RATES SHALL MATCH THOSE FOUND ON THE PROPOSED BASIS OF ESTIMATE FOR ROADWAY OR AS DIRECTED BY THE ENGINEER



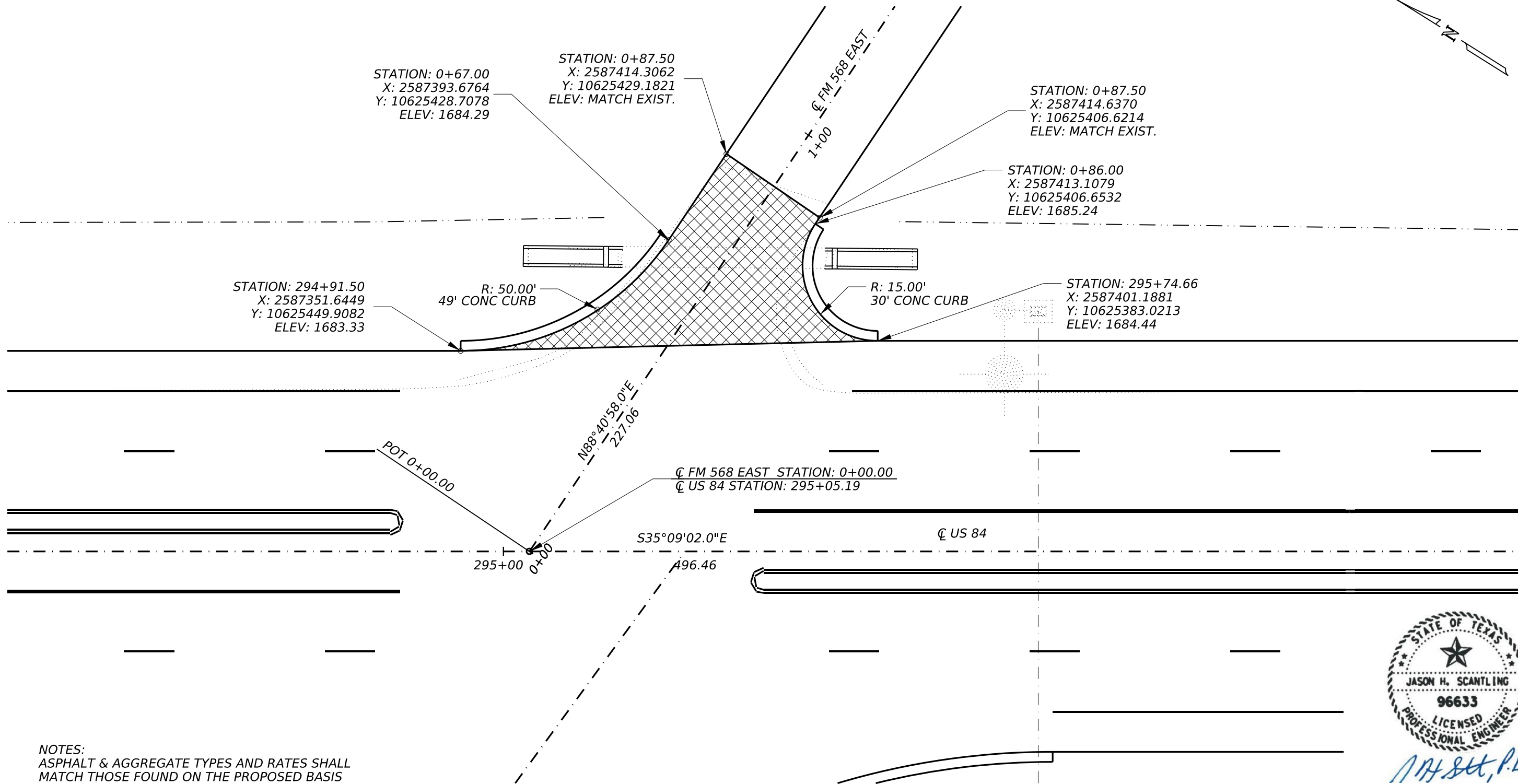
02/09/2023

**US 84 @
 CR 196
 INTERSECTION
 LAYOUT**



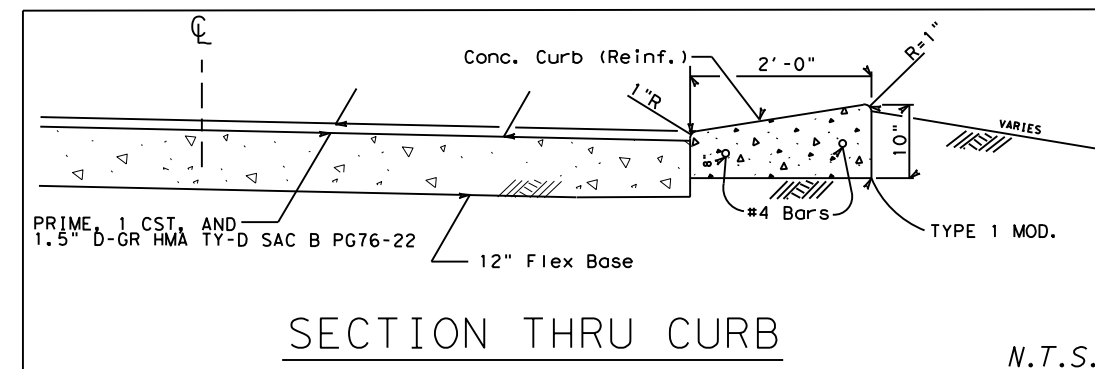
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0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	54	

DATE: 2/9/2023 1:56:25 PM
 FILE: ...FM_568_East_Intersection_Layout.dgn



NOTES:
 ASPHALT & AGGREGATE TYPES AND RATES SHALL
 MATCH THOSE FOUND ON THE PROPOSED BASIS
 OF ESTIMATE FOR ROADWAY OR AS
 DIRECTED BY THE ENGINEER

ITEM	DESCRIPTION	QUANTITY	UNIT
104-6021	REMOVING CONC (CURB)	75.0	LF
529-6014	CONC CURB (TY 1 MOD)	79.0	LF
530-6002	INTERSECTION (ACP)	129.0	SY



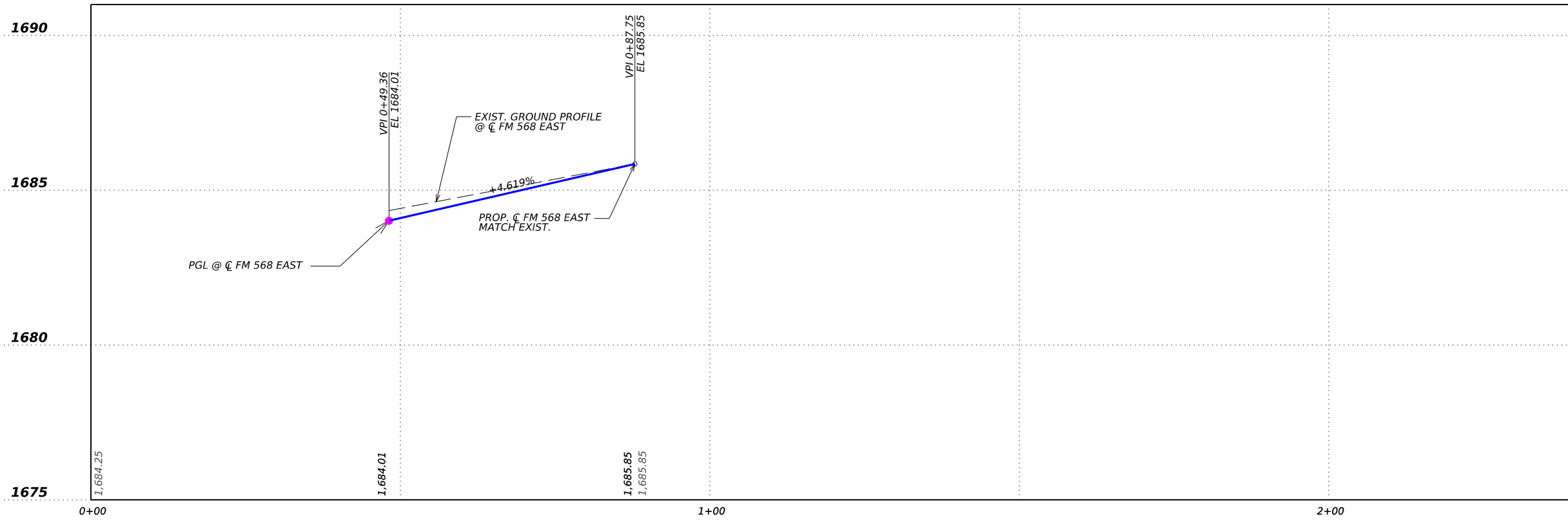
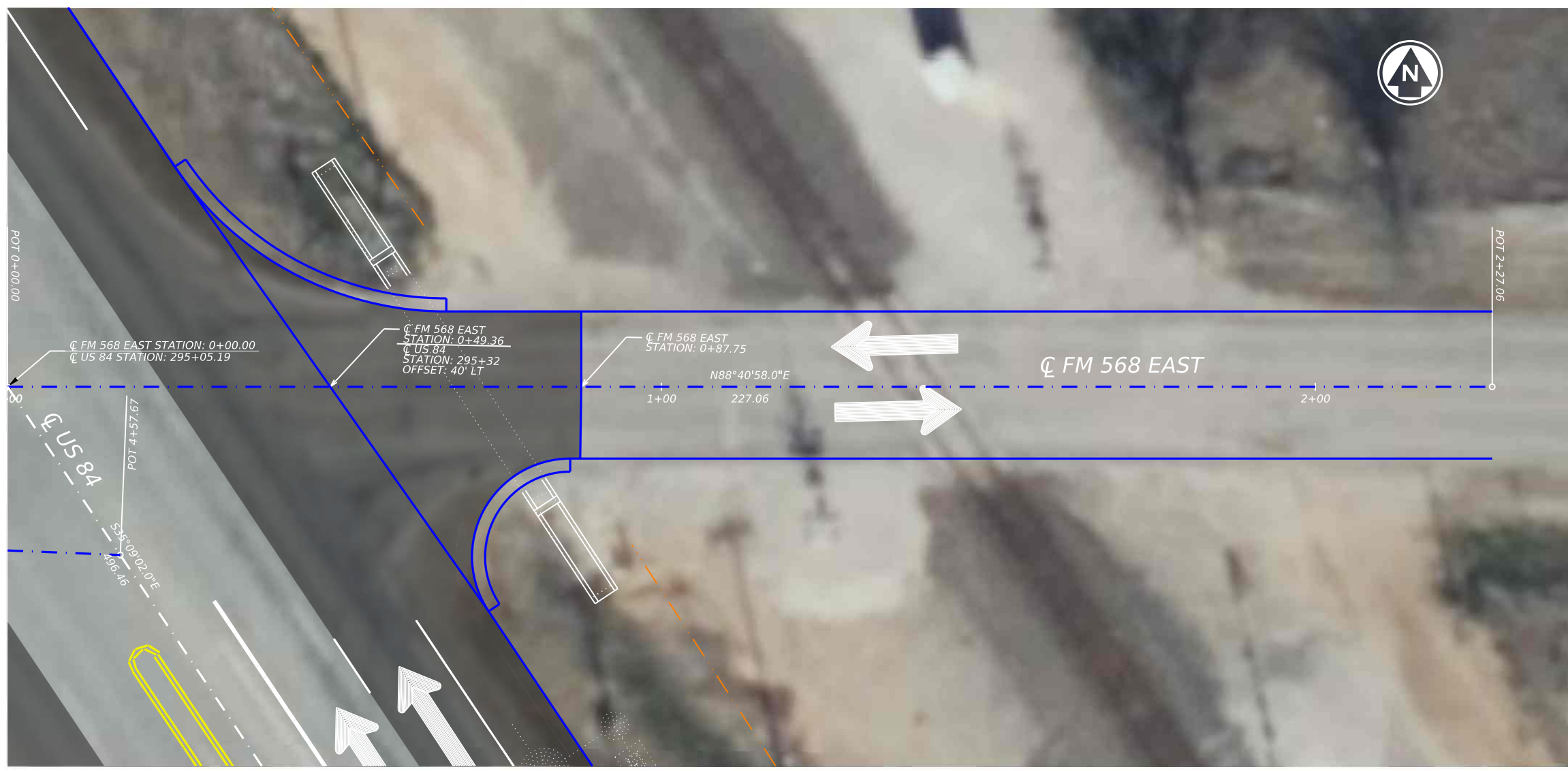
**US 84 @
 FM 568 EAST
 INTERSECTION
 LAYOUT**

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 Texas Department of Transportation

CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		55

DN: CK: DW: CK: CK:

DATE: 1/11/2023 2:24:27 PM
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01/26/2023

Texas Department of Transportation

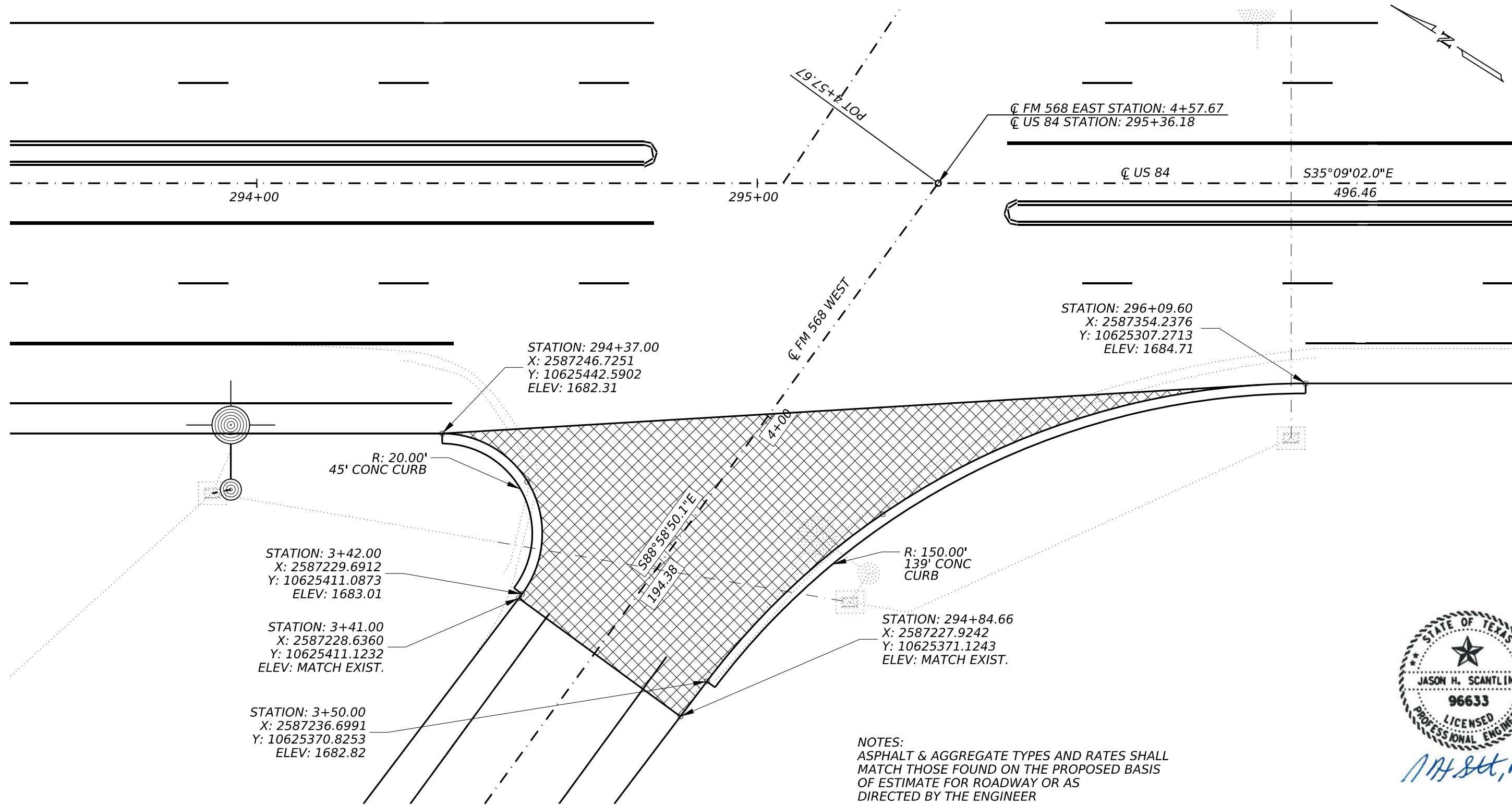
US 84

**FM 568 EAST INTERSECTION
 PLAN & PROFILE
 SCALE - 1:20**

SHEET 1 OF 1

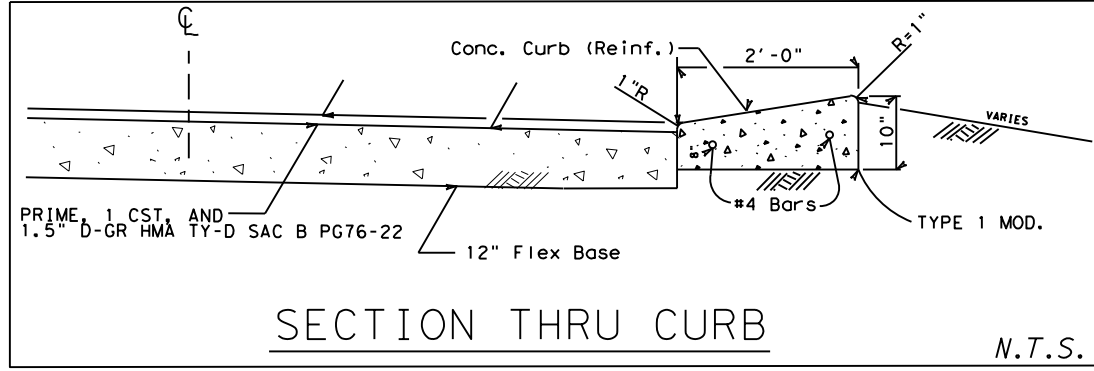
CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST		COUNTY	SHEET NO.
BWD		COLEMAN CO.	56

DATE: 2/9/2023 1:57:55 PM
 FILE: ...FM_568_West_Intersection_Layout.dgn



NOTES:
 ASPHALT & AGGREGATE TYPES AND RATES SHALL
 MATCH THOSE FOUND ON THE PROPOSED BASIS
 OF ESTIMATE FOR ROADWAY OR AS
 DIRECTED BY THE ENGINEER

ITEM	DESCRIPTION	QUANTITY	UNIT
104-6021	REMOVING CONC (CURB)	200.0	LF
529-6014	CONC CURB (TY 1 MOD)	184.0	LF
530-6002	INTERSECTION (ACP)	391.0	SY

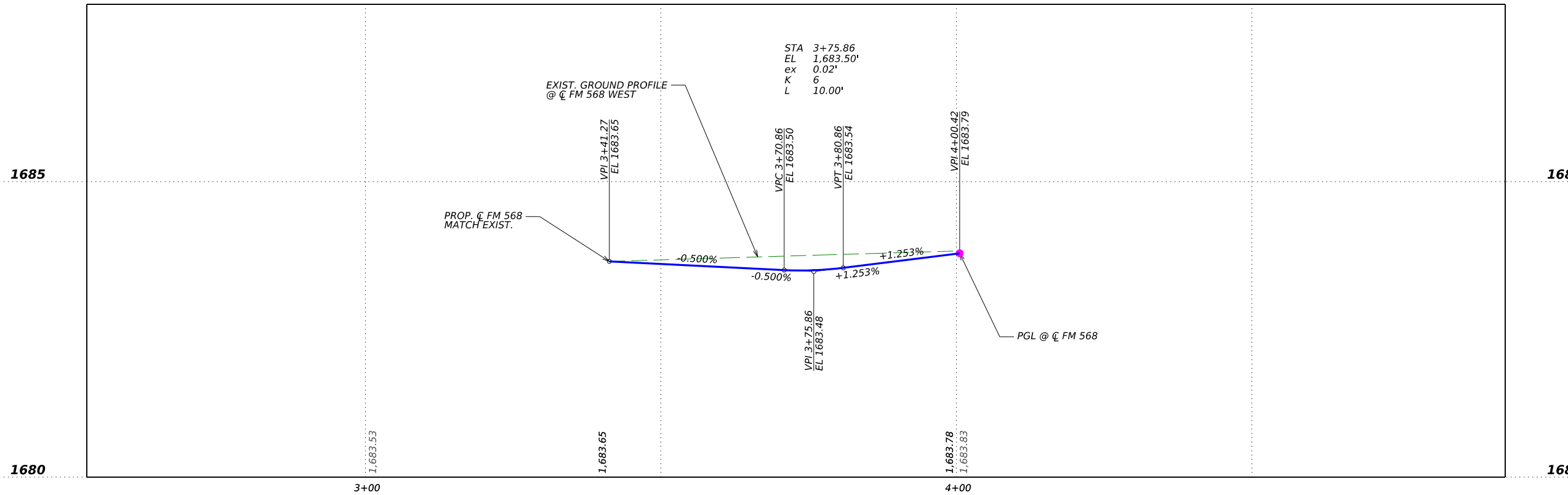
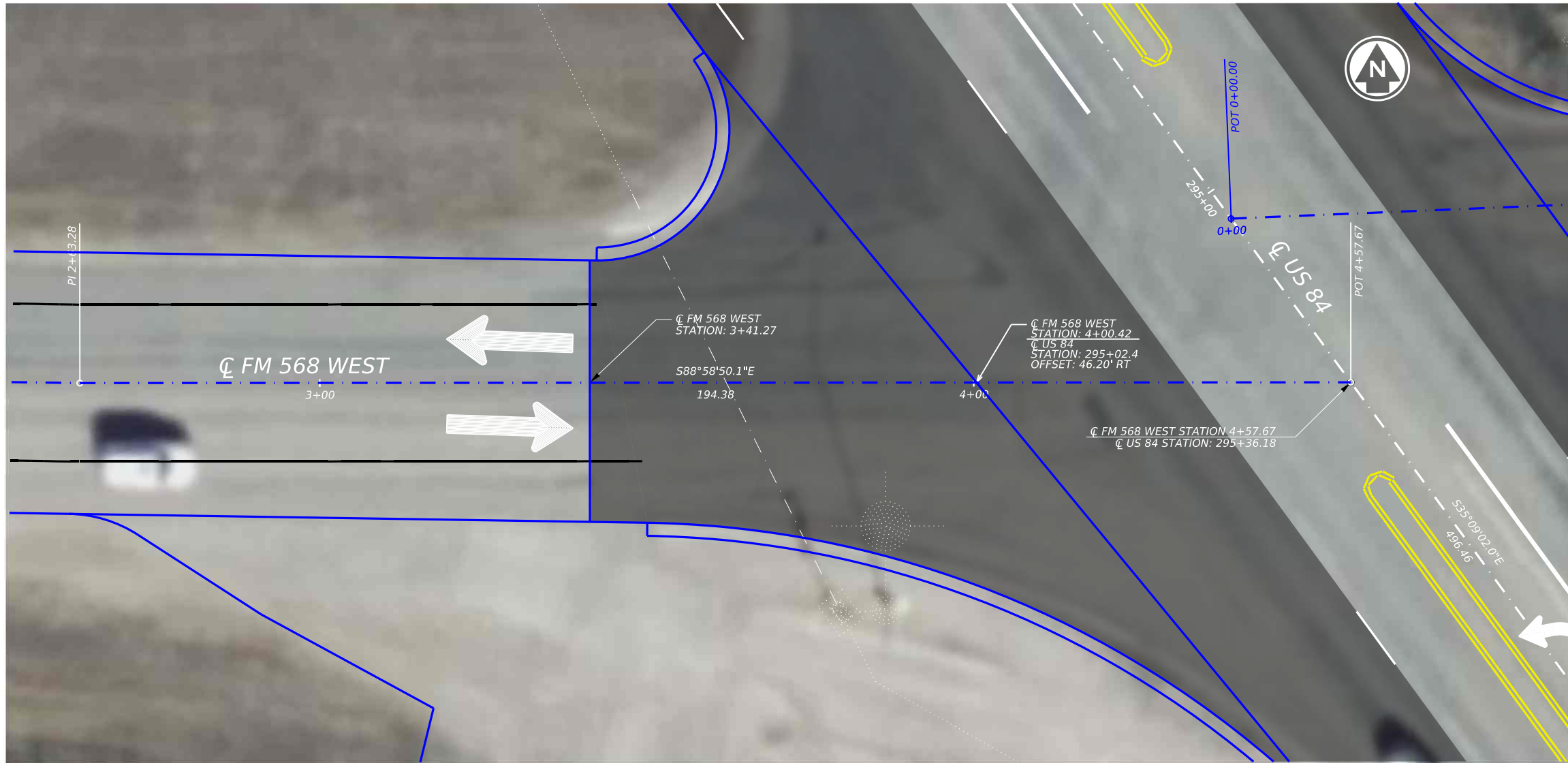



**US 84 @
 FM 568 WEST
 INTERSECTION
 LAYOUT**



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		57


DATE: 1/11/2023 1:55:14 PM
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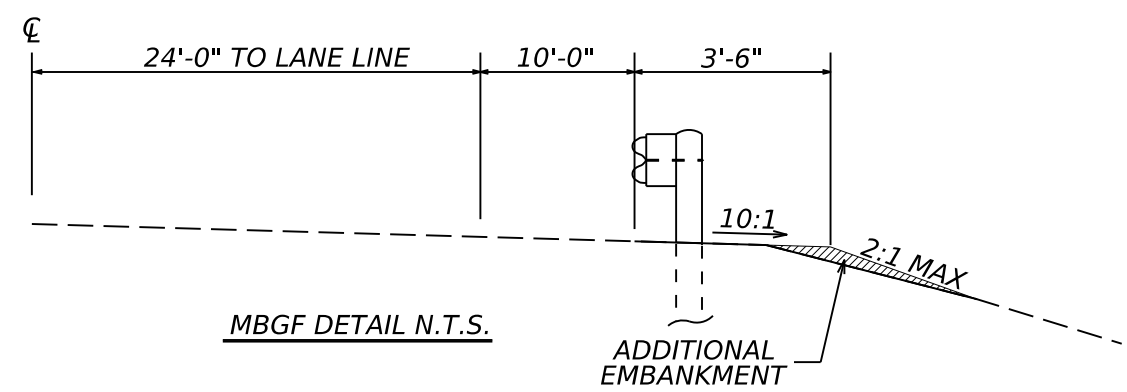
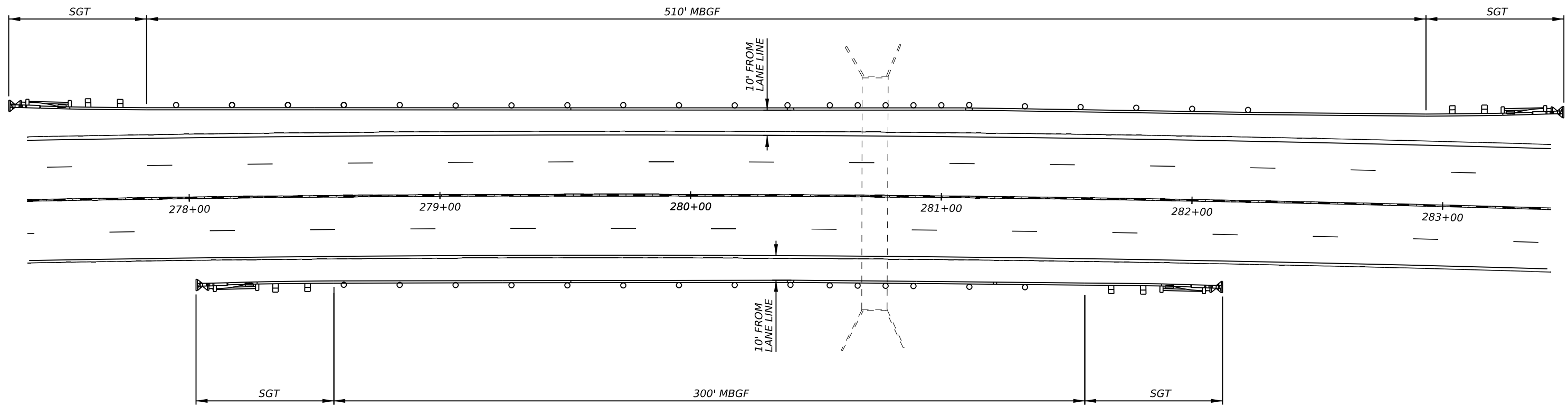
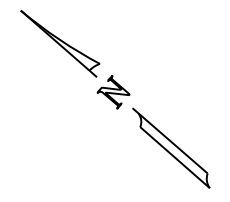
01/26/2023



US 84

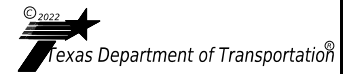
FM 568 WEST INTERSECTION
PLAN & PROFILE
SCALE - 1:20

CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST		COUNTY	SHEET NO.
BWD		COLEMAN CO.	58



ITEM	CODE	DESCRIPTION	QUANT	UNIT
540	6002	MTL W-BEAM GD FEN	810.0	LF
542	6001	REMOVE METAL BEAM GUARD FENCE	900.0	LF
544	6001	GUARDRAIL END TREATMENT (INSTALL)	4.0	EA
658	6062	INSTL DEL ASSM (D-SW)SZ1 (BRF) GF2 (BI)	22.0	EA

US 84
MBGF
CULVERT
LAYOUT

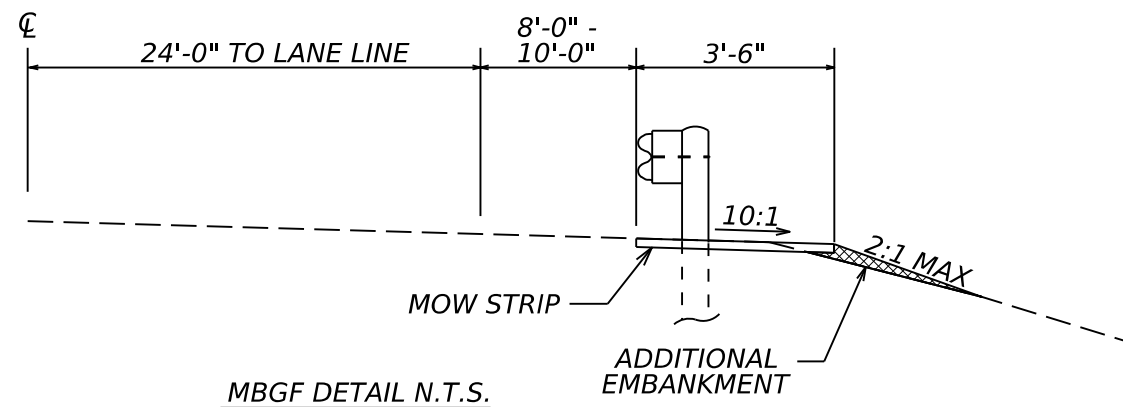
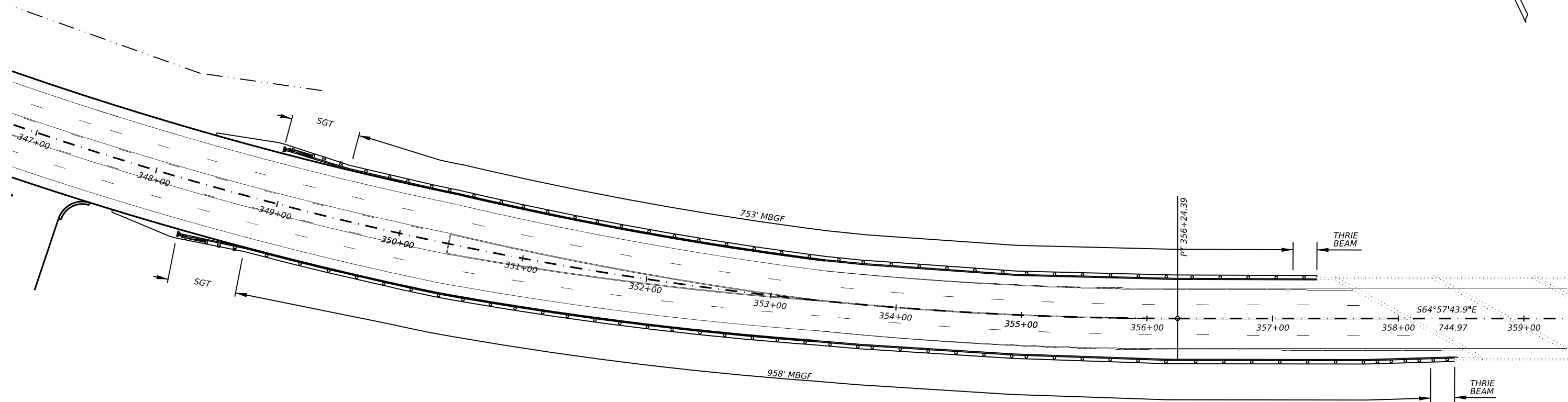
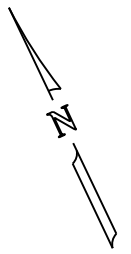


CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	59	

DATE: 2/27/2023 12:54:11 PM
FILE: ...MBGF_280+74_Layout.dgn

Sta. 280+74
9' X 9' 42" BOX CULVERT

PI 346+87.29
 Δ 39°07'44.4" (LT)
 D 02°00'09.5"
 T 1016.76'
 L 1953.86'
 R 2860.99'
 PC 336+70.53
 PT 356+24.39

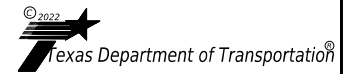


ITEM	CODE	DESCRIPTION	QUANT	UNIT
104	6054	REMOVING CONCRETE (MOW STRIP)	900.0	LF
432	6045	RIPRAP (MOW STRIP)(4 IN)	57.0	CY
540	6002	MTL W-BEAM GD FEN	1711.0	LF
540	6006	MTL BEAM GD FEN TRANS (THRIE-BEAM)	2.0	EA
542	6001	REMOVE METAL BEAM GUARD FENCE	1800.0	LF
544	6001	GUARDRAIL END TREATMENT (INSTALL)	2.0	EA
658	6062	INSTL DEL ASSM (D-SW)SZ1 (BRF) GF2 (BI)	35.0	EA



JH Scantling, P.E.

US 84
 MBGF
 BRIDGE
 LAYOUT

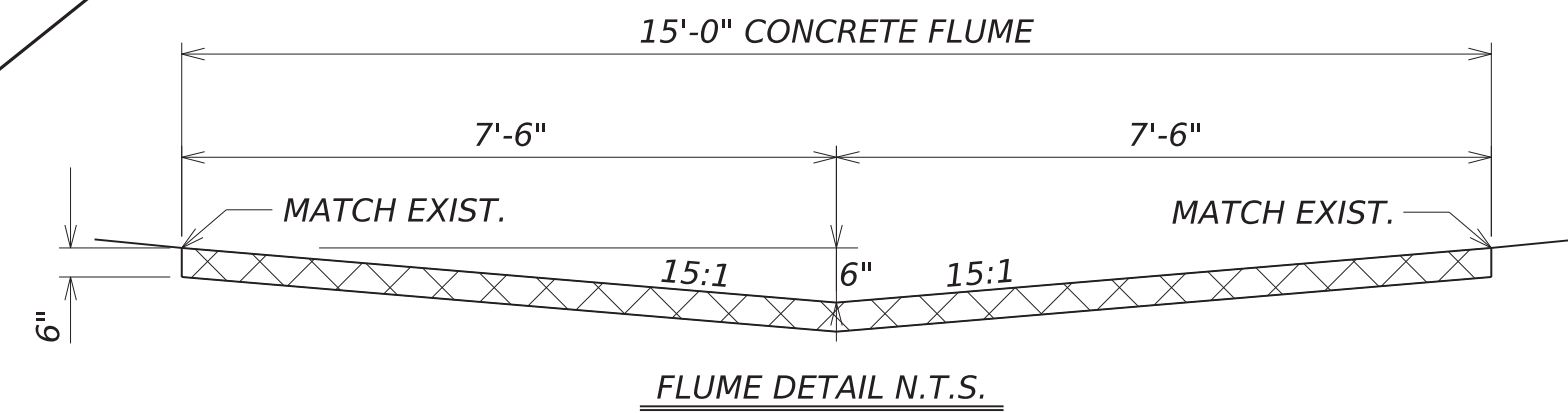
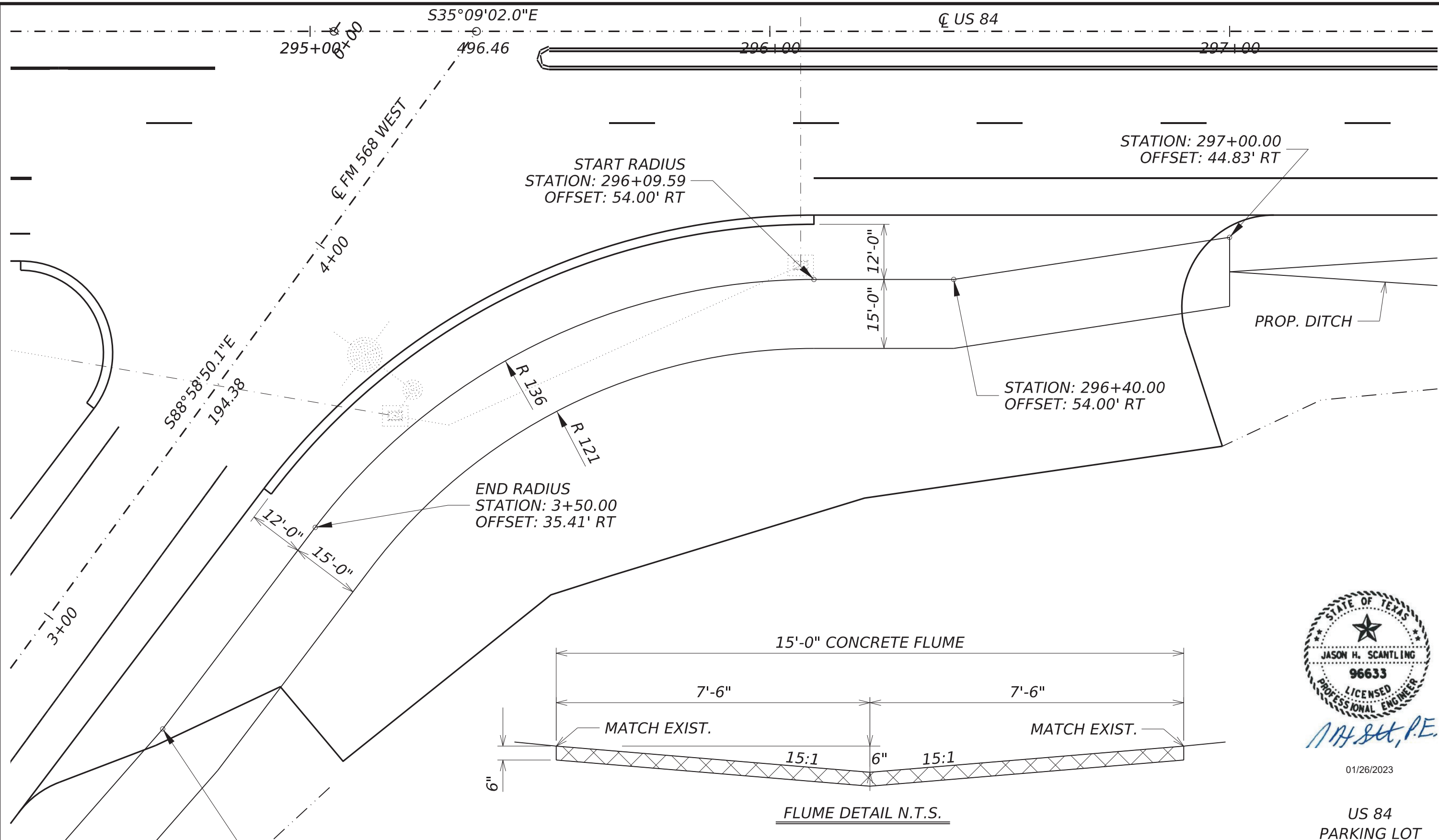


NOTE: EXISTING MOW STRIP TO REMAIN IN PLACE AND USED WITH PROPOSED METAL BEAM GUARD FENCE

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CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	60	

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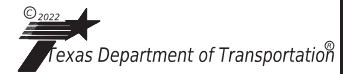
ITEM	CODE	DESCRIPTION	QUANT.	UNIT
432	6003	RIPRAP (CONC) (6 IN)	86.0	CY

NOTE: REINFORCEMENT WILL BE #4 BARS ON 18" CENTERS
 CLASS C CONCRETE WILL BE USED UNLESS OTHERWISE APPROVED



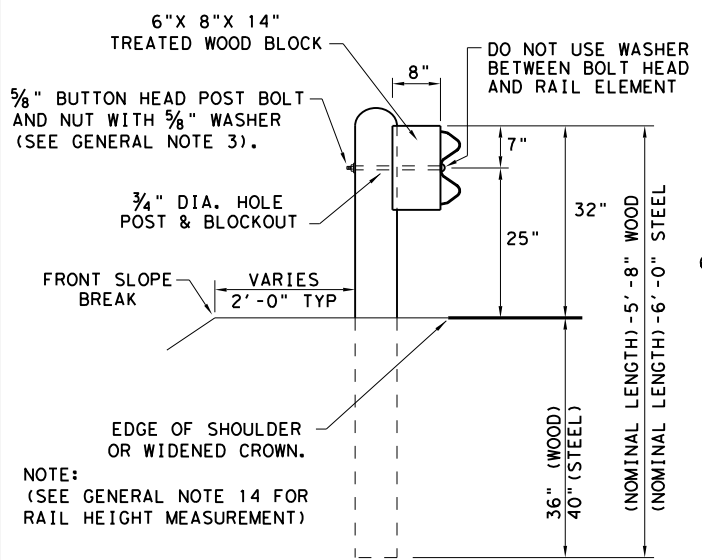
01/26/2023

US 84
 PARKING LOT
 FLUME LAYOUT
 1:20

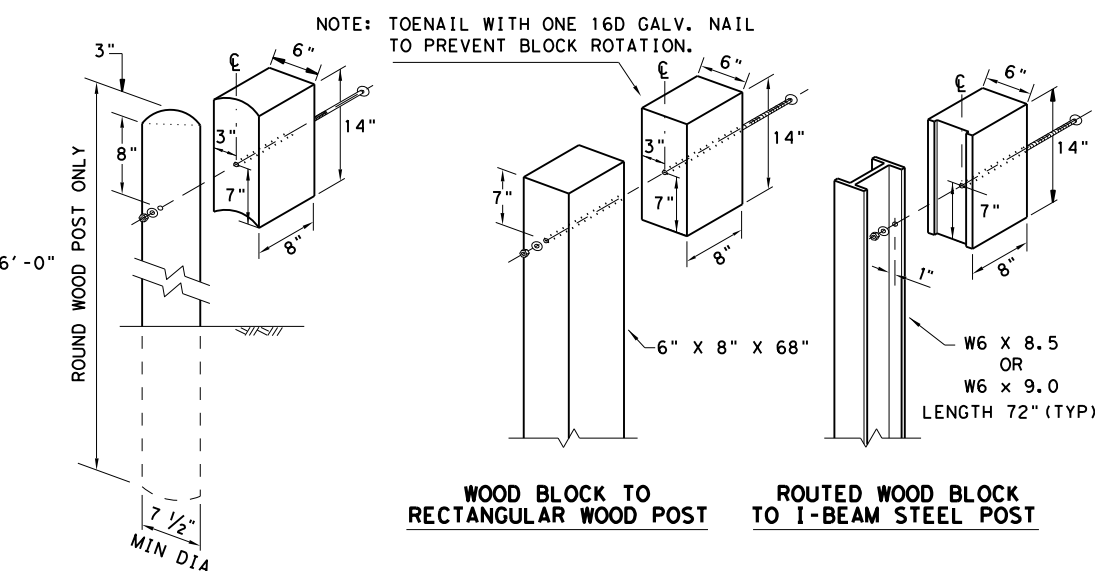


COUNTY	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	61	

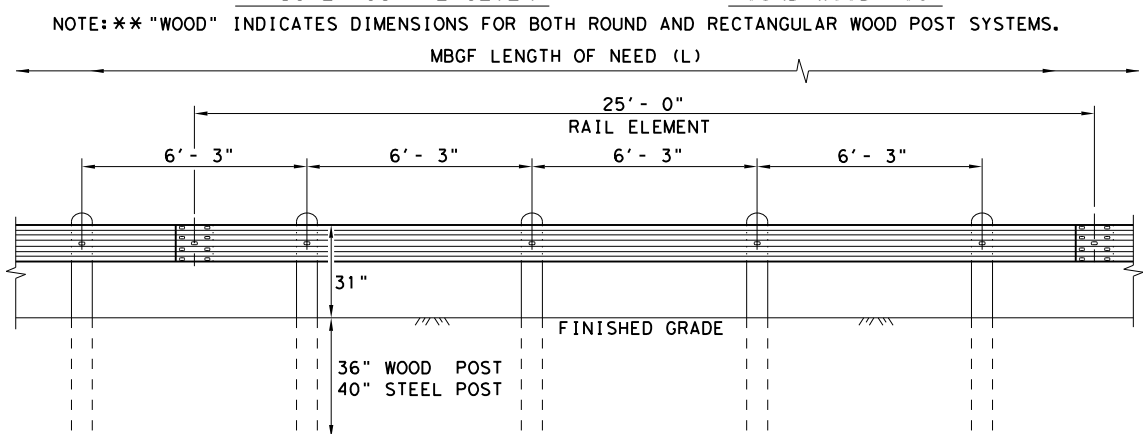
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 DISCLAIMER: THE USE OF THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.



TYPICAL POST PLACEMENT

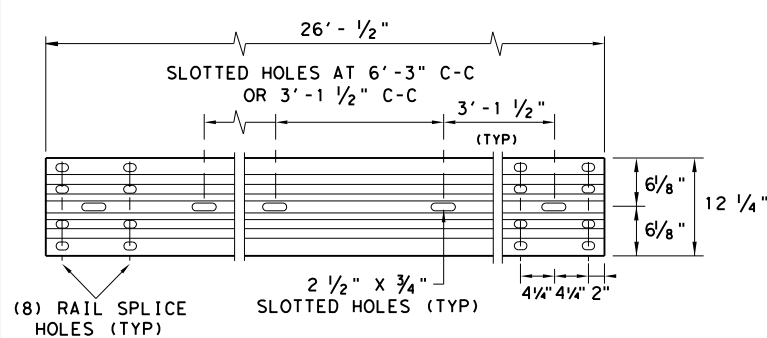


WOOD BLOCK TO ROUND WOOD POST **ROUTED WOOD BLOCK TO I-BEAM STEEL POST**



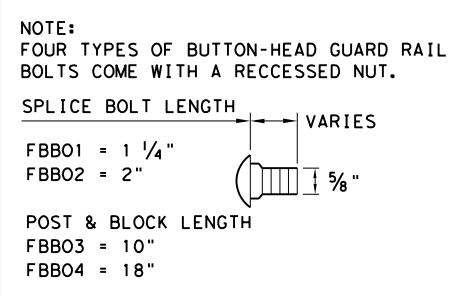
ELEVATION MID-SPAN RAIL SPLICE

SHOWING A 25' - 0" SECTION OF W-BEAM RAIL. (SEE GENERAL NOTE 2)



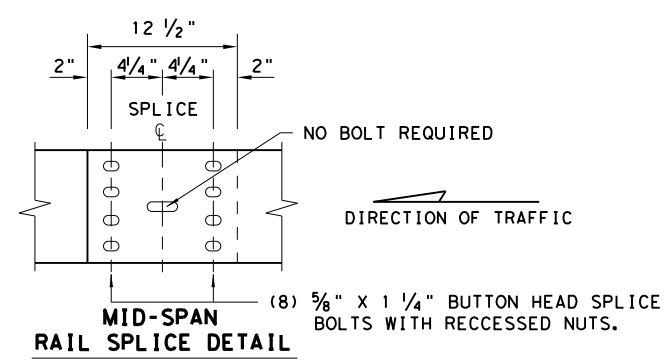
ELEVATION 25' - 0" (NOM.) W-BEAM SECTION

NOTES: SEE GENERAL NOTE 2 FOR ALLOWABLE RAIL TYPES. SEE RAIL SPLICE DETAIL FOR REQUIRED HARDWARE.



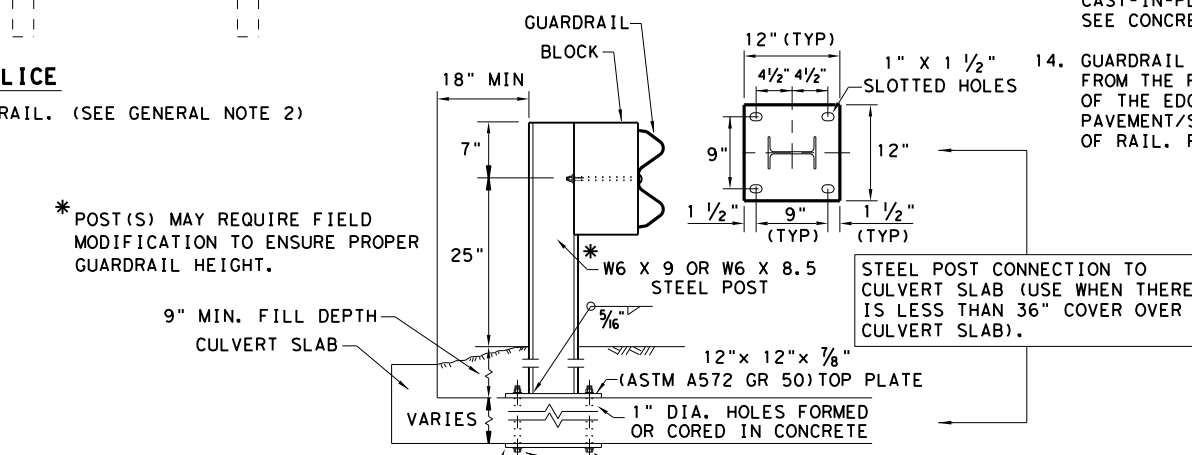
BUTTON HEAD BOLT

NOTE: SEE GENERAL NOTE 3 FOR SPLICE & POST BOLT DETAILS.



MID-SPAN RAIL SPLICE DETAIL

NOTE: GF(31), MID-SPAN RAIL SPLICES ARE REQUIRED WITH 6'-3" POST SPACINGS.



LOW FILL CULVERT POST

- NOTE: TWO INSTALLATION OPTIONS.
- BOLT-THROUGH OPTION:** REQUIRES A 6" MIN. SLAB THICKNESS. 7/8" DIA (ASTM A449) HEAVY HEX BOLTS WITH TWO HARDENED WASHER EACH AND HEAVY HEX NUTS. NOTE: BOLT LENGTH = SLAB PLUS 2 1/4" MIN.
 - EPOXY ANCHOR OPTION:** THIS OPTION MAY ONLY BE USED IF THE CULVERT SLAB IS 9" MIN. THICK. THREADED ANCHOR RODS MUST BE 7/8" DIA. ASTM A449 OR A193 GRADE B7 WITH HEAVY HEX NUT, AND ONE HARDENED WASHER EACH. EMBED ANCHOR RODS 6" WITH HILTI HIT RE 500 EPOXY ADHESIVE. OTHER TYPE III CLASS C EPOXY ADHESIVES MEETING THE REQUIREMENTS OF DMS-6100, "EPOXIES AND ADHESIVES", MAY BE USED IF IT CAN BE DEMONSTRATED THAT THEY MEET OR EXCEED THE STRENGTH OF HILTI HIT RE 500 WITH THE SAME EMBEDMENT DEPTH AND THREADED ROD DIA. FOLLOW THE MANUFACTURER'S REQUIREMENTS FOR INSTALLING EPOXIED THREADED RODS. EXTEND RODS 1/4" MIN. BEYOND NUT.

NOTE: CULVERTS OF 25 FT. OR LESS, SEE GF(31)LS STANDARD FOR "LONG SPAN" OPTION.

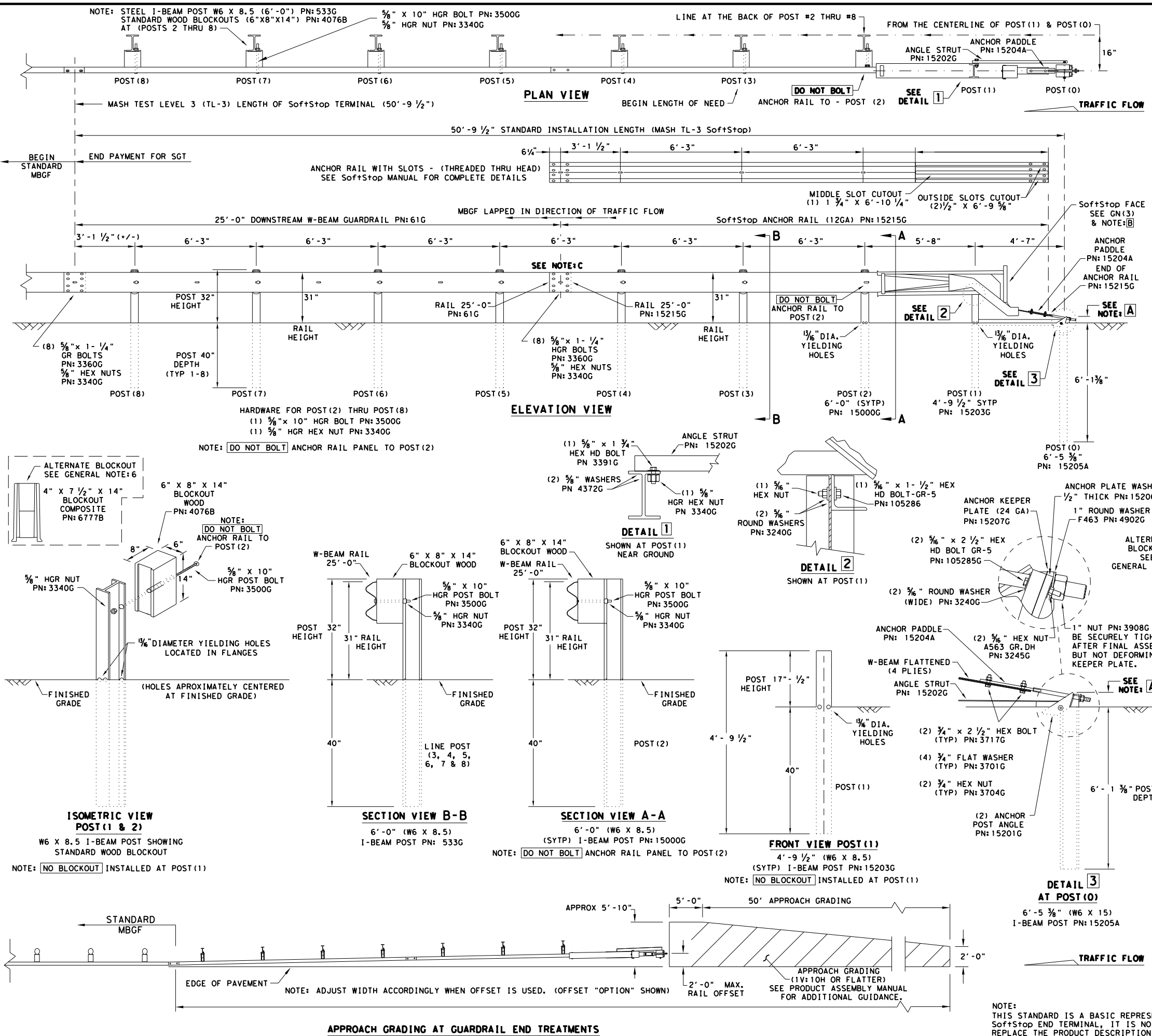
GENERAL NOTES

- THE TYPE OF POST (ROUND WOOD POST, RECTANGULAR WOOD POST, OR STEEL POST) WILL BE AS SHOWN IN THE PLANS. THE EXACT POSITION OF MBGF SHALL BE SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. STEEL POSTS TO BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING."
- RAIL ELEMENTS SHALL MEET THE REQUIREMENTS OF ITEM 540, "METAL BEAM GUARD FENCE" EXCEPT AS MODIFIED IN THE PLANS. THE CONTRACTOR MAY FURNISH RAIL ELEMENTS OF 25'-0", OR 12'-6" (NOM.) LENGTHS. RAIL ELEMENTS MAY HAVE SLOTTED HOLES AT 3'-1 1/2" C-C OR 6'-3" C-C. A SPECIAL LENGTH OF RAIL MAY BE MANUFACTURED TO ACCOMMODATE THE DOWNSTREAM ANCHOR TERMINAL (DAT) AND THE TRANSITION SECTIONS OF GUARDRAIL.
- BUTTON HEAD "POST BOLTS & NUTS" SHALL MEET THE REQUIREMENTS OF (ASTM A307), AND SHALL BE OF SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND 5/8" WASHER (FWC16G) AND NOT MORE THAN 1" BEYOND IT. TRIM REMAINING BOLT LENGTH TO MEET REQUIRED LENGTH.
- FITTINGS (BOLTS, NUTS, AND WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING." FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
- CROWN SHALL BE WIDENED TO ACCOMMODATE THE METAL BEAM GUARD FENCE.
- THE LATERAL APPROACH TO THE GUARD FENCE, SHALL HAVE A MAXIMUM SLOPE OF 1V:10H.
- IF SHOWN ELSEWHERE IN THE PLANS OR AS DIRECTED BY THE ENGINEER, THE GUARD FENCE MAY BE FLARED AT A RATE OF 25:1 OR FLATTER.
- UNLESS OTHERWISE SHOWN IN THE PLANS, GUARD FENCE PLACED IN THE VICINITY OF CURBS SHALL BE POSITIONED SO THAT THE FACE OF CURB IS LOCATED DIRECTLY BELOW OR BEHIND THE FACE OF THE RAIL. RAIL PLACED OVER CURBS SHALL BE INSTALLED SO THAT THE POST BOLT IS LOCATED APPROXIMATELY 25 INCHES ABOVE THE GUTTER PAN OR EDGE OF SHOULDER.
- APPLICATIONS IN SOLID ROCK ARE ONLY ALLOWED WITH STEEL POSTS. IF SOLID ROCK IS ENCOUNTERED WITHIN 0 TO 18" OF THE FINISHED GRADE, DRILL A 24" DIA. HOLE, 24" INTO THE ROCK. IF SOLID ROCK IS ENCOUNTERED BELOW 18", DRILL A 12" DIA. HOLE, 12" INTO THE ROCK OR TO THE STANDARD EMBEDMENT DEPTH, WHICHEVER MAYBE LESS. ANY EXCESS POST LENGTH, AFTER MEETING THESE DEPTHS, MAY BE FIELD CUT TO ENSURE PROPER GUARDRAIL MOUNTING HEIGHT. BACKFILL WITH COARSE AGGREGATE MATERIAL.
- POSTS SHALL NOT BE SET IN CONCRETE, OF ANY DEPTH.
- SPECIAL FABRICATION WILL BE REQUIRED AT INSTALLATION LOCATIONS HAVING A CURVATURE OF LESS THAN 150 FT. RADIUS.
- UNLESS OTHERWISE SHOWN IN THE PLANS, A COMPOSITE MATERIAL BLOCK THAT MEETS THE REQUIREMENTS OF DMS-7210, "COMPOSITE MATERIAL POSTS AND BLOCKS FOR METAL BEAM GUARD FENCE" MAY BE SUBSTITUTED FOR BLOCKS OF SIMILAR DIMENSIONS. THE CONSTRUCTION DIVISION, TXDOT MAINTAINS A MATERIAL PRODUCER LIST (MPL) FOR PRODUCERS OF MATERIALS CONFORMING TO DMS-7210 ONLY PRODUCERS ON THE MPL MAY FURNISH COMPOSITE MATERIAL BLOCKS.
- FOR THE LOW FILL CULVERT OPTION, POSTS LOCATED PARTIALLY OR WHOLLY BETWEEN PRECAST BOX CULVERT UNITS, THE USE OF A CAST-IN-PLACE CONCRETE CLOSURE BETWEEN BOXES IS REQUIRED. THE LENGTH OF THE CAST-IN-PLACE CONCRETE CLOSURE SHALL ACCOMMODATE THE PLACEMENT OF THE LOW FILL CULVERT OPTION. SEE CONCRETE CLOSURE DETAILS ON BRIDGE STANDARD SCP-MD.
- GUARDRAIL HEIGHT MEASUREMENT: WHEN THE GUARDRAIL IS LOCATED ABOVE PAVEMENT, MEASURE THE HEIGHT FROM THE PAVEMENT TO THE TOP OF THE W-BEAM RAIL. WHEN THE GUARDRAIL IS LOCATED UP TO 2 FT. OFF OF THE EDGE OF PAVEMENT OR FOR A PAVEMENT OVERLAY, USE A 10-FOOT STRAIGHTEDGE TO EXTEND THE PAVEMENT/SHOULDER SLOPE TO THE BACK OF RAIL, MEASURE FROM THE BOTTOM OF STRAIGHTEDGE TO THE TOP OF RAIL. FOR GUARDRAIL LOCATED DOWN A 10:1 SLOPE, MEASURE FROM THE NOMINAL TERRAIN.

NOTE: TRANSITIONS TO BRIDGE RAILS OR TRAFFIC BARRIERS. SEE GF(31)TL3 TR STANDARD FOR HIGH-SPEED TL-3 TRANSITIONS. SEE GF(31)TL2 TR STANDARD FOR LOW-SPEED TL-2 TRANSITIONS.

				Design Division Standard
METAL BEAM GUARD FENCE TL-3 MASH COMPLIANT GF(31)-19				
FILE: gf3119.dgn	DN: TXDOT	CK: KM	DW: VP	CK: CGL/AG
©TXDOT: NOVEMBER 2019	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
	DIST	COUNTY	SHEET NO.	
	BWD	COLEMAN CO.	62	

DATE: 2/22/2023
 FILE: T:\BWDSSGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196V4 - Design Standards\Roadway\sgt10s3116.dgn
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- GENERAL NOTES**
- FOR SPECIFIC INFORMATION REGARDING INSTALLATION AND TECHNICAL GUIDANCE OF THE SYSTEM, CONTACT: TRINITY HIGHWAY AT 1(888)323-6374, 2525 N. STEMMONS FREEWAY, DALLAS, TX 75207
 - FOR INSTALLATION, REPAIR AND MAINTENANCE REFER TO THE; SoftStop END TERMINAL, PRODUCT DESCRIPTION ASSEMBLY MANUAL. PN:620237B
 - APPLY HIGH INTENSITY REFLECTIVE SHEETING, "OBJECT MARKER" ON THE FRONT FACE OF THE DEVICE PER MANUFACTURER'S RECOMMENDATIONS. OBJECT MARKER SHALL CONFORM TO THE STANDARDS REQUIRED IN TEXAS MUTCD.
 - FOR POST (LEAVE-OUT) INSTALLATION AND GUIDANCE SEE TxDOT'S LATEST ROADWAY MOW STRIP STANDARD.
 - HARDWARE (BOLTS, NUTS, & WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING". FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
 - A COMPOSITE MATERIAL BLOCKOUT THAT MEETS THE REQUIREMENTS OF DMS-7210, MAY BE SUBSTITUTED FOR BLOCKOUTS OF SIMILAR DIMENSIONS. SEE CONSTRUCTION DIVISION MATERIAL PRODUCER LIST (MPL) FOR CERTIFIED PRODUCERS.
 - IF SOLID ROCK IS ENCOUNTERED SEE THE MANUFACTURER'S INSTALLATION MANUAL AND REFER TO THE LATEST ROADWAY MOW STRIP STANDARD FOR INSTALLATION GUIDANCE.
 - POSTS SHALL NOT BE SET IN CONCRETE.
 - IT IS ACCEPTABLE TO INSTALL THE SoftStop IMPACT HEAD PARALLEL TO THE GRADE LINE OR WITH AN UPWARD TILT.
 - DO NOT ATTACH THE SoftStop SYSTEM DIRECTLY TO A RIGID BARRIER.
 - UNDER NO CIRCUMSTANCES SHALL THE GUARDRAIL WITHIN THE SoftStop SYSTEM BE CURVED.
 - A FLARE RATE OF UP TO 25:1 MAY BE USED TO PREVENT THE TERMINAL HEAD FROM ENCRoaching ON THE SHOULDER. THE FLARE MAY BE DECREASED OR ELIMINATED FOR SPECIFIC INSTALLATIONS, IF DIRECTED BY THE ENGINEER.

NOTE: A THE INSTALLATION HEIGHT OF FULLY ASSEMBLED ANCHOR POST WILL VARY FROM 3'-3/4" MIN. TO 4" MAX. ABOVE FINISHED GRADE.

NOTE: B PART PN:5852B RIGHT-SIDE (HIGH INTENSITY REFLECTIVE SHEETING) PART PN:5851B LEFT-SIDE (HIGH INTENSITY REFLECTIVE SHEETING)

NOTE: C W-BEAM SPLICE LOCATED BETWEEN LINE POST (4) AND LINE POST (5) GUARDRAIL PANEL 25'-0" PN:61G ANCHOR RAIL 25'-0" PN:15215G LAP GUARDRAIL IN DIRECTION OF TRAFFIC FLOW.

PART	QTY	MAIN SYSTEM COMPONENTS
620237B	1	PRODUCT DESCRIPTION ASSEMBLY MANUAL (LATEST REV.)
15208A	1	SoftStop HEAD (SEE MANUAL FOR RIGHT-LEFT APPROACH)
15215G	1	SoftStop ANCHOR RAIL (12GA) WITH CUTOUT SLOTS
61G	1	SoftStop DOWNSTREAM W-BEAM RAIL (12GA) (25' - 0")
15205A	1	POST #0 - ANCHOR POST (6' - 5 3/8")
15203G	1	POST #1 - (SYTP) (4' - 9 1/2")
15000G	1	POST #2 - (SYTP) (6' - 0")
533G	6	POST #3 THRU #8 - I-BEAM (W6 X 8.5) (6' - 0")
4076B	7	BLOCKOUT - WOOD (ROUTED) (6" X 8" X 14")
6777B	7	BLOCKOUT - COMPOSITE (4" X 7 1/2" X 14")
15204A	1	ANCHOR PADDLE
15207G	1	ANCHOR KEEPER PLATE (24 GA)
15206G	1	ANCHOR PLATE WASHER (1/2" THICK)
15201G	2	ANCHOR POST ANGLE (10" LONG)
15202G	1	ANGLE STRUT

HARDWARE		
4902G	1	1" ROUND WASHER F436
3908G	1	1" HEAVY HEX NUT A563 GR.DH
3717G	2	3/4" X 2 1/2" HEX BOLT A325
3701G	4	3/4" ROUND WASHER F436
3704G	2	3/4" HEAVY HEX NUT A563 GR.DH
3360G	16	5/8" X 1 1/4" W-BEAM RAIL SPLICE BOLTS HGR
3340G	25	5/8" W-BEAM RAIL SPLICE NUTS HGR
3500G	7	5/8" X 10" HGR POST BOLT A307
3391G	1	5/8" X 1 3/4" HEX HD BOLT A325
4489G	1	5/8" X 9" HEX HD BOLT A325
4372G	4	5/8" WASHER F436
105285G	2	5/8" X 2 1/2" HEX HD BOLT GR-5
105286G	1	5/8" X 1 1/2" HEX HD BOLT GR-5
3240G	6	3/8" ROUND WASHER (WIDE)
3245G	3	5/8" HEX NUT A563 GR.DH
5852B	1	HIGH INTENSITY REFLECTIVE SHEETING - SEE NOTE: B

Design Division Standard

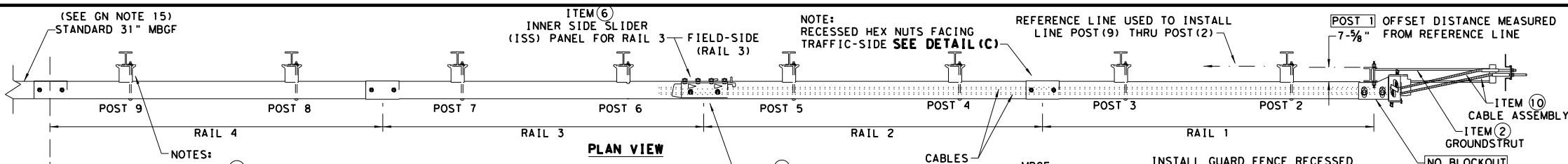
TRINITY HIGHWAY SOFTSTOP END TERMINAL MASH - TL-3 SGT (10S) 31-16

FILE: sgt10s3116	DW: TxDOT	CK: KM	DW: VP	CK: MB/VP
© TxDOT: JULY 2016	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
DIST	COUNTY	SHEET NO.		
BWD	COLEMAN CO.	63		

NOTE: THIS STANDARD IS A BASIC REPRESENTATION OF THE SoftStop END TERMINAL, IT IS NOT INTENDED TO REPLACE THE PRODUCT DESCRIPTION ASSEMBLY MANUAL.

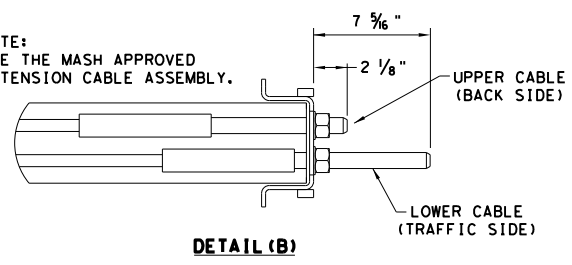
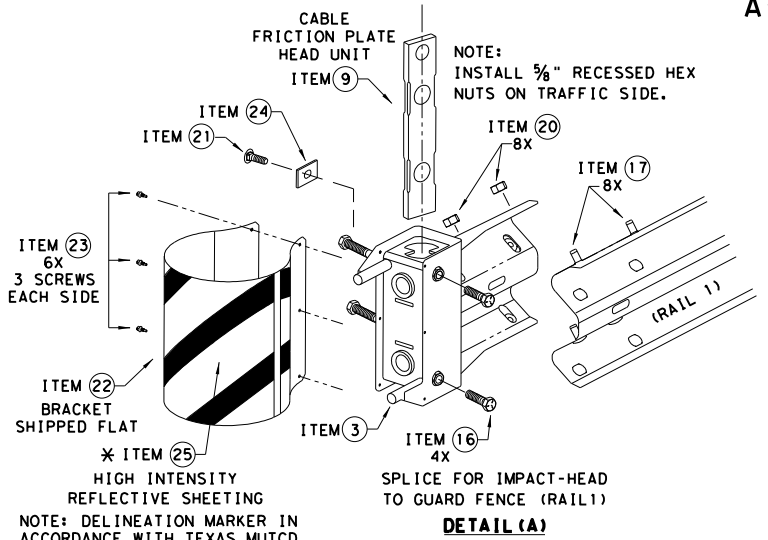
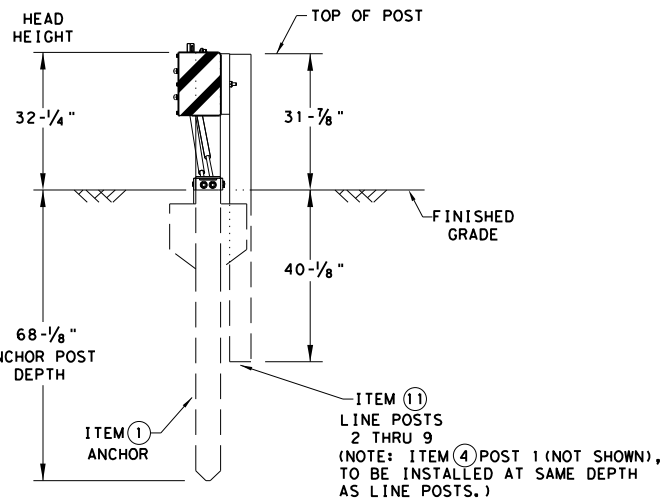
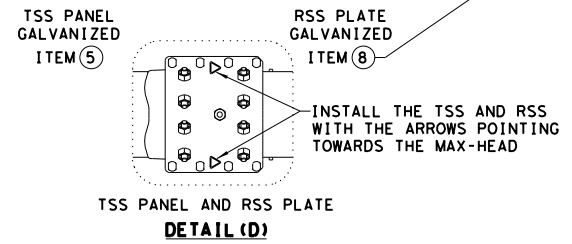
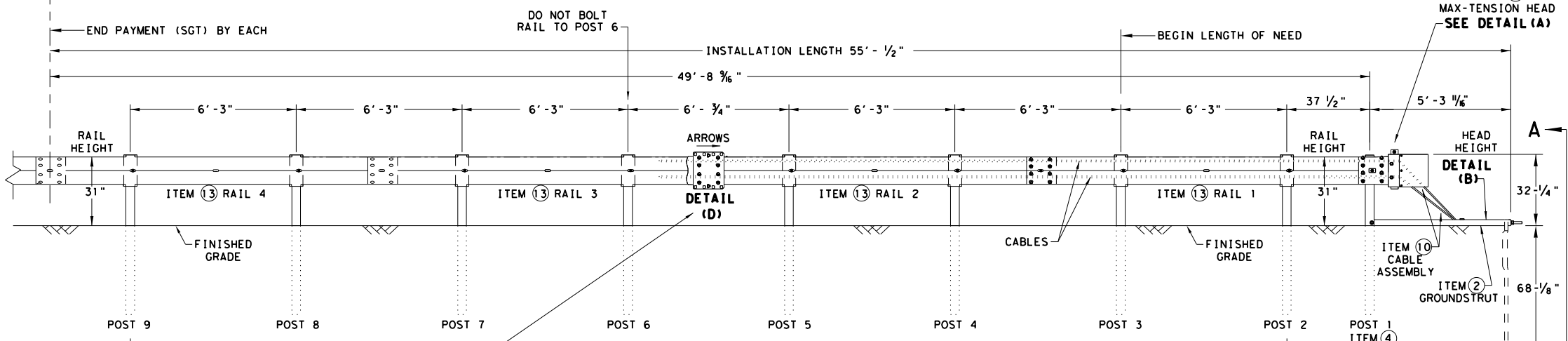
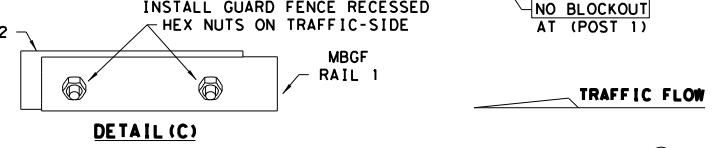
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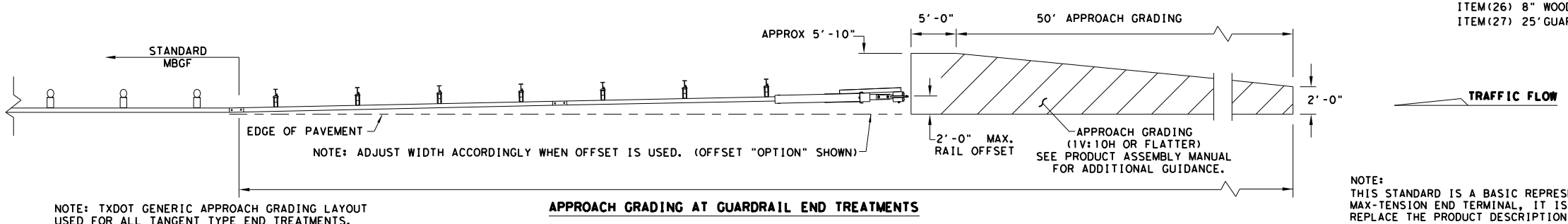
NOTES:
 1. ITEM (2) COMPOSITE BLOCKOUTS INSTALLED AT LINE POST (9) THRU LINE POST (2).
 2. DO NOT INSTALL A BLOCKOUT AT LINE POST (1).

ITEM (5) TRAFFIC SIDE SLIDER (TSS) PANEL FOR RAIL 2
 NOTE: SECURE THE (TSS) PANEL TO OUTSIDE OF RAIL 2 WITH THE PANEL ARROWS POINTING TOWARDS THE HEAD.



- GENERAL NOTES**
- FOR SPECIFIC INFORMATION REGARDING INSTALLATION AND TECHNICAL GUIDANCE OF THE SYSTEM, CONTACT: LINDSAY TRANSPORTATION SOLUTIONS (LTS) - BARRIER SYSTEMS, INC. AT (707) 374-6800
 - FOR INSTALLATION, REPAIR, & MAINTENANCE REFER TO THE: MAX-TENSION INSTALLATION INSTRUCTION MANUAL, P/N MANMAX REV D (ECN 3516).
 - APPLY HIGH INTENSITY REFLECTIVE SHEETING, "OBJECT MARKER" ON THE FRONT FACE OF THE DEVICE PER MANUFACTURER'S RECOMMENDATIONS. OBJECT MARKER SHALL CONFORM TO THE STANDARDS REQUIRED IN TEXAS MUTCD.
 - FOR POST (LEAVE-OUT) INSTALLATION AND GUIDANCE SEE TxDOT'S LATEST ROADWAY MOW STRIP STANDARD.
 - ALL STEEL COMPONENTS ARE GALVANIZED PER ASTM A123 OR EQUIVALENT UNLESS OTHERWISE STATED.
 - SYSTEM SHOWN USING STEEL WIDE FLANGE POST WITH COMPOSITE BLOCKOUTS.
 - COMPOSITE MATERIAL BLOCKOUT THAT MEETS THE REQUIREMENTS OF DMS-7210, MAY BE SUBSTITUTED FOR BLOCKOUTS SIMILAR DIMENSIONS. SEE CONSTRUCTION DIVISION MATERIAL PRODUCER LIST (MPL) FOR CERTIFIED PRODUCERS.
 - REFER TO INSTALLATION MANUAL FOR SPECIFIC PANEL LAPPING GUIDANCE.
 - IF SOLID ROCK IS ENCOUNTERED SEE THE MANUFACTURER'S INSTALLATION MANUAL FOR INSTALLATION GUIDANCE.
 - POSTS SHALL NOT BE SET IN CONCRETE.
 - A DRIVING CAP WITH A TIMBER OR PLASTIC INSERT SHALL BE USED WHEN DRIVING POST TO PREVENT DAMAGE TO THE GALVANIZING ON TOP OF THE POST.
 - MAX-TENSION SYSTEM SHALL NEVER BE INSTALLED WITHIN A CURVED SECTION OF GUARDRAIL.
 - IF A DELINEATION MARKER IS REQUIRED, MARKER SHALL BE IN ACCORDANCE WITH TEXAS MUTCD.
 - THE SYSTEM IS SHOWN WITH 12'-6" MBOF PANELS, 25'-0" MBOF PANELS ARE ALSO ALLOWED.
 - A MINIMUM OF 12'-6" OF 12GA. MBOF IS REQUIRED IMMEDIATELY DOWNSTREAM OF THE MAX-TENSION SYSTEM.

ITEM #	PART NUMBER	DESCRIPTION	QTY
1	BSI-1610060-00	SOIL ANCHOR - GALVANIZED	1
2	BSI-1610061-00	GROUND STRUT - GALVANIZED	1
3	BSI-1610062-00	MAX-TENSION IMPACT HEAD	1
4	BSI-1610063-00	W6x9 I-BEAM POST 6FT.-GALVANIZED	1
5	BSI-1610064-00	TSS PANEL - TRAFFIC SIDE SLIDER	1
6	BSI-1610065-00	ISS PANEL - INNER SIDE SLIDER	1
7	BSI-1610066-00	TOOTH - GEOMET	1
8	BSI-1610067-00	RSS PLATE - REAR SIDE SLIDER	1
9	B061058	CABLE FRICTION PLATE - HEAD UNIT	1
10	BSI-1610069-00	CABLE ASSEMBLY - MASH X-TENSION	2
11	BSI-1012078-00	X-LITE LINE POST-GALVANIZED	8
12	B090534	8" W-BEAM COMPOSITE-BLOCKOUT XT110	8
13	BSI-4004386	12'-6" W-BEAM GUARD FENCE PANELS 12GA.	4
14	BSI-1102027-00	X-LITE SQUARE WASHER	1
15	BSI-2001886	3/8" X 7" THREAD BOLT HH (GR.5)GEOMET	1
16	BSI-2001885	3/4" X 3" ALL-THREAD BOLT HH (GR.5)GEOMET	4
17	4001115	5/8" X 1 1/4" GUARD FENCE BOLTS (GR.2)MGAL	48
18	2001840	5/8" X 10" GUARD FENCE BOLTS MGAL	8
19	2001636	5/8" WASHER F436 STRUCTURAL MGAL	2
20	4001116	5/8" RECESSED GUARD FENCE NUT (GR.2)MGAL	59
21	BSI-2001888	3/8" X 2" ALL THREAD BOLT (GR.5)GEOMET	1
22	BSI-1701063-00	DELINEATION MOUNTING (BRACKET)	1
23	BSI-2001887	1/4" X 3/4" SCREW SD HH 410SS	7
24	4002051	GUARDRAIL WASHER RECT AASHTO FWRO3	1
25	SEE NOTE BELOW	HIGH INTENSITY REFLECTIVE SHEETING	1
26	4002337	8" W-BEAM TIMBER-BLOCKOUT, PDB01B	8
27	BSI-4004431	25' W-BEAM GUARDRAIL PANEL, 8-SPACE, 12GA.	2
28	MANMAX Rev- (D)	MAX-TENSION INSTALLATION INSTRUCTIONS	1



NOTE: TxDOT GENERIC APPROACH GRADING LAYOUT USED FOR ALL TANGENT TYPE END TREATMENTS.

NOTE:
 THIS STANDARD IS A BASIC REPRESENTATION OF THE MAX-TENSION END TERMINAL, IT IS NOT INTENDED TO REPLACE THE PRODUCT DESCRIPTION ASSEMBLY MANUAL.

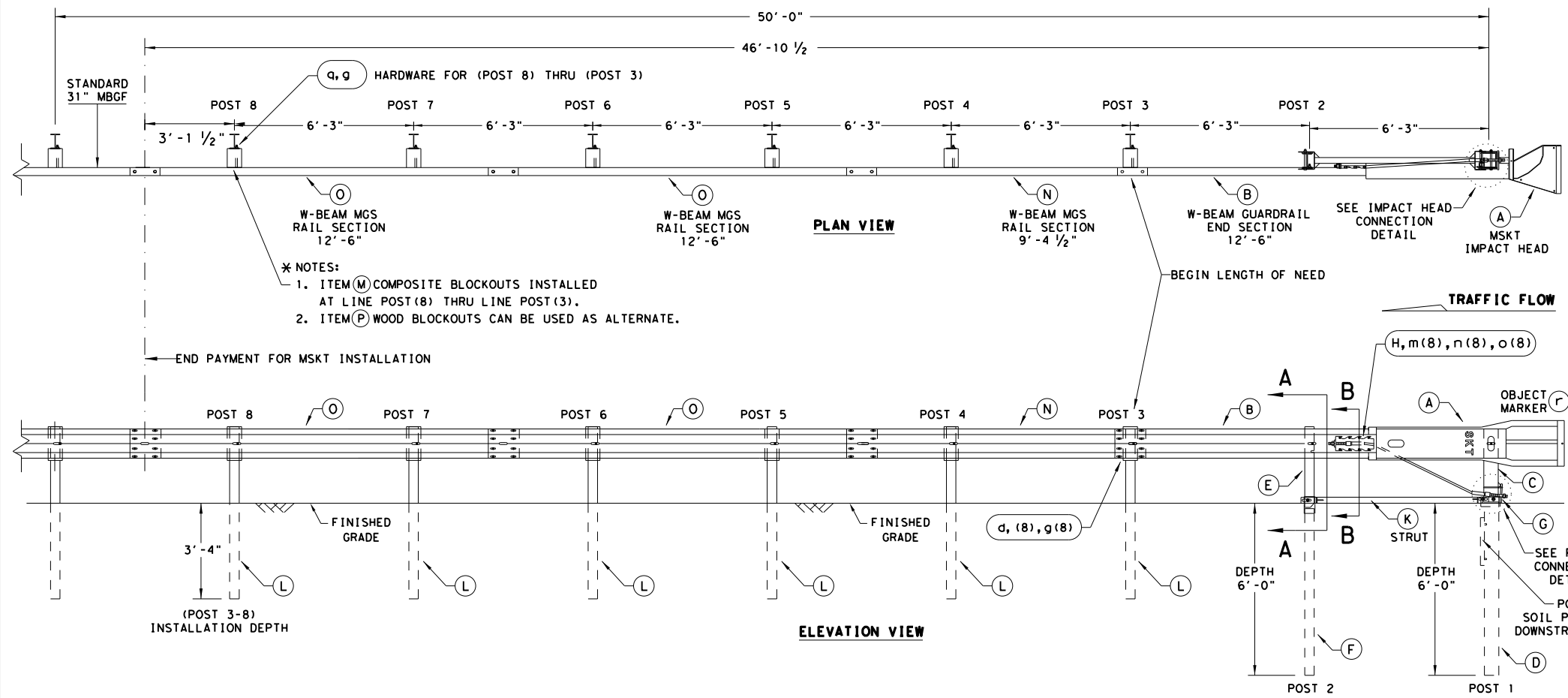
* TO BE PROVIDED BY DISTRIBUTOR OR CONTRACTOR.
 ** ALTERNATIVE ITEMS NOT SHOWN.
 ITEM (26) 8" WOOD-BLOCKOUTS
 ITEM (27) 25' GUARD FENCE PANELS

Texas Department of Transportation Design Division Standard

**MAX-TENSION END TERMINAL
 MASH - TL-3
 SGT (11S) 31-18**

FILE: sg11s3118.dgn	DN: TxDOT	CK: KM	DW: TxDOT	CK: CL
© TxDOT: FEBRUARY 2018	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
DIST	COUNTY		SHEET NO.	
BWD	COLEMAN CO.		64	

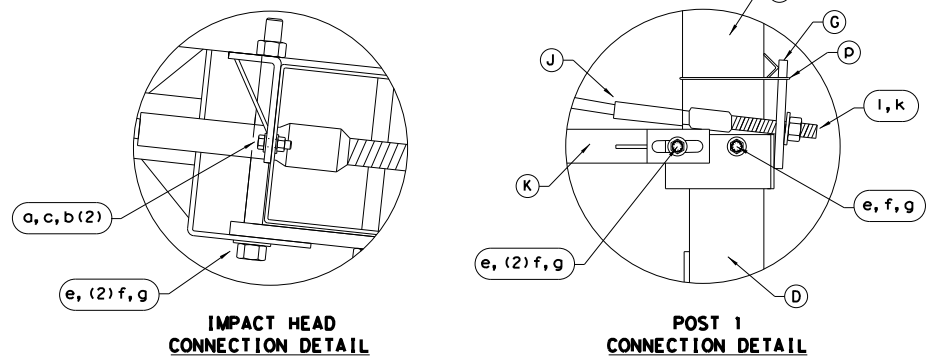
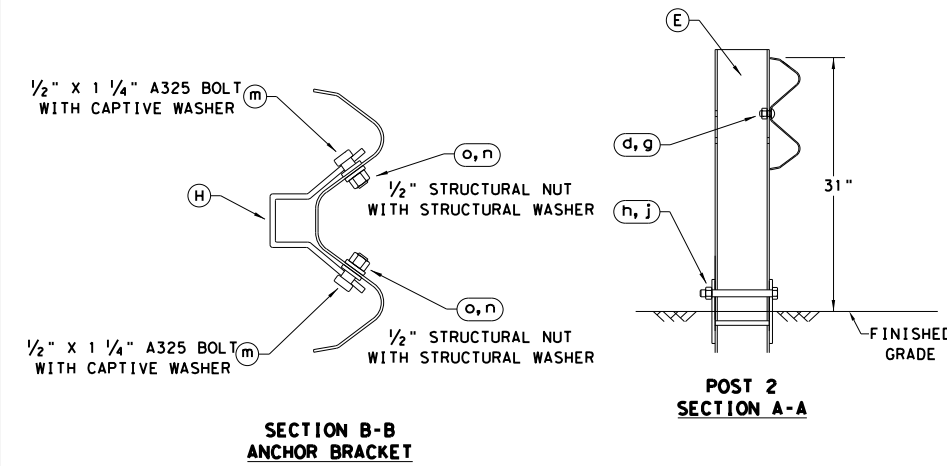
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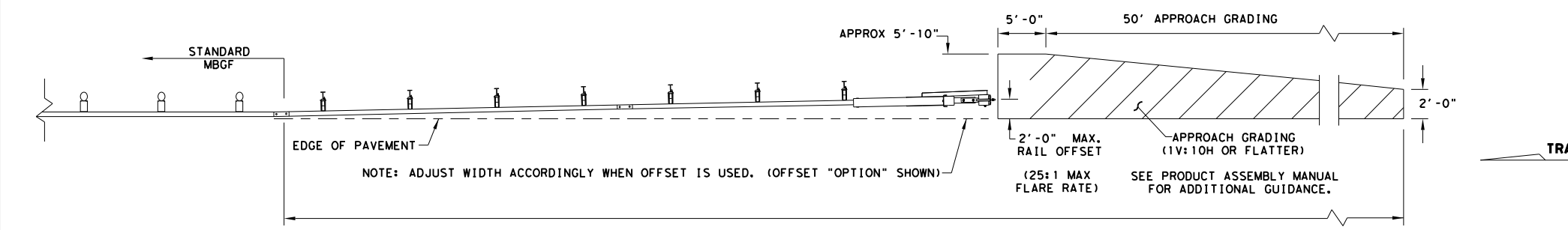
- * NOTES:**
- ITEM (M) COMPOSITE BLOCKOUTS INSTALLED AT LINE POST (8) THRU LINE POST (3).
 - ITEM (P) WOOD BLOCKOUTS CAN BE USED AS ALTERNATE.

