


STATE OF TEXAS DEPARTMENT OF TRANSPORTATION

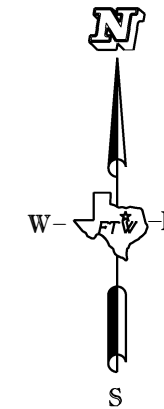
PLANS OF PROPOSED STATE HIGHWAY IMPROVEMENT

FEDERAL-AID PROJECT NO. F 2022(672)
NET LENGTH OF PROJECT (0008-15-059) = 528.00 FEET = 0.100 MILE

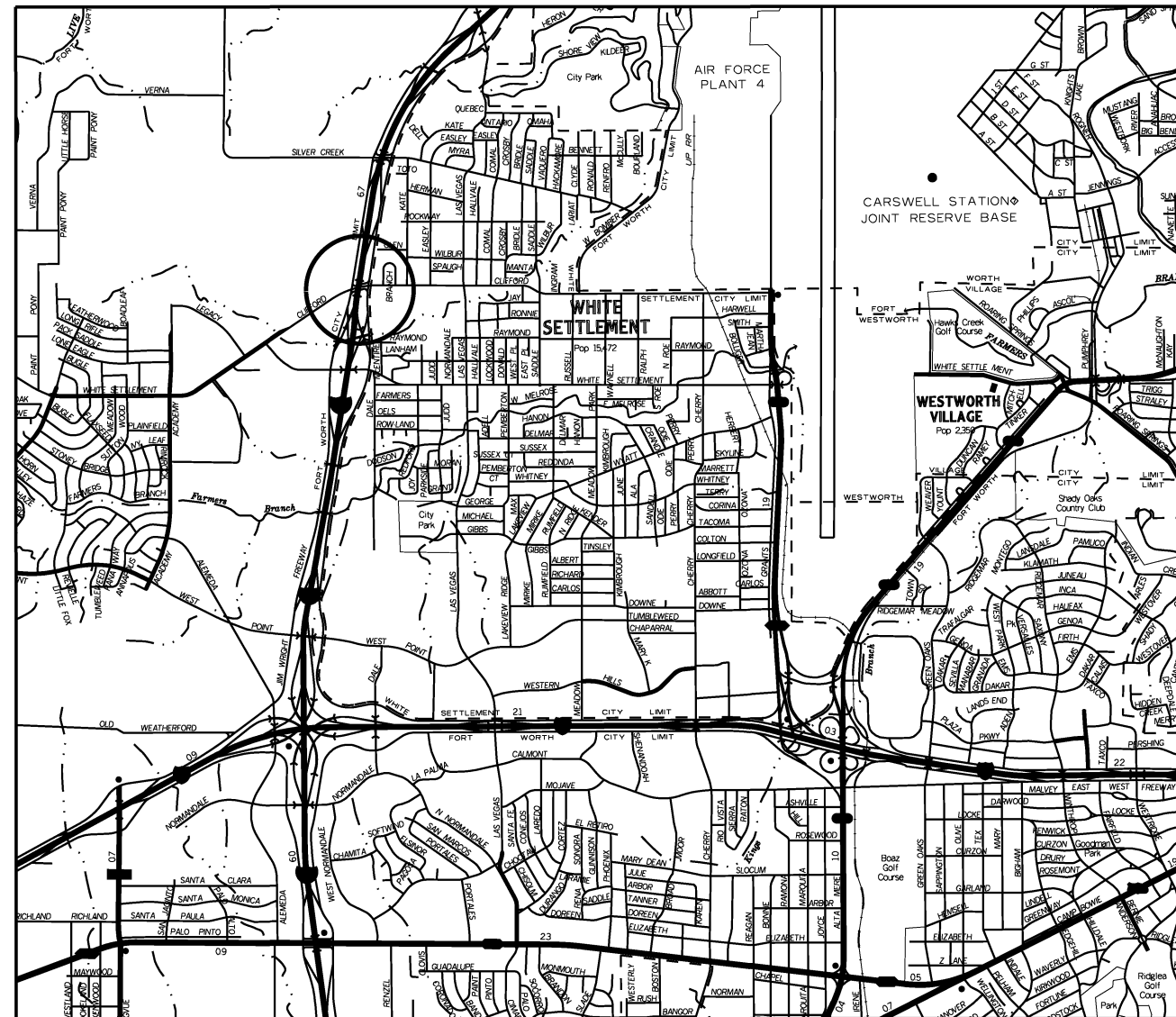
TARRANT COUNTY IH 820

AT CLIFFORD STREET
FOR THE CONSTRUCTION OF LANDSCAPE AND SCENIC ENHANCEMENT
CONSISTING OF LANDSCAPE DEVELOPMENT, ADA CURB RAMPS AND SIDEWALKS

 Texas Department of Transportation		© 2022
FED. RD. DIV. NO. 6	PROJECT NO. F 2022 (672)	SHEET NO. 1
STATE TEXAS	DIST. FTW	COUNTY TARRANT
CONT. 0008	SECT. 15	JOB 059
		HIGHWAY NO. IH 820

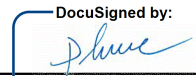


"REGISTERED ACCESSIBILITY SPECIALIST (RAS)
INSPECTION REQUIRED"
"TDLR TABS2022014554"



TEXAS DEPARTMENT OF TRANSPORTATION

SUBMITTED FOR LETTING: 3/22/2022

DocuSigned by:

AREA ENGINEER

RECOMMENDED FOR LETTING: 3/23/2022

DocuSigned by:

7879B0B... Director

APPROVED FOR LETTING: 3/24/2022

DocuSigned by:

2FE36139... DISTRICT ENGINEER

FINAL PLANS	
DATE CONTRACT LETTING: _____	
DATE CONTRACTOR BEGAN WORK: _____	
DATE WORK COMPLETED & ACCEPTED: _____	
CONTRACTOR: _____	
USED _____ OF _____ ALLOTTED DAYS	
FINAL CONTRACT COST: \$ _____	

FINAL AS BUILT PLANS	
THE CONSTRUCTION WAS PERFORMED UNDER MY SUPERVISION IN ACCORDANCE WITH THE PLANS AND CONTRACT	
DATE _____	AREA ENGINEER _____

COUNTY TARRANT PROJ. NO. _____
HWY. NO. IH 820 LETTING DATE _____
DATE ACCEPTED _____

SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION, NOVEMBER 1, 2014 AND SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS, SHALL GOVERN ON THIS PROJECT: REQUIRED CONTRACT PROVISIONS FOR ALL FEDERAL CONSTRUCTION PROJECTS (FORM FHWA 1273, MAY 1, 2012)