- GENERAL NOTES**
- FOR SPECIFIC INFORMATION REGARDING INSTALLATION AND TECHNICAL GUIDANCE OF THE SYSTEM, CONTACT: ROAD SYSTEMS, INC. (432)263-2435. 3616 OLD HOWARD COUNTY AIRPORT, BIG SPRING, TX 79720
 - FOR INSTALLATION, REPAIR AND MAINTENANCE REFER TO THE: MSKT END TERMINAL, PRODUCT DESCRIPTION ASSEMBLY MANUAL (PUBLICATION-062717).
 - APPLY HIGH INTENSITY REFLECTIVE SHEETING, "OBJECT MARKER" ON THE FRONT FACE OF THE DEVICE PER MANUFACTURER'S RECOMMENDATIONS. OBJECT MARKER SHALL CONFORM TO THE STANDARDS REQUIRED IN TEXAS MUTCD.
 - FOR POST (LEAVE-OUT) INSTALLATION AND GUIDANCE SEE TXDOT'S LATEST ROADWAY MOW STRIP STANDARD.
 - HARDWARE (BOLTS, NUTS, & WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING". FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
 - SYSTEM SHOWN USING STEEL WIDE FLANGE POSTS WITH COMPOSITE BLOCKOUTS.
 - A COMPOSITE MATERIAL BLOCKOUTS THAT MEETS THE REQUIREMENTS OF DMS-7210, MAY BE SUBSTITUTED FOR BLOCKOUTS OF SIMILAR DIMENSIONS. SEE CONSTRUCTION DIVISION MATERIAL PRODUCER LIST (MPL) FOR CERTIFIED PRODUCERS.
 - IF SOLID ROCK IS ENCOUNTERED IN THE AREA OF (POST 1) AND / OR (POST 2) CONTACT THE MANUFACTURER, & REFER TO THE LATEST ROADWAY MBSGF STANDARD FOR INSTALLATION GUIDANCE.
 - POSTS SHALL NOT BE SET IN CONCRETE.
 - SYSTEM MUST BE ATTACHED TO STANDARD 31" MBSGF.
 - UNDER NO CIRCUMSTANCES SHALL THE GUARDRAIL WITHIN THE MSKT SYSTEM BE CURVED.
 - A FLARE RATE OF UP TO 25:1 MAY BE USED TO PREVENT THE TERMINAL HEAD FROM ENCRANCHING ON THE SHOULDER. THE FLARE MAY BE DECREASED OR ELIMINATED FOR SPECIFIC INSTALLATIONS, IF DIRECTED BY THE ENGINEER.
 - THE SYSTEM IS SHOWN WITH TWO 12'-6" MBSGF PANELS, ONE 25'-0" MBSGF PANEL IS ALSO ALLOWED IN ITS PLACE.
 - A DRIVING CAP WITH A TIMBER OR PLASTIC INSERT SHALL BE USED WHEN DRIVING POSTS 3-8 TO PREVENT DAMAGE TO THE GALVANIZING ON TOP OF THE POST. SPECIAL DRIVING CAP TO BE USED ON LOWER POSTS 1 & 2 TO PREVENT DAMAGE TO THE WELDED PLATES.

ITEM	QTY	MAIN SYSTEM COMPONENTS	ITEM NUMBERS
A	1	MSKT IMPACT HEAD	MS3000
B	1	W-BEAM GUARDRAIL END SECTION, 12 Go.	SF1303
C	1	POST 1 - TOP (6" X 6" X 1/8" TUBE)	MTPHP1A
D	1	POST 1 - BOTTOM (6' W6X15)	MTPHP1B
E	1	POST 2 - ASSEMBLY TOP	UHP2A
F	1	POST 2 - ASSEMBLY BOTTOM (6' W6X9)	HP2B
G	1	BEARING PLATE	E750
H	1	CABLE ANCHOR BOX	S760
J	1	BCT CABLE ANCHOR ASSEMBLY	E770
K	1	GROUND STRUT	MS785
L	6	W6X9 OR W6X8.5 STEEL POST	P621
M	6	COMPOSITE BLOCKOUTS	CBSP-14
N	1	W-BEAM MGS RAIL SECTION (9'-4 1/2")	G12025
O	2	W-BEAM MGS RAIL SECTION (12'-6")	G1203A
P	6	WOOD BLOCKOUT 6" X 8" X 14"	P675
Q	1	W-BEAM MGS RAIL SECTION (25'-0")	G1209
SMALL HARDWARE			
a	2	3/8" x 1" HEX BOLT (GRD 5)	B5160104A
b	4	3/8" WASHER	W0516
c	2	3/8" HEX NUT	N0516
d	25	3/8" Dia. x 1 1/4" SPLICE BOLT (POST 2)	B580122
e	2	3/8" Dia. x 9" HEX BOLT (GRD A449)	B580904A
f	3	3/8" WASHER	W050
g	33	3/8" Dia. H.G.R NUT	N050
h	1	3/4" Dia. x 8 1/2" HEX BOLT (GRD A449)	B340854A
j	1	3/4" Dia. HEX NUT	N030
k	2	1 ANCHOR CABLE HEX NUT	N100
l	2	1 ANCHOR CABLE WASHER	W100
m	8	1/2" x 1 1/4" A325 BOLT WITH CAPTIVE WASHER	SB12A
n	8	1/2" STRUCTURAL NUTS	N012A
o	8	1 1/8" O.D. x 3/8" I.D. STRUCTURAL WASHERS	W012A
p	1	BEARING PLATE RETAINER TIE	CT-100ST
q	6	3/8" x 10" H.G.R. BOLT	B581002
r	1	OBJECT MARKER 18" X 18"	E3151



ALTERNATIVE ITEMS NOT SHOWN. * *
 * ITEM (P) 8" WOOD-BLOCKOUT
 * * ITEM (Q) 25' GUARD FENCE PANEL



NOTE: TXDOT GENERIC APPROACH GRADING LAYOUT USED FOR ALL TANGENT TYPE END TREATMENTS.

NOTE: THIS STANDARD IS A BASIC REPRESENTATION OF THE MSKT END TERMINAL, IT IS NOT INTENDED TO REPLACE THE PRODUCT DESCRIPTION ASSEMBLY MANUAL.

Design Division Standard

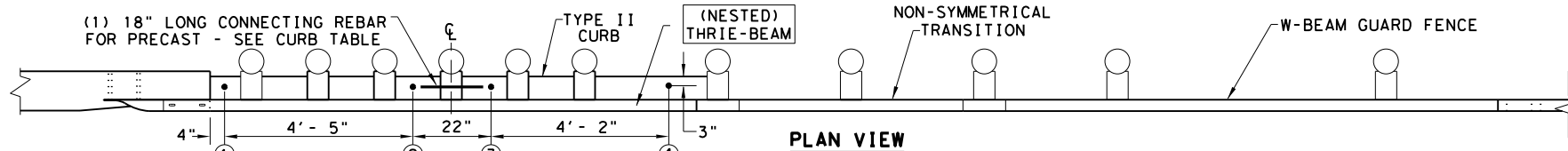
SINGLE GUARDRAIL TERMINAL

MSKT-MASH-TL-3

SGT (12S) 31-18

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© TXDOT: APRIL 2018	CONT	SECT	JOB	HIGHWAY
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	DIST	COUNTY	SHEET NO.	
	BWD	COLEMAN CO.	65	

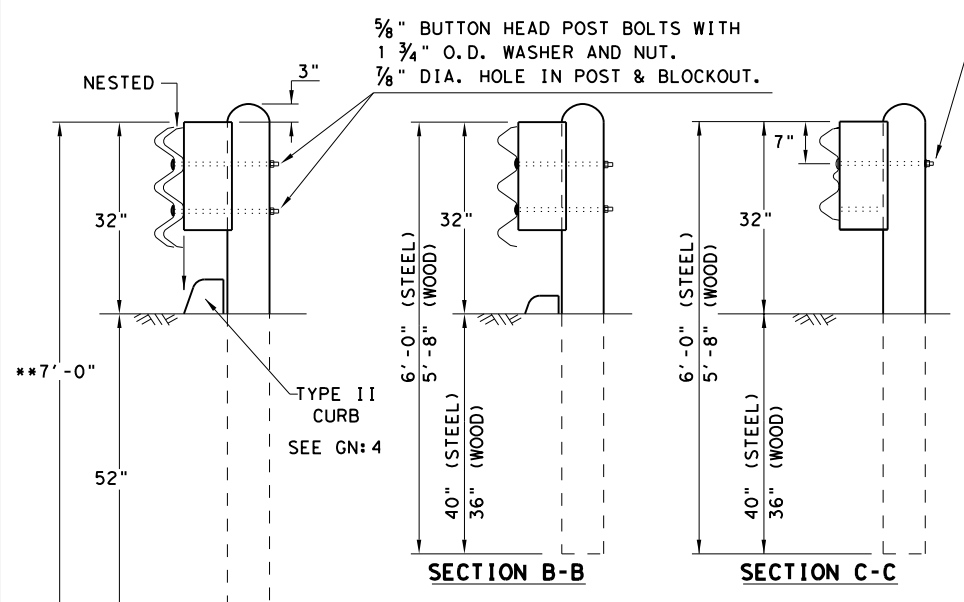
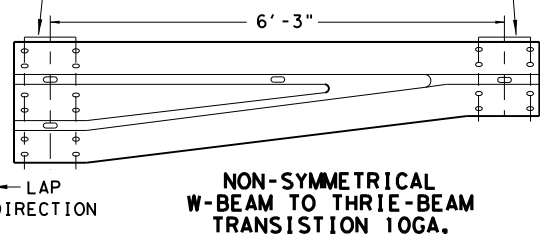
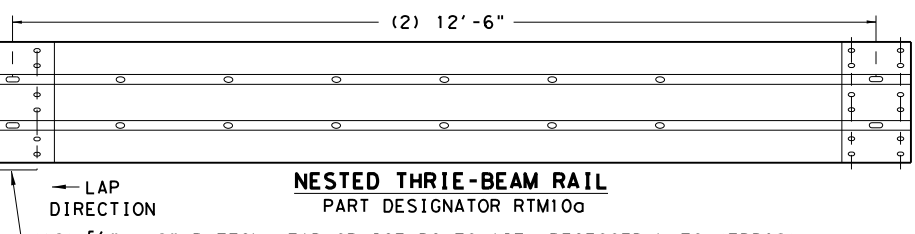
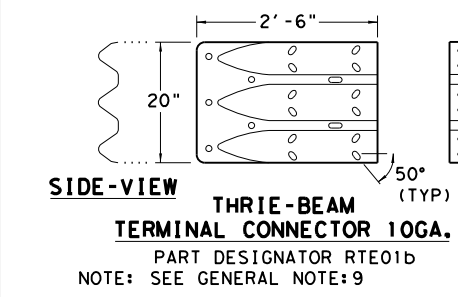
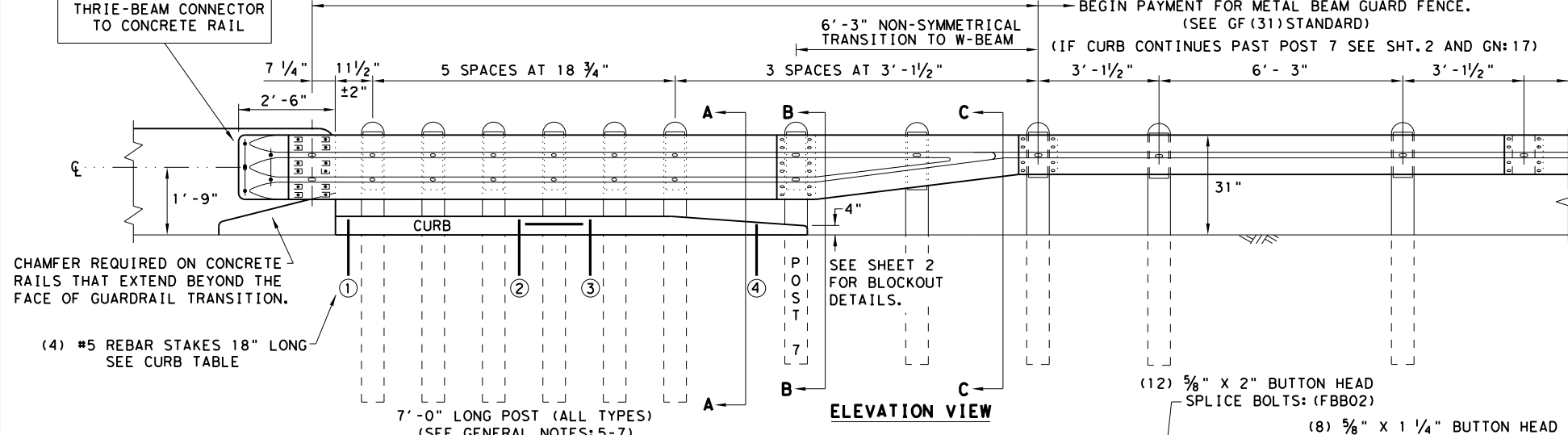
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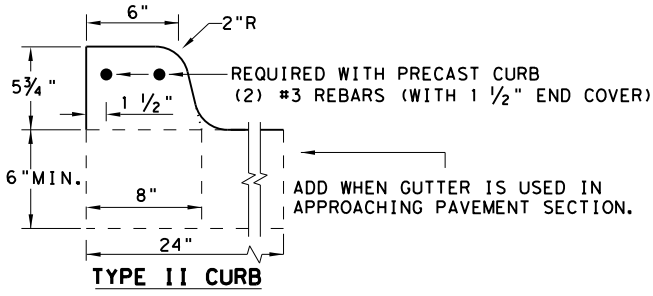
- (5) 1" DIA. HOLES.
- (5) 7/8" DIA. HEAVY HEX HEAD BOLTS (FACING TRAFFIC SIDE) (ASTM F3125 GR A325 OR A449).
- (10) 1 3/4" O.D. WASHER UNDER EACH HEX BOLT HEAD AND NUT.
- (5) 7/8" DIA. HEAVY HEX NUTS (ASTM A194 OR A563).

NOTE:
HEAVY HEX BOLT LENGTH WILL VARY DEPENDING ON WIDTH CONCRETE RAIL, LEAVE 1" OF BOLT LENGTH PAST THE 7/8" HEX NUT. TRIM AS REQUIRED.

NOTE:
CURB IS A REQUIRED COMPONENT FOR THE TRANSITION TO FUNCTION PROPERLY. SEE GENERAL NOTES: 2-4 AND 16-17.



THRIE-BEAM TERMINAL - CURB TABLE	
PRECAST CURB FULL LENGTH EQUALS 12'- 2"	
THE PRECAST CURB MAY BE FORMED INTO TWO SECTIONS.	
CURB (1) LENGTH	5'- 8"
CURB (2) LENGTH	6'- 6"
TAPER CURB (2) TO A HEIGHT OF 4" AT POST 7	
CONNECTING PRECAST CURB SECTIONS (1) & (2):	
FORM OR CORE	1" DIA. HOLE 9" LONG INTO EACH CURB END.
USE	(1) #5 GR.60 REBAR 18" LONG TO CONNECT BOTH CURBS.
SECURING PRECAST OR CAST-IN-PLACE TO FINISHED GRADE *:	
FORM OR CORE	(4) 1" DIA. HOLES, SEE PLAN AND ELEVATION VIEWS FOR HOLE LOCATIONS. DRIVE (4) #5 GR.60 REBAR STAKES 18" LONG INTO THE GROUND AND 1/2" BELOW TOP OF CURB.
FILL HOLES	WITH APPROVED GROUT MIXTURE.



* NOTES: NOT NEEDED FOR CAST-IN-PLACE. SEE TYPE II CURB DETAIL FOR REBAR AND COVER REQUIREMENTS. PERCUSSION DRILLING IS NOT PERMITTED WITH: TYPE II CURB, BRIDGE RAIL OR CONCRETE TRAFFIC RAIL.

GENERAL NOTES

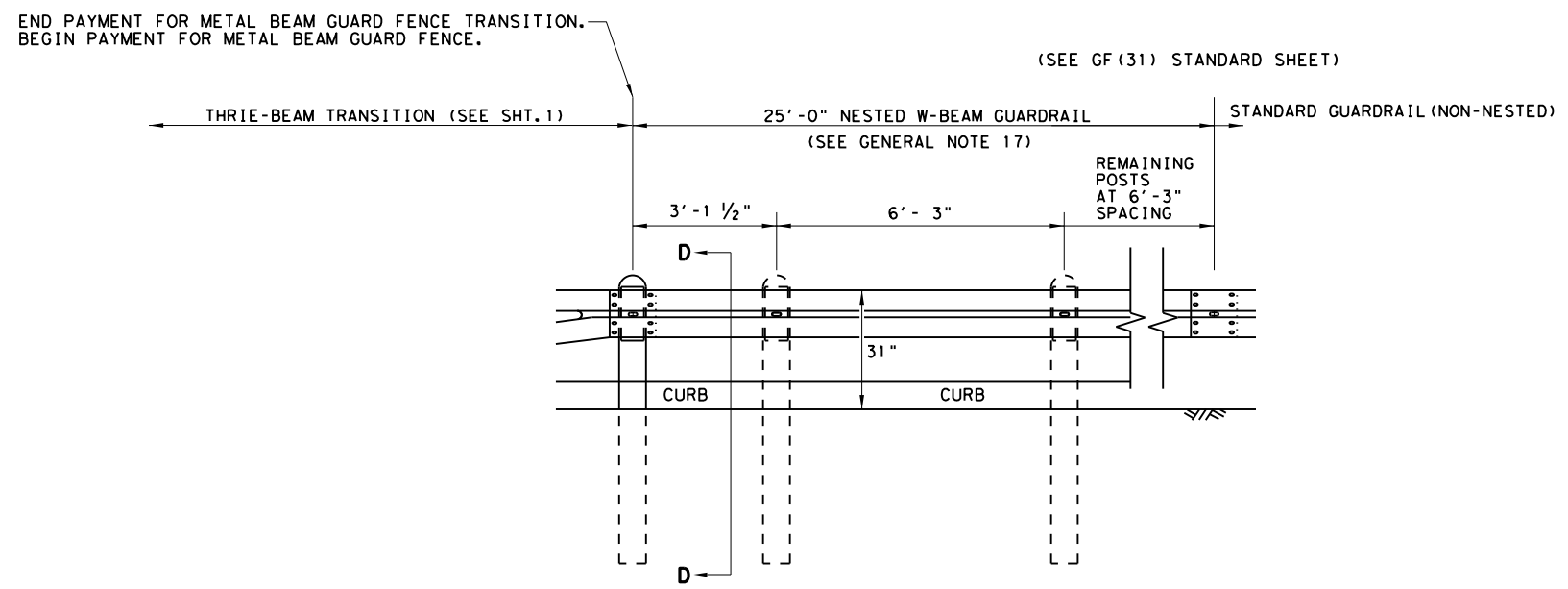
1. CONTACT THE DESIGN DIVISION FOR DRAINAGE CUT OUT OPTIONS NEEDED WITHIN THE CURB SECTION OF THE THRIE-BEAM TRANSITION. (512) 416-2678
2. CONCRETE CURB MAY BE CAST-IN-PLACE OR PRECAST AS SHOWN ON THIS SHEET. WHEN USED IN CONJUNCTION WITH THE THRIE-BEAM TRANSITIONS, CURB SHALL BE TYPE II (5- 3/4" HEIGHT); SEE CURRENT CCGG STANDARD SHEET FOR FURTHER DETAILS. IF OTHER CURB HEIGHTS ARE SHOWN IN THE PLANS IN CONJUNCTION WITH THE TRANSITION, THE CURB HEIGHT MAY BE FROM 4" TO 8" WITH A RELATIVELY VERTICAL FACE. CONCRETE CURB SHALL BE CONTINUOUS TO THE SEVENTH POST UNLESS OTHERWISE SHOWN IN THE PLANS. SEE GENERAL NOTE:17 FOR CIRCUMSTANCES WHERE CURB CONTINUES PAST POST 7.
3. CONCRETE CURB TYPE II SUBSIDIARY TO "METAL BEAM GUARD FENCE TRANSITION". IF NO ADDITIONAL CURB IS INDICATED BEYOND THE TRANSITION, THEN ANY CURB HEIGHT GREATER THAN 4" WILL BE TAPERED DOWN BEGINNING AT THE LAST 7 FT. POST TO A MAXIMUM HEIGHT OF 4" AT POST 7. IF SHOWN ELSEWHERE IN THE PLANS, ADDITIONAL CURB UNDERNEATH GUARDRAIL WILL BE PAID FOR BY THE LINEAR FOOT.
4. UNLESS OTHERWISE SHOWN IN THE PLANS, TRANSITIONS SHALL BE PLACED WITH THE BLOCKOUT FACE IN FRONT OF OR DIRECTLY ABOVE THE CURB FACE. SEE SECTION A-A.
5. FOR ROUND WOOD POST SYSTEMS, ALL ROUND WOOD POSTS SHALL BE 7 1/2" DIA. MINIMUM THROUGHOUT THE THRIE-BEAM TRANSITION.
6. THE TYPE OF POST (ROUND WOOD POST, RECTANGULAR WOOD POST OR STEEL POST) WILL BE AS SHOWN IN THE PLANS. REFER TO GF (31) STANDARD SHEET.
7. THE POST LENGTH SHALL BE MARKED ON ALL 7'- 0" LONG POSTS BY THE MANUFACTURER. THE MARK SHALL BE LOCATED WITHIN THE TOP 1 FT. REGION OF THE POST, AT LEAST 5/8" IN HEIGHT, AND VISIBLE AFTER INSTALLATION. WOODEN POSTS SHALL BE MARKED WITH A BRAND, AND STEEL POSTS WITH A STENCIL BEFORE GALVANIZING.
8. POSTS SHALL NOT BE SET IN CONCRETE, OF ANY DEPTH.
9. RAIL ELEMENTS SHALL MEET THE REQUIREMENTS OF ITEM 540, "METAL BEAM GUARD FENCE" EXCEPT AS MODIFIED ON THE PLANS. THE THRIE-BEAM TERMINAL CONNECTOR AND THE THRIE-BEAM TRANSITION TO W-BEAM SHALL BE OF THE SAME MATERIAL, BUT SHALL NOT BE LESS THAN 10 GAUGE. CONTRACTOR SHALL VERIFY THAT THE LOCATIONS OF BOLT HOLES MATCH THOSE IN THE THRIE-BEAM TERMINAL CONNECTOR PRIOR TO ORDERING MATERIALS.
10. BUTTON HEAD "POST BOLTS & NUTS" SHALL MEET THE REQUIREMENTS OF (ASTM A307), AND SHALL BE OF SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND 5/8" WASHER (FWC16G) AND NOT MORE THAN 1" BEYOND IT. TRIM REMAINING BOLT LENGTH TO MEET REQUIRED LENGTH.
11. FITTINGS (BOLTS, NUTS, AND WASHERS) SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 445, "GALVANIZING". FITTINGS SHALL BE SUBSIDIARY TO THE BID ITEM.
12. CROWN SHALL BE WIDENED TO ACCOMMODATE TRANSITIONS.
13. WHERE SOLID ROCK IS ENCOUNTERED, CONTACT THE DESIGN DIVISION FOR ADDITIONAL GUIDANCE. (512) 416-2678
14. UNLESS OTHERWISE SHOWN IN THE PLANS, A COMPOSITE MATERIAL BLOCK THAT MEETS THE REQUIREMENTS OF DMS-7210, "COMPOSITE MATERIAL POSTS AND BLOCKS FOR METAL BEAM GUARD FENCE" MAY BE SUBSTITUTED FOR BLOCKS OF SIMILAR DIMENSIONS. TXDOT'S MATERIALS AND TESTS DIVISION MAINTAINS A MATERIAL PRODUCER LIST (MPL) FOR PRODUCERS OF MATERIALS CONFORMING TO DMS-7210. ONLY PRODUCERS ON THE MPL CAN FURNISH COMPOSITE MATERIAL BLOCKS.
15. REFER TO GF (31) STANDARD SHEET & BRIDGE RAILING DETAILS FOR ADDITIONAL DETAILS.
16. THE INSTALLATION OF THE TYPE II CURB IS CRITICAL FOR THE PERFORMANCE OF THE THRIE-BEAM TRANSITION SYSTEM. THE CURB PREVENTS (VEHICLE WHEEL SNAGGING) AT THE CONCRETE RAIL AND IS REQUIRED TO MEET MASH CRASH TEST CRITERIA.
17. IF CURB EXTENDS BEYOND POST 7, 25' OF NESTED W-BEAM GUARDRAIL SHALL BE INSTALLED BEYOND THE PAY LIMITS OF THRIE-BEAM TRANSITION SECTION, (SEE SHT.2). PAYMENT FOR THIS 25' SECTION WILL BE BY LINEAR FOOT, PAY ITEM "0540 6XXX MTL W-BEAM GD FEN (NESTED) (TIM POST)" OR "540 6XXX MTL W-BEAM GD FEN (NESTED) (STEEL POST)" AS APPLICABLE FOR POST TYPE. SEE SHT.2 FOR ADDITIONAL INFORMATION.

**HIGH-SPEED TRANSITION
SHEET 1 OF 2**

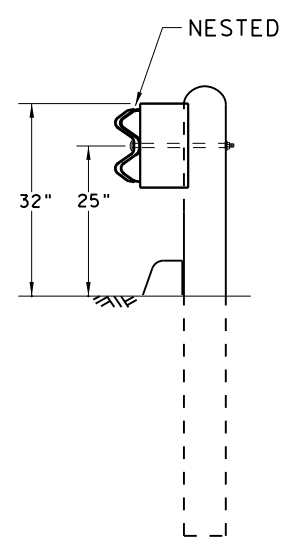
		Design Division Standard	
METAL BEAM GUARD FENCE THRIE-BEAM TRANSITION TL-3 MASH COMPLIANT GF (31) TR TL3-20			
FILE: gf31tr+1320.dgn	DN: TxDOT	CK: KM	DW: VP
© TXDOT: NOVEMBER 2020	CONT	SECT	JOB
REVISIONS	0054	04	123
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	66	

DATE: 2/22/2023
 FILE: T:\BWDSDTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\4. Design\Standard\Roadway\gf31tr+1320.dgn
 DISCLAIMER: THE USE OF THIS STANDARD IS GOVERNED BY THE "TEXAS ENGINEERING PRACTICE ACT". NO WARRANTY OF ANY KIND IS MADE BY TXDOT FOR ANY PURPOSE WHATSOEVER. TXDOT ASSUMES NO RESPONSIBILITY FOR THE CONVERSION OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.

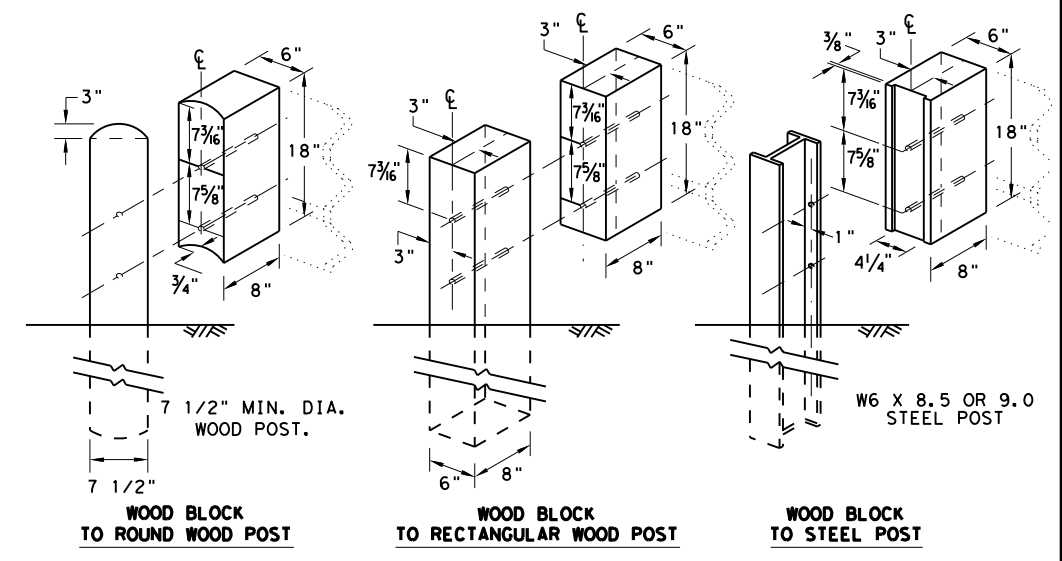
REQUIRED ALTERNATIVE FOR CONTINUOUS CURB EXTENDING PAST POST 7 (SEE SHT. 1 GENERAL NOTE 17)



ELEVATION VIEW



SECTION D-D



THREE BEAM TRANSITION BLOCKOUT DETAILS

HIGH-SPEED TRANSITION

SHEET 2 OF 2

				Design Division Standard
METAL BEAM GUARD FENCE THREE-BEAM TRANSITION TL-3 MASH COMPLIANT GF (31) TR TL3-20				
FILE: gf31tr+1320.dgn	DN: TXDOT	CK: KM	DW: KM	CK: CGL/AG
©TXDOT: NOVEMBER 2020	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
	DIST	COUNTY	SHEET NO.	
	BWD	COLEMAN CO.	67	

DATE: 1/11/2023 8:38:34 AM
 FILE: T:\BWDSSGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to GPR\BWDSSGTEAM\0054-04-123 US 84 - SH 153 to GPR\REFLECTOR AND OBJECT MARKER.dwg
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REFLECTOR UNIT SIZES FOR DELINEATORS AND OBJECT MARKERS				DELINEATORS				D & OM DESCRIPTIVE CODES		
DEVICE	SIZE 1	SIZE 2	SIZE 3	SIZE 4	SINGLE		DOUBLE		INSTL DEL ASSM (D-XX)SZ X (XXXX)XXX (XX) NUMBER OF REFLECTORS S = Single D = Double COLOR OF REFLECTORS W = White Y = Yellow R = Red REFLECTOR UNIT SIZE 1 or 2 TYPE OF POST OR DELINEATOR WC = Wing Channel Post YFLX = Yellow Flexible Post WFLX = White Flexible Post BRF = Barrier Reflector TYPE OF MOUNT GND = Embedded (drivable or set in concrete) CTB = Concrete Barrier Mount GF1 or GF2 = Guard Fence Attachment SRF = Surface Mount DIRECTION If Required BI = Bi-Directional BR = Bi-Directional with red on back	
SHEETING	Yellow, White or Red Type B or C reflective sheeting				SHEETING		Yellow, White or Red Type B or C Reflective Sheeting		INSTL OM ASSM (OM-XX) (XXXX)XXX (XX) TYPE OF OBJECT MARKER 1, 2, 3, or 4 NUMBER OF REFLECTORS OR DIRECTION X = 3-Size 2 reflector units (Type 2 only) Y = 1-Size 2 reflector unit (Type 2 only) Z = 3-Size 1 or 1-Size 4 reflector unit(s) (Type 2 only) L = Left Side (Type 3 Object Marker only) R = Right Side (Type 3 Object Marker only) C = Center (Type 3 Object Marker only) TYPE OF POST WC = Wing Channel Post WFLX = White Flexible Post TWT = Thin Walled Tubing TYPE OF MOUNT GND = Embedded (drivable) SRF = Surface Mount WAS = Wedge Anchor Steel WAP = Wedge Anchor Plastic DIRECTION If Required BI = Bi-Directional	
NOTE	1. Size 1 and 4 - Direct applied reflective sheeting for use on flexible post (fix). 2. Size 2 and 3 - For use on wing channel (wc) post only. Use approved metal, plastic or fiberglass backplate with 17/64" mounting holes.				POST TYPE	WC	YFLX, WFLX	WC		YFLX, WFLX
				OBJECT MARKERS						
DEVICE	Type 1 (OM-1)		Type 2 (OM-2)		Type 3 (OM-3)			Type 4 (OM-4)		
	OM-1	OM-2X	OM-2Y	OM-2Z	OM-3L	OM-3R	OM-3C	OM-4		
SHEETING	Yellow-Type B _{FL} or C _{FL} Sheeting		Yellow - Type B or C Sheeting		Alternating acrylic black and retroreflective yellow - Type B _{FL} or C _{FL} Sheeting			Red -Type B _{FL} or C _{FL} Sheeting		
POST TYPE	TWT		WC	WC	WFLX	TWT		TWT		
MOUNT TYPE	WAS, WAP		GND	GND	GND, SRF	WAS, WAP		WAS, WAP		
BARRIER REFLECTORS (BRF)			CHEVRONS				ONE DIRECTION LARGE ARROW			
DEVICE	GF1	GF2	CTB	DEVICE				DEVICE		
1. Barrier reflectors shall meet the requirements of DMS 8600. 2. Approved Barrier Reflectors are listed on the "Barrier Reflectors" Material Producer List at: www.txdot.gov.			SIZE (W x L)	18"x 24" (Conventional)	24"x 30" (Conventional Oversize)	30"x 36" (Expressway)	36" x 48" (Freeway)	SIZE (W x L)	48" x 24" (Conventional)	60" x 30" (Expressway & Freeway)
			MOUNTING HEIGHT	4'-0" or 7'-0"		7'-0" Only		MOUNTING HEIGHT	7'-0"	
			NOTE	1. CHEVRON (W1-8) signs and ONE DIRECTION LARGE ARROW (W1-6) Signs shall be installed per Sign Mounting Details (SMD) Standard Sheets and paid under Item 644 (Small Roadside Sign Assemblies). 2. When there is a need to increase conspicuity, the Texas version of the ONE DIRECTION LARGE ARROW sign (W1-9T) may be used instead of the ONE DIRECTION LARGE ARROW (W1-6).						
SHEETING			Yellow, White, Red							
NOTE			1. Reflective sheeting shall have a minimum dimension of 3 inches and minimum surface area of 9 square inches.							
DEPARTMENTAL MATERIAL SPECIFICATIONS FLEXIBLE DELINEATOR & OBJECT MARKER POSTS (EMBEDDED & SURFACE MOUNT TYPES) DMS-4400 SIGN FACE MATERIALS DMS-8300 DELINEATORS, OBJECT MARKERS AND BARRIER REFLECTORS DMS-8600										
NOTE: Delineator and object marker substrates and sign substrates shall be 0.080" Aluminum sign blank to conform to ASTM B-209 Alloy 6061-T6 or approved alternative.										
Texas Department of Transportation								Traffic Safety Division Standard		
DELINEATOR & OBJECT MARKER MATERIAL DESCRIPTION D & OM(1)-20										
FILE: dom1-20.dgn		DN: TXDOT		CK: TXDOT		DW: TXDOT		CR: TXDOT		
© TXDOT August 2004		CONT		SECT		JOB		HIGHWAY		
REVISIONS		0054		04		123		US 84		
10-09 3-15		DIST		COUNTY		SHEET NO.				
4-10 7-20		BWD		COLEMAN CO.		68				

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POST TYPE AND SUPPORT FOUNDATION DETAILS

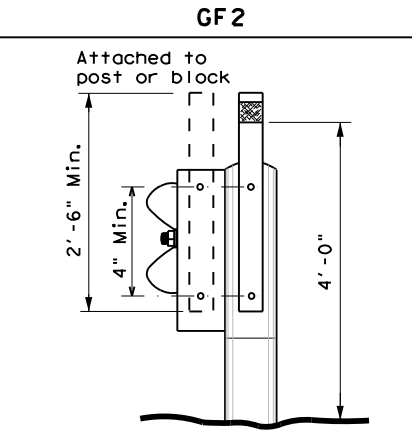
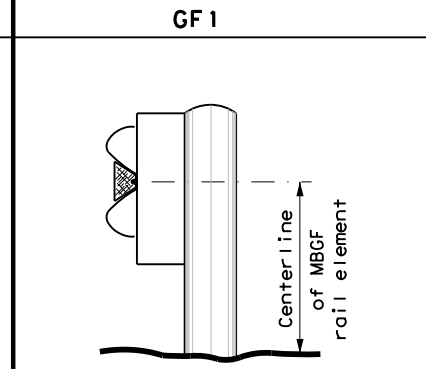
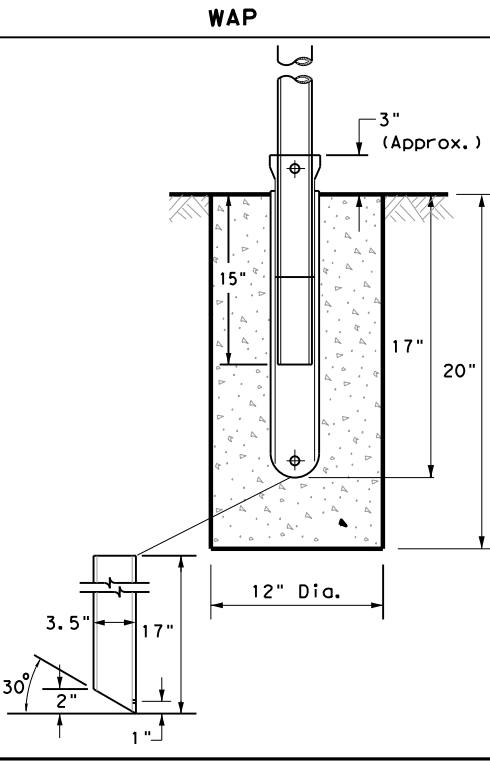
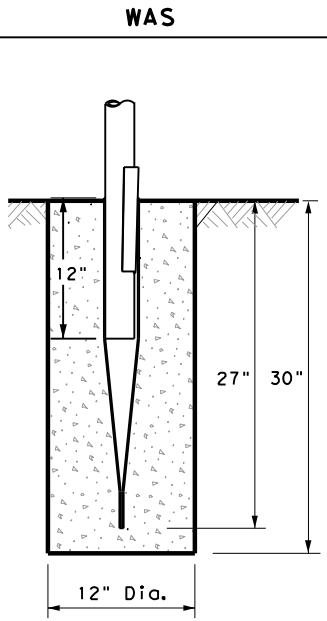
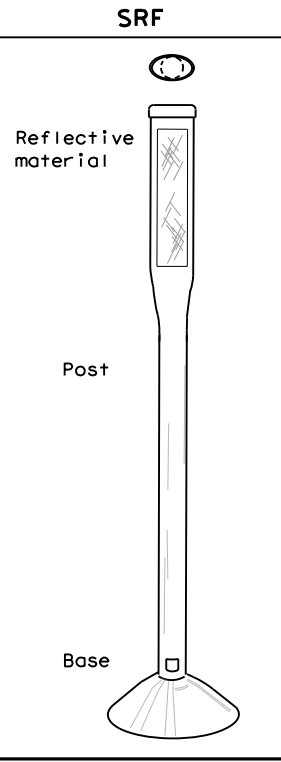
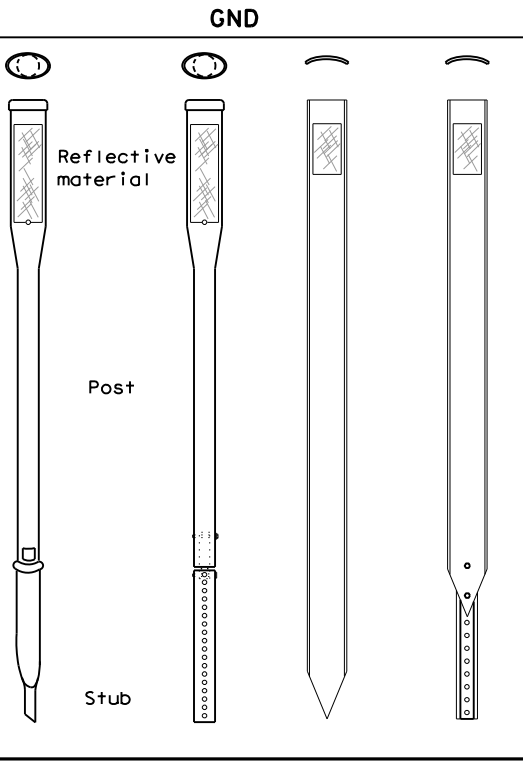
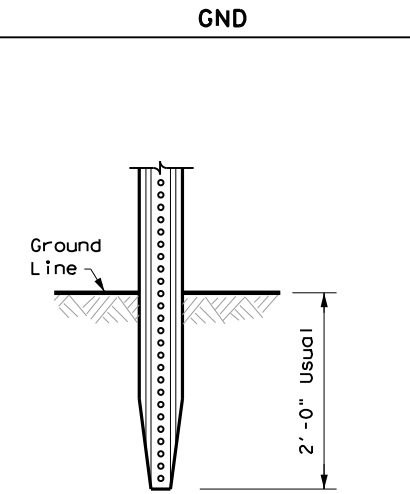
TYPE OF BARRIER MOUNTS

WING CHANNEL (WC)

FLEXIBLE POSTS (YFLX, WFLX)

WEDGE ANCHOR SYSTEMS

GUARD FENCE ATTACHMENT



NOTES

1. Embedded Wing Channel (WC) post option may be used for Type 2 Object Markers and Delineators only.
2. 1.12 lbs/ft steel per ASTM A 1011 SS Gr. 50, or ASTM A499.

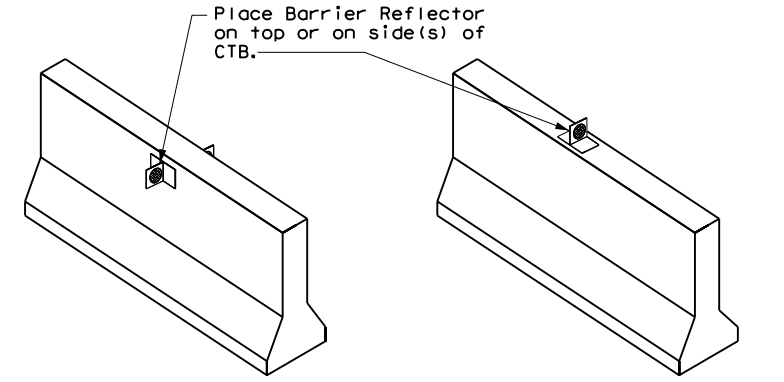
NOTES

1. See "Flexible Delineator and Object Marker Posts" Material Producer List for approved devices.
2. Install per manufacturer's recommendations.
3. Post length may vary to meet field conditions.
4. When using yellow delineators with flexible posts to separate opposing direction of travel, such as centerline or median use, the flexible posts shall be yellow.

NOTE

1. Install per manufacturer's recommendations.

CONCRETE TRAFFIC BARRIER (CTB)



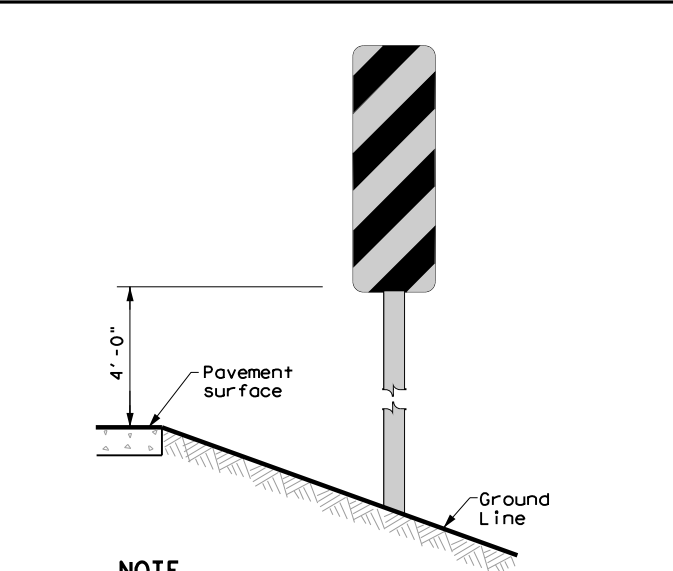
GENERAL NOTES

1. Place delineators on a section of roadway at a consistent distance from the edge of pavement.
2. Where a restriction prevents consistent placement from the pavement edge, place the affected object markers in line with the innermost edge of the obstruction.
3. When Type 2 object markers and delineators are more than 8'-0" from the edge of the pavement, it may not be possible to maintain a height of approximately 4'-0". If this is the case, place the object marker or delineator as close to the desired height as possible.
4. Install all delineators, object markers and barrier reflectors in accordance with the manufacturer's recommendation.
5. Barrier reflectors should be installed a minimum of 18 inches above the edge of the pavement surface.
6. Diagonal stripes on Type 3 object markers shall slope down toward the intended travel lane.

TYPES 1,3, AND 4 OBJECT MARKERS AND CHEVRONS

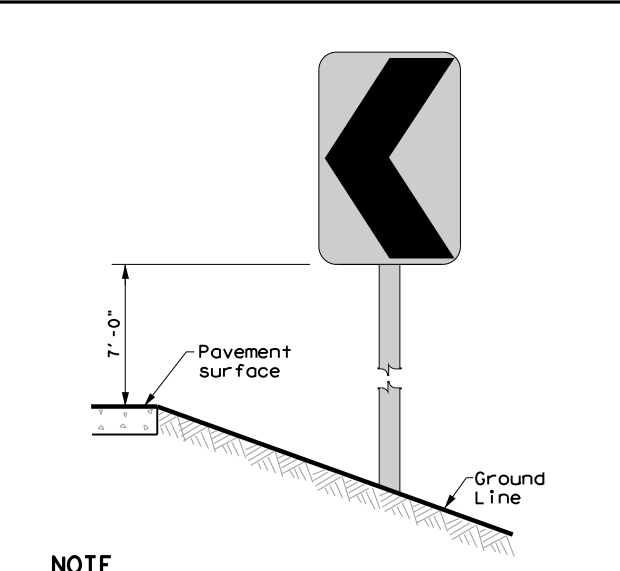
CHEVRONS AND ONE DIRECTION LARGE ARROW SIGN

DELINEATORS AND TYPE 2 OBJECT MARKERS



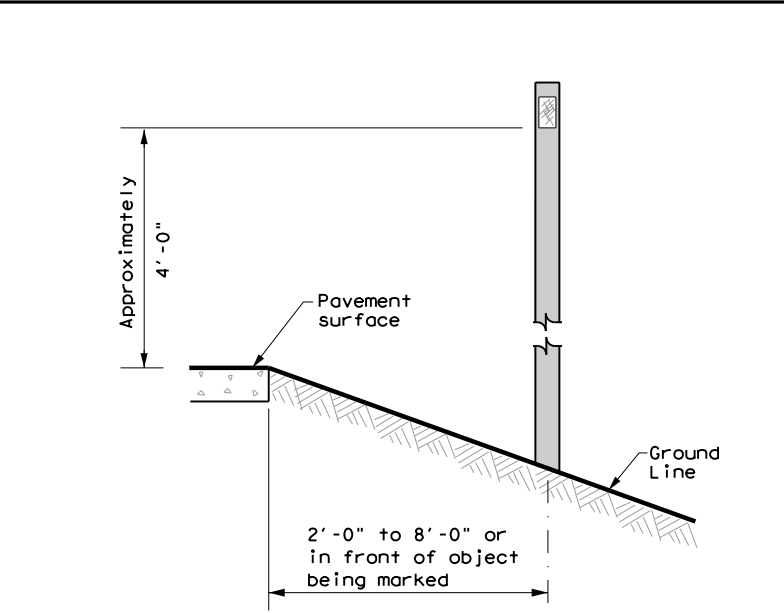
NOTE

Mounting at 4 feet to the bottom of the chevron is permitted for chevrons that will not exceed a height of 6'-6" to the top of the chevron (sizes 24" x 30" and smaller)




NOTE

Chevrons 30" x 36" and larger shall be mounted at a height of 7' to the bottom of the chevron. Chevron sign and ONE DIRECTION LARGE ARROW sign (W1-9T) shall be installed per SMD standard sheets and paid under item 644.



See general notes 1, 2 and 3.

 **Texas Department of Transportation**
Traffic Safety Division Standard

DELINEATOR & OBJECT MARKER INSTALLATION
 D & OM(2)-20

FILE: dcm2-20.dgn	DW: TxDOT	CK: TxDOT	OW: TxDOT	CR: TxDOT	
© TxDOT August 2004	CONT	SECT	JOB	HIGHWAY	
REVISIONS		0054	04	123	US 84
10-09 3-15	DIST	COUNTY		SHEET NO.	
4-10 7-20	BWD	COLEMAN CO.		69	

20B

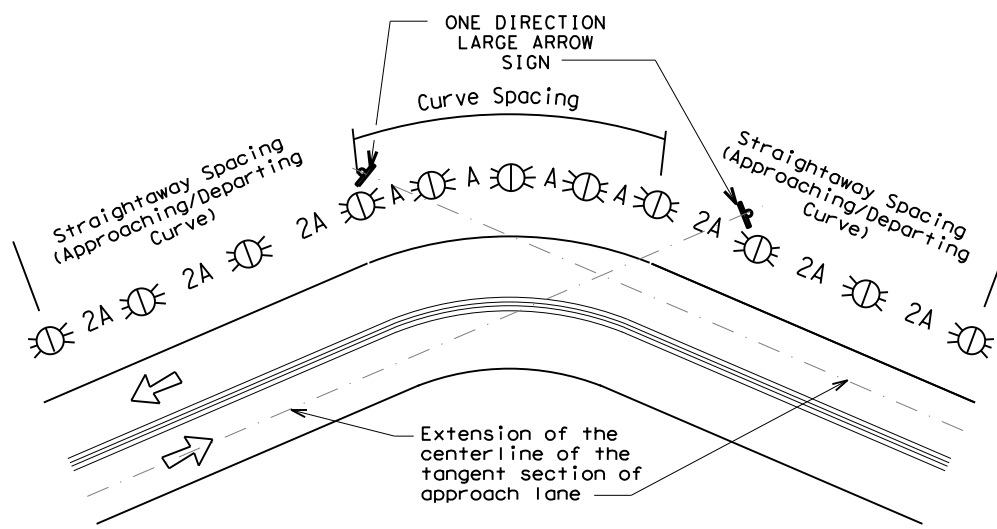
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 FILE: T:\BWDSS\TEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to improve safety of bridge over creek at 0.5 miles from SH 153 intersection

MINIMUM WARNING DEVICES AT CURVES WITH ADVISORY SPEEDS

Amount by which Advisory Speed is less than Posted Speed	Curve Advisory Speed	
	Turn (30 MPH or less)	Curve (35 MPH or more)
5 MPH & 10 MPH	• RPMs	• RPMs
15 MPH & 20 MPH	• RPMs and One Direction Large Arrow sign	• RPMs and Chevrons; or • RPMs and One Direction Large Arrow sign where geometric conditions or roadside obstacles prevent the installation of chevrons.
25 MPH & more	• RPMs and Chevrons; or • RPMs and One Direction Large Arrow sign where geometric conditions or roadside obstacles prevent the installation of chevrons	• RPMs and Chevrons

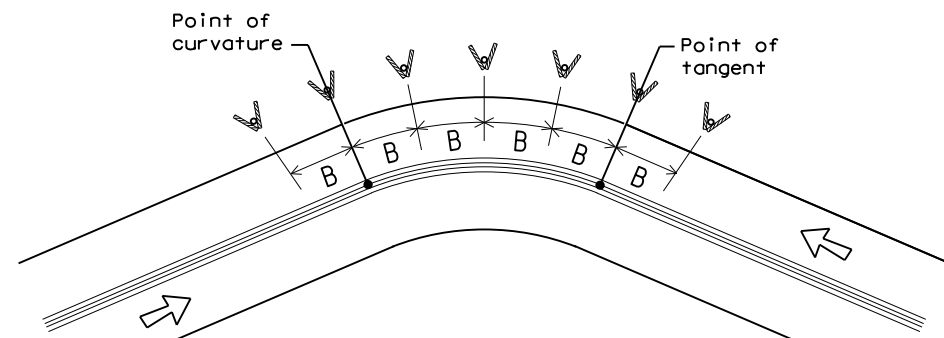
SUGGESTED SPACING FOR DELINEATORS ON HORIZONTAL CURVES



NOTE

ONE DIRECTION LARGE ARROW (W1-6) sign should be located at approximately and perpendicular to the extension of the centerline of the tangent section of approach lane.

SUGGESTED SPACING FOR CHEVRONS ON HORIZONTAL CURVES



NOTE

At least one chevron pair is installed beyond the point of tangent in tangent section.

DELINEATOR AND CHEVRON SPACING

WHEN DEGREE OF CURVE OR RADIUS IS KNOWN				
Degree of Curve	FEET			
	Radius of Curve	Spacing in Curve	Spacing in Straightaway	Chevron Spacing in Curve
		A	2A	B
1	5730	225	450	—
2	2865	160	320	—
3	1910	130	260	200
4	1433	110	220	160
5	1146	100	200	160
6	955	90	180	160
7	819	85	170	160
8	716	75	150	160
9	637	75	150	120
10	573	70	140	120
11	521	65	130	120
12	478	60	120	120
13	441	60	120	120
14	409	55	110	80
15	382	55	110	80
16	358	55	110	80
19	302	50	100	80
23	249	40	80	80
29	198	35	70	40
38	151	30	60	40
57	101	20	40	40

Curve delineator approach and departure spacing should include 3 delineators spaced at 2A. This spacing should be used during design preparation or when the degree of curve is known.

DELINEATOR AND CHEVRON SPACING

WHEN DEGREE OF CURVE OR RADIUS IS NOT KNOWN			
Advisory Speed (MPH)	Spacing in Curve	Spacing in Straightaway	Chevron Spacing in Curve
	A	2xA	B
65	130	260	200
60	110	220	160
55	100	200	160
50	85	170	160
45	75	150	120
40	70	140	120
35	60	120	120
30	55	110	80
25	50	100	80
20	40	80	80
15	35	70	40

If the degree of curve is not known, delineator spacing may be determined based on the Advisory Speed of the curve. Use the delineator curve spacing for each Advisory Speed (MPH).

DELINEATOR AND OBJECT MARKER APPLICATION AND SPACING

CONDITION	REQUIRED TREATMENT	MINIMUM SPACING
Frwy./Exp. Tangent	RPMs	See PM-series and FPM-series standard sheets
Frwy./Exp. Curve	Single delineators on right side	See delineator spacing table
Frwy/Exp. Ramp	Single delineators on at least one side of ramp (should be on outside of curves) (see Detail 3 on D&OM(4))	100 feet on ramp tangents Use delineator spacing table for ramp curves ("straightway spacing" does not apply to ramp curves)
Acceleration/Deceleration Lane	Double delineators (see Detail 3 on D&OM(4))	100 feet (See Detail 3 on D & OM (4))
Truck Escape Ramp	Single red delineators on both sides	50 feet
Bridge Rail (steel or concrete) and Metal Beam Guard Fence	Bi-Directional Delineators when undivided with one lane each direction Single Delineators when multiple lanes each direction	Equal spacing (100' max) but not less than 3 delineators
Concrete Traffic Barrier (CTB) or Steel Traffic Barrier	Barrier reflectors matching the color of the edge line	Equal spacing 100' max
Cable Barrier	Reflectors matching the color of the edge line	Every 5th cable barrier post (up to 100' max)
Guard Rail Terminus/Impact Head	Divided highway - Object marker on approach end Undivided 2-lane highways - Object marker on approach and departure end	Requires reflective sheeting provided by manufacturer per D & OM (VIA) or a Type 3 Object Marker (OM-3) in front of the terminal end See D & OM (5) and D & OM (6)
Bridges with no Approach Rail	Type 3 Object Marker (OM-3) at end of rail and 3 single delineators approaching rail	See D & OM(5)
Reduced Width Approaches to Bridge Rail	Type 2 and Type 3 Object Markers (OM-3) and 3 single delineators approaching bridge	Requires reflective sheeting provided by manufacturer per D & OM (VIA) or a Type 3 Object Marker (OM-3) in front of the terminal end See D & OM (5)
Culverts without MBGF	Type 2 Object Markers	See Detail 2 on D & OM(4)
Crossovers	Double yellow delineators and RPMs	See Detail 1 on D & OM (4)
Pavement Narrowing (lane merge) on Freeways/Expressway	Single delineators adjacent to affected lane for full length of transition	100 feet

NOTES

- Unless indicated otherwise, the delineator or barrier reflector color shall conform to the color of the pavement edge line on the side of the road where the delineators or barrier reflectors are placed.
- Barrier reflectors may be used to replace required delineators.
- Single red delineators may be mounted on the back side of delineator posts for wrong way driver applications

LEGEND	
	Bi-directional Delineator
	Delineator
	Sign

Texas Department of Transportation

Traffic Safety Division Standard

DELINEATOR & OBJECT MARKER PLACEMENT DETAILS

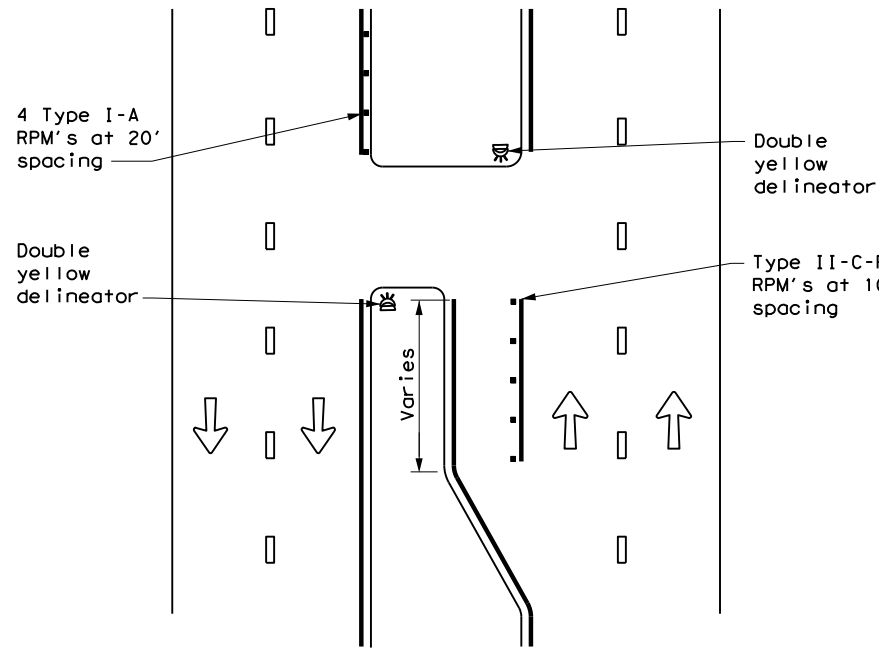
D & OM(3)-20

FILE: dom3-20.dgn	DW: TXDOT	CK: TXDOT	OW: TXDOT	CR: TXDOT
© TXDOT August 2004	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
3-15 8-15	DIST	COUNTY	SHEET NO.	
8-15 7-20	BWD	COLEMAN CO.	70	

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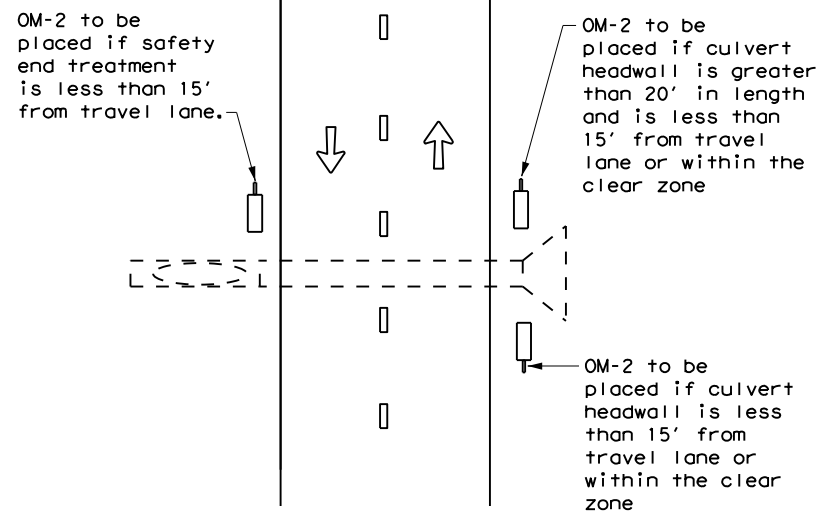
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CROSSOVERS



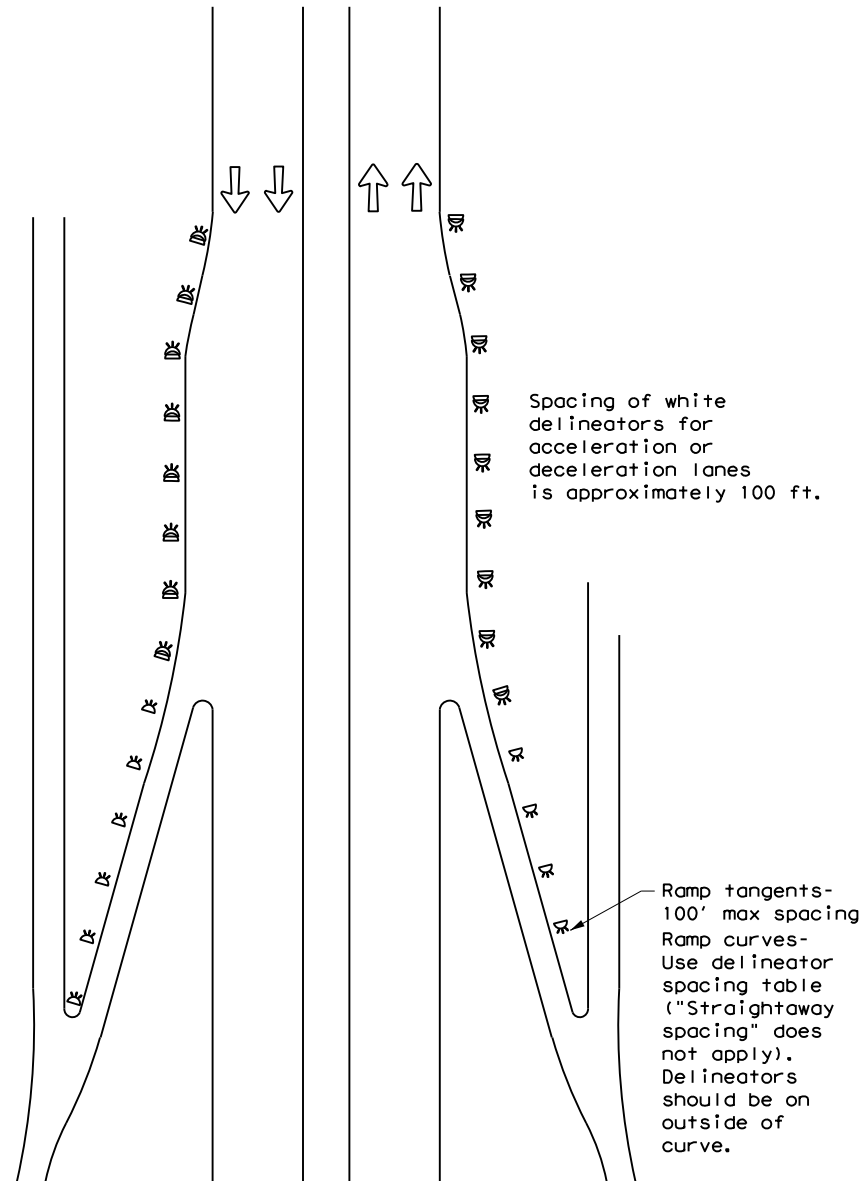
DETAIL 1

FOR CULVERTS WITHOUT MBGF



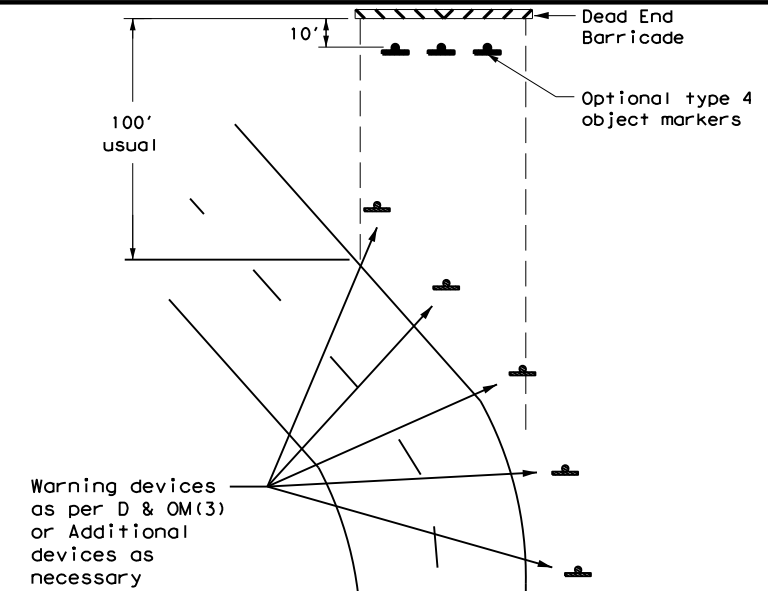
DETAIL 2

FREEWAY DELINEATION FOR RAMPS AND ACCELERATION/DECELERATION LANES



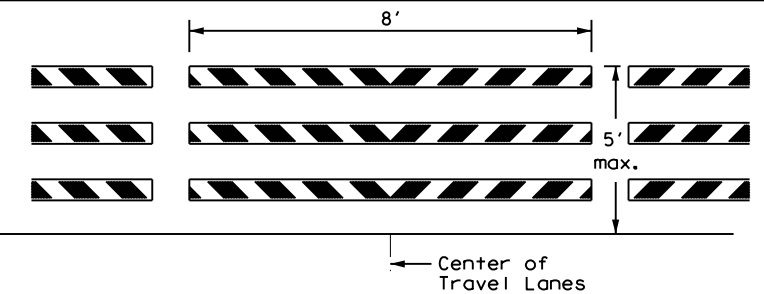
DETAIL 3

TYPICAL APPLICATION OF DEAD END BARRICADE



DETAIL 4

TYPICAL DEAD END BARRICADE INSTALLATION



NOTES

- Barricade striping shall be red and white reflective sheeting for all permanent road closures.
- Barricade striping is red and white sloping toward the center of the roadway.
- Type 3 Barricade Supports should be anchored to soil or pavement as described in compliant Work Zone Traffic Control Devices List, section D.2.f and D.2.g.

DETAIL 5

LEGEND	
	Bidirectional Delineator
	Delineator
	OM-3
	Barricade
	Sign
	OM-2
	Double Delineator



DELINEATOR & OBJECT MARKER PLACEMENT DETAILS

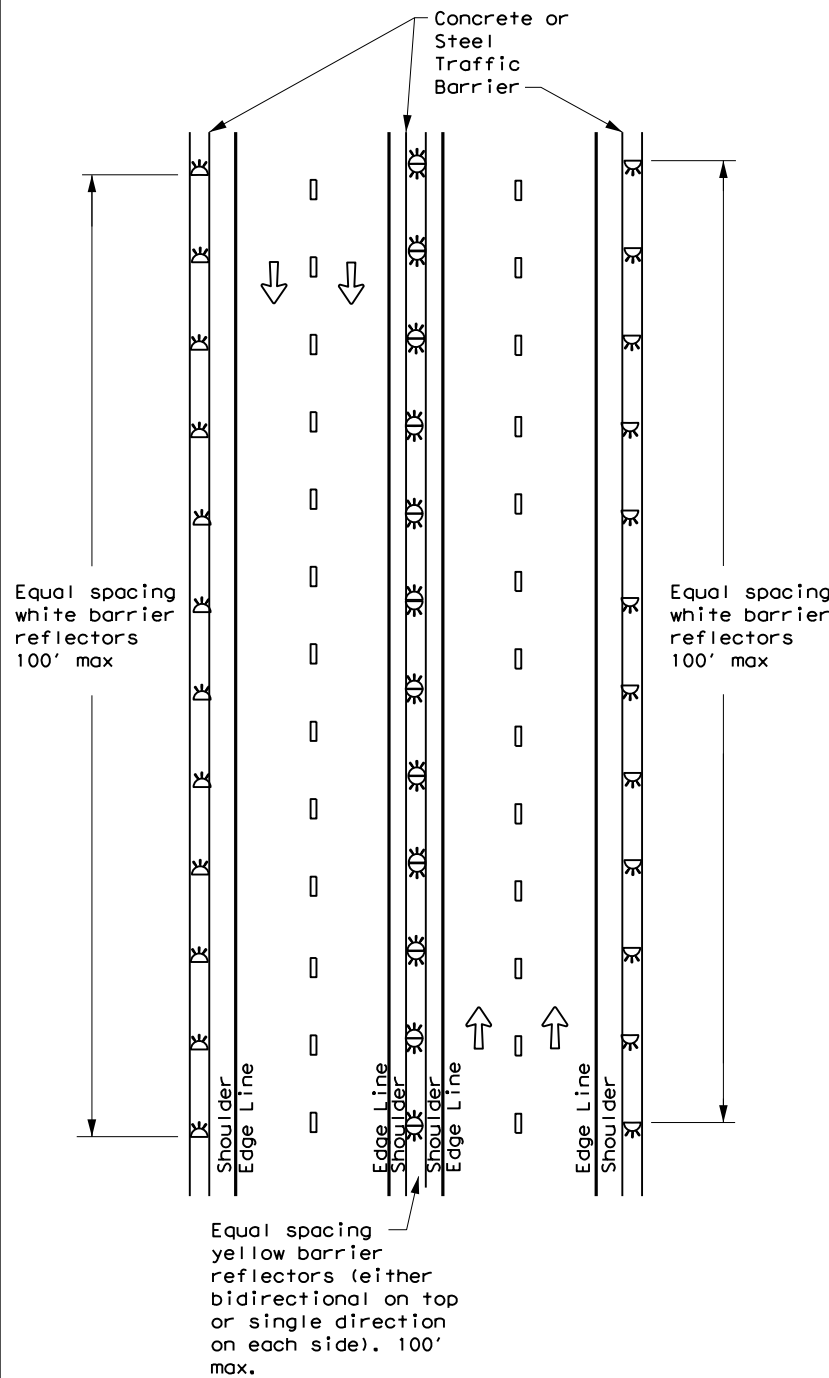
D & OM(4) -20

FILE: dom4-20.dgn	DN: TXDOT	CK: TXDOT	OW: TXDOT	CR: TXDOT
© TXDOT August 2004	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
3-15	DIST	COUNTY	SHEET NO.	
7-20	BWD	COLEMAN CO.	71	

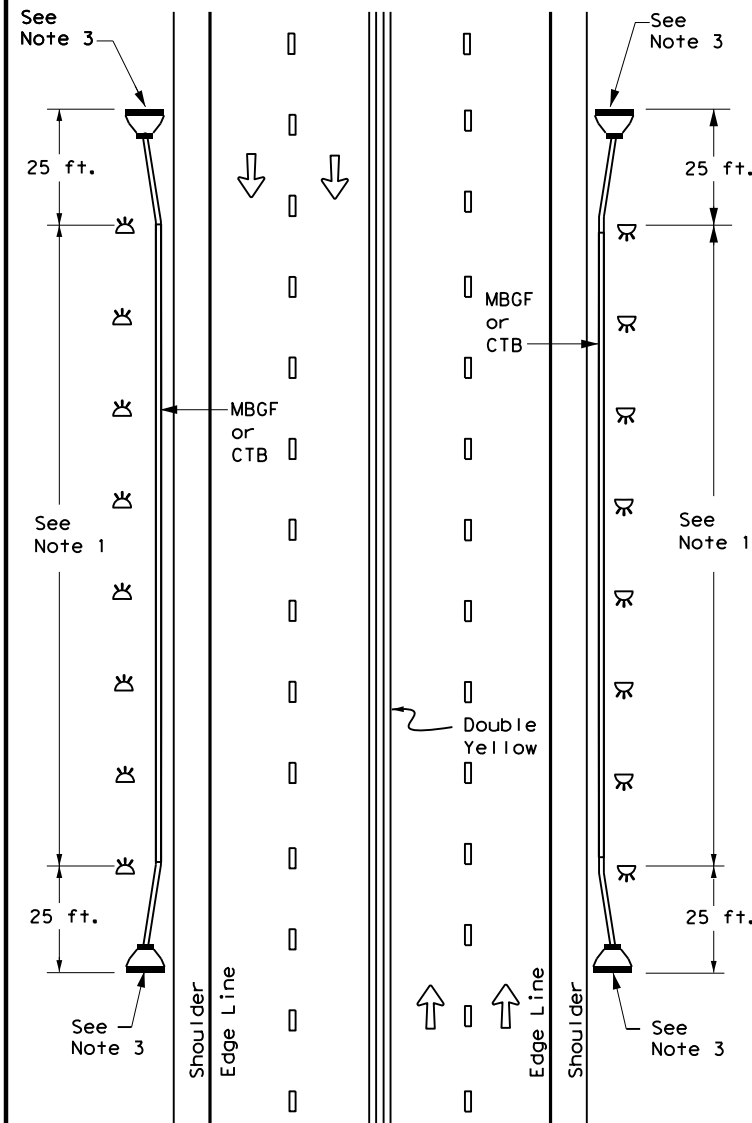
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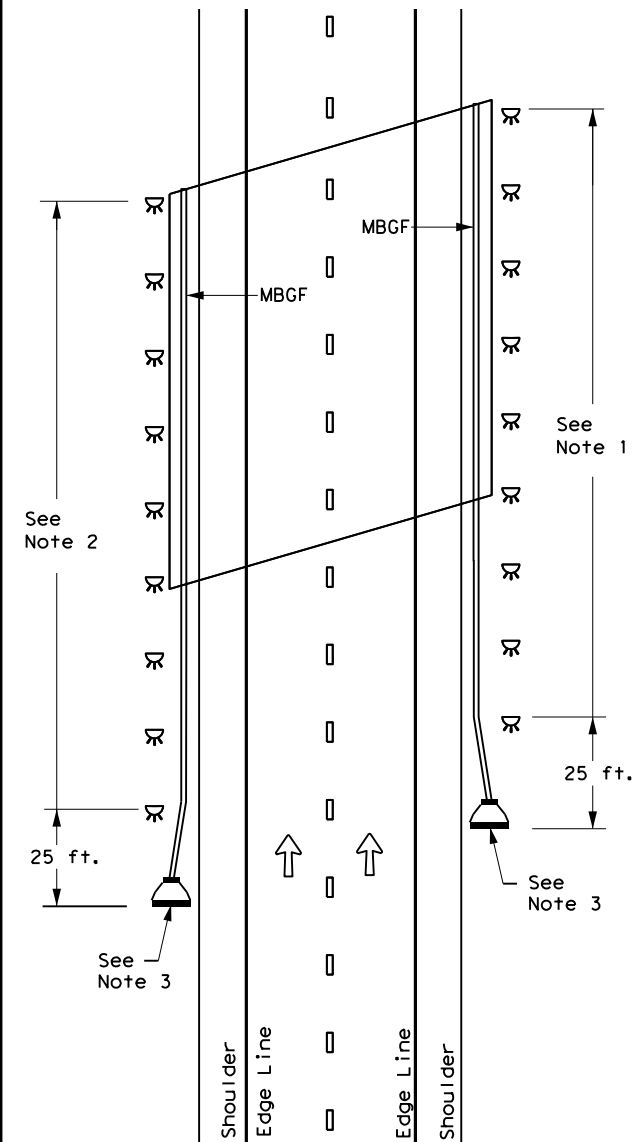
CONTINUOUS CONCRETE OR STEEL BARRIER



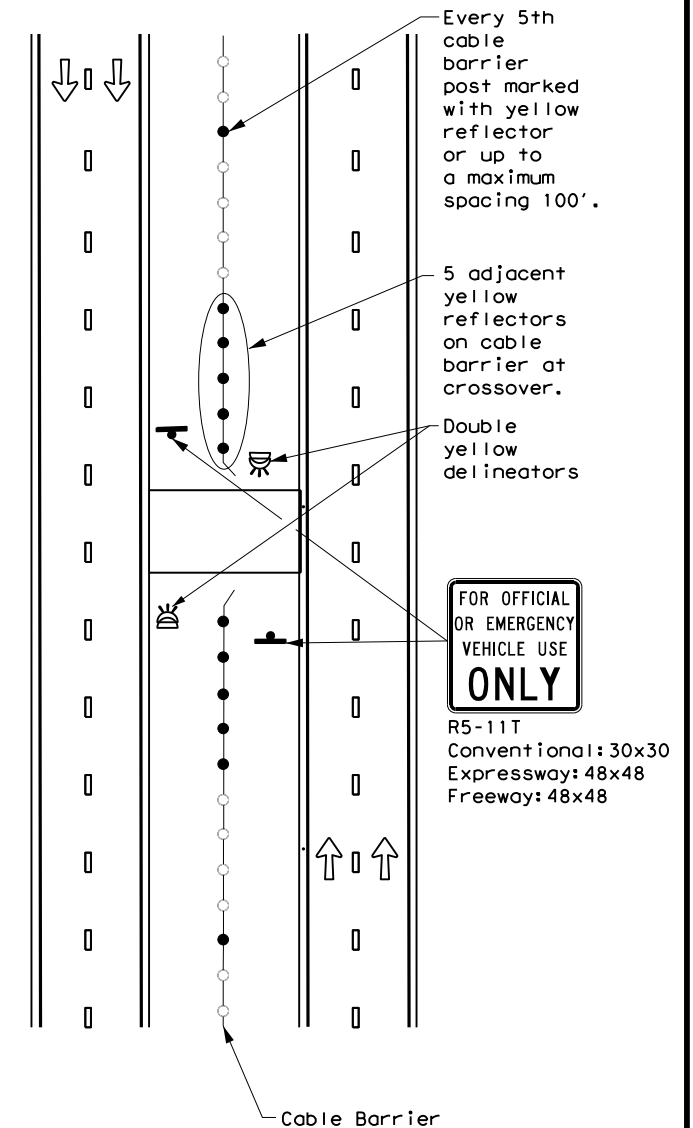
MULTI-LANE UNDIVIDED, TWO-WAY ROADWAY WITH METAL BEAM GUARD FENCE (MBGF)



DIVIDED ROADWAY WITH METAL BEAM GUARD FENCE (MBGF)



EMERGENCY CROSSOVER



NOTES

1. Equal spacing (100' max), but not less than 3 single directional white barrier reflectors or delineators. On Continuous Barrier, equal spacing (100' max.)
2. Equal spacing (100' max), but not less than 3 single directional yellow barrier reflectors or delineators.
3. Terminal ends require reflective sheeting provided by manufacturer per D & OM (VIA) or a Type 3 Object Marker (OM-3) in front of the terminal end.

LEGEND

	Bidirectional Delineator
	Delineator
	OM-3
	OM-2
	Terminal End
	Traffic Flow



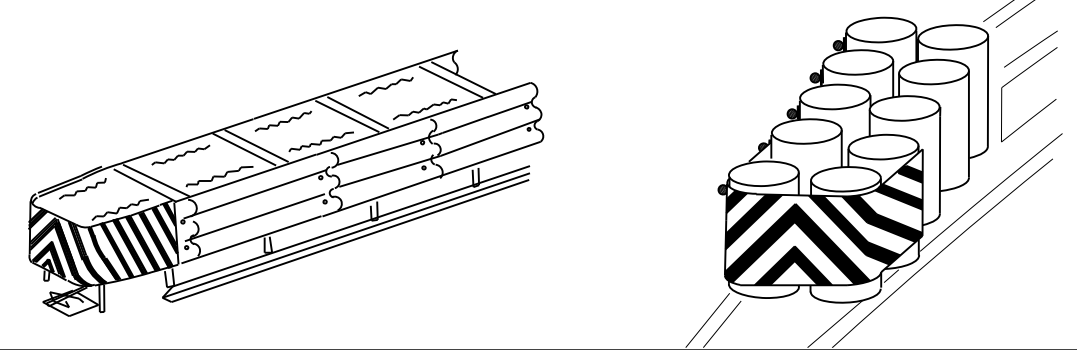
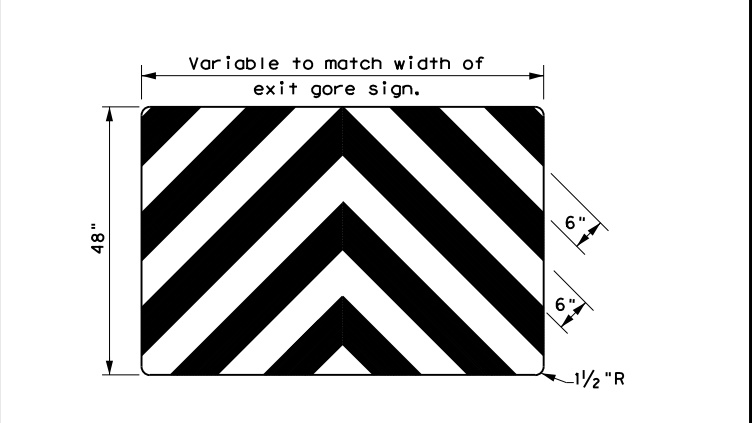
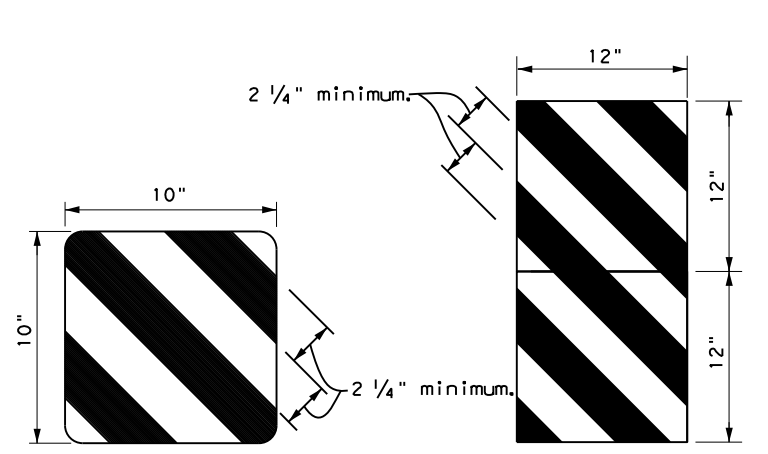
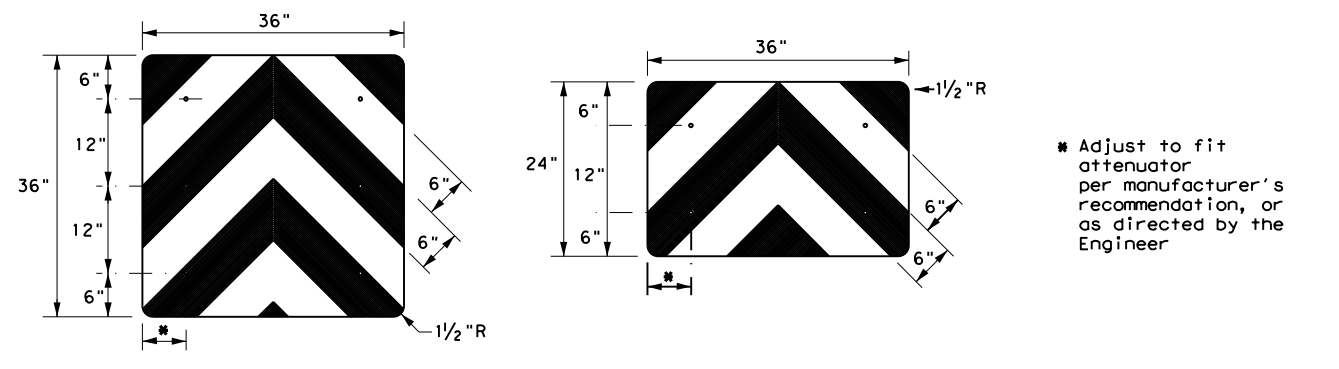
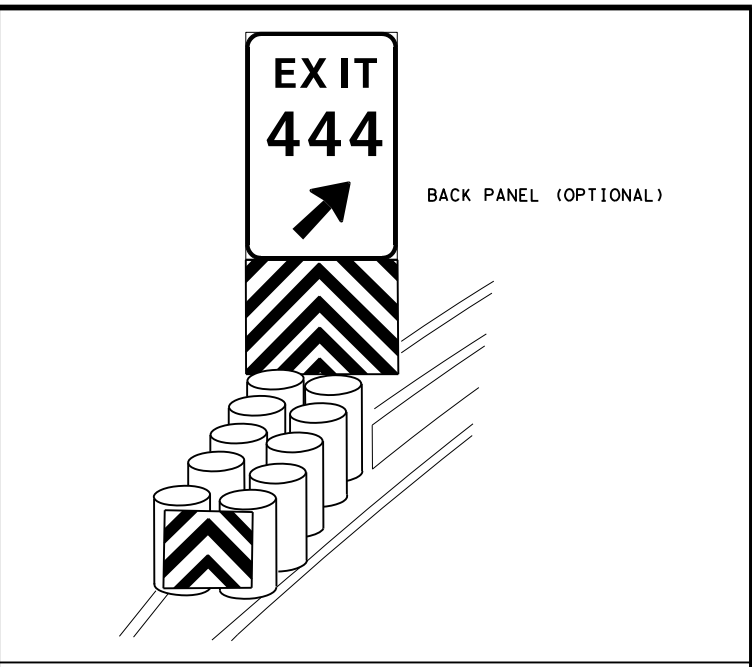
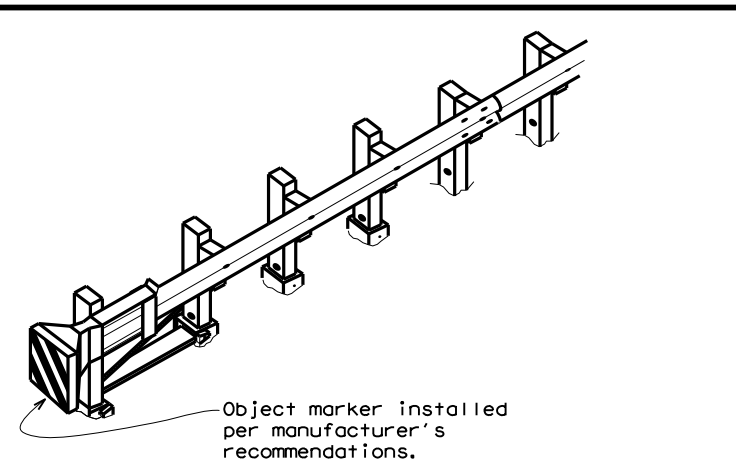
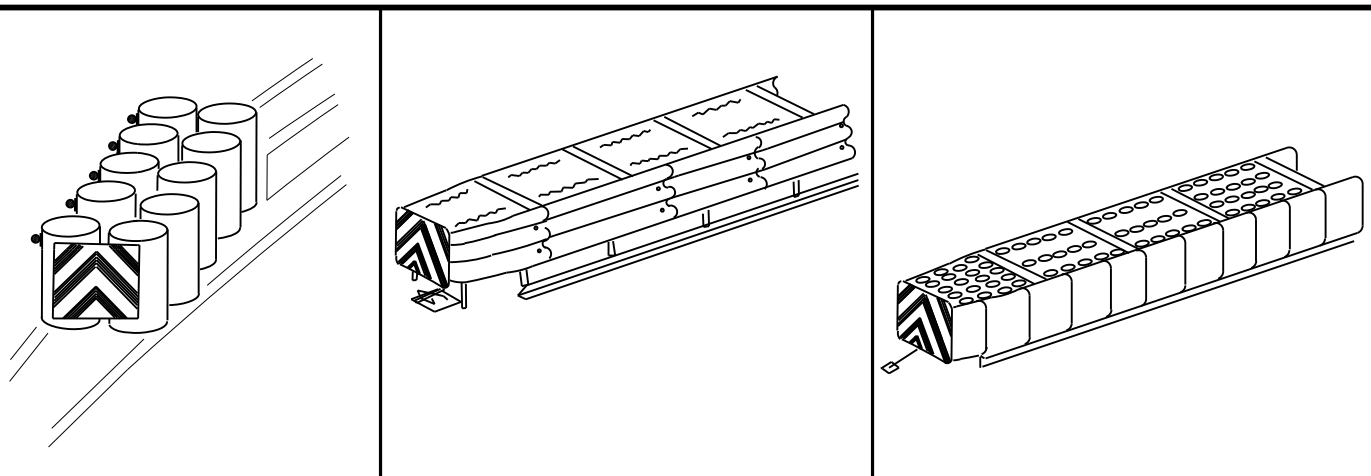
DELINEATOR & OBJECT MARKER PLACEMENT DETAILS

D & OM(6)-20

FILE: dom6-20.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
© TxDOT August 2015	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
7-20	DIST	COUNTY	SHEET NO.	
	BWD	COLEMAN CO.	72	

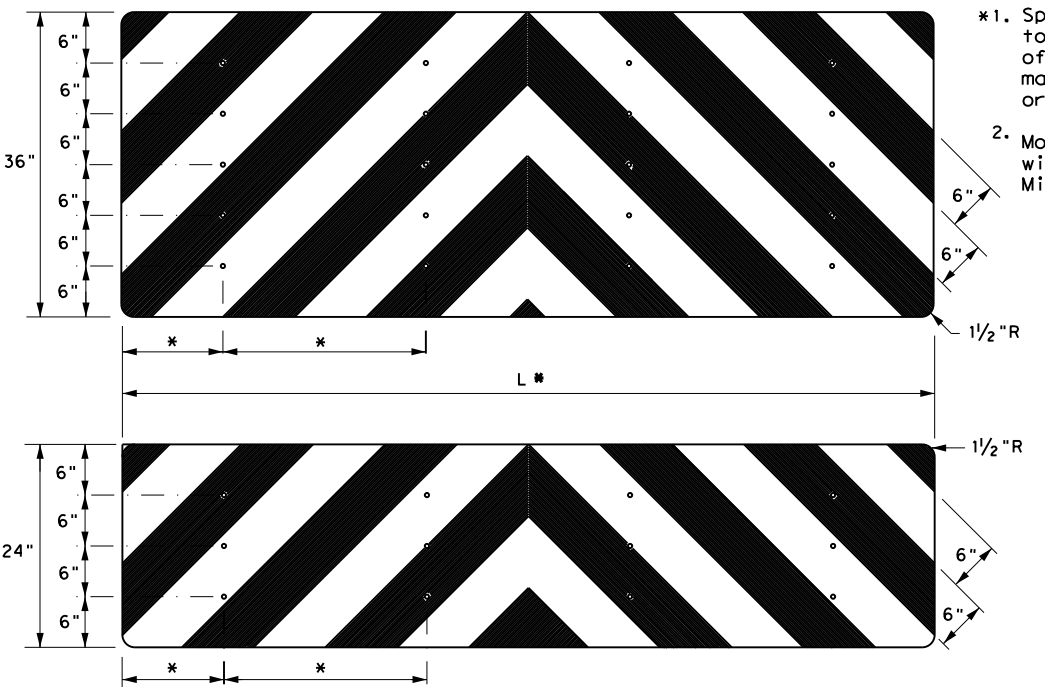
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DATE: 1/11/2023 8:42:04 AM
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OBJECT MARKERS SMALLER THAN 3 FT²

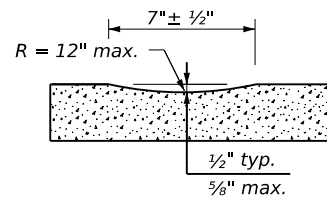
- NOTES**
- Spacing should be adjusted to attach through centerline of drum, per attenuator manufacturer's recommendation, or as directed by the Engineer.
 - Mounting should be flush with top of attenuator. Minimum size 96" x 24".



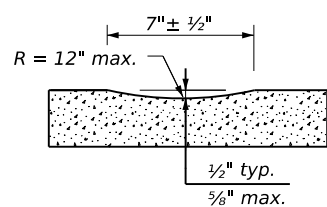
- NOTES**
- Object Markers shall conform to the Texas MUTCD and meet the color and reflectivity requirement of Department Material Specification DMS 8300. Background shall be yellow reflective sheeting (Type B or C) and Chevron shall be black.
 - Object Markers may be fabricated from adhesive backed reflective sheeting applied directly to guardrail end treatment, or applied directly to an "end cap" as per the manufacturer's recommendation. Direct applied sheeting shall provide a smooth surface and have no wrinkles, air bubbles, cuts or tears. A radius at the corners is not required for direct applied sheeting.
 - Object Marker size may be reduced to fit smaller devices. Width of alternating black and yellow stripes are typically 6". Object Markers smaller than 3ft may have reduced width stripes of a minimum of 2 1/4".
 - Pop rivets, screws, or nuts and bolts may be used to attach object markers and reflectors. Holes, slots or other openings may be cut or drilled through object markers to allow cable or other attachments.
 - Object Marker at nose of attenuator is subsidiary to the attenuator.
 - See D & OM (1-4) for required barrier reflectors.

DELINEATOR & OBJECT MARKER FOR VEHICLE IMPACT ATTENUATORS D & OM(VIA) -20			
FILE: domvia20.dgn	DN: TXDOT	CK: TXDOT	DW: TXDOT
© TXDOT December 1989	CONT	SECT	JOB
REVISIONS	0054	04	123
4-92 8-04	DIST	COUNTY	SHEET NO.
8-95 3-15	BWD	COLEMAN CO.	73
4-98 7-20			

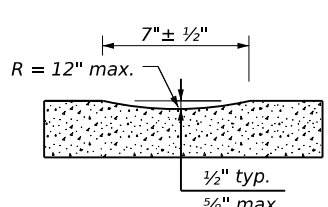
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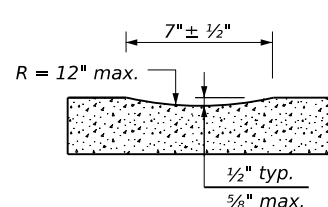
PROFILE VIEW
OPTION 1



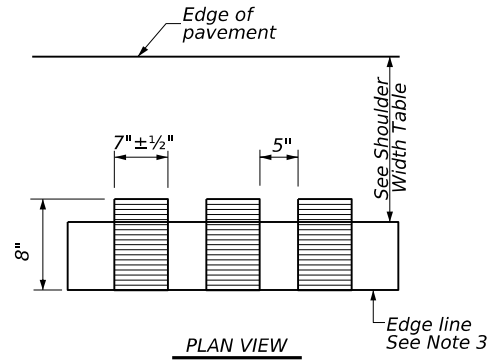
PROFILE VIEW
OPTION 2



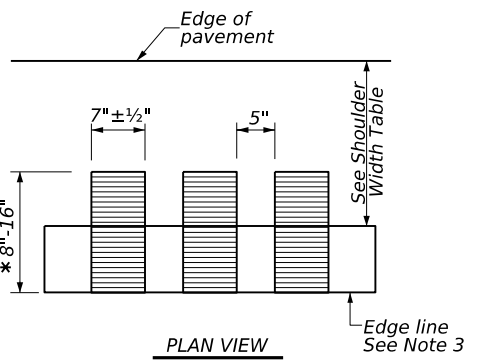
PROFILE VIEW
OPTION 3



PROFILE VIEW
OPTION 4

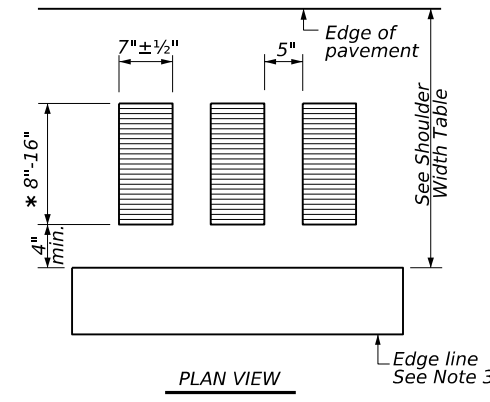


PLAN VIEW



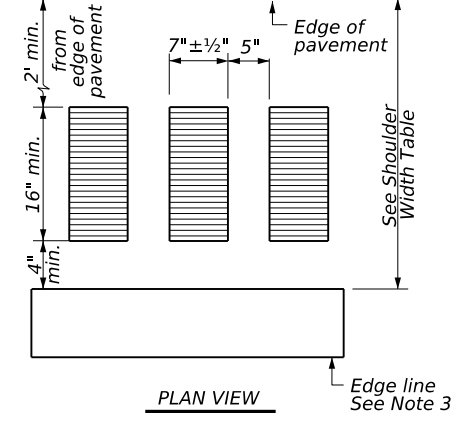
PLAN VIEW

* This distance may vary based on width of shoulder



PLAN VIEW

* This distance may vary based on width of shoulder



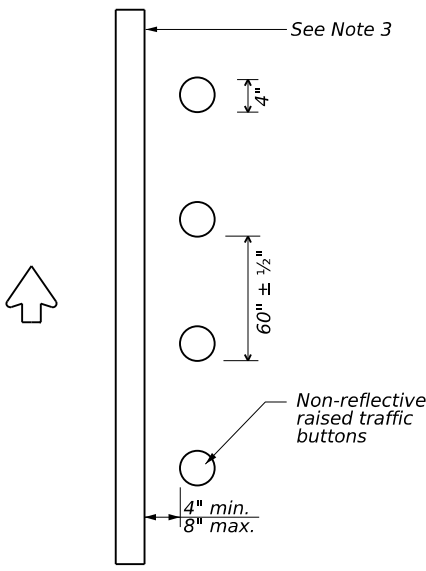
PLAN VIEW

CONTINUOUS MILLED DEPRESSIONS (Rumble Strips)

CONTINUOUS MILLED DEPRESSIONS (Rumble Strips)

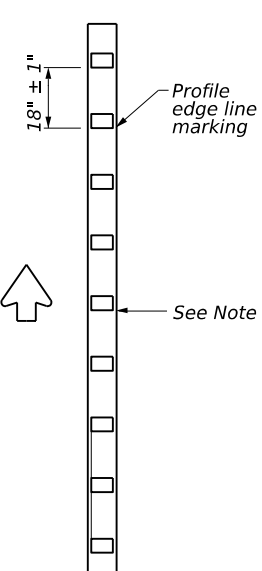
CONTINUOUS MILLED DEPRESSIONS (Rumble Strips)

CONTINUOUS MILLED DEPRESSIONS (Rumble Strips)



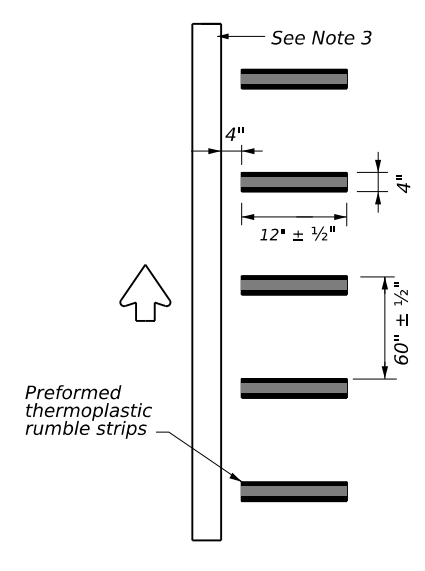
PLAN VIEW
OPTION 5

RAISED EDGE LINE (Rumble Strips)



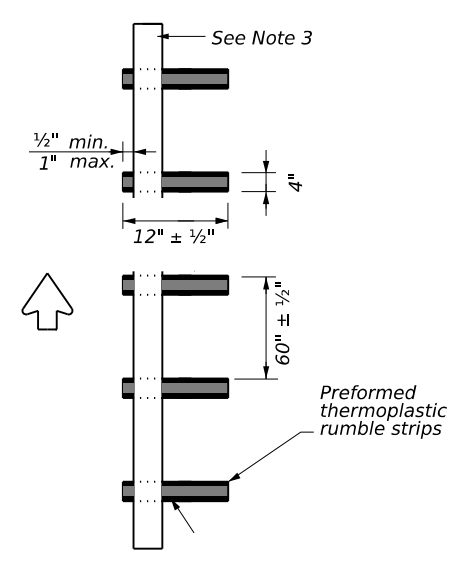
PLAN VIEW
OPTION 6

PROFILE EDGE LINE MARKINGS (Rumble Strips)



PLAN VIEW
OPTION 7

PREFORMED THERMOPLASTIC EDGE LINE (Rumble Strips)



PLAN VIEW
OPTION 8

PREFORMED THERMOPLASTIC EDGE LINE (Rumble Strips)

SHOULDER WIDTH TABLE		
EQUAL TO OR LESS THAN 2 FEET	GREATER THAN 2 FEET LESS THAN 4 FEET	EQUAL TO OR GREATER THAN 4 FEET
Option 1, 5, 6 or 8	Option 1, 2, 3, 5, 6 or 7	Option 2, 4, 5, 6 or 7

GENERAL NOTES

- Rumble strips and profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.
- Milled rumble strips are preferred when adequate pavement depth is available. If pavement thickness is less than 2 inches, milled rumble strips shall not be used. Rumble strips shall not be milled or depressed into bridge decks.
- Use Standard Sheet PM(2) and FPM(1) for positioning, dimensioning, and spacing of all reflective raised pavement markers, pavement markings, and profile markings.
- See the Shoulder Width Table below for determining what options may be used for edge line rumble strips.
- Breaks in edge line rumble strips shall occur at least 50 feet and no more than 150 feet in advance of bridges, railroad crossings, intersections, or driveways with high usage of large trucks when installed on conventional highways.
- Rumble strips shall not be placed across exit or entrance ramps, acceleration or deceleration lanes, crossovers, gore areas, or intersections with other roadways.
- Consideration should be given to noise levels when edgeline rumble strips are to be installed near residential areas, schools, churches, etc. A 3/8 inch deep (minimum) milled rumble strip may be considered in these areas.
- Consideration shall be given to bicyclists. See RS(6).

WHEN INSTALLING MILLED DEPRESSION EDGE LINE RUMBLE STRIPS:

- See dimensions for milled rumble strips. Other shapes and dimensions may be used if approved by the Traffic Safety Division.
- Pavement markings can be applied over milled shoulder rumble strips to create an edge line rumble strip.

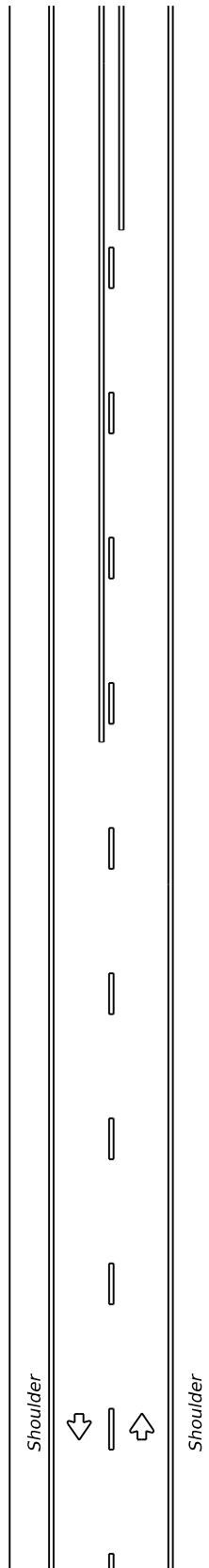
WHEN INSTALLING RAISED OR PROFILE EDGE LINE RUMBLE STRIPS:

- Raised rumble strips consisting of non-reflective raised traffic buttons may be used. Non-reflective raised traffic buttons can be affixed to asphalt or concrete with bitumen or adhesives, as per the manufacturer's recommendations.
- Non-reflective traffic buttons shall be placed adjacent to the pavement marking delineating the edge line when used as a rumble strip. The color of the button should match the color of the adjacent edge line marking (white or yellow). The buttons will be paid for under Item 672, "Raised Pavement Markers." Non-reflective traffic buttons must meet the requirements of DMS-4300.
- Non-reflective traffic buttons shall not be placed across exit or entrance ramps, acceleration and deceleration lanes, crossovers, gore areas or intersections with other roadways.
- The minimum distance between the edge line and the buttons should be used if the shoulder is less than 8 feet in width.
- Raised profile thermoplastic markings used as edge lines may substitute for buttons.

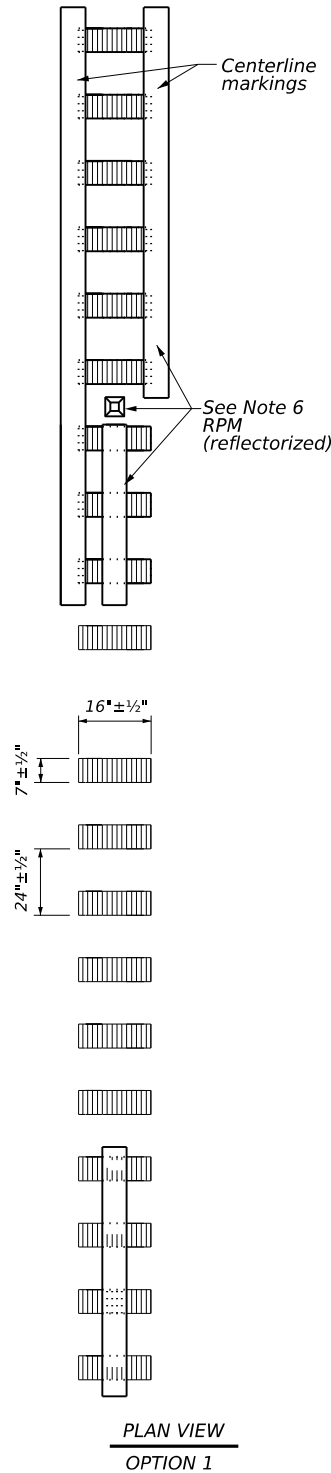
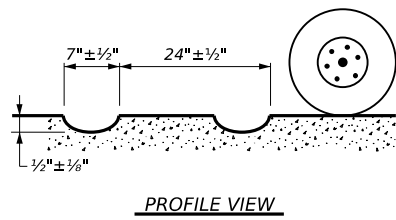
EDGE LINE RUMBLE STRIPS ON UNDIVIDED OR TWO LANE HIGHWAYS RS(2)-23			
FILE: rs(2)-23.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
© TxDOT	January 2023	COWT SECT	JOB HIGHWAY
REVISIONS		0054 04	123 US 84
10-13 1-23	DIST	COUNTY	SHEET NO.
BWD	COLEMAN CO.		74

DATE: 2/22/2023 3:17:15 PM
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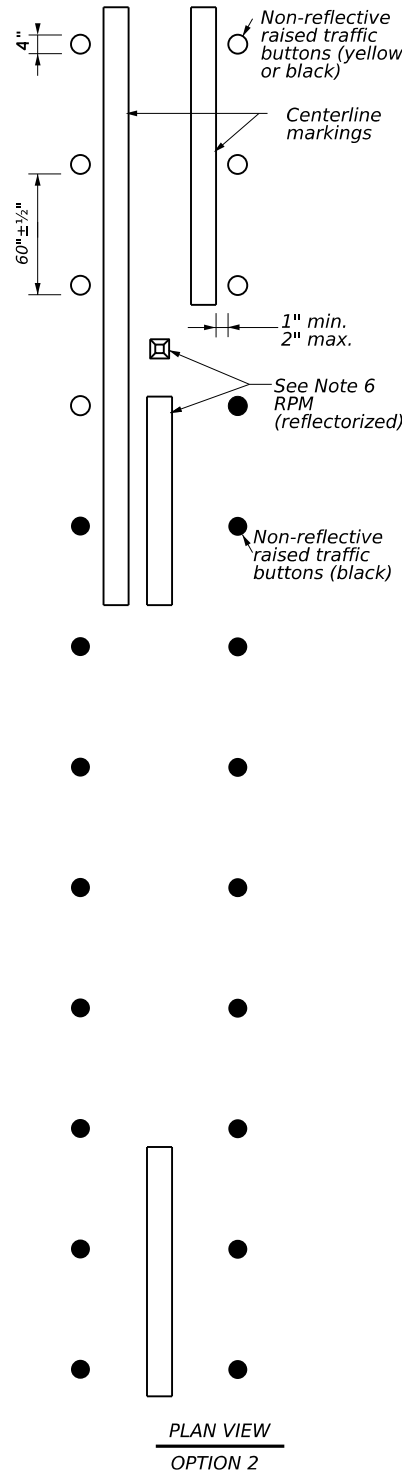
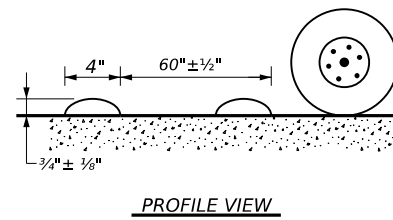
TWO LANE TWO-WAY HIGHWAYS



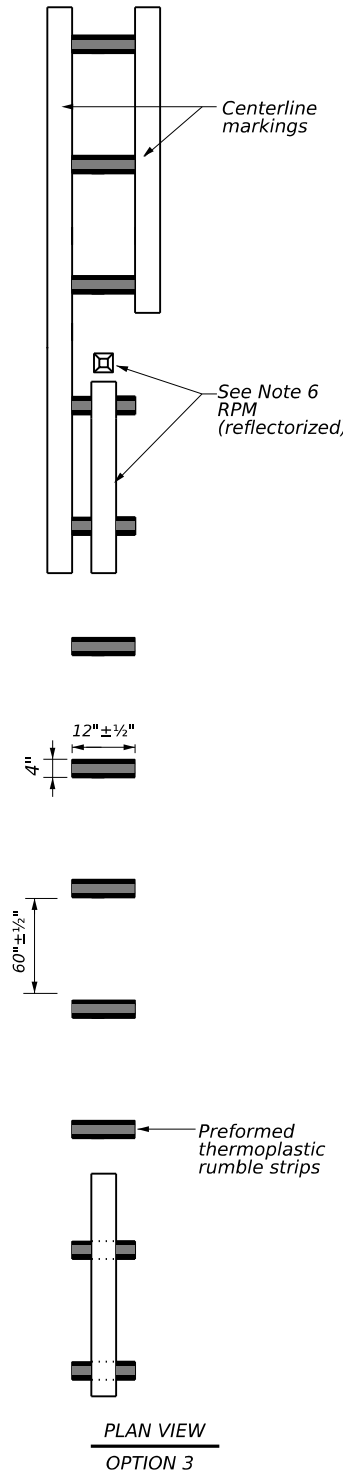
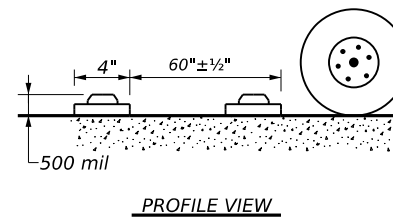
CENTERLINE RUMBLE STRIPS



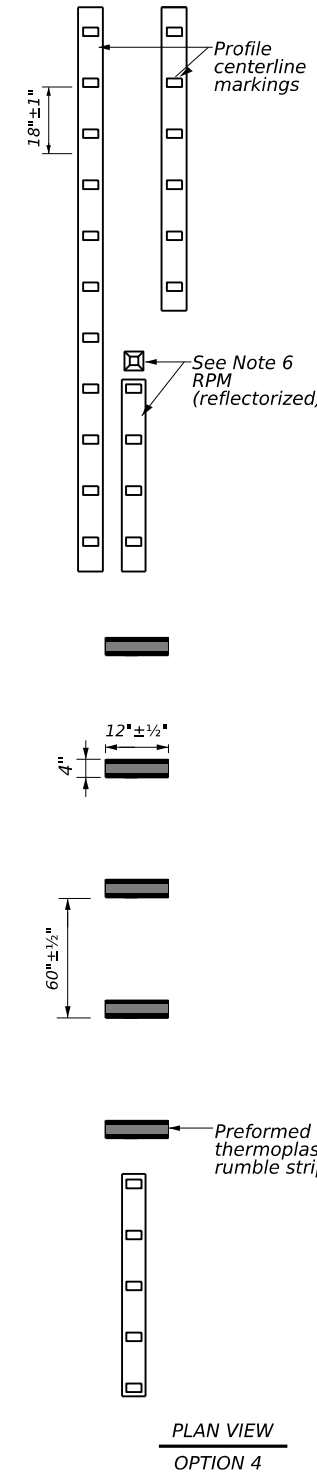
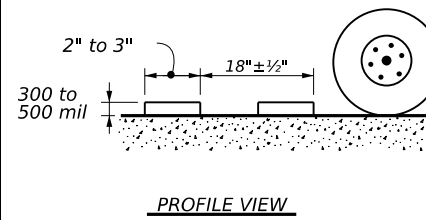
MILLED CENTERLINE RUMBLE STRIPS



RAISED CENTERLINE RUMBLE STRIPS



PREFORMED THERMOPLASTIC RUMBLE STRIPS



PROFILE CENTERLINE MARKINGS AND PREFORMED THERMOPLASTIC RUMBLE STRIPS

GENERAL NOTES

1. This standard sheet provides guidelines for installing centerline rumble strips on two-lane highways with or without shoulders.
2. Centerline and edge line rumble strips or profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.
3. Milled rumble strips are preferred when adequate pavement depth is available. If pavement thickness is less than 2 inches, milled rumble strips shall not be used. Rumble strips shall not be milled or depressed into bridge decks.
4. See dimensions for milled rumble strips. Other shapes and dimensions may be used if approved by the Traffic Safety Division.
5. Breaks in milled centerline rumble strips shall occur at least 50 feet and no more than 150 feet in advance of bridges, railroad crossings, intersections or driveways with high usage of large trucks.
6. Use standard sheet PM(2) for positioning, dimensioning, and spacing of all reflective raised pavement markers, pavement markings and profile markings.
7. Consideration should be given to noise levels when centerline rumble strips are to be installed near residential areas, schools, churches, etc. A 3/8 inch deep (minimum) milled rumble strip may be considered in these areas.
8. Pavement markings must be applied over milled centerline rumble strips.

WHEN INSTALLING CENTERLINE RUMBLE STRIPS:

9. Raised rumble strips consisting of non-reflective raised traffic buttons may be used. Non-reflective raised traffic buttons can be affixed to asphalt or concrete with bitumen or adhesives, as per manufacturer's recommendations.
10. When using non-reflective raised traffic buttons as a centerline rumble strip, the button shall be placed adjacent to the pavement marking delineating the centerline. The buttons will be paid for under Item 672, "Raised Pavement Markers." Non-reflective traffic buttons must meet the requirements of DMS-4300.
11. The color of the button should be yellow for a continuous no passing roadway. Black buttons should be used in areas where passing is allowed.
12. Consideration shall be given to bicyclists. See RS(6).

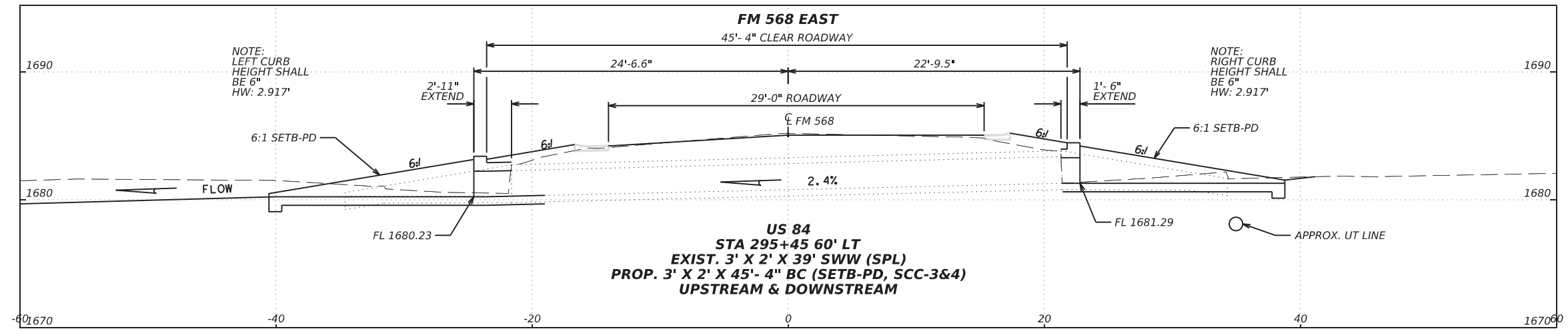
WHEN INSTALLING EDGE LINE RUMBLE STRIPS WITH OR WITHOUT CENTERLINE RUMBLE STRIPS ON UNDIVIDED HIGHWAYS:

13. See standard sheet RS(2).

CENTERLINE RUMBLE STRIPS ON TWO LANE TWO-WAY HIGHWAYS RS(4)-23			
FILE: rs(4)-23.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
© TxDOT January 2023	COWT: 0054	SECT: 04	JOB: 123
10-13 1-23	DIST: BWD	COUNTY: COLEMAN CO.	SHEET NO. 75

DATE: 1/11/2023 2:26:07 PM
 FILE: T:\BWD\SGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\4_Design\Master Design Files\SheetFiles\Culvert_Layout.dgn

DW: _____
 CK: _____
 CK: _____



ITEM	CODE	DESCRIPTION	QUANT.	UNIT
420	6051	*CL C CONC (CULV)	1.5	CY
467	6109	SET (TY I)(S=3FT)(HW=3FT)(6:1)(C)	2.0	EA
658	6073	INSTL OM ASSM (OM-2Y)(WC) GND (BI)	2.0	EA

*CLASS C CONCRETE QUANTITY INCLUDES BOTH CURBS



01/26/2023

CULVERT LAYOUT



SHEET OF		SHEET NO.				
CONT	SECT	JOB	HIGHWAY	DIST	COUNTY	SHEET NO.
0054	04	123	US 84	BWD	COLEMAN CO.	76

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DATE: 1/11/2023 8:43:33 AM
FILE: T:\BWDSDTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to Riprap Apron Design Standard.dgn

Culvert Station and/or Creek Name followed by applicable end (Lt, Rt or Both)	Description of Box Culvert No. Spans ~ Span X Height	Max Fill Height (Ft)	Applicable Box Culvert Standard (4)	Applicable Wingwall or End Treatment Standard	Skew Angle (0°, 15°, 30° or 45°)	Side Slope or Channel Slope Ratio (SL:1)	T Culvert Top Slab Thickness (In)	U Culvert Wall Thickness (In)	C Estimated Curb Height (Ft)	Hw (1) Height of Wingwall (Ft)	A Curb to End of Wingwall (Ft)	B Offset of End of Wingwall (Ft)	Lw Length of Longest Wingwall (Ft)	Ltw Culvert Toewall Length (Ft)	Atw Anchor Toewall Length (Ft)	Riprap Apron (CY)	Class "C" Conc (Curb) (CY) (2)	Class "C" Conc (Wingwall) (CY) (3)	Total Wingwall Area (SF)
US 84 293+75 60' LT	BOTH	1	SCC-3&4	SETB-PD	0	6:1	8	7	.5	2.917	N/A	N/A	16	N/A	4.167	0.0	.2	5.2	N/A

NOTES:

Skew = 0° on SW-0, FW-0, SETB-CD, SETB-SW-0, and SETB-FW-0 standard sheets;
30° maximum for safety end treatment

- SL:1 = Horizontal : 1 Vertical
- Side slope at culvert for flared or straight wingwalls.
 - Channel slope for parallel wingwalls.
 - Slope must be 3:1 or flatter for safety end treatments.

T = Box culvert top slab thickness. Dimension can be found on the applicable box culvert standard sheet.

U = Box culvert wall thickness. Dimension can be found on the applicable box culvert standard sheet.

C = Curb height

See applicable wing or end treatment standard sheets for calculations of Hw, A, B, Lw, Ltw, Atw, and Total Wingwall Area.

Hw = Height of wingwall

A = Distance from face of curb to end of wingwall (not applicable to parallel or straight wingwalls)

B = Offset of end of wingwall (not applicable to parallel or straight wingwalls)

Lw = Length of longest wingwall.

Ltw = Length of culvert toewall (not applicable when using riprap apron)

Atw = Length of anchor toewall (applicable to safety end treatment only)

Total Wingwall Area = Wingwall area in sq. ft. for two wingwalls (one structure end) if Lt or Rt.
Area for four wingwalls (two structure ends) if Both.

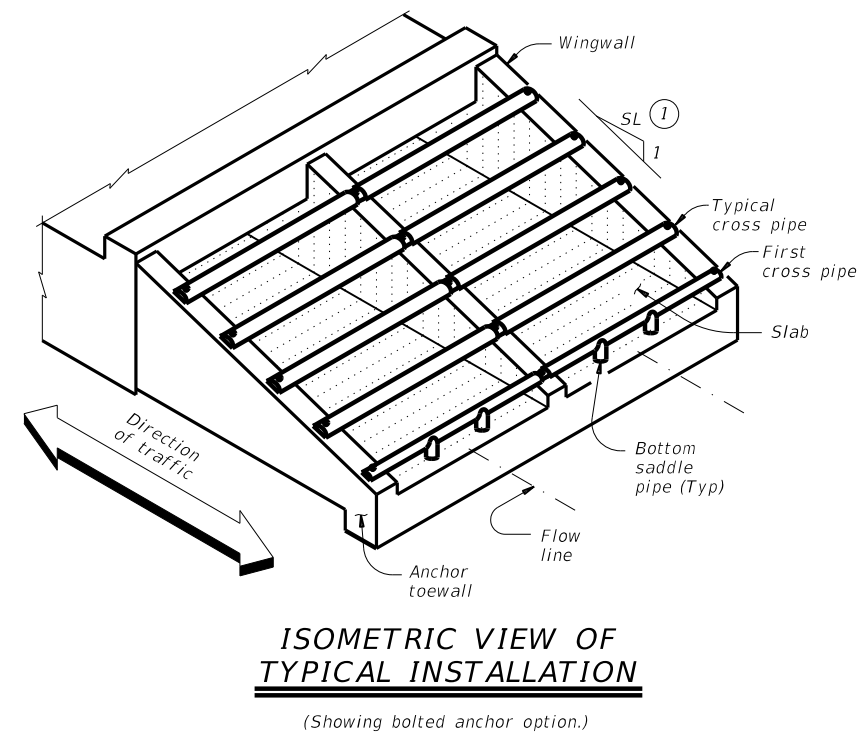
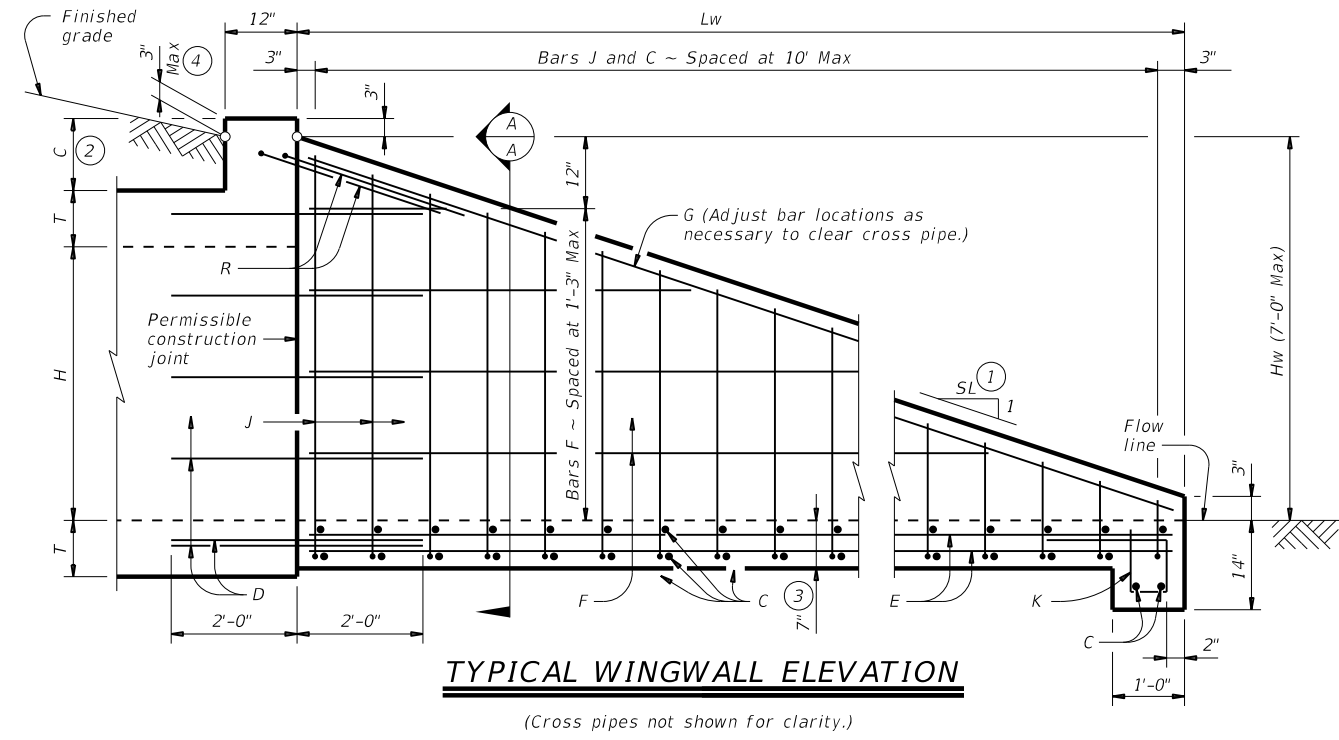
- ① Round the wall heights shown to the nearest foot for bidding purposes.
- ② Concrete volume shown is for box culvert curb only. For curbs using the Box Culvert Rail Mounting Details (RAC) standard sheet quantities shown must be increased by a factor of 2.25. If Class S concrete is required for the top slab of the culvert, also provide Class S concrete for the curb. Curb concrete is considered part of the Box Culvert for payment.
- ③ Concrete volume shown is total of wings, footings, culvert toewall (if any), anchor toewalls (if any) and wingwall toewalls. Riprap aprons, culverts, and curb quantities are not included.
- ④ Regardless of the type of culvert shown on this sheet, the Contractor has the option of furnishing cast-in-place or precast culverts unless otherwise shown elsewhere on the plans. If the Contractor elects to provide culverts of a different type than those shown on this sheet, it is the Contractor's responsibility to make the necessary adjustments to the dimensions and quantities shown.



01/26/2023

				Bridge Division Standard	
BOX CULVERT SUPPLEMENT WINGS AND END TREATMENTS					
BCS					
FILE: bcsstd1-20.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT	
©TxDOT February 2020	CONT 0054	SECT 04	JOB 123	HIGHWAY US 84	
REVISIONS	DIST BWD	COUNTY COLEMAN CO.	SHEET NO. 77		

DATE: 1/11/2023 8:44:53 AM
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 PROJECT: 0054-04-123 US 84 - SH 153 to CR 196
 DRAWING: SETB-PD
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WING DIMENSION CALCULATIONS:

$Hw = H + T + C - 0.250'$
 $Lw = (Hw - 0.250') (SL)$

For cast-in-place culverts:
 $Atw = (N) (S) + (N + 1) (U)$

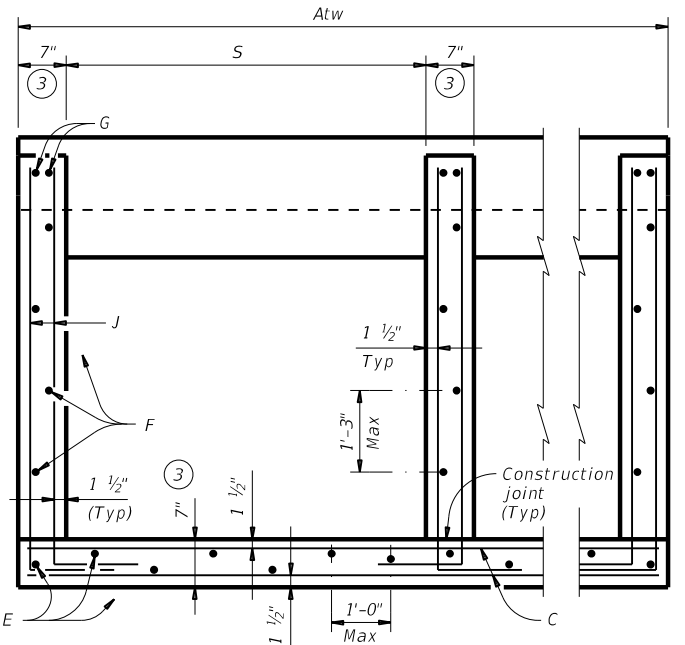
For precast culverts:
 $Atw = (N) (2U + S) + (N - 1) (0.500')$

Total Wingwall Area (SF)
 $= (0.5) (Hw + 0.250') (Lw) (N - 1)$

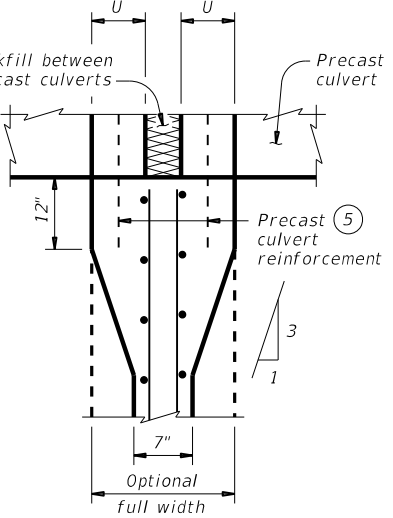
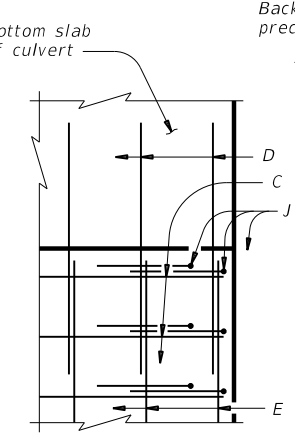
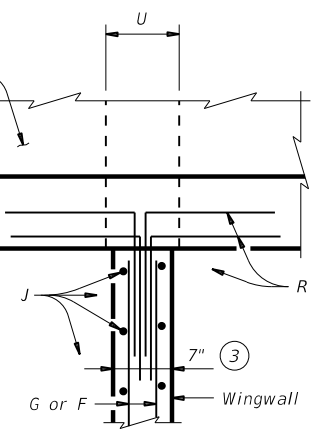
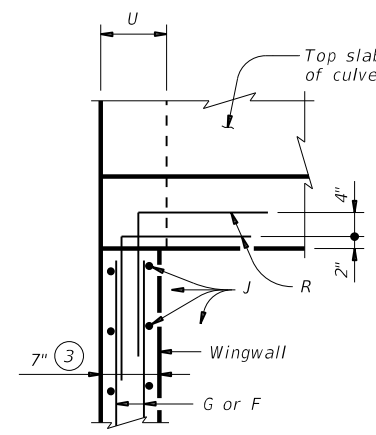
Total Concrete Volume (CY)
 $= [(Wingwall Area) (0.583') + (Lw) (Atw) (0.583') + (Atw) (1.000') (1.167' - 0.583')] \div (27)$

Total Reinforcing (Lb)
 $= (1.55) (Lw) (Atw) + (4.43) (Atw) + (K) (Hw) (N + 1) (\sqrt{Lw})$

C = Height of curb above top of top slab (feet)
 Hw = Height of wingwall (feet)
 K = Constant value for use in formulas
 Slope SL:1 K
 6:1 ~ 10.41
 Atw = Anchor toewall length (feet)
 Lw = Length of wingwall (feet)
 N = Number of culvert barrels
 SL:1 = Side slope ratio (horizontal : 1 vertical)
 See applicable box culvert standard for H, S, T, and U values.



SECTION A-A
 (Showing typical wingwall and wing slab reinforcing. Pipe runners not shown for clarity.)



PLAN VIEWS OF CORNER DETAILS

MATERIAL NOTES:

Provide Grade 60 reinforcing steel.
 Provide galvanized reinforcing steel if required elsewhere in the plans. Adjust reinforcing as necessary to provide a minimum clear cover of 1 1/2".
 Provide Class "C" concrete (f'c = 3,600 psi).
 Provide pipe runners, cross pipes, and anchor pipes meeting the requirements of ASTM A53 (Type E or S, Gr B), ASTM A500 Gr B, or API 5LX52.
 Provide ASTM A307 bolts.
 Galvanize all steel components, except the concrete reinforcing, unless required elsewhere in the plans, after fabrication.
 Repair galvanizing damaged during transport or construction in accordance with Item 445, "Galvanizing."

GENERAL NOTES:

Designed according to AASHTO LRFD Bridge Design Specifications.
 The safety end treatments shown herein are intended for use in those installations where out of control vehicles are likely to traverse the openings approximately perpendicular to the cross pipes.
 Cross pipes are designed for a traversing load of 10,000 pounds at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981.
 The quantities for concrete, reinforcing steel, and cross pipes resulting from the formulas given herein are for Contractor's information only.
 See the Box Culvert Supplement (BCS) standard sheet for additional dimensions and information.
 Alternate design drawings bearing the seal of a professional engineer will be acceptable for precast construction of the safety end treatments.

Cover dimensions are clear dimensions, unless noted otherwise. Reinforcing dimensions are out-to-out of bars.

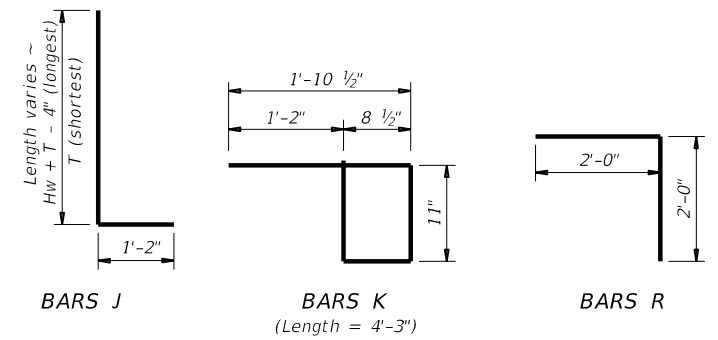


TABLE OF REINFORCING BAR SIZES AND SPACING

Bar	Size	Spacing
C	#4	10" Max
D	#4	Match F and E
E	#4	1'- 0" Max
F	#4	1'- 3" Max
G	#6	As shown
J	#4	10" Max
K	#4	1'- 0" Max
R	#4	As shown

- Provide 6:1 or flatter slope.
- 0" Min to 5'-0" Max. Estimated curb heights are shown elsewhere in the plans. For structures without railing and curbs taller than 1'-0", refer to Extended Curb Details the Extended Curb Details (ECD) standard sheet.
- Wingwall and slab thicknesses may be the same as the adjacent culvert wall and slab thicknesses (7" Minimum). If thicknesses greater than the minimum (7") are used, no changes will be made in quantities and no additional compensation will be allowed.
- For vehicle safety, reduce height, if necessary, to provide a maximum 3" projection above finished grade. No changes will be made in quantities and no additional compensation will be allowed for this work.
- For culverts with C = 0", the precast culvert reinforcing may extend 1'-0" minimum into wingwall. Wingwall bars D and R may be omitted. Otherwise, refer to the Wingwall Connection detail on the Box Culvert Precast Miscellaneous Details (SCP-MD) standard sheet.

Texas Department of Transportation
 Bridge Division Standard

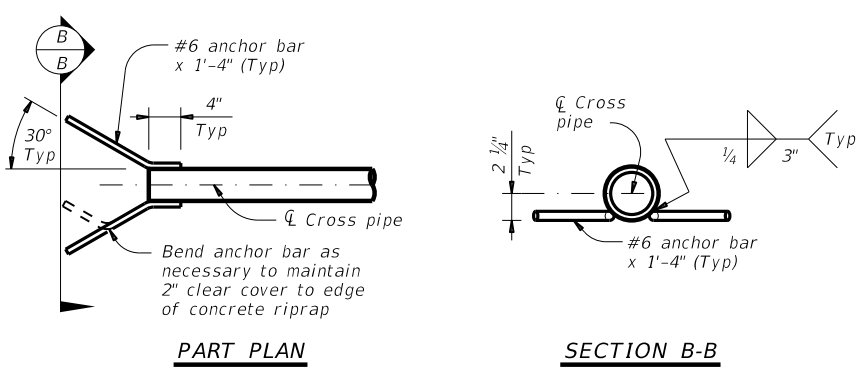
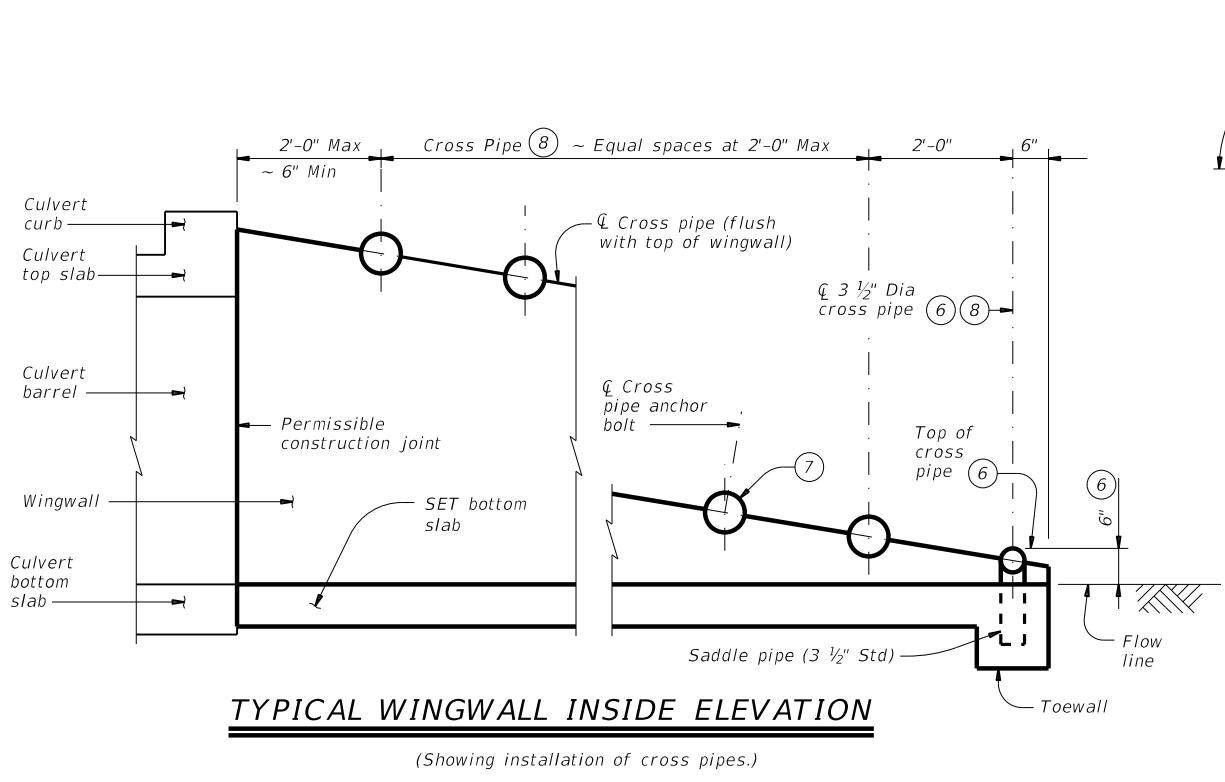
SAFETY END TREATMENT FOR BOX CULVERTS (MAXIMUM Hw = 7'-0") TYPE I ~ PARALLEL DRAINAGE

SETB-PD

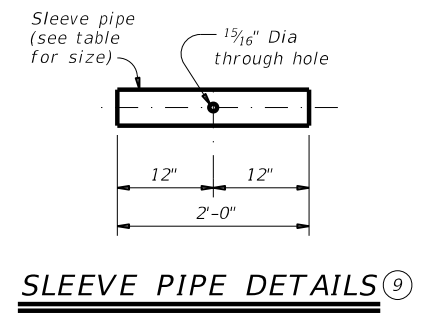
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REVISIONS	0054	04	123	US 84
06-2022 - Wing dimensions	DIST	COUNTY	SHEET NO.	
	BWD	COLEMAN CO.	78	

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 DESIGNER: Deshaun Williams
 CHECKER: TxDOT
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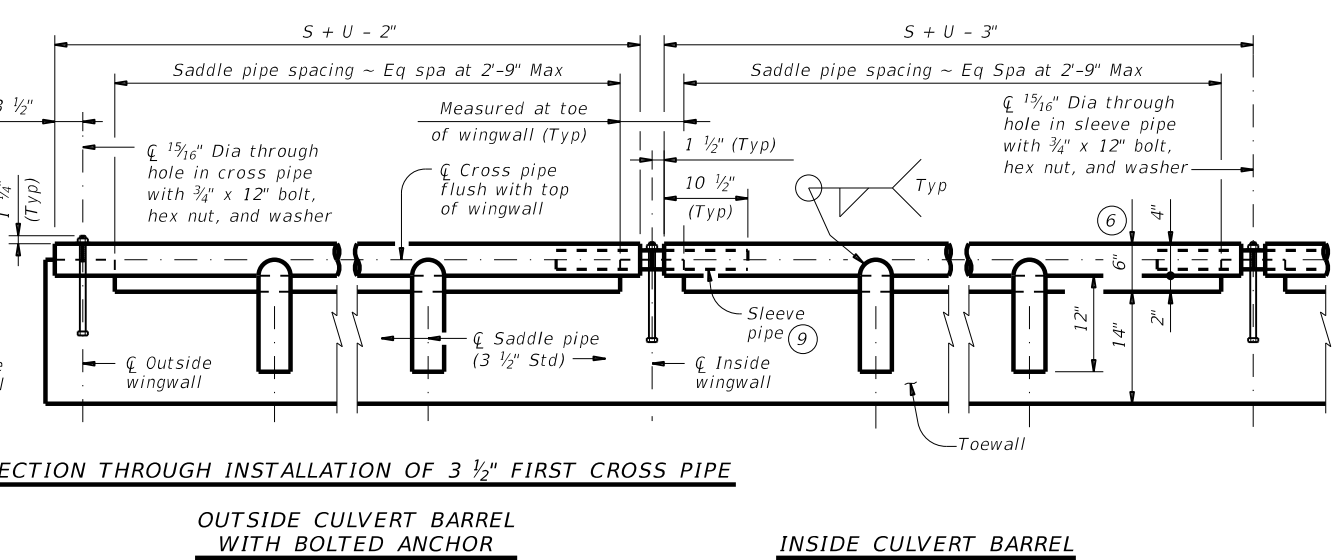
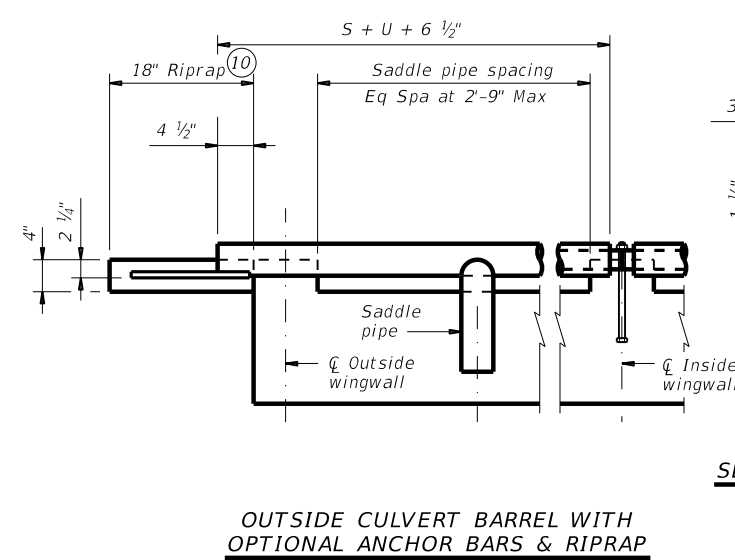
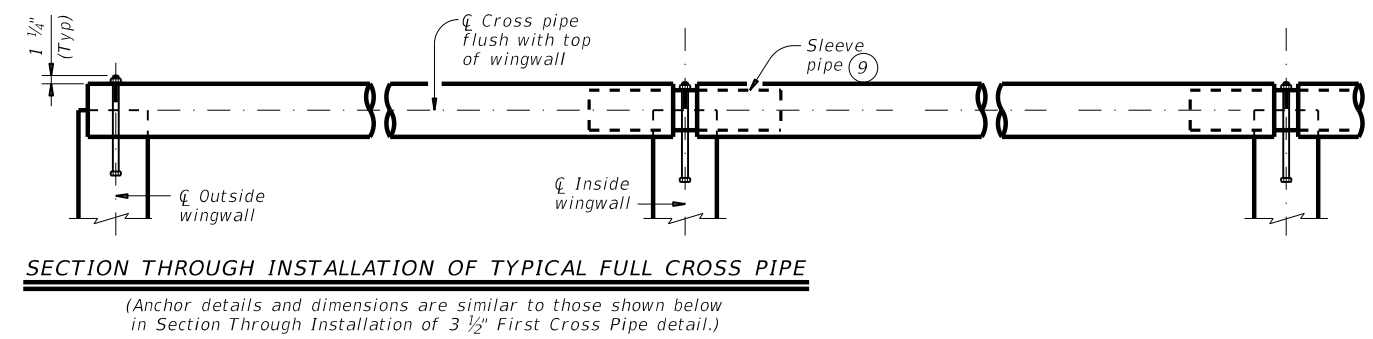
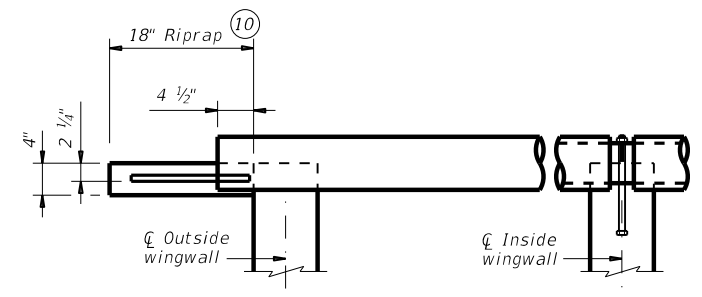
REQUIRED PIPE SIZES (8)			STANDARD PIPE SIZES		
Culvert Span Sizes	Cross Pipe Size	Sleeve Pipe Size (9)	Pipe Size	Pipe O.D.	Pipe I.D.
First Pipe	3 1/2" STD	2 1/2" STD	2 1/2" STD	2.875"	2.469"
30" to 42"	4" STD	3" STD	3" STD	3.500"	3.068"
48" to 72"	5" STD	4" STD	3 1/2" STD	4.000"	3.548"
78" to 120"	6" STD	5" STD	4" STD	4.500"	4.026"
			5" STD	5.563"	5.047"
			6" STD	6.625"	6.065"



OPTIONAL ANCHOR BAR DETAILS



- (6) The proper installation of the first cross pipe is critical for vehicle safety. Place the top of the first cross pipe at no more than 6" above the flow line.
- (7) Always install the third cross pipe from the bottom of the culvert using a bolted connection. Take care to ensure that concrete does not flow into this cross pipe so as to permit disassembly of the bolted connection to allow cleanout access.
- (8) Provide cross pipes and sleeve pipes (if required) as shown in the Required Pipe Sizes table. Provide 3 #2" saddle pipes for the 3 #2" first cross pipe.
- (9) At Contractor's option, make the cross pipe continuous across the inside wingwalls. If this option is selected, omit the sleeve pipe and make a 15/16" diameter throughhole in the cross pipe to accept the anchor bolt at the centerline of each interior wingwall.
- (10) Provide riprap when using the Optional Anchor Bar details. Riprap is included in the bid price for Safety End Treatment. Provide riprap in accordance with Item 432, "Riprap".



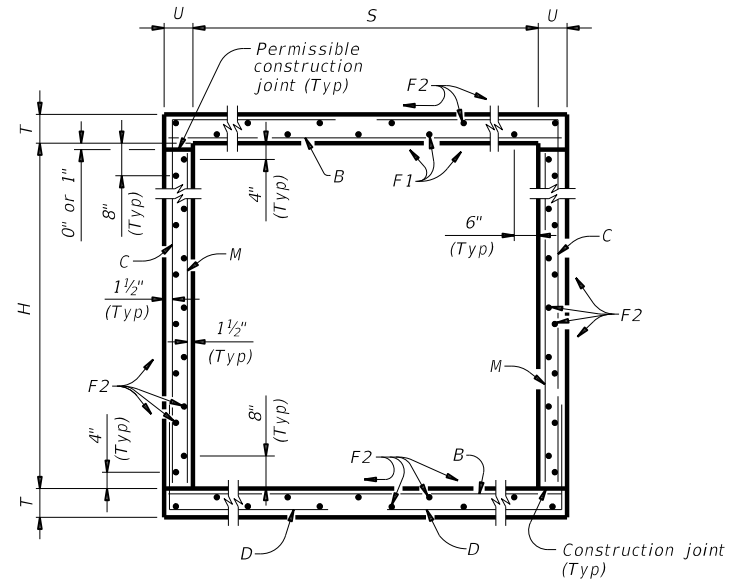
CROSS PIPE INSTALLATION DETAILS

SHEET 2 OF 2

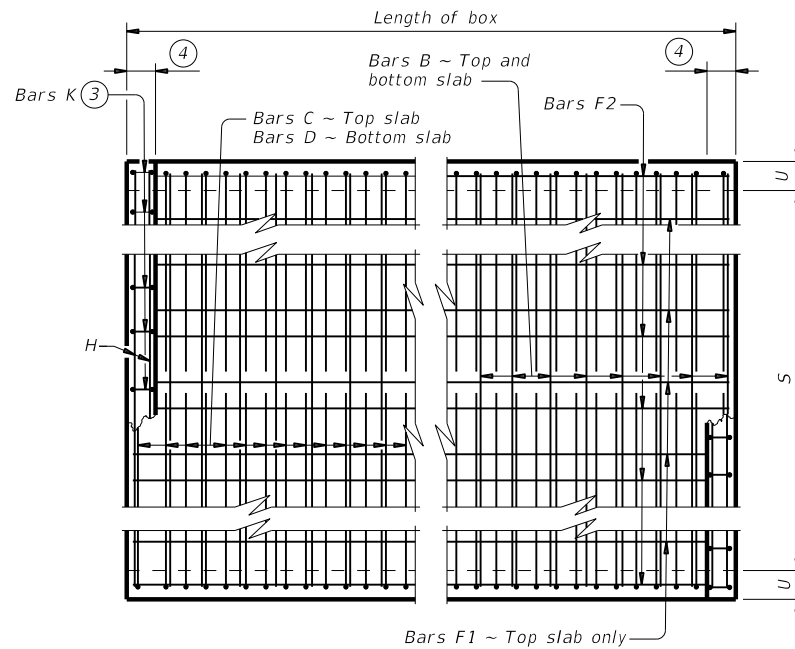
Texas Department of Transportation				Bridge Division Standard	
SAFETY END TREATMENT FOR BOX CULVERTS (MAXIMUM Hw = 7'-0") TYPE I ~ PARALLEL DRAINAGE					
SETB-PD					
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©TxDOT February 2020	CONT	SECT	JOB	HIGHWAY	
REVISIONS	0054	04	123	US 84	
06-2022 - Wing dimensions	DIST	COUNTY	SHEET NO.		
	BWD	COLEMAN CO.	79		

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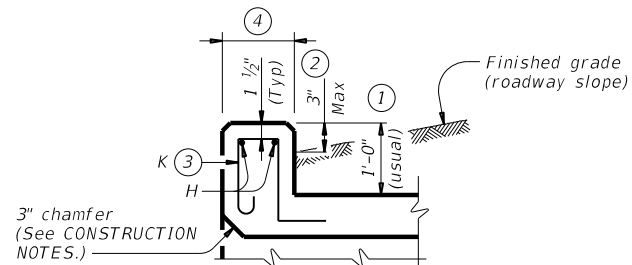
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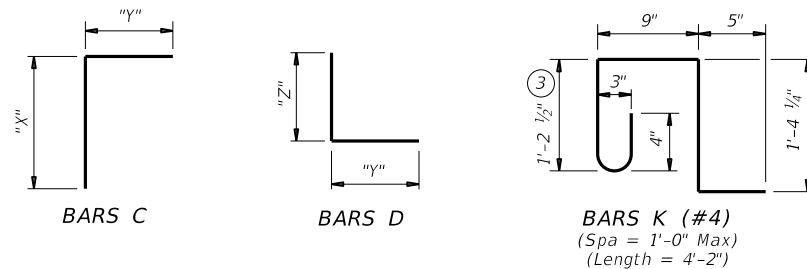
TYPICAL SECTION



PLAN OF REINF STEEL



SECTION THRU CURB



- ① 0" Min to 5'-0" Max. Estimated curb heights are shown elsewhere in the plans. For structures with pedestrian rail or curbs taller than 1'-0", refer to the Extended Curb Details (ECD) standard sheet. For structures with T631 or T631LS bridge rail, refer to the Mounting Details for T631 & T631LS Rails (T631-CM) standard sheet. Refer to the Rail Anchorage Curb (RAC) standard sheet for structures with bridge rail other than T631 or T631LS.
- ② For vehicle safety, the following requirements must be met:
 - For structures without bridge rail, construct curbs no more than 3" above finished grade.
 - For structures with bridge rail, construct curbs flush with finished grade. Reduce curb heights, if necessary, to meet the above requirements. No changes will be made in quantities and no additional compensation will be allowed for this work.
- ③ For curbs less than 1'-0" high, tilt Bars K or reduce bar height as necessary to maintain cover. For curbs less than 3" high, Bars K may be omitted.
- ④ 1'-0" typical. 2'-3" when the Rail Anchorage Curb (RAC) standard sheet is referred to elsewhere in the plans.

The Contractor may replace Bars B, C, D, E, F1, F2, M, Y, and/or Z with deformed welded wire reinforcement (WWR) meeting the requirements of ASTM A1064. The area of required reinforcement may be reduced by the ratio of 60 ksi / 70 ksi. Spacing of WWR is limited to 4" Min and 18" Max. When required, provide lap splices in the WWR of the same length required for the equivalent bar size, rounded up for wire sizes between conventional bar sizes. The lap length required for WWR is never less than the lap length required for uncoated #4 bars.

Example conversion: Replacing No. 6 Gr 60 at 6" Spacing with WWR.
 Required WWR = (0.44 sq. in. per 0.5 ft.) x (60 ksi / 70 ksi) = 0.755 sq. in. per ft.
 If D30.6 wire is used to meet the 0.755 sq. in. per ft. requirement in this example, the required spacing = (0.306 sq. in.) / (0.755 sq. in. per ft.) x (12 in. per ft.) = 4.86" Max spacing. Required lap length for the provided D30.6 wire is 2'-1" (the same minimum lap length required for uncoated #5 bars, as listed under MATERIAL NOTES).

CONSTRUCTION NOTES:

- Do not use permanent forms.
- Chamfer the bottom edge of the top slab 3" at the entrance.
- Optionally, raise construction joints shown at the flow line by a maximum of 6". If this option is taken, Bars M may be cut off or raised, Bars C and D may be reversed.

MATERIAL NOTES:

- Provide Grade 60 reinforcing steel.
- Provide galvanized reinforcing steel if required elsewhere in the plans.
- Provide Class C concrete (f'c = 3,600 psi) for culvert barrel and curb, with the following exceptions: provide Class S concrete (f'c = 4,000 psi) for top slabs of:
 - culverts with overlay,
 - culverts with 1-to-2 course surface treatment, or
 - culverts with the top slab as the final riding surface.
- Provide bar laps, where required, as follows:
 - Uncoated or galvanized ~ #4 = 1'-8" Min
 - Uncoated or galvanized ~ #5 = 2'-1" Min

GENERAL NOTES:

- Designed according to AASHTO LRFD Bridge Design Specifications for the range of fill heights shown.
- See the Single Box Culverts Cast-In-Place Miscellaneous Detail (SCC-MD) standard sheet for details pertaining to skewed ends, angle sections, and lengthening.

Cover dimensions are clear dimensions, unless noted otherwise.
 Reinforcing bar dimensions shown are out-to-out of bar.

HL93 LOADING		SHEET 1 OF 2	
			Bridge Division Standard
SINGLE BOX CULVERTS CAST-IN-PLACE 0' TO 30' FILL			
SCC-3 & 4			
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©TxDOT February 2020	CONT	SECT	JOB
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SECTION DIMENSIONS				FILL HEIGHT ⁵	BILLS OF REINFORCING STEEL (For Box Length = 40 feet)																								QUANTITIES														
					Bars B					Bars C					Bars D					Bars M ~ #4				Bars F1 ~ #4 at 18" Spa			Bars F2 ~ #4 at 18" Spa			Bars H 4 ~ #4		Bars K		Per Foot of Barrel		Curb		Total					
					S	H	T	U	No.	Size	Spa	Length	Weight	No.	Size	Spa	Length	Weight	" X "	" Y "	No.	Size	Spa	Length	Weight	" Y "	" Z "	No.	Spa	Length	Weight	No.	Length	Wt	No.	Length	Weight	Length	Wt	No.	Wt	Conc (CY)	Reinf (Lb)
3' - 0"	2' - 0"	8"	7"	30'	108	#5	9"	3' - 11"	441	108	#4	9"	5' - 4"	385	2' - 6"	2' - 10"	108	#4	9"	5' - 1"	367	2' - 10"	2' - 3"	108	9"	2' - 0"	144	3	39' - 9"	80	19	39' - 9"	505	3' - 11"	10	10	28	0.292	48.1	0.3	38	12.0	1,960
3' - 0"	3' - 0"	8"	7"	30'	108	#5	9"	3' - 11"	441	108	#4	9"	6' - 4"	457	3' - 6"	2' - 10"	108	#4	9"	5' - 1"	367	2' - 10"	2' - 3"	108	9"	3' - 0"	216	3	39' - 9"	80	23	39' - 9"	611	3' - 11"	10	10	28	0.335	54.3	0.3	38	13.7	2,210
4' - 0"	2' - 0"	8"	7"	30'	108	#5	9"	4' - 11"	554	162	#4	6"	5' - 8"	613	2' - 6"	3' - 2"	162	#4	6"	5' - 5"	586	3' - 2"	2' - 3"	108	9"	2' - 0"	144	3	39' - 9"	80	21	39' - 9"	558	4' - 11"	13	12	33	0.342	63.4	0.4	46	14.1	2,581
4' - 0"	3' - 0"	8"	7"	30'	108	#5	9"	4' - 11"	554	162	#4	6"	6' - 8"	721	3' - 6"	3' - 2"	162	#4	6"	5' - 5"	586	3' - 2"	2' - 3"	108	9"	3' - 0"	216	3	39' - 9"	80	25	39' - 9"	664	4' - 11"	13	12	33	0.385	70.5	0.4	46	15.8	2,867
4' - 0"	4' - 0"	8"	7"	30'	108	#5	9"	4' - 11"	554	162	#4	6"	7' - 8"	830	4' - 6"	3' - 2"	162	#4	6"	5' - 5"	586	3' - 2"	2' - 3"	108	9"	4' - 0"	289	3	39' - 9"	80	25	39' - 9"	664	4' - 11"	13	12	33	0.428	75.1	0.4	46	17.5	3,049

⁵ For direct traffic culverts (fill height ≤ 2 ft.), identify the required box size and select the option with the minimum fill height.

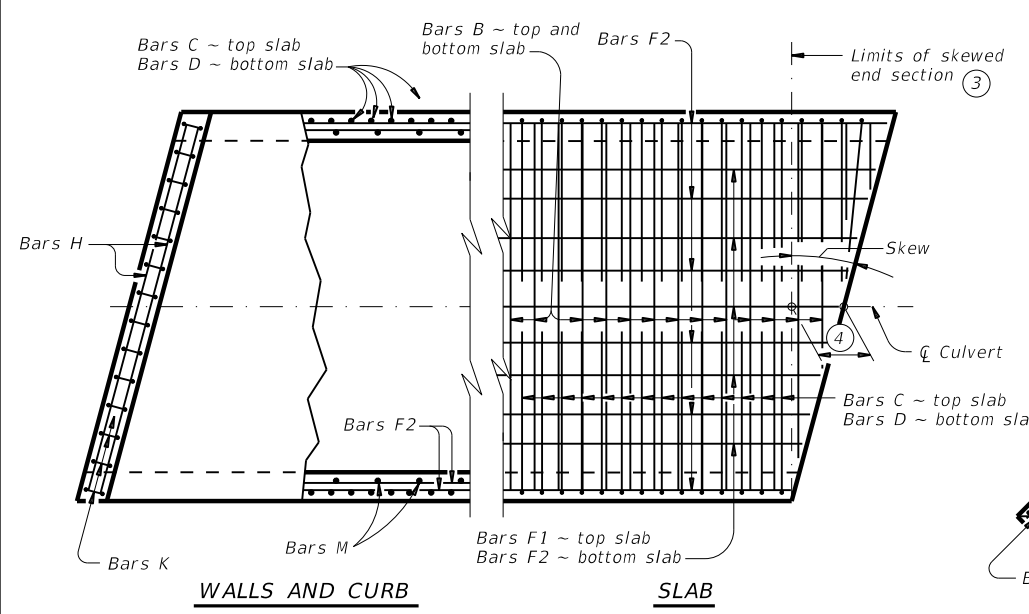


**SINGLE BOX CULVERTS
CAST-IN-PLACE
0' TO 30' FILL**

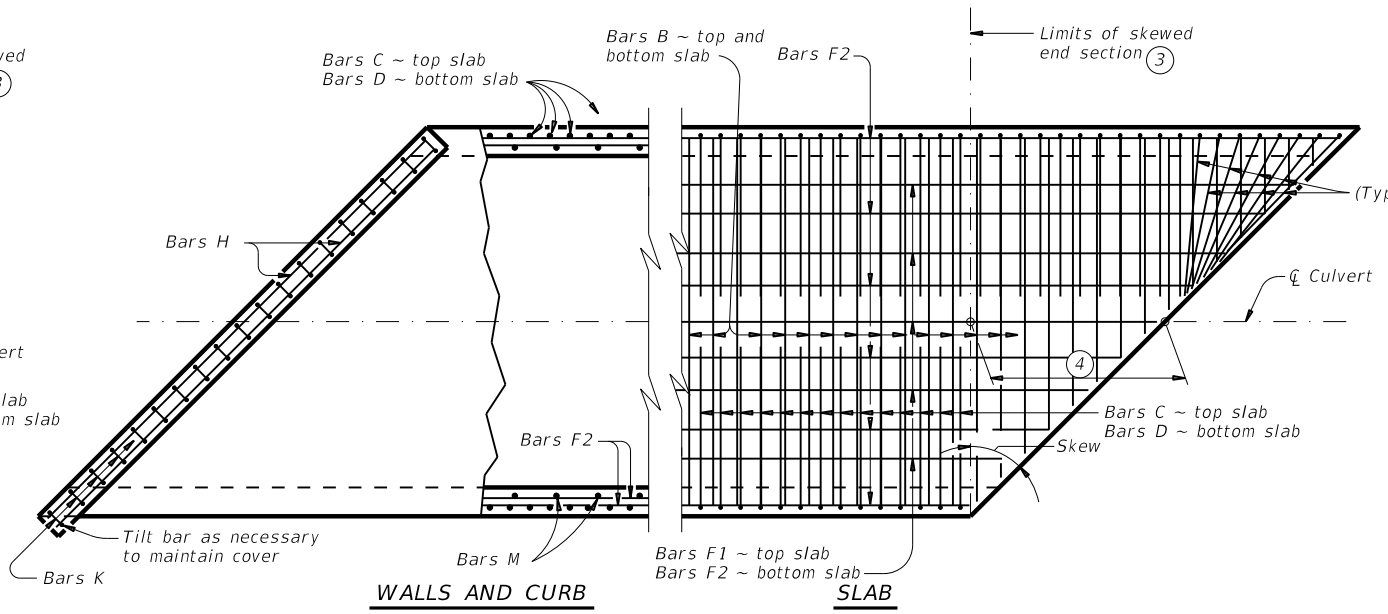
SCC-3 & 4

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04/2021 Updated X values.	DIST	COUNTY	SHEET NO.	
	BWD	COLEMAN CO.	81	

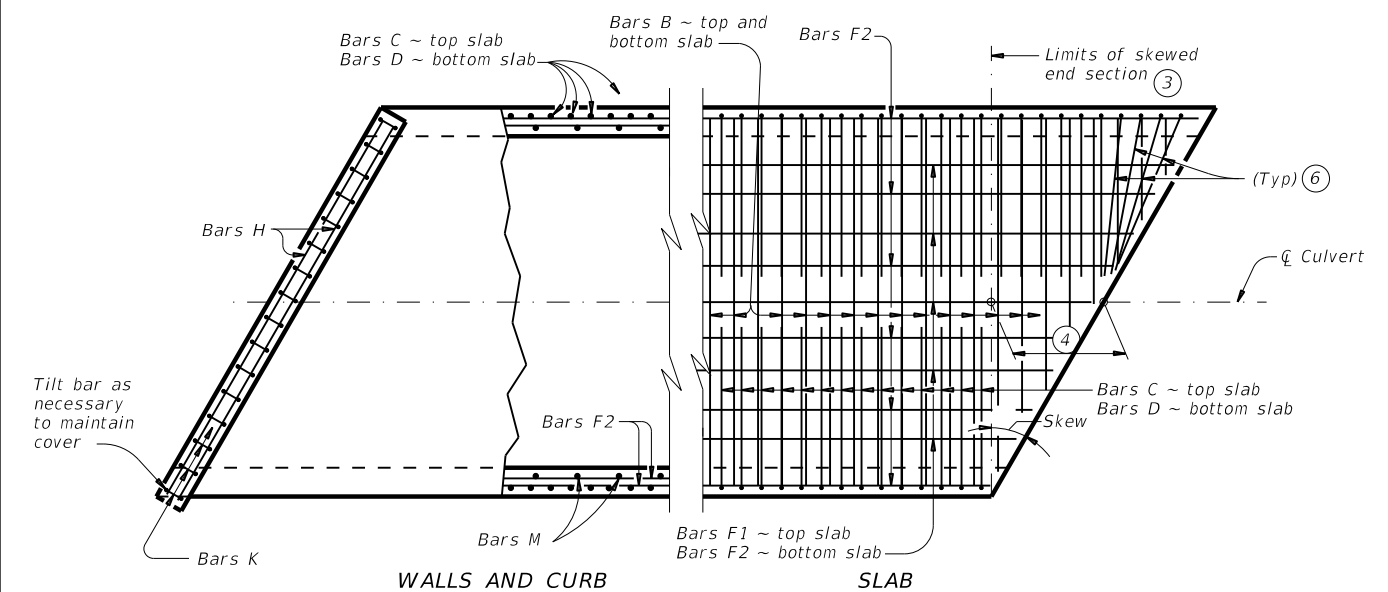
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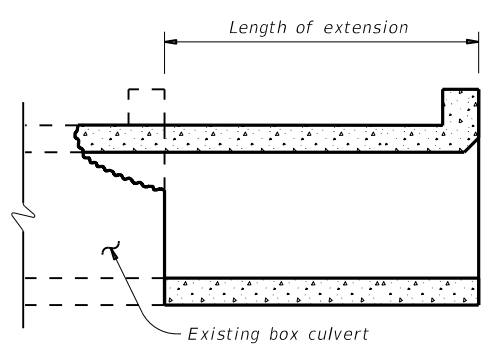
PLAN OF SKEWED ENDS ~ FROM 0° TO 15°



PLAN OF SKEWED ENDS ~ OVER 30° TO 45°



PLAN OF SKEWED ENDS ~ OVER 15° TO 30°



LENGTHENING DETAIL

① For skewed box culverts with less than 2'-0" of fill, break back the top slab to provide a 1'-10" minimum lap of the existing longitudinal bars with the longitudinal bars in the extension.
 For non-skewed box culverts with less than 2'-0" of fill and for skewed or non-skewed culverts with a fill depth of 2'-0" or greater, break back the top slab to provide a 1'-10" minimum lap of the existing longitudinal bars with the longitudinal bars in the extension. Alternatively, if the box is non-skewed, embed #6 anchor bars with a Type III, C, D, E, or F anchor adhesive into the existing walls, top and bottom slab at 1'-6" center-to-center spacing. Minimum embedment depth is 8". Anchor adhesive chosen must be able to achieve a basic bond strength in tension, Nba, of 26.4 kips. Submit signed and sealed calculations or the manufacturer's published literature showing the proposed anchor adhesive's ability to develop this load to the Engineer for approval prior to use. Anchor installation, including hole size, drilling, and clean out, must be in accordance with Item 450, "Railing." Test adhesive anchors in accordance with Item 450.3.3, "Tests." Test 3 anchors per 100 anchors installed.
 Break back wings and apron as necessary to install the extension. Clean and extend the exposed wingwall and apron reinforcing into the extension. When lengthening existing box culverts with dimensions different than current standard dimensions, form horizontal and vertical transitions as directed by the Engineer. Match bottom slabs to maintain an uninterrupted flow line. Field bend existing and new reinforcing into transitions and maintain specified cover requirements. For top slabs of culverts with overlay, with 1-to-2 course surface treatment, or with the top slab as the final riding surface, adjust the "H" dimension to provide a smooth riding surface.

- ② When the spacing between Bars B becomes less than half of the normal spacing, cut bars to avoid conflict.
- ③ The length of Bars B vary in the skewed end sections.
- ④ $[One\ half\ of\ overall\ width] \times [tangent\ of\ the\ skew\ angle]$
- ⑤ Place Bars F1 and F2 continuously through the angle section. Bend Bars F1 and F2 to remain parallel to the walls of the box culvert.
- ⑥ When necessary to avoid conflict in acute corners, shorten the slab extension leg of Bars C and Bars D to a minimum of 1'-6" for skews of 30° thru 45°.
- ⑦ At the Contractor's option, for skews of 15° or less, place Bars B, C, and D parallel to the skewed end while maintaining spacing along centerline of box. Increase lengths of Bars B shown on the Single Box Culverts Cast-In-Place (SCC) standards sheets to accommodate the skew.

CONSTRUCTION NOTES:
 Do not use permanent forms.
 When required, lap Bars H 1'-8" for uncoated or galvanized bars.
 Provide a minimum of 1 1/2" clear cover.

MATERIAL NOTES:
 Provide Grade 60 reinforcing steel.
 Provide galvanized reinforcing steel, if required elsewhere in the plans.
 Provide Class C concrete (f'c = 3,600 psi) with these exceptions:
 provide Class S concrete (f'c = 4,000 psi) for top slabs of culverts with overlay, with 1-to-2 course surface treatment, or with the top slab as the final riding surface.

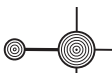

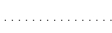



GENERAL NOTES:
 Designed according to AASHTO LRFD Bridge Design Specifications.
 Refer to Single Box Culverts Cast-in-Place (SCC) standard sheets for details of straight sections of culvert.
 For skewed sections and angle sections, refer to Single Box Culverts Cast-in-Place (SCC) standard sheets for slab and wall dimensions, bar sizes, maximum bar spacing, and any other details not shown.
 For skewed ends with curbs, adjust length of Bars H, number of Bars K, curb concrete volume, and reinforcing steel weight by dividing the values shown on the culvert Single Box Culverts Cast-In-Place (SCC) standard sheets by the cosine of the skew angle.

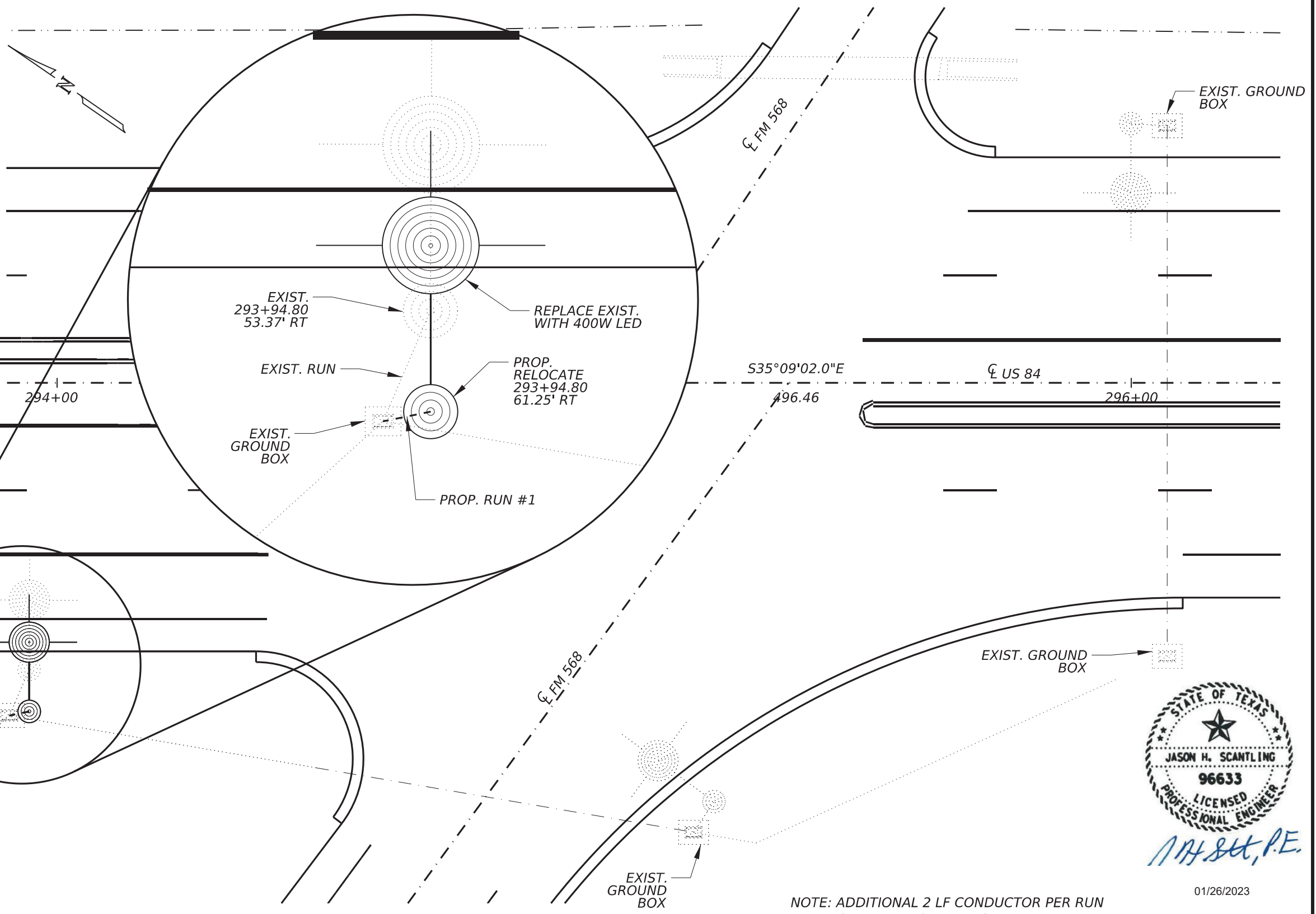
Cover dimensions are clear dimensions, unless noted otherwise.

HL93 LOADING

		Bridge Division Standard	
SINGLE BOX CULVERTS CAST-IN-PLACE MISCELLANEOUS DETAILS			
SCC-MD			
FILE: sccmdste-20.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
REVISIONS	CONTRACT NO. 0054	SECTION 04	JOB NO. 123
			HIGHWAY US 84
	DIST. BWD	COUNTY COLEMAN CO.	SHEET NO. 82

LEGEND

-  RDWY ILL ASSEMBLY
-  EXIST. ELEC CONDR - BORE
-  EXSIT. ELEC CONDR
-  PROP ELEC CONDR
-  GROUND BOX TY A W/APRON
-  PROP ELEC SERV TY D (120/240)



01/26/2023

NOTE: ADDITIONAL 2 LF CONDUCTOR PER RUN ESTIMATED FOR TIE-INS

CSJ: 0054-04-123

ITEM	CODE	DESCRIPTION	UNIT	QUANT.
416	6029	DRILL SHAFT (RDWY ILL POLE) (30")	LF	10.0
610	6004	RELOCATE RD IL ASM (TRANS-BASE)	EA	1.0

RUN NO.	CONDUIT LENGTH (FT)	ELEC CONDUCTOR		DESCRIPTION
		NO. 8 BARE (LF)	NO. 8 INSULATED (LF)	
EXIST. RUN	6.5	6.5	13.0	ILLUMINATION
PROP. RUN #1	4.0	6.0	12.0	ILLUMINATION

US 84 @ FM 568 ILLUMINATION LAYOUT



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	83	

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FILE: ...\\illumination_layout.dgn

DATE: 1/11/2023 9:00:36 AM
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GENERAL NOTES FOR ALL ELECTRICAL WORK

- The location of all conduits, junction boxes, ground boxes, and electrical services is diagrammatic and may be shifted to accommodate field conditions.
- Provide new and unused materials. Ensure that all materials and installations comply with the applicable articles of the National Electrical Code (NEC), TxDOT standards and specifications, National Electrical Manufacturers Association (NEMA), and are listed by Underwriters Laboratories (UL) or a Nationally Recognized Testing Lab (NRTL). NRTLs such as Canadian Standard Association (CSA), Intertek Testing Services NA Inc., or FM Approvals LLC can be considered equivalent to UL. Where reference is made to NEMA listed devices, International Electrotechnical Commission (IEC) listed devices will not be considered an acceptable equal to a NEMA listed device. Acceptable devices may have both a NEMA and IEC listing. Faulty fabrication or poor workmanship in any material, equipment, or installation is justification for rejection. Replace or reinstall rejected material or equipment at no additional cost to the Department.
- Miscellaneous nuts, bolts and hardware, except for high strength bolts, may be stainless steel when plans specify galvanized, provided the bolt size is 1/2 in. or less in diameter.
- Provide the following test equipment as required by the Engineer to confirm compliance with the contract and the NEC: voltmeter, ammeter, megohm meter (1000 volt DC), ground resistance tester, torque wrenches, and torque screwdrivers. Ensure all equipment has been properly calibrated within the last year. Provide calibration certification to the Engineer upon request. Operate test equipment during inspection as requested by the Engineer.
- Install grounding as shown on the plans and in accordance with the NEC. Ensure all metallic conduits; metal poles; luminaires; and metal enclosures are bonded to the equipment grounding conductor. Provide stranded bare copper or green insulated grounding conductors. Ground rods, connectors, and bonding jumpers are subsidiary to the various bid items.
- When required by the Engineer, notify the Department in writing of materials from the Material Producers List (MPL) intended for use on each project. Prequalified materials are listed on the MPL on TxDOT's website under "Roadway Illumination and Electrical Supplies." No substitutions will be allowed for materials on this list.

CONDUIT

A. MATERIALS

- Provide conduit, junction boxes, fittings, and hardware as per TxDOT Departmental Material Specification (DMS) 11030 "Conduit" and Item 618 "Conduit" of TxDOT's "Standard Specifications For Construction And Maintenance Of Highways, Streets, And Bridges," latest edition. Provide conduits listed under Item 618 on the MPL under "Roadway Illumination and Electrical Supplies." Provide conduit types according to the descriptive code or as shown on the plans. Do not substitute other types of conduits for those shown. Provide liquidtight flexible metal conduit (LFMC) when flexible conduit is called for on galvanized steel rigid metallic conduit (RMC) systems. Provide liquidtight flexible nonmetallic conduit (LFNC) when flexible conduit is called for on polyvinyl chloride (PVC) systems.
- Provide galvanized steel RMC for all exposed conduits, unless otherwise shown on the plans. Properly bond all metal conduits.
- Unless otherwise shown on the plans, provide junction boxes with a minimum size as shown in the following table, which applies to the greatest number of conductors entering the box through one conduit with no more than four conduits per box. When a mixture of conductor sizes is present, count the conductors as if all are of the larger size. For situations not applicable to the table, size junction boxes in accordance with NEC.



AWG	3 CONDUCTORS	5 CONDUCTORS	7 CONDUCTORS
#1	10" x 10" x 4"	12" x 12" x 4"	16" x 16" x 4"
#2	8" x 8" x 4"	10" x 10" x 4"	12" x 12" x 4"
#4	8" x 8" x 4"	10" x 10" x 4"	10" x 10" x 4"
#6	8" x 8" x 4"	8" x 8" x 4"	10" x 10" x 4"
#8	8" x 8" x 4"	8" x 8" x 4"	8" x 8" x 4"

- Junction boxes with an internal volume of less than 100 cu. in. and supported by entering raceways must have threaded entries or hubs identified for the intended purpose and supported by connection of two or more rigid metal conduits. Secure conduit within 3 ft. of the enclosure or within 18 in. of the enclosure if all conduit entries are on the same side. Mechanically secure all junction boxes with an internal volume greater than 100 cu. inches.
- Provide hot dipped galvanized cast iron or sand cast aluminum outlet boxes for junction boxes containing only 10 AWG or 12 AWG conductors. Do not use die cast aluminum boxes. Size outlet boxes according to the NEC.
- Do not use intermediate metal conduit (IMC) or electrical metallic tubing (EMT) unless specifically required by the plan sheets. When EMT is called for, provide junction boxes made from galvanized steel sheeting, listed and approved for outdoor use, unless otherwise noted on the plans. Size all galvanized steel junction boxes in accordance with the NEC. Provide junction boxes for IMC conduit systems that meet the same requirements for junction boxes used with RMC systems.
- Provide PVC junction boxes intended for outdoor use on PVC conduit systems, unless otherwise noted on the plans.