N. T. S.
NO R. R. CROSSINGS
NO EQUATIONS
NO EXCEPTIONS
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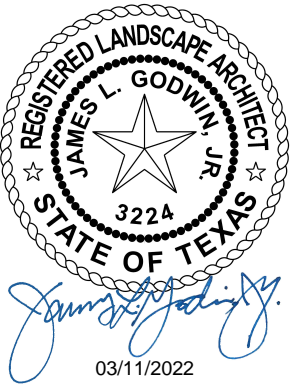
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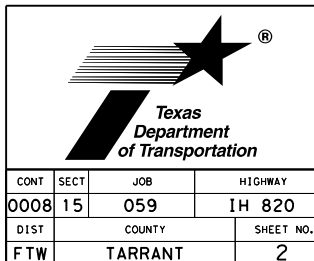
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Project Number:
STPF2022(672)

Sheet 3

County: TARRANT

Control: 0008-15-059

Highway: IH 820

PROJECT SPECIFIC NOTES:

Sweep within the project. Keep roadways and sidewalks free of sediment. Construct all ramps, sidewalks, , curb ramps, and other pedestrian elements in accordance with Texas Accessibility Standards (TAS) issued by the Texas Department of Licensing and Regulation. Maintain one copy of TAS at the project site at all times.

GENERAL NOTES:

GENERAL.

Perform work during good weather. If work is damaged by a weather event, the Contractor is responsible for all costs associated with replacing damaged work.

References to manufacturer’s trade name or catalog numbers are for the purpose of identification only. Similar materials from other manufacturers are permitted if they are equal quality, comply with the specifications for this project, and are approved.

If work is performed at Contractor’s option, when inclement weather is impending, and the work is damaged by subsequent precipitation, the Contractor is responsible for all costs associated with replacing the work, if required.

Contractor questions on this project are to be addressed to the following individuals:

Minh Tran, P.E.	Minh.Tran@txdot.gov	817-399-4300
James Bell, P.E.	James.Bell1@txdot.gov	817-399-4302

Contractor questions will be accepted through email, phone, and in person by the above individuals.

All Contractor questions will be reviewed by the Engineer. Once a response is developed, it will be posted to TxDOT’s Public FTP at the following Address:

<https://ftp.dot.state.tx.us/pub/txdot-info/Pre-Letting Responses/>

All questions submitted that generate a response will be posted through this site. The site is organized by District, Project Type (Construction or Maintenance), Letting Date, CCSJ/Project Name.

Project Number: STP ()

Sheet 3

County: TARRANT

Control: 0008-15-059

Highway: IH 820

ITEM 4. SCOPE OF WORK

Preserve the integrity of all right of way monuments within project limits. Right of way monuments damaged or destroyed during construction must be replaced by a registered professional land surveyor (RPLS), at the Contractor’s expense.

ITEM 5. CONTROL OF THE WORK

If utility lines need adjustments during construction operations, modify operations and continue the work in a manner that will allow others to make the utility adjustments. Additional working time may be allowed for delays caused by these utility adjustments.

Utility locations shown on the plans are approximate. Contact TxDOT Tyler District for utility locates. Contact utilities in accordance with Article 5.6, “Cooperating With Utilities.”

ITEM 7. LEGAL RELATIONS AND RESPONSIBILITIES

Concrete truck drivers and concrete pump operators are required to wash out only in designated areas specifically constructed for eliminating run-off. Dispose of materials in accordance with federal, state, and local requirements.

Maintain positive drainage for permanent and temporary work for the duration of the project. The Contractor will be responsible for any items associated with the temporary or interim drainage and all related maintenance. This work will be subsidiary to various bid items.

The total disturbed area for this project is 0.9 acres. The disturbed area in this project and the Contractor Project Specific Locations (PSL’s) within 1 mile of the project limits for the Contract will further establish the authorization requirements for storm water discharges. The Department will obtain an authorization to discharge storm water from the Texas Commission on Environmental Quality (TCEQ) for the construction activities shown on the plans. Obtain any required authorization from the TCEQ for any Contractor PSL for construction support activities on or off the ROW. When the total area disturbed for all projects in the Contract and PSLs within 1 mile of the project limits exceed 5 acres, before disturbance, provide a copy of the Contractor NOI for PSLs on the ROW and within 1 mile of the project limits to the Engineer and to any local government that operates a Municipal Separate Storm Sewer System (MSSS).

No significant traffic generator events identified.

Project Number: STP ()

Sheet 3A

County: TARRANT

Control: 0008-15-059

Highway: IH 820

ITEM 8. PROSECUTION AND PROGRESS

Prepare the progress schedule as a bar chart.

ITEM 9. MEASUREMENT & PAYMENT

In accordance with Article 9.1, "Measurement of Quantities," furnish the tare and maximum gross weights as well as the volume capacity of all vehicles, trucks, truck-tractors, trailers, semi-trailers, or combination of such vehicles used to deliver materials for this Contract. Also, furnish calculations supporting these weights and capacities. Provide all measurements required for pay a minimum of 2 days before the trucks are used.

ITEM 104. REMOVING CONCRETE

Blasting will not be permitted on this project.

ITEM 162. SODDING FOR EROSION CONTROL

Use *Cynodon dactylon* (Bermudagrass) or established adjacent species for block sod.

Blade and rake smooth the area before laying block sod. Refer to the plans and details for areas to receive the sod. Remove 1 in. of soil along paved edges and curb lines before laying sod and dress the slope to match all exposed edges after placing the sod. Fertilize the ground with a slow-release homogeneous coated fertilizer at a rate of 1 lb. per 9 sq. yd. before installation of the sod.

ITEM 166. FERTILIZER

Place fertilizer at the rate of 1 lb. per 9 sq. yd. on areas prepared for block sod.

ITEM 168. VEGETATIVE WATERING

Apply water to all newly placed sod or seeded areas the same day of installation. Maintain the sod or seeded areas in a completely watered condition. Do not allow sod or seeded areas to dry out so that water stress is evident.

Project Number: STP ()

Sheet 3A

County: TARRANT

Control: 0008-15-059

Highway: IH 820

ITEM 421. HYDRAULIC CEMENT CONCRETE

The Engineer will provide strength-testing equipment.

Air is not required on concrete cast-in-place elements on this project. If the Contractor proposes the use of an existing concrete design containing air, the Engineer must approve the design in writing prior to placement. If utilized, air testing will be performed in accordance with the specifications. Optimized aggregate gradation is required for this project.

ITEM 432. RIPRAP

Locations and quantities may be varied as directed by the Engineer to accommodate field conditions. Use rebar for all reinforcement. Do not use wire mesh or synthetic fiber.

ITEM 502. BARRICADES, SIGNS, AND TRAFFIC HANDLING

The traffic control plan for this Contract consists of: the installation and maintenance of warning signs and other traffic control devices shown on the plans; specification data, which may be included in the general notes; applicable provisions of the Texas Manual on Uniform Traffic Control Devices (TMUTCD); traffic control plan sheets included on the plans; standard BC sheets; Compliant Work Zone Traffic Control Device List, and Item 502 of the standard specifications.

Inspect and correct deficiencies each day throughout the duration of the Contract.

Provide at least one employee on call nights and weekends (or any other time that work is not in progress) for maintenance of signs and traffic control devices. This employee must have an address and telephone number near the project, as approved. Notify the Engineer in writing of the name, address, and telephone number of this employee. The Engineer will furnish this information to local law enforcement officials.

In addition to providing a Contractor's Responsible Person and a phone number for emergency contact, have an employee available to respond on the project for emergencies and for taking corrective measures within 30 minutes.

Sign all roads intersecting the project in accordance with current BC standards.

Refer to the traffic control plan sheets for traffic handling through the work area. Contractor may vary the signing arrangement and spacing as necessary to fit field conditions; however, any proposed changes in the traffic control plan must be approved before implementation.

Project Number: STP ()

Sheet 3B

County: TARRANT

Control: 0008-15-059

Highway: IH 820

When the sequence of work is shown on the plans, the Contractor may submit an alternate proposal for approval. Submit in writing all proposed variations and revisions.

High-visibility safety apparel is required for workers in accordance with the General Notes on current BC standards.

Place and maintain signs, channelizing devices, and flaggers to direct and route traffic at any location and for any period of time as may be required or directed.

When operations require a lane closure, provide cones, vertical panels, drums, signs, flaggers, and flashing arrow panels as necessary to route traffic around the closed lane as shown on the plans and as directed. Lane closures will be limited to one specific lane as directed.

Unless otherwise approved, lane closures for minor or major construction operations will not be allowed on Good Friday, Easter weekend, Memorial Day, Memorial Day weekend, July 4th, Labor Day, Labor Day weekend, Thanksgiving Day thru Sunday, Christmas Eve, Christmas Day, New Year's Eve, New Year's Day, or on any other high traffic days or holidays as determined.

Maintain existing roadside signs within this project's limits during this Contract. In order to accommodate the grading or other operations, temporarily relocate these signs in accordance with the TMUTCD as directed. This work will not be paid for directly, but will be subsidiary to Item 502.

Provide truck-mounted attenuators (TMA) as shown on the appropriate traffic control plan sheets. Provide a letter certifying that all TMA used on this project meet NCHRP 350 or AASHTO Manual for Assessing Safety Hardware (MASH) requirements.

Regulate all construction activities and equipment to minimize inconvenience to the traveling public. At points where it is necessary for trucks to stop, load, or unload, provide warning signs and flaggers to protect the traveling public.

The pavement must be entirely open to traffic each night. Remove or clearly barricade all material stockpiles, equipment left overnight, or any obstruction within 30 ft. of a travelway as approved.

The Contractor Force Account "Safety Contingency" is intended to be used for work zone enhancements that could not be foreseen in the project planning and design stage for the purpose of improving the effectiveness of the Traffic Control Plan. These enhancements will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent traffic management reviews on the project. The Engineer may choose to use existing bid items if it does not slow the implementation of enhancement.

Project Number: STP ()

Sheet 3B

County: TARRANT

Control: 0008-15-059

Highway: IH 820

Prior to beginning work, the Contractor and Engineer must agree on the allowable length of lane closure.

All work required by these general notes, except as provided for by Item 502, will not be paid for directly, but will be subsidiary to Item 502 unless otherwise shown on the plans.

ITEM 506. TEMPORARY EROSION, SEDIMENTATION, AND ENVIRONMENTAL CONTROLS

Place countermeasures only after approved by the Engineer.

Remove dirt, silt, rocks, debris, and other foreign matter that accumulates in all structures due to project erosion and Contractor's operations. Keep stream channels open at all times. This work will not be paid for directly, but will be subsidiary to this Item.

Provide the following Items for the SW3P for this Contract as directed on a force account basis:

Erosion Control Logs

This SW3P for this project will consist of using the following items as directed:

- Construction Exits
- Construction Perimeter Fence
- Erosion Control Logs

ITEM 531. SIDEWALKS

An air-entraining admixture is not required.

Proposed curb ramps, sidewalks, curbs, and riprap is to be doveled 8in minimum into existing, using 1/2in reinforcement placed on 12in centers.

Areas labeled with a "T" on the construction drawings allow the contractor to transition to existing conditions. Slope and grade of all transitions must be approved by the engineer.

The curb ramp locations shown in the plans have taken into account the geometric features of the intersection, traffic signals, and the pavement markings. If anything changes during construction, the location of curb ramps must be adjusted to ensure they meet TAS requirements.

Any approval, inspection, or checking of the contractor's layout by TxDOT and the acceptance of all or any part of it shall not relieve the contractor of his responsibility to secure the proper dimensions, grades and elevations of the various parts of the work.

Project Number: STP()

Sheet 3C

County: TARRANT

Control: 0008-15-059

Highway: IH 820

Construction of each curb ramp is to be completed within seven (7) working days after start of construction process. Construction process of curb ramps shall include: demolition of existing conditions, placement of concrete or brick, removal of lips, street surface patching in front of the curb or ramp, adjustment of counter slope within 24-inches of the bottom of the ramp or curb and gutter, street level landings, backfill, placement of topsoil, grading and sodding, and clean-up. All other related work such as adjustment of crosswalk, special heat-welds, asphalt overlays, and other work that does not affect accessibility shall be completed per a schedule pre-approved by TxDOT. The furnishing and installation of the sand cushion in proposed sidewalks, sidewalk ramps, and driveways will not be paid for directly but will be subsidiary to this bid item.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

PROJECT SPECIFIC NOTES:

Sweep within the project. Keep roadways and sidewalks free of sediment. Construct all ramps, sidewalks, curb ramps, and other pedestrian elements in accordance with Public ROW Accessibility Guidelines (PROWAG) issued by the Texas Department of Licensing and Regulation. Maintain one copy of TAS at the project site at all times.

GENERAL NOTES:

GENERAL.

Perform work during good weather. If work is damaged by a weather event, the Contractor is responsible for all costs associated with replacing damaged work.

References to manufacturer’s trade name or catalog numbers are for the purpose of identification only. Similar materials from other manufacturers are permitted if they are equal quality, comply with the specifications for this project, and are approved.

If work is performed at Contractor’s option, when inclement weather is impending, and the work is damaged by subsequent precipitation, the Contractor is responsible for all costs associated with replacing the work, if required.

Contractor questions on this project are to be addressed to the following individuals:

Minh Tran, P.E.	Minh.Tran@txdot.gov	817-399-4300
James Bell, P.E.	James.Bell1@txdot.gov	817-399-4302

Contractor questions will be accepted through email, phone, and in person by the above individuals.