- Provide PVC elbows in PVC conduit systems, unless otherwise shown on the plans. Use only a flat, high tensile strength polyester fiber pull tape for pulling conductors through the PVC conduit system. When galvanized steel RMC elbows are specifically called for in the plans and any portion of the RMC elbow is buried less than 18 in., ground the RMC elbow by means of a grounding bushing on a rigid metal extension. Grounding of the rigid metal elbow is not required if the entire RMC elbow is encased in a minimum of 2 in. of concrete. PVC extensions are allowed on these concrete encased rigid metal elbows. RMC or PVC elbows are subsidiary to various bid items.
- When required, provide High-Density Polyethylene (HDPE) conduit with factory installed internal conductors according to Item 622 "Duct Cable." At the Contractor's request and with approval by the Engineer, substitute HDPE conduit with no conductors for bored schedule 40 or schedule 80 PVC conduit bid under Item 618. Ensure bored HDPE substituted for PVC is schedule 40 and of the same size PVC called for in the plans. Ensure the substituted HDPE meets the requirements of Item 622, except that the conduit is supplied without factory-installed conductors. Make the transition of the HDPE conduit to PVC (or RMC elbow when required) at the bore pit. Provide conduit of the size and schedule as shown on the plans. Do not extend substituted conduit into ground boxes or foundations. Provide PVC or galvanized steel RMC elbows as called for at all ground boxes and foundations.
- Use two-hole straps when supporting 2 in. and larger conduits. On electrical service poles, properly sized stainless steel or hot dipped galvanized one-hole standoff straps are allowed on the service riser conduit.

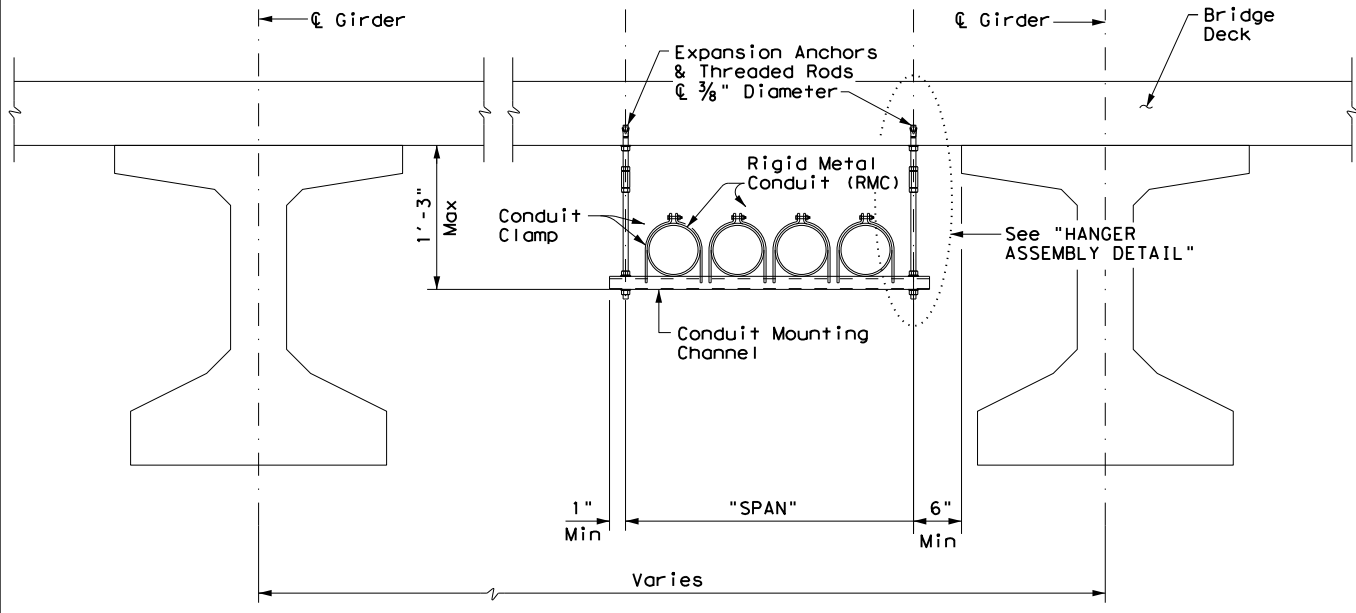
B. CONSTRUCTION METHODS

- Provide and install expansion joint conduit fittings on all structure-mounted conduits at the structure's expansion joints to allow for movement of the conduit. In addition, provide and install expansion joint fittings on all continuous runs of galvanized steel RMC conduit externally exposed on structures such as bridges at maximum intervals of 150 ft. When requested by the project Engineer, supply manufacturer's specification sheet for expansion joint conduit fittings. Repair or replace expansion joint fittings that do not allow for movement at no additional cost to the Department. Provide the method of determining the amount of expansion to the Engineer upon request. Do not use LFMC or LFNC as a substitute for the required expansion conduit fittings.
- Space all conduit supports at maximum intervals of 5 ft. Install conduit spacers when attaching metal conduit to surface of concrete structures. See "Conduit Mounting Options" on ED(2). Install conduit support within 3 ft. of all enclosures and conduit terminations.
- Do not attach conduit supports directly to pre-stressed concrete beams except as shown specifically in the plans or as approved by the Engineer.
- Unless otherwise shown on the plans, jack or bore conduit placed beneath existing roadways, driveways, sidewalks, or after the base or surfacing operation has begun. Backfill and compact the bore pits below the conduit per Item 476 "Jacking, Boring, or Tunneling Pipe or Box" prior to installing conduit or duct cable to prevent bending of the connections.
- When placing conduit in the sub-grade of new roadways, backfill all trenches with excavated material unless otherwise noted on the plans. When placing conduit in the sub-base of new roadways, backfill all trenches with cement-stabilized base as per requirements of Items 110 "Excavation", 400 "Excavation and Backfill for Structures", 401 "Flowable Backfill", 402 "Trench Excavation Protection", and 403 "Temporary Special Shoring."
- Provide and place warning tape approximately 10 in. above all trenched conduit as per Item 618.
- During construction, temporarily cap or plug open ends of all conduit and raceways immediately after installation to prevent entry of dirt, debris and animals. Temporary caps constructed of durable duct tape are allowed. Tightly fix the tape to the conduit opening. Clean out the conduit and prove it clear in accordance with Item 618 prior to installing any conductors.
- Ensure conduit entry into the top of any enclosure is waterproof by installing conduit sealing hubs or using boxes with threaded bosses. This includes surface mounted safety switches, meter cans, service enclosures, auxiliary enclosures and junction boxes. Grounding bushings on water tight sealing hubs are not required.
- Fit the ends of all PVC conduit terminations with bushings or bell end fittings. Provide and install a grounding type bushing on all metal conduit terminations.
- Install a bonding jumper from each grounding bushing to the nearest ground rod, grounding lug, or equipment grounding conductor. Ensure all bonding jumpers are the same size as the equipment grounding conductor. Bonding of conduit used as a casing under roadways for duct cable is not required, if the duct extends the full length through the casing.
- At all electrical services, install a 6 AWG solid copper grounding electrode conductor.
- Place conduits entering ground boxes so that the conduit openings are between 3 in. and 6 in. from the bottom of the box. See the ground box detail on sheet ED(4).
- Seal ends of all conduits with duct seal, expandable foam, or by other methods approved by the Engineer. Seal conduit immediately after completion of conductor installation and pull tests. Do not use duct tape as a permanent conduit sealant. Do not use silicone caulk as a conduit sealant.
- File smooth the cut ends of all mounting strut and conduit. Before installing, paint the field cut ends of all mounting strut and RMC (threaded or non-threaded) with zinc rich paint (94% or more zinc content) to alleviate overspray. Use zinc rich paint to touch up galvanized material as allowed under Item 445 "Galvanizing." Do not paint non-galvanized material with a zinc rich paint as an alternative for materials required to be galvanized.

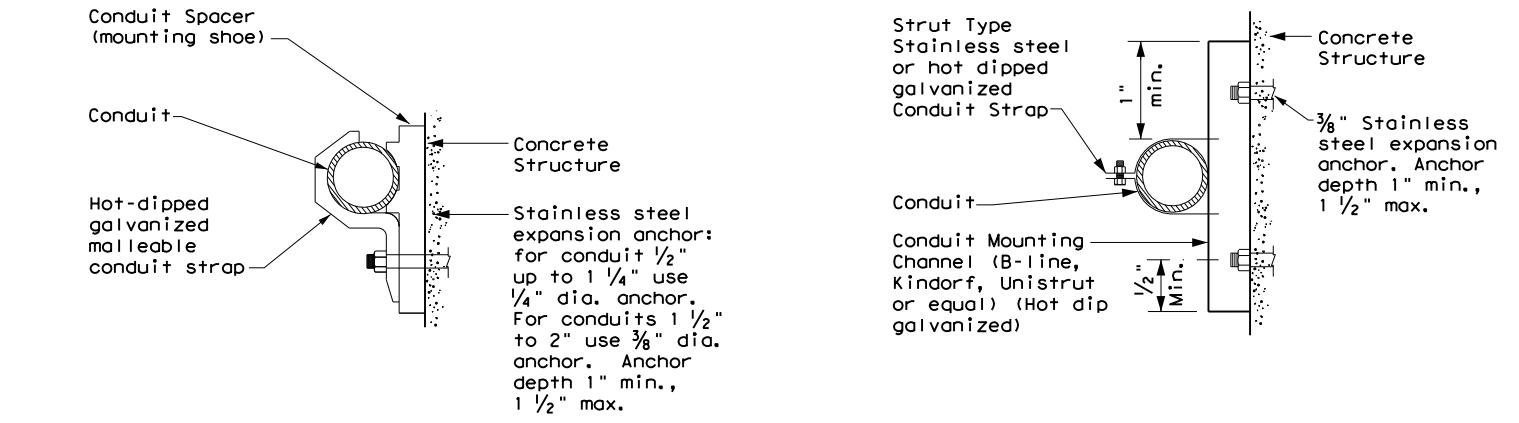
			
<p>ELECTRICAL DETAILS CONDUITS & NOTES</p>			
<p>ED(1) - 14</p>			
FILE:	ed1-14.dgn	DWG:	CK:
© TxDOT	October 2014	CONT	SECT
REVISIONS		0054	04
		123	US 84
		DIST	COUNTY
		BWD	COLEMAN CO.
		SHEET NO.	
		84	

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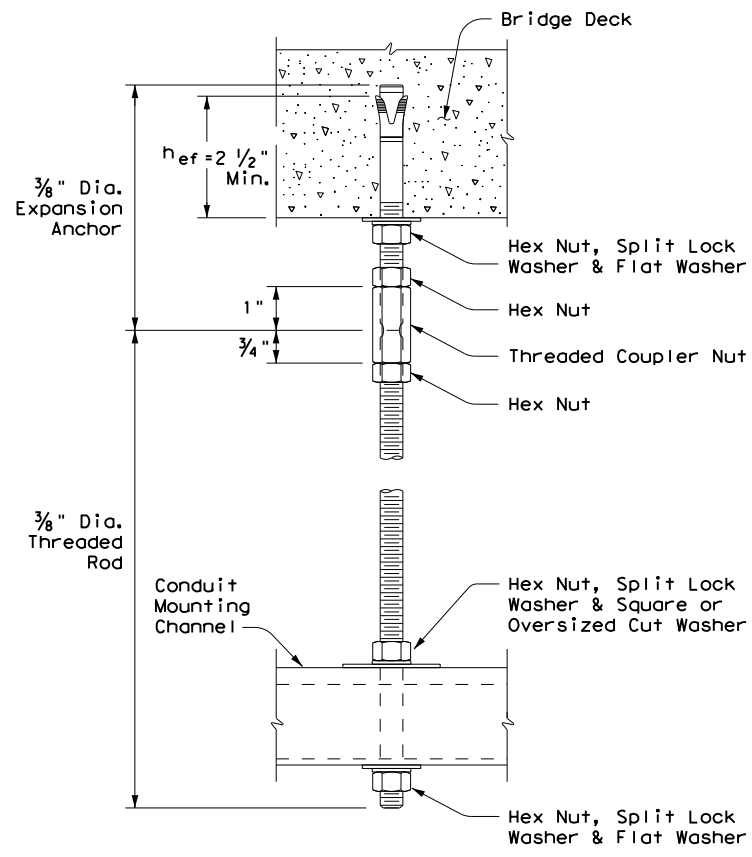
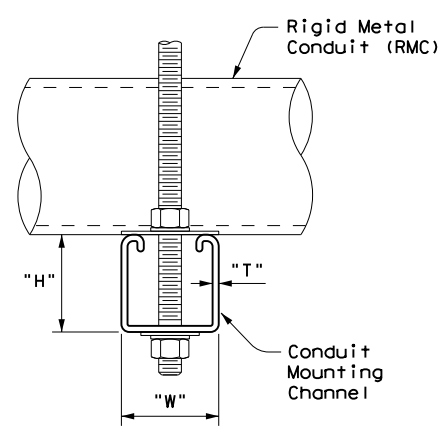
CONDUIT HANGING DETAIL



CONDUIT MOUNTING OPTIONS
 Attachment to concrete surfaces
 See ED(1)B.2

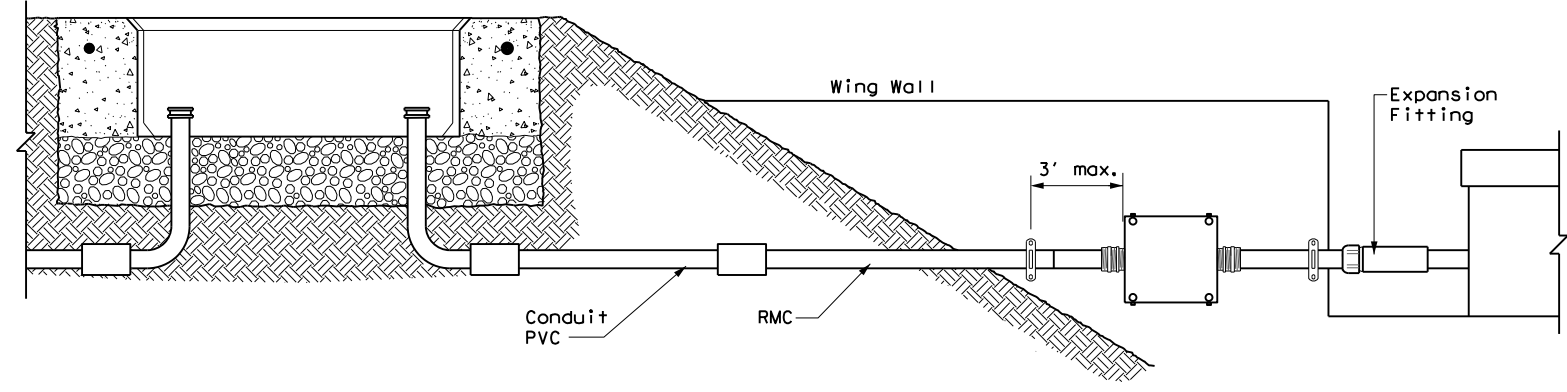
CONDUIT MOUNTING CHANNEL		
"SPAN"	"W" x "H"	"T"
less than 2'	1 5/8" x 1 3/8"	12 Ga.
2'-0" to 2'-6"	1 5/8" x 1 5/8"	12 Ga.
>2'-6" to 3'-0"	1 5/8" x 2 1/16"	12 Ga.

Channels with round or short slotted hole patterns are allowed, if the load carrying capacity is not reduced by more than 15%.



HANGER ASSEMBLY DETAIL

ELECTRIC CONDUIT TO BRIDGE DECK ATTACHMENT



TYPICAL CONDUIT ENTRY TO BRIDGE STRUCTURE DETAIL

EXPANSION ANCHOR NOTES FOR BRIDGE DECK ATTACHMENT

1. Use torque controlled mechanical expansion anchors that are approved for use in cracked concrete by the International Code Council, Evaluation Service (ICC-ES). The chosen anchor product shall have a designated ICC-ES Evaluation Report number, and its approval status shall be maintained on the ICC-ES website under Division 031600 for Concrete Anchors.
2. Unless otherwise approved by the Engineer: do not use adhesive anchors; do not use expansion anchors that are not included in the ICC-ES approval list; and do not use expansion anchors that are only approved for use in uncracked concrete.
3. Use anchors manufactured with stainless steel expansion wedges. Anchors manufactured with carbon steel expansion wedges are not allowed. Anchor bodies can be either zinc-plated carbon steel or stainless steel. For application in marine environment, both the anchor body and expansion wedge shall be stainless steel.
4. Install anchors as shown on the plans and in accordance with the anchor manufacturer's published installation instructions. Arrange a field demonstration test to evaluate the procedures and tools. The test shall be witnessed and approved by the Engineer prior to furnishing anchors on the structure.
5. Prior to hole drilling, use rebar locator to ensure clearing of existing deck strands or reinforcement. Install anchors to ensure a minimum effective embedment depth, (h_{ef}), as shown. Increase (h_{ef}) as needed to ensure sufficient thread length for proper torquing and tightening of anchors.
6. Use anchors of minimum 1600 Lbs tensile capacity (minimum of steel, concrete breakout, and concrete pullout strengths as determined by ACI 318 Appendix D) at the required minimum embedment depth (h_{ef}). No lateral loads shall be introduced after conduit installation.

		Traffic Operations Division Standard	
<h2>ELECTRICAL DETAILS CONDUIT SUPPORTS</h2>			
<h3>ED(2) - 14</h3>			
FILE: ed2-14.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
© TxDOT October 2014	CONT: 0054	SECT: 04	JOB: 123
REVISIONS			US 84
DIST: BWD	COUNTY: COLEMAN CO.	SHEET NO.: 85	

ELECTRICAL CONDUCTORS

A. MATERIAL INFORMATION

1. Provide Type XHHW insulated conductors in accordance with Departmental Material Specification (DMS) 11040 "Conductors" and Item 620 "Electrical Conductors." Provide conductors as listed on the Material Producers List (MPL) on the Department web site under "Roadway Illumination and Electrical Supplies" Item 620. Color code insulated conductors in conformance with the NEC. Identify grounded (neutral) conductors with white insulation. Identify grounding conductors (ground wires) with green insulation or bare conductors. Identify ungrounded (hot) conductors with any color insulation except green, white, or gray. Keep color scheme consistent throughout the wiring system. Identify conductors 6 American Wire Gauge (AWG) and smaller by continuous color jacket. Identify electrical conductors 4 AWG and larger by continuous color jacket or by colored tape. When identifying conductors with colored tape, mark at least 6 in. of the conductor's insulation with half laps of tape.
2. Provide a solid copper 6 AWG grounding electrode conductor to bond the electrical service equipment to the concrete encased grounding electrode or the ground rod at the service location. Connect the grounding electrode conductor to the ground rod with a UL listed connector in accordance with DMS 11040. Connect the grounding electrode conductor to the concrete encased grounding electrode as shown in the plans.
3. Where two or more circuits are present in one conduit or enclosure, permanently identify the conductors of each branch circuit by attaching a non-metallic tag around both circuit conductors at each accessible location. Provide tags with two straps, large enough to indicate circuit number, letter, or other identification as shown in the plans. Print circuit identification on the tag with a permanent marker.
4. Use listed compression or screw type pressure connectors, terminal blocks, or split bolt connectors for splicing as specified in DMS 11040. Use hot melt adhesive tape to fill the gap and seal the ends of heat shrink tubing. Provide UL listed gel-filled insulating splice covers. Splicing materials, insulating materials, breakaway disconnects, splice covers, and fuse holders are subsidiary to various bid items.

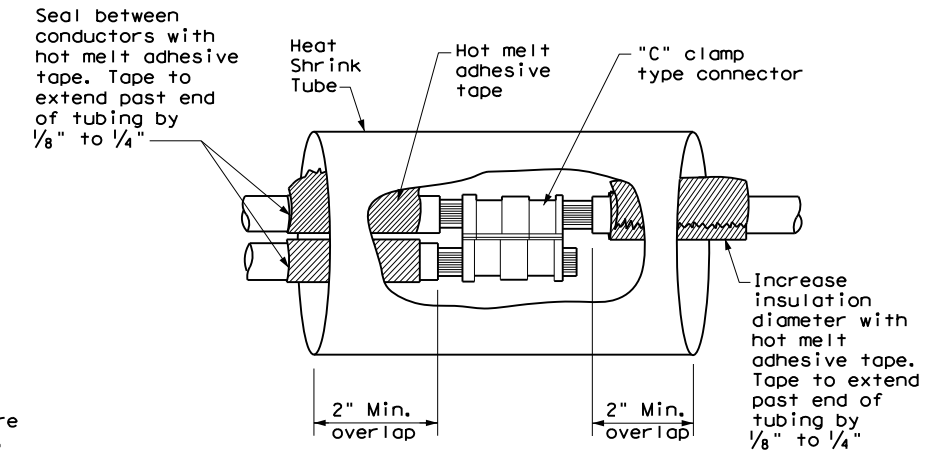
B. CONSTRUCTION METHODS

1. Use only a flat, high tensile strength polyester fiber pull tape for pulling conductors through the conduit system. After installing conductors in conduit, perform conductor pull test. If a conductor cannot be freely pulled, make any needed alterations or repairs at no additional cost to the department. Perform insulation resistance tests in accordance with Item 620. Coordinate with the Engineer to witness the tests.
2. Leave 2 ft. minimum, 3 ft. maximum length for each conductor up to the splice in ground boxes. Leave 3 ft. minimum, 4 ft. maximum length of conductor in ground boxes when pulled through with no splice. Leave 1 ft. minimum, 1.5 ft. maximum length of conductor at enclosures, weatherheads and pole bases.
3. Make splices only in junction boxes, ground boxes, pole bases, or electrical enclosures and use only listed compression or screw type pressure connectors, terminal blocks, or split bolt connectors. Insulate splices with heavy wall heat shrink tubing or gel-filled insulating splice covers to provide a watertight splice. Overlap conductor insulation with heat shrink tubing a minimum of 2 in. past both sides of the splice. Where heat shrink tubing may not shrink sufficiently to provide a watertight seal around the individual conductors, prior to heating the tubing, increase the diameter of the conductor insulation using hot melt adhesive tape to provide a watertight seal between the individual conductors and the heat shrink tubing. Ensure the tape extends past the heat shrink tubing. Use hot melt adhesive tape to fill the gap and seal the ends of heat shrink tubing. Heat shrink tubing that appears to have been burned, or overheated, is considered defective and must be replaced.
4. Size and install gel-filled insulating splice covers according to manufacturer's specifications when used in place of heat shrink tubing.
5. Wire nuts with factory applied waterproof sealant may be used for 8 AWG or smaller conductors in above ground junction boxes, but not in pole bases or ground boxes. Install wire nuts in an upright position to prevent the accumulation of water.
6. Support conductors in illumination poles with a J-hook at the top of the pole.
7. When terminating conductors, remove the insulation and jacketing material without nicking the individual strands of the conductor. Conductors with nicked individual conductor strands or removed strands will be considered damaged.
8. Replace conductors and cables that are damaged beyond repair or that fail an insulation resistance test at no additional cost to the department.
9. Do not repair damaged conductors with duct tape, electrical tape, or wire nuts. Use only approved splicing methods.
10. Do not terminate more than one conductor under a single connector, unless the connector is rated for multiple conductors. Do not exceed the pressure connector's listing for maximum number and size of conductors allowed.
11. Install breakaway connectors on conductors bid under Item 620 whenever those conductors pass through a breakaway support device. Follow manufacturer's instructions when terminating conductors to breakaway connectors. Properly torque threaded connections. Proper terminations are critical to the safe operation of breakaway devices. Trim waterproofing boots on breakaway connectors to fit snugly around the conductor to ensure waterproof connection. Only one conductor may enter a single opening in a boot. Provide waterproof boots with the correct number of openings. Leave unused openings factory sealed. Use prequalified breakaway connectors as shown on the MPL.

12. Provide and install a separate stranded equipment grounding conductor (EGC) in all conduits that contain circuit wiring of 50 volts or more. Unless shown elsewhere, size the EGC to be the same size as the largest current carrying conductor contained in the conduit. Ensure all EGCs are bonded together at every accessible location. For traffic signal installations, provide a minimum size 8 AWG EGC. The EGC is paid for under Item 620.

C. TEMPORARY WIRING

1. Install temporary conductors and electrical equipment in accordance with the NEC article "Temporary Installations" and Department standard sheets.
2. Provide a ground fault circuit interrupter (GFCI) for power outlets for portable electrical equipment, power tools, ice machines, ice storage bins and refrigerators located outdoors at grade. GFCI may be any one of the following: molded cord and plug set, receptacle, or circuit breaker type.
3. Use listed wire nuts with factory applied sealant for temporary wiring where approved.
4. Enclose conductor splices within a listed enclosure or ground box, or ensure the splices are more than 10 ft. above grade vertically and more than 5 ft. horizontally from any metal structure. Where installing temporary conductors in areas subject to vehicle traffic or mobile construction equipment, ensure the vertical clearance to ground is at least 18 ft. when measured at the lowest point. Ground messenger wires that support power conductors in conformance with the NEC.
5. Protect and when necessary repair any existing electrical conduits uncovered during the construction process in a timely manner and in conformance with the NEC.



**SPLICE OPTION 1
Compression Type**

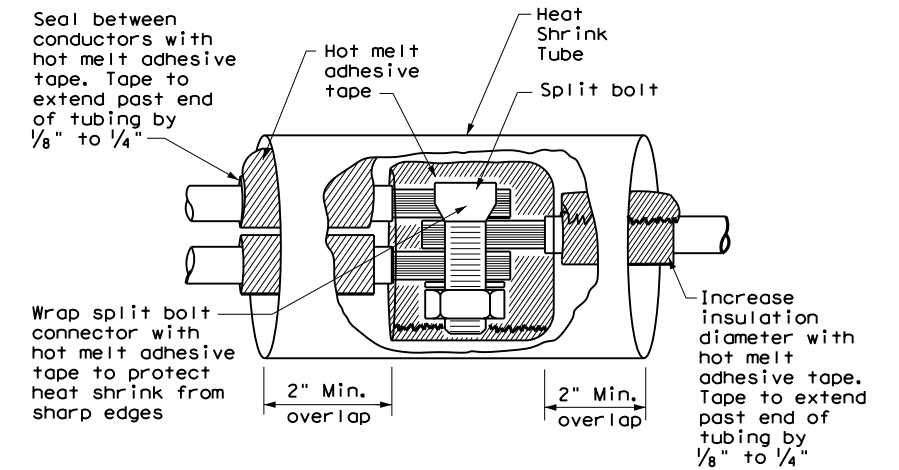
GROUND RODS & GROUNDING ELECTRODES

A. MATERIAL INFORMATION

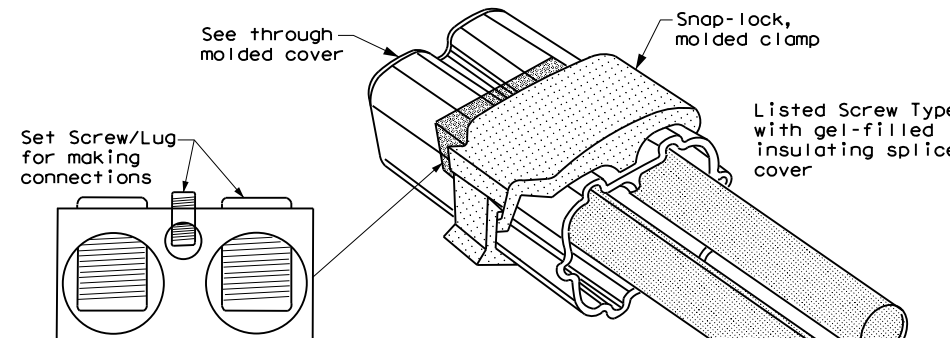
1. Provide and install a grounding electrode at electrical services. Provide ground rods according to DMS 11040 and the plans. Larger diameter or longer length rods may be called for in some specific locations, see the individual plans sheets. Concrete encased grounding electrodes may be called for in specific locations including electrical service, see individual plan sheets.

B. CONSTRUCTION METHODS

1. Furnish auxiliary ground rods for lightning protection and install in soil, concrete, or both, as called for in the plans. For ground rods installed in concrete, ensure the connection of the conductor to the ground rod is readily accessible for inspection or repairs. For ground rods installed in soil, ensure that the upper end is between 2 to 4 in. below finished grade.
2. Do not place ground rods in the same drilled hole as a timber pole.
3. Install ground rods so the imprinted part number is at the upper end of the rod.
4. Remove all non-conductive coatings such as concrete splatter from the rod at the clamp location.
5. Route all conductors as short and straight as possible for connection to lightning protection ground rods. When a bend is required, ensure a minimum radius bend of four inches for these conductors.
6. Unless otherwise called for in the plans, protect grounding electrode conductors with non-metallic conduit. When protecting grounding electrode conductors with metal conduit, provide and install a grounding type bushing and properly sized bonding jumper on each end of the metal conduit.
7. Written authorization is required before installing a ground rod in a horizontal trench for rocky soil or a solid rock bottom.



**SPLICE OPTION 2
Split Bolt Type**



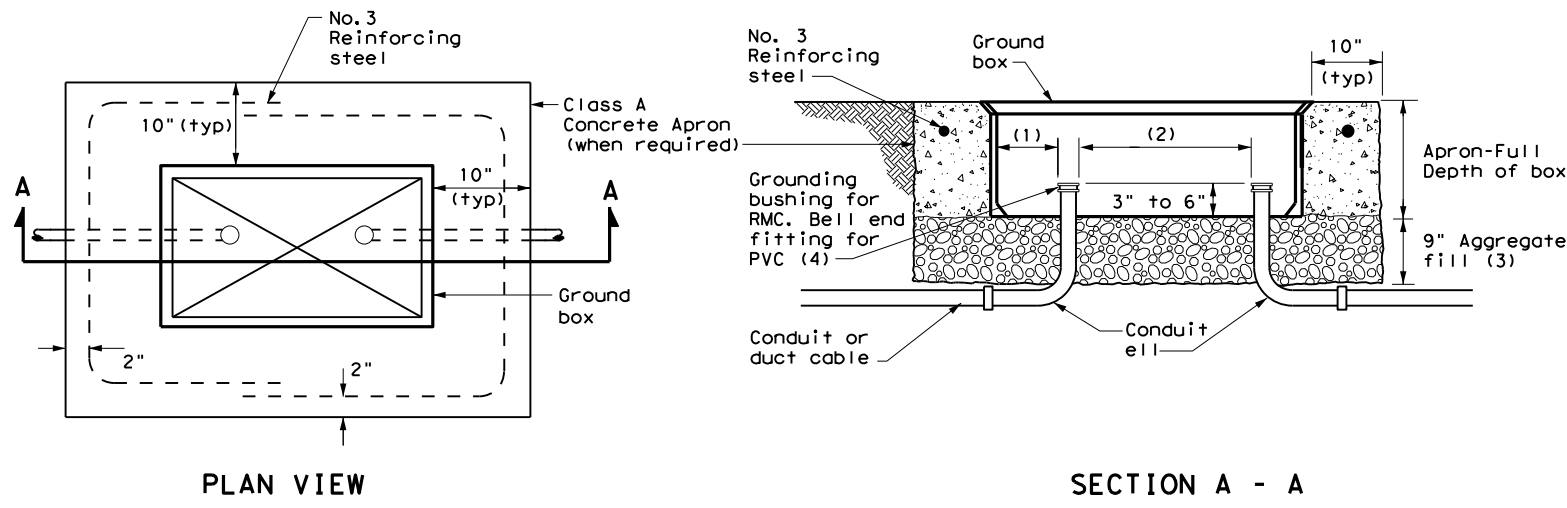
**SPLICE OPTION 3
Listed Screw Type**

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		Traffic Operations Division Standard	
<h1>ELECTRICAL DETAILS CONDUCTORS</h1>			
<h2>ED(3) - 14</h2>			
FILE: ed3-14.dgn	DW: TxDOT	CK: TxDOT	CR: TxDOT
© TxDOT October 2014	CONT	SECT	JOB
REVISIONS	0054	04	123
	DIST	COUNTY	SHEET NO.
	BWD	COLEMAN CO.	86

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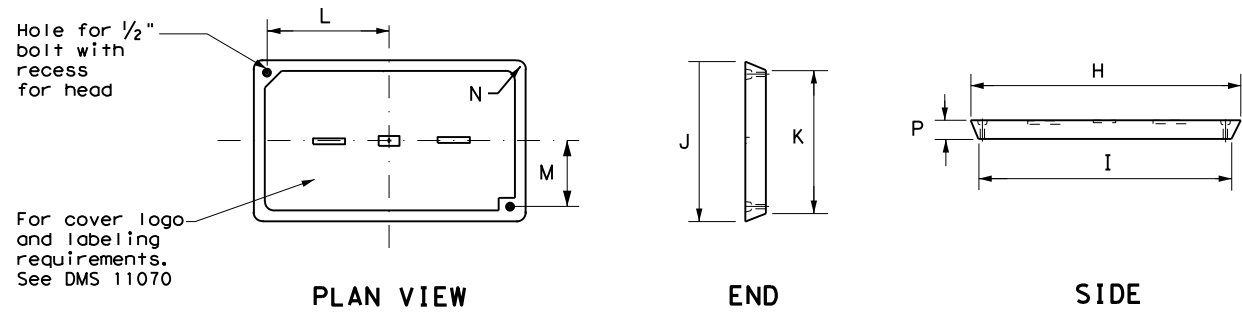


APRON FOR GROUND BOX

- (1) Uniformly space ends of conduits within the ground box. Position ends of conduits so that ground box walls do not interfere with the installation of grounding bushings or bell end fittings.
- (2) Maintain sufficient space between conduits to allow for proper installation of bushing.
- (3) Place aggregate under the box, not in the box. Aggregate should not encroach on the interior volume of the box.
- (4) Install a grounding bushing on the upper end of all RMC terminating in a ground box. Ground RMC elbows when any part of the elbow is less than 18 in. below the bottom of the ground box. Install a PVC bushing or bell end fitting on the upper end of all PVC conduits terminating in a ground box.

GROUND BOX DIMENSIONS	
TYPE	OUTSIDE DIMENSIONS (INCHES) (Width x Length X Depth)
A	12 X 23 X 11
B	12 X 23 X 22
C	16 X 29 X 11
D	16 X 29 X 22
E	12 X 23 X 17

GROUND BOX COVER DIMENSIONS								
TYPE	DIMENSIONS (INCHES)							
	H	I	J	K	L	M	N	P
A, B & E	23 1/4	23	13 3/4	13 1/2	9 7/8	5 1/8	1 3/8	2
C & D	30 1/2	30 1/4	17 1/2	17 1/4	13 1/4	6 3/4	1 3/8	2



GROUND BOX COVER

GROUND BOXES

A. MATERIALS

1. Provide polymer concrete ground boxes measuring 16x30x24 in. (WxLxD) or smaller in accordance with Departmental Material Specification (DMS) 11070 "Ground Boxes" and Item 624 "Ground Boxes."
2. Provide Type A, B, C, D, and E ground boxes as shown in the plans, and as listed on the Material Producers List (MPL) on the Department web site under "Roadway Illumination and Electrical Supplies," Item 624.
3. Ensure ground box cover is correctly labeled in accordance with DMS 11070.
4. Provide larger ground boxes in accordance with Item 624 and as shown in the plans.

B. CONSTRUCTION METHODS

1. Remove all gravel and dirt from conduit. Cap all conduits prior to placing aggregate and setting ground box. Provide Grade 3 or 4 coarse aggregate as shown on Table 2 of Item 302 "Aggregates for Surface Treatments." Ensure aggregate bed is in place and at least 9 inches deep, prior to setting the ground box. Install ground box on top of aggregate.
2. Cast ground box aprons in place. Reinforcing steel may be field bent. Ensure the depth of concrete for the apron extends from finished grade to the top of the aggregate bed under the box. Ground box aprons, including concrete and reinforcing steel, are subsidiary to ground boxes when called for by descriptive code.
3. Keep bolt holes in the box clear of dirt. Bolt covers down when not working in ground boxes.
4. Install all conduits and ells in a neat and workmanlike manner. Uniformly space conduits so grounding bushings and bell end fittings can easily be installed.
5. Temporarily seal all conduits in the ground box until conductors are installed.
6. Permanently seal conduits immediately after the completion of conductor installation and pull tests. Permanently seal the ends of all conduits with duct seal, expandable foam, or other method as approved. Do not use duct tape as a permanent conduit sealant. Do not use silicone caulk as a sealant.
7. When a ground rod is present in a ground box, bond all equipment grounding conductors together and to the ground rod with listed connectors.
8. When a type B or D ground box is stacked to meet volume requirements, it is allowable to cut an appropriately sized hole for conduit entry in the side wall at least 18 inches below grade.
9. If an existing ground box in the contract has a metal cover, bond the cover to the equipment grounding conductor with a 3 ft. long stranded bonding jumper the same size as the grounding conductor. The bonding jumper is subsidiary to various bid items. Verify existing ground boxes with metal covers are shown on the plans, with notes fully describing the work required.
10. If other ground boxes with metal covers are within the project limits but are not part of the contract, the Engineer may direct the Contractor to bond the metal covers, identifying the specific boxes in writing. This work will be paid for separately.
11. Bond metal ground box covers to the grounding conductor with a tank ground type lug.

				Traffic Operations Division Standard	
<h2>ELECTRICAL DETAILS GROUND BOXES</h2>					
<h3>ED(4) - 14</h3>					
FILE:	ed4-14.dgn	DN:	TxDOT	CK:	TxDOT
© TxDOT	October 2014	CONT	SECT	JOB	HIGHWAY
REVISIONS		0054	04	123	US 84
DIST	COUNTY	SHEET NO.			
BWD	COLEMAN CO.	87			

ELECTRICAL SERVICES NOTES

1. Provide new materials. Ensure installation and materials comply with the applicable provisions of the National Electrical Code (NEC) and National Electrical Manufacturers Association (NEMA) standards. Ensure material is Underwriters Laboratories (UL) listed. Provide and install electrical service conduits, conductors, disconnects, contactors, circuit breaker panels, and branch circuit breakers as shown on the Electrical Service Data chart in the plans. Faulty fabrication or poor workmanship in material, equipment, or installation is justification for rejection. Where manufacturers provide warranties and guarantees as a customary trade practice, furnish these to the State.
2. Provide electrical services in accordance with Electrical Details standard sheets, Departmental Material Specification (DMS) 11080 "Electrical Services," DMS 11081 "Electrical Services-Type A," DMS 11082 "Electrical Services-Type C," DMS 11083 "Electrical Services-Type D," DMS 11084 "Electrical Services-Type T," DMS 11085 "Electrical Services-Pedestal (PS)", and Item 628 "Electrical Services" of the Standard Specifications. Provide electrical service types A, C, and D, as listed on the Material Producers List (MPL) on the Department web site under "Roadway Illumination and Electrical Supplies," Item 628. Provide other service types as detailed on the plans.
3. Provide all work, materials, services, and any incidentals needed to install a complete electrical service as specified in the plans.
4. Coordinate with the Engineer and the utility provider for metering and compliance with utility requirements. Primary line extensions, connection charges, meter charges, and other charges by the utility company to provide power to the location are paid for in accordance with Item 628. Get approval for the costs associated with these charges prior to engaging the utility company to do the work. Consult with the utility provider to determine costs and requirements, and coordinate the work as approved.
5. The enclosure manufacturer will provide Master Lock Type 2 with brass tumblers keyed #2195 for all custom electrical enclosures. Installing Contractor is to provide Master Lock #2195 Type 2 with brass tumblers for "off the shelf" enclosures. Master Lock #2195 keys and locks become property of the State. Unless otherwise approved, do not energize electrical service equipment until locks are installed.
6. Enclosures with external disconnects that de-energize all equipment inside the enclosure do not need a dead front trim. Protect incoming line terminations from incidental contact as required by the NEC.
7. When galvanized is specified for nuts, screws, bolts or miscellaneous hardware, stainless steel may be used.
8. Provide wiring and electrical components rated for 75°C. Provide red, black, and white colored XHHW service entrance conductors of minimum size 6 American Wire Gauge (AWG). Identify size 6 AWG conductors by continuous color jacket. Identify electrical conductors sized 4 AWG and larger by continuous color jacket or by colored tape. Mark at least 6 inches of the conductor's insulation with half laps of colored tape, when identifying conductors. Ensure each service entrance conductor exits through a separately bushed non-metallic opening in the weatherhead. The lengths of the conductors outside the weatherhead are to be 12 inches minimum, 18 inches maximum, or as required by utility.
9. All electrical service conduit and conductors attached to the electrical service including the riser or the elbow below ground are subsidiary to the electrical service. For an underground utility feed, all service conduit and conductors after the elbow, including service conduit and conductors for the utility pole riser when furnished by the Contractor, will be paid for separately.
10. Provide rigid metal conduit (RMC) for all conduits on service, except for the 1/2 in. PVC conduit containing the electrical service grounding electrode conductor. Size the service entrance conduit as shown in the plans. Ensure conduit for branch circuit entry to enclosure is the same size as that shown on the layout sheets for branch circuit conduit. Extend all rigid metal conduits a minimum of 6 inches underground and then couple to the type and schedule of the conduit shown on the layout for that particular branch circuit. Install a grounding bushing on the RMC where it terminates in the service enclosure.
11. Use of liquidtight flexible metal conduit (LFMC) is allowed between the meter and service enclosure when they are mounted 90 to 180 degrees to each other. Size the LFMC the same size as service entrance conduit. LFMC must not exceed 3 feet in length. Strap LFMC within 1 foot of each end. LFMC less than 12 inches in length need not be strapped. Each end of LFMC must have a grounding bushing or be terminated with a grounding fitting. The LFMC must contain a grounded (neutral) conductor. Ensure any bend in LFMC never exceeds 180 degrees. A pull test is required on all installed conductors, with at least six inches of free conductor movement demonstrated to the satisfaction of the Engineer.
12. Ensure all mounting hardware and installation details of services conform to utility company specifications.
13. For all electrical service enclosures listed under Item 628 on the MPL, the UL 508 enclosure manufacturers will prepare and submit a schematic drawing unique to each service. Before shipment to the job site, place the applicable laminated schematic drawings and the laminated plan sheet showing the electrical service data chart used to build the enclosure in the enclosure's data pocket. The installing contractor will copy and laminate the actual project plan sheets detailing all equipment and branch circuits supplied by that service. The laminated plan sheets are to be placed in the service enclosure's document pocket. Reduce 11 in. x 17 in. plan sheets to 8 1/2 in. x 11 in. before laminating. If the installation differs from the plan sheets, the installing contractor is to redline plan sheets before laminating.
14. When providing an "Off The Shelf" Type D or Type T service, provide laminated plan sheets detailing equipment and branch circuits supplied by that service. Reduce 11 in. x 17 in. plan sheets to 8 1/2 in. x 11 in. before laminating. Deliver these drawings before completion of the work to the Engineer, instead of placing in enclosure that has no door pocket.
15. Do not install conduit in the back wall of a service enclosure where it would penetrate the equipment mounting panel inside the enclosure. Provide grounding bushings on all metal conduits, and terminate bonding jumpers to grounding bus. Grounding bushings are not required when the end of the metal conduit is fitted with a conduit sealing hub or threaded boss, such as a meter base hub.

SERVICE ASSEMBLY ENCLOSURE

1. Provide threaded hub for all conduit entries into the top of enclosure.
2. Type galvanized steel (GS) enclosures may be used for Type C panelboards and for Type D and T services that do not use an enclosure mounted photocell or lighting contactor. Provide GS enclosures in accordance with DMS 11080, 11082, 11083, and 11084.
3. Provide aluminum (AL) and stainless steel (SS) enclosures for Types A, C, and D in accordance with DMS 11080, 11081, 11082, 11083, and 11084. Do not paint stainless steel.
4. Provide pedestal service (PS) enclosures in accordance with ED(9) and DMS 11080 and 11085. Do not provide GS pedestal services. If GS is shown in the PS descriptive code, provide an AL enclosure.

MAIN DISCONNECT & BRANCH CIRCUIT BREAKERS

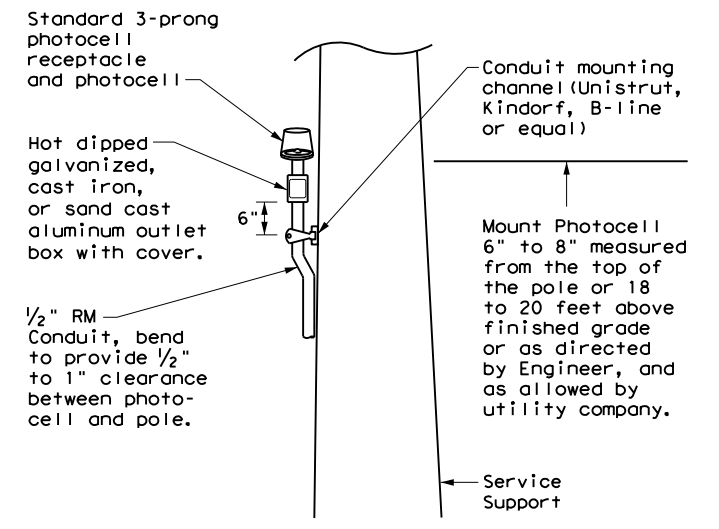
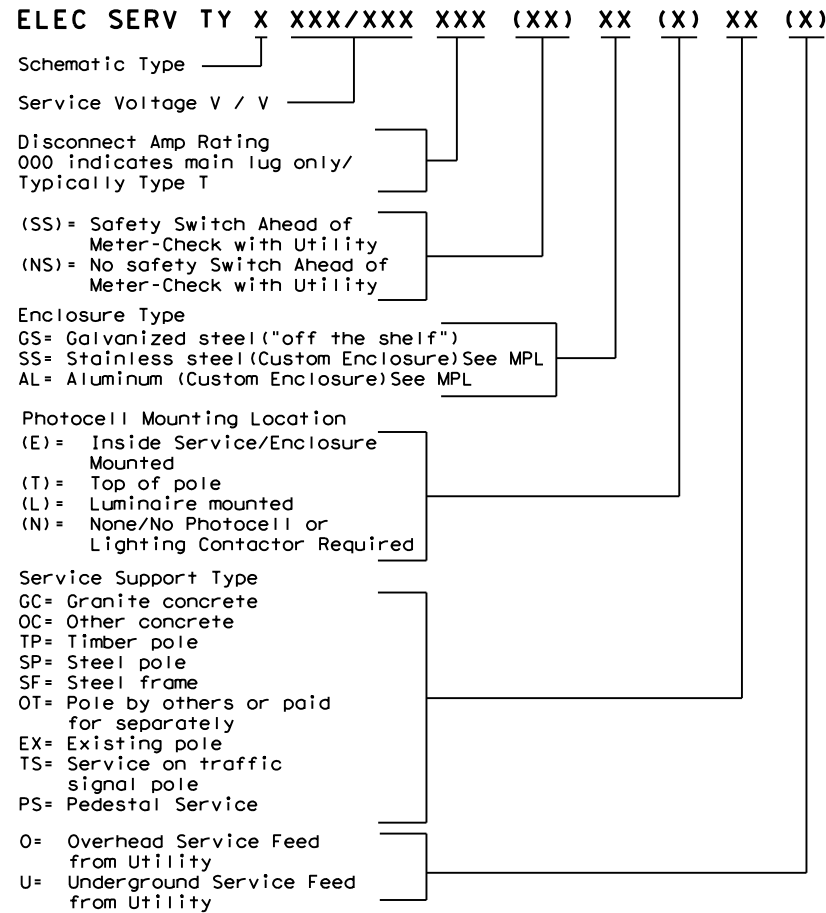
1. Field drill flange-mounted remote operator handle if needed, to ensure handle is lockable in both the "On" and "Off" positions.
2. When the utility company provides a transformer larger than 50 KVA, verify that the available fault current is less than the circuit breaker's ampere interrupting capacity (AIC) rating and provide documentation from the electric utility provider to the Engineer.

PHOTOELECTRIC CONTROL

1. Provide photocell as listed on the MPL. Move, adjust, or shield the photocell from stray or ambient night time light to ensure proper operation. Mount photocell facing north when practical. Mount top of pole photocells as shown on Top Mounted Photocell Detail.

* ELECTRICAL SERVICE DATA												
Elec. Service ID	Plan Sheet Number	Electrical Service Description	Service Conduit **Size	Service Conductors No./Size	Safety Switch Amps	Main Ckt. Bkr. Pole/Amps	Two-Pole Contractor Amps	Panelbd/ Loadcenter Amp Rating	Branch Circuit ID	Branch Ckt. Bkr. Pole/Amps	Branch Circuit Amps	KVA Load

EXPLANATION OF ELECTRICAL SERVICE DESCRIPTIVE CODE



TOP MOUNTED PHOTOCELL

Install conduit strap maximum 3 feet from box. 5 foot maximum spacing between straps supporting conduit.

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Texas Department of Transportation
 Traffic Operations Division Standard

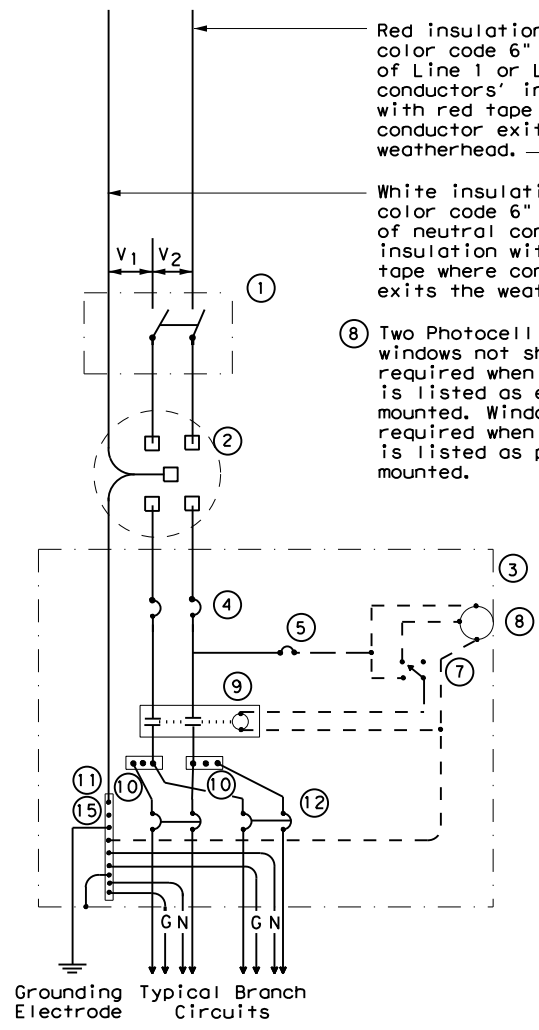
ELECTRICAL DETAILS SERVICE NOTES & DATA

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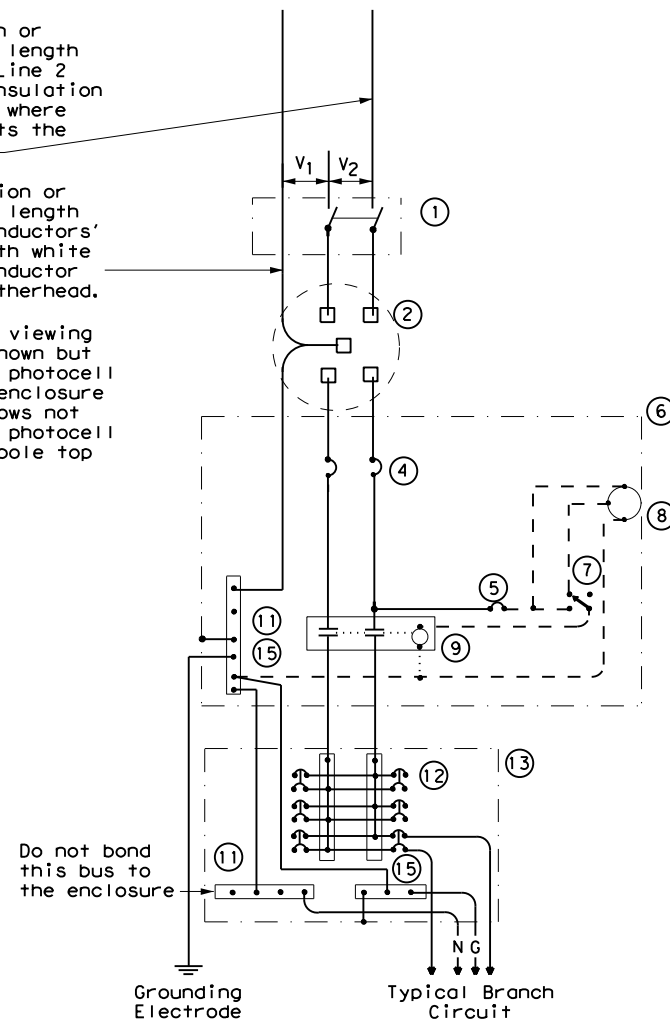
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REVISIONS		0054	04	123	US 84
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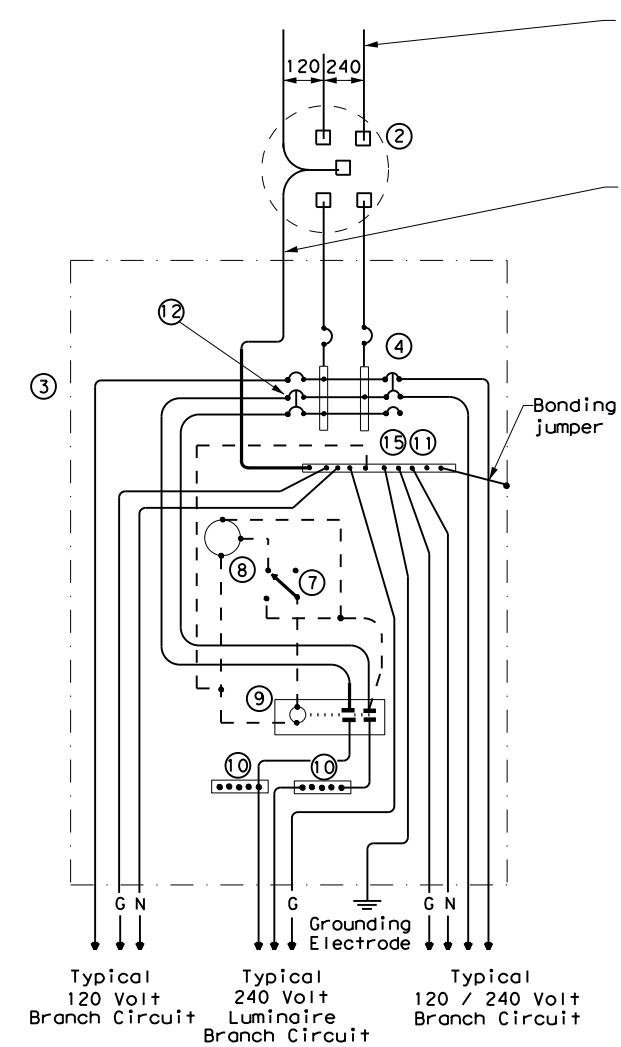
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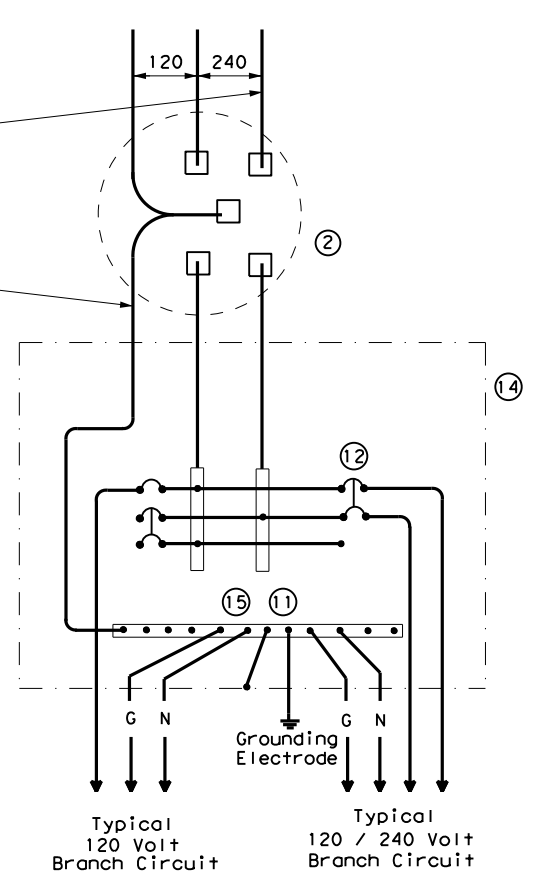
**SCHEMATIC TYPE A
THREE WIRE**



**SCHEMATIC TYPE C
THREE WIRE**



**SCHEMATIC TYPE D - CUSTOM
120/240 VOLTS - THREE WIRE**



**SCHEMATIC TYPE T
120/240 VOLTS - THREE WIRE**
 Galvanized steel - "Buy Off The Shelf" only. When required install photocell top of the pole or on luminaire only, no lighting contractor will be installed.

WIRING LEGEND	
—	Power Wiring
- - - -	Control Wiring
—N—	Neutral Conductor
—G—	Equipment grounding conductor-always required

SCHEMATIC LEGEND	
1	Safety Switch (when required)
2	Meter (when required-verify with electric utility provider)
3	Service Assembly Enclosure
4	Main Disconnect Breaker (See Electrical Service Data)
5	Circuit Breaker, 15 Amp (Control Circuit)
6	Auxiliary Enclosure
7	Control Station ("H-O-A" Switch)
8	Photo Electric Control (enclosure-mounted shown)
9	Lighting Contactor
10	Power Distribution Terminal Blocks
11	Neutral Bus
12	Branch Circuit Breaker (See Electrical Service Data)
13	Separate Circuit Breaker Panelboard
14	Load Center
15	Ground Bus

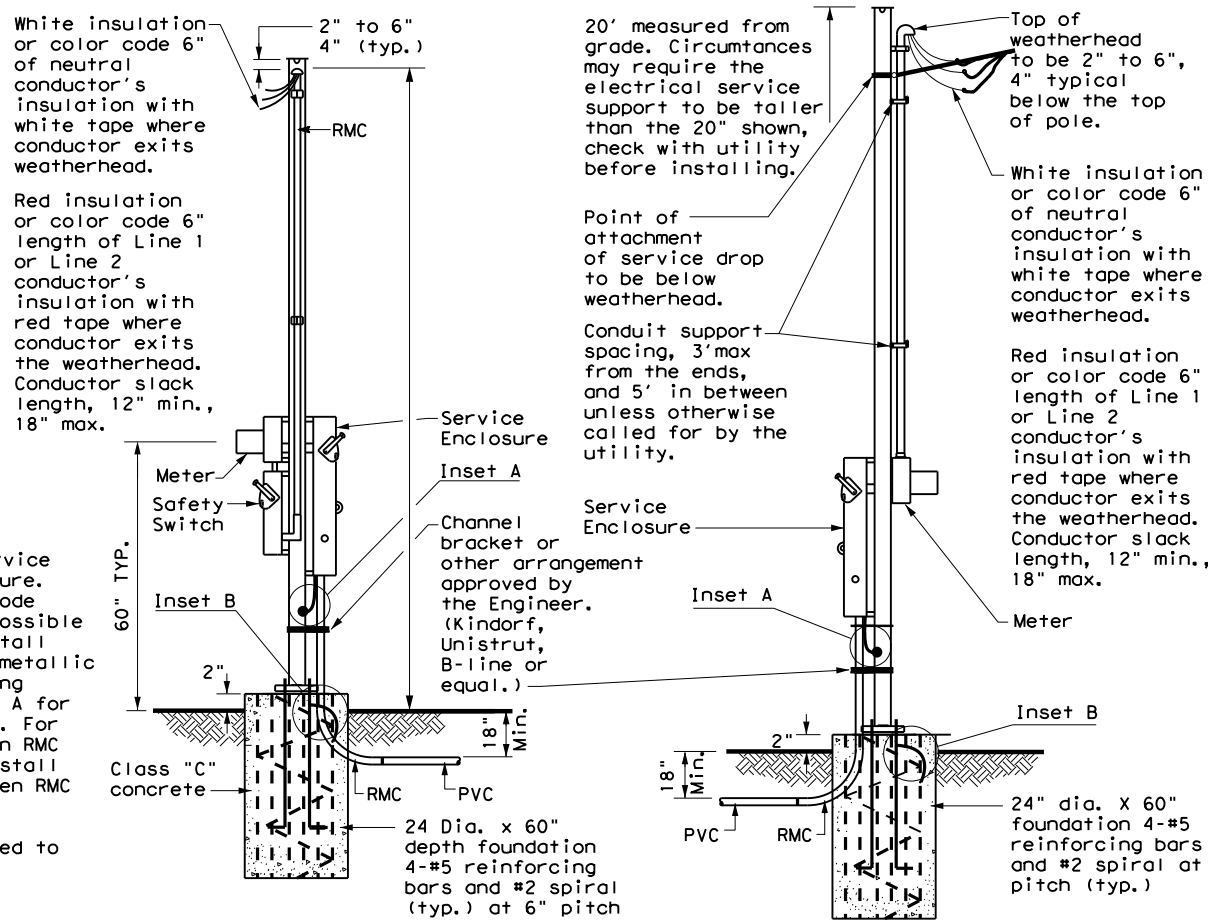
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ED(6) - 14					
FILE:	ed6-14.dgn	DN:	TxDOT	CK:	TxDOT
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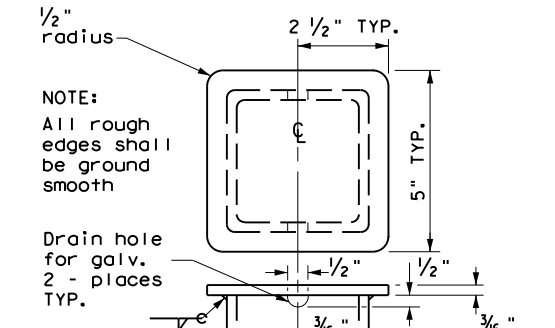
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SUPPORT TYPE STEEL POLE (SP) AND STEEL FRAME (SF)

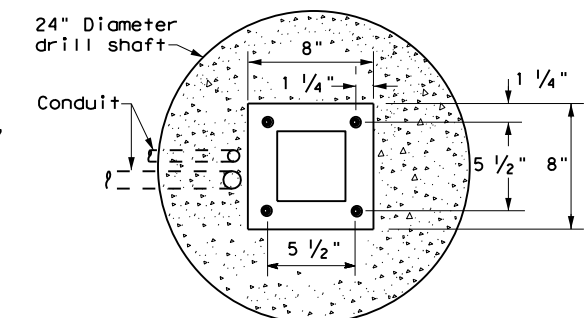
1. Provide steel pole and steel frame supports as per TxDOT Departmental Material Specification (DMS)11080 "Electrical Services." Mount all equipment and conduit on 12 gauge galvanized steel or stainless steel channel strut, 1 1/2 in. or 1 3/8 in. wide by 1 in. up to 3 3/4 in. deep Unistrut, Kindorf, B-line or equal. Bolt or weld all channel and hardware to vertical members as approved. Do not stack channel. File smooth and paint field cut ends of all channel with zinc-rich paint before installing.
2. Provide poles for overhead service with an eyebolt or similar fitting for attachment of the service drop to the pole in conformance with the electric utility provider's specifications.
3. Provide and install galvanized 3/4 in. x 18 in. x 4 in. (dia. x length x hook length) anchor bolts for underground service supports. Provide and install galvanized 3/4 in. x 56 in. x 4 in. anchor bolts for overhead service supports. Ensure anchor bolts have 3 in. of thread, with 3 1/4 in. to 3 1/2 in. of the exposed anchor bolt projecting above finished foundation. Provide and install leveling nuts for all anchor bolts.
4. Bond one of the anchor bolts to the rebar cage with 6 AWG bare stranded copper conductor. Use listed mechanical connectors rated for embedment in concrete. See Inset B.
5. Furnish and install rigid metallic ellis in all steel pole and steel frame foundations for all conduits entering the service from underground.
6. Use class C concrete for foundations. Ensure reinforcing steel is Grade 60 with 3" of unobstructed concrete cover.
7. Drill and tap steel poles and frames for 1/2 in. X 13 UNC tank ground fitting. For steel pole service supports, provide and install tank ground fitting 4 in. to 6 in. below electrical service enclosure. Provide properly sized hole through the bottom of the enclosure for the service grounding electrode conductor. Ensure electrical service grounding electrode conductor is as short and straight as possible from the enclosure to the tank ground fitting. For steel frame service supports, provide and install tank ground fitting on steel frame post. Install service grounding electrode conductor in a non-metallic conduit or tubing from the enclosure to the steel frame post. Connect electrical service grounding electrode conductor to the tank ground fitting. See steel frame and steel pole details and Inset A for more information. Size service entrance conduit and branch circuit conduit as shown in the plans. For underground conduit runs from the electrical service, extend RMC from the service enclosure to an RMC elbow, and then connect the schedule type and size of conduit shown in the plans. Provide and install grounding bushings where RMC terminates in the enclosure. Grounding bushings are not required when RMC is fitted into a sealing hub or threaded boss.
8. If Steel pole or frame is painted, bond each separate painted piece with a bonding jumper attached to a tapped hole.
9. Provide 1/4" - 20 machine screws for bonding. Do not use sheet metal screws. Remove all non-conductive material at contact points. Terminate bonding jumpers with listed devices. Install minimum size 6 AWG stranded copper bonding jumpers. Make up all threaded bonding connections wrench tight.
10. Avoid contact of the service drop and service entrance conductors with the metal pole to prevent abrasion of the insulated conductors.
11. Shop drawings are not required for service support structure unless specifically stated elsewhere or directed by the Engineer.



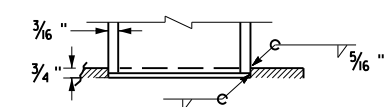
WITH SAFETY SWITCH WITHOUT SAFETY SWITCH
SERVICE SUPPORT TYPE SP (O) - OVERHEAD SERVICE



POLE TOP PLATE

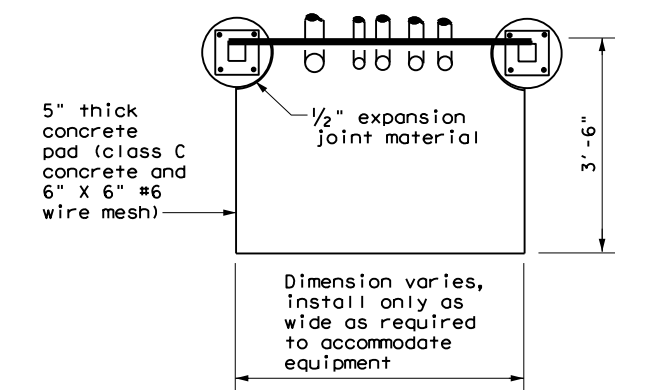


BASE PLATE DETAIL

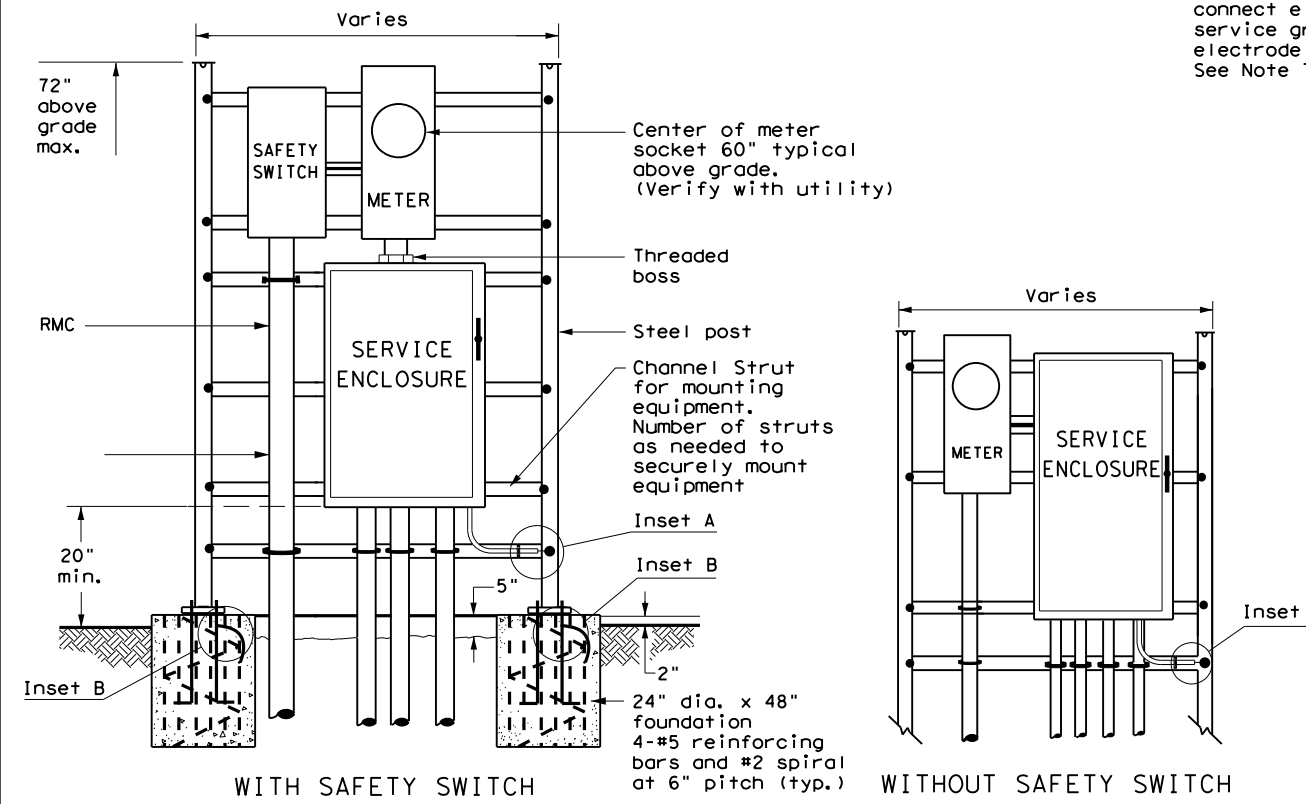


BOTTOM OF POLE

SERVICE SUPPORT TYPE SF & SP

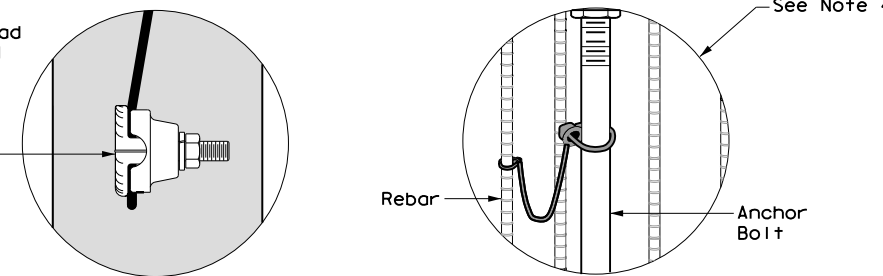


TOP VIEW
SERVICE SUPPORT TY SF (O) & SF (U)

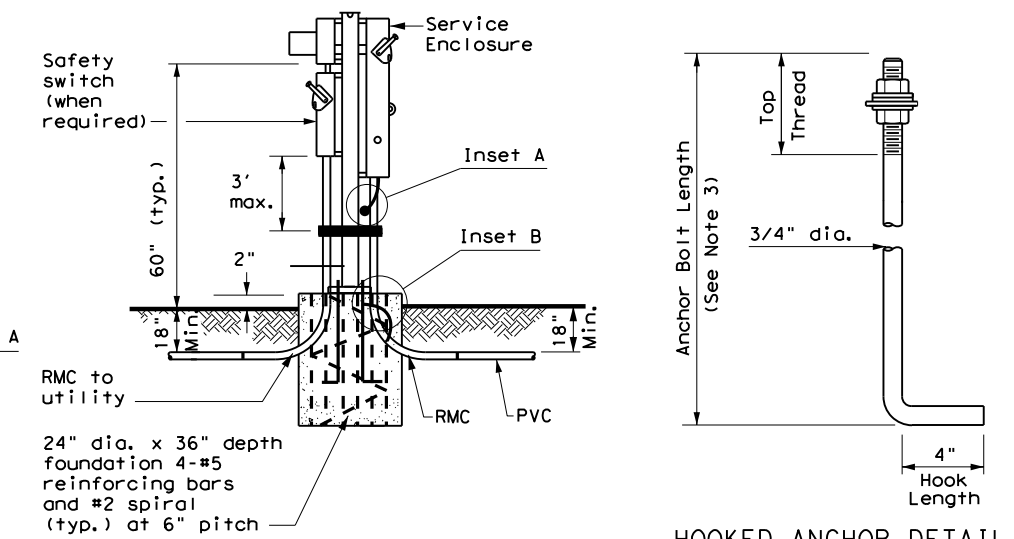


WITH SAFETY SWITCH WITHOUT SAFETY SWITCH
FRONT VIEW
SERVICE SUPPORT TYPE SF (U) - UNDERGROUND SERVICE

Drill, top, and thread 1/2" X 13 UNC. Install tank ground fitting, connect electrical service grounding electrode conductor. See Note 7.



FRONT VIEW
INSET A
INSET B



WITH SAFETY SWITCH HOOKED ANCHOR DETAIL
SERVICE SUPPORT TYPE SP (U) - UNDERGROUND SERVICE

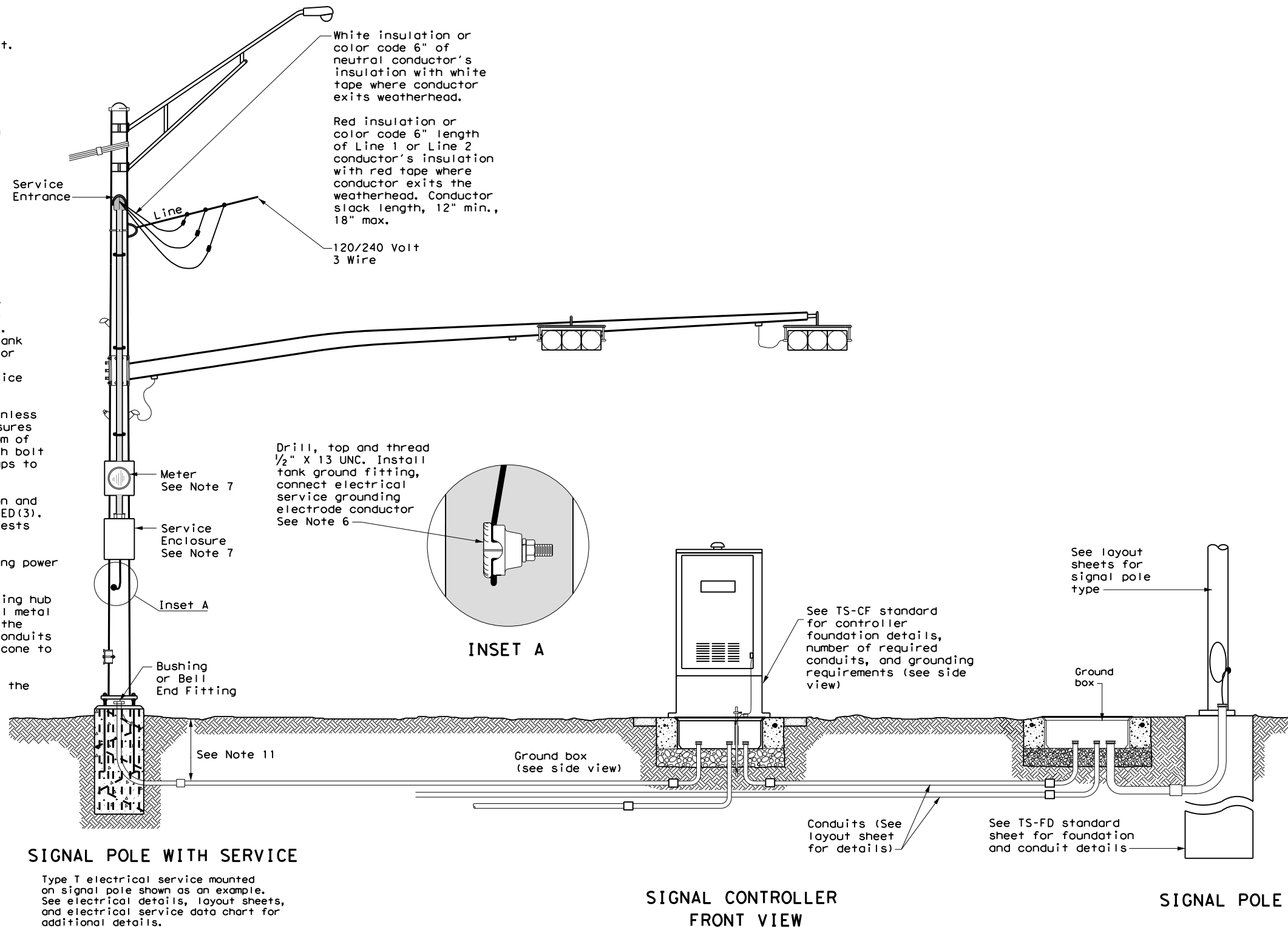
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TRAFFIC SIGNAL NOTES

1. Do not pass luminaire conductors through the signal controller cabinet.
2. Include an equipment grounding conductor in all conduits throughout the electrical system. Bond all exposed metal parts to the grounding conductor.
3. Provide roadway luminaires, when required, in accordance with the material and construction sections of Item 610, "Roadway Illumination Assemblies," except for performance testing of luminaires. Test installed roadway luminaires for proper operation as a part of the associated traffic signal system test.
4. If internally illuminated street name signs are approved for use, ground the fixture to the pole with a 12 AWG green XHHW conductor.
5. Bond anchor bolts to rebar cage in two locations using #3 bars or 6 AWG stranded copper conductors. Use listed mechanical connectors rated for embedment in concrete. See TxDOT standard TS-FD for further details.
6. Drill and tap signal poles for 1/2 in. X 13 UNC tank ground fitting. Provide and install tank ground fitting 4 in. to 6 in. directly below electrical service enclosure. Provide properly sized hole through the bottom of the enclosure for the service grounding electrode conductor. Connect the electrical service grounding electrode conductor to the tank ground fitting. Ensure electrical service grounding electrode conductor is as short and straight as possible from the enclosure to the tank ground fitting. See Inset A detail for further information. Size service entrance conduit and branch circuit conduit as shown in the plans.
7. Mount electrical service enclosure and meter to signal pole with stainless steel bands. Ensure bands are a minimum width of 3/4 in. Secure enclosures to bands using two-bolt brackets. Install brackets near top and bottom of each enclosure. Install properly sized stainless steel washers on each bolt in the enclosure. Band or drill and tap properly sized stand-off straps to signal pole for attaching conduit.
8. Conduct pull tests and insulation resistance tests on all illumination and power conductors as required in Item 620 "Electrical Conductors" and ED(3). To prevent electronics damage, do not conduct insulation resistance tests on traffic signal cables after termination.
9. Lock all enclosures and bolt down all ground box covers before applying power to the signal installation.
10. Terminate conduits entering the top of enclosures with a conduit-sealing hub or threaded boss such as meter hub. Install a grounding bushing on all metal conduits not connected to conduit-sealing hub or threaded boss. Bond the grounding bushing to the ground bus with a bonding jumper. Seal all conduits entering enclosures with duct seal or expanding foam. Do not use silicone to seal conduit ends.
11. For all conduits, ensure the burial depth is a minimum of 18". Ensure the minimum burial depth for conduit placed under a roadway is 24".

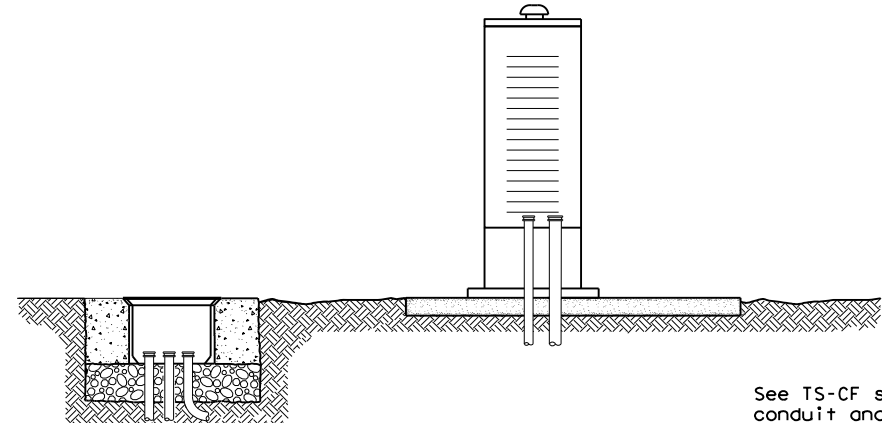


SIGNAL POLE WITH SERVICE

Type T electrical service mounted on signal pole shown as an example. See electrical details, layout sheets, and electrical service data chart for additional details.

SIGNAL CONTROLLER FRONT VIEW

SIGNAL POLE



SIGNAL CONTROLLER SIDE VIEW

See TS-CF standard for conduit and grounding requirements. See layout sheets for ground box locations and any additional conduits that are required.

**ELECTRICAL DETAILS
 TYPICAL TRAFFIC SIGNAL
 SYSTEM DETAILS**

ED(8) - 14

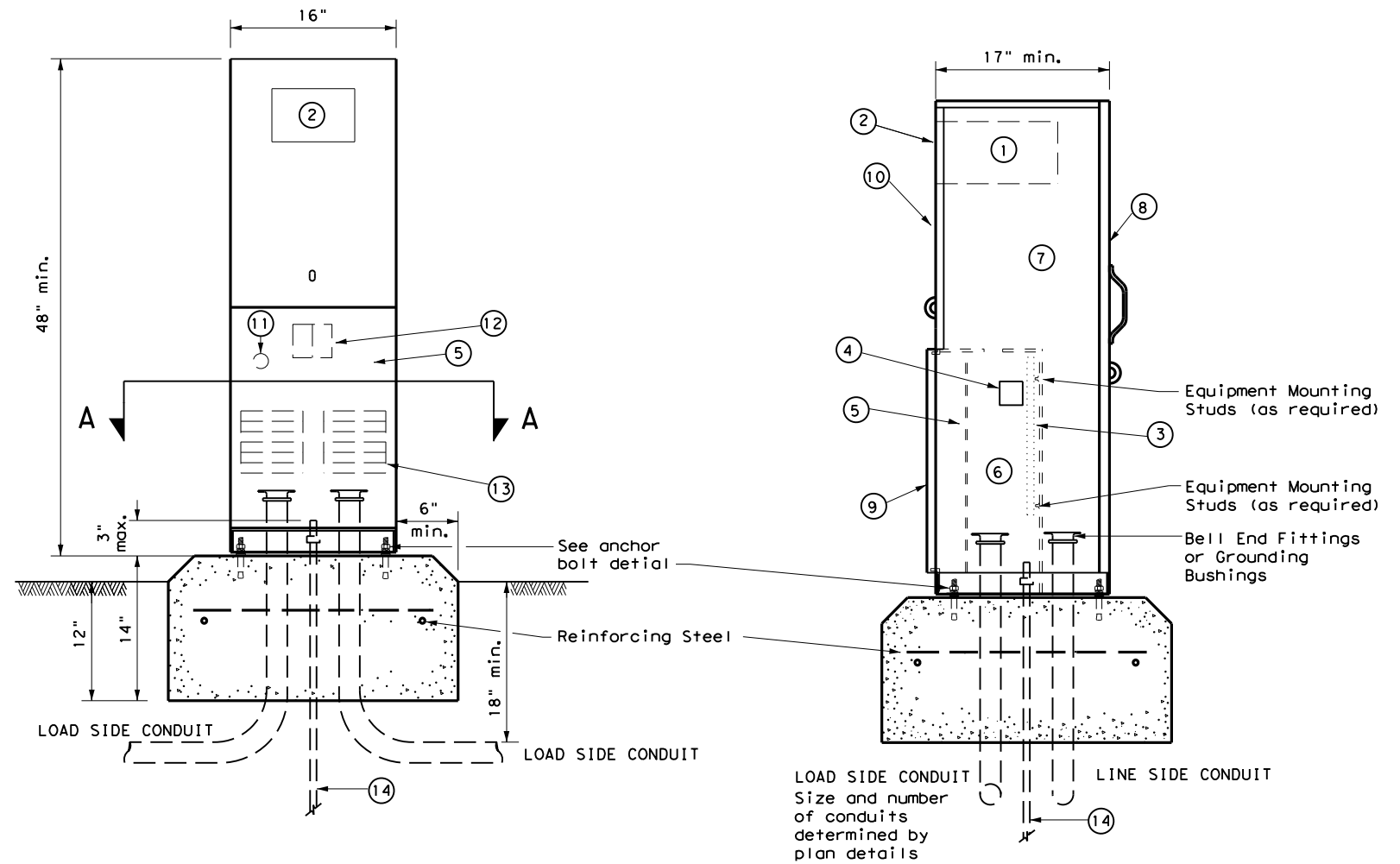
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REVISIONS	0054	04	123	US 84
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PEDESTAL SERVICE NOTES

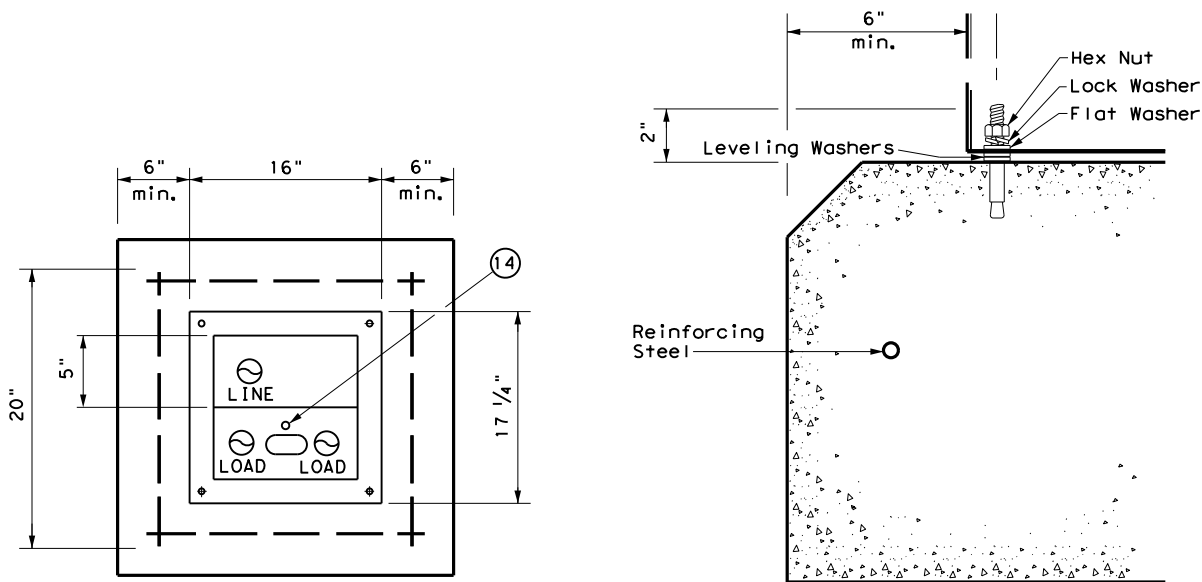
1. Manufacture pedestal electrical services in accordance with Departmental Material Specifications (DMS) 11080 "Electrical Services", 11085 "Electrical Services-Pedestal (PS)" and Item 628 "Electrical Services." Provide pedestal electrical services as listed on the Material Producers List (MPL) on the Department's web site under "Roadway Illumination and Electrical Supplies," Item 628. Ensure all mounting hardware and installation details of services meet utility company specifications. Contact the local utility company for approval of pedestal details prior to installing the electrical pedestal service. Submit any changes required by the utility company prior to manufacturing the pedestal enclosure.
2. When a meter socket is required, provide a socket with a minimum 100 amp rating that complies with local utility requirements.
3. Provide Class A or C concrete for pedestal service foundations in accordance with Item 420, "Concrete Substructures," except that concrete will not be paid for directly but is considered subsidiary to Item 628.
4. Provide #4 reinforcing steel for foundations in accordance with Item 440, "Reinforcement for Concrete."
5. Install 1/2 in. X 2 1/16 in. minimum length concrete single expansion type anchors for mounting pedestal enclosure to foundation. Anchor location to match mounting holes in each corner of enclosure. Secure each of the four corners of the pedestal enclosure to the anchors in the foundation with a 1/2 in. galvanized or stainless steel machine thread bolt, a properly sized locknut and a flat washer.
6. Finish top of concrete foundation in a neat and workmanlike manner. If leveling washers are used, ensure no more than 1/8 in. gap at any corner. Do not exceed a maximum dip or rise in the foundation of 1/8 in. per foot. When properly installed, ensure the top of the service enclosure is level front to back and side to side within 1/4 in. Repair rocking or movement of the service enclosure at no additional cost to the department.
7. Do not use liquidtight flexible metal conduit (LFMC) on pedestal type services.
8. Ensure all elbows in the foundation are sized as per utility provider's conduit requirements for underground conduit and feeders. PVC extensions may be installed provided the ends of the rigid metal conduits are more than 2 in. below the top of the concrete foundation. Where extension conduits are metal, grounding bushings must be installed with a bonding jumper properly terminated.



FRONT VIEW

SIDE VIEW

TYPE C shown, TYPE A similar except that TYPE A shall have individual circuit breakers (CB) mounted on an equipment mounting panel. CB Handles shall protrude through hinged deadfront trim.