All Contractor questions will be reviewed by the Engineer. Once a response is developed, it will be posted to TxDOT’s Public FTP at the following Address:

<https://ftp.dot.state.tx.us/pub/txdot-info/Pre-Letting Responses/>

All questions submitted that generate a response will be posted through this site. The site is organized by District, Project Type (Construction or Maintenance), Letting Date, CCSJ/Project Name.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

ITEM 4. SCOPE OF WORK

Preserve the integrity of all right of way monuments within project limits. Right of way monuments damaged or destroyed during construction must be replaced by a registered professional land surveyor (RPLS), at the Contractor’s expense.

ITEM 5. CONTROL OF THE WORK

If utility lines need adjustments during construction operations, modify operations and continue the work in a manner that will allow others to make the utility adjustments. Additional working time may be allowed for delays caused by these utility adjustments.

Utility locations shown on the plans are approximate. Contact TxDOT Fort Worth District for utility locates. Contact utilities in accordance with Article 5.6, “Cooperating With Utilities.”

ITEM 7. LEGAL RELATIONS AND RESPONSIBILITIES

Concrete truck drivers and concrete pump operators are required to wash out only in designated areas specifically constructed for eliminating run-off. Dispose of materials in accordance with federal, state, and local requirements.

Maintain positive drainage for permanent and temporary work for the duration of the project. The Contractor will be responsible for any items associated with the temporary or interim drainage and all related maintenance. This work will be subsidiary to various bid items.

The total disturbed area for this project is 0.45 acres. The disturbed area in this project and the Contractor Project Specific Locations (PSL’s) within 1 mile of the project limits for the Contract will further establish the authorization requirements for storm water discharges. The Department will obtain an authorization to discharge storm water from the Texas Commission on Environmental Quality (TCEQ) for the construction activities shown on the plans. Obtain any required authorization from the TCEQ for any Contractor PSL for construction support activities on or off the ROW. When the total area disturbed for all projects in the Contract and PSLs within 1 mile of the project limits exceed 5 acres, before disturbance, provide a copy of the Contractor NOI for PSLs on the ROW and within 1 mile of the project limits to the Engineer and to any local government that operates a Municipal Separate Storm Sewer System (MSSS).

No significant traffic generator events identified.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

Single lane closures, except as otherwise shown in the plans, will be restricted to off-peak hours as defined in the following table:

Peak Hours		Off-Peak Hours	
6 to 9 AM Monday through Friday	3 to 7 PM Monday through Friday	9 AM to 3 PM and 7 PM to 6 AM Monday through Friday	All day Saturday and Sunday

Work that requires closure of multiple travel lanes in the same direction, except as otherwise shown in the plans, are restricted to night hours between 9 PM and 6 AM, or as directed in writing by the Engineer.

ITEM 8. PROSECUTION AND PROGRESS

Working days will be computed and charged in accordance with Section 8.3.1.1, ‘Five-Day Workweek.’

Prepare the progress schedule as a bar chart, include all planned work activities and sequences, and show Contract completion within the number of working days specified. Submit an updated hard copy when changes to the schedule occur or when requested.

ITEM 9. MEASUREMENT & PAYMENT

In accordance with Article 9.1, “Measurement of Quantities,” furnish the tare and maximum gross weights as well as the volume capacity of all vehicles, trucks, truck-tractors, trailers, semi-trailers, or combination of such vehicles used to deliver materials for this Contract. Also, furnish calculations supporting these weights and capacities. Provide all measurements required for pay a minimum of 2 days before the trucks are used.

ITEM 104. REMOVING CONCRETE

Blasting will not be permitted on this project.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

ITEM 192. LANDSCAPE PLANTING

No planting shall occur between June 1st and September 15th without written approval from the Engineer. Replace dead or dying plant material within 10 days of notification by the Engineer unless otherwise indicated in the notification. Plant material replacement will be subsidiary to this item.

This item includes an additional 3-months of plant maintenance required after construction/installation, subsidiary to Item 192 (192.3.15). Begin 90-day maintenance period only after all live plant material and functional irrigation systems have been installed as shown on plans.

The planting bed area shall be laid out in the field in such a manner that the configuration may be examined and approved by the Engineer.

ITEM 421. HYDRAULIC CEMENT CONCRETE

The Engineer will provide strength-testing equipment.

Air is not required on concrete cast-in-place elements on this project. If the Contractor proposes the use of an existing concrete design containing air, the Engineer must approve the design in writing prior to placement. If utilized, air testing will be performed in accordance with the specifications. Optimized aggregate gradation is required for this project.

ITEM 502. BARRICADES, SIGNS, AND TRAFFIC HANDLING

The traffic control plan for this Contract consists of: the installation and maintenance of warning signs and other traffic control devices shown on the plans; specification data, which may be included in the general notes; applicable provisions of the Texas Manual on Uniform Traffic Control Devices (TMUTCD); traffic control plan sheets included on the plans; standard BC sheets; Compliant Work Zone Traffic Control Device List, and Item 502 of the standard specifications.

Inspect and correct deficiencies each day throughout the duration of the Contract.

Provide at least one employee on call nights and weekends (or any other time that work is not in progress) for maintenance of signs and traffic control devices. This employee must have an address and telephone number near the project, as approved. Notify the Engineer in writing of the name, address, and telephone number of this employee. The Engineer will furnish this information to local law enforcement officials.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

In addition to providing a Contractor's Responsible Person and a phone number for emergency contact, have an employee available to respond on the project for emergencies and for taking corrective measures within 30 minutes.

Sign all roads intersecting the project in accordance with current BC standards.

Refer to the traffic control plan sheets for traffic handling through the work area. Contractor may vary the signing arrangement and spacing as necessary to fit field conditions; however, any proposed changes in the traffic control plan must be approved before implementation.

When the sequence of work is shown on the plans, the Contractor may submit an alternate proposal for approval. Submit in writing all proposed variations and revisions.

High-visibility safety apparel is required for workers in accordance with the General Notes on current BC standards.

Place and maintain signs, channelizing devices, and flaggers to direct and route traffic at any location and for any period of time as may be required or directed.

When operations require a lane closure, provide cones, vertical panels, drums, signs, flaggers, and flashing arrow panels as necessary to route traffic around the closed lane as shown on the plans and as directed. Lane closures will be limited to one specific lane as directed.

Unless otherwise approved, lane closures for minor or major construction operations will not be allowed on Good Friday, Easter weekend, Memorial Day, Memorial Day weekend, July 4th, Labor Day, Labor Day weekend, Thanksgiving Day thru Sunday, Christmas Eve, Christmas Day, New Year's Eve, New Year's Day, or on any other high traffic days or holidays as determined.

Maintain existing roadside signs within this project's limits during this Contract. In order to accommodate the grading or other operations, temporarily relocate these signs in accordance with the TMUTCD as directed. This work will not be paid for directly, but will be subsidiary to Item 502.

Provide truck-mounted attenuators (TMA) as shown on the appropriate traffic control plan sheets. Provide a letter certifying that all TMA used on this project meet NCHRP 350 or AASHTO Manual for Assessing Safety Hardware (MASH) requirements.

Regulate all construction activities and equipment to minimize inconvenience to the traveling public. At points where it is necessary for trucks to stop, load, or unload, provide warning signs and flaggers to protect the traveling public.

The pavement must be entirely open to traffic each night. Remove or clearly barricade all material stockpiles, equipment left overnight, or any obstruction within 30 ft. of a travelway as approved.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

The Contractor Force Account "Safety Contingency" is intended to be used for work zone enhancements that could not be foreseen in the project planning and design stage for the purpose of improving the effectiveness of the Traffic Control Plan. These enhancements will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent traffic management reviews on the project. The Engineer may choose to use existing bid items if it does not slow the implementation of enhancement.

Prior to beginning work, the Contractor and Engineer must agree on the allowable length of lane closure.

All work required by these general notes, except as provided for by Item 502, will not be paid for directly, but will be subsidiary to Item 502 unless otherwise shown on the plans.

ITEM 506. TEMPORARY EROSION, SEDIMENTATION, AND ENVIRONMENTAL CONTROLS

Place countermeasures only after approved by the Engineer.

Remove dirt, silt, rocks, debris, and other foreign matter that accumulates in all structures due to project erosion and Contractor's operations. Keep stream channels open at all times. This work will not be paid for directly, but will be subsidiary to this Item.

Provide the following Items for the SW3P for this Contract as directed on a force account basis:

Erosion Control Logs

ITEM 531. SIDEWALKS

An air-entraining admixture is not required.

Proposed curb ramps, sidewalks, curbs, and riprap is to be doveled 8in minimum into existing, using 1/2in reinforcement placed on 12in centers.

Areas labeled with a "T" on the construction drawings allow the contractor to transition to existing conditions. Slope and grade of all transitions must be approved by the engineer.

The curb ramp locations shown in the plans have taken into account the geometric features of the intersection, traffic signals, and the pavement markings. If anything changes during construction, the location of curb ramps must be adjusted to ensure they meet TAS requirements.

Project Number: F 2022(672)

County: TARRANT

Highway: IH 820

Control: 0008-15-059

Any approval, inspection, or checking of the contractor's layout by TxDOT and the acceptance of all or any part of it shall not relieve the contractor of his responsibility to secure the proper dimensions, grades and elevations of the various parts of the work.

Construction of each curb ramp is to be completed within seven (7) working days after start of construction process. Construction process of curb ramps shall include: demolition of existing conditions, placement of concrete or brick, removal of lips, street surface patching in front of the curb or ramp, adjustment of counter slope within 24-inches of the bottom of the ramp or curb and gutter, street level landings, backfill, placement of topsoil, grading and sodding, and clean-up. All other related work such as adjustment of crosswalk, special heat-welds, asphalt overlays, and other work that does not affect accessibility shall be completed per a schedule pre-approved by TxDOT. The furnishing and installation of the sand cushion in proposed sidewalks, sidewalk ramps, and driveways will not be paid for directly but will be subsidiary to this bid item.



CONTROLLING PROJECT ID 0008-15-059

DISTRICT Fort Worth
HIGHWAY IH 820

COUNTY Tarrant

Estimate & Quantity Sheet

CONTROL SECTION JOB		0008-15-059		TOTAL EST.		TOTAL FINAL	
PROJECT ID		A00183061					
COUNTY		Tarrant					
HIGHWAY		IH 820					
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL		
	104-6021	REMOVING CONC (CURB)	LF	453.000		453.000	
	110-6003	EXCAVATION (SPECIAL)	CY	133.650		133.650	
	170-6001	IRRIGATION SYSTEM	LS	1.000		1.000	
	192-6002	PLANT MATERIAL (1-GAL)	EA	227.000		227.000	
	192-6004	PLANT MATERIAL (5-GAL)	EA	138.000		138.000	
	192-6014	PLANT SOIL MIX	CY	88.200		88.200	
	192-6015	LANDSCAPE EDGE	LF	863.000		863.000	
	192-6017	VEGETATION BARRIER	SY	178.000		178.000	
	500-6003	MOBILIZATION (CALLOUT 1)	EA	1.000		1.000	
	502-6001	BARRICADES, SIGNS AND TRAFFIC HANDLING	MO	3.000		3.000	
	506-6041	BIODEG EROSN CONT LOGS (INSTL) (12")	LF	200.000		200.000	
	506-6043	BIODEG EROSN CONT LOGS (REMOVE)	LF	200.000		200.000	
	528-6004	LANDSCAPE PAVERS	SY	1,220.000		1,220.000	
	529-6002	CONC CURB (TY II)	LF	453.000		453.000	
	531-6003	CONC SIDEWALKS (6")	SY	488.000		488.000	
	531-6035	CURB RAMPS	SY	175.000		175.000	
	624-6009	GROUND BOX TY D (162922)	EA	2.000		2.000	
	624-6028	REMOVE GROUND BOX	EA	2.000		2.000	
	644-6068	RELOCATE SM RD SN SUPSAM TY 10BWG	EA	4.000		4.000	
	1002-6002	LANDSCAPE AMENITY (TY 1)	EA	3.000		3.000	
	1002-6003	LANDSCAPE AMENITY (TY 2)	EA	2.000		2.000	
	1002-6004	LANDSCAPE AMENITY (TY 3)	EA	4.000		4.000	
	1005-6001	LOOSE AGGR FOR GROUNDCOVER (TYPE I)	CY	15.000		15.000	
	1005-6002	LOOSE AGGR FOR GROUNDCOVER (TYPE II)	CY	29.800		29.800	
	5009-6002	STONE MASONRY (ROCK WALL)	SF	1,664.000		1,664.000	
	6185-6002	TMA (STATIONARY)	DAY	20.000		20.000	
	18	EROSION CONTROL MAINTENANCE: CONTRACTOR FORCE ACCOUNT WORK (PART)	LS	1.000		1.000	
		SAFETY CONTINGENCY: CONTRACTOR FORCE ACCOUNT WORK (PARTICIPATING)	LS	1.000		1.000	

DISTRICT	COUNTY	CCSJ	SHEET
Fort Worth	Tarrant	0008-15-059	4

SUMMARY OF QUANTITIES														
ITEM NUMBER	104 6021	110 6003	170 6001	192 6002	192 6004	192 6014	192 6015	192 6017	528 6004	529 6002	531 6003	531 6035	624 6009	624 6028
DESCRIPTION	REMOVING CONC (CURB)	EXCAVATION (SPECIAL)	IRRIGATION SYSTEM	PLANT MATERIAL (1-GAL)	PLANT MATERIAL (5-GAL)	PLANT SOIL MIX	LANDSCAPE EDGE	VEGETATION BARRIER	LANDSCAPE PAVERS	CONC CURB (TY II)	CONC SIDEWALKS (6")	CURB RAMPS	GROUND BOX TY D (162922)	REMOVE GROUND BOX
UNITS	LF	CY	LS	EA	EA	CY	LF	SY	SY	LF	SY	SY	EA	EA
SHEET TOTALS	453	133.65	1	227	138	88.2	863	178	1220	453	488	175	2	2