SECTION A-A

ANCHOR BOLT DETAIL

LEGEND

1	Meter Socket, (when required)
2	Meter Socket Window, (when required)
3	Equipment Mounting Panel
4	Photo Electric Control Window, (When required)
5	Hinged Deadfront Trim
6	Load Side Conduit Trim
7	Line Side Conduit Area
8	Utility Access Door, with handle
9	Pedestal Door
10	Hinged Meter Access
11	Control Station (H-O-A Switch)
12	Main Disconnect
13	Branch Circuit Breakers
14	Copper Clad Ground Rod - 5/8" X 10'

		Traffic Operations Division Standard	
ELECTRICAL DETAILS ELECTRICAL SERVICE SUPPORT PEDESTAL SERVICE TYPE PS			
ED(9) - 14			
FILE: ed9-14.dgn	DW: TxDOT	CK: TxDOT	DW: TxDOT
© TxDOT October 2014	CONT: 0054	SECT: 04	JOB: 123
REVISIONS	DIST: BWD	COUNTY: COLEMAN CO.	HIGHWAY: US 84
			SHEET NO.: 92

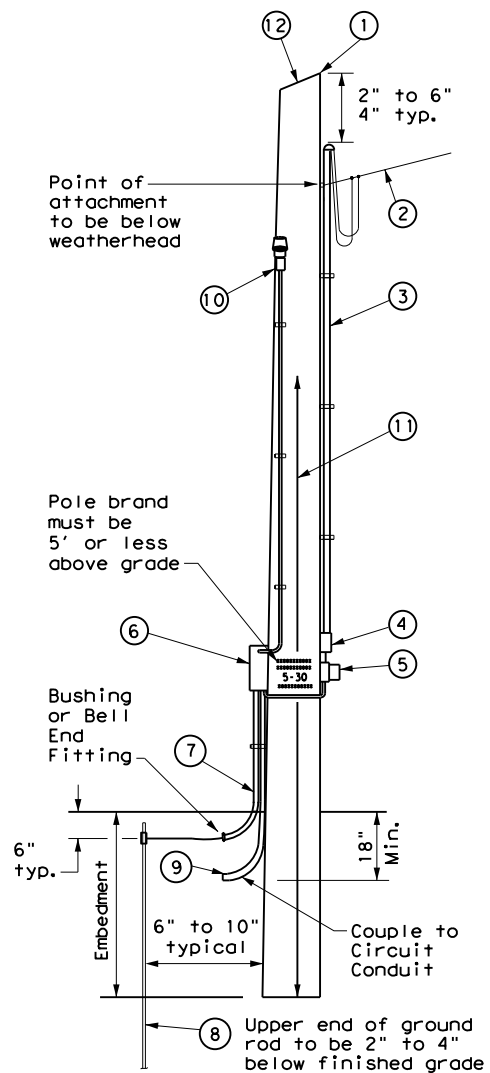
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TIMBER POLE (TP) SERVICE SUPPORT NOTES

1. Ensure electrical service support is a class 5 treated timber pole as per Item 627 "Treated Timber Poles." Embed timber pole to depth required in Item 627.
2. Conduit and electrical conductors attached to the electrical service pole and underground within 12 in. of service pole are not paid for directly but are subsidiary to the electrical service.
3. Install pole-top mounted photocell (T) on north side of pole, or in service enclosure (E) as required. See Electrical Service Data chart in plan set.
4. Gain pole as required to provide flat surface for each channel. Gain timber pole to 3/8 in. max. depth and 1 7/8 in. max. height. Gain pole in a neat and workmanlike manner.
5. Mount meter and service equipment on stainless steel or galvanized channel (Unistrut, Kindorf, or equal). Provide channel sized 1 in. to 3 3/4 in. maximum depth, and 1 1/2 in. to 1 5/8 in. maximum width. File smooth the cut ends of galvanized channel and paint with zinc rich paint before installing on pole. Secure each channel section to timber pole with two galvanized or SS lag bolts, 1/4 in. minimum diameter by 1 1/2 in. minimum length. Use a galvanized or SS flat washer on each lag bolt. Do not stack channel.
6. When excess length must be trimmed from poles, trim from the top end only.

- 1 Class 5 pole, height as required
- 2 Service drop from utility company (attached below weatherhead)
- 3 Service conduit (RMC) and service entrance conductors - One Red, One Black, One White (See Electrical Service Data)
- 4 Safety switch (when required)
- 5 Meter (when required)
- 6 Service enclosure
- 7 6 AWG bare grounding electrode conductor in 1/2 in. PVC to ground rod - extend 1/2 in. PVC 6 in. underground.
- 8 5/8 in. x 8 ft. Copper clad ground rod - drive ground rod to a depth of 2 in. to 4 in. below grade.
- 9 RMC same size as branch circuit conduit.
- 10 See pole-top mounted photocell detail on ED(5).
- 11 When required by the serving utility provide bare 6 AWG copper conductor. Run wire from pole top to butt wrap or copper butt plate. Protect conductor with non-conductive material to a height of 8 ft. above finished grade.
- 12 When required by utility, cut top of pole at an angle to enhance rain run off.

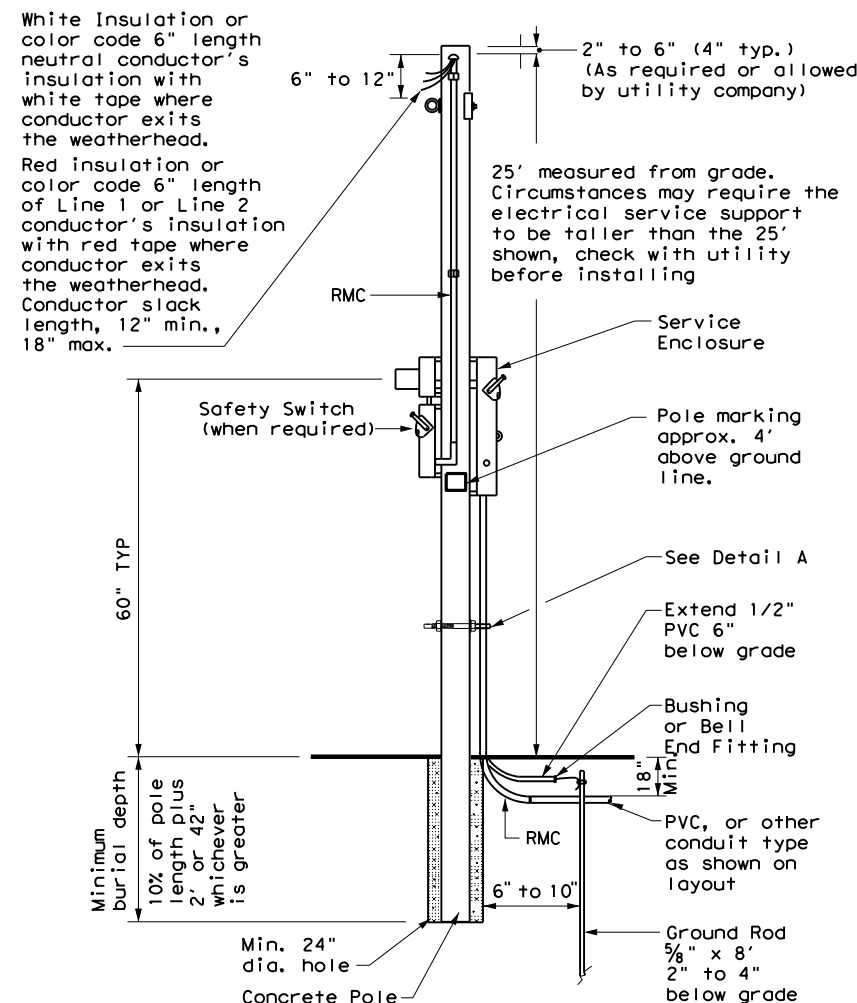


SERVICE SUPPORT TYPE TP (O)

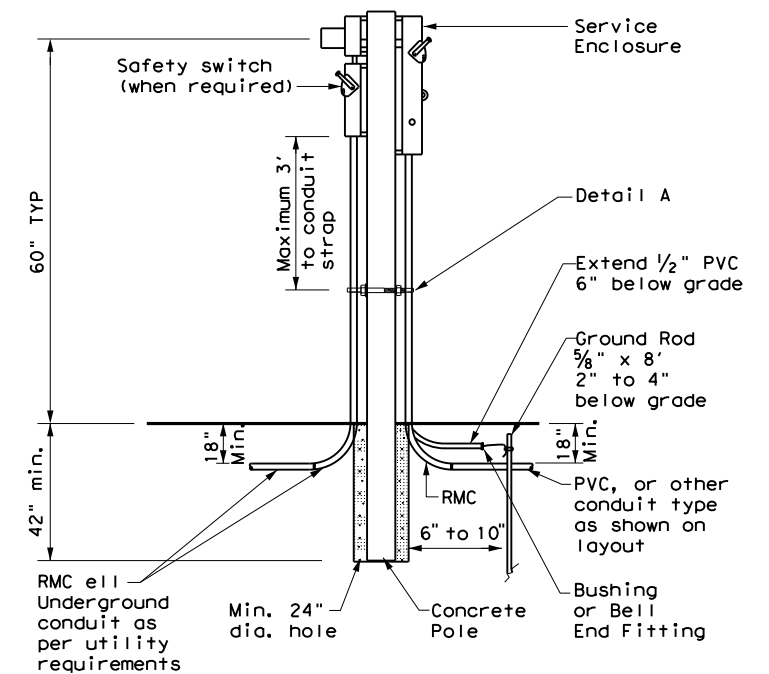
GRANITE CONCRETE (GC) & OTHER CONCRETE (OC) NOTES

Ensure electrical service support structures bid as type Granite Concrete (GC) or Other Concrete (OC) meet the following requirements.

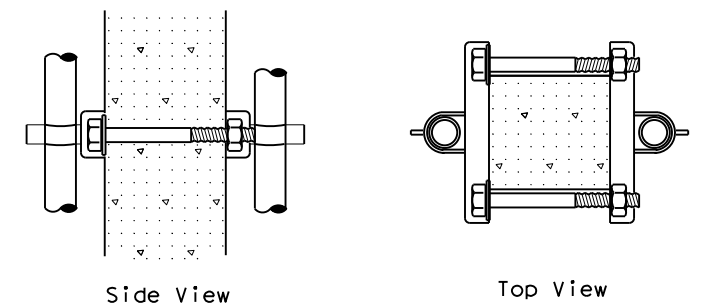
1. Provide GC and OC poles that meet the requirements of DMS 11080 "Electrical Services."
2. Provide prestressed concrete poles suitable for direct embedment into the ground without special foundations.
3. Verify poles are marked as required on DMS 11080. Location of marking should be approximately 4' above final grade. Use the two-point pickup locations when handling pole in horizontal position, and one-point pickup location for use in raising the pole to a vertical position. These marks are small but conspicuous.
4. Embed poles 42 in. or 10% of the length plus 2 ft., whichever is greater.
5. Ensure all installation details of services are in accordance with utility company specifications.
6. Install a one point rack or eye bolt bracket 6 inches to 12 inches below the weatherhead as an overhead service drop anchoring point for the electric utility.
7. Furnish and install galvanized or stainless steel channel strut 1 1/2 in. or 1 5/8 in. wide by 1 in. up to 3 3/4 in. deep (Unistrut, Kindorf, B-line or equal). Attach channel strut with stainless steel concrete anchors (max. 1" depth), square U-bolts or back to back channel strut with long bolts, or other secure mounting as approved by the Engineer. Ensure bolts are galvanized in accordance with ASTM A153. Do not stack channel struts.
8. Backfill the holes thoroughly by tamping in 6 in. lifts. After tamping to grade, place additional backfill material in a 6 inch high cone around the pole to allow for settling. Use material equal in composition and density to the surrounding area. Backfilling will not be paid for directly but is subsidiary to various bid items.



CONCRETE SERVICE SUPPORT Overhead (O)



CONCRETE SERVICE SUPPORT Underground (U)



DETAIL A

See Note 7. Before installing channel that has been cut, file sharp edges and paint with zinc-rich paint. Ensure there is no paint splatter on the pole.

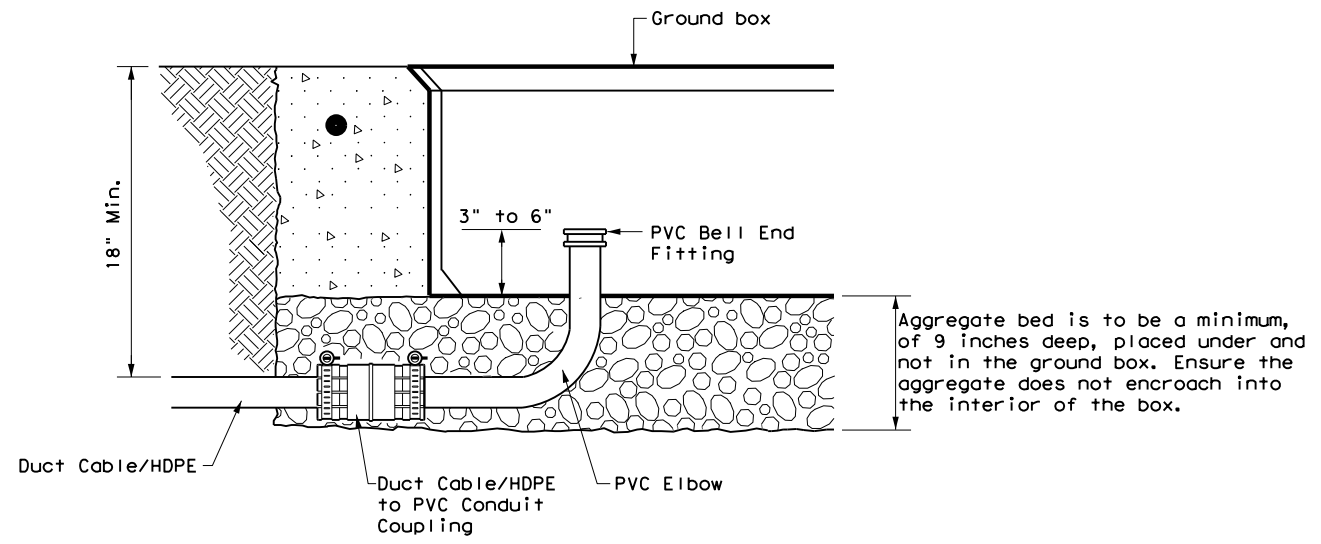
		Texas Department of Transportation		Traffic Operations Division Standard	
ELECTRICAL DETAILS SERVICE SUPPORT TYPES GC, OC, & TP					
ED(10)-14					
FILE:	ed10-14.dgn	DN:	TxDOT	CK:	TxDOT
© TxDOT	October 2014	CONT	SECT	JOB	HIGHWAY
REVISIONS		0054	04	123	US 84
		DIST	COUNTY	SHEET NO.	
		BWD	COLEMAN CO.	93	

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DUCT CABLE & HDPE CONDUIT NOTES

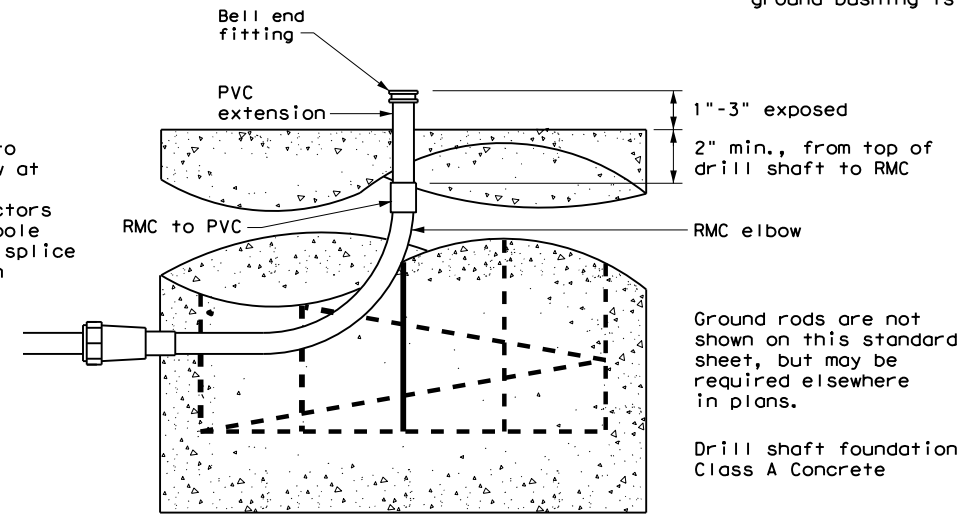
1. Provide duct cable in accordance with Departmental Material Specification (DMS) 11060 "Duct Cable" and Item 622 "Duct Cable." Provide duct cable as listed on the Material Producer List (MPL) on the Department web site under "Roadway Illumination and Electrical Supplies" Item 622.
2. Provide High-Density Polyethylene (HDPE) conduit in accordance with DMS 11060 and Item 618, "Conduit." Provide HDPE as listed on the MPL on the Department web site under "Roadway Illumination and Electrical Supplies," Item 618.
3. Supply duct cable with a minimum 2 in. diameter, unless otherwise shown in the plans. Provide duct cable and HDPE conduit as shown by descriptive code or on the plans. Bend duct cable and HDPE conduit as recommended by the manufacturer, with a minimum bending radius of 26 in. for 2 in. duct. Follow manufacturers' recommendations when handling duct cable and HDPE conduit reels and during installation of duct cable and HDPE conduit.
4. Do not splice conductors within duct cable or HDPE conduit. Couple duct cable and HDPE entering a ground box or foundation to a PVC elbow. When galvanized steel RMC elbows are called for in the plans and any portion of the RMC elbow is buried less than 18" from possible contact, ground the RMC elbow.
5. Furnish and install duct cable with factory installed conductors, sized as shown in the plans and as required by the National Electrical Code (NEC). The NEC contains specific requirements for duct cable in Article, "Nonmetallic Underground Conduit with Conductors: Type NUCC."
6. When conduit casing is called for in the plans, extend duct cable or HDPE conduit through the conduit casing in one continuous length without connection to the casing.
7. Seal the ends of duct cable or HDPE conduit with duct seal, expandable foam, or other approved method after completing the pull tests required by Item 622.
8. Provide minimum cover of 24 in. under roadways, 18 in. in other locations, or as shown on the plans.
9. Furnish and install listed fittings to couple duct cable or HDPE conduit to other types of conduit. Duct cable and HDPE conduit may be field-threaded and spliced with PVC or RMC threaded couplings; connected with listed tie-wrap fittings; connected using listed coupling made of HDPE with stainless steel external banding clamps and locking rings; connected with approved electrofusion conduit couplings; or connected using an approved chemical fusion method using an epoxy or adhesive specifically designed for HDPE couplings and connectors all installed in accordance with their manufacturer's instructions. Do not use PVC glue on HDPE. Do not use water pipe fittings, or connect conduit with heat shrink tubing.



DUCT CABLE/HDPE AT GROUND BOX

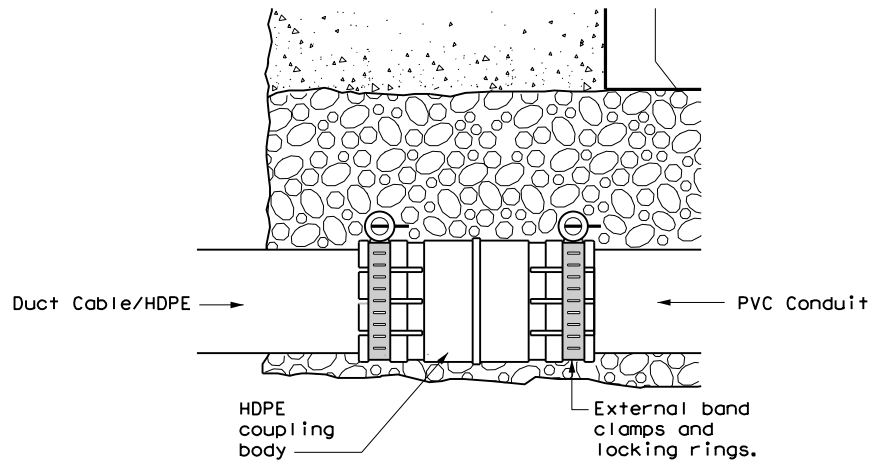
When the upper end of an RMC Ell does not enter the ground box, it may be extended with a SCH-40 PVC conduit nipple and bell end, provided there is a minimum of 18" of cover over all parts of the elbow. If not, a rigid extension and ground bushing is required.

Couple duct to conduit elbow at foundations. Ensure conductors extend into pole base. Do not splice conductors in conduit.

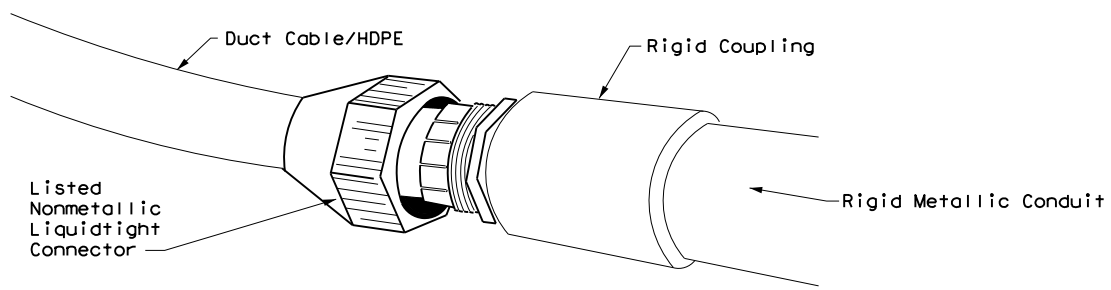


DUCT CABLE / HDPE AT FOUNDATION

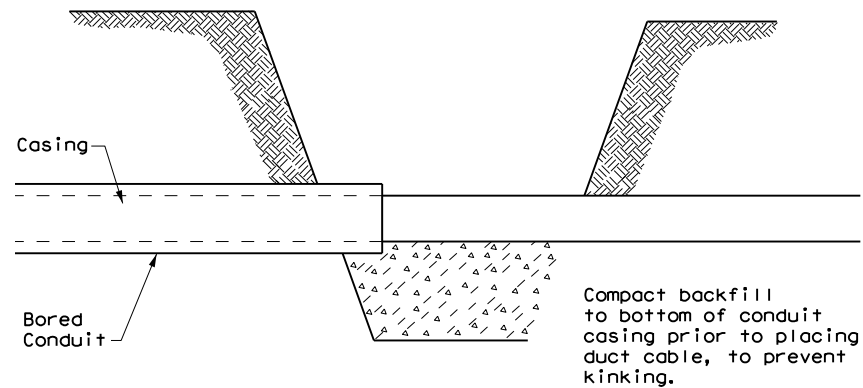
Ground rods are not shown on this standard sheet, but may be required elsewhere in plans.
 Drill shaft foundation Class A Concrete



DUCT CABLE/HDPE TO PVC



DUCT CABLE/HDPE TO RMC



BORE PIT DETAIL

		Traffic Operations Division Standard	
ELECTRICAL DETAILS DUCT CABLE/ HDPE CONDUIT			
ED(11)-14			
FILE: ed11-14.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
©TxDOT October 2014	CONT	SECT	JOB
REVISIONS	0054	04	123
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		94

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 FILE: T:\BWDSSGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to apr...
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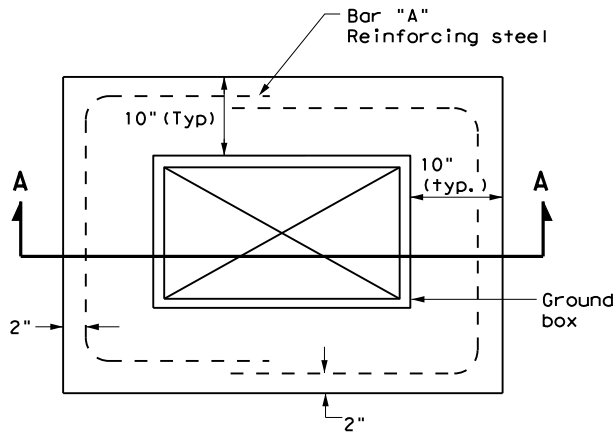
BATTERY BOX GROUND BOXES NOTES

A. MATERIALS

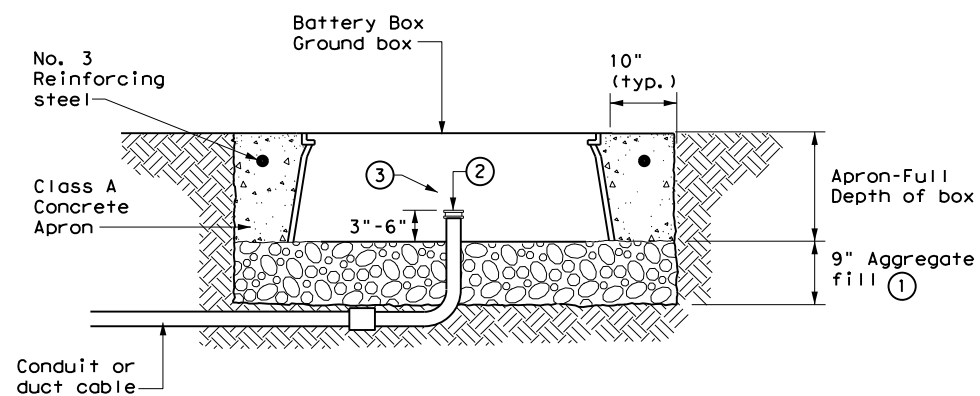
1. Provide polymer concrete or fiberglass reinforced plastic (FRP) battery box ground box and cover in accordance with Departmental Material Specification (DMS) 11071 "Battery Box Ground Boxes." Battery box will accommodate up to 4 batteries, each measuring 8 in. x 13.5 in. x 10 in. (W x L x D). Label battery box ground box cover in accordance with DMS 11071.
2. Supply a marine grade batteries with covers. Secure the marine grade batteries with covers to the stainless steel rack in the bottom of the ground box with tie down straps.

B. CONSTRUCTION METHODS

1. Ensure conduit entry will not interfere with placement of the batteries in the battery box ground box.
2. Remove all gravel and dirt from conduit. Cap all conduits prior to placing aggregate and setting battery box ground box. Provide Grade 3 or 4 coarse aggregate as shown on Table 2 of Item 302 "Aggregates for Surface Treatments." Ensure the aggregate bed is in place and is a minimum of 9 in. deep prior to setting the box. Install battery box ground box on top of aggregate.
3. Cast battery box aprons in place. Reinforcing steel may be field bent. Ensure the depth of concrete for the apron extends from finished grade to the top of the aggregate bed under the box. Battery box ground box aprons, including concrete and reinforcing steel, are subsidiary to battery box ground boxes when called for by descriptive code.
4. Bolt covers down when not working in battery box ground boxes. Keep bolt holes in the box clear of dirt.



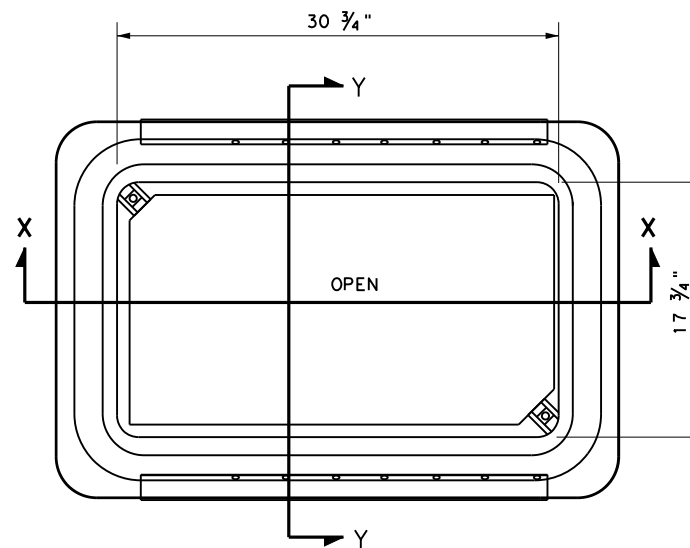
PLAN VIEW



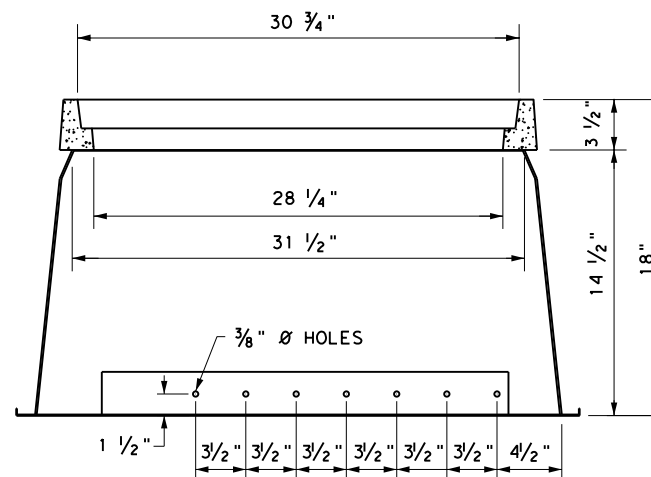
SECTION A - A

APRON FOR BATTERY BOX GROUND BOXES

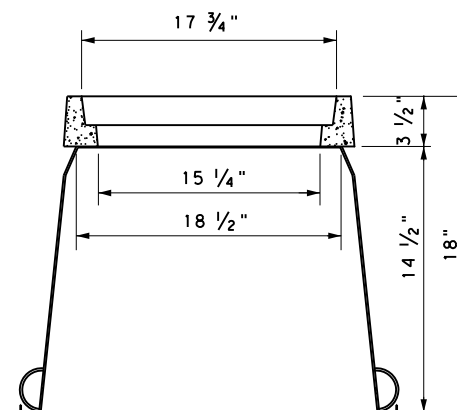
- ① Place aggregate under the box and not in the box. Aggregate should not encroach on the interior volume of the box.
- ② Install bushing or bell end fitting on the upper end of allells.
- ③ Install all conduits in a neat and workmanlike manner.



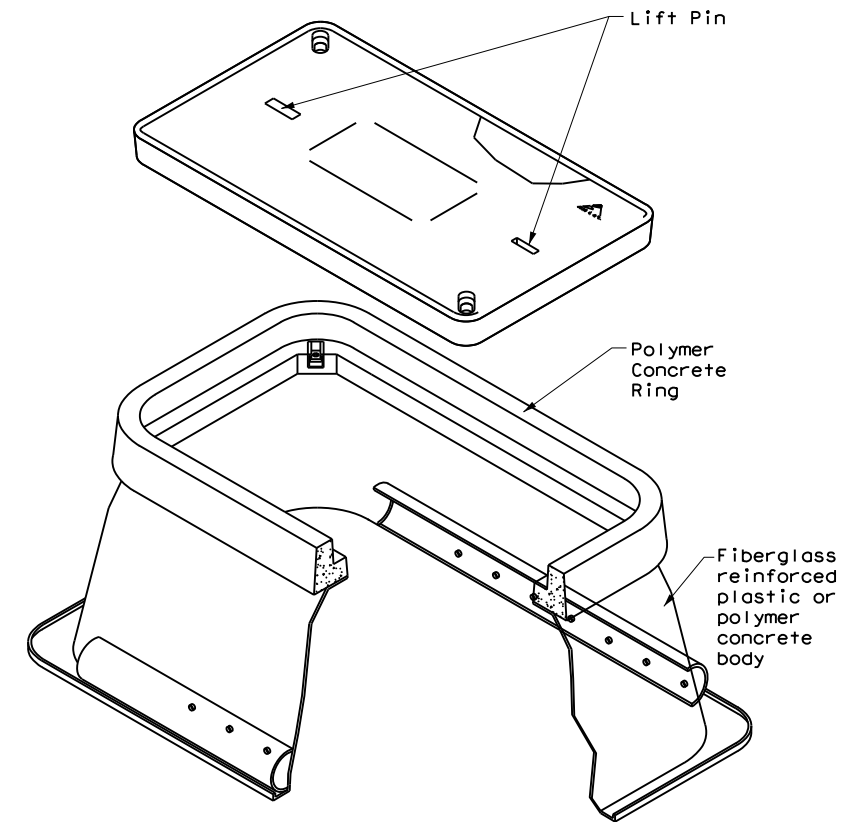
BATTERY BOX TOP VIEW



SECTION X-X



SECTION Y-Y



		Traffic Operations Division Standard	
<h2>ELECTRICAL DETAILS</h2> <h3>BATTERY BOX GROUND BOXES</h3> <h4>ED(12)-14</h4>			
FILE: ed12-14.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT
© TxDOT October 2014	CONT: 0054	SECT: 04	JOB: 123
REVISIONS	DIST: BWD	COUNTY: COLEMAN CO.	HIGHWAY: US 84
			SHEET NO.: 95

ROADWAY ILLUMINATION ASSEMBLY NOTES

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1. Details apply to roadway lighting installations bid or referenced under Item 610, "Roadway Illumination Assemblies." Provide, furnish, and install all other materials not shown on the plans which may be necessary for complete and proper construction. Where manufacturers provide warranties or guarantees as a customary trade practice, furnish to the State such warranties or guarantees.
2. The locations of poles and fixtures may be shifted by the Engineer to accommodate local conditions. Install or remove poles and luminaires located near overhead electrical lines using established industry and utility safety practices and in accordance with laws governing such work. Consult with the appropriate utility company prior to beginning such work.
3. Provide new and unused materials. Ensure that all materials and installations comply with the applicable articles of the National Electrical Code (NEC), TxDOT standards and specifications, National Electrical Manufacturers Association (NEMA), and are listed by Underwriters Laboratories (UL) or a Nationally Recognized Testing Lab (NRTL). NRTLs such as Canadian Standard Association, Intertek Testing Services NA Inc., or FM Approvals LLC can be considered equivalent to UL. Faulty fabrication or poor workmanship in any material, equipment, or installation is justification for rejection.
4. Provide Roadway Illumination Light Fixtures as per TxDOT Departmental Material Specification (DMS) 11010, Item 610, and as shown on the Material Producers List (MPL) for Roadway Illumination and Electrical Supplies.
5. Fabricate steel roadway illumination poles in accordance with Roadway Illumination Poles (RIP) standards and Item 610. Poles fabricated according to RIP standards do not require shop drawing submittals.
 - a. Alternate designs to RIP standards or the use of aluminum to fabricate poles will require the submission of shop drawings electronically. For instructions on submitting shop drawings electronically see "Guide to Electronic Shop Drawing Submittal" on the TxDOT web site.
 - b. Limitations on use of the RIP standard: The RIP standard details were developed for installations in locations where the 3-second gust basic maximum wind speed is 110 mph, and where the elevation of the base of the pole is less than (i.e. not more than) 25' above the elevation of the surrounding terrain, in accordance with the "AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals," 6th Edition (2013) of the AASHTO Design Specifications. For poles to be installed in regions where the maximum basic wind speed exceeds 110 mph or to be mounted more than 25' above the surrounding terrain, provide poles meeting the following requirements:
 - i. Submittals. Following the electronic shop drawing submittal process (see Guide to Electronic Shop Drawing Submittal on the TxDOT web site), submit to the Engineer for approval fabrication drawings and calculations for the poles, sealed by a Texas licensed professional engineer (P.E.).
 - ii. Luminaire Structural Support Requirements. Provide light poles, arms, and anchor bolt assemblies with a 25 year design life to safely resist dead loads, ice loads and the required basic wind speeds at the location of installation in accordance with the 6th edition (2013) of the AASHTO Design Specifications. For transformer base poles, include transformer base and connecting hardware in calculations and shop drawing submittals. Structurally test all transformer bases to resist the theoretical plastic moment capacity of the pole. Submit certification of the plastic moment load test and FHWA breakaway requirement test of the model of base being furnished with the shop drawings. Show breakaway base model number, manufacturer's name, and logo on shop drawings. Include on manufacturer's shop drawings the ASTM designations for all materials to be used.
6. For both transformer and shoe-base type illumination poles, provide and install double-pole breakaway fuse holders as specified by DMS-11040. Breakaway fuse holders are listed on the MPL for Roadway Illumination and Electrical Supplies under Items 610 & 620. Provide 10 amp time delay fuses for breakaway connectors in light poles, or inside the light fixture for underpass luminaires. In each pole, connect luminaires to the breakaway connector with continuous stranded 12 AWG copper conductors as listed on the MPL. Bond all equipment grounding conductors together and to the ground lug in the transformer base or hand hole.
7. Tighten anchor bolts for shoe base, concrete traffic barrier base, and bridge mount roadway illumination poles, in accordance with Item 449.
8. Install T-Base with following procedure:
 - a. Anchor Bolt Tightening.
 - i. Coat the threads of the anchor bolts with electrically conductive lubricant.
 - ii. Place the T-base over the anchor bolts. Foundation must be level and flat. The maximum permissible gap under any one corner of the t-base is 1/8" before nuts are tightened.
 - iii. Coat the bearing surfaces of the nuts and washers with electrically conductive lubricant. Install (1) 1/2" hold down washer, (1) lock washer, and (1) nut on each anchor bolt. Turn the nuts onto the bolts so that each is hand-tight against the washer.
 - iv. Using a torque wrench, tighten each nut to 150 ft-lb. Uniform contact is required between the foundation and the T-base in the corner regions of the T-base, and all corner gaps must be closed after applying torque. If a gap still exists after torquing to 150 ft-lbs, continue torquing each bolt incrementally until gap is closed or maximum allowable torque of 250 ft. pound is reached, whichever comes first. If 250 ft-lbs is not enough to close the gap the foundation must be leveled. Gaps along the straight sides of the T-bases and the foundation are permissible. Ensure that no high point of contact occurs between the straight sides of the T-base and the foundation.
 - v. Check top of T-base for level. If not level then foundation must be leveled.
 - b. Top Bolt Procedure
 - i. Erect pole over T-base with crane. Coat bolts, nuts, washers, and lock washers with electrically conductive lubricant.

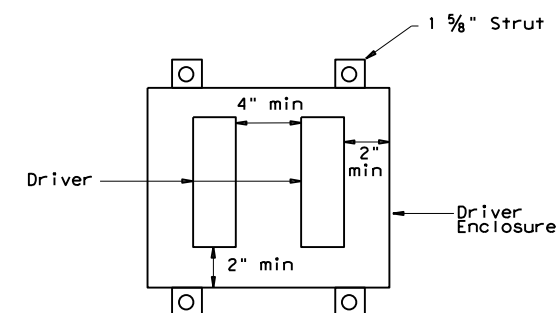
- ii. Install bolts and 1/2" connecting washers from the inside of the T-base, thread up through the pole base. Install flat washers, lock washers and nuts snug tight according to Item 447, "Structural Bolting."
 - iii. Tighten each nut to 150 ft-lb. using a torque wrench.
- c. Level and Plumb
- i. Ensure pole is plumb and mast arm is perpendicular to the roadway according to plans to within 5 degrees.
9. Construct luminaire pole foundations in accordance with Item 416, "Drilled Shaft Foundations," and TxDOT standard sheet RID(2).
 10. Provide and install underpass luminaires in accordance with Item 610, DMS-11010, and TxDOT standard sheet RID(3). Typical luminaire size for underpass luminaires is 150W HPS or 150W EQ LED.
 11. Mount luminaires on arms level as shown by the luminaire level indicator.
 12. Orient luminaires perpendicular to the roadway intended to be lit unless otherwise shown on the plans.

Wiring Diagram Notes:

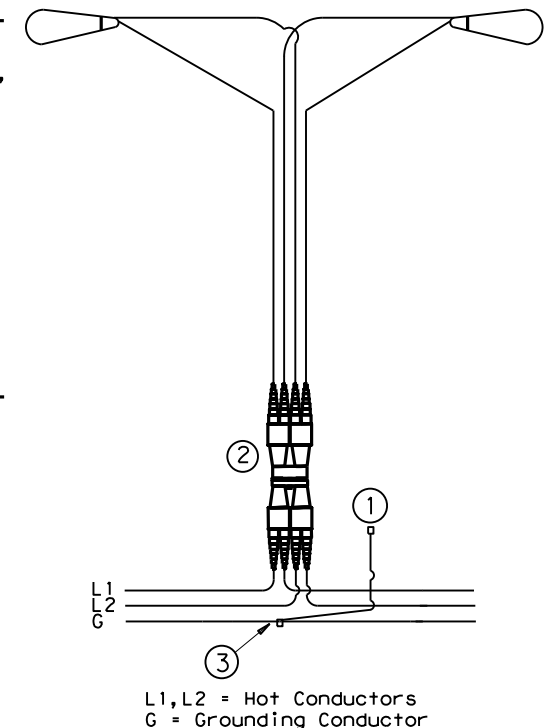
- ① Use 1/2 in. -13 UNC threaded, copper or tin-plated copper, pole bonding connector, sized appropriately for conductors, bonded to T-base, or use ground lug in handhole as available.
- ② Use pre-qualified two-pole breakaway connectors for all luminaire pole installations. For luminaires fed by a circuit with a neutral conductor, use double pole breakaway connectors with the neutral side unfused and marked white.
- ③ Split Bolt or other connector.

Decorative LED Lighting Notes:

1. LED Drivers in Remote Outdoor enclosures (for drivers that do not include an enclosure as part of a factory assembly):
 - a. Provide NEMA 3R outdoor enclosure or as approved.
 - b. Install enclosure at least 12" above ground or other horizontal surface. Mount vertically or on ceiling, and avoid direct sun where possible.
 - c. Install drivers with at least 2 inches of space from enclosure walls.
 - d. For multiple drivers in an enclosure, provide at least 4 inches side to side and 1 inch end to end from other drivers or electronic equipment
 - e. For drivers mounted on back wall of enclosure, mount enclosure on 1 5/8" strut or other standoff to dissipate heat, or mount driver to side of the enclosure or to the metal cover.
 - f. Provide remote drivers with a maximum of 100 watts
 - g. Provide drivers with documentation of 100,000 hr lifetime at Tcase of 65C or higher.



Driver Spacing In Remote Enclosure



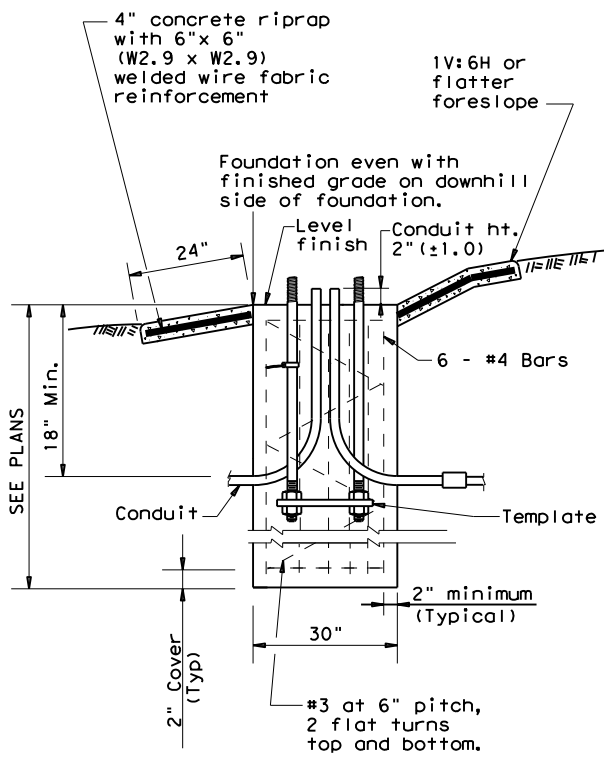
TYPICAL WIRING DIAGRAM

LUMINAIRES SERVED AT 480V ON 240/480 VOLT SERVICE OR LUMINAIRES SERVED AT 240V FOR 120/240 VOLT SERVICE.

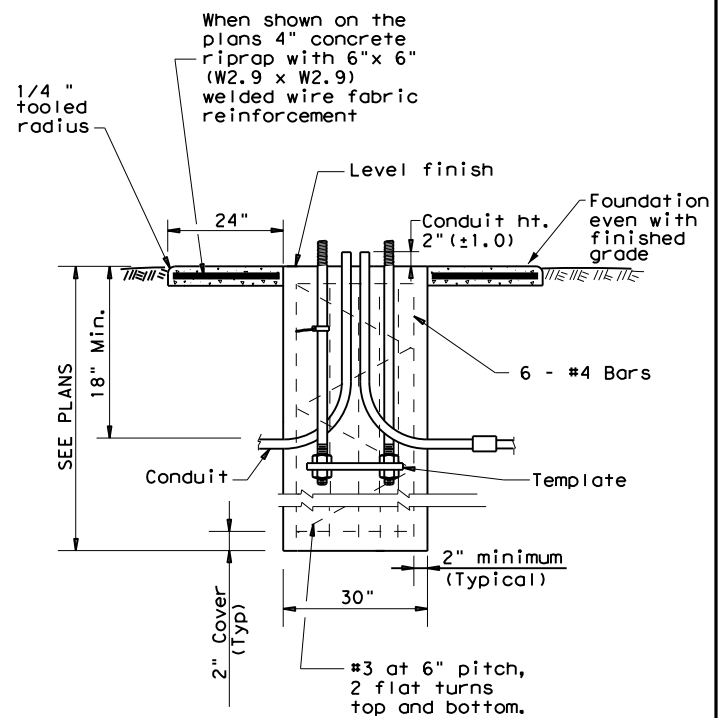
<h3>ROADWAY ILLUMINATION DETAILS</h3> <h3>RID(1)-20</h3>			
FILE: rid1-20.dgn	DN: January 2007	CK: 0054	DW: 04
REVISIONS 0054 04		JOB 123	HIGHWAY US 84
7-17 12-20	DIST BWD	COUNTY COLEMAN CO.	SHEET NO. 96

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SECTION A-A
SHOWING SLOPED GRADE



SECTION A-A
SHOWING CONSTANT GRADE

TABLE 1

ANCHOR BOLTS

POLE MOUNTING HEIGHT	BOLT CIRCLE		ANCHOR BOLT SIZE
	Shoe Base	T-Base	
<40 ft.	13 in.	14 in.	1 in. x 30 in.
40-50 ft.	15 in.	17 1/4 in.	1 1/4 in. x 30 in.

TABLE 2

RECOMMENDED FOUNDATION LENGTHS
(See note 1)

MOUNTING HEIGHT	TEXAS CONE PENETROMETER N Blows/ft		
	10	15	40
<20 ft.	6'	6'	6'
>20 ft. to 30 ft.	8'	6'	6'
>30 ft. to 40 ft.	8'	8'	6'
>40 ft. to 50 ft.	10'	8'	6'

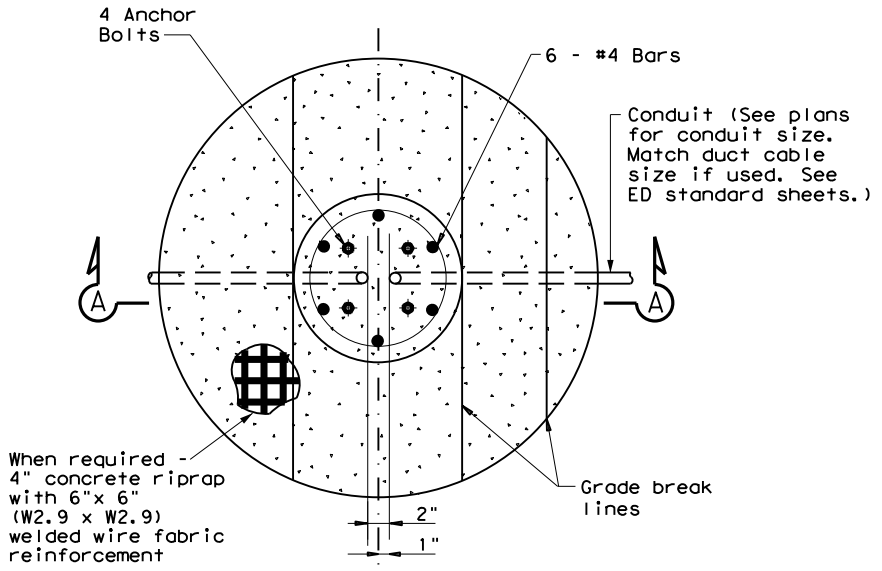
TABLE 3

PAY QUANTITY OF RIPRAP PER FOUNDATION
(Install only when shown on the plans)

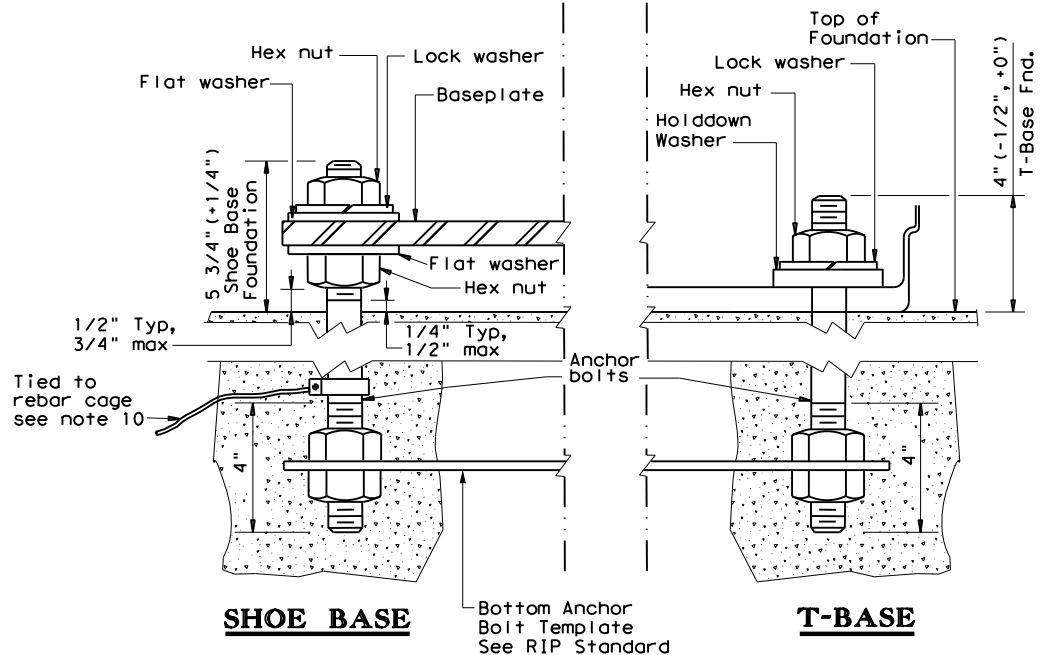
Foundation Diameter	RIPRAP DIAMETER	RIPRAP (CONC) (CL B)
30 in.	78 in.	0.35 CY

GENERAL NOTES:

- "Recommended Foundation Lengths" table is for information purposes only. Foundation lengths shall be as shown on the plans, or as directed by the Engineer. Foundations will be paid for under Item 416, "Drilled Shaft Foundations," unless otherwise shown on the plans.
- Erect roadway illumination assembly poles plumb and true. Form and level the top 6" of the foundation so the pole will be plumb. Use leveling nuts to plumb shoe base poles. Do not use shims or leveling nuts under transformer bases. Do not grout between baseplate and the foundation.
- Ensure Class 2A and 2B fit for anchor bolts and nuts. Tap and chase nuts after galvanizing. Anchor bolt body with rolled threads need not be full size.
- Use appropriate class of concrete as specified in Items 416 and 432. Concrete for riprap may be upgraded to Class C at no extra cost to the Department.
- Place riprap around the foundation when called for elsewhere in the plans. Riprap will be paid for under Item 432.
- Locate breakaway roadway illumination assemblies as shown in the placement table, unless otherwise dimensioned on the plans. Protect non-breakaway illumination assemblies from vehicular impact (i.e. 2.5 ft. behind guard rail or mounted on traffic barrier), or located outside the clear zone, except that 2.5 ft. from curb face is minimum desired for light poles on city streets, 45 mph or less. See Roadway Design Manual for further information.
- Use 4 hold down and 4 connecting washers on transformer base poles as recommended by the manufacturer and supplied with base.
- Install a minimum of 2 conduits in each foundation. See lighting layout sheets for locations of foundations with more than 2 conduits. Cap unused conduits in foundations on both ends.
- Conduit location in foundations is critical for breakaway devices. Place conduits 2 in. apart on centerline as shown.
- Bond anchor bolt to rebar cage with #6 bare stranded copper conductor. Use listed mechanical connectors rated for embedment in concrete. The bonded steel in the foundation creates a concrete encased grounding electrode which replaces the ground rod.
- Grade earthwork around T-base foundations even with the finished grade as shown in Section A-A to ensure proper function of the breakaway device. Use riprap on T-base foundations that are located on sloped grades, and as shown on the plans for level grades.



FOUNDATION DETAIL



ANCHOR BOLT DETAIL

TABLE 4

BREAKAWAY POLE PLACEMENT (See note 6)

ROADWAY FUNCTIONAL CLASSIFICATION	** POLE OFFSET (DISTANCE TO FACE OF TRANSFORMER BASE)
Freeway Mainlanes (roadway with full control of access)	15 ft. (minimum and typical) from lane edge
All curbed, 45 mph or less design speed	2.5 ft. minimum (15 ft. desirable) from curb face
All others	10 ft. minimum*(15 ft. desirable) from lane edge

* or as close to ROW line as is practical

** provide 2/5 of the luminaire mounting height behind the pole for "falling area" to prevent encroachment on the other travel lanes. See design guidelines.

Texas Department of Transportation
 Traffic Safety Division Standard

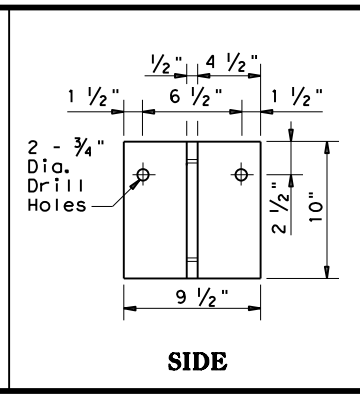
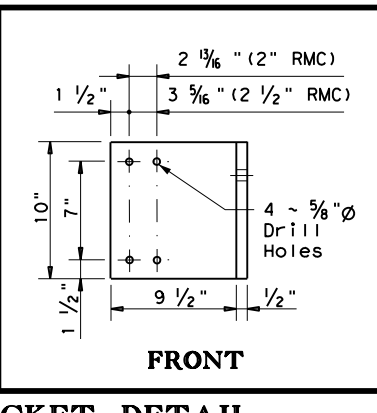
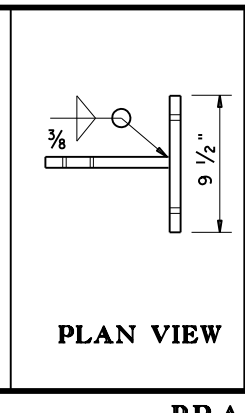
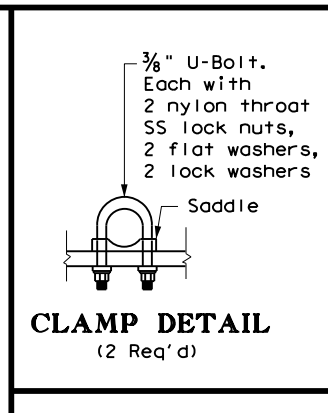
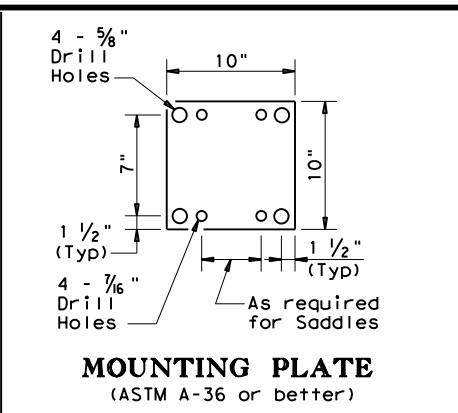
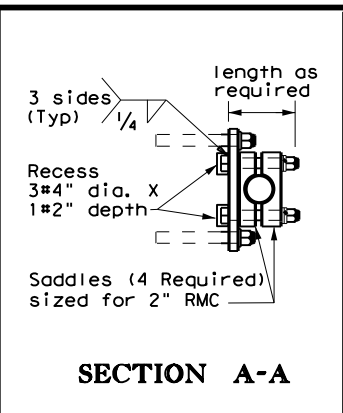
ROADWAY ILLUMINATION DETAILS (RDWY ILLUM FOUNDATIONS)

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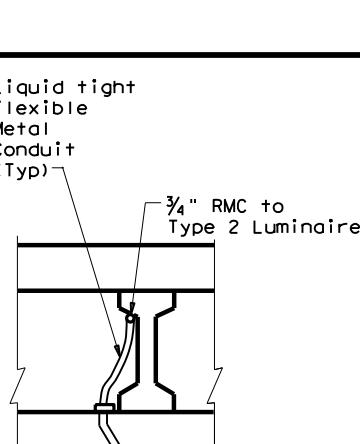
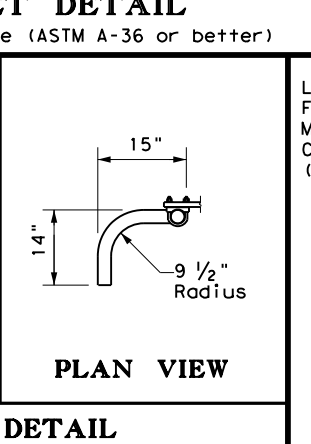
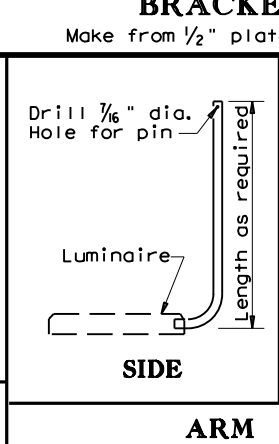
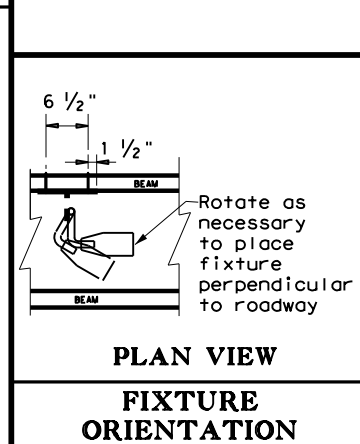
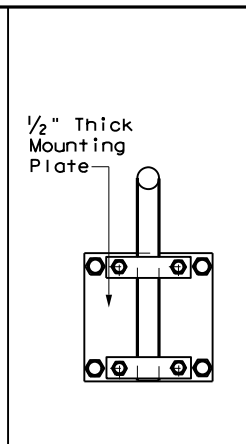
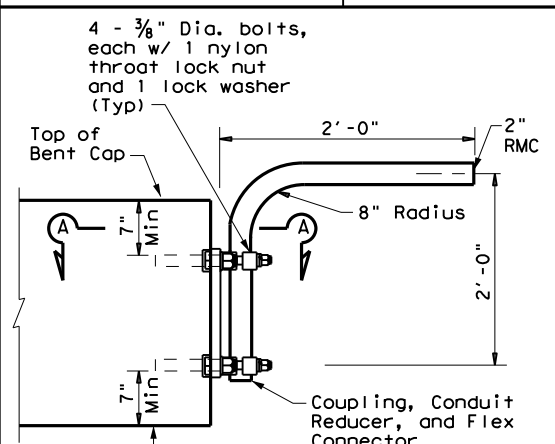
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© TxDOT January 2007	CONT	SECT	JOB	HIGHWAY
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1-11	DIST	COUNTY	SHEET NO.	
7-17	BWD	COLEMAN CO.	97	
12-20				

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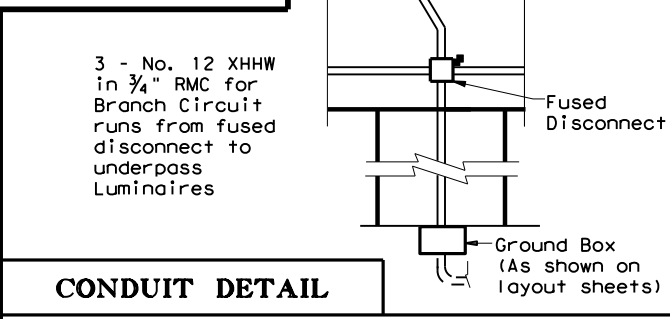
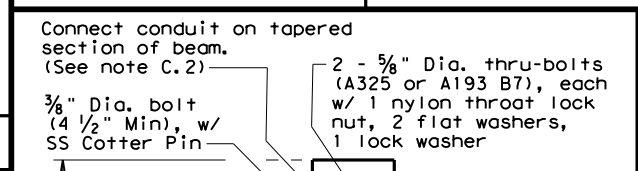
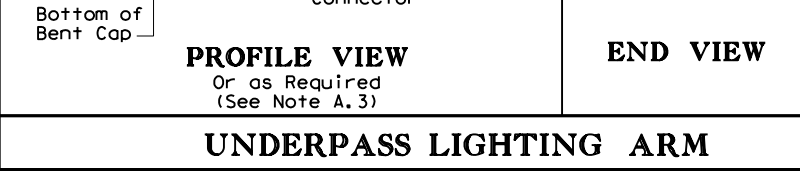
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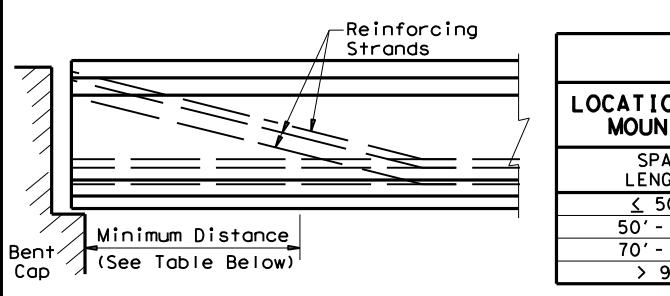
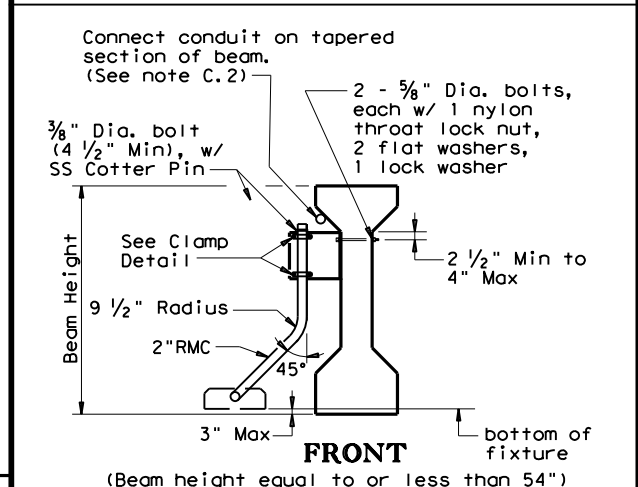
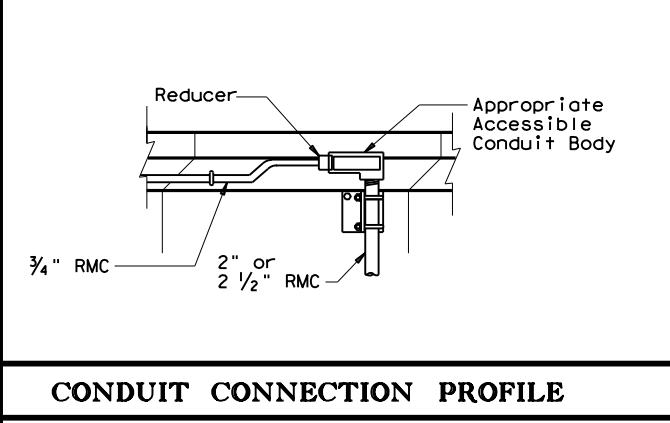
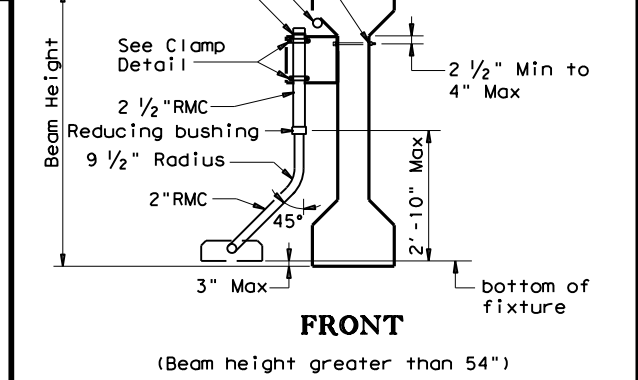
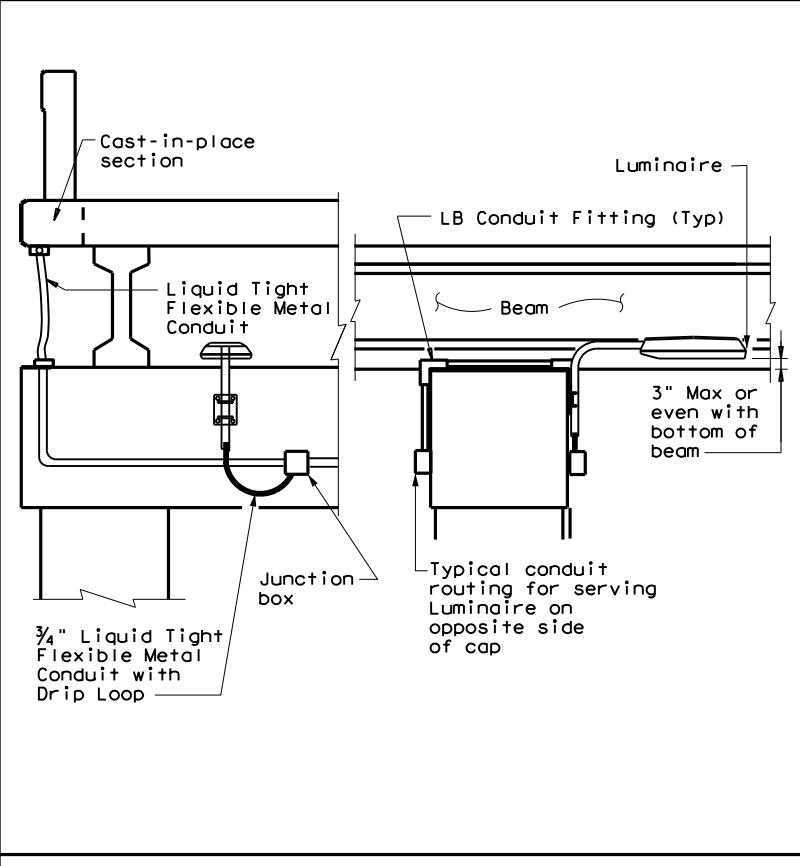
- GENERAL NOTES:**
- A. ALL 150 watt HPS and 150 watt equivalent LED Luminaires**
- Luminaire locations, conduit and conductor sizes and routing are typical and diagrammatic only. See project layout sheets for specific details.
 - Conduit will be paid for under Item 618, "Conduit" and conductors will be paid for under Item 620, "Electrical Conductors," unless otherwise shown on the plans.
 - Adjust conduit in saddles to place fixture height and orientation as required. See fixture orientation detail and plans. Where practicable, place luminaires so the bottom of luminaire is above the bottom of the beam, maximum of 3 in. (See detail UNDERPASS LIGHTING ARM TYPE 2)
 - Except as noted, galvanize all structural steel and exposed bolts, nuts, and washers in accordance with Item 445 "Galvanizing".
 - Fabrication of brackets and support arms will not be paid for directly but is subsidiary to Item 610, "Roadway Illumination Assemblies."
 - Install a heavy duty NEMA 3R fused disconnect or breaker enclosure rated at 30 amps and 480 volts to switch underpass luminaires as shown on plans, with at least one per bridge circuit. Install 20 amp time-delay fuses or inverse-time circuit breakers. Mount disconnect or breaker enclosure 10 ft. (min) above grade on columns or bent caps as approved by the Department. Modify disconnect to allow padlocking in the "ON" and "OFF" positions. Padlocks and disconnect switches or circuit breakers for underpass fixtures will not be paid for directly but are subsidiary to the various bid items of the contract.
 - Conduit on columns, caps, and slab is shown surface mounted. For new columns and caps, embed PVC conduit in concrete. Bond and ground metal junction boxes and conduit.



- B. TYPE 1**
- Provide 2 in. rigid metal conduit (2.375" O.D., 0.146" wall) for Type 1 arm shaft.
 - Use 3/8 in. stainless steel bolt or stud non-epoxy type expansion anchors for concrete for Type 1 mounting. Except as noted, provide an allowable 2650 lbs minimum pull-out force (after consideration of adjustment factors for edge distance and bolt spacing) for each anchor. Install each anchor to the embedment depth recommended by the manufacturer.
 - Attach conduit to plate with 4 saddles, four - 3/8 in. diameter bolts, nylon throat lock nuts, and lock washers.



- C. TYPE 2**
- Provide 2 in. rigid metal conduit (2.375" O.D., 0.146" wall) or provide a combination of 2 1/2 in. (2.875" O.D., 0.193" wall) and 2 in. (2.375" O.D., 0.146" wall) rigid metal conduits with a reducing bushing as beam height stipulated for Type 2 arm shaft. Field cutting and threading will be permitted. Paint cut and threaded areas with zinc rich paint after conduit is connected to adjacent fitting.
 - Connecting conduit may be strapped to tapered section only of precast beams as shown. Anchor as approved by the Engineer. Maximum anchor depth is 1 in.
 - Indiscriminate drilling into precast concrete beams may result in reduced beam strength. Use drilling location and method as directed by the Engineer. See Location of Underpass Lighting Mounting Bracket detail. The locations shown in the table are such that reinforcing strands will not be damaged.



SPAN LENGTH	MINIMUM DISTANCE
≤ 50'	10'-0"
50' - 70'	15'-0"
70' - 90'	20'-0"
> 90'	25'-0"

Texas Department of Transportation

ROADWAY ILLUMINATION DETAILS
 (UNDERPASS LIGHT FIXTURES)
RID(3)-20

Traffic Safety Division Standard

UNDERPASS LIGHTING TYPE 1

IN RD IL AM (U/P)(TY 1)
 If bridge has pre-cast panels under deck, run circuit under deck edge.

UNDERPASS LIGHTING TYPE 2

IN RD IL AM (U/P)(TY 2)

LOCATION OF UNDERPASS LIGHT MOUNTING BRACKET

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 © TxDOT May 2013 CONT: 0054 SECT: 04 JOB: 123 HIGHWAY: US 84
 REVISIONS: DIST: BWD COUNTY: COLEMAN CO. SHEET NO.: 98
 2-14
 7-17
 12-20
 72C

SHIPPING PARTS LIST - POLES AND LUMINAIRE ARMS

Nominal Mounting Ht. (ft)	Shoe Base					T-Base					CSB/SSCB Mounted				
	Designation				Quantity	Designation				Quantity	Designation				Quantity
	Pole	A1	A2	Luminaire		Pole	A1	A2	Luminaire		Pole	A1	A2	Luminaire	
20	(Type SA 20 S - 4)			(150W EQ) LED		(Type SA 20 T - 4)			(150W EQ) LED						
	(Type SA 20 S - 4 - 4)			(150W EQ) LED		(Type SA 20 T - 4 - 4)			(150W EQ) LED						
30	(Type SA 30 S - 4)			(250W EQ) LED		(Type SA 30 T - 4)			(250W EQ) LED		(Type SP 28 S - 4)		(250W EQ) LED		
	(Type SA 30 S - 4 - 4)			(250W EQ) LED		(Type SA 30 T - 4 - 4)			(250W EQ) LED		(Type SP 28 S - 4 - 4)		(250W EQ) LED		
40	(Type SA 30 S - 8)			(250W EQ) LED		(Type SA 30 T - 8)			(250W EQ) LED		(Type SP 28 S - 8)		(250W EQ) LED		
	(Type SA 30 S - 8 - 8)			(250W EQ) LED		(Type SA 30 T - 8 - 8)			(250W EQ) LED		(Type SP 28 S - 8 - 8)		(250W EQ) LED		
	(Type SA 40 S - 4)			(250W EQ) LED		(Type SA 40 T - 4)			(250W EQ) LED		(Type SP 38 S - 4)		(250W EQ) LED		
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	(Type SA 40 S - 10)			(250W EQ) LED		(Type SA 40 T - 10)			(250W EQ) LED		(Type SP 38 S - 10)		(250W EQ) LED		
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50	(Type SA 50 S - 4)			(400W EQ) LED		(Type SA 50 T - 4)			(400W EQ) LED		(Type SP 48 S - 4)		(400W EQ) LED		
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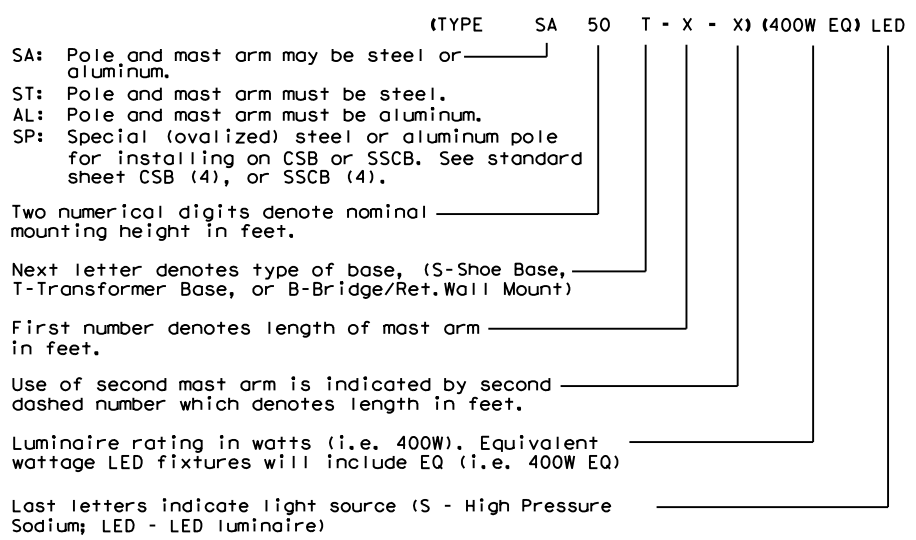
OTHER				
Designation				Quantity
Pole	A1	A2	Luminaire	

GENERAL NOTES:

- All work, materials and services not shown on the plans which may be necessary for complete and proper construction shall be performed, furnished and installed by the Contractor. Faulty fabrication or poor workmanship in any material, equipment or installation will be considered justification for rejection. Where manufacturers provide warranties or guarantees as a customary trade practice, furnish to the Department such warranties or guarantees.
- The location of poles and fixtures are diagrammatic only and may be shifted by the Engineer to accommodate local conditions. Install or remove poles and luminaires located near overhead electrical lines using established industry and utility safety practices and in accordance with laws governing such work. Consult with the appropriate utility company prior to beginning such work.
- Standard Steel Pole Designs. Steel poles fabricated in accordance with the details and dimensions shown herein, shall be considered standard designs. Submission of shop drawings and design calculations for standard designs is not required.
- Optional Steel Pole Designs. Multi-sided steel poles may be allowed as optional designs, if steel poles are permitted or required, pending approval by the Department as outlined below.
 - Shop Drawings. Optional designs require submission of shop drawings and design calculations bearing the seal of an engineer licensed in the State of Texas, in accordance with Item 441, "Steel Structures." The Department may elect to pre-approve some shop drawings for optionally designed poles. Submission of shop drawings and design calculations is not required for structures fabricated in accordance with the details of shop drawings on the pre-approved list maintained by the TxDOT Traffic Operations Division. Any deviation from the pre-approved shop drawings will require submission of shop drawings of the complete assembly and design calculations as described above.
 - Structural Support Design for Luminaires. Lighting support structures shall be designed for a 25 year design life in accordance with the AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals, 6th Edition (2013) and Interim Revisions thereto. All poles shall be designed for 110 mph 3-second gust wind speeds. The Gust Factor, G, and Wind Importance Factor, Ir, shall be applied as per the AASHTO Specifications assuming a 25-year design life. The design wind pressure for hurricane wind velocities greater than 100 mph shall not be less than the design wind pressure using 100 mph with the non-hurricane Wind Importance Factor, Ir, value. For transformer base poles, fabricator shall include transformer base and connecting hardware in design calculations and shop drawing submittals. All transformer bases shall have been structurally tested to resist the theoretical plastic moment capacity of the pole. Certification of the plastic moment load test and FHWA breakaway requirement test of the model of base being furnished shall be submitted with the shop drawings. Shop drawings shall show breakaway base model number, and manufacturer's name and logo. Manufacturer's shop drawings shall include the ASTM designations for all materials to be used.
 - Mast Arm Attachments. All poles and attachments shall be structurally designed to support two 12-foot mast arms and luminaires. Poles shall be supplied with mast arm combinations as shown in the plans. All mast arms shall be designed for a 60-pound luminaire having an effective projected area of 1.6 square feet.
 - Anchor Bolt Assembly. Anchor bolt assemblies for optionally designed poles shall be the same as those shown herein.
- Aluminum Pole Designs. Aluminum pole designs may be allowed, if aluminum poles are permitted or required, pending approval by the Department as outlined below.
 - Meet all of the requirements stated above for optional steel pole designs and the following:
 - Aluminum poles shall be fabricated in accordance with "Structural Welding Code-Aluminum" AWS D1.2.
 - Aluminum pole designs shall use the same anchor bolt assembly and be subject to the same geometric restraints and other requirements for steel poles specified herein.
 - Aluminum poles shall be equipped with vibration mitigation devices, as approved by the engineer.
 - Pole components shall be constructed using the following material:

Shaft: ASTM B221 or B241 Alloy 6063-T6, ASTM B209 Alloy 5086-H34, ASTM B221 Alloy 6005-T5.
 Base Flange: ASTM B26 Alloy 356.0-T6 or ASTM B108 Alloy 356.0-T6 (Yield strength test required).
 Mast Arm Fitting: ASTM B209 Alloy 6061-T6 or ASTM B221 Alloy 6005-T5.
 Mast Arms: ASTM B241 Alloy 6061-T6 or Alloy 6063-T6.
 Pole Cap: ASTM B209 Alloy 5086-H32 or ASTM B108 or B26 Alloy 356.0-T6.
 Bolts: Stainless Steel AISI 300 series. Bolts threading into aluminum threads shall be treated with anti-seize compound, Never-Seez Compound, Permatex 133K or equal.
- Special Designs. Poles with architectural treatments shall meet the requirements shown elsewhere in the plans.
- Luminaire Mounting Height. Actual luminaire mounting height shall be the nominal mounting height given on RIP(2) for all pole-arm combinations except for poles with 4 ft. luminaire arms, which shall be 3'-0" lower than the nominal height, unless otherwise shown or directed.

EXPLANATION OF ROADWAY ILLUMINATION ASSEMBLY DESIGNATIONS



ROADWAY ILLUMINATION POLES

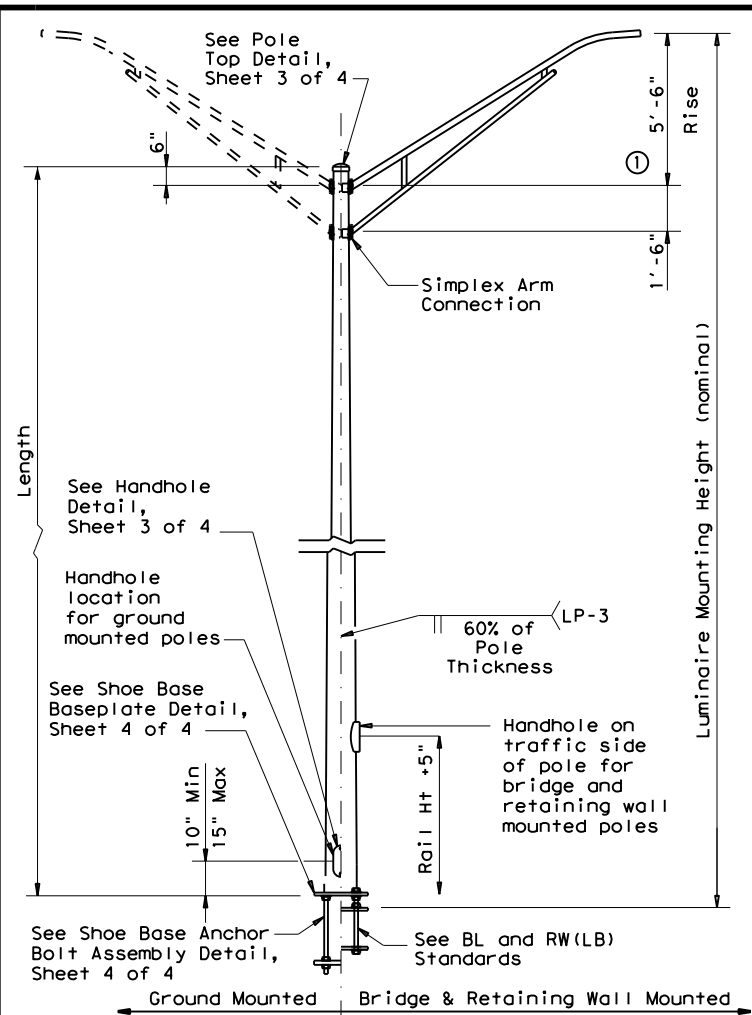
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7-17 12-19		COUNTY: COLEMAN CO.		SHEET NO.: 99	

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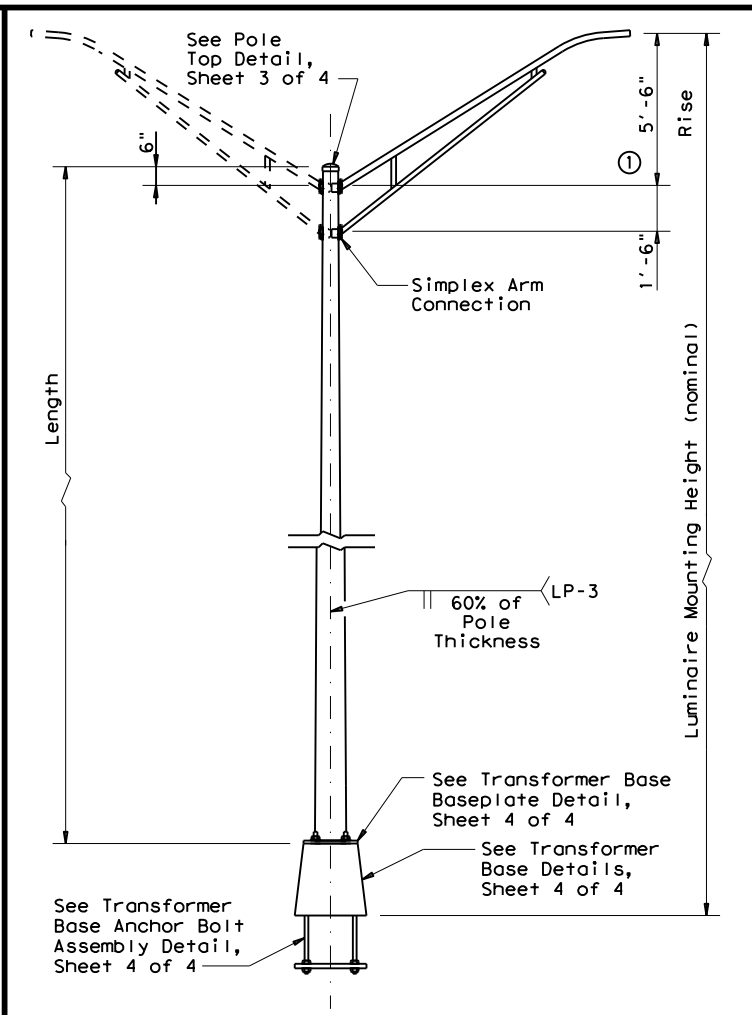
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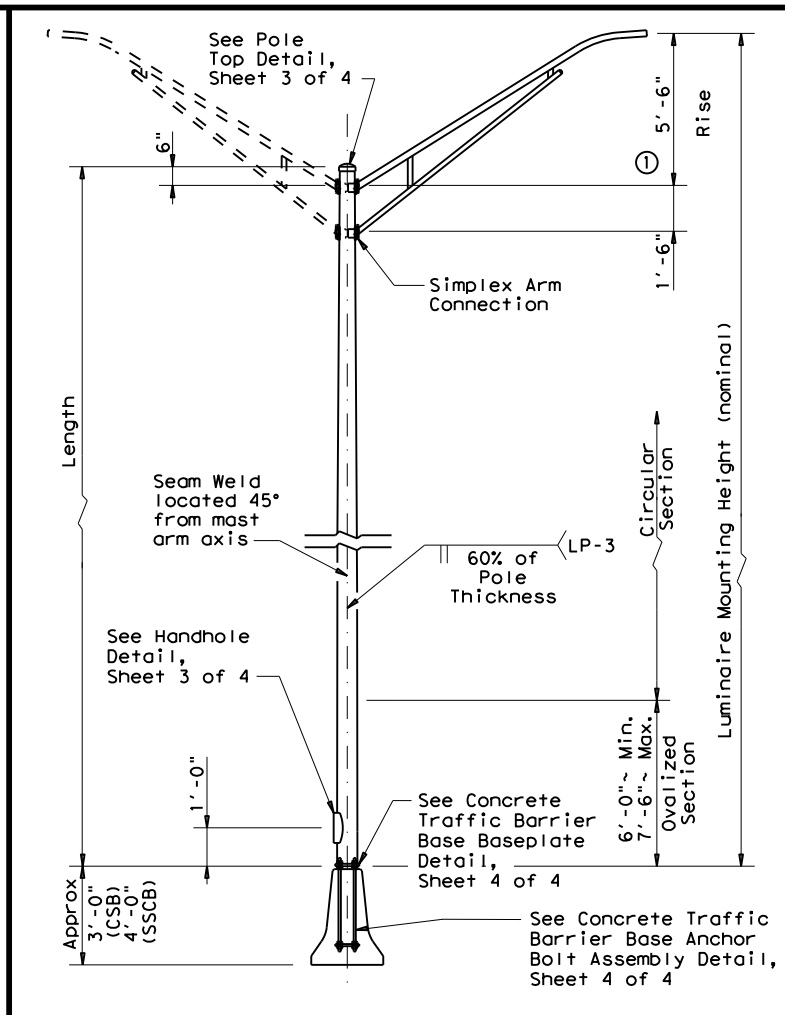
SHOE BASE POLE

SHOE BASE POLE					
Luminaire Mounting Height (Nominal) (ft)	Base Diameter (in)	Top Diameter (in)	Length (ft)	Pole Thickness (in)	Design Moment (K-ft)
20.00	7.00	4.90	15.00	0.1196	7.1
30.00	7.50	4.00	25.00	0.1196	13.2
31.00-39.00	8.00	4.36-3.24	26.00-34.00	0.1196	20.7
40.00	8.50	3.60	35.00	0.1196	20.7
50.00	10.50	4.20	45.00	0.1196	30.3



TRANSFORMER BASE POLE

TRANSFORMER BASE POLE					
Luminaire Mounting Height (Nominal) (ft)	Base Diameter (in)	Top Diameter (in)	Length (ft)	Pole Thickness (in)	Design Moment (K-ft)
20.00	7.00	5.11	13.50	0.1196	7.1
30.00	7.50	4.21	23.50	0.1196	13.2
31.00-39.00	8.00	4.57-3.45	24.50-32.50	0.1196	20.7
40.00	8.50	3.81	33.50	0.1196	20.7
50.00	10.00	3.91	43.50	0.1196	30.3



CONCRETE TRAFFIC BARRIER BASE POLE

CONCRETE TRAFFIC BARRIER BASE POLE (CSB/SSCB)						
Luminaire Mounting Height (Nominal) (ft)	Base Diameter (in)	Top Diameter (in)	Length (ft)	Pole Thickness (in)	Design Moment (K-ft)	
					About C of Rail	Perp. to Rail
28.00	9.00	5.78	23.00	0.1196	10.3	13.2
38.00	9.00	4.38	33.00	0.1196	16.6	20.8
48.00	10.50	4.48	43.00	0.1345	25.1	30.5

GENERAL NOTES:

- Designs conform to AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminares, and Traffic Signals, 6th Edition (2013) and Interim Revisions thereto. Design 3-Second Gust Wind Speed equals 110 mph with a 1.14 gust factor. A wind importance factor of 0.80 is applied to adjust the wind speed to a 25 year recurrence interval. Design moments listed in tables assume base of pole is 25' above natural ground level.
- Structures are designed to support two 12' luminaire mast arms and luminaires. Mast arms are designed to support a 60-pound luminaire having an effective projected area of 1.6 square feet.
- Fabrication shall be in accordance with the Specifications and with the details, dimensions, and weld procedures shown herein. Do not submit shop drawings for roadway illumination pole assemblies fabricated in accordance with the details, dimensions, and weld procedures shown herein. Weld references call for preapproved weld procedures which the Fabricator must obtain prior to fabrication. Materials, fabrication tolerances, and shipping practices shall meet the requirements of these sheets and the Specifications. In the absence of specified fabrication tolerances, dimensions shall be within the tolerances generally obtainable in normal fabrication practice.
- For mounting heights between values shown in the tables, use base diameter and thickness values for the larger height.
- Unless otherwise noted, all steel parts shall be galvanized in accordance with Item 445, "Galvanizing."
- Steel poles shall be fabricated in accordance with Item 441, "Steel Structures." Longitudinal seam welds for pole sections shall have 60% minimum penetration. All welding shall be in accordance with AWS D1.1, Structural Welding Code-Steel.
- Two-section poles joined by circumferential welds will not be permitted, unless otherwise shown on the plans. Poles may be fabricated in two sections and field-assembled by the lap-joint method. The two sections shall telescope together with a lap length of not less than 1-1/2 times the shaft diameter at the lap joint.
- Alternate material equal to or better than material specified may be substituted with the approval of the Engineer.
- Lubricate and tighten anchor bolts, when erecting shoe base poles and concrete traffic barrier base poles, in accordance with Item 449, "Anchor Bolts."
- All poles, except Transformer Base Poles, shall have hand holes with reinforcing frames and covers. For ground mounted shoe base poles, hand holes shall be placed 90 degrees to mast arm unless otherwise noted on the plans. For poles mounted on a concrete traffic barrier with one luminaire arm, hand holes shall be located 180 degrees from luminaire arm. For poles mounted on a concrete traffic barrier with two luminaire arms, all hand holes shall be on the same side of the barrier. For poles mounted on a bridge lighting bracket or a retaining wall lighting bracket, hand hole shall be on traffic side of the pole, at a height that will clear the barrier.
- The finished pole shall have a smooth, uniform finish free of pits, blisters, or other defects. Scratched, chipped, and other damaged galvanized areas on poles and mast arms shall be repaired in accordance with Item 445, "Galvanizing."
- Pole length is based on a 5'-6" luminaire arm rise. 4 ft. luminaire arms have a 2'-6" rise. A pole with 4 ft. luminaire arms will have an actual mounting height 3'-0" less than the nominal mounting height. Increasing the pole length to meet the nominal mounting height is allowed, but unnecessary unless otherwise directed by the engineer.
- Erect transformer base poles in accordance with sheet RID(1).

MATERIAL DATA

COMPONENT	ASTM DESIGNATION	MIN. YIELD (ksi)
Pole Shaft (0.14"/ft. Taper)	A572 Gr 50, A595 Gr A, A1011 HSLAS Gr 50 Cl 2 ③, or A1008 HSLAS Gr 50 Cl 2	50
Base Plate and Handhole Frame	A572 Gr.50, or A36	36
T-Base Connecting Bolts	F3125 Gr A325	92
Anchor Bolts	F1554 Gr 55, A193-B7 or A321	55 105
Anchor Bolt Templates	A36	36
Heavy Hex (H.H.) Nuts	A194 Gr 2H, or A563 Gr DH	
Flat Washers	F436	

NOTES:

- 2'-6" rise for 4 ft. luminaire arms.
- Before ovalized as shown on Concrete Traffic Barrier Base Baseplate details, Sheet 4 of 4.
- A1011 SS Gr 50 may be used instead of HSLAS, provided the material meets the elongation requirements for HSLAS.