SUMMARY OF QUANTITIES													
ITEM NUMBER	644 6068	1002 6002	1002 6003	1002 6004	1005 6001	1005 6002	5009 6002						
DESCRIPTION	RELOCATE SM RD SN SUP&AM TY 10BWG	LANDSCAPE AMENITY (TY 1)	LANDSCAPE AMENITY (TY 2)	LANDSCAPE AMENITY (TY 3)	LOOSE AGGR FOR GROUNDCOVER (TYPE I)	LOOSE AGGR FOR GROUNDCOVER (TYPE II)	STONE MASONRY (ROCK WALL)	* CURB RAMPS (TY 1)	* CURB RAMPS (TY 5)	* CURB RAMPS (TY 7)	* CURB RAMPS (TY 21)	* CURB RAMPS (TY 22)	
UNITS	EA	EA	EA	EA	CY	CY	SF	EA	EA	EA	EA	EA	
SHEET TOTALS	4	3	2	4	15	29.8	1664	3	2	5	1	1	

*FOR CONTRACTOR'S INFORMATION ONLY

SUMMARY OF INDEFINITES (AS DIRECTED BY ENGINEER)		
ITEM NUMBER	506 6041	506 6043
DESCRIPTION	BIODEG EROSN CONT LOGS (INSTL) (12")	BIODEG EROSN CONT LOGS (REMOVE)
UNITS	LF	LF
SHEET TOTALS	200	200

BU 59T SUMMARY OF QUANTITIES



SHEET 1 OF 1

CONT	SECT	JOB	HWY
0008	15	059	IH 820
DISTRICT	COUNTY		SHEET
FTW	TARRANT		5

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BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES:

- The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction pavement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.
- The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.
- The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer.
- Geometric design of lane shifts and detours should, when possible, meet the applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.
- When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.
- The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.
- All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.
- The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.
- Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES, CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.
- Traffic control devices should be in place only while work is actually in progress or a definite need exists.
- The Engineer has the final decision on the location of all traffic control devices.
- Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

WORKER SAFETY NOTES:

- Workers on foot who are exposed to traffic or to construction equipment within the right-of-way shall wear high-visibility safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel," or equivalent revisions, and labeled as ANSI 107-2004 standard performance for Class 2 or 3 risk exposure. Class 3 garments should be considered for high traffic volume work areas or night time work.
- Except in emergency situations, flagger stations shall be illuminated when flagging is used at night.

COMPLIANT WORKZONE TRAFFIC CONTROL DEVICES

- Only pre-qualified products shall be used. The "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products and their sources.
- Work zone traffic control devices shall be compliant with the Manual for Assessing safety Hardware (MASH).

THE DOCUMENTS BELOW CAN BE FOUND ON-LINE AT http://www.txdot.gov
COMPLIANT WORK ZONE TRAFFIC CONTROL DEVICES LIST (CWZTCD)
DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS)
MATERIAL PRODUCER LIST (MPL)
ROADWAY DESIGN MANUAL - SEE "MANUALS (ONLINE MANUALS)"
STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS (SHSD)
TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD)
TRAFFIC ENGINEERING STANDARD SHEETS

SHEET 1 OF 12



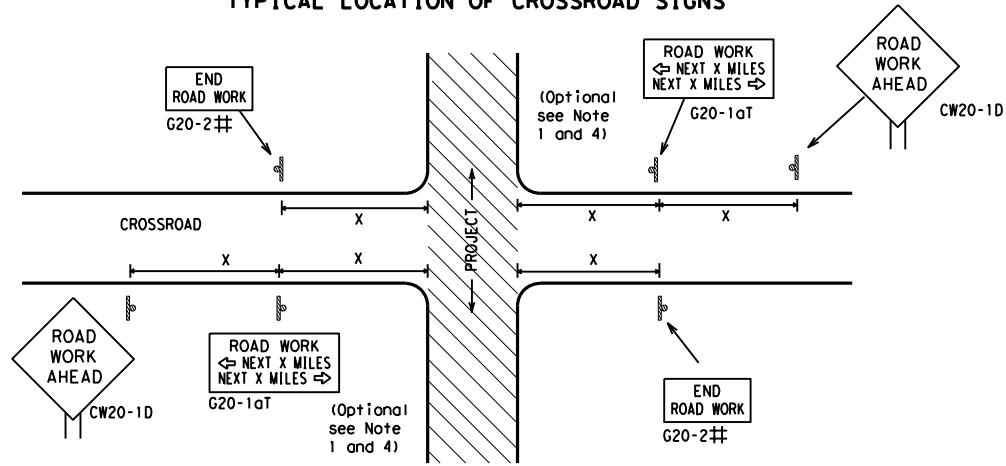
**BARRICADE AND CONSTRUCTION
 GENERAL NOTES
 AND REQUIREMENTS**

BC (1) -21

FILE:	bc-21.dgn	DN:	TxDOT	CK:	TxDOT	DW:	TxDOT	CR:	TxDOT
© TxDOT	November 2002	CONT	0008	SECT	15	JOB	059	HIGHWAY	IH 820
REVISIONS		DIST	COUNTY		SHEET NO.				
4-03	7-13	FTW	TARRANT		6				
9-07	8-14								
5-10	5-21								

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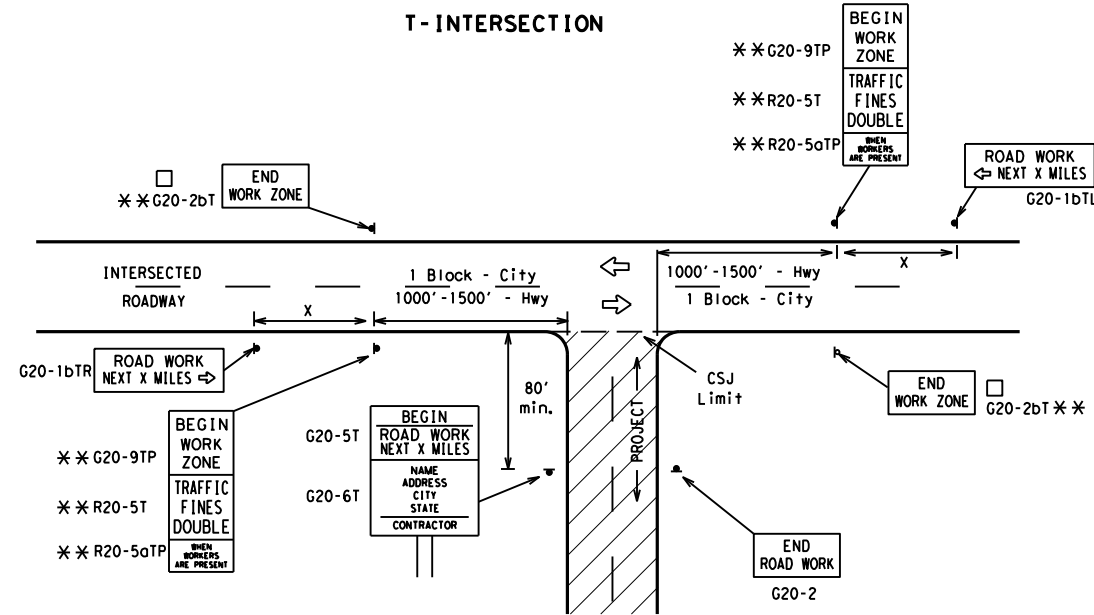
TYPICAL LOCATION OF CROSSROAD SIGNS



May be mounted on back of "ROAD WORK AHEAD" (CW20-1D) sign with approval of Engineer. (See note 2 below)

- The typical minimum signing on a crossroad approach should be a "ROAD WORK AHEAD" (CW20-1D) sign and a (G20-2) "END ROAD WORK" sign, unless noted otherwise in plans.
- The Engineer may use the reduced size 36" x 36" ROAD WORK AHEAD (CW20-1D) sign mounted back to back with the reduced size 36" x 18" "END ROAD WORK" (G20-2) sign on low volume crossroads (see Note 4 under "Typical Construction Warning Sign Size and Spacing"). See the "Standard Highway Sign Designs for Texas" manual for sign details. The Engineer may omit the advance warning signs on low volume crossroads. The Engineer will determine whether a road is low volume as per TMUTCD Part 5. This information shall be shown in the plans.
- Based on existing field conditions, the Engineer/Inspector may require additional signs such as FLAGGER AHEAD, LOOSE GRAVEL, or other appropriate signs. When additional signs are required, these signs will be considered part of the minimum requirements. The Engineer/Inspector will determine the proper location and spacing of any sign not shown on the BC sheets, Traffic Control Plan sheets or the Work Zone Standard Sheets.
- The "ROAD WORK NEXT X MILES" (G20-1aT) sign shall be required at high volume crossroads to advise motorists of the length of construction in either direction from the intersection. The Engineer will determine whether a roadway is considered high volume.
- Additional traffic control devices may be shown elsewhere in the plans for higher volume crossroads.
- When work occurs in the intersection area, appropriate traffic control devices, as shown elsewhere in the plans or as determined by the Engineer/Inspector, shall be in place.

T-INTERSECTION



CSJ LIMITS AT T-INTERSECTION

- The Engineer will determine the types and location of any additional traffic control devices, such as a flagger and accompanying signs, or other signs, that should be used when work is being performed at or near an intersection.
- If construction closes the road at a T-intersection, the Contractor shall place the "CONTRACTOR NAME" (G20-6T) sign behind the Type 3 Barricades for the road closure (see BC(10) also). The "ROAD WORK NEXT X MILES" left arrow (G20-1bTL) and "ROAD WORK NEXT X MILES" right arrow (G20-1bTR) signs shall be replaced by the detour signing called for in the plans.

TYPICAL CONSTRUCTION WARNING SIGN SIZE AND SPACING^{1,5,6}

Sign Number or Series	SIZE		SPACING	
	Conventional Road	Expressway/Freeway	Posted Speed MPH	Sign Δ Spacing "x" Feet (Apprx.)
CW20 ⁴	48" x 48"	48" x 48"	30	120
CW21			35	160
CW22			40	240
CW23			45	320
CW25			50	400
CW1, CW2, CW7, CW8, CW9, CW11, CW14	36" x 36"	48" x 48"	55	500 ²
CW3, CW4, CW5, CW6, CW8-3, CW10, CW12	48" x 48"	48" x 48"	60	600 ²
			65	700 ²
			70	800 ²
			75	900 ²
			80	1000 ²
			*	* ³

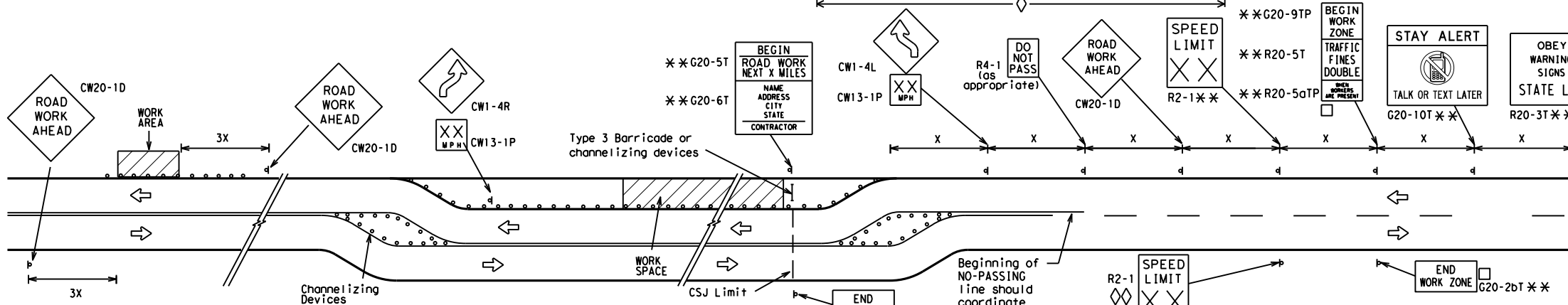
* For typical sign spacings on divided highways, expressways and freeways, see Part 6 of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) typical application diagrams or TCP Standard Sheets.

Δ Minimum distance from work area to first Advance Warning sign nearest the work area and/or distance between each additional sign.