POLE ASSEMBLY FABRICATION TOLERANCES TABLE

DIMENSION	TOLERANCE
Shaft length	+1"
I.D. of outside piece of slip fitting pieces	+1/8", -1/16"
O.D. of inside piece of slip fitting pieces	+1/32", -1/8"
Shaft diameter: other	+3/16"
Out of "round"	1/4"
Straightness of shaft	±1/4" in 10 ft
Twist in multi-sided shaft	4° in 50 ft
Perpendicular to baseplate	1/8" in 24"
Pole centered on baseplate	±1/4"
Location of Attachments	±1/4"
Bolt hole spacing	±1/16"

Texas Department of Transportation
Traffic Safety Division Standard

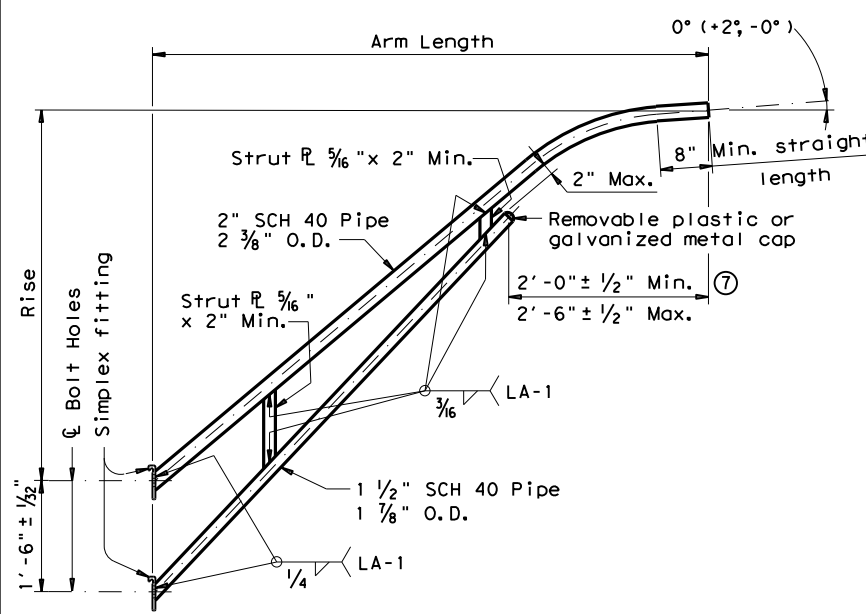
ROADWAY ILLUMINATION POLES

RIP(2) - 19

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© TxDOT January 2007	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
7-17	DIST	COUNTY	SHEET NO.	
12-19	BWD	COLEMAN CO.	100	

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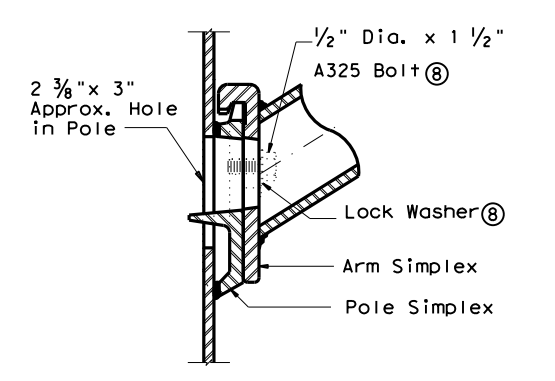
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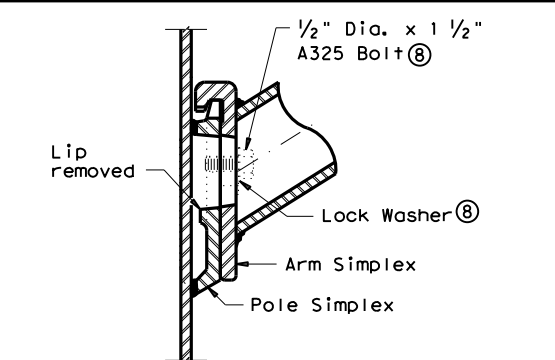
LUMINAIRE ARM

LUMINAIRE ARM DIMENSIONS		
Nominal Arm Length	Arm Length	Rise
4'-0"	3'-6"	2'-6"
6'-0"	5'-6"	5'-6"
8'-0"	7'-6"	5'-6"
10'-0"	9'-6"	5'-6"
12'-0"	11'-6"	5'-6"

ARM ASSEMBLY FABRICATION TOLERANCES TABLE	
DIMENSION	TOLERANCE
Arm Length	±1"
Arm Rise	±1"
Deviation from flat	1/8" in 12"
Spacing between holes	±1/32"

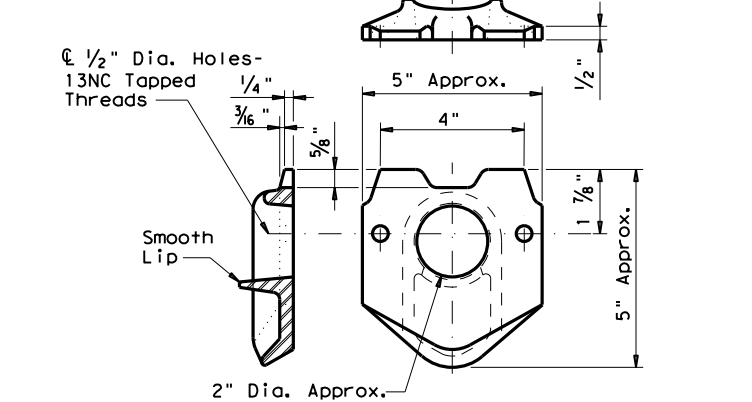


UPPER SIMPLEX FITTING
(Gusset not shown for clarity)

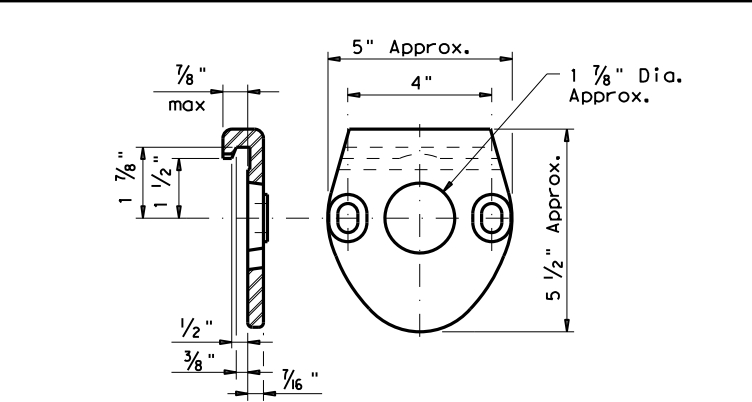


LOWER SIMPLEX FITTING
(Gusset not shown for clarity)

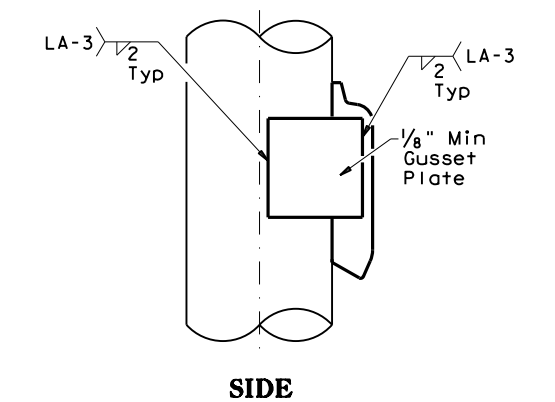
SECTION B-B



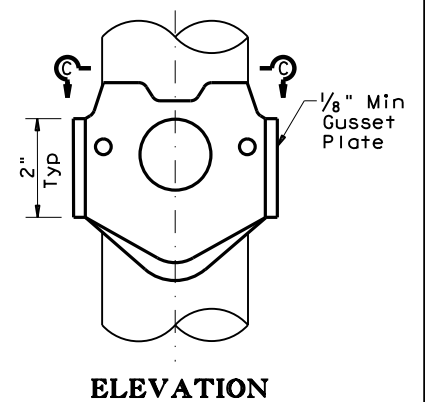
POLE SIMPLEX DETAIL



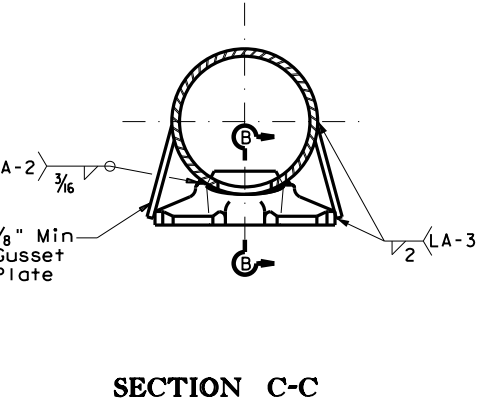
ARM SIMPLEX DETAIL



SIDE

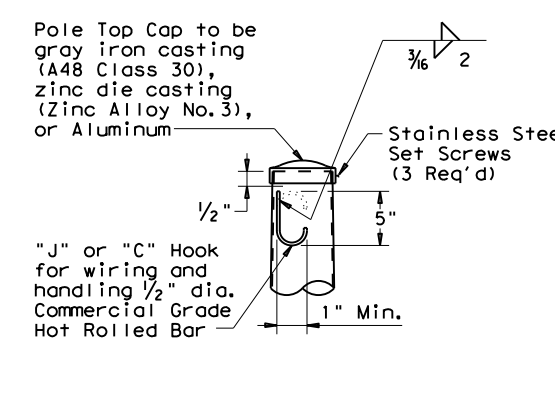


ELEVATION

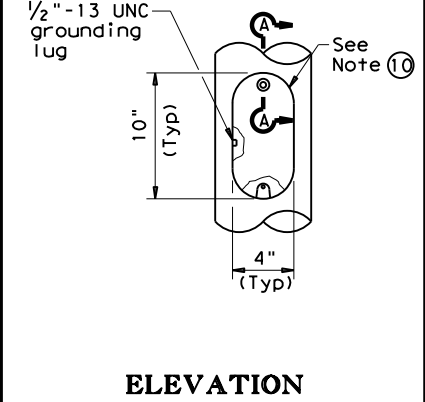


SECTION C-C

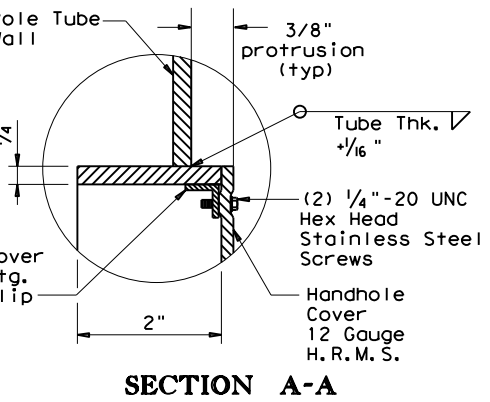
SIMPLEX ATTACHMENT DETAIL



POLE TOP



ELEVATION



SECTION A-A

HANDHOLE

NOTES:

- ④ Any of the materials listed for plates may be used where the drawings do not specify a particular ASTM designation.
- ⑤ A576 must be suitable for forging and also meet minimum tensile strength of 65 ksi, minimum yield of 35 ksi, and elongation in 2 inches of 22 percent.
- ⑥ A572, A1008 HSLAS-F, and A1011 HSLAS-F materials may have higher yield strengths but shall not have less elongation than the grade indicated.
- ⑦ Dimensional limits are given to show acceptable variation in design. All of a Fabricator's production of a particular arm length shall have the same dimensions within specified tolerances.
- ⑧ Each pole simplex fitting shall be supplied with 2 bolts and 2 lock washers of the size specified. The bolts and lock washers shall be secured to the pole with the other hardware items called for in the plans.
- ⑨ Proposed deviations in arm simplex dimensions or materials must be submitted to the Department for approval.
- ⑩ A welded handhole frame is permissible. Maximum of two (2) CJP weld splices is allowed.

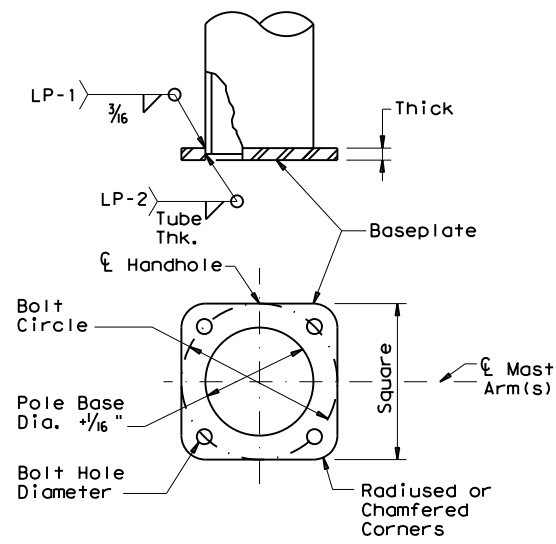
MATERIALS

Pole or Arm Simplex	ASTM A27 Gr 65-35 or Gr 70-36, A148 Gr 80-50, A576 Gr 1021 ⑤, or A36 (Arm only)
Arm Pipes	ASTM A53 Gr A or B, A500 Gr B, A501, A 1008 HSLAS-F Gr 50 ⑥, or A1011 HSLAS-F Gr 50 ⑥
Arm Struts and Gusset Plates ④	ASTM A36, A572 Gr 50 ⑥, or A588
Misc.	ASTM designations as noted

ROADWAY ILLUMINATION POLES
RIP(3)-19

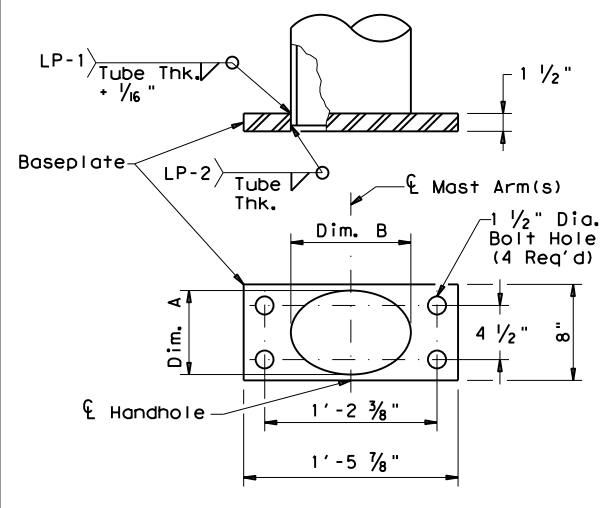
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©TxDOT January 2007	CONT	SECT	JOB	HIGHWAY
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7-17	DIST	COUNTY	SHEET NO.	
12-19	BWD	COLEMAN CO.	101	

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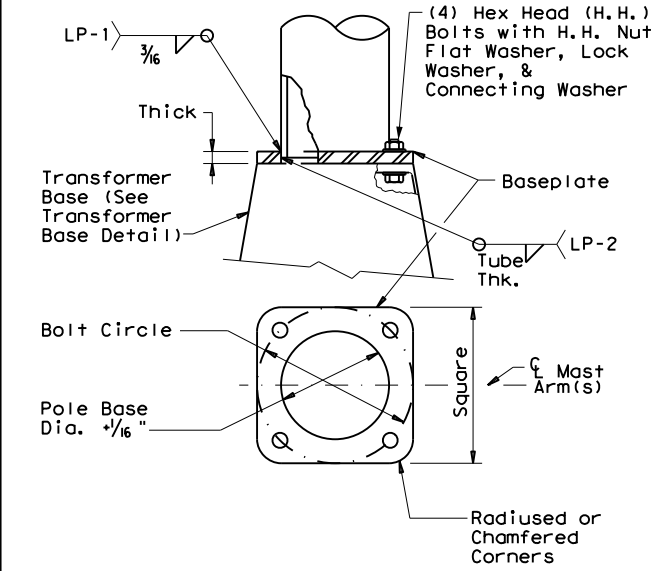
SHOE BASE BASEPLATE

SHOE BASE BASEPLATE TABLE				
MOUNTING HEIGHTS (nominal)	BOLT CIRCLE	SQUARE	THICK	BOLT HOLE DIAMETER
20' - 39'	13"	13"	1 1/4"	1 1/4"
40'	15"	15"	1 1/4"	1 1/2"
50'	15"	15"	1 1/2"	1 1/2"



CONCRETE TRAFFIC BARRIER BASE BASEPLATE

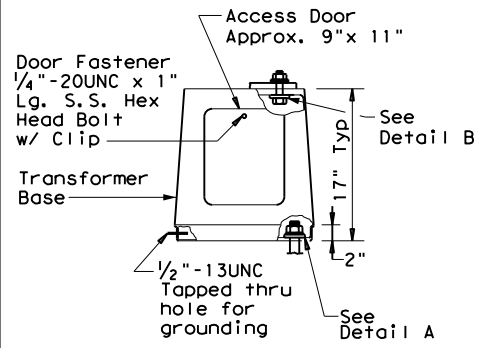
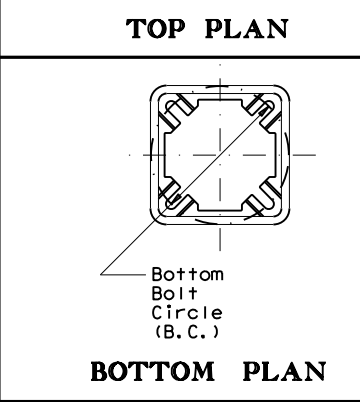
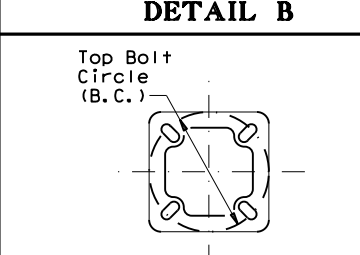
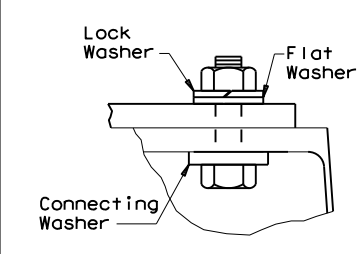
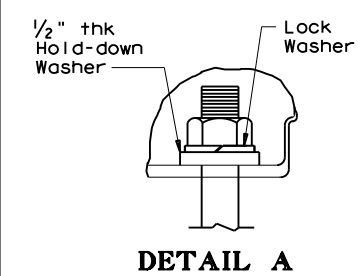
CONCRETE TRAFFIC BARRIER BASE BASEPLATE TABLE			
MOUNTING HEIGHTS (nominal)	POLE DIA. (12)	DIM. A	DIM. B
28' - 38'	9"	7" ± 1/4"	10" ± 1/4"
48'	10 1/2"	7" ± 1/4"	13" ± 1/4"



TRANSFORMER BASE BASEPLATE

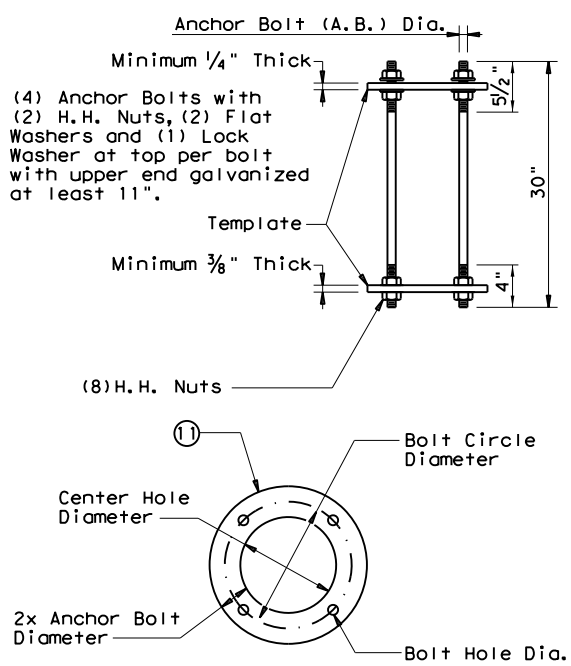
TRANSFORMER BASE BASEPLATE TABLE						
MOUNTING HEIGHTS (nominal)	BOLT CIRCLE	SQUARE	THICK	CONNECTING BOLT DIA.	BOLT HOLE DIAMETER	TRANSFORMER BASE TYPE
20' - 39'	13"	13"	1 1/4"	1"	1 1/4"	A
40'	15"	15"	1 1/4"	1 1/4"	1 1/2"	B
50'	15"	15"	1 1/2"	1 1/4"	1 1/2"	B

TRANSFORMER BASE TABLE		
TYPE	TOP B.C.	BTM. B.C.
A	13"	14"
B	15"	17 1/4"



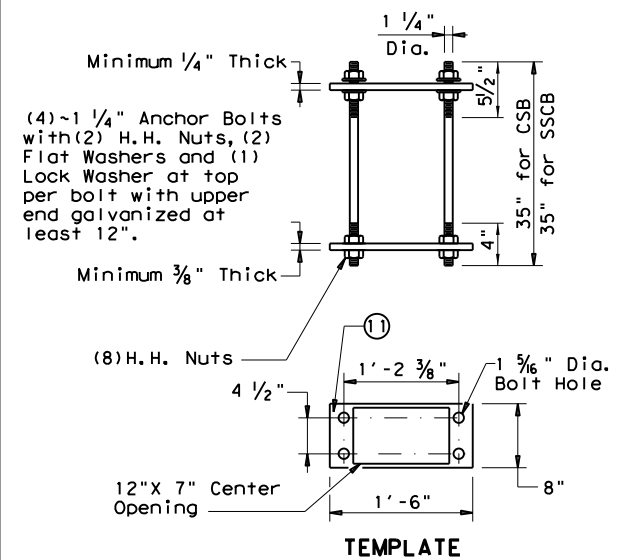
TRANSFORMER BASE DETAILS

- GENERAL NOTES:**
- For mounting heights between those shown in the table, use the values in the table for the larger mounting height.
 - All breakaway bases shall meet the breakaway requirements of the AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals, 6th Edition (2013) and Interim Revisions thereto, and shall have been tested by FHWA-approved methods. All bases shall have been structurally tested to resist 150% of the design moment.
 - Transformer bases shall be cast from aluminum, ASTM B108 or B26 Alloy 356.0-T6, or other material approved by the Engineer. Four Hex Head (H.H.) bolts with four H.H. nuts, four lock washers, four flat washers, and connecting and hold-down washers as recommended by the manufacturer, galvanized to ASTM A153 Class C or D, or B695 Class 50, shall be provided with each transformer base for connecting the pole. Bolts shall be ASTM A325 or approved equal. Nuts shall be ASTM A563 grade DH galvanized.
 - Bases shall be stamped, incised or by other approved permanent means, marked to show fabricator's name or logo, and model number. Such information shall be placed in a readily seen location, inside or outside the base, but shall not be placed on the door.
 - Doors for transformer bases shall be made of plastic, fiberglass or other non-metallic material approved by the Engineer and shall be attached with stainless steel screws or bolts. Transformer bases shall be cleaned by grit blast cleaning after heat treatment. Certification by the manufacturer of heat treatment shall be furnished with transformer bases. The certification shall show the metal alloy and temper and that the base meets those requirements, chemical and physical. The certification shall also show the material ASTM specification. Transformer bases shall be cast with a removable tab bar for material testing. Some bars may have been removed by the manufacturer for testing.



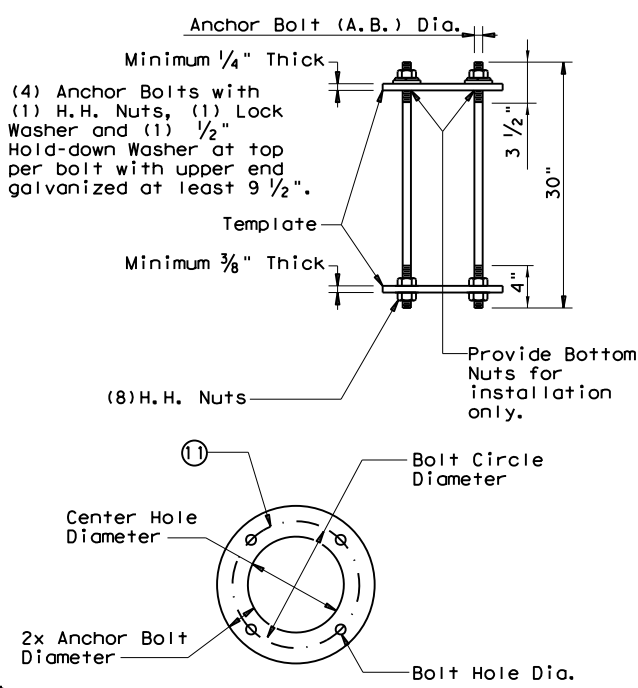
SHOE BASE ANCHOR BOLT ASSEMBLY

SHOE BASE ANCHOR BOLT ASSEMBLY TABLE				
MOUNTING HEIGHTS (nominal)	A.B. Dia.	BOLT CIRCLE DIAMETER	CTR. HOLE DIAMETER	BOLT HOLE DIAMETER
20' - 39'	1"	13"	11"	1 1/16"
40' - 50'	1 1/4"	15"	12 1/2"	1 5/16"



CONCRETE TRAFFIC BARRIER BASE ANCHOR BOLT ASSEMBLY

CONCRETE TRAFFIC BARRIER BASE ANCHOR BOLT ASSEMBLY TABLE				
MOUNTING HEIGHTS (nominal)	A.B. Dia.	BOLT CIRCLE DIAMETER	CTR. HOLE DIAMETER	BOLT HOLE DIAMETER
20' - 39'	1"	14"	12"	1 1/16"
40' - 50'	1 1/4"	17 1/4"	14 3/4"	1 5/16"



TRANSFORMER BASE ANCHOR BOLT ASSEMBLY

NOTES:

- Anchor Bolt Templates do not need to be galvanized.
- Pole diameter before ovalized.

ANCHOR BOLT FABRICATION TOLERANCES TABLE	
DIMENSION	TOLERANCE
Length	± 1/2"
Threaded length	± 1/2"
Galvanized length (if required)	- 1/4"

SHEET 4 OF 4

Texas Department of Transportation
 Traffic Safety Division Standard

ROADWAY ILLUMINATION POLES

RIP(4) - 19

FILE: rip-19.dgn	DN:	CK:	DW:	CK:
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REVISIONS	0054	04	123	US 84
7-17	DIST	COUNTY	SHEET NO.	
12-19	BWD	COLEMAN CO.	102	

SUMMARY OF SMALL SIGNS

PROP. STATION	SIGN NOMENCLATURE	REMOV SM RD SN SUP & AM	SIGN	DIMENSIONS IN INCHES	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X-XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION	
							FRP = Fiberglass TWT = Thin-Wall 10BWG = 10 BWG S80 = Sch 80	1 or 2	UA=Universal Conc UB=Universal Bolt SA=Slipbase-Conc SB=Slipbase-Bolt WS=Wedge Steel WP=Wedge Plastic	PREFABRICATED P = "Plain" T = "T" U = "U" 1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel EXAL= Extruded Alum Sign Panels	
289+61 LT	R4-2aT	X	LEFT LANE FOR PASSING ONLY	24X36	X		10BWG	1	SA	T	
293+00 LT	M3-4	X	WEST	24X12	X		10BWG	1	SA	P	
	M1-4		84	24X24	X						
	M3-1		NORTH	24X12	X						
	M1-4		283	30X24	X						
294+22 RT	M1-6F	X	FM 568	24X24	X		10BWG	1	SA	P	
	M6-4		←→	21X15	X						
294+30 RT	M1-4	X	84	24X24	X		10BWG	1	SA	P	
	M1-4		283	30X24	X						
	M6-4		←→	21X15	X						
295+14 LT	R1-1	X	STOP	48X48	X		10BWG	1	SA	T	
295+63 LT	W10-1	X	RAILROAD CROSSING	36X36	X		10BWG	1	SA	T	
295+91 LT	M1-4	X	84	24X24	X		10BWG	1	SA	P	
	M1-4		283	30X24	X						
	M6-4		←→	21X15	X						
296+13 LT	M1-6F	X	FM 568	24X24	X		10BWG	1	SA	P	
	M6-4		←→	21X15	X						
297+77 RT	M3-2	X	EAST	24X12	X		10BWG	1	SA	P	
	M1-4		84	24X24	X						
	M3-3		SOUTH	24X12	X						
	M1-4		283	30X24	X						
301+00 RT	R2-1	X	SPEED LIMIT 75	30X36	X		10BWG	1	SA	P	
302+20 LT	D1-1	X	← BALLINGER	VAR.X18	X		10BWG	1	SA	T	
304+56 RT	D2-2	X	SANTA ANNA 7 BROWNWOOD 27	VAR.X30	X		10BWG	1	SA	T	
308+85 RT	R4-2aT	X	LEFT LANE FOR PASSING ONLY	24X36	X		10BWG	1	SA	T	
312+03 LT	M2-1	X	JCT	21X15	X		10BWG	1	SA	P	
	M1-6F		FM 568	24X24	X						
340+66 RT	D20-5T	X	CO RD 196	24X42	X		10BWG	1	SA	P	
			→								
			110								
			→								
342+70 RT	W8-13aT	X	BRIDGE MAY ICE IN COLD WEATHER	36X36	X		10BWG	1	SA	T	
347+47 RT	R1-1	X	STOP	48X48	X		10BWG	1	SA	T	

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website:
<http://www.txdot.gov/>

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).



SUMMARY OF SMALL SIGNS

SOSS

FILE: sums16.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
©TxDOT May 1987	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
4-16	DIST	COUNTY	SHEET NO.	
8-16	BWD	COLEMAN CO.	103	

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SUMMARY OF SMALL SIGNS

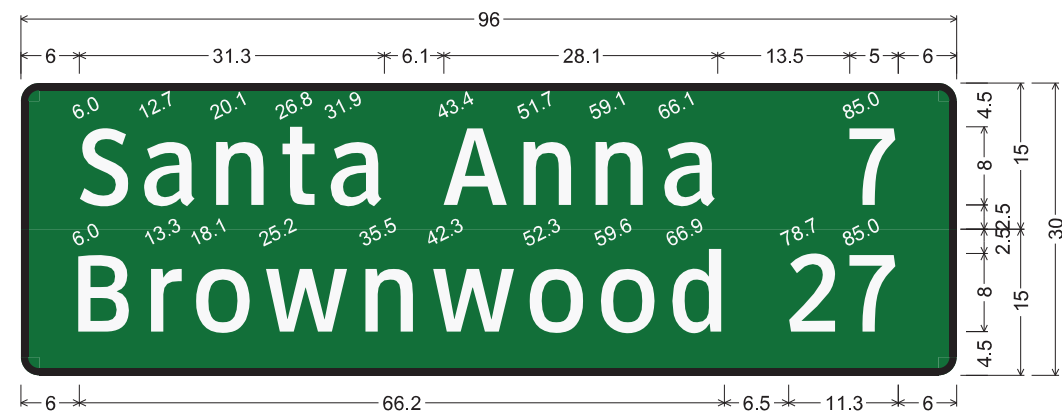
PROP. STATION	SIGN NOMENCLATURE	REMOV SM RD SN SUP & AM	SIGN	DIMENSIONS IN INCHES	FLAT ALUMINUM (TYPE A)	EXAL ALUMINUM (TYPE G)	SM RD SGN ASSM TY XXXXX (X) XX (X- XXXX)				BRIDGE MOUNT CLEARANCE SIGNS (See Note 2)	
							POST TYPE	POSTS	ANCHOR TYPE	MOUNTING DESIGNATION		
							FRP = Fiberglass TWT = Thin-Wall 10BWG = 10 BWG S80 = Sch 80	1 or 2	UA=Universal Conc UB=Universal Bolt SA=Slipbase-Conc SB=Slipbase-Bolt WS=Wedge Steel WP=Wedge Plastic	PREFABRICATED P = "Plain" T = "T" U = "U" 1EXT or 2EXT = # of Ext BM = Extruded Wind Beam WC = 1.12 #/ft Wing Channel EXAL= Extruded Alum Sign Panels		
347+52 LT		X	ADOPT A HIGHWAY NEXT 2 MILES FIRST UNTIED METHODIST YOUTH GROUP									
348+38 RT	M1-4 M1-4	X	84 283	24X24 30X24	X X		10BWG	1	SA	P		
350+50 LT	D20-5T	X	CO RD 196 ← 110 ←	24X42	X		10BWG	1	SA	P		

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080"
7.5 to 15	0.100"
Greater than 15	0.125"

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website.
<http://www.txdot.gov/>

ITEM	DESCRIPTION	QUANT.	UNIT
0636 6001	ALUMINUM SIGNS (TY A)	31.0	SF
0644 6001	IN SM RD SN SUP&AM TY10BWG(1)SA(P)	11.0	EA
0644 6004	IN SM RD SN SUP&AM TY10BWG(1)SA(T)	8.0	EA
0644 6076	REMOV SM RD SN SUP & AM	20.0	EA

- NOTE:**
- Sign supports shall be located as shown on the plans, except that the Engineer may shift the sign supports, within design guidelines, where necessary to secure a more desirable location or to avoid conflict with utilities. Unless otherwise shown on the plans, the Contractor shall stake and the Engineer will verify all sign support locations.
 - For installation of bridge mount clearance signs, see Bridge Mounted Clearance Sign Assembly (BMCS) Standard Sheet.
 - For Sign Support Descriptive Codes, see Sign Mounting Details Small Roadside Signs General Notes & Details SMD(GEN).



D2-2 8in;
1.9" Radius, 0.8" Border, White on Green;
"Santa Anna", ClearviewHwy-3-W; "7", ClearviewHwy-3-W;
1.9" Radius, 0.8" Border, White on Green;
"Brownwood", ClearviewHwy-3-W; "27", ClearviewHwy-3-W;



D1-1 8in LT;
1.5" Radius, 0.5" Border, White on Green;
Standard Arrow Custom 12.0" X 7.1" 180°; "Ballinger", ClearviewHwy-3-W;



01/26/2023

Texas Department of Transportation				Traffic Operations Division Standard	
SUMMARY OF SMALL SIGNS					
SOSS					
FILE: sums16.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT	
©TxDOT May 1987	CONT: 0054	SECT: 04	JOB: 123	HIGHWAY: US 84	
4-16	DIST: BWD	COUNTY: COLEMAN CO.	SHEET NO.:	104	

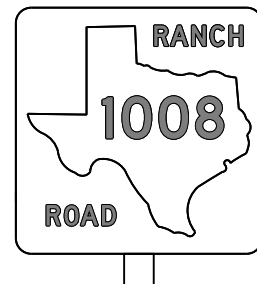
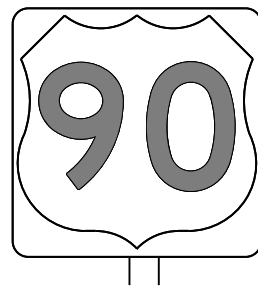
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DATE: 1/11/2023 9:12:27 AM
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REQUIREMENTS FOR INDEPENDENT MOUNTED ROUTE SIGNS

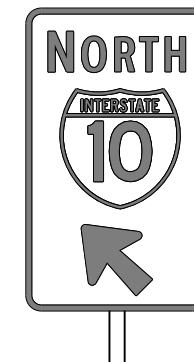
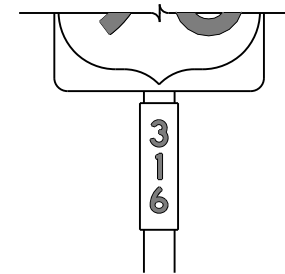
SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	WHITE	TYPE A SHEETING
BACKGROUND	ALL OTHERS	TYPE B OR C SHEETING
LEGEND & BORDERS	WHITE	TYPE A SHEETING
LEGEND & BORDERS	BLACK	ACRYLIC NON-REFLECTIVE FILM
LEGEND & BORDERS	ALL OTHERS	TYPE B or C SHEETING



TYPICAL EXAMPLES

REQUIREMENTS FOR BLUE, BROWN & GREEN D AND I SERIES GUIDE SIGNS

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	ALL	TYPE B OR C SHEETING
LEGEND & BORDERS	WHITE	TYPE D SHEETING
LEGEND, SYMBOLS & BORDERS	ALL OTHERS	TYPE B OR C SHEETING



TYPICAL EXAMPLES

GENERAL NOTES

- Signs to be furnished shall be as detailed elsewhere in the plans and/or as shown on sign tabulation sheet. Standard sign designs and arrow dimensions can be found in the "Standard Highway Sign Designs for Texas" (SHSD).
- White legend shall use the Clearview Alphabet. The following Clearview fonts shall be used to replace the existing white Federal Highway Administration (FHWA) Standard Highway Alphabets, when not specified in the SHSD, or in the plans.

B	CV-1W
C	CV-2W
D	CV-3W
E	CV-4W
Emod	CV-5WR
F	CV-6W

- Route sign legend (ie. IH, US, SH and FM shields) shall use the Federal Highway Administration (FHWA) Standard Highway Alphabets B, C, D, E, Emod or F).
- Lateral spacing between letters and numerals shall conform with the SHSD, and any approved changes thereto. Lateral spacing of legend shall provide a balanced appearance when spacing is not shown.
- Independent mounted route sign with white or colored legend and borders shall be applied by screening process with transparent color ink, transparent colored overlay film to white background sheeting or cut-out white sheeting to colored background sheeting, or combination thereof. White legend, symbols and borders on all other signs shall be cut-out white sheeting applied to colored background sheeting.
- Information regarding borders and radii for signs is found in the "Standard Highway Sign Designs for Texas". Dimensions shown and described for borders and corner radii on parent sign are nominal. Borders may vary in width as much as 1/2 inch. Corner radii above 3 inches may vary in width as much as 1 inch. Borders and corner radii within a parent sign must be of matching widths. The sign area outside the corner radius should be trimmed or rounded.
- Sign substrate shall be any material that meets the Departmental Material Specification requirements of DMS-7110 or approved alternative.
- Mounting details of roadside signs are shown in the "SMD series" Standard Plan Sheets.

DEPARTMENTAL MATERIAL SPECIFICATIONS	
ALUMINUM SIGN BLANKS	DMS-7110
SIGN FACE MATERIALS	DMS-8300

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080
7.5 to 15	0.100
Greater than 15	0.125

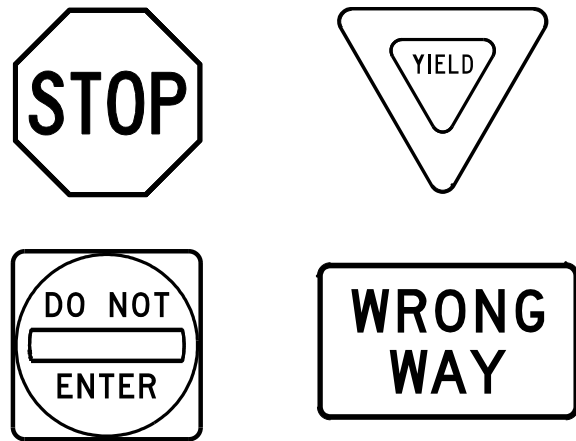
The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website:
<http://www.txdot.gov/>

		Traffic Operations Division Standard	
<h3>TYPICAL SIGN REQUIREMENTS</h3> <h4>TSR(3) - 13</h4>			
FILE:	tsr3-13.dgn	DN:	TxDOT
©TxDOT	October 2003	CK:	TxDOT
REVISIONS		DW:	TxDOT
		CON:	SECT
		0054	04
		JOB	123
		HIGHWAY	US 84
12-03	7-13	DIST	COUNTY
9-08		BWD	COLEMAN CO.
		SHEET NO.	105

DATE: 1/11/2023 9:13:02 AM
 FILE: T:\BWDSSGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to cfr the use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to digital format.

REQUIREMENTS FOR RED BACKGROUND REGULATORY SIGNS

(STOP, YIELD, DO NOT ENTER AND WRONG WAY SIGNS)



REQUIREMENTS FOR FOUR SPECIFIC SIGNS ONLY

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	WHITE	TYPE B OR C SHEETING
LEGEND & BORDERS	WHITE	TYPE B OR C SHEETING
LEGEND	RED	TYPE B OR C SHEETING

REQUIREMENTS FOR WHITE BACKGROUND REGULATORY SIGNS

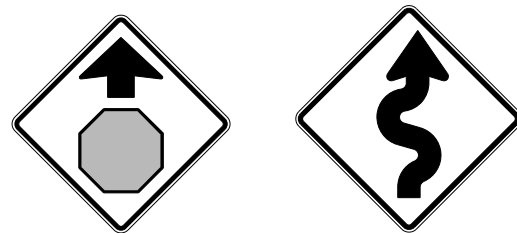
(EXCLUDING STOP, YIELD, DO NOT ENTER AND WRONG WAY SIGNS)



TYPICAL EXAMPLES

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	WHITE	TYPE A SHEETING
BACKGROUND	ALL OTHERS	TYPE B OR C SHEETING
LEGEND, BORDERS AND SYMBOLS	BLACK	ACRYLIC NON-REFLECTIVE FILM
LEGEND, BORDERS AND SYMBOLS	ALL OTHER	TYPE B OR C SHEETING

REQUIREMENTS FOR WARNING SIGNS



TYPICAL EXAMPLES

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	FLOURESCENT YELLOW	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND & BORDERS	BLACK	ACRYLIC NON-REFLECTIVE FILM
LEGEND & SYMBOLS	ALL OTHER	TYPE B OR C SHEETING

REQUIREMENTS FOR SCHOOL SIGNS



TYPICAL EXAMPLES

SHEETING REQUIREMENTS		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	WHITE	TYPE A SHEETING
BACKGROUND	FLOURESCENT YELLOW GREEN	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND, BORDERS AND SYMBOLS	BLACK	ACRYLIC NON-REFLECTIVE FILM
SYMBOLS	RED	TYPE B OR C SHEETING

GENERAL NOTES

- Signs to be furnished shall be as detailed elsewhere in the plans and/or as shown on sign tabulation sheet. Standard sign designs and arrow dimensions can be found in the "Standard Highway Sign Designs for Texas" (SHSD).
- Sign legend shall use the Federal Highway Administration (FHWA) Standard Highway Alphabets (B, C, D, E, Emod or F).
- Lateral spacing between letters and numerals shall conform with the SHSD, and any approved changes thereto. Lateral spacing of legend shall provide a balanced appearance when spacing is not shown.
- Black legend and borders shall be applied by screening process or cut-out acrylic non-reflective black film to background sheeting, or combination thereof.
- White legend and borders shall be applied by screening process with transparent colored ink, transparent colored overlay film to white background sheeting or cut-out white sheeting to colored background sheeting, or combination thereof.
- Colored legend shall be applied by screening process with transparent colored ink, transparent colored overlay film or colored sheeting to background sheeting, or combination thereof.
- Sign substrate shall be any material that meets the Departmental Material Specification requirements of DMS-7110 or approved alternative.
- Mounting details for roadside mounted signs are shown in the "SMD series" Standard Plan Sheets.

ALUMINUM SIGN BLANKS THICKNESS	
Square Feet	Minimum Thickness
Less than 7.5	0.080
7.5 to 15	0.100
Greater than 15	0.125

DEPARTMENTAL MATERIAL SPECIFICATIONS	
ALUMINUM SIGN BLANKS	DMS-7110
SIGN FACE MATERIALS	DMS-8300

The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website:
<http://www.txdot.gov/>

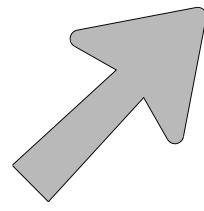
				<i>Traffic Operations Division Standard</i>	
<h2>TYPICAL SIGN REQUIREMENTS</h2>					
<h3>TSR(4) - 13</h3>					
FILE:	tsr4-13.dgn	DN:	TxDOT	CK:	TxDOT
© TxDOT	October 2003	CONT	SECT	JOB	HIGHWAY
REVISIONS		0054	04	123	US 84
12-03	7-13	DIST	COUNTY	SHEET NO.	
9-08		BWD	COLEMAN CO.	106	

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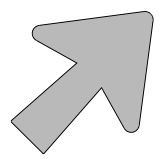
DATE: 1/11/2023 9:13:45 AM
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ARROW DETAILS

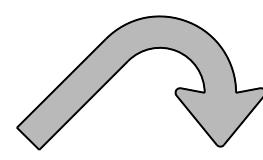
for Large Ground-Mounted and Overhead Guide Signs



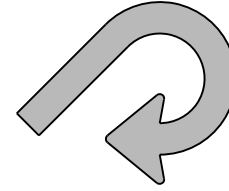
Type A



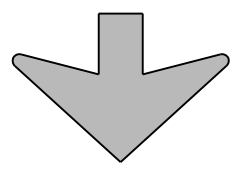
Type B



E-3



E-4



Down Arrow

TYPE	LETTER SIZE	USE
A-1	10.67" U/L and 10" Caps	Single Lane Exits
A-2	13.33" U/L and 12" Caps	
A-3	16" & 20" U/L	
B-1	10.67" U/L and 10" Caps	Multiple Lane Exits
B-2	13.33" U/L and 12" Caps	
B-3	16" & 20" U/L	

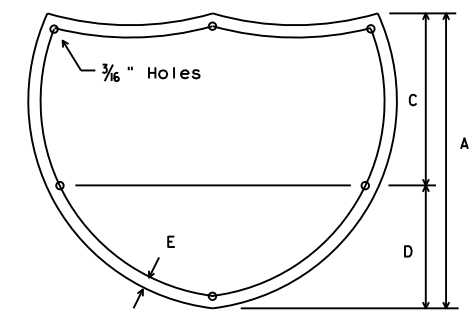
CODE	USED ON SIGN NO.
E-3	E5-1aT
E-4	E5-1bT

NOTE

Arrow dimensions are shown in the "Standard Highway Sign Designs for Texas" manual.

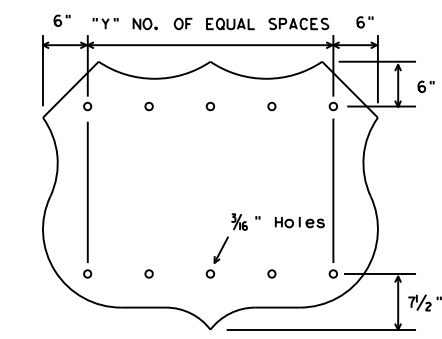
The Standard Highway Sign Designs for Texas (SHSD) can be found at the following website:
<http://www.txdot.gov/>

SIGN BLANK PUNCHING DETAILS FOR ATTACHMENTS WHEN SPECIFIED TO BE TYPE A ALUMINUM SIGNS (FOR MOUNTING TO GUIDE SIGN FACE)



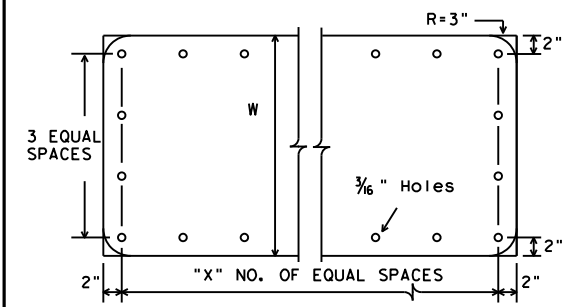
INTERSTATE ROUTE MARKERS

A	C	D	E
36	21	15	1 1/2
48	28	20	1 3/4



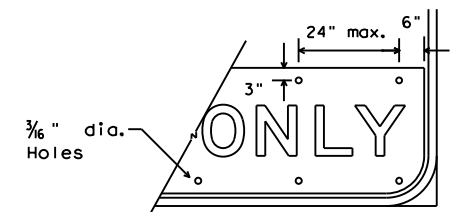
U.S. ROUTE MARKERS

Sign Size	"Y"
24x24	2
30x24	3
36x36	3
45x36	4
48x48	4
60x48	5



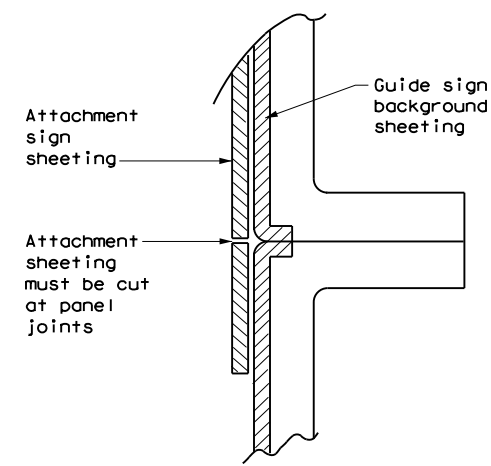
STATE ROUTE MARKERS

No. of Digits	W	X
4	24	4
4	36	5
4	48	6
3	24	3
3	36	4
3	48	5

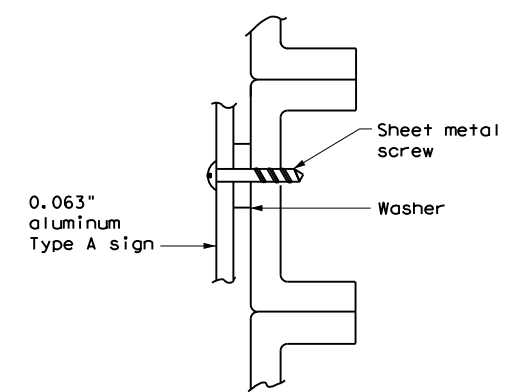


EXIT ONLY PANEL

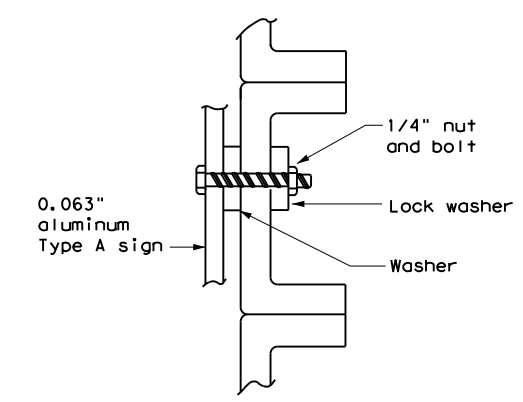
MOUNTING DETAILS OF ATTACHMENTS TO GUIDE SIGN FACE ("EXIT ONLY" AND "LEFT EXIT" PANELS, ROUTE MARKERS AND OTHER ATTACHMENTS)



DIRECT APPLIED ATTACHMENT



SCREW ATTACHMENT

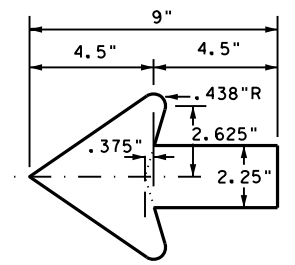


NUT/BOLT ATTACHMENT

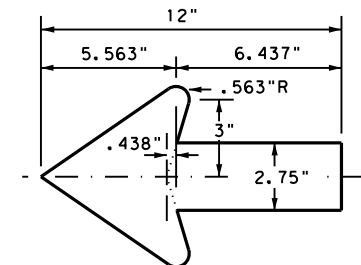
- NOTE:**
- Sheeting for legend, symbols, and borders must be cut at panel joints.
 - Direct applied attachment signs will be subsidiary to "Aluminum Signs" or "Fiberglass Signs".

- NOTE:**
- Furnish Type A aluminum sign attachments only when specified in the plans. These signs will be paid for under "Aluminum Signs".

ARROW DETAILS for Destination Signs (Type D)



Standard arrow to be used with 6 inch letters.



Standard arrow to be used with 8 inch letters.



TYPICAL SIGN REQUIREMENTS

TSR (5) - 13

FILE: tsr5-13.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CR: TxDOT
© TxDOT October 2003	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
12-03 7-13	DIST	COUNTY	SHEET NO.	
9-08	BWD	COLEMAN CO.	107	

DATE: 1/11/2023 9:14:28 AM
 FILE: T:\BWD\SGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\4. Design\Standards\Signing\SMD(GEN)-08.dgn
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SIGN SUPPORT DESCRIPTIVE CODES

(Descriptive Codes correspond to project estimate and quantities sheets)

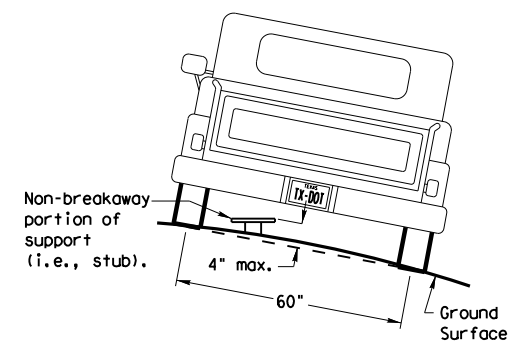
SM RD SGN ASSM TY XXXXX(X)XX(X-XXXX)

Post Type
 FRP = Fiberglass Reinforced Plastic Pipe (see SMD(FRP))
 TWT = Thin-Walled Tubing (see SMD(TWT))
 10BWG = 10 BWG Tubing (see SMD(SLIP-1) to (SLIP-3))
 S80 = Schedule 80 Pipe (see SMD(SLIP-1) to (SLIP-3))

Number of Posts (1 or 2)
Anchor Type
 UA = Universal Anchor - Concreted (see SMD(FRP) and (TWT))
 UB = Universal Anchor - Bolted down (see SMD(FRP) and (TWT))
 WS = Wedge Anchor Steel - (see SMD(TWT))
 WP = Wedge Anchor Plastic (see SMD(TWT))
 SA = Slipbase - Concreted (see SMD(SLIP-1) to (SLIP-3))
 SB = Slipbase - Bolted Down (see SMD(SLIP-1) to (SLIP-3))

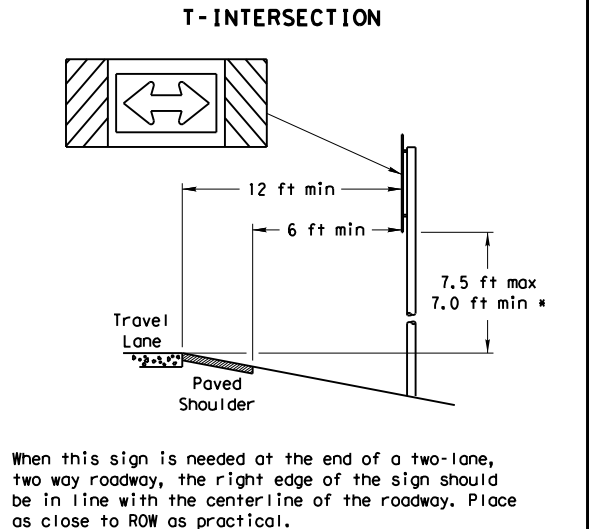
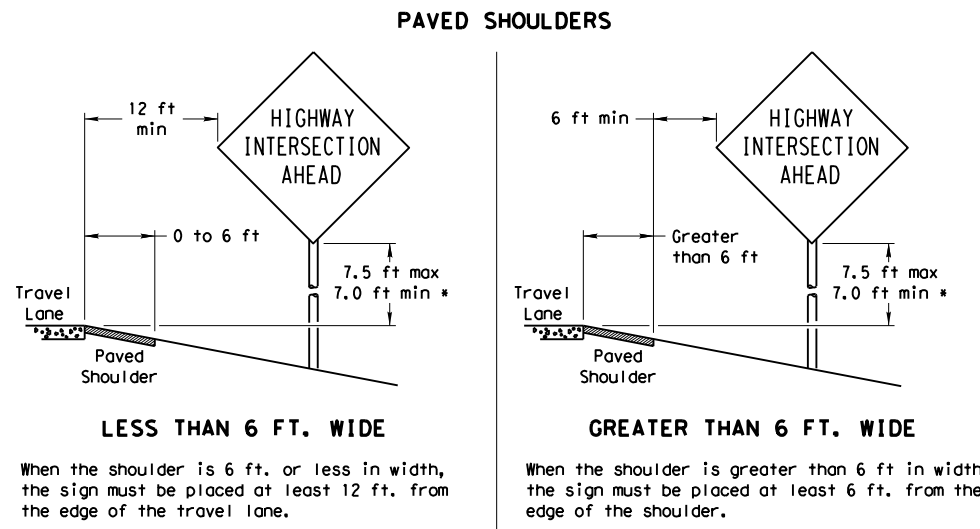
Sign Mounting Designation
 P = Prefab. "Plain" (see SMD(SLIP-1) to (SLIP-3), (TWT), (FRP))
 T = Prefab. "T" (see SMD(SLIP-1) to (SLIP-3), (TWT))
 U = Prefab. "U" (see SMD(SLIP-1) to (SLIP-3))
 IF REQUIRED
 1EXT or 2EXT = Number of Extensions (see SMD(SLIP-1) to (SLIP-3), (TWT))
 BM = Extruded Wind Beam (see SMD(SLIP-1) to (SLIP-3))
 WC = 1.12 #/ft Wing Channel (see SMD(SLIP-1) to (SLIP-3))
 EXAL = Extruded Aluminum Sign Panels (see SMD(SLIP-3))

REQUIRED CLEARANCE FOR BREAKAWAY SUPPORT

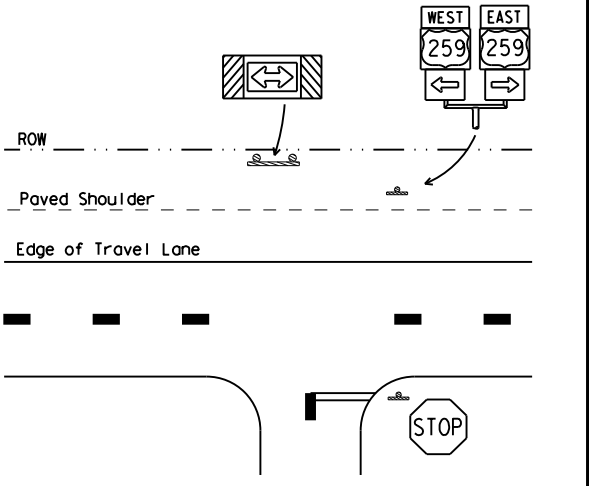
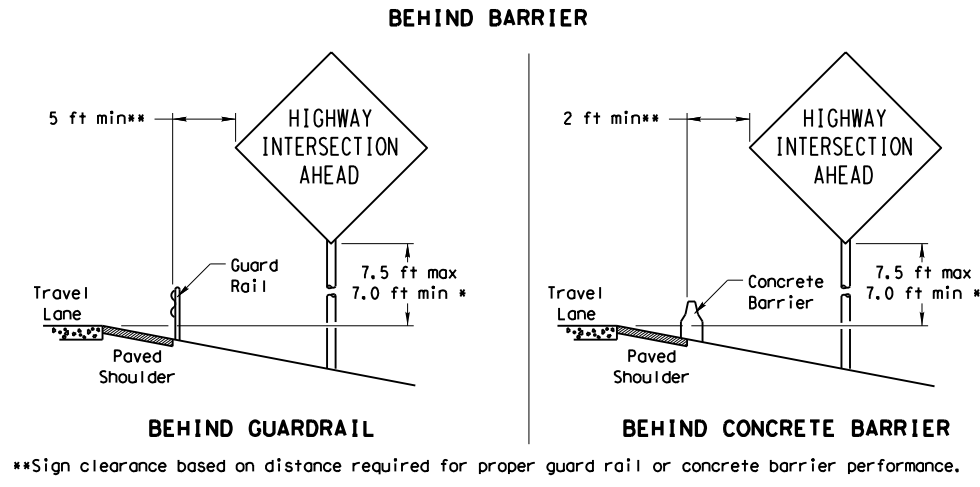
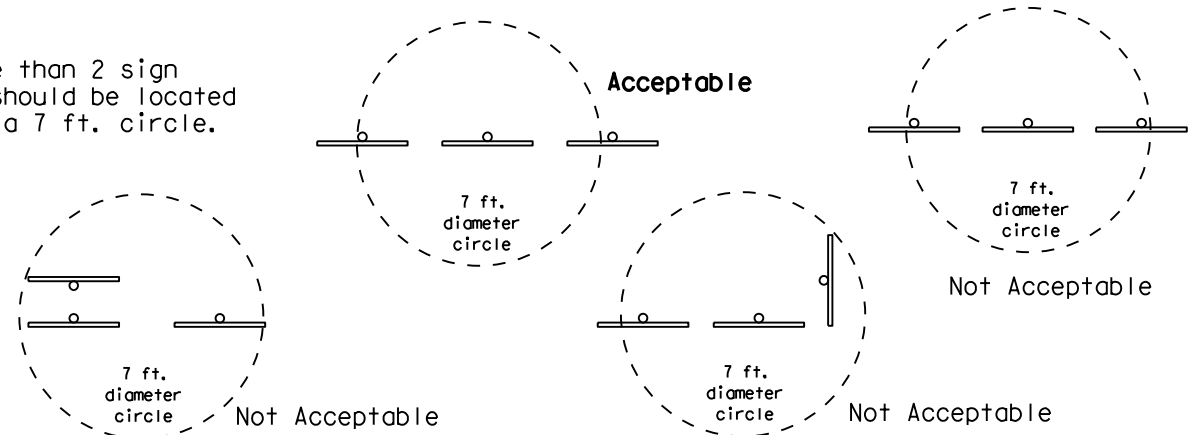


To avoid vehicle undercarriage snagging, any substantial remains of a breakaway support, when it is broken away, should not project more than 4 inches above a 60-inch chord (i.e., typical space between wheel paths).

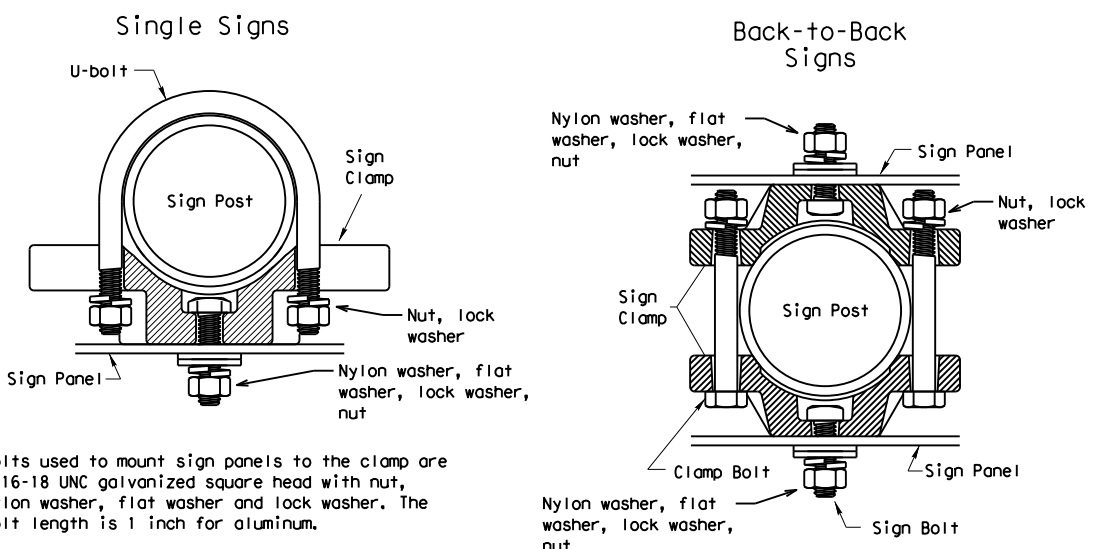
SIGN LOCATION



No more than 2 sign posts should be located within a 7 ft. circle.



TYPICAL SIGN ATTACHMENT DETAIL



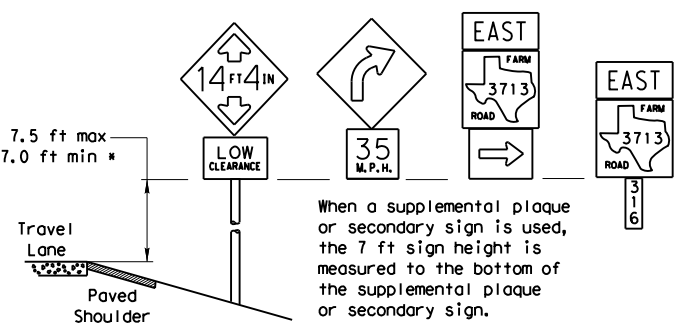
Bolts used to mount sign panels to the clamp are 5/16-18 UNC galvanized square head with nut, nylon washer, flat washer and lock washer. The bolt length is 1 inch for aluminum.

When two sign clamps are used to mount signs back-to-back, use a 5/16-18 UNC galvanized hex head per ASTM A307 with nut and helical-spring lock washer. The approximate bolt lengths for various post sizes and sign clamp types are given in the table at right. The bolt length may need to be adjusted depending upon field conditions.

Sign clamps may be either the specific size clamp or the universal clamp.

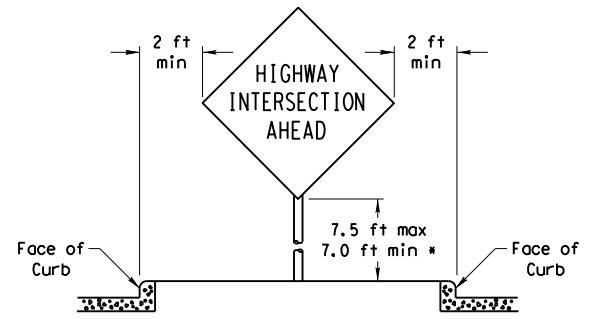
Pipe Diameter	Approximate Bolt Length	
	Specific Clamp	Universal Clamp
2" nominal	3"	3 or 3 1/2"
2 1/2" nominal	3 or 3 1/2"	3 1/2 or 4"
3" nominal	3 1/2 or 4"	4 1/2"

SIGNS WITH PLAQUES

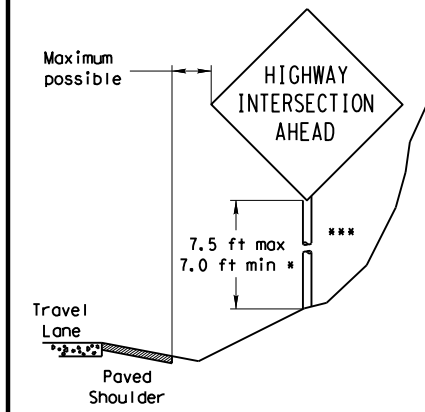


When a supplemental plaque or secondary sign is used, the 7 ft sign height is measured to the bottom of the supplemental plaque or secondary sign.

CURB & GUTTER OR RAISED ISLAND



RESTRICTED RIGHT-OF-WAY (When 6 ft min. is not possible.)



Right-of-way restrictions may be created by rocks, water, vegetation, forest, buildings, a narrow island, or other factors.

In situations where a lateral restriction prevents the minimum horizontal clearance from the edge of the travel lane, signs should be placed as far from the travel lane as practical.

*** Post may be shorter if protected by guardrail or if Engineer determines the post could not be hit due to extreme slope.

* Signs shall be mounted using the following condition that results in the greatest sign elevation:

- (1) a minimum of 7 to a maximum of 7.5 feet above the edge of the travel lane or
- (2) a minimum of 7 to a maximum of 7.5 feet above the grade at the base of the support when sign is installed on the backslope.

The maximum values may be increased when directed by the Engineer.

See the Traffic Operations Division website for detailed drawings of sign clamps, Triangular Slipbase System components and Wedge Anchor System components.

The website address is:
<http://www.txdot.gov/publications/traffic.htm>

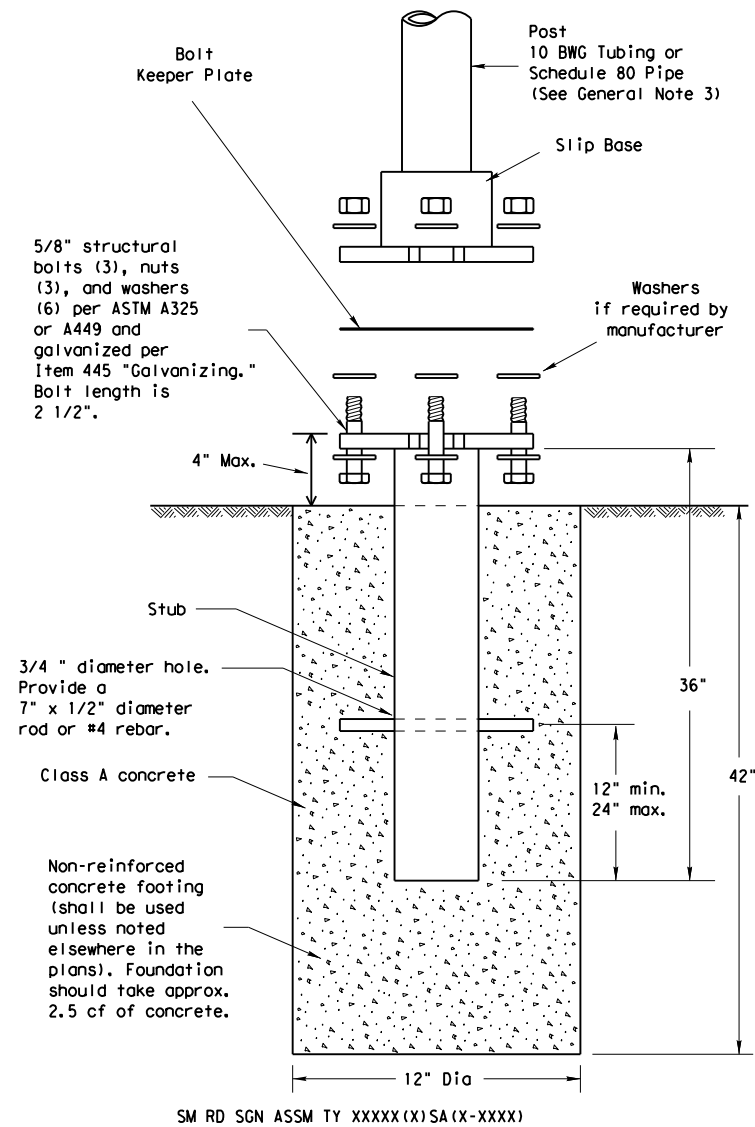


SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS GENERAL NOTES & DETAILS

SMD(GEN)-08

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9-08	REVISIONS	CONT	SECT	JOB	HIGHWAY
		0054	04	123	US 84
		DIST	COUNTY		SHEET NO.
		BWD	COLEMAN CO.		108

TRIANGULAR SLIPBASE INSTALLATION GENERAL REQUIREMENTS



NOTE

There are various devices approved for the Triangular Slipbase System. Please reference the Material Producer List for approved slip base systems. http://www.txdot.gov/business/producer_list.htm The devices shall be installed per manufacturers' recommendations. Installation procedures shall be provided to the Engineer by Contractor.

GENERAL NOTES:

- Slip base shall be permanently marked to indicate manufacturer. Method, design, and location of marking are subject to approval of the TxDOT Traffic Standards Engineer.
- Material used as post with this system shall conform to the following specifications:
 - 10 BWG Tubing (2.875" outside diameter)
 - 0.134" nominal wall thickness
 - Seamless or electric-resistance welded steel tubing or pipe
 - Steel shall be HSLAS Gr 55 per ASTM A1011 or ASTM A1008
 - Other steels may be used if they meet the following:
 - 55,000 PSI minimum yield strength
 - 70,000 PSI minimum tensile strength
 - 20% minimum elongation in 2"
 - Wall thickness (uncoated) shall be within the range of 0.122" to 0.138"
 - Outside diameter (uncoated) shall be within the range of 2.867" to 2.883"
 - Galvanization per ASTM A123 or ASTM A653 G210. For precoated steel tubing (ASTM A653), recoat tube outside diameter weld seam by metallizing with zinc wire per ASTM B833.
 - Schedule 80 Pipe (2.875" outside diameter)
 - 0.276" nominal wall thickness
 - Steel tubing per ASTM A500 Gr C
 - Other seamless or electric-resistance welded steel tubing or pipe with equivalent outside diameter and wall thickness may be used if they meet the following:
 - 46,000 PSI minimum yield strength
 - 62,000 PSI minimum tensile strength
 - 21% minimum elongation in 2"
 - Wall thickness (uncoated) shall be within the range of 0.248" to 0.304"
 - Outside diameter (uncoated) shall be within the range of 2.855" to 2.895"
 - Galvanization per ASTM A123
- See the Traffic Operations Division website for detailed drawings of sign clamps and Texas Universal Triangular Slipbase System components. The website address is: <http://www.txdot.gov/publications/traffic.htm>
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.

ASSEMBLY PROCEDURE

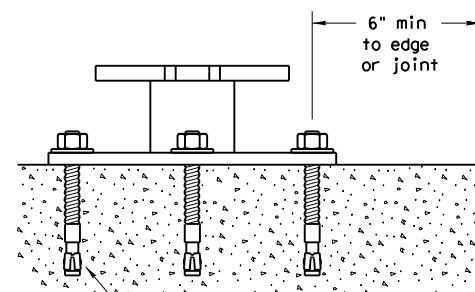
Foundation

- Prepare 12-inch diameter by 42-inch deep hole. If solid rock is encountered, the depth of the foundation may be reduced such that it is embedded a minimum of 18 inches into the solid rock.
- The Engineer may permit batches of concrete less than 2 cubic yards to be mixed with a portable, motor-driven concrete mixer. For small placements less than 0.5 cubic yards, hand mixing in a suitable container may be allowed by Engineer. Concrete shall be Class A.
- Push the pipe end of the slip base stub into the center of the concrete. Rotate the stub back and forth while pushing it down into the concrete to assure good contact between the concrete and stub. Continue to work the stub into the concrete until it is between 2 to 4 inches above the ground.
- Plumb the stub. Allow a minimum of 4 days to set, unless otherwise directed by the Engineer.
- The triangular slipbase system is multidirectional and is designed to release when struck from any direction.

Support

- Cut support so that the bottom of the sign will be 7 to 7.5 feet above the edge of the travelway (i.e., edge of the closest lane) when slip plate is below the edge of pavement or 7 to 7.5 feet above slip plate when the slip plate is above the edge of the travelway. The cut shall be plumb and straight.
- Attach sign to support using connections shown. When multiple signs are installed on the same support, ensure the minimum clearance between each sign is maintained. See SMD(SLIP-2) for clearances based on sign types.

CONCRETE ANCHOR



5/8" diameter Concrete Anchor - 8 places (embed a minimum of 5 1/2" and torque to min. of 50 ft-lbs). Anchor may be expansion or adhesive type.

SM RD SGN ASSM TY XXXXX(X)SB(X-XXXX)

Concrete anchor consists of 5/8" diameter stud bolt with UNC series bolt threads on the upper end. Heavy hex nut per ASTM A563, and hardened washer per ASTM F436. The stud bolt shall have a minimum yield and ultimate tensile strength of 50 and 75 KSI, respectively. Nuts, bolts and washers shall be galvanized per Item 445, "Galvanizing." Adhesive type anchors shall have stud bolts installed with Type III epoxy per DMS-6100, "Epoxyes and Adhesives." Adhesive anchors may be loaded after adequate epoxy cure time per the manufacturer's recommendations. Top of bolt shall extend at least flush with top of the nut when installed. The anchor, when installed in 4000 psi normal-weight concrete with a 5 1/2" minimum embedment, shall have a minimum allowable tension and shear of 3900 and 3100 psi, respectively.

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Texas Department of Transportation
Traffic Operations Division

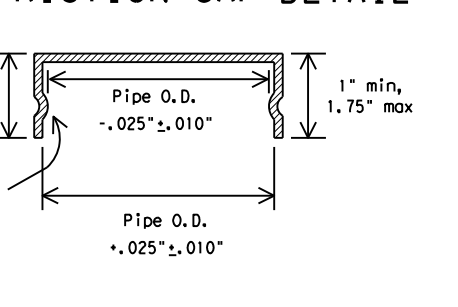
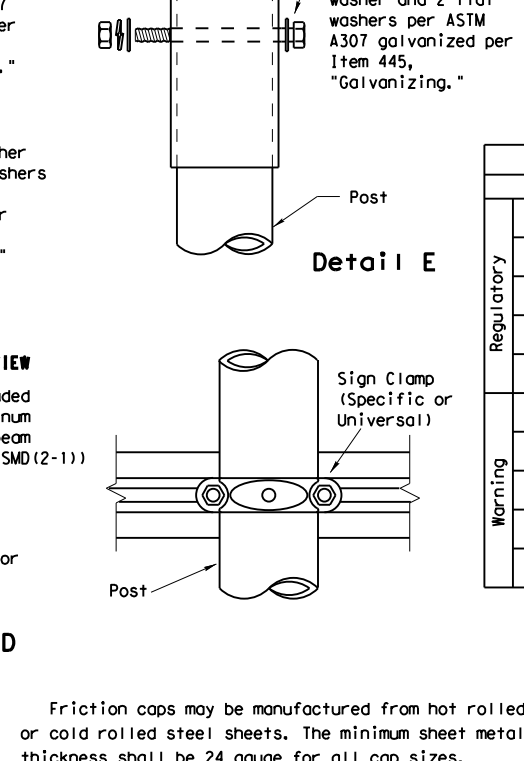
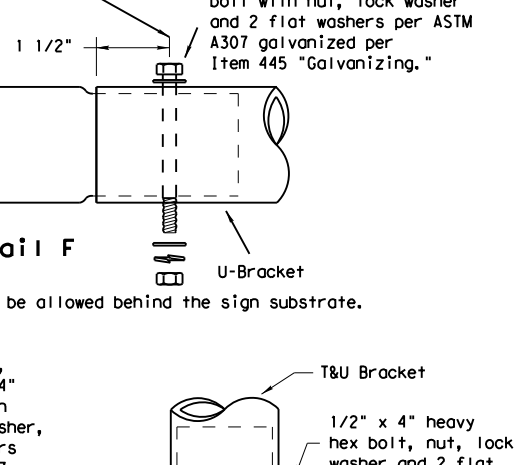
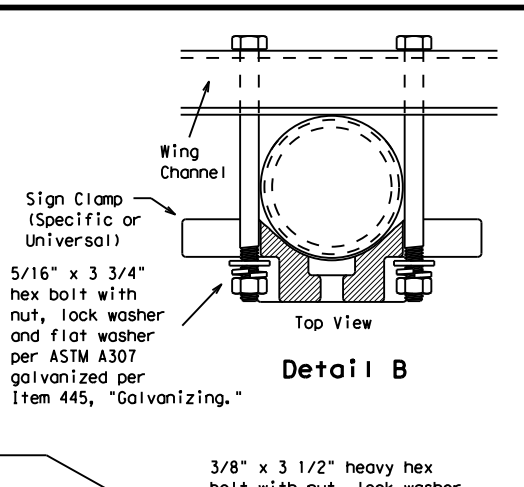
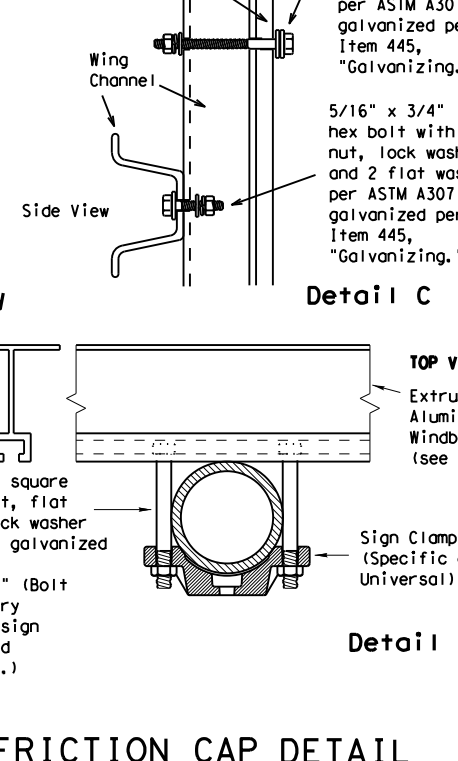
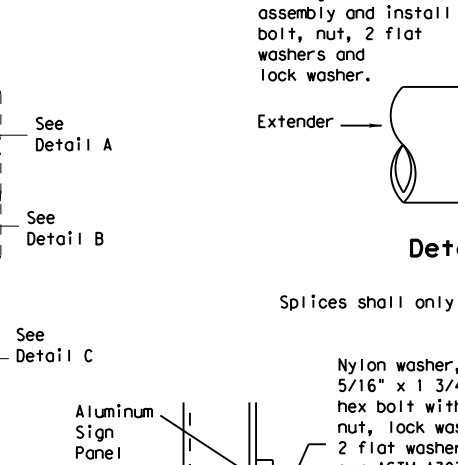
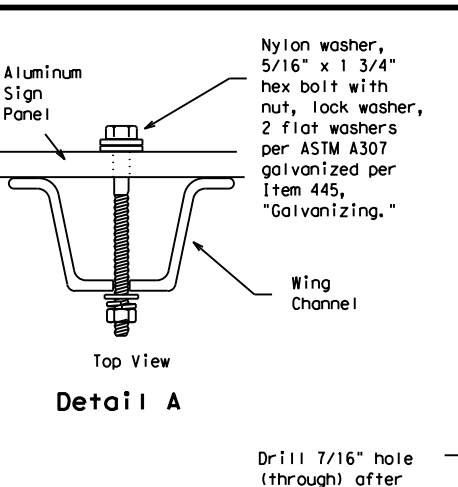
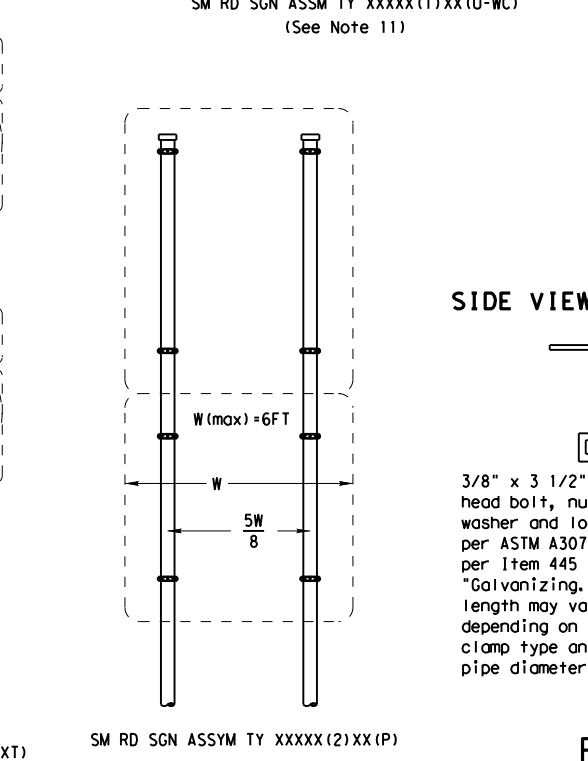
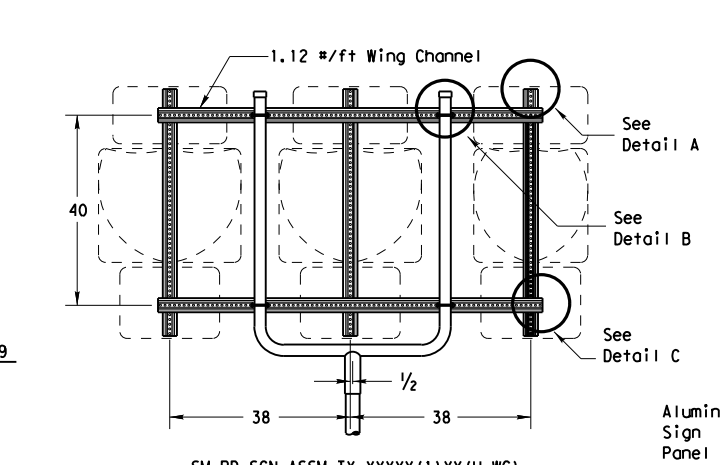
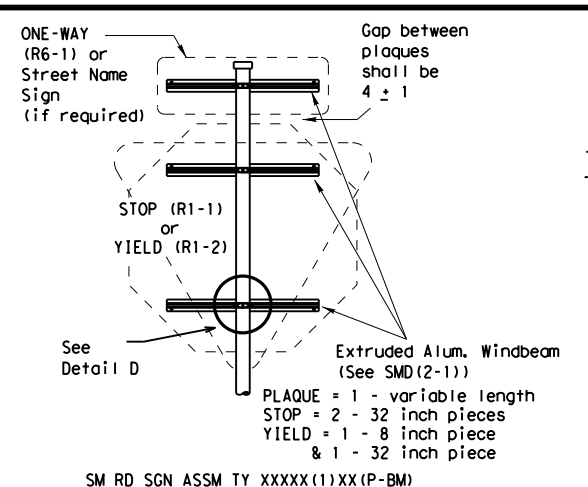
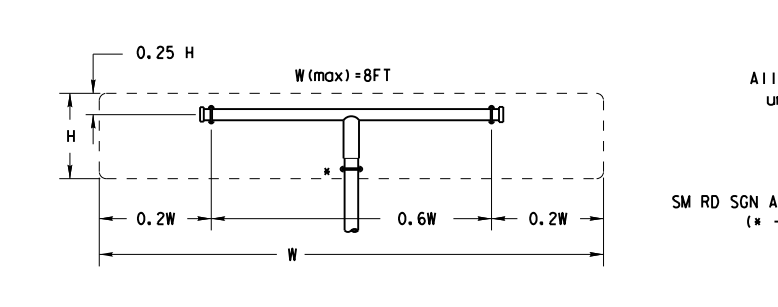
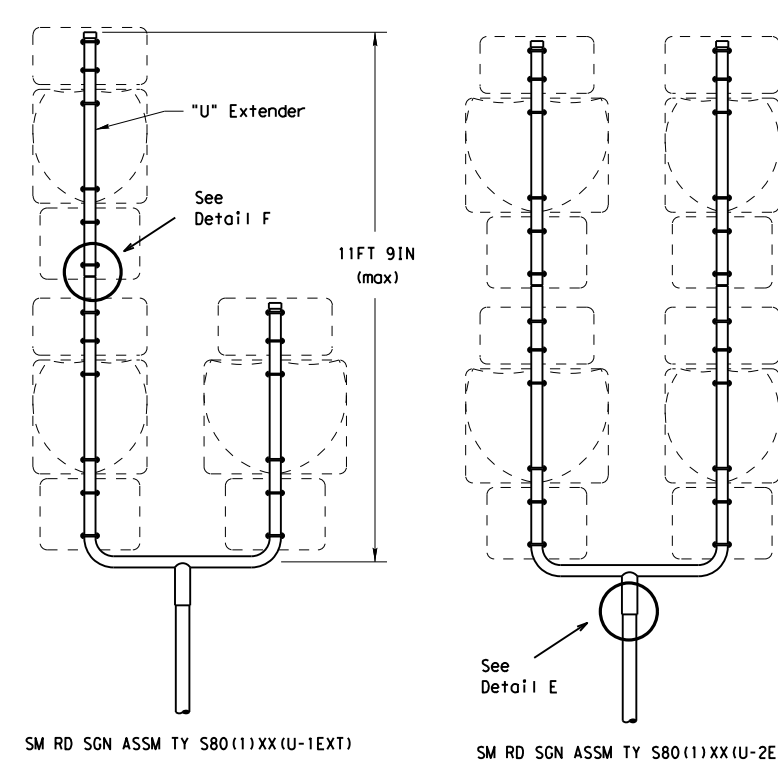
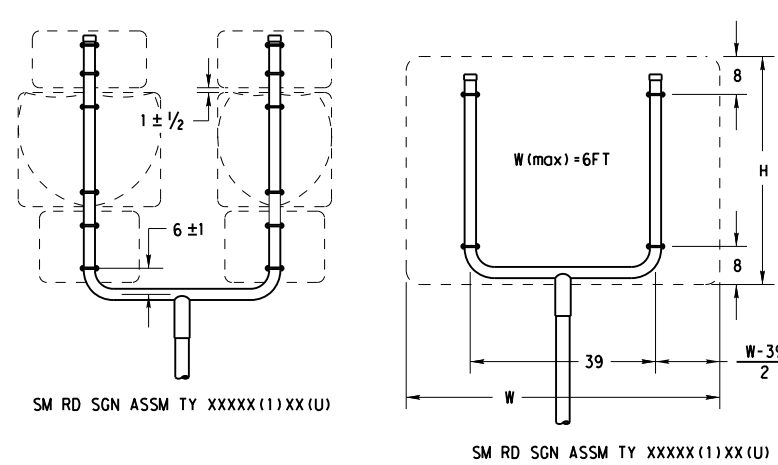
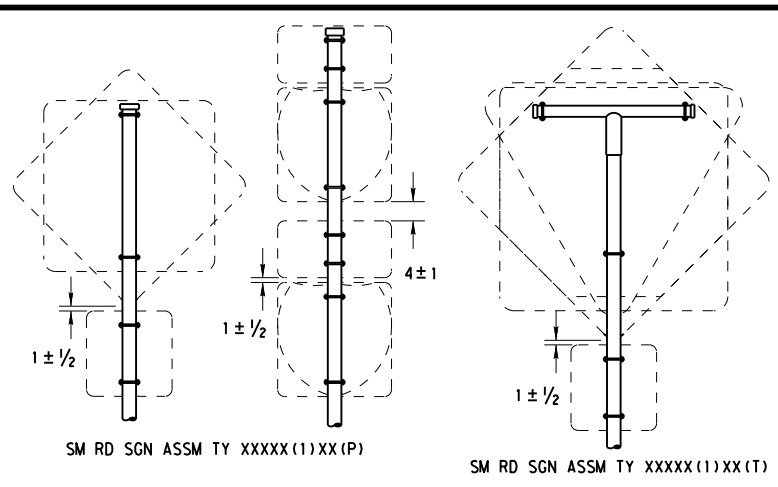
SIGN MOUNTING DETAILS
SMALL ROADSIDE SIGNS
TRIANGULAR SLIPBASE SYSTEM

SMD(SLIP-1)-08

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9-08	REVISIONS	CONT	SECT	JOB	HIGHWAY
		0054	04	123	US 84
		DIST	COUNTY	SHEET NO.	
		BWD	COLEMAN CO.	109	

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- GENERAL NOTES:**
1. SIGN SUPPORT # OF POSTS MAX. SIGN AREA

10 BWG	1	16 SF
10 BWG	2	32 SF
Sch 80	1	32 SF
Sch 80	2	64 SF
 2. The Engineer may require that a Schedule 80 post be used in place of a 10 BWG where a sign height is abnormally high due to a fill slope.
 3. Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.
 4. Aluminum sign blanks shall conform to Departmental Material Specifications DMS-7110 and shall have the following minimum thicknesses: 0.080 for signs less than 7.5 sq. ft., 0.100 for signs 7.5 to 15 sq. ft., and 0.125 for signs greater than 15 sq. ft.
 5. Signs that require specific supports due to reasons in addition to windloading are indicated on the "REQUIRED SUPPORT" table on this sheet.
 6. For horizontal rectangular signs fabricated from flat aluminum, T-brackets are used for signs 24 inches or less in height. U-brackets are used for signs of greater height.
 7. When two triangular slipbase supports are used to support a single sign, they shall not be "rigidly" connected to each other except through the sign panel. This will allow each support to act independently when impacted by an errant vehicle.
 8. Wing channel shall meet ASTM A 1011 SS Gr 50 and be galvanized per ASTM A 123.
 9. Excess pipe, wing channel, or windbeam shall be cut off so that it does not extend beyond the sign panel (i.e., excess support shall not be visible when the sign is viewed from the front.) Repair galvanized coating at cut support ends per Item 445, "Galvanizing."
 10. Additional route markers may be added vertically, provided the total sign area does not exceed the maximum allowable amount per Note 1.
 11. Additional sign clamp required on the "T-bracket" post for 24 inch height signs. Place the clamp 3 inches above bottom of sign when possible.
 12. Post open ends shall be fitted with Friction Caps.
 13. Sign blanks shall be the sizes and shapes shown on the plans.

REQUIRED SUPPORT		
SIGN DESCRIPTION	SUPPORT	
Regulatory	48-inch STOP sign (R1-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	60-inch YIELD sign (R1-2)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	48x16-inch ONE-WAY sign (R6-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
Warning	36x48, 48x36, and 48x48-inch signs	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
	48x48-inch signs (diamond or square)	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
	48-inch Advance School X-ing sign (S1-1)	TY 10BWG(1)XX(T)
48-inch School X-ing sign (S2-1)	TY 10BWG(1)XX(T)	
Large Arrow sign (W1-6 & W1-7)	TY 10BWG(1)XX(T)	

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 Traffic Operations Division

SIGN MOUNTING DETAILS
SMALL ROADSIDE SIGNS
TRIANGULAR SLIPBASE SYSTEM
SMD(SLIP-2)-08

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		DIST: BWD	COUNTY: COLEMAN CO.	HIGHWAY: US 84
				SHEET NO.: 110

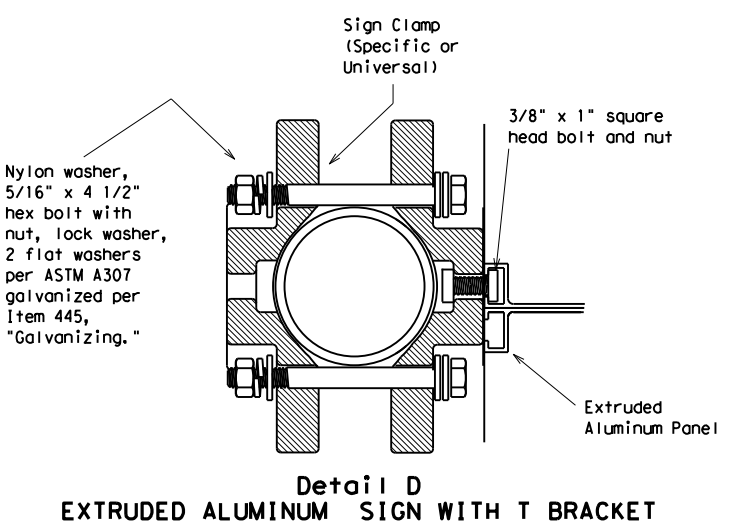
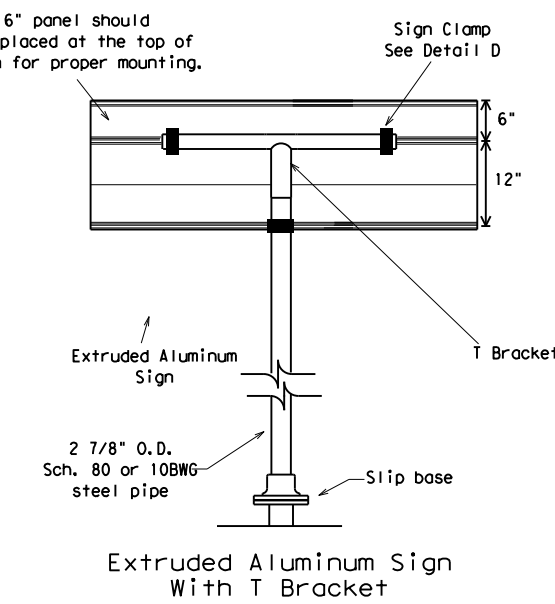
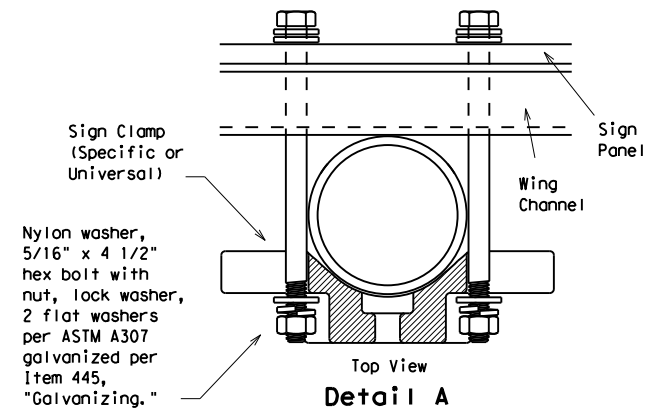
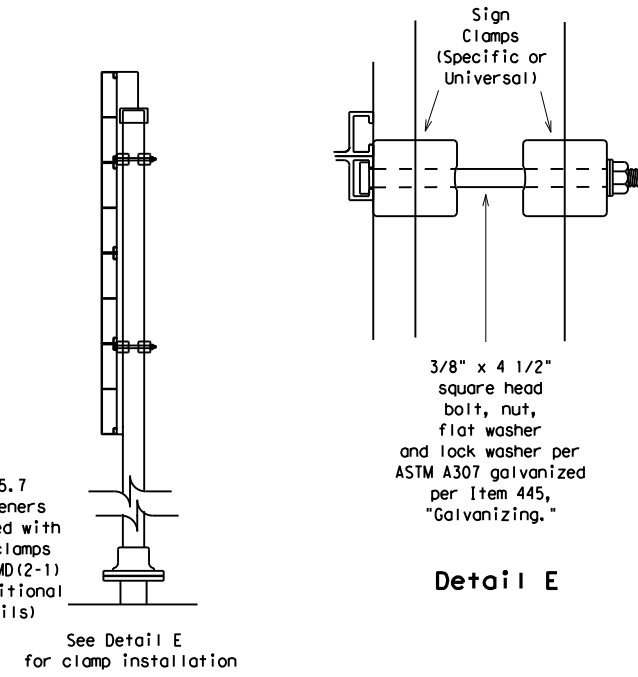
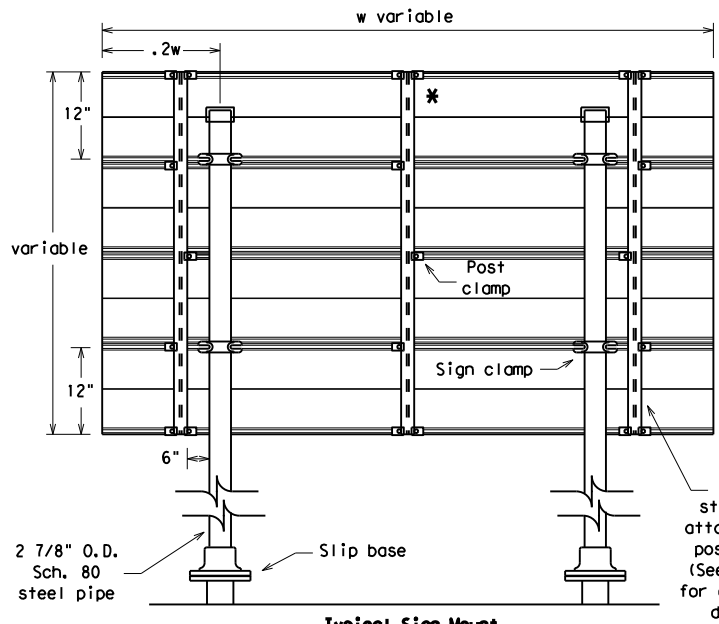
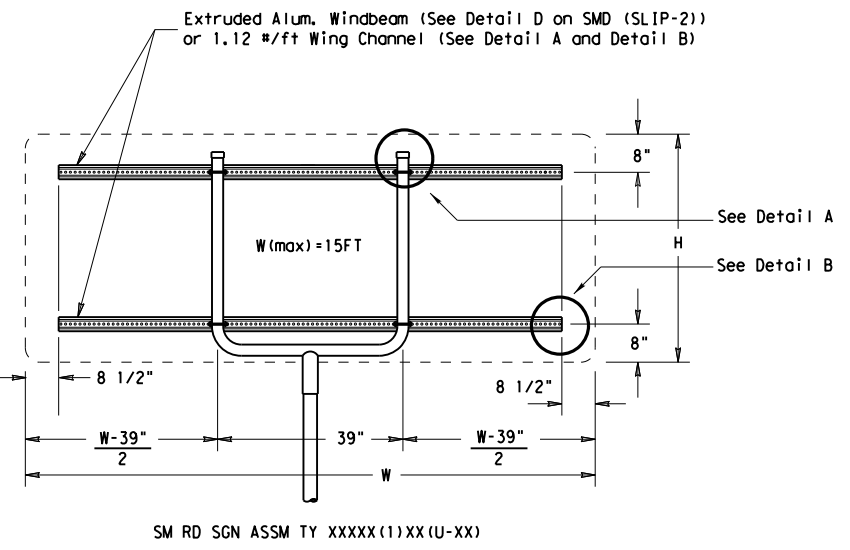
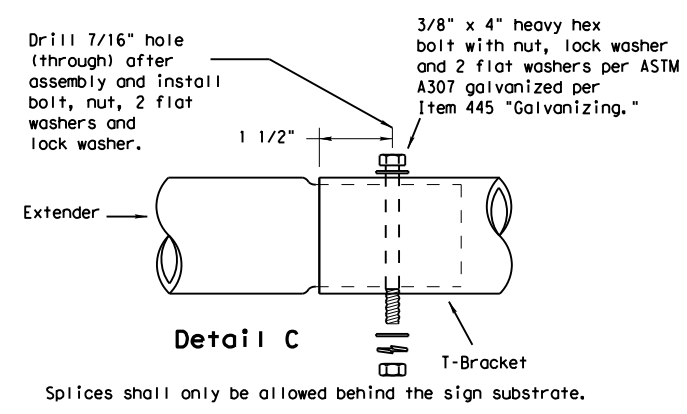
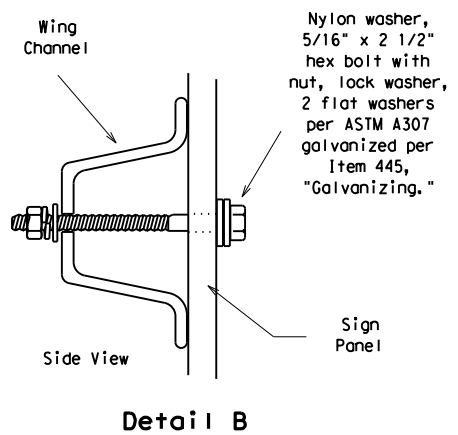
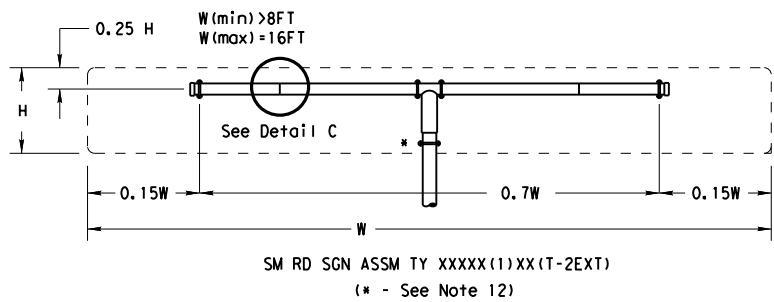
All dimensions are in english unless detailed otherwise.

SM RD SGN ASSM TY XXXXX(1)XX(T) (* - See Note 12)

Friction caps may be manufactured from hot rolled or cold rolled steel sheets. The minimum sheet metal thickness shall be 24 gauge for all cap sizes. The rim edges shall be reasonably straight and smooth. Caps shall be sized and formed in such a manner as to produce a drive-on friction fit and have no tendency to rock when seated on the pipe. The depth shall be sufficient to give positive protection against entrance of rainwater. They shall be free of sharp creases or indentations and show no evidence of metal fracture. Caps shall have an electrodeposited coating of zinc in accordance with the requirements of ASTM B633 Class FE/ZN 8.

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GENERAL NOTES:

- | SIGN SUPPORT | # OF POSTS | MAX. SIGN AREA |
|--------------|------------|----------------|
| 10 BWG | 1 | 16 SF |
| 10 BWG | 2 | 32 SF |
| Sch 80 | 1 | 32 SF |
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- Wing channel shall meet ASTM A 1011 SS Gr 50 and be galvanized per ASTM A 123.
- Excess pipe, wing channel, or windbeam shall be cut off so that it does not extend beyond the sign panel (i.e., excess support shall not be visible when the sign is viewed from the front.) Repair galvanized coating at cut support ends per Item 445, "Galvanizing."
- Sign blanks shall be the sizes and shapes shown on the plans.
- Additional sign clamp required on the "T-bracket" post for 24 inch high signs. Place the clamp 3 inches above bottom of sign when possible.
- Post open ends shall be fitted with Friction Caps.

REQUIRED SUPPORT		
	SIGN DESCRIPTION	SUPPORT
Regulatory	48-inch STOP sign (R1-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	60-inch YIELD sign (R1-2)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	48x16-inch ONE-WAY sign (R6-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	36x48, 48x36, and 48x48-inch signs	TY 10BWG(1)XX(T)
Warning	48x60-inch signs	TY S80(1)XX(T)
	48x48-inch signs (diamond or square)	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
	48-inch Advance School X-ing sign (S1-1)	TY 10BWG(1)XX(T)
	48-inch School X-ing sign (S2-1)	TY 10BWG(1)XX(T)
	Large Arrow sign (W1-6 & W1-7)	TY 10BWG(1)XX(T)

Texas Department of Transportation
 Traffic Operations Division

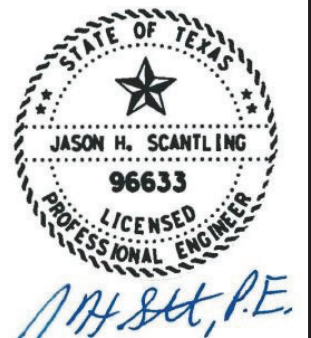
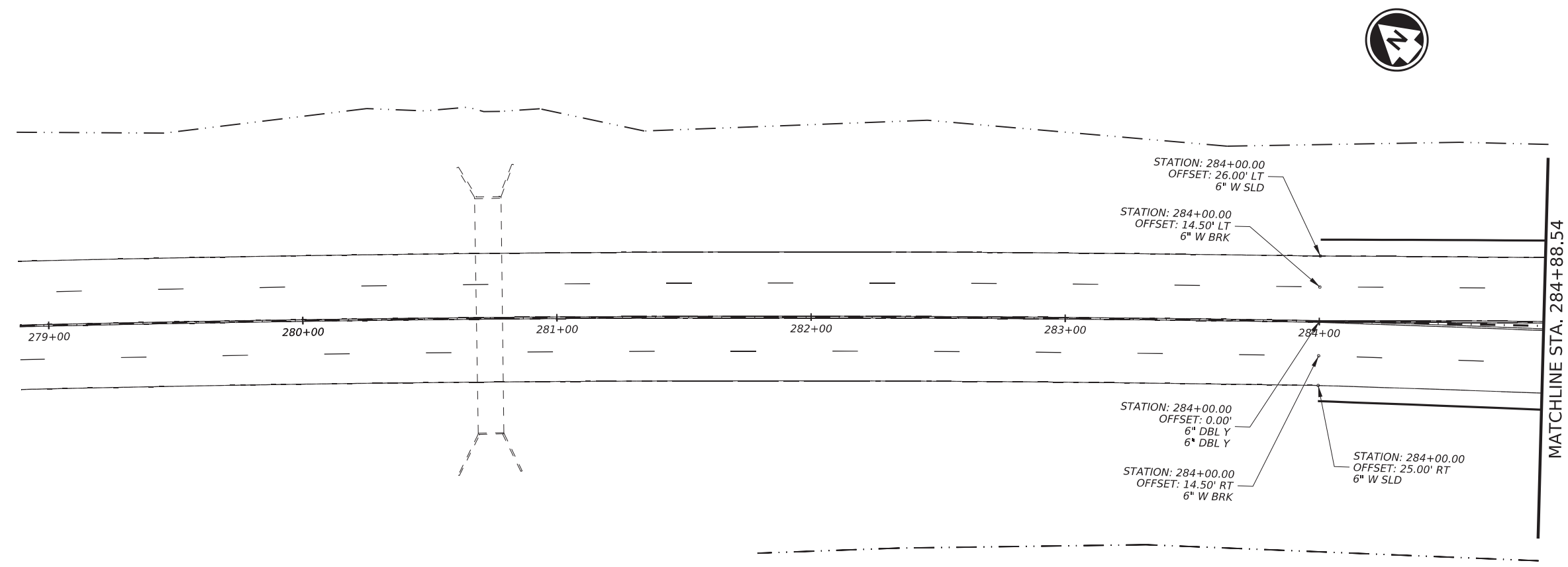
**SIGN MOUNTING DETAILS
 SMALL ROADSIDE SIGNS
 TRIANGULAR SLIPBASE SYSTEM**

SMD(SLIP-3)-08

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9-08	REVISIONS	CON: 0054	SECT: 04	JOB: 123	HIGHWAY: US 84
		DIST: BWD	COUNTY: COLEMAN CO.	SHEET NO. 111	

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01/26/2023



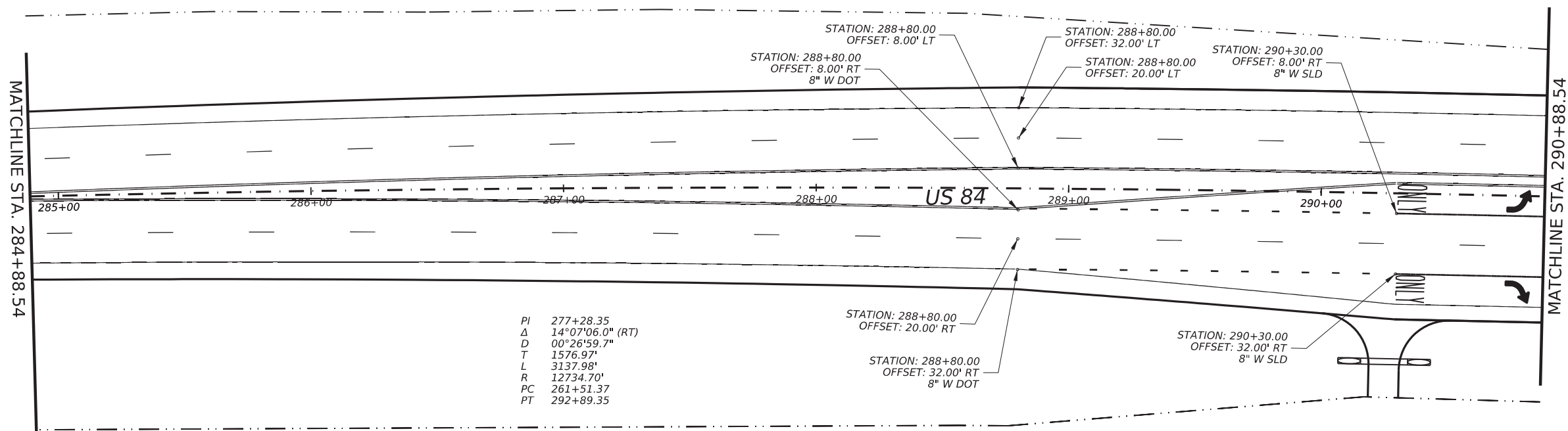
US 84
 STRIPING LAYOUT

SHEET 1 OF 8

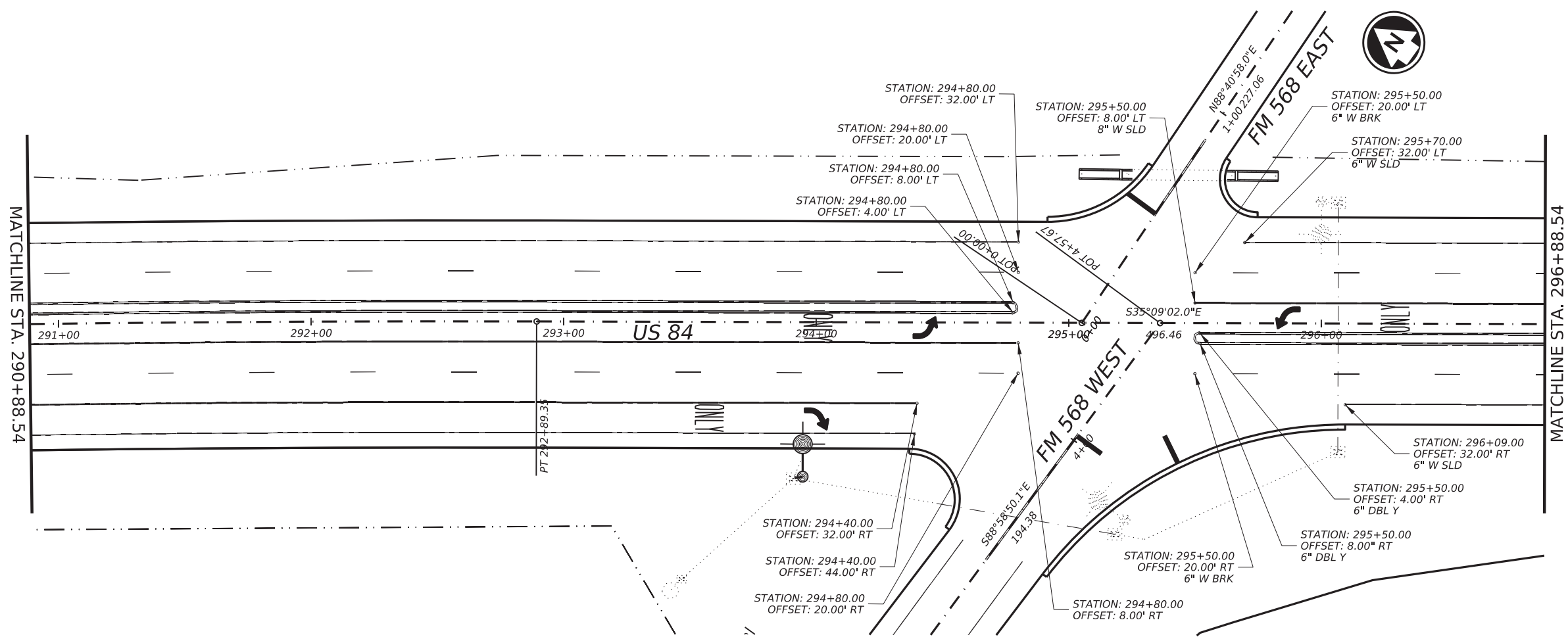
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JH Scantling, P.E.

01/28/2023

Texas Department of Transportation

US 84

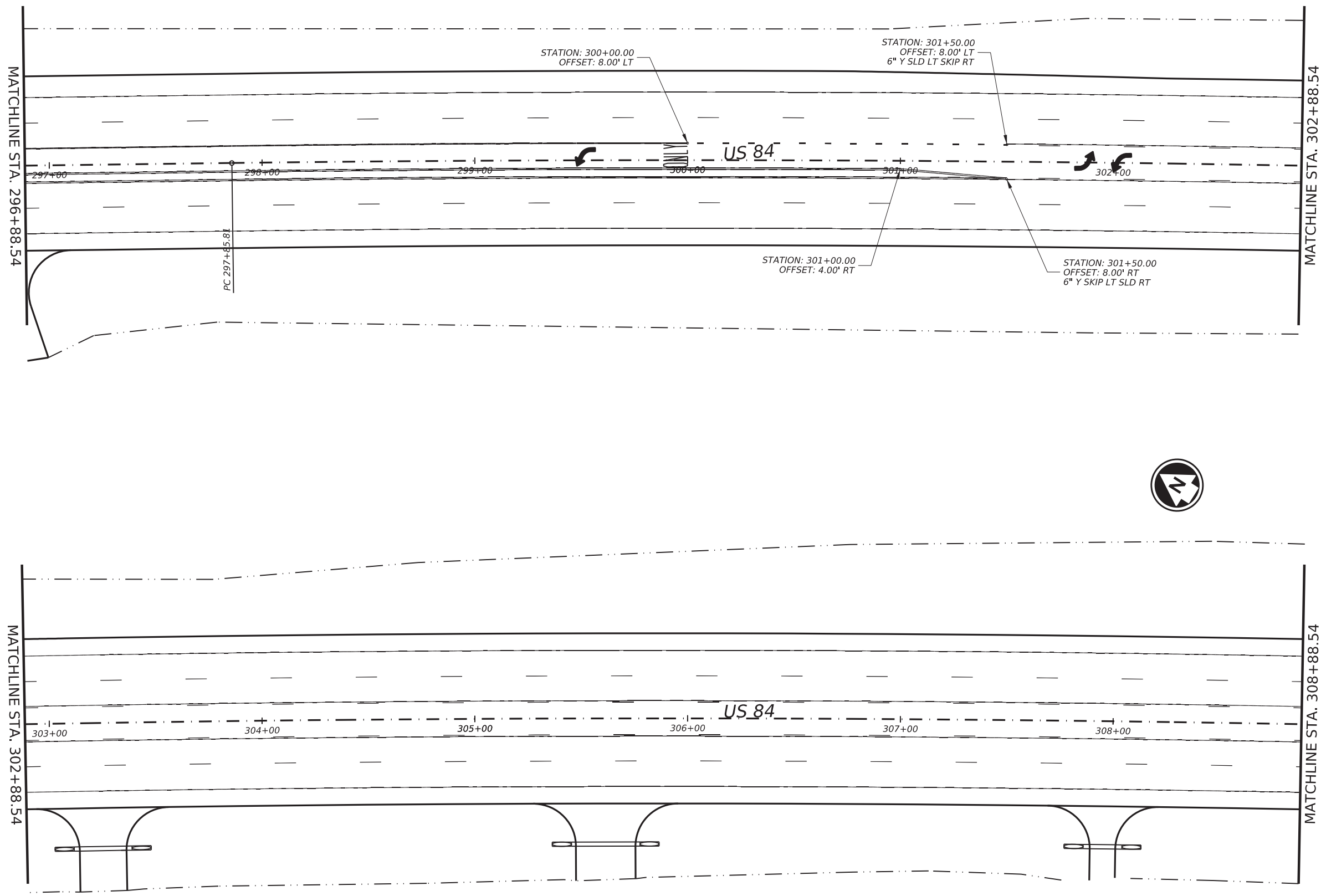
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JH Scantling, P.E.

01/28/2023



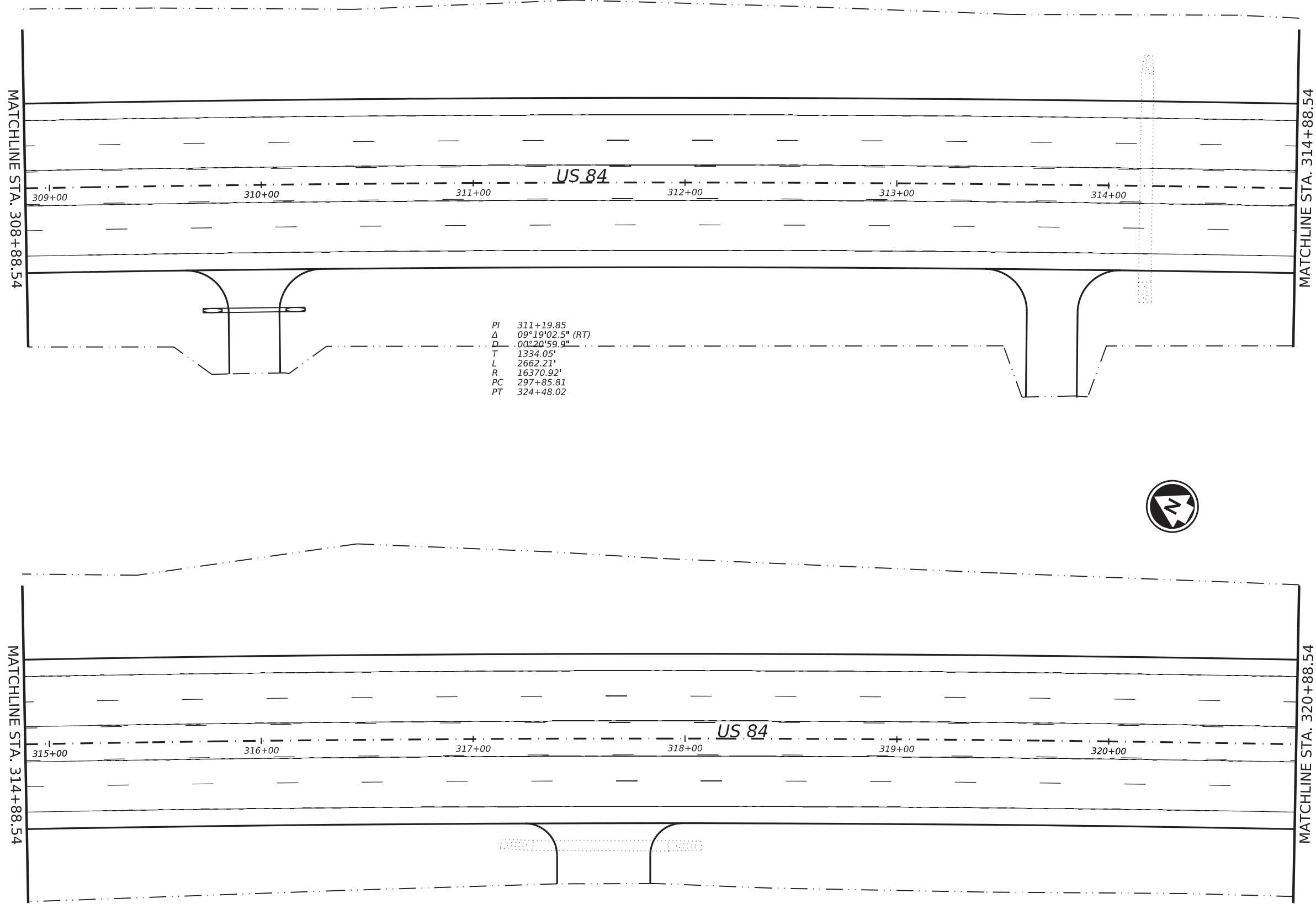
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SHEET 3 OF 8

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01/26/2023

Texas Department of Transportation

US 84

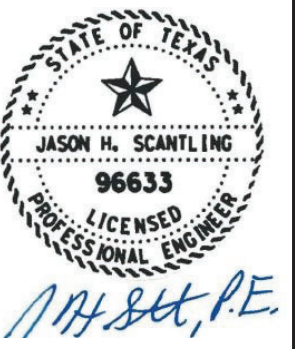
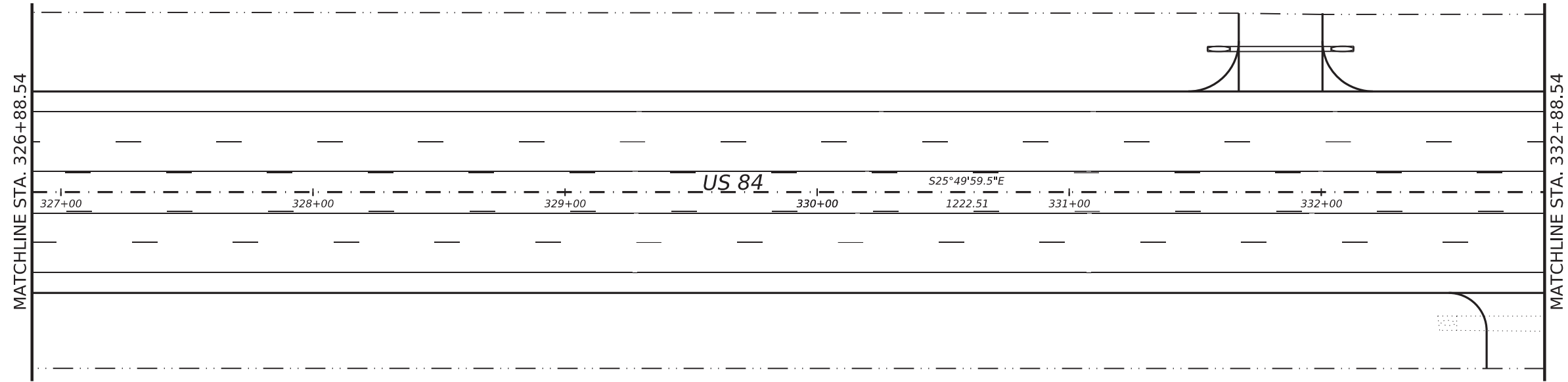
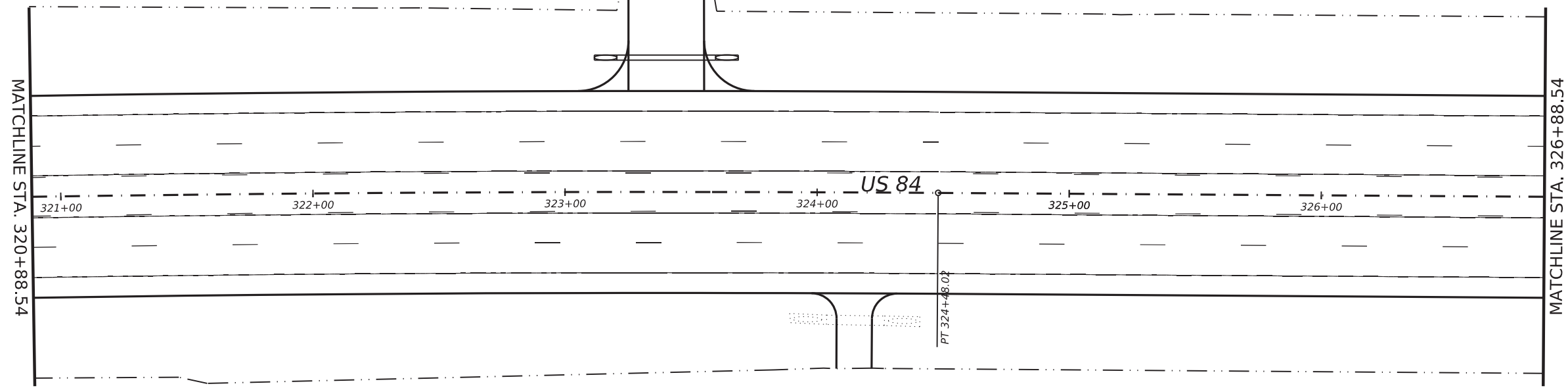
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SHEET 4 OF 8

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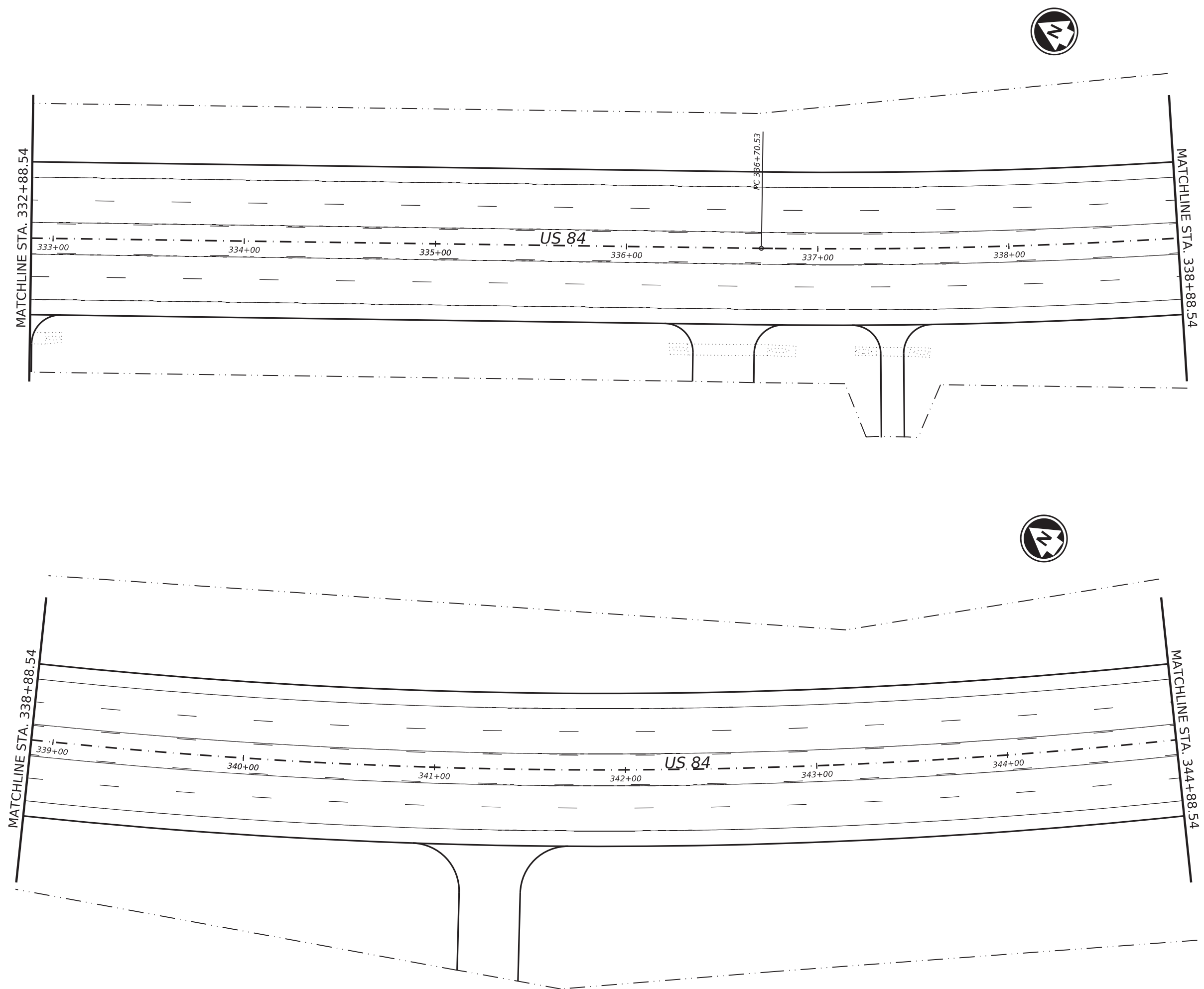
US 84
 STRIPING LAYOUT

SHEET 5 OF 8

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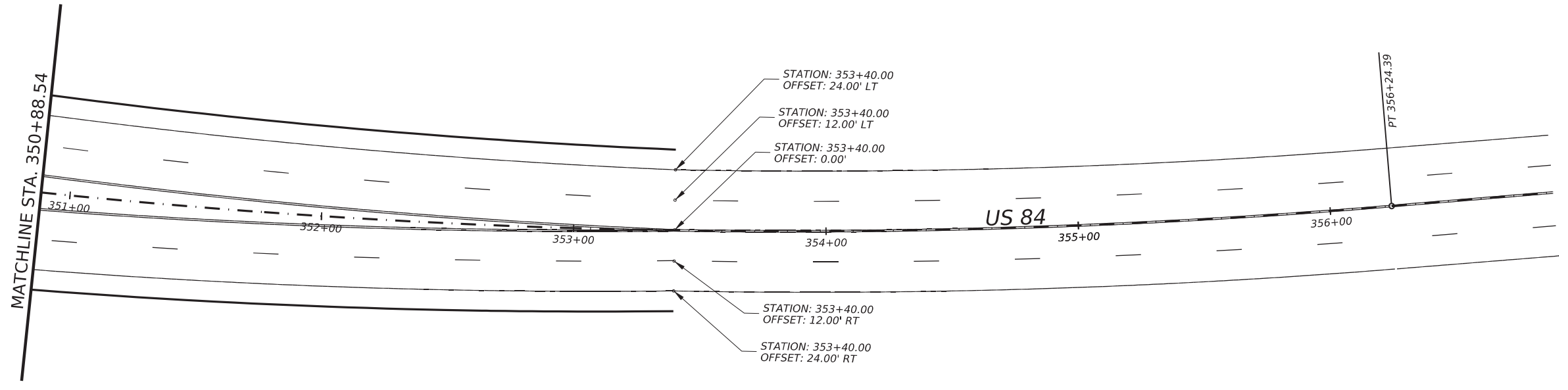
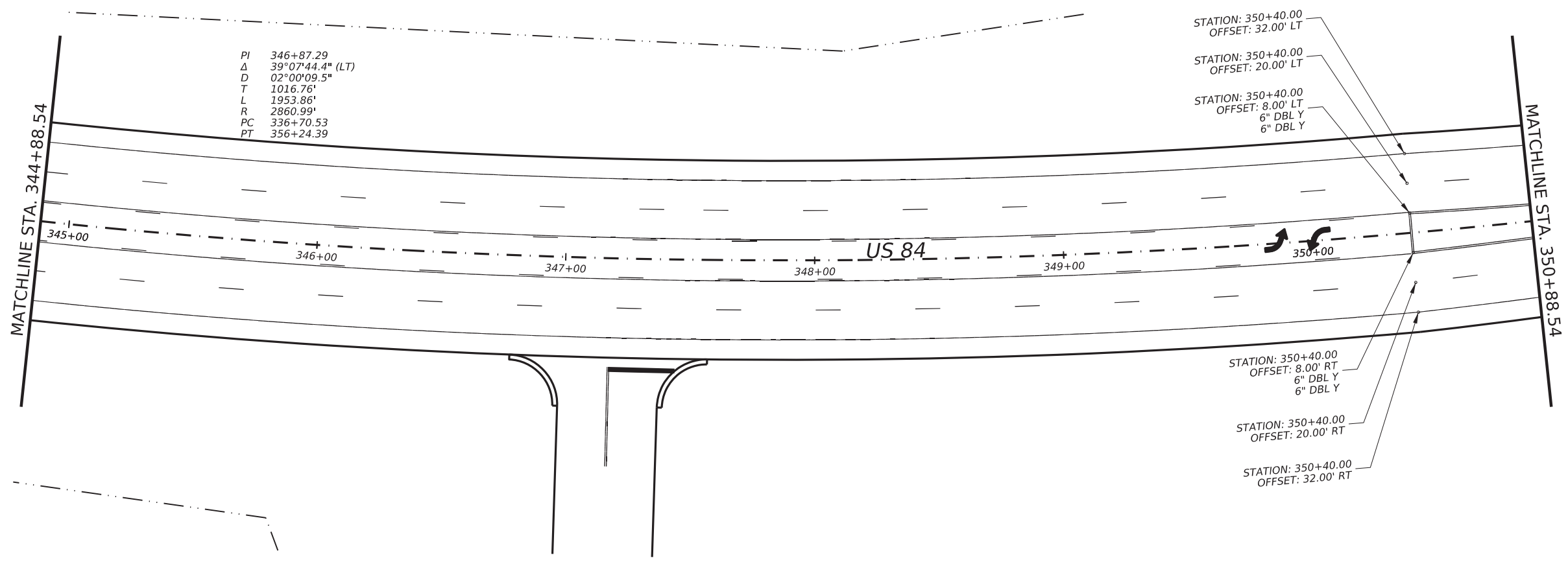
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SHEET 6 OF 8

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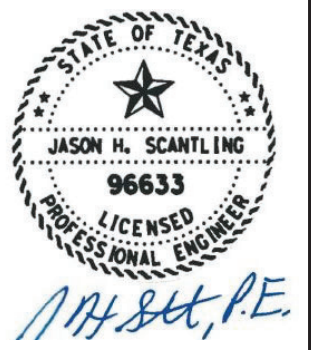
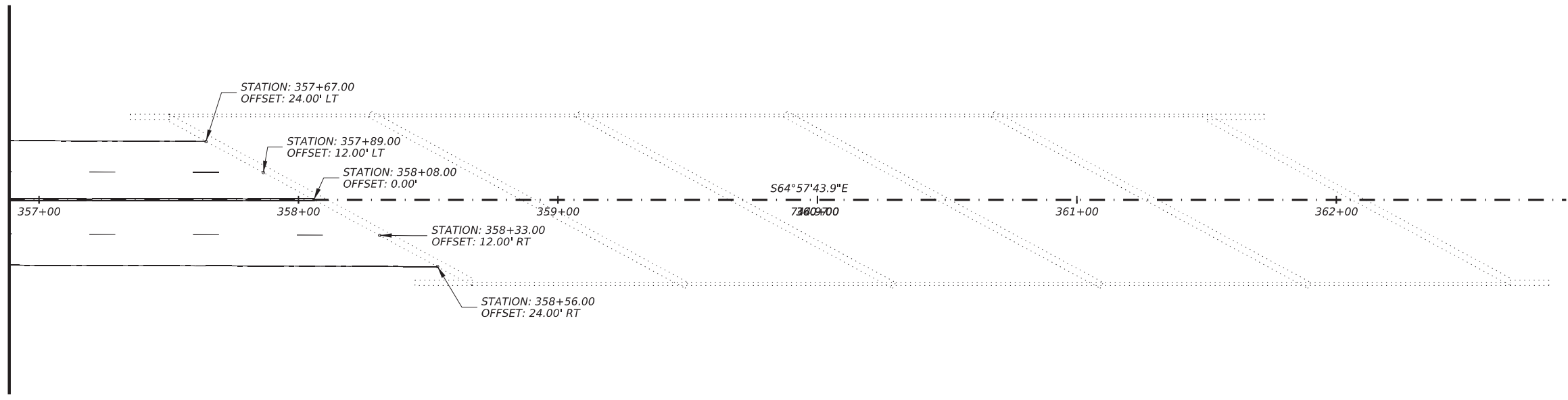
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SHEET 7 OF 8

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01/26/2023

US 84 STRIPING LAYOUT			
SHEET 8 OF 8			
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DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	119	

DATE: 1/11/2023 1:02:55 PM
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0054-04-123 STATION/LOCATION	0533-6001 RUMBLE STRIPS (SHOULDER) LF	0533-6002 RUMBLE STRIPS (CENTERLINE) LF	0666 6030 REFL PAV MRK TY I (W) 8" (DOT)(100MIL) LF	0666 6036 REFL PAV MRK TY I (W) 8" (SLD)(100MIL) LF	0666 6048 REFL PAV MRK TY I (W)24" (SLD)(100MIL) LF	0666 6306 RE PM W/RET REQ TY I (W)6" (BRK)(100MIL) LF	0666 6309 RE PM W/RET REQ TY I (W)6" (SLD)(100MIL) LF	0666 6318 RE PM W/RET REQ TY I (Y)6" (BRK)(100MIL) LF	0666 6321 RE PM W/RET REQ TY I (Y)6" (SLD)(100MIL) LF	0672 6007 REFL PAV MRKR TY I-C EA	0672 6009 REFL PAV MRKR TY II-A-A EA	0668 6077 PREFAB PAV MRK TY C (W) (ARROW) EA	0668 6085 PREFAB PAV MRK TY C (W) (WORD) EA
US 84													
284+00 ~ 288+80	960	960				240	960		1920	12	48		
288+80 ~ 290+30	300	300	75			75	300		300	11	16		
290+30 ~ 294+40	820	820		820		205	820		1640	52	41	3	4
294+40 ~ 294+80	40	80		40		10	40		160	22	4	1	
294+80 ~ 295+50					38				196				
295+50 ~ 295+70		40		20		10			80	2	2		
295+70 ~ 296+09	39	78		39		20	39		156	3	4	1	
296+09 ~ 300+00	782	782		391		196	782		1564	30	40	1	2
300+00 ~ 301+50	300	300	38			75	300		600	8	15		
301+50 ~ 350+40	9780	9780			27	2445	9780	2445	9780	122	245	4	
350+40 ~ 353+40	600	600				150	600		1232	8	62		
353+40 ~ 358+56	1032	516				258	1032		1032	13	14		
TOTALS	14653	14256	113	1310	65	3684	14653	2445	18660	283	491	10	6



01/26/2023

**US 84
STRIPING
SUMMARY**

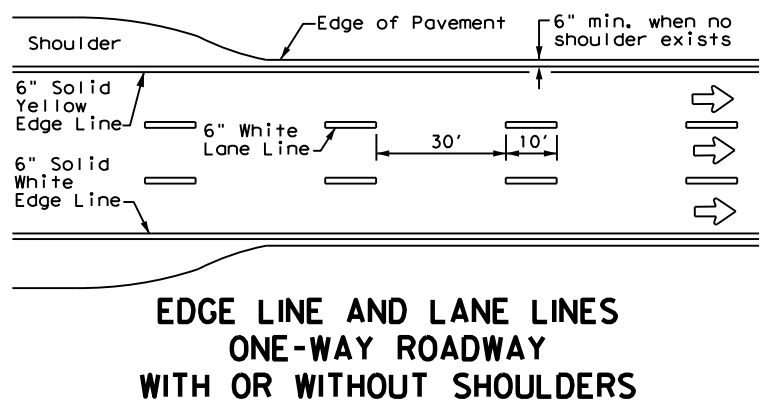
NOTE: COORDINATE FINAL STRIPING WITH AREA ENGINEER BEFORE PLACEMENT.
 OPTION 4 SHALL BE USED FOR RUMBLE STRIP PLACEMENT WHEN SHOULDER WIDTH IS 4' OR GREATER

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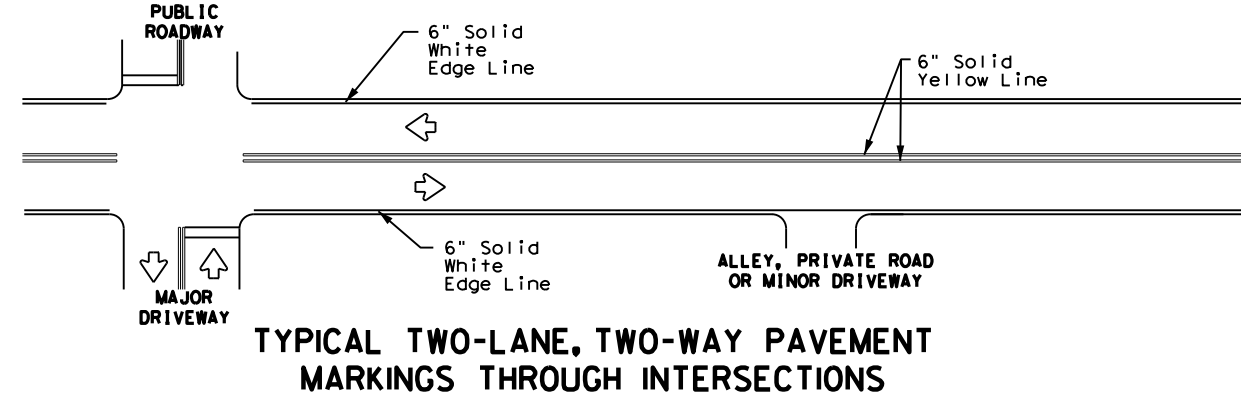
CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		120

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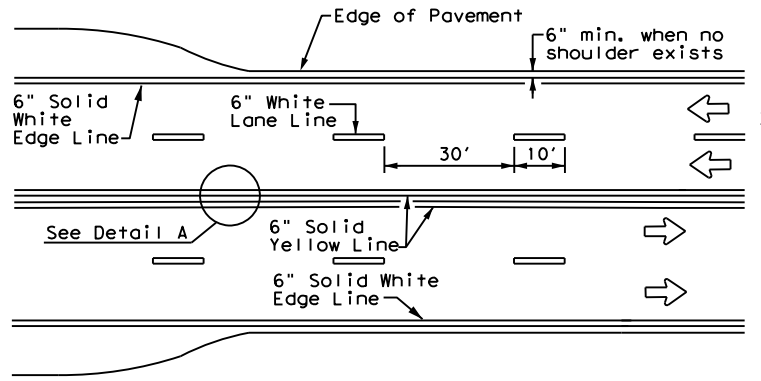
DATE: 2/22/2023 3:48:31 PM
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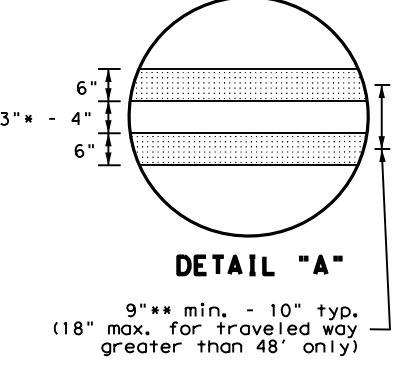
**EDGE LINE AND LANE LINES
ONE-WAY ROADWAY
WITH OR WITHOUT SHOULDERS**



**TYPICAL TWO-LANE, TWO-WAY PAVEMENT
MARKINGS THROUGH INTERSECTIONS**

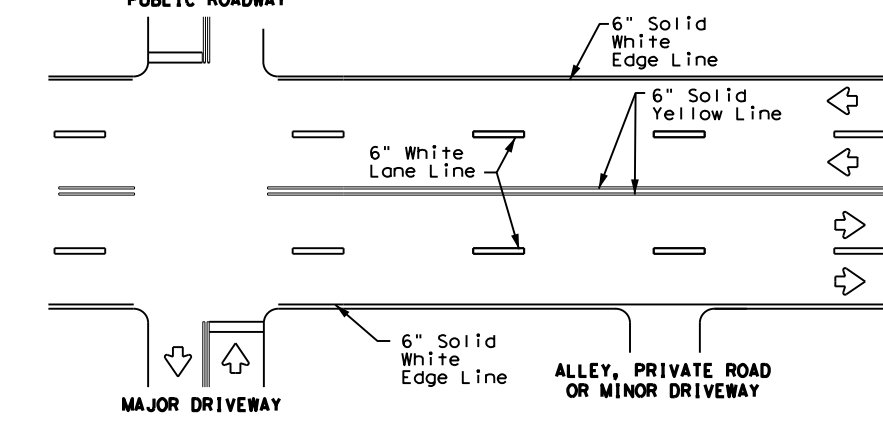


**CENTERLINE AND LANE LINES
FOUR LANE TWO-WAY ROADWAY
WITH OR WITHOUT SHOULDERS**

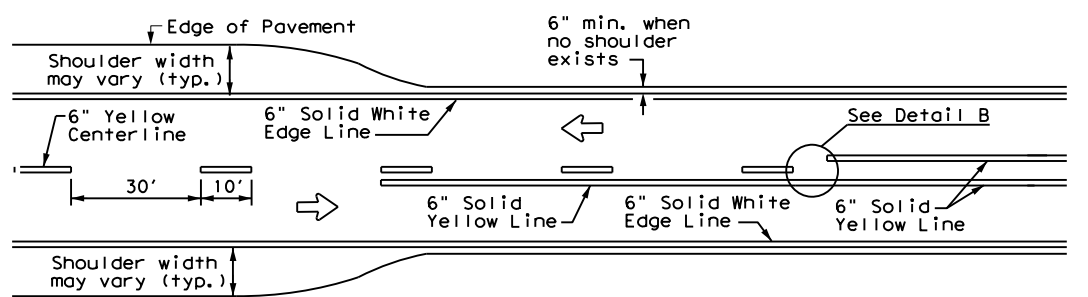


DETAIL "A"
 9" min. - 10" typ.
 (18" max. for traveled way
 greater than 48' only)

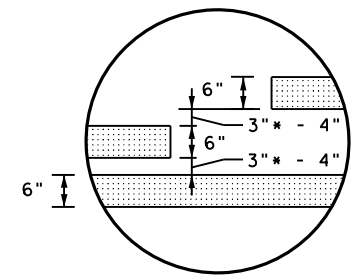
* 2" minimum for restripe projects when approved by the Engineer.
 ** 8" minimum for restripe projects when approved by the Engineer.



**TYPICAL MULTI-LANE, TWO-WAY PAVEMENT
MARKINGS THROUGH INTERSECTIONS**

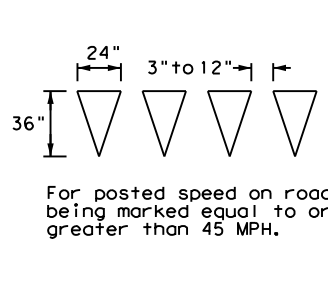


**TWO LANE TWO-WAY ROADWAY
WITH OR WITHOUT SHOULDERS**

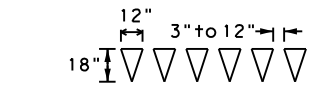


DETAIL "B"
 16" min. - 20" max.
 (16" minimum for
 restripe projects
 when approved by
 the Engineer.)

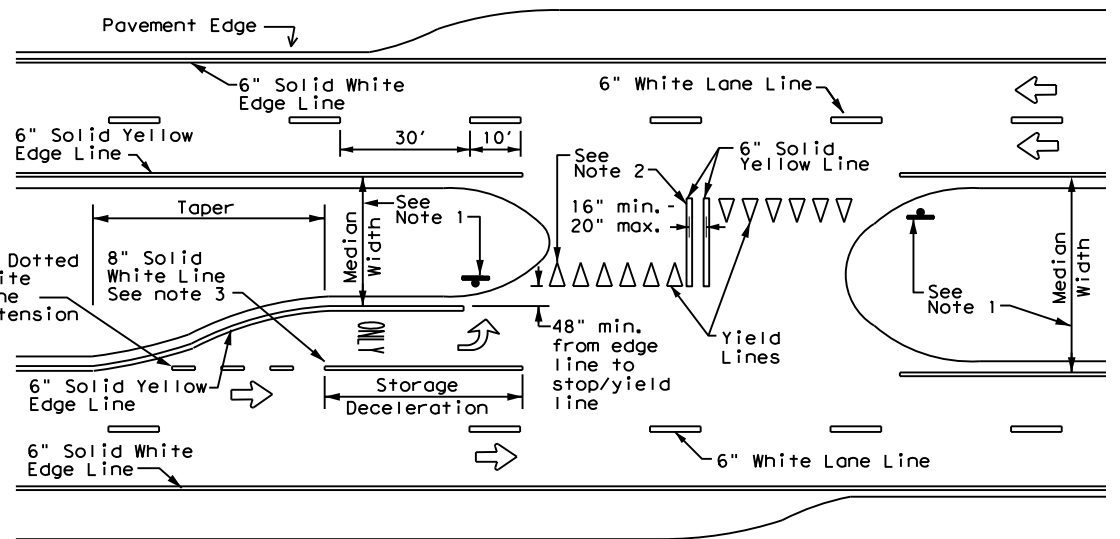
* 2" minimum for restripe projects when approved by the Engineer.



YIELD LINES



For posted speed on road
 being marked equal to or
 less than 40 MPH.



FOUR LANE DIVIDED ROADWAY CROSSOVERS

NOTES

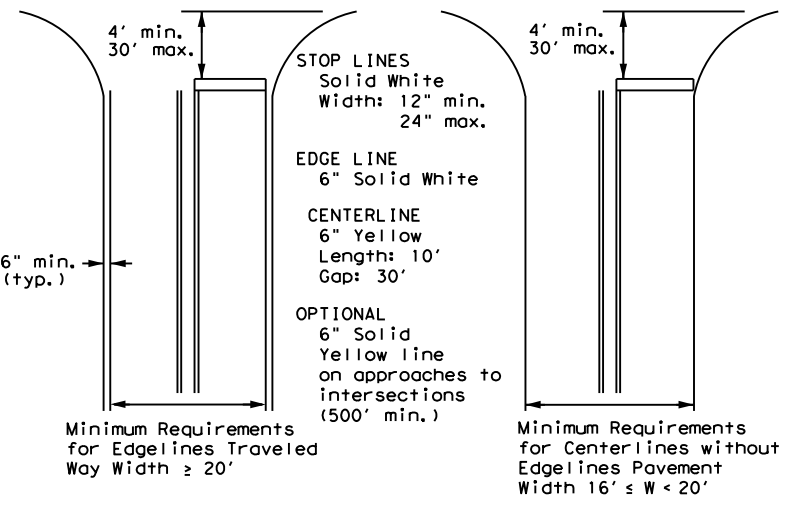
- Where divided highways are separated by median widths at the median opening itself of 30 feet or more, median openings shall be signed as two separate intersections. Each median opening has two width measurements, with one measurement for each approach. The narrow median width will be the controlling width to determine if signs are required. Yield signs are the typical intersection control. Stop signs and stop bars are optional as determined by the Engineer.
- Install median striping (double yellow centerlines and stop lines/yield lines) when a 50' or greater median centerline can be placed. Stop lines shall only be used with stop signs. Yield lines shall only be used with yield signs.
- Length of turn bays, including taper, deceleration, and storage lengths shall be as shown on the plans or as directed by the Engineer.

GENERAL NOTES

- Edge line striping shall be as shown in the plans or as directed by the Engineer. The edge line should not be placed less than 6 inches from the edge of pavement. This distance may vary due to pavement raveling or other conditions. Edge lines are not required in curb and gutter sections of roadways.
- The traveled way includes only that portion of the roadway used for vehicular travel. It does not include the parking lanes, sidewalks, berms and shoulders. The traveled ways shall be measured from the center of edge line to the center of edge line of a two lane roadway.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.



NOTE: Traveled way is exclusive of shoulder widths. Refer to General Note 2 for additional details.

**GUIDE FOR PLACEMENT OF STOP LINES,
EDGE LINE & CENTERLINE**
 Based on Traveled Way and Pavement Widths
 for Undivided Roadways

Texas Department of Transportation
 Traffic Safety Division Standard

**TYPICAL STANDARD
PAVEMENT MARKINGS**

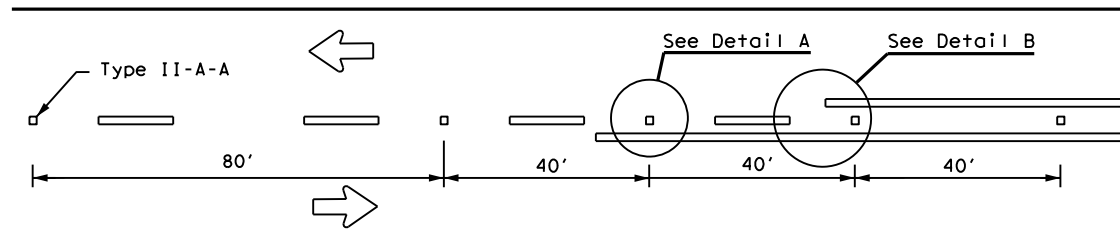
PM(1) - 22

FILE: pml-22.dgn	DN:	CK:	DW:	CK:
© TxDOT December 2022	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
11-78 8-00 6-20	DIST	COUNTY	SHEET NO.	
8-95 3-03 12-22	BWD	COLEMAN CO.	121	
5-00 2-12				

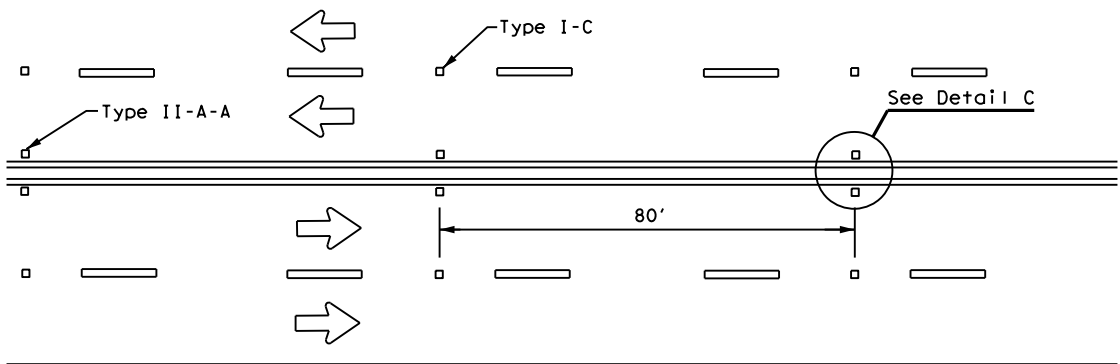
22A

REFLECTIVE RAISED PAVEMENT MARKERS FOR VEHICLE POSITIONING GUIDANCE

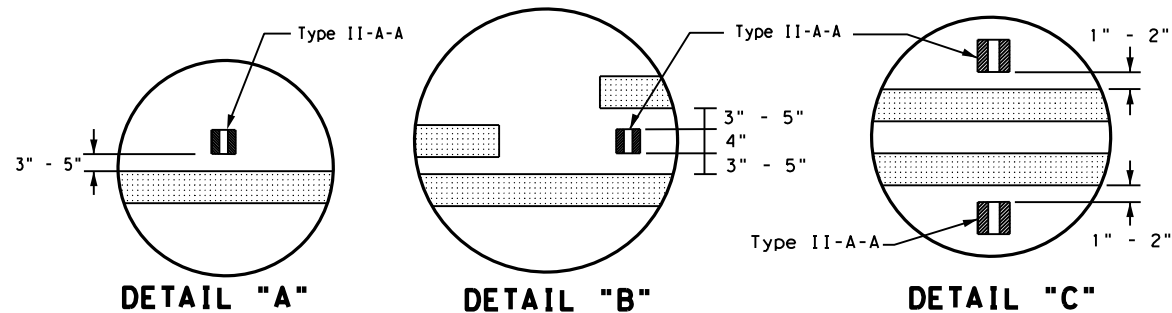
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to any other units of measurement.



CENTERLINE FOR ALL TWO LANE TWO-WAY ROADWAYS



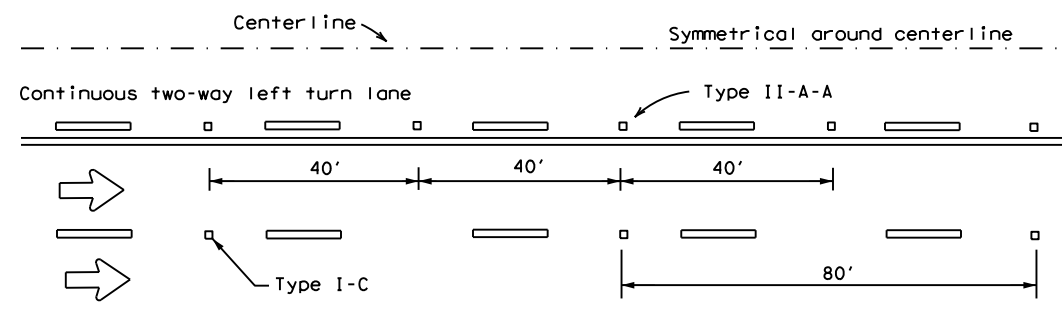
**CENTERLINE & LANE LINES
FOR FOUR LANE TWO-WAY ROADWAYS**



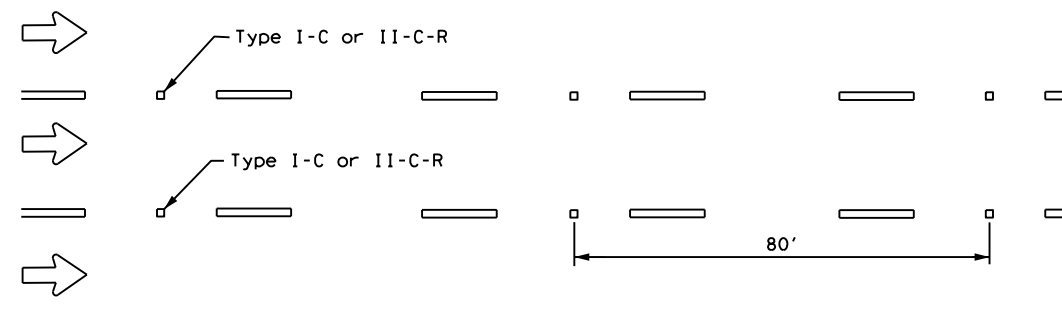
DETAIL "A"

DETAIL "B"

DETAIL "C"

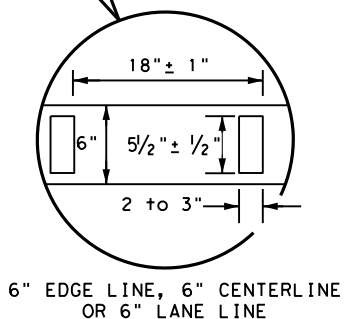
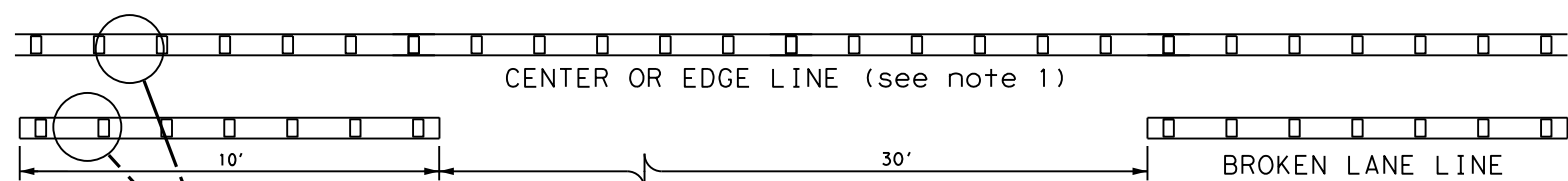


CENTERLINE AND LANE LINES FOR TWO-WAY LEFT TURN LANE



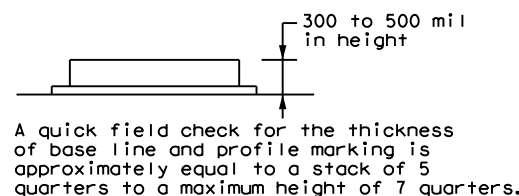
LANE LINES FOR ONE-WAY ROADWAY (NON-FREEWAY FACILITIES)

Raised pavement markers Type II-C-R shall have clear face toward normal traffic and red face toward wrong-way traffic.
 See Note 3.



**REFLECTORIZED PROFILE
PATTERN DETAIL**

USING REFLECTIVE PROFILE PAVEMENT MARKINGS



NOTES

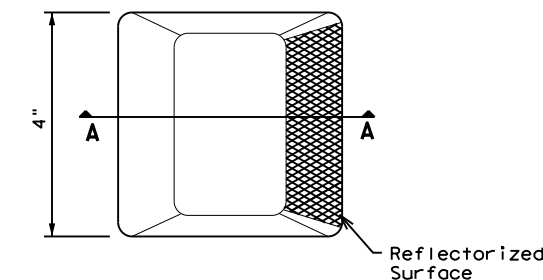
1. Edge lines should typically be 6" wide and the materials shall be specified in the plans.
2. Profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.

GENERAL NOTES

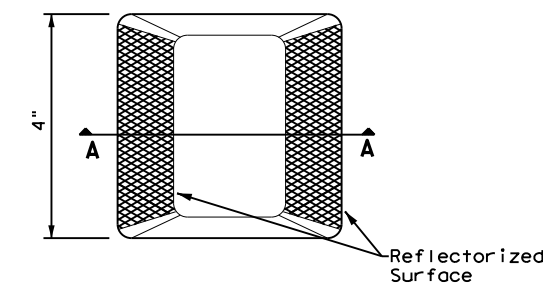
1. All raised pavement markers placed along broken lines shall be placed in line with and midway between the stripes.
2. On concrete pavements the raised pavement markers should be placed to one side of the longitudinal joints.
3. Use raised pavement marker Type I-C with undivided roadways, flush medians and two way left turn lanes. Use raised pavement marker Type II-C-R with divided highways and raised medians.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

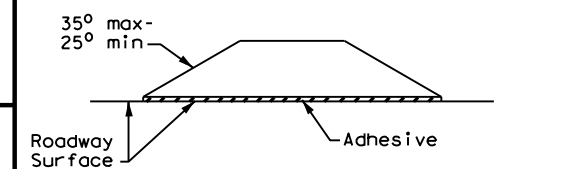
All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.



Type I (Top View)



Type II (Top View)



SECTION A

RAISED PAVEMENT MARKERS

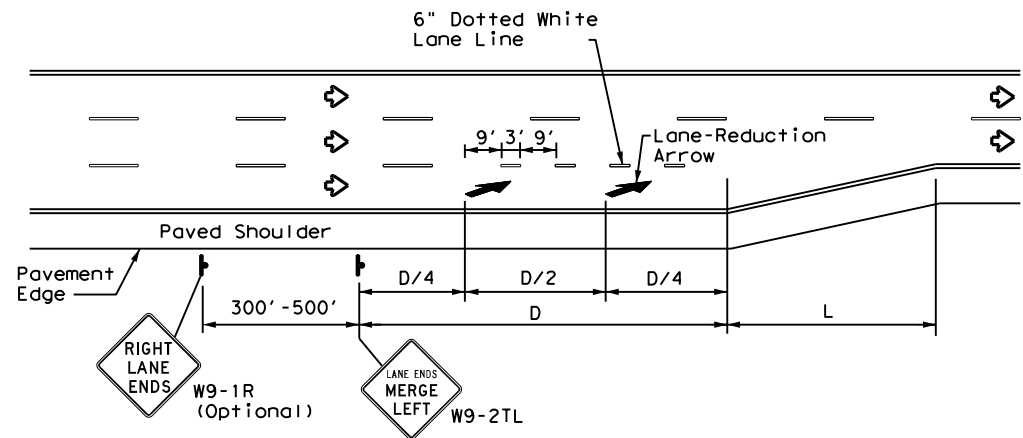


**POSITION GUIDANCE USING
RAISED MARKERS
REFLECTORIZED PROFILE
MARKINGS
PM(2) - 22**

FILE: pm2-22.dgn	DN:	CK:	DW:	CK:
© TxDOT December 2022	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
4-77 8-00 6-20	DIST	COUNTY	SHEET NO.	
4-92 2-10 12-22	BWD	COLEMAN CO.	122	
5-00 2-12				

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DATE: 2/22/2023 3:50:17 PM
 FILE: T:\BWDSDTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to add left turn lanes and right turn lanes



LANE REDUCTION

NOTES

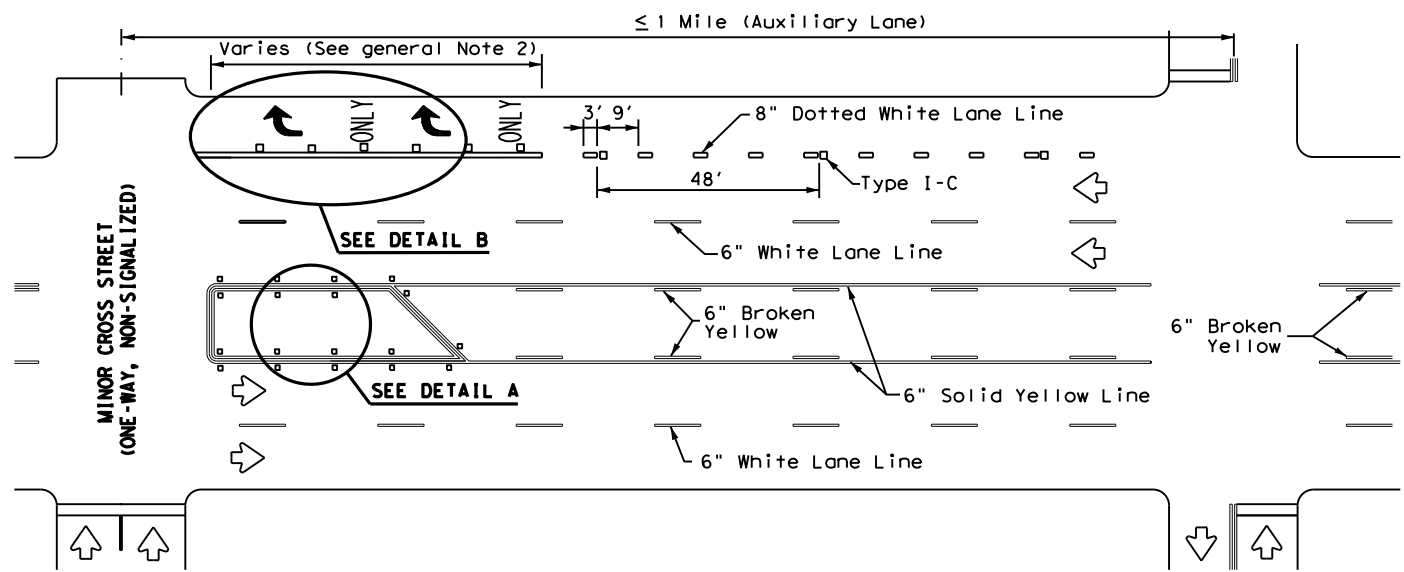
- Lane reduction pavement markings are used where the number of through lanes is reduced because of narrowing of the roadway or because of a section of on-street parking in what would otherwise be a through lane. For Texas Super 2 Passing Lanes, see TS2(PL) standard sheets.
- On divided highways, an additional RIGHT LANE ENDS (W9-1R) sign may be installed in the median aligned with the W9-1R sign on the right side of the highway.
- Lane reduction arrows are required for speeds of 45 mph or greater. An optional third lane reduction arrow may be added based on engineering judgement. If used, the optional third lane reduction arrow should be centered between the first and last lane reduction arrows.
- For lane reductions on Freeways and Expressways, signing shall conform to the TxDOT Freeway Signing Handbook.

GENERAL NOTES

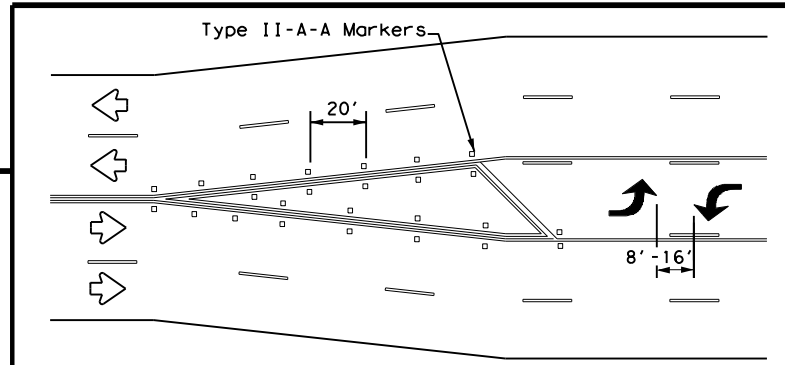
- Lane use word and arrow markings shall be used where through lanes approaching an intersection become mandatory turn lanes. Lane use word and arrow markings should be used in auxiliary lanes of substantial length. Lane use arrow markings or word and arrow markings may be used in other lanes and turn bays for emphasis. Details for words and arrows are as shown in the Standard Highway Sign Designs for Texas.
- When lane-use words and arrow markings are used, two sets of arrows should be used if the length of the bay is greater than 180 feet. When a single lane use arrow or word and arrow marking is used for a short turn lane, it should be located at or near the upstream end of the full-width turn lane.
- Use raised pavement marker Type I-C with undivided highways, flush medians and two way left turn lanes. Use raised pavement marker Type II-C-R with divided highways and raised medians.
- Length of turn bays, including taper, deceleration, and storage lengths shall be as shown on the plans or as directed by the Engineer. See Chapter 3 of the Roadway Design Manual for additional information on turning lanes or storage lengths.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.

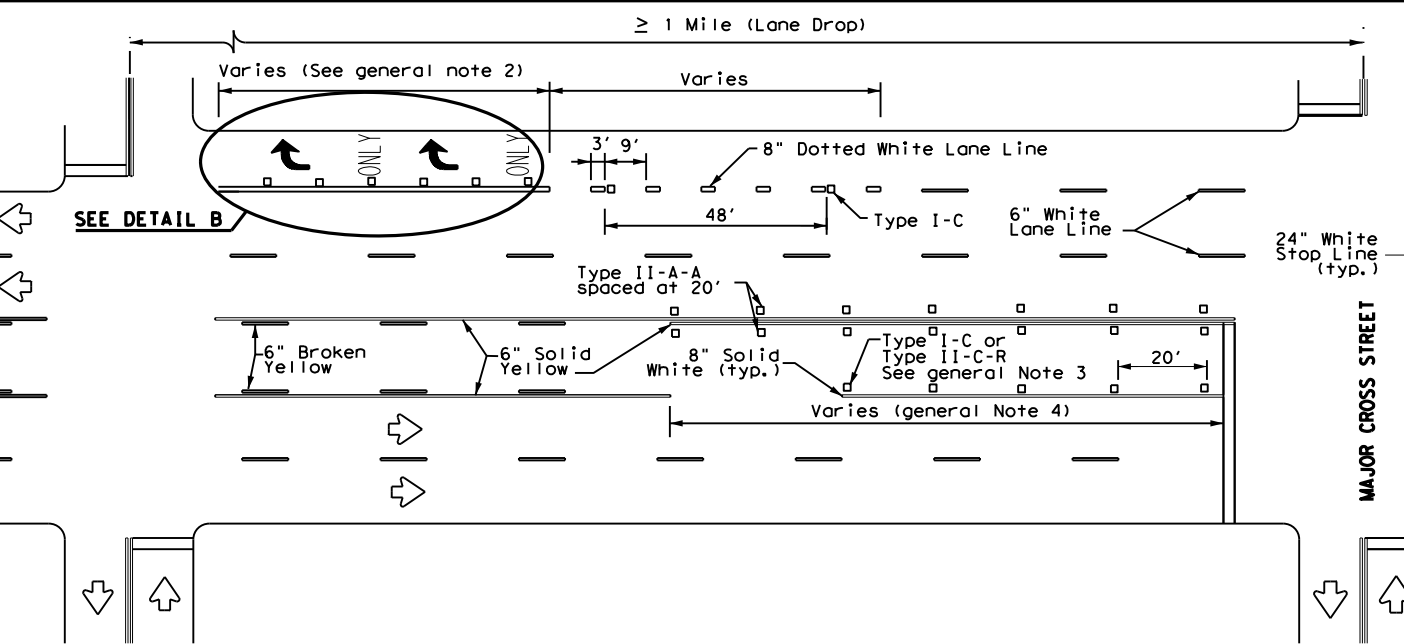


TYPICAL TWLTL AT ONE-WAY STREET AND RIGHT TURN AUXILIARY LANE

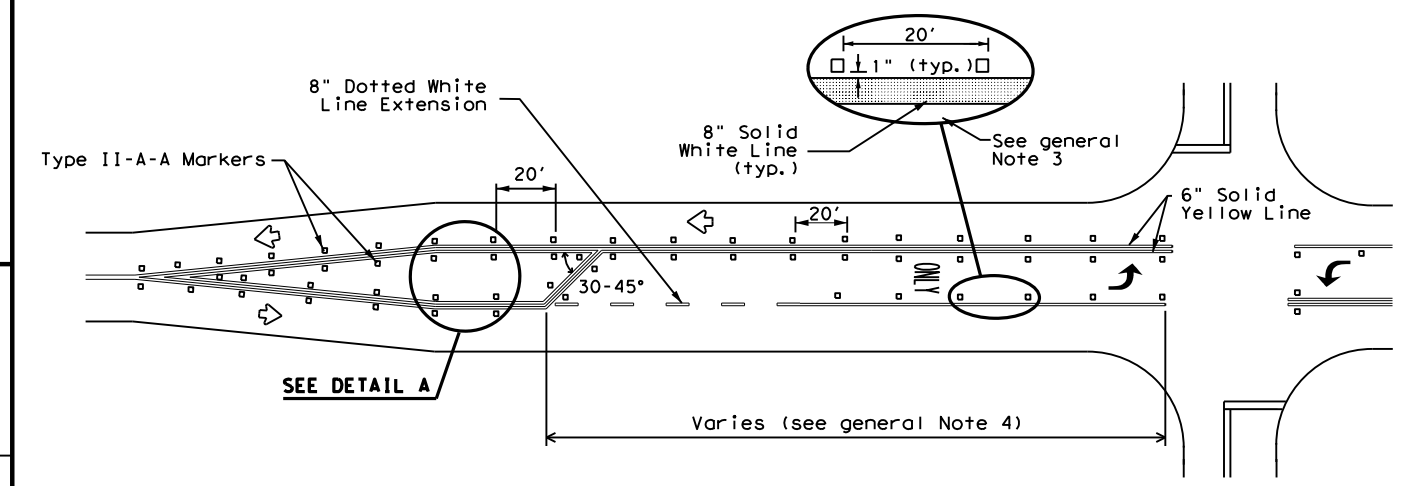


A two-way left-turn (TWLTL) lane-use arrow pavement marking should be used at or just downstream from the beginning of a two-way left-turn lane within a corridor. Repeating the marking after each intersection or dedicated turn bay is not required unless stated elsewhere in the plans.

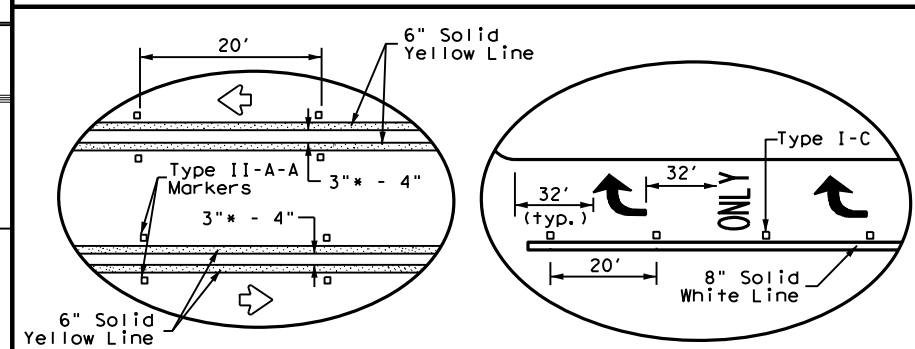
TYPICAL TRANSITION FOR TWLTL AND DIVIDED HIGHWAY



TYPICAL TWLTL AT TWO-WAY CROSS STREET AND RIGHT TURN LANE DROP



TYPICAL TWO-LANE ROADWAY INTERSECTION WITH LEFT TURN BAYS



DETAIL A

DETAIL B

* 2" minimum allowed for restripe projects when approved by the Engineer.

Texas Department of Transportation
 Traffic Safety Division Standard

TWO-WAY LEFT TURN LANES, RURAL LEFT TURN BAYS, AND LANE REDUCTION PAVEMENT MARKINGS PM(3) - 22

FILE: pm3-22.dgn	DN:	CK:	DW:	CK:
© TxDOT December 2022	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
4-98 3-03 6-20	DIST	COUNTY	SHEET NO.	
5-00 2-10 12-22	BWD	COLEMAN CO.	123	
8-00 2-12				

Prepared by: Andrew Chisholm DATE: 1/11/2023 1:05:08 PM FILE: I:\BDD\SS\TEAM_Design_Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\6.Environmental\EPIC_005404123.dgn UPDATED 6/1/2017

During the planning phase of project development the following environmental permits, issues, and commitments have been developed during coordination with resource agencies, local governmental entities, and the general public. Any change orders and/or deviations from the final design must be reported to the Engineer prior to the commencement of construction activities, as additional environmental clearances may be required.

I. Clean Water Act, Sec. 402 Texas Pollutant Discharge Elimination System

*(Addresses CGP and MS4 Storm Water requirements for the project.)
 (In the event that the Contractor implements a PSL on or within one mile of the project, a Site Notice and/or a NOI will apply.)*

No Action Required Required Action

Action No. 1
 The project disturbs five or more acres of surface area: TxDOT must file a NOI and coordinate with TCEQ for CGP. The contractor is responsible for the PSL as defined in the Standard Specifications for Construction and Maintenance of Highways, Streets, and Bridges (2014 Edition, Section 7.7.6, Page 42). The total disturbed acreage is the combined acreage to be disturbed on the project and the contractor's PSL. This includes, as required, posting a site notice and NOI for the PSL.

Commitment No. 1
 Comply with TPDES CGP. The project requires that a NOI and a Large Site Notice be posted. TxDOT must file an NOI with TCEQ and send a copy to any non-TxDOT MS4 operator that receives discharge from the project. Implement and maintain the SW3P. Refer to the SW3P Plan Sheet, BMPs, and Detail.

Commitment No. 2
 The contractor must stabilize the project site as stated in the SW3P.

MS4 operators that receive discharge from the project: -N/A-

Action No. 2
 TxDOT must file a NOT for the project when final stabilization has been achieved.

II. Clean Water Act, Section 401 and 404 Compliance

*(Addresses Nationwide Permits, Individual Permits, and Wetlands.)
 (Filling, dredging, or excavating in any water bodies, rivers, creeks, streams, wetlands, or wet area is prohibited unless specified in the USACE permit and approved by the Engineer.)
 (When temporary fills implemented, only stated TxDOT standards will be used unless written authorization for an alternative is obtained from the Engineer. No equipment is allowed in any stream channel below the Ordinary High Water Mark except on temporary stream crossings or drill pads.)*

No Action Required 404 Permit and 401 Certification Required

Permit	Required Action	Waters of the US	App. Plan Sheet(s)
NA	NA	NA	NA

Best Management Practices for applicable 401 General Conditions:

General Condition 12 - Categories I and II BMPs required

Category I (Erosion Control)

Temporary Vegetation Blankets, Matting

Mulch Sod

Interceptor Swale Diversion Dike

Erosion Control Compost Mulch Filter Berms and Socks

Compost Filter Berms and Socks Compost Blankets

Category II (Sedimentation Control)

Sand Bag Berm Rock Berm

Silt Fence Hay Bale Dike

Triangular Filter Dike Brush Berms

Stone Outlet Sediment Traps Sediment Basins

Erosion Control Compost Mulch Filter Berms and Socks

Compost Filter Berms and Socks

General Condition 25 - Category III BMPs required

Category III (Post-Construction TSS Control)

Retention/Irrigation Constructed Wetlands

Extended Detention Basin Wet Basins

Vegetative Filter Strips Vegetation-Lined Ditches

Grassy Swales Sand Filter Systems

Erosion Control Compost Mulch filter Berms and Socks

Compost Filter Berms and Socks Sedimentation Chambers

III. Cultural Resources

*(Addresses any special circumstances associated with cultural resources, such as archeological or historic sites.)
 (Upon discovery of archeological artifacts (bones, burnt rock, flint, pottery, etc.) cease work in the immediate area and contact the Engineer immediately.)*

No Action Required Required Action

Action No.	Station (R/L/I)	Commitment
1.	---	---

IV. Vegetation Resources

(Addresses any special circumstances associated with vegetation, such as large trees to be avoided, or mitigation that will occur as part of the project.)