GENERAL NOTES

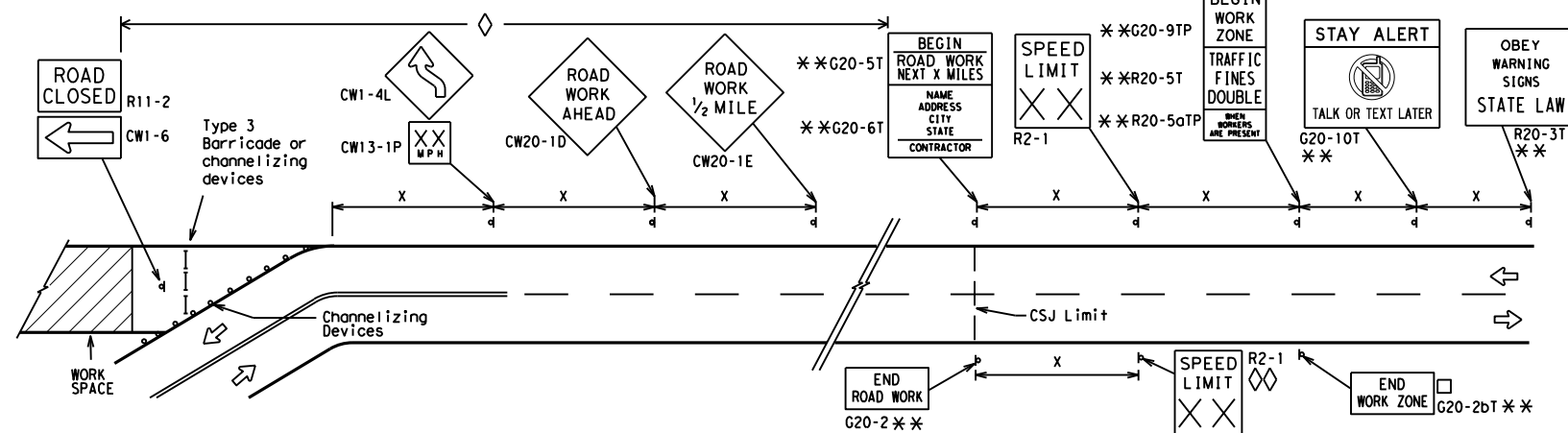
- Special or larger size signs may be used as necessary.
- Distance between signs should be increased as required to have 1500 feet advance warning.
- Distance between signs should be increased as required to have 1/2 mile or more advance warning.
- 36" x 36" "ROAD WORK AHEAD" (CW20-1D) signs may be used on low volume crossroads at the discretion of the Engineer as per TMUTCD Part 5. See Note 2 under "Typical Location of Crossroad Signs".
- Only diamond shaped warning sign sizes are indicated.
- See sign size listing in "TMUTCD", Sign Appendix or the "Standard Highway Sign Designs for Texas" manual for complete list of available sign design sizes.

WORK AREAS IN MULTIPLE LOCATIONS WITHIN CSJ LIMITS



When extended distances occur between minimal work spaces, the Engineer/Inspector should ensure additional "ROAD WORK AHEAD" (CW20-1D) signs are placed in advance of these work areas to remind drivers they are still within the project limits. See the applicable TCP sheets for exact location and spacing of signs and channelizing devices.

SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING DOWNSTREAM OF THE CSJ LIMITS



NOTES

- The Contractor shall determine the appropriate distance to be placed on the G20-1 series signs and "BEGIN ROAD WORK NEXT X MILES" (G20-5T) sign for each specific project. This distance shall replace the "x" and shall be rounded to the nearest whole mile with the approval of the Engineer. No decimals shall be used.
- The "BEGIN WORK ZONE" (G20-9TP) and "END WORK ZONE" (G20-2bT) shall be used as shown on the sample layout when advance signs are required outside the CSJ Limits. They inform the motorist of entering or leaving a part of the work zone lying outside the CSJ Limits where traffic fines may double if workers are present.
 - CSJ limit signing is required for highway construction and maintenance work, with the exception of mobile operations.
 - Area for placement of "ROAD WORK AHEAD" (CW20-1D) sign and other signs or devices as called for on the Traffic Control Plan.
 - Contractor will install a regulatory speed limit sign at the end of the work zone.

LEGEND	
—	Type 3 Barricade
○ ○ ○	Channelizing Devices
■	Sign
X	See Typical Construction Warning Sign Size and Spacing chart or the TMUTCD for sign spacing requirements.

SHEET 2 OF 12



BARRICADE AND CONSTRUCTION PROJECT LIMIT

BC (2) - 21

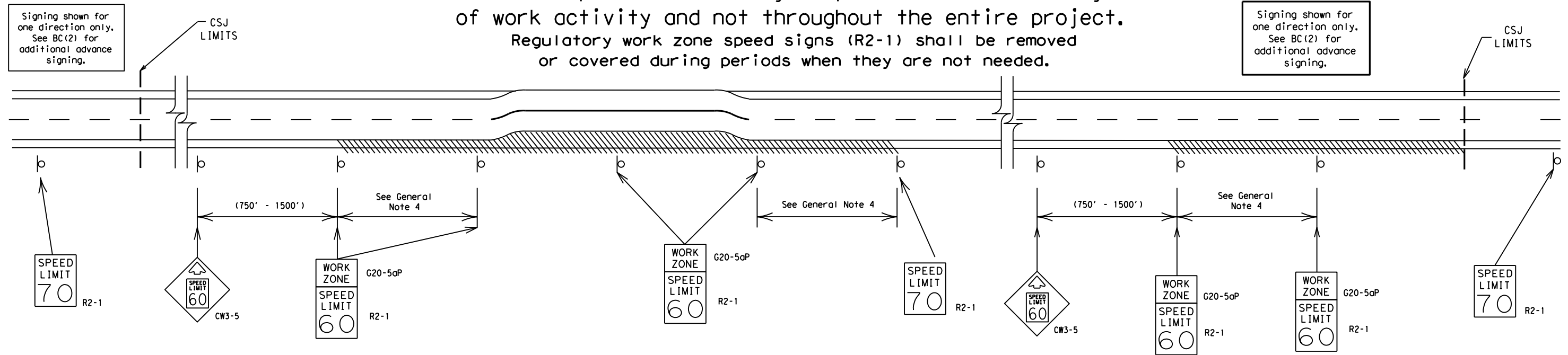
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CR: TxDOT
© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
9-07 8-14	DIST	COUNTY	SHEET NO.	
7-13 5-21	FTW	TARRANT	7	

DATE: FILE:

TYPICAL APPLICATION OF WORK ZONE SPEED LIMIT SIGNS

Work zone speed limits shall be regulatory, established in accordance with the "Procedures for Establishing Speed Zones," and approved by the Texas Transportation Commission, or by City Ordinance when within Incorporated City Limits.

Reduced speeds should only be posted in the vicinity of work activity and not throughout the entire project. Regulatory work zone speed signs (R2-1) shall be removed or covered during periods when they are not needed.



GUIDANCE FOR USE:

LONG/INTERMEDIATE TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit should be included on the design of the traffic control plans when restricted geometrics with a lower design speed are present in the work zone and modification of the geometrics to a higher design speed is not feasible.

Long/Intermediate Term Work Zone Speed Limit signs, when approved as described above, should be posted and visible to the motorist when work activity is present. Work activity may also be defined as a change in the roadway that requires a reduced speed for motorists to safely negotiate the work area, including:

- rough road or damaged pavement surface
- substantial alteration of roadway geometrics (diversions)
- construction detours
- grade
- width
- other conditions readily apparent to the driver

As long as any of these conditions exist, the work zone speed limit signs should remain in place.

SHORT TERM WORK ZONE SPEED LIMITS

This type of work zone speed limit may be included on the design of the traffic control plans when workers or equipment are not behind concrete barrier, when work activity is within 10 feet of the traveled way or actually in the traveled way.

Short Term Work Zone Speed Limit signs should be posted and visible to the motorists only when work activity is present. When work activity is not present, signs shall be removed or covered. (See Removing or Covering on BC