No Action Required Required Action

Action No.	Station (R/L/I)	Commitment
1.	All	Avoid non-mow locations for stockpiles and equipment parking/storage.
2.	Project Limits	Preserve native vegetation to the extent practical. Contractor must adhere to Construction Specification Requirements Specs 162, 164, 192, 193, 506, 730, 751, 752 in order to comply with requirements for invasive species, beneficial landscaping, and tree/brush removal commitments.

V. Federal Listed, Proposed, Threatened, Endangered Species, Critical Habitat, State Listed Species, Candidate Species, and Migratory Bird Treaty Act (MBTA)

(Addresses any special habitat that may need to be avoided, lists any threatened or endangered species whose habitat was observed and might be impacted within the project area, and lists any precautions such as nesting seasons for migratory birds.)

No Action Required Required Action

Species Potentially within Project Area & Description	Habitat Description
Construction personnel are advised of the potential to encounter various species during construction. Avoid harm to all species if encountered and contact the District Environmental Coordinator, Andrew Chisholm, office: 325-643-0442 or cell: 325-203-0414 if impacts to species are anticipated.	

The Migratory Bird Treaty Act of 1918 states that it is unlawful to kill, capture, collect, possess, buy, sell, trade, or transport any migratory bird, nest, young, feather, or egg in part or in whole, without a federal permit issued in accordance with the Act's policies and regulations. Migration patterns would not be affected by the proposed project. The contractor will remove all old migratory bird nests from any structure where work would be done from September 1 through the end of February. In addition, the contractor will be prepared to prevent migratory birds from building nests between March 1 and August 31, per the Environmental Permits, Issues, and Commitments (EPIC) plans. In the event that migratory birds are encountered on-site during project construction, adverse impacts on protected birds, active nests, eggs, and/or young shall be avoided.

VI. Hazardous Material or Contamination Issues

(Addresses any previously identified high risk sites associated with hazardous materials that may be encountered during construction.)

Comply with the Hazard Communication Act (the Act) for personnel who will be working with hazardous materials by conducting safety meetings prior to beginning construction and making workers aware of potential hazards in the workplace. Ensure that all workers are provided with personal protective equipment appropriate for any hazardous materials used.

Obtain and keep on-site Material Safety Data Sheets (MSDS) for all hazardous products used on the project, which may include, but are not limited to the following categories: Paints, acids, solvents, asphalt products, chemical additives, fuels and concrete curing compounds or additives. Provide protected storage, off bare ground and covered, for products which may be hazardous. Maintain product labeling as required by the Act.

Maintain an adequate supply of on-site spill response materials, as indicated in the MSDS. In the event of a spill, take actions to mitigate the spill as indicated in the MSDS, in accordance with safe work practices, and contact the District Spill Coordinator immediately. The Contractor shall be responsible for the proper containment and cleanup of all product spills.

Contractor will follow all applicable storage and management requirements for liquid oil products, liquid petroleum products, and other chemical liquids as per 40 CFR 112 (a.k.a. SPCC) and/or TCEQ Construction General Permit for storm water management.

Contact the Engineer if any of the following are detected:

- Dead or distressed vegetation (not identified as normal)
- Trash piles, drums, canisters, barrels, etc.
- Undesirable smells/odors
- Underground storage tanks
- Evidence of leaching or seepage of substances
- Any other evidence indicating possible hazardous materials or contamination discovered on-site

Does the project involve any bridge class structure rehabilitation or replacements (bridge class structure not including box culverts)?

Yes No

If "No", then no further action is required.

If "Yes", then TxDOT is responsible for completing an asbestos assessment/inspection. Are the results of the asbestos inspection positive (is asbestos present)?

Yes No

If "Yes", then TxDOT must retain a Texas Department of State Health Services (DSHS) licensed asbestos consultant to assist with the notification, develop abatement/mitigation procedures, and perform management activities as necessary. The notification form to DSHS must be postmarked at least 15 working days prior to scheduled abatement and/or demolition.

If "No", then TxDOT is still required to notify DSHS 15 working days prior to any scheduled demolition.

In either case, the Contractor is responsible for providing the date(s) for abatement activities and/or demolition with careful coordination between the Engineer and asbestos consultant in order to minimize construction delays and subsequent claims.

Bridges on this project may contain Lead-Containing Paint (LCP) or other items that contain lead. The location of (LCP) is identified in the General Notes, Item 6.10.1.2 in the 2014 TxDOT Standard Specifications shall be utilized for this project.

VII. Other Environmental Issues

(Addresses any other environmental issues that may not have been covered in other sections.)


No Action Required Required Action

Action No.	Station (R/L/I)	Commitment
1.	---	---

LIST OF ABBREVIATIONS

BMP: Best Management Practice
 CGP: Construction General Permit
 DSHS: Texas Department of State Health Services
 FEMA: Federal Emergency Management Agency
 FHWA: Federal Highway Administration
 MOA: Memorandum of Agreement
 MOU: Memorandum of Understanding
 MS4: Municipal Separate Stormwater Sewer System
 MBTA: Migratory Bird Treaty Act
 NOI: Notice of Intent
 NOT: Notice of Termination
 NWP: Nationwide Permit
 SPCC: Spill Prevention Control and Countermeasure
 SW3P: Storm Water Pollution Prevention Plan
 PCN: Pre-Construction Notification
 PSL: Project Specific Location
 TCEQ: Texas Commission on Environmental Quality
 TPDES: Texas Pollutant Discharge Elimination System
 TPWD: Texas Parks and Wildlife Department
 TxDOT: Texas Department of Transportation
 T&E: Threatened and Endangered Species
 USACE: U.S. Army Corp of Engineers
 USFWS: U.S. Fish and Wildlife Service

US 84 ENVIRONMENTAL PERMITS, ISSUES, AND COMMITMENTS (EPIC)


 TEXAS DEPARTMENT OF TRANSPORTATION
 BROWNWOOD DISTRICT

CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		124

STORMWATER POLLUTION PREVENTION PLAN (SWP3):

This SWP3 has been developed in accordance with the TPDES Construction General Permit TXR150000 (CGP). The Texas Department of Transportation (TxDOT) ensures that project specifications include adequate best management practices (BMPs) for this project.

For all projects with any soil disturbing activities, TxDOT will maintain a SWP3 with all pertinent records, correspondence, environmental documents, etc. at the project field office. If no field office is available, then this SWP3 shall be kept in the appropriate TxDOT Area Office.

This SWP3 is consistent with requirements specified in applicable stormwater plans and the projects environmental permits, issues, and commitments (EPICs). A copy of the CGP is included in Attachment 2.12 of the SWP3 binder.

1.0 SITE/PROJECT DESCRIPTION

1.1 PROJECT CONTROL SECTION JOB (CSJ):

0054-04-123

1.2 PROJECT LIMITS:

From: .2 miles north of FM 568

To: .2 south of CR 196

1.3 PROJECT COORDINATES:

BEGIN: (Lat) 31.815367, (Long) -99.400272

END: (Lat) 31.799077, (Long) -99.387016

1.4 TOTAL PROJECT AREA (Acres): 25.5 acres

1.5 TOTAL AREA TO BE DISTURBED (Acres): 7.0 acres

1.6 NATURE OF CONSTRUCTION ACTIVITY:

WIDEN PAVED SHOULDER, INSTALL CONTINUOUS TURN LANE

1.7 MAJOR SOIL TYPES:

Soil Type	Description
Callahan Loam 1 to 3% slopes	80% clay, well drained, high rate of runoff
Rowena Clay Loam 1 to 3% slopes	90% clay, well drained, medium rate of runoff

1.8 PROJECT SPECIFIC LOCATIONS (PSLs):

PSLs must be depicted on the Environmental Layout Sheets in Attachment 1.2 of this SWP3. PSLs may be identified during preconstruction meetings or during the construction process. Please choose from the options below:

- PSLs determined during preconstruction meeting
- PSLs determined during construction
- No PSLs planned for construction

Type	Sheet #s

All off-ROW PSLs required by the Contractor are the Contractor's responsibility. The Contractor shall secure all permits required by local, state, federal laws for off-ROW PSLs. The contractor shall provide diagrams, areas of disturbance, acreage, and BMPs for all off-ROW PSLs within one mile of the project.

1.9 CONSTRUCTION ACTIVITIES:

(Use the following list as a starting point when developing the Construction Activity Schedule and Ceasing Record in Attachment 2.5.)

- X Mobilization
- X Install sediment and erosion controls
- X Blade existing topsoil into windrows, prep ROW, clear and grub
- X Remove existing pavement
- X Grading operations, excavation, and embankment
- X Excavate and prepare subgrade for proposed pavement widening
- X Remove existing culverts, safety end treatments (SETs)
- X Remove existing metal beam guard fence (MBGF), bridge rail
- X Install proposed pavement per plans
- X Install culverts, culvert extensions, SETs
- X Install mow strip, MBGF, bridge rail
- X Place flex base
- X Rework slopes, grade ditches
- X Blade windrowed material back across slopes
- X Revegetation of unpaved areas
- X Achieve site stabilization and remove sediment and erosion control measures
- Other: _____
- Other: _____
- Other: _____

1.10 POTENTIAL POLLUTANTS AND SOURCES:

- X Sediment laden stormwater from stormwater conveyance over disturbed area
- X Fuels, oils, and lubricants from construction vehicles, equipment, and storage
- X Solvents, paints, adhesives, etc. from various construction activities
- X Transported soils from offsite vehicle tracking
- X Construction debris and waste from various construction activities
- X Contaminated water from excavation or dewatering pump-out water
- X Sanitary waste from onsite restroom facilities
- X Trash from various construction activities/receptacles
- X Long-term stockpiles of material and waste
- Other: _____
- Other: _____
- Other: _____

1.11 RECEIVING WATERS:

Receiving waters must be depicted on the Environmental Layout Sheets in Attachment 1.2 of this SWP3. Include Segment # for receiving waters.

Tributaries	Classified Waterbody
Lake Brownwood (1418)	

* Add (*) for impaired waterbodies with pollutant in ().

1.12 ROLES AND RESPONSIBILITIES: TxDOT

- X Development of plans and specifications
- X Submit Notice of Intent (NOI) to TCEQ (≥5 acres)
- X Post Construction Site Notice
- X Submit NOI/CSN to local MS4
- X Perform SWP3 inspections
- X Maintain SWP3 records and update to reflect daily operations
- X Complete and submit Notice of Termination to TCEQ
- X Maintain SWP3 records for 3 years
- Other: _____
- Other: _____
- Other: _____

1.13 ROLES AND RESPONSIBILITIES: CONTRACTOR

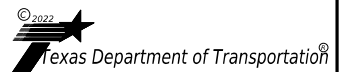
- X Day To Day Operational Control
- X Submit Notice of Intent (NOI) to TCEQ (≥5 acres)
- X Post Construction Site Notice
- X Submit NOI/CSN to local MS4
- X Maintain schedule of major construction activities
- X Install, maintain and modify BMPs
- X Complete and submit Notice of Termination to TCEQ
- X Maintain SWP3 records for 3 years
- Other: _____
- Other: _____
- Other: _____

1.14 LOCAL MUNICIPAL SEPARATE STORM SEWER SYSTEM (MS4) OPERATOR COORDINATION:

MS4 Entity

STORMWATER POLLUTION PREVENTION PLAN (SWP3)

Sheet 1 of 2



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	125	

STORMWATER POLLUTION PREVENTION PLAN (SWP3):

2.0 BEST MANAGEMENT PRACTICES (BMPs) AND CONTROLS, INSPECTION, AND MAINTENANCE

The Contractor shall be the responsible party for implementing the BMPs described herein and for complying with the SWP3 for control of erosion and sedimentation during day-to-day operations. The Contractor shall implement changes to this SWP3 approved by TxDOT within the times specified in this SWP3 or the CGP.

2.1 EROSION CONTROL AND SOIL STABILIZATION BMPs:

T / P

- Protection of Existing Vegetation
- Vegetated Buffer Zones
- Soil Retention Blankets
- Geotextiles
- Mulching/ Hydromulching
- Soil Surface Treatments
- Temporary Seeding
- Permanent Planting, Sodding or Seeding
- Biodegradable Erosion Control Logs
- Rock Filter Dams/ Rock Check Dams
- Vertical Tracking
- Interceptor Swale
- Riprap
- Diversion Dike
- Temporary Pipe Slope Drain
- Embankment for Erosion Control
- Paved Flumes
- Other: _____
- Other: _____
- Other: _____
- Other: _____

2.2 SEDIMENT CONTROL BMPs:

T / P

- Biodegradable Erosion Control Logs
- Dewatering Controls
- Inlet Protection
- Rock Filter Dams/ Rock Check Dams
- Sandbag Berms
- Sediment Control Fence
- Stabilized Construction Exit
- Floating Turbidity Barrier
- Vegetated Buffer Zones
- Vegetated Filter Strips
- Other: _____
- Other: _____
- Other: _____
- Other: _____

Refer to the Environmental Layout Sheets/ SWP3 Layout Sheets located in Attachment 1.2 of this SWP3

Sediment control BMPs requiring design capacity calculations (See SWP3 Attachment 1.3.):

T / P

- Sediment Trap
 - Calculated volume runoff from 2-year, 24-hour storm for each acre of disturbed area
 - 3,600 cubic feet of storage per acre drained
- Sedimentation Basin
 - Not required (<10 acres disturbed)
 - Required (>10 acres) and implemented.
 - Calculated volume runoff from 2-year, 24-hour storm for each acre of disturbed area
 - 3,600 cubic feet of storage per acre drained
 - Required (>10 acres), but not feasible due to:
 - Available area/Site geometry
 - Site slope/Drainage patterns
 - Site soils/Geotechnical factors
 - Public safety
 - Other: No new right of way

2.3 PERMANENT CONTROLS:

(Coordinate post-construction BMPs with appropriate TxDOT maintenance sections.)

BMPs To Be Left In Place Post Construction:

Type	Stationing	
	From	To

Refer to the Environmental Layout Sheets/ SWP3 Layout Sheets located in Attachment 1.2 of this SWP3

2.4 OFFSITE VEHICLE TRACKING CONTROLS:

- Excess dirt/mud on road removed daily
- Haul roads dampened for dust control
- Loaded haul trucks to be covered with tarpaulin
- Stabilized construction exit
- Other: _____
- Other: _____
- Other: _____
- Other: _____

2.5 POLLUTION PREVENTION MEASURES:

- Chemical Management
- Concrete and Materials Waste Management
- Debris and Trash Management
- Dust Control
- Sanitary Facilities
- Other: _____
- Other: _____
- Other: _____
- Other: _____

2.6 VEGETATED BUFFER ZONES:

Natural vegetated buffers shall be maintained as feasible to protect adjacent surface waters. If vegetated natural buffer zones are not feasible due to site geometry, the appropriate additional sediment control measures have been incorporated into this SWP3.

Type	Stationing	
	From	To
Erosion Control Logs	280+00	280+74
Erosion Control Logs	280+74	282+00

Refer to the Environmental Layout Sheets/ SWP3 Layout Sheets located in Attachment 1.2 of this SWP3

2.7 ALLOWABLE NON-STORMWATER DISCHARGES:

- Fire hydrant flushings
- Irrigation drainage
- Pavement washwater (where spills or leaks have not occurred, and detergents are not used)
- Potable water sources
- Springs
- Uncontaminated groundwater
- Water used to wash vehicles or control dust
- Other allowable non-stormwater discharges as allowed by TPDES GP TXR150000.

2.8 INSPECTIONS:

All disturbed areas and erosion and sediment control devices shall be inspected at least once every seven (7) days. Inspections shall be performed by TxDOT as indicated on the Field Inspection and Maintenance Report Form 2118 and retained in Attachment 2.5 of this SWP3 .

2.9 MAINTENANCE:

Control measures shall be properly installed according to specifications. If it is determined that a BMP or control measure is not operating effectively, maintenance must be accomplished as soon as possible and before the next anticipated rain event, but in no case later than 7 calendar days after being able to access the site. Maintenance shall be performed by the Contractor as indicated on the Field Inspection and Maintenance Report Form 2118 and retained in Attachment 2.5 of this SWP3.

STORMWATER POLLUTION PREVENTION PLAN (SWP3)

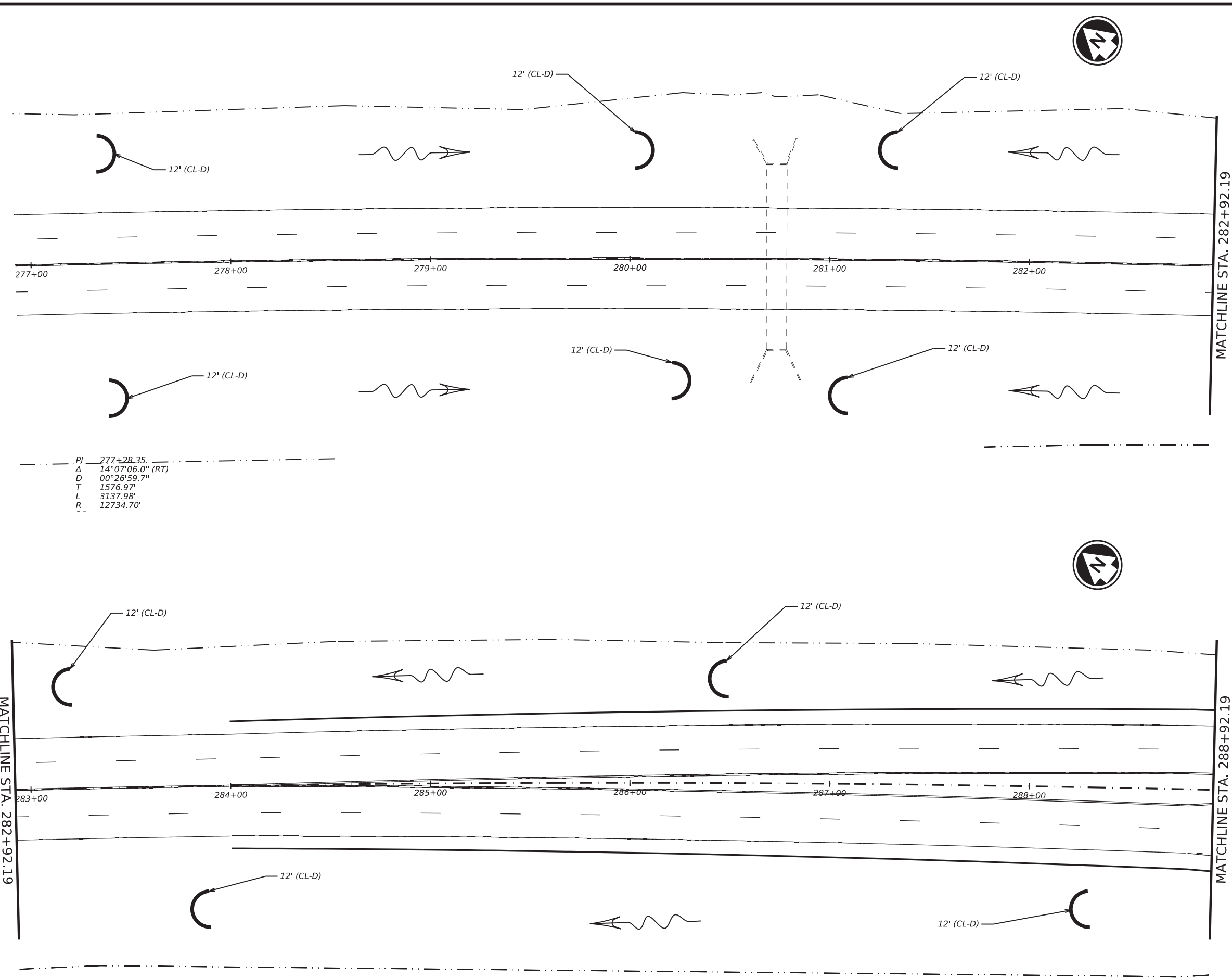
Sheet 2 of 2



CONT	SECT	JOB	HIGHWAY
0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		126

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PI 277+28.35
 Δ 14°07'06.0" (RT)
 D 00°26'59.7"
 T 1576.97'
 L 3137.98'
 R 12734.70'

LEGEND

- Ersn Cnt Logs (CL-D)
- Flow Direction
- Silt Fence SCF



01/26/2023

Texas Department of Transportation

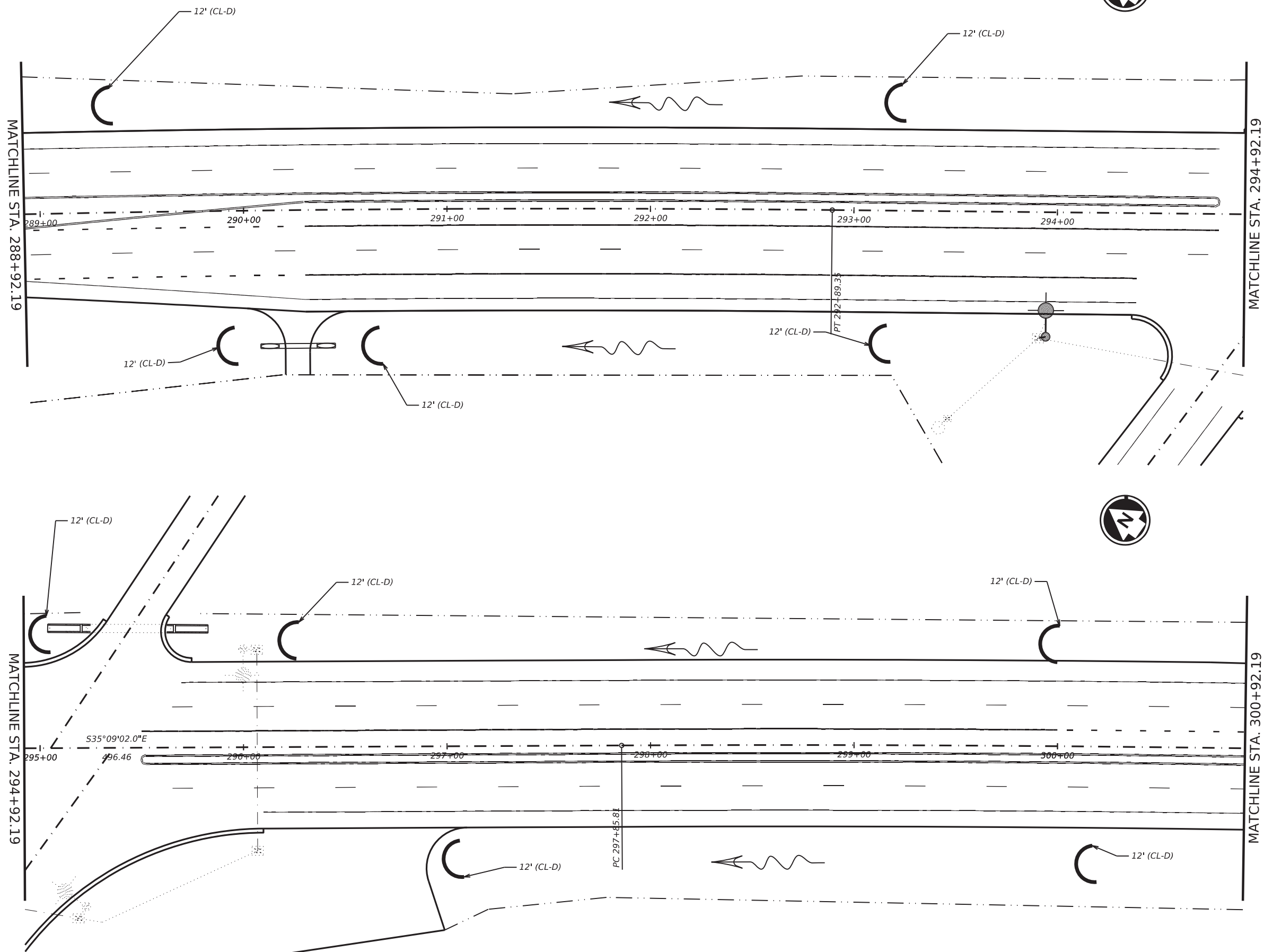
US 84
SW3P
LAYOUT

SHEET 1 OF 7

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- Erosn Cnt Logs (CL-D)
- Flow Direction
- Silt Fence SCF



01/26/2023

Texas Department of Transportation

US 84

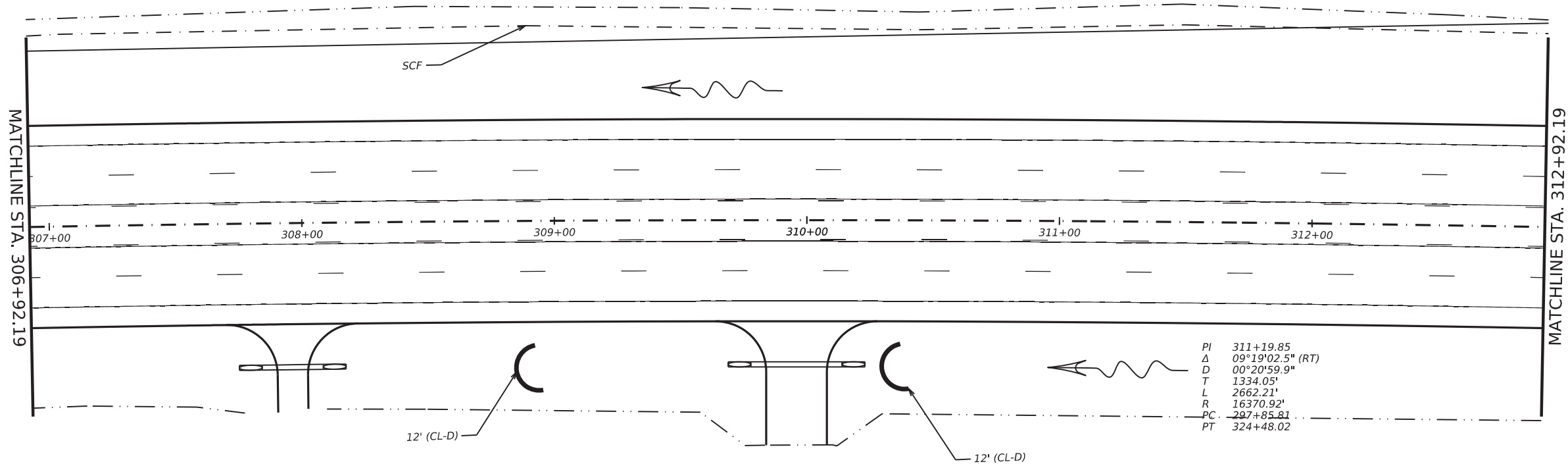
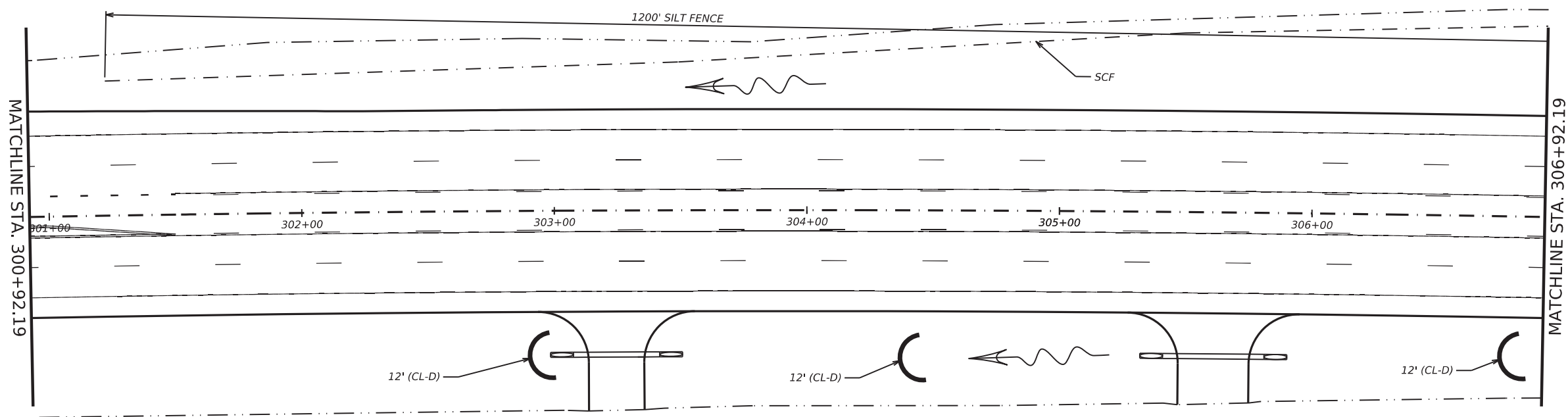
SW3P LAYOUT

SHEET 2 OF 7

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LEGEND

- Ersn Cnt Logs (CL-D)
- Flow Direction
- Silt Fence SCF



01/27/2023

Texas Department of Transportation

US 84

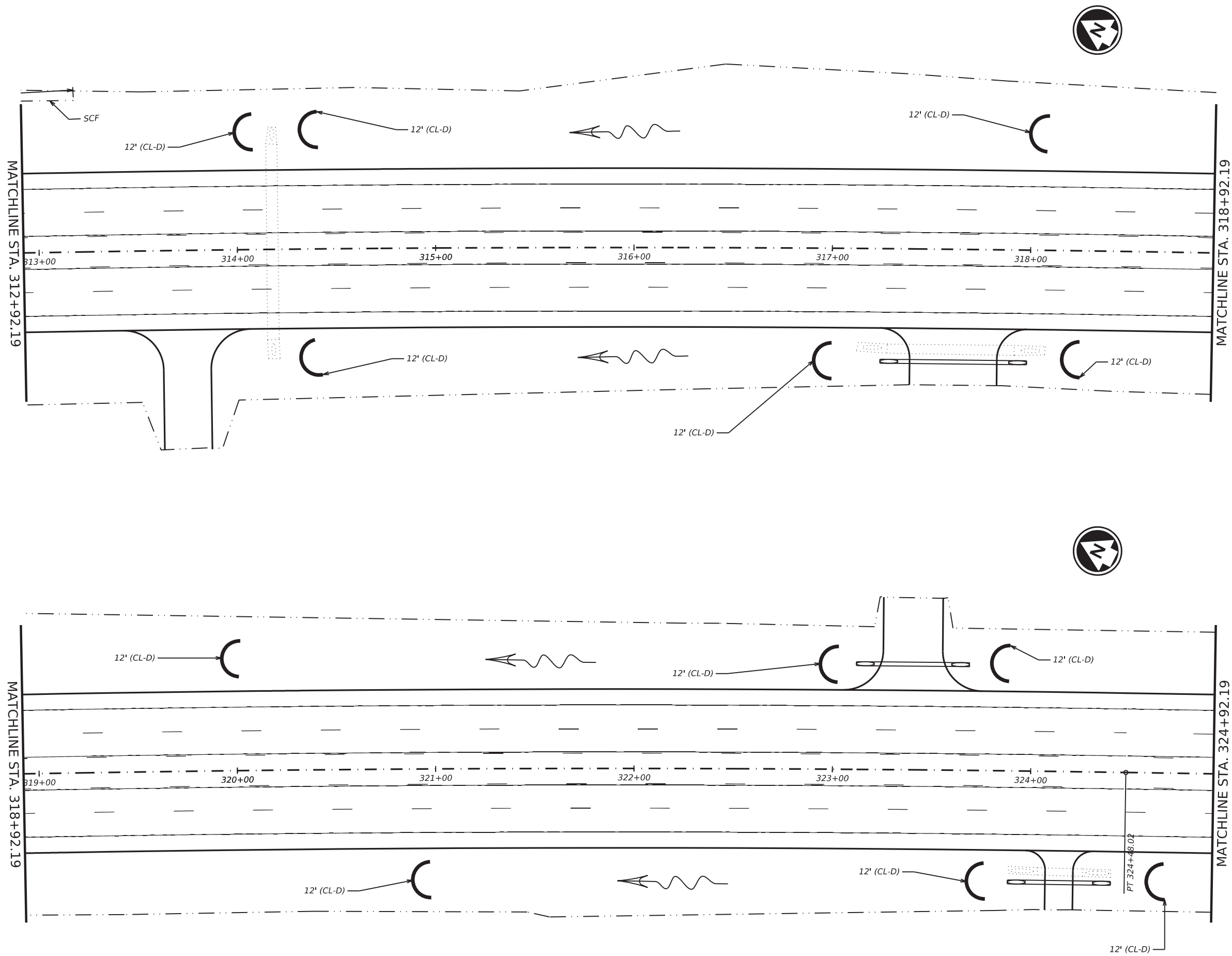
SW3P LAYOUT

SHEET 3 OF 7

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- Flow Direction
- Silt Fence SCF

01/26/2023

Texas Department of Transportation

US 84

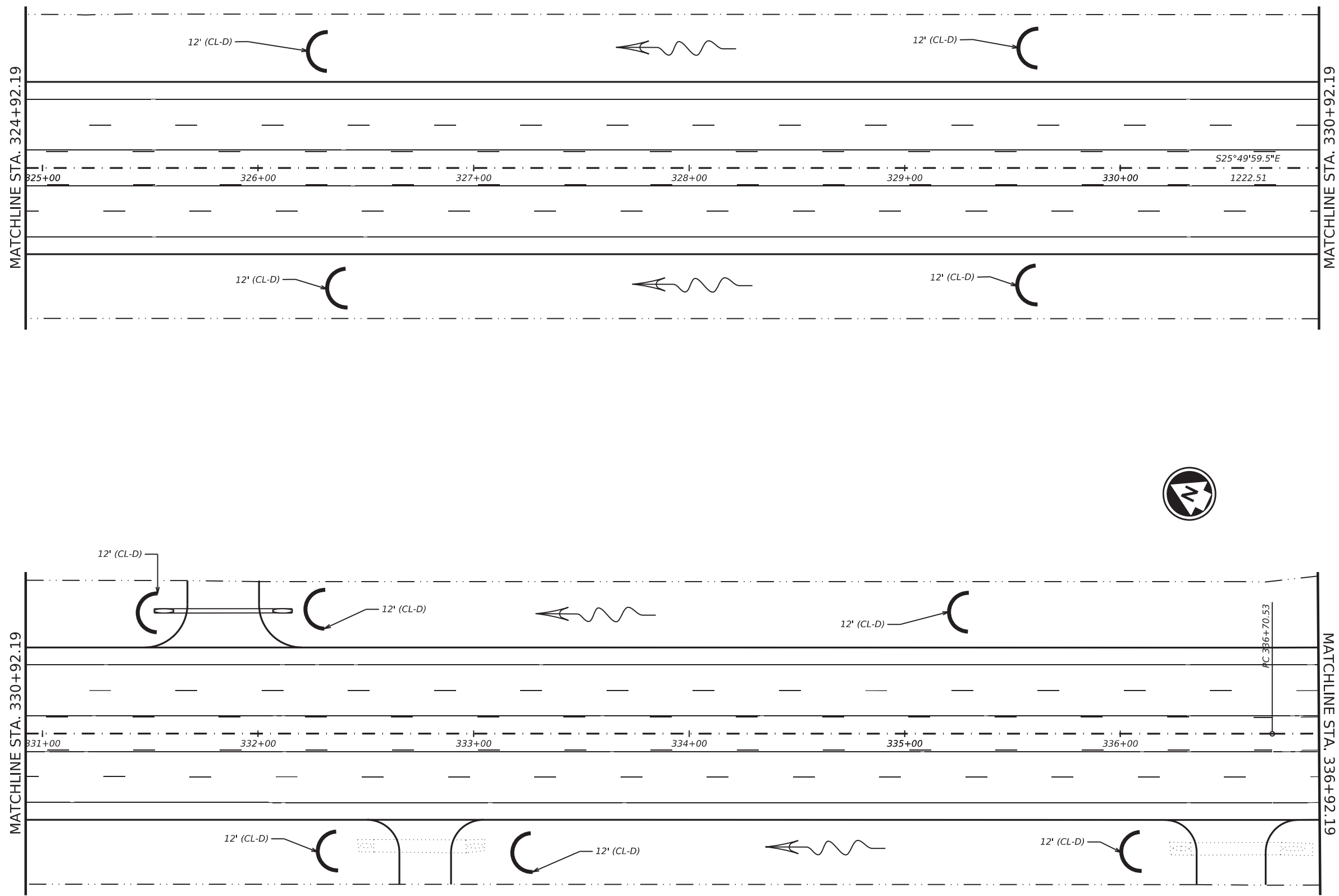
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SHEET 4 OF 7

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LEGEND

- Ersn Cnt Logs (CL-D)
- Flow Direction
- Silt Fence SCF

JH Scantling, P.E.

01/26/2023

Texas Department of Transportation

US 84

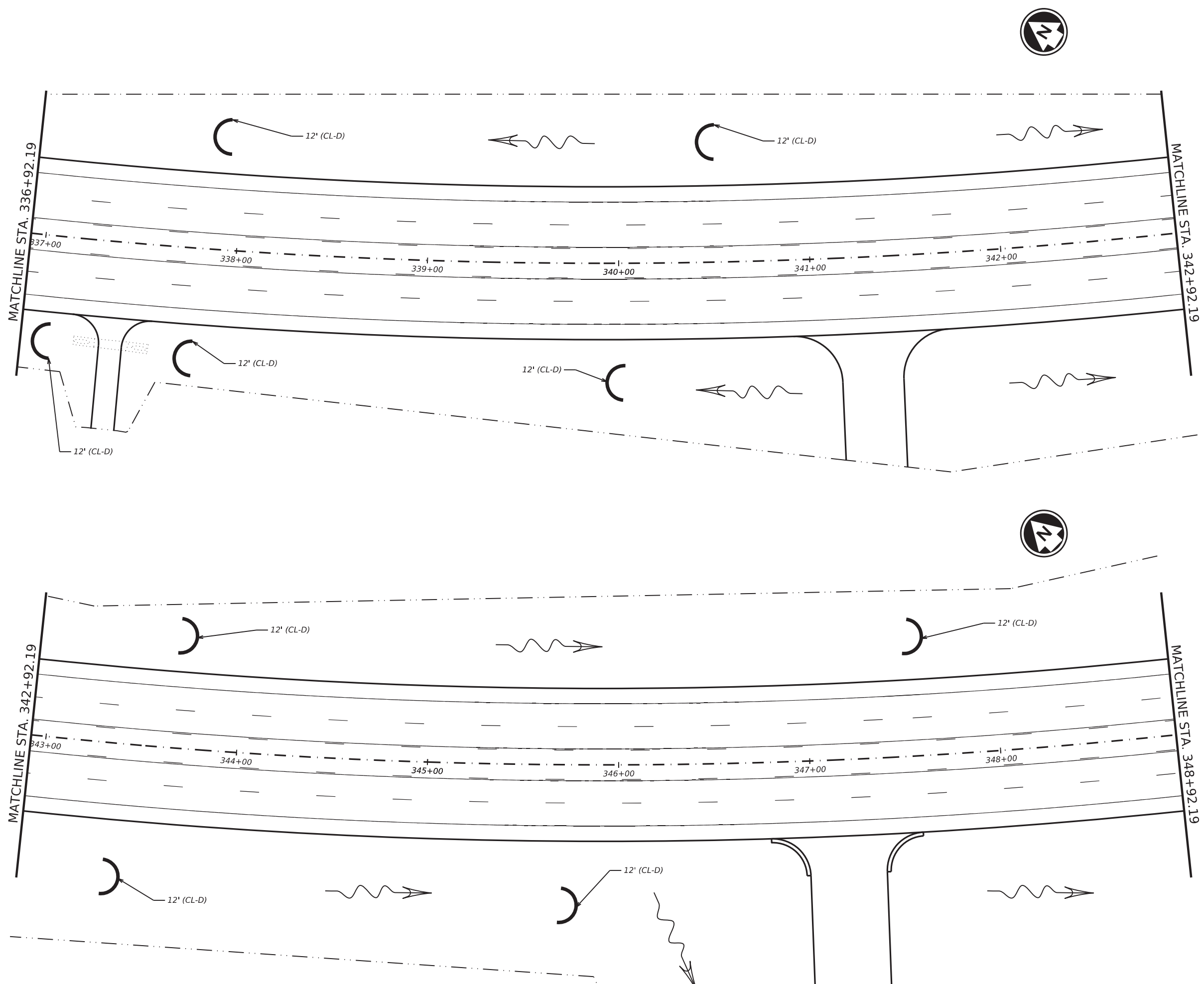
SW3P LAYOUT

SHEET 5 OF 7

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LEGEND

- Erosn Cnt Logs (CL-D)
- Flow Direction
- Silt Fence SCF

J.H. Scantling, P.E.

01/26/2023

Texas Department of Transportation

US 84

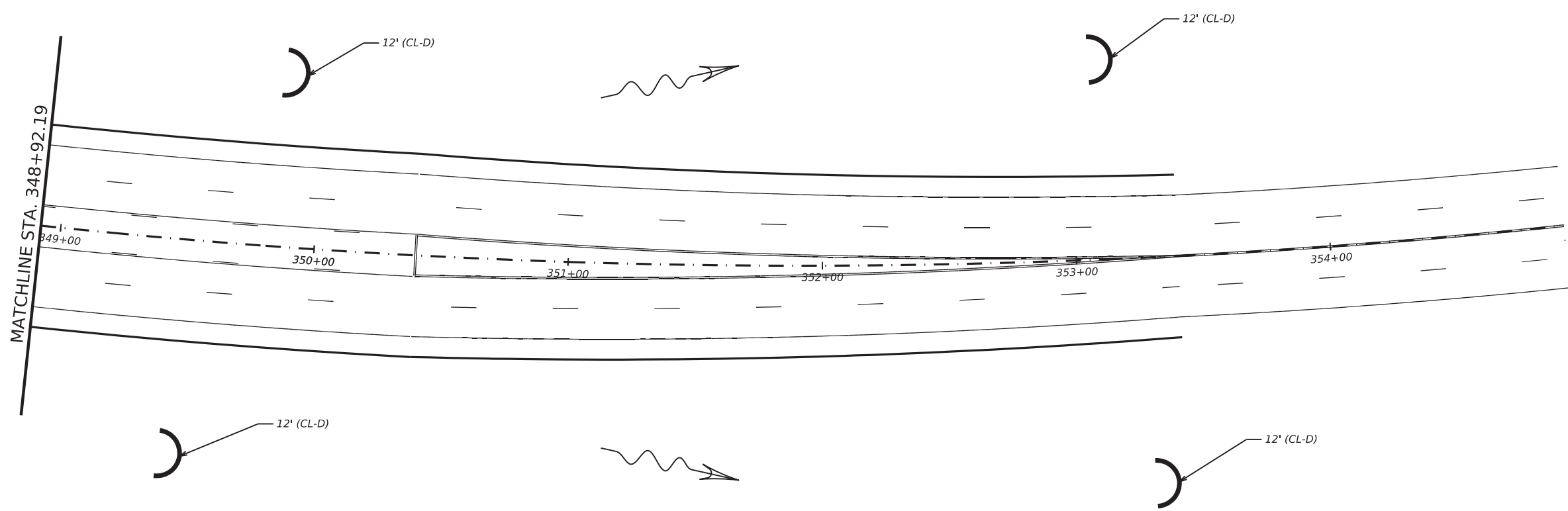
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SHEET 6 OF 7

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LEGEND

Erosn Cnt Logs (CL-D)

Flow Direction

Silt Fence SCF

CSJ - 0054-04-123				
ITEM	CODE	DESCRIPTION	QUANT	UNIT
164	6009	BROADCAST SEED (TEMP)(WARM)	16956.0	SY
164	6011	BROADCAST SEED (TEMP)(COOL)	16956.0	SY
168	6001	VEGETATIVE WATERING	757.0	MG
506	6038	TEMP SEDMT CONT FENCE (INSTALL)	1200.0	LF
506	6039	TEMP SEDMT CONT FENCE (REMOVE)	1200.0	LF
506	6041	BIODEG EROSN CONT LOGS (INSTL) (12")	696.0	LF
506	6043	BIODEG EROSN CONT LOGS (REMOVE)	696.0	LF

EXACT LOCATION & QUANTITIES OF SW3P ITEMS TO BE DETERMINED IN THE FIELD

01/26/2023

Texas Department of Transportation

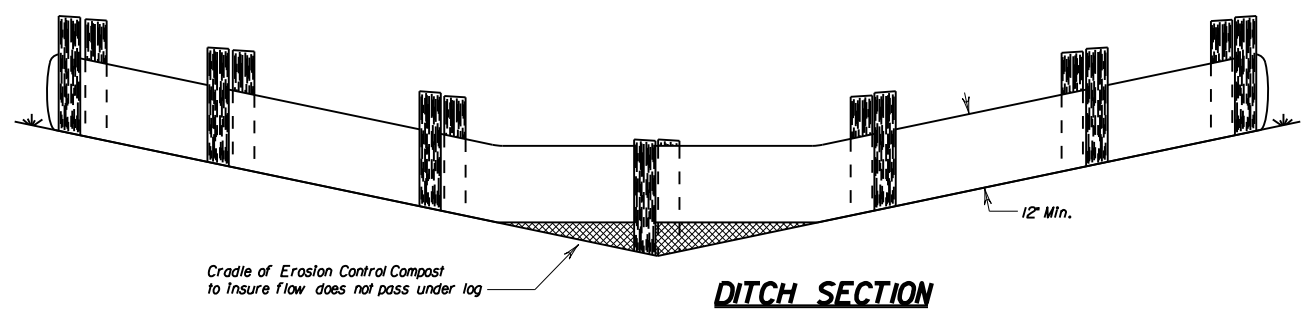
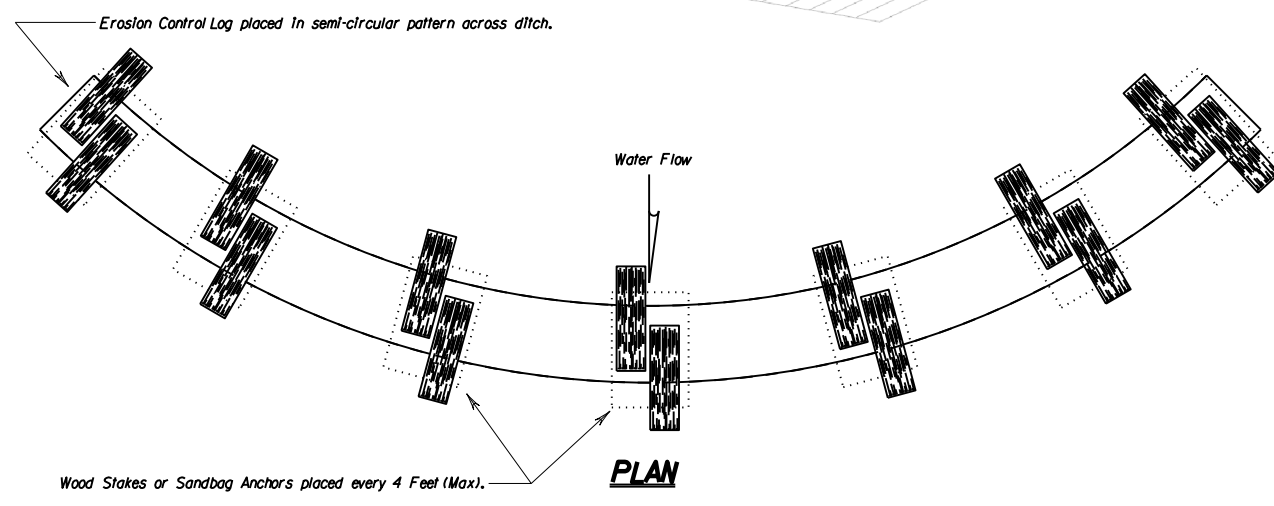
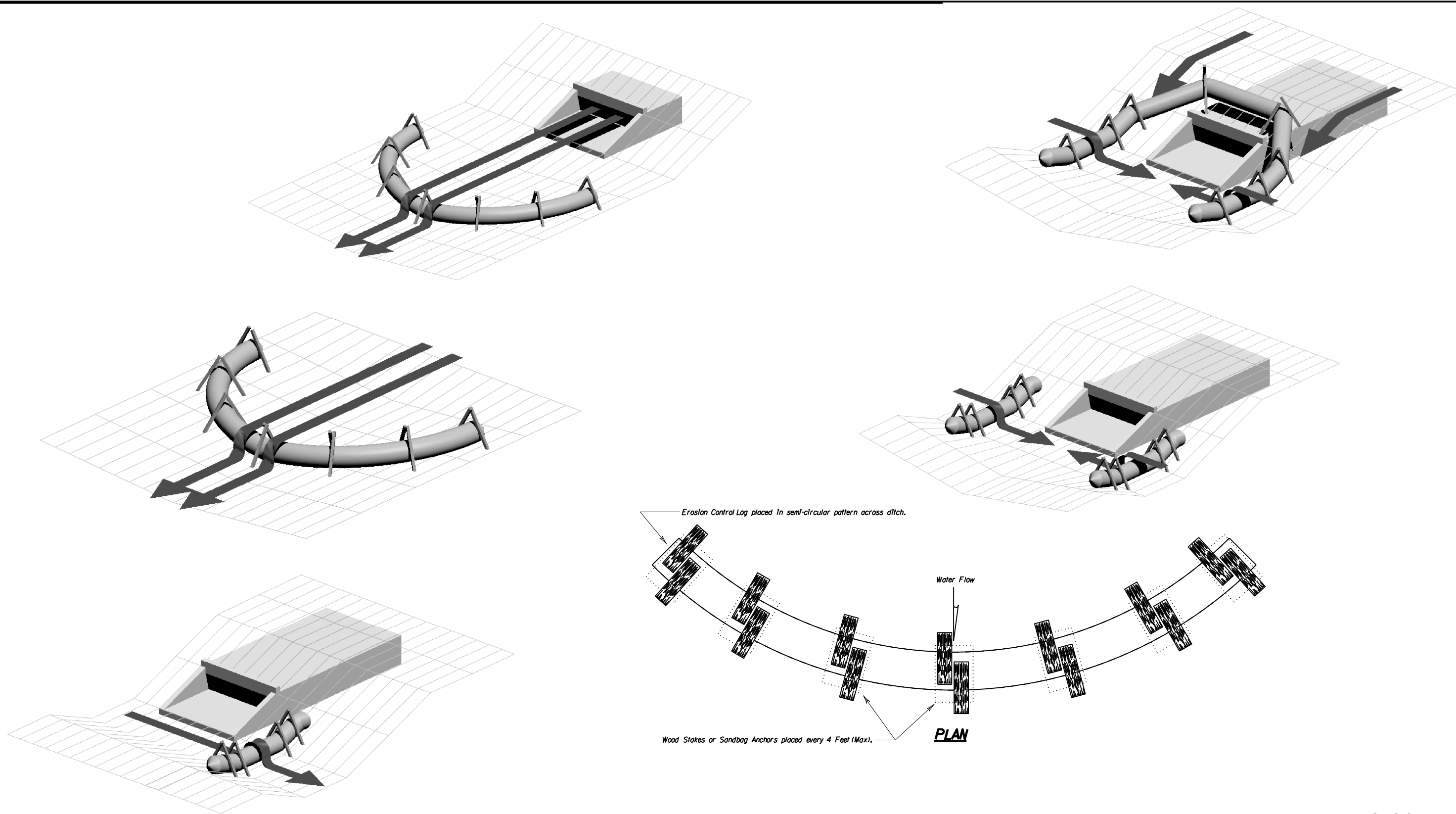
US 84

SW3P LAYOUT

SHEET 7 OF 7

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EROSION LOG GENERAL NOTES

Sandbags used as anchors will be placed on top of logs and will be of sufficient size to hold logs in place

Wood stakes will be 2" X 2" minimum size.

Do not place stakes through containment mesh.

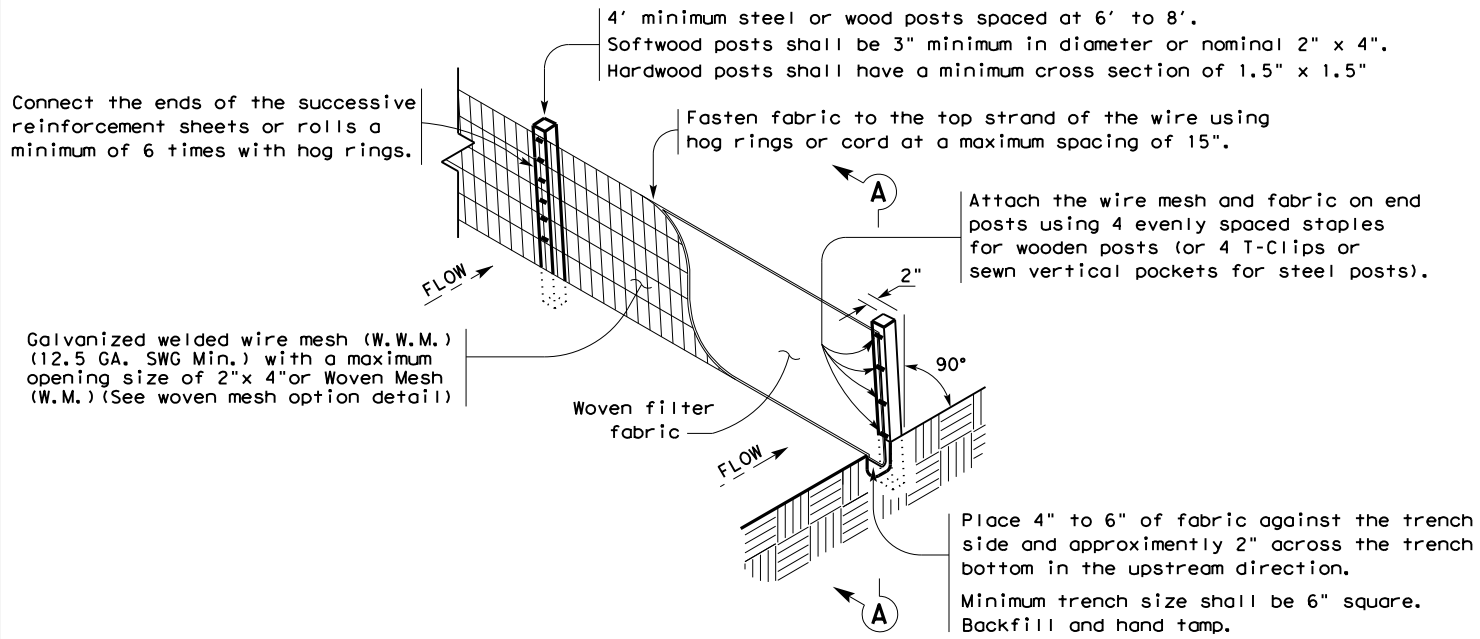
See Item 506 "Temporary Erosion, Sedimentation, and Environmental Controls" for additional details.

**US 84
 EROSION
 CONTROL LOG
 PLACEMENT
 DETAILS**



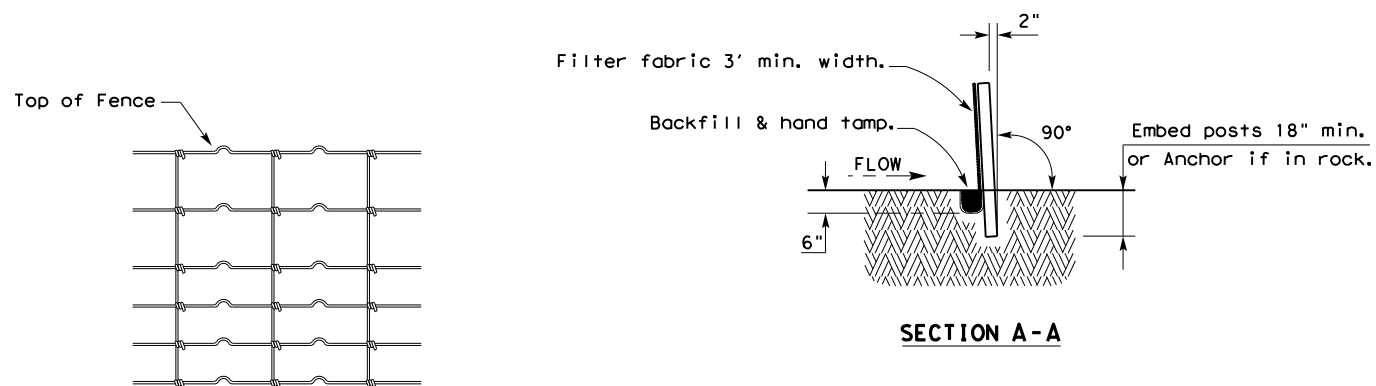
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0054	04	123	US 84
DIST	COUNTY		SHEET NO.
BWD	COLEMAN CO.		134

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TEMPORARY SEDIMENT CONTROL FENCE

SCF



HINGE JOINT KNOT WOVEN MESH (OPTION) DETAIL

Galvanized hinge joint knot woven mesh (12.5 GA. SWG Min.) requires a minimum of five horizontal wires spaced at a maximum of 12 inches apart and all vertical wires spaced at a maximum of 12 inches apart.

SEDIMENT CONTROL FENCE USAGE GUIDELINES

A sediment control fence may be constructed near the downstream perimeter of a disturbed area along a contour to intercept sediment from overland runoff. A 2 year storm frequency may be used to calculate the flow rate to be filtered.

Sediment control fence should be sized to filter a maximum flow through rate of 100 GPM/FT². Sediment control fence is not recommended to control erosion from a drainage area larger than 2 acres.

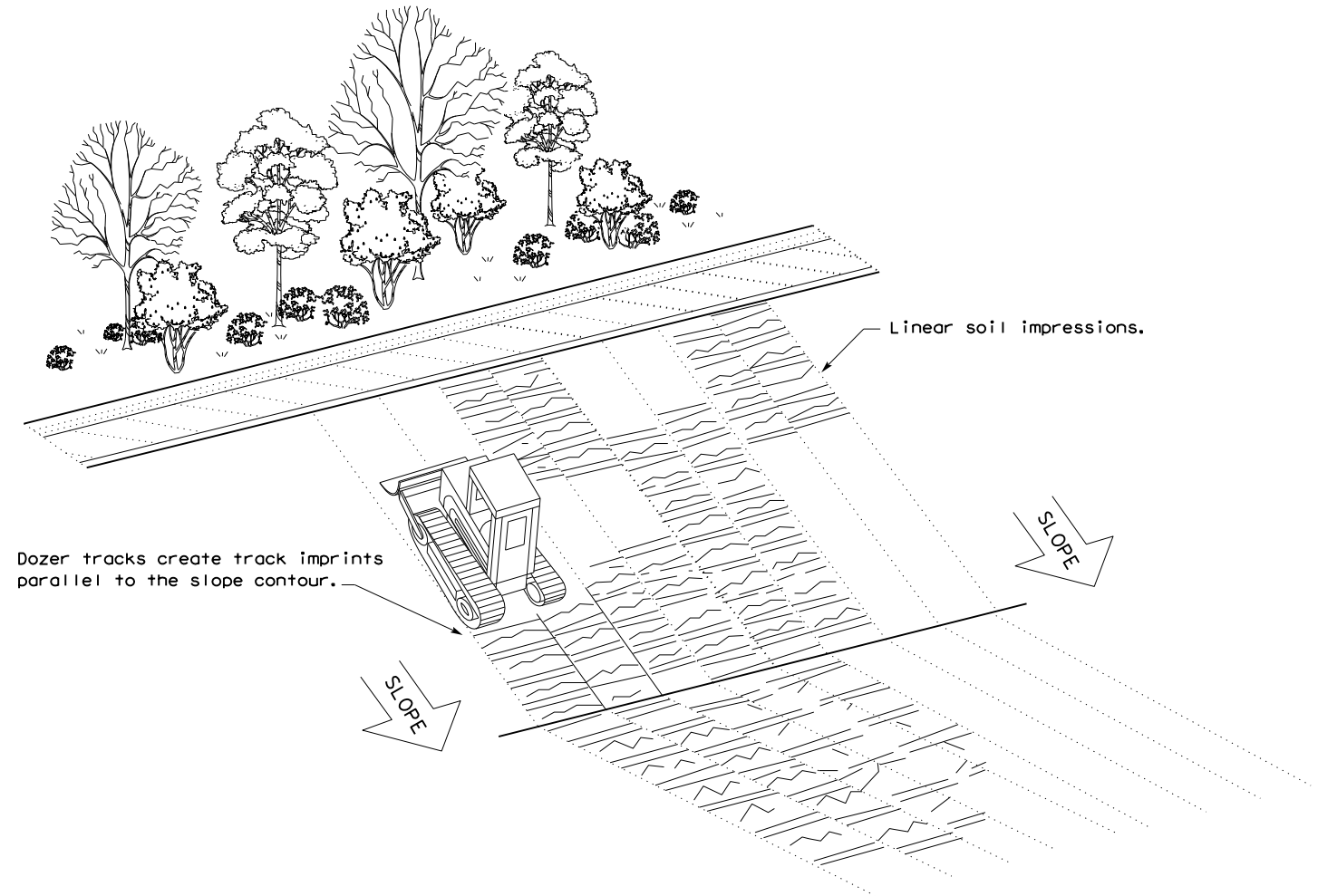
LEGEND

Sediment Control Fence

SCF

GENERAL NOTES

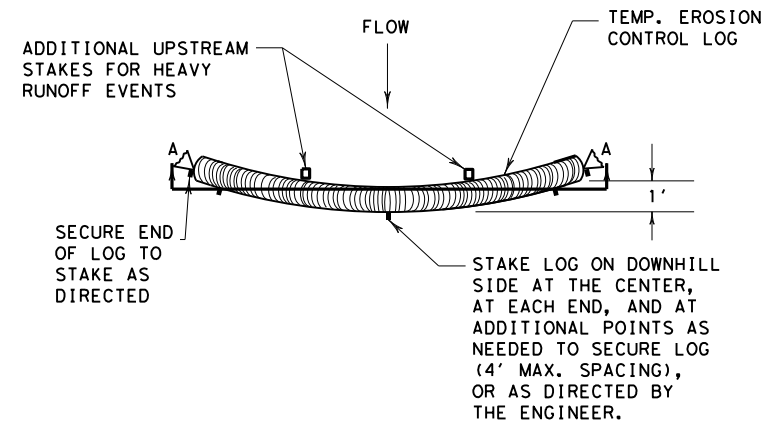
1. Vertical tracking is required on projects where soil distributing activities have occurred unless otherwise approved.
2. Perform vertical tracking on slopes to temporarily stabilize soil.
3. Provide equipment with a track undercarriage capable of producing linear soil impressions measuring a minimum of 12" in length by 2" to 4" in width by 1/2" to 2" in depth.
4. Do not exceed 12" between track impressions.
5. Install continuous linear track impressions where the minimum 12" length impressions are perpendicular to the slope or direction of water flow.



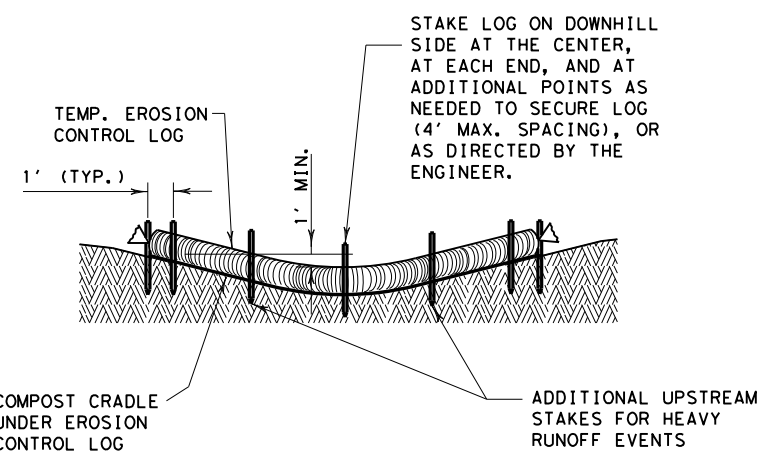
VERTICAL TRACKING

				Design Division Standard	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES FENCE & VERTICAL TRACKING EC(1)-16					
FILE: ec116	DN: TxDOT	CK: KM	DW: VP	DN/CK: LS	
© TxDOT: JULY 2016	CONT	SECT	JOB	HIGHWAY	
REVISIONS	0054	04	123	US 84	
	DIST	COUNTY	SHEET NO.		
	BWD	COLEMAN CO.	135		

DATE: 1/11/2023
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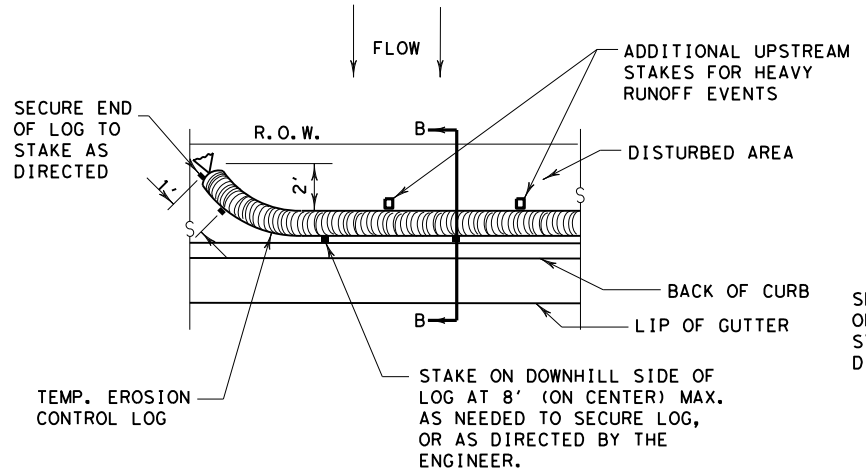


PLAN VIEW

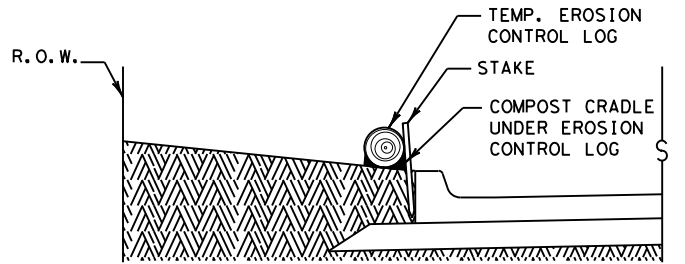


SECTION A-A
EROSION CONTROL LOG DAM

CL-D

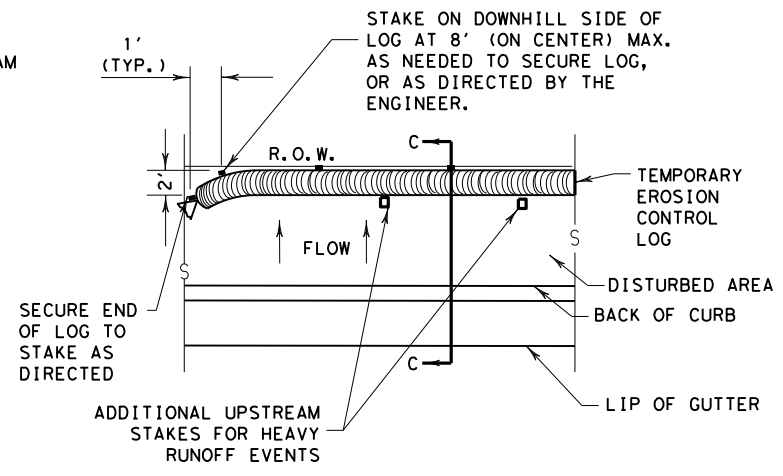


PLAN VIEW

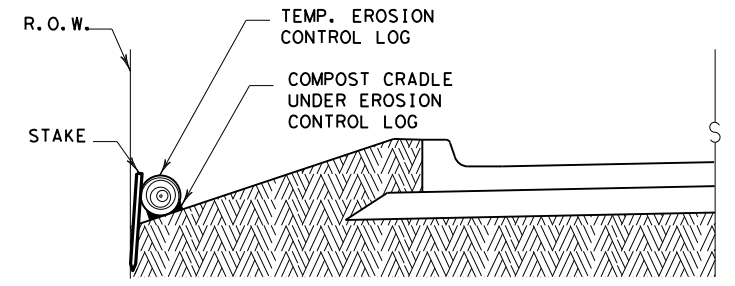


SECTION B-B
EROSION CONTROL LOG AT BACK OF CURB

CL-BOC



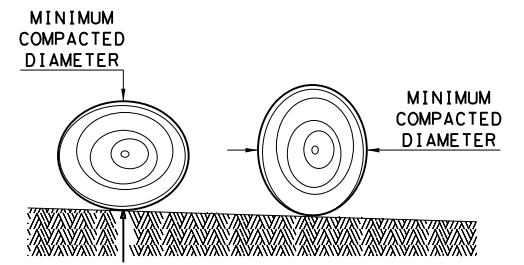
PLAN VIEW



SECTION C-C

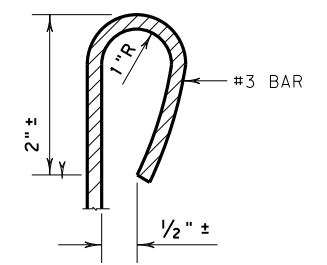
EROSION CONTROL LOG AT EDGE OF RIGHT-OF-WAY

CL-ROW



DIAMETER MEASUREMENTS OF EROSION CONTROL LOGS SPECIFIED IN PLANS

- LEGEND**
- CL-D EROSION CONTROL LOG DAM
 - CL-BOC EROSION CONTROL LOG AT BACK OF CURB
 - CL-ROW EROSION CONTROL LOG AT EDGE OF RIGHT-OF-WAY
 - CL-SST EROSION CONTROL LOGS ON SLOPES STAKE AND TRENCHING ANCHORING
 - CL-SSL EROSION CONTROL LOGS ON SLOPES STAKE AND LASHING ANCHORING
 - CL-DI EROSION CONTROL LOG AT DROP INLET
 - CL-CI EROSION CONTROL LOG AT CURB INLET
 - CL-GI EROSION CONTROL LOG AT CURB & GRATE INLET



REBAR STAKE DETAIL

SEDIMENT BASIN & TRAP USAGE GUIDELINES

An erosion control log sediment trap may be used to filter sediment out of runoff draining from an unstabilized area.

Log Traps: The drainage area for a sediment trap should not exceed 5 acres. The trap capacity should be 1800 CF/Acre (0.5" over the drainage area).

Control logs should be placed in the following locations:

1. Within drainage ditches spaced as needed or min. 500' on center
2. Immediately preceding ditch inlets or drain inlets
3. Just before the drainage enters a water course
4. Just before the drainage leaves the right of way
5. Just before the drainage leaves the construction limits where drainage flows away from the project.

The logs should be cleaned when the sediment has accumulated to a depth of 1/2 the log diameter.

Cleaning and removal of accumulated sediment deposits is incidental and will not be paid for separately.

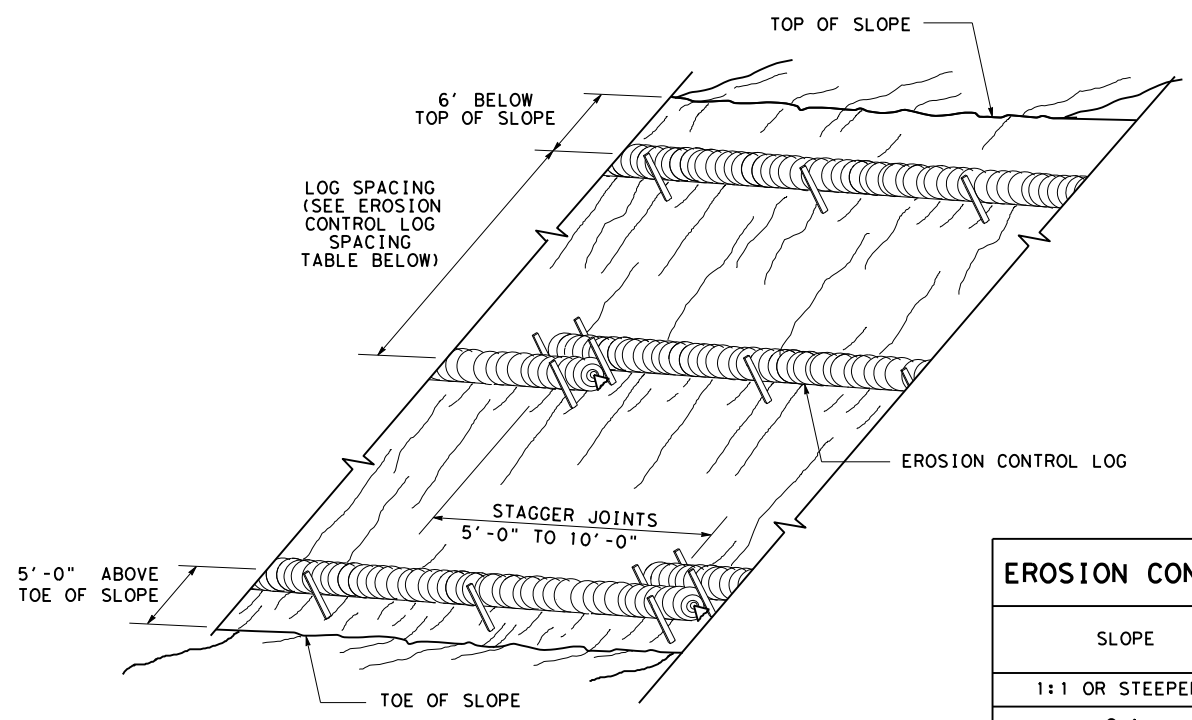
GENERAL NOTES:

1. EROSION CONTROL LOGS SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS, OR AS DIRECTED BY THE ENGINEER.
2. LENGTHS OF EROSION CONTROL LOGS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND AS REQUIRED FOR THE PURPOSE INTENDED.
3. UNLESS OTHERWISE DIRECTED, USE BIODEGRADABLE OR PHOTODEGRADABLE CONTAINMENT MESH ONLY WHERE LOG WILL REMAIN IN PLACE AS PART OF A VEGETATIVE SYSTEM. FOR TEMPORARY INSTALLATIONS, USE RECYCLABLE CONTAINMENT MESH.
4. FILL LOGS WITH SUFFICIENT FILTER MATERIAL TO ACHIEVE THE MINIMUM COMPACTED DIAMETER SPECIFIED IN THE PLANS WITHOUT EXCESSIVE DEFORMATION.
5. STAKES SHALL BE 2" X 2" WOOD OR #3 REBAR, 2'-4' LONG, EMBEDDED SUCH THAT 2" PROTRUDES ABOVE LOG, OR AS DIRECTED BY THE ENGINEER.
6. DO NOT PLACE STAKES THROUGH CONTAINMENT MESH.
7. COMPOST CRADLE MATERIAL IS INCIDENTAL & WILL NOT BE PAID FOR SEPARATELY.
8. SANDBAGS USED AS ANCHORS SHALL BE PLACED ON TOP OF LOGS & SHALL BE OF SUFFICIENT SIZE TO HOLD LOGS IN PLACE.
9. TURN THE ENDS OF EACH ROW OF LOGS UPSLOPE TO PREVENT RUNOFF FROM FLOWING AROUND THE LOG.
10. FOR HEAVY RUNOFF EVENTS, ADDITIONAL UPSTREAM STAKES MAY BE NECESSARY TO KEEP LOG FROM FOLDING IN ON ITSELF.

SHEET 1 OF 3

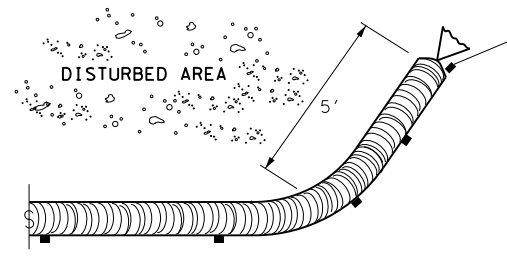
		<i>Design Division Standard</i>	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec916	DN: TxDOT	CK: KM	DW: LS/PT
© TxDOT: JULY 2016	CONT	SECT	JOB
REVISIONS	0054	04	123
DIST	COUNTY	SHEET NO.	
BWD	COLEMAN CO.	136	

DATE: 1/11/2023
 FILE: T:\BWDSSGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\4. Design\Standards\SW3P\EC(9)-16.dgn
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**EROSION CONTROL LOGS ON SLOPES
STAKE AND TRENCHING ANCHORING**

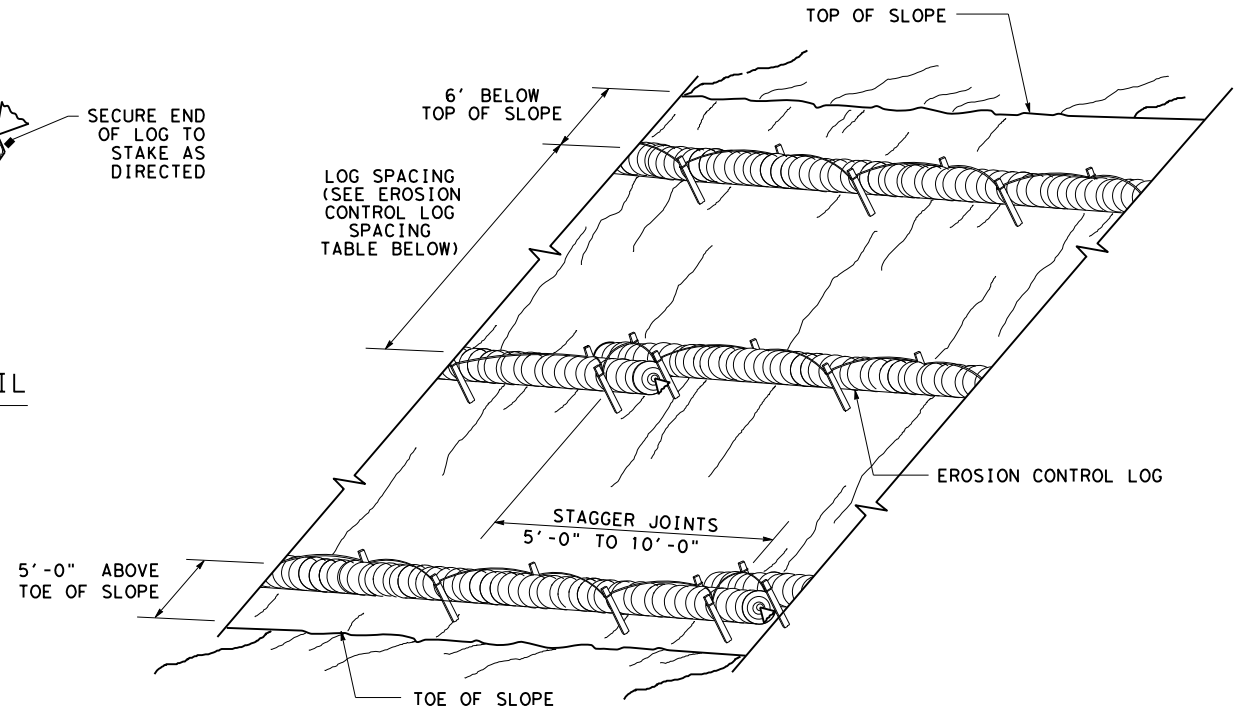
CL-SST



END SECTION RAP DETAIL

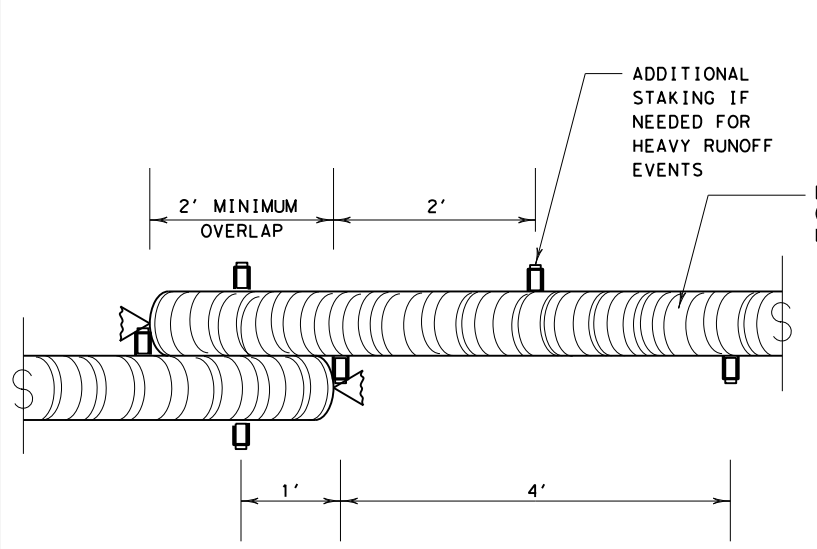
SLOPE	LOG DIAMETER			
	6"	8"	12"	18"
1:1 OR STEEPER	5'	10'	15'	20'
2:1	10'	20'	30'	40'
3:1	15'	30'	45'	60'
4:1 OR FLATTER	20'	40'	60'	80'

* ADJUSTMENTS CAN BE MADE FOR SOIL TYPE:
 SOFT, LOAMY SOILS-ADJUST ROWS CLOSER TOGETHER;
 HARD, ROCKY SOILS- ADJUST ROWS FARTHER APART



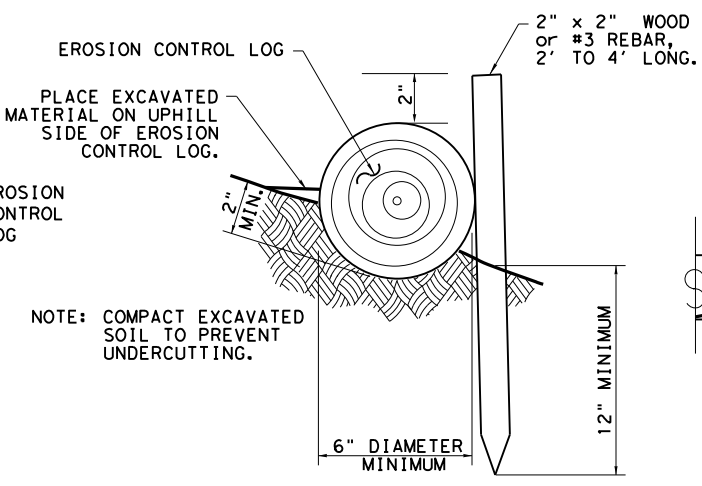
**EROSION CONTROL LOGS ON SLOPES
STAKE AND LASHING ANCHORING**

CL-SSL



STAKE AND TRENCHING ANCHORING DETAIL

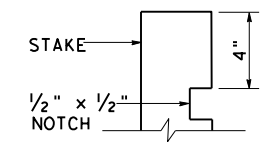
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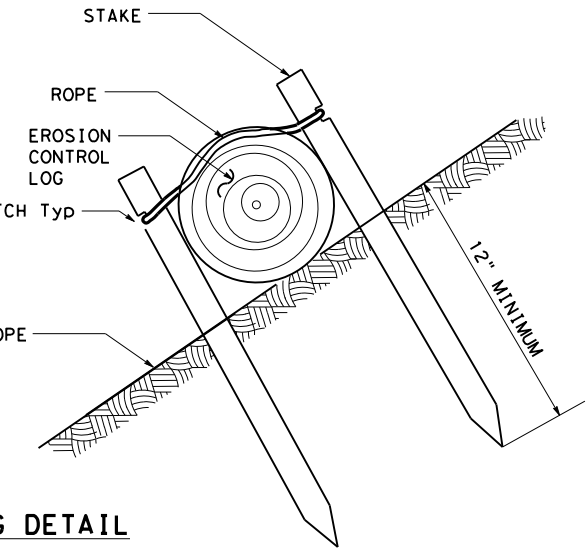
STAKE AND LASHING ANCHORING DETAIL

CL-SSL

LOG DIAMETER	DEPTH
6"	2"
8"	3"
12"	4"
18"	5"



STAKE NOTCH DETAIL



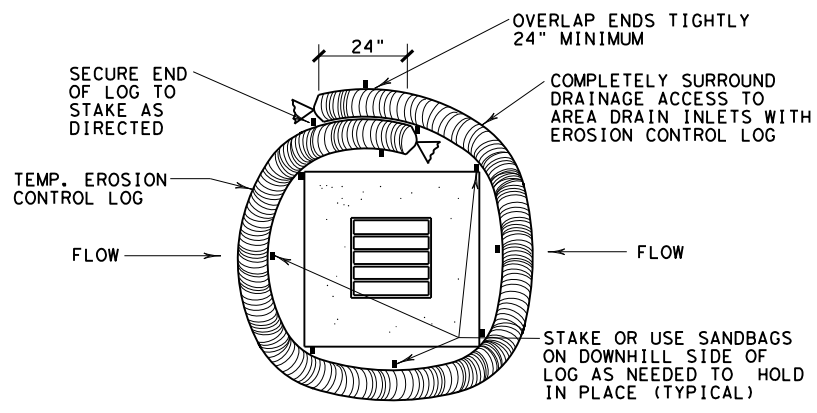
SHEET 2 OF 3

Design Division Standard

**TEMPORARY EROSION,
 SEDIMENT AND WATER
 POLLUTION CONTROL MEASURES
 EROSION CONTROL LOG
 EC(9) - 16**

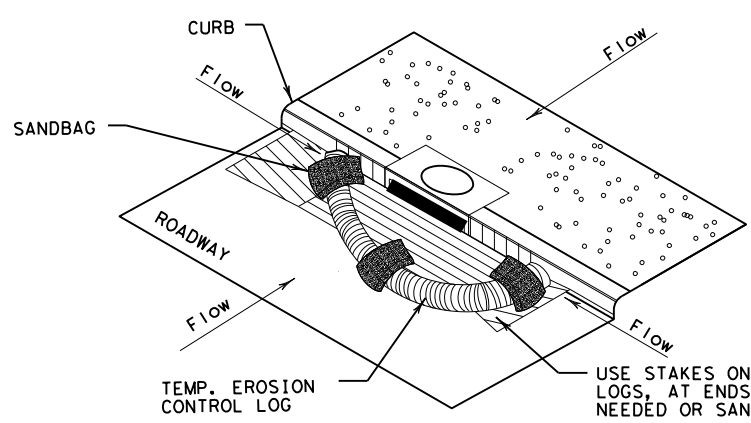
FILE: ec116	DN: TxDOT	CK: KM	DW: LS/PT	CK: LS
© TxDOT: JULY 2016	CONT	SECT	JOB	HIGHWAY
REVISIONS	0054	04	123	US 84
DIST	COUNTY	SHEET NO.		
BWD	COLEMAN CO.	137		

DATE: 1/11/2023
 FILE: T:\BWDSCGTEAM\Design Projects\Coleman\0054-04-123 US 84 - SH 153 to CR 196\4. Design\Standards\SW3P\EC(9)-16.dgn
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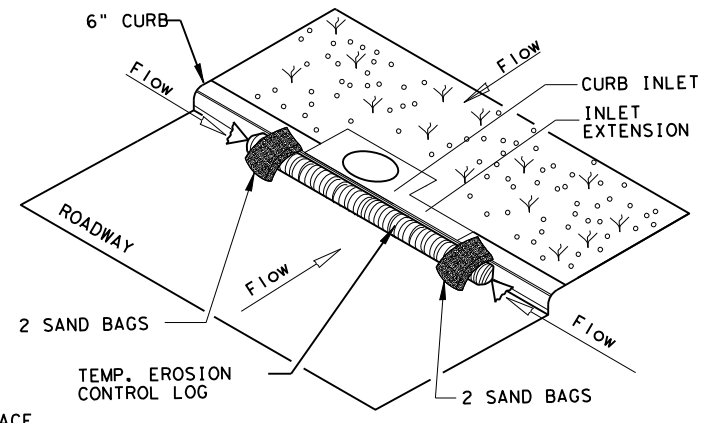
EROSION CONTROL LOG AT DROP INLET

CL-DI



EROSION CONTROL LOG AT CURB INLET

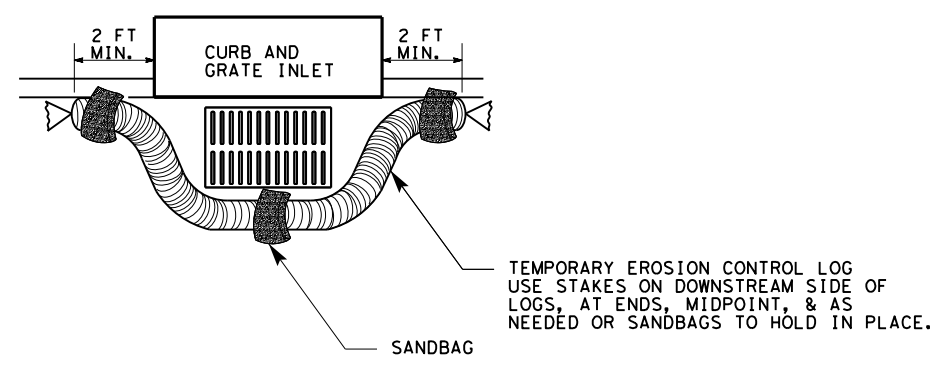
CL-CI



EROSION CONTROL LOG AT CURB INLET

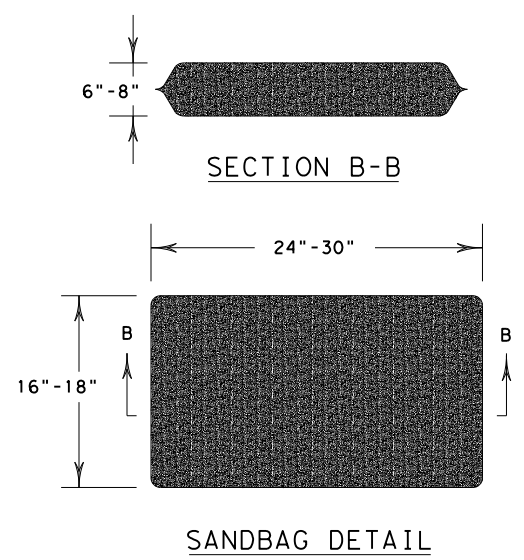
CL-CI

NOTE:
 EROSION CONTROL LOGS USED AT CURB INLETS SHOULD ONLY BE USED IF THEY WILL NOT IMPEDE TRAFFIC OR FLOOD THE ROADWAY OR WHEN THE STORM SEWER SYSTEM IS NOT FULLY FUNCTIONAL.



EROSION CONTROL LOG AT CURB & GRADE INLET

CL-GI



SHEET 3 OF 3

		<i>Design Division Standard</i>	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec916	DN: TxDOT	CK: KM	DW: LS/PT
© TxDOT: JULY 2016	CONT: 0054	SECT: 04	JOB: 123
REVISIONS	DIST: BWD		COUNTY: COLEMAN CO.
			SH 84
			SHEET NO. 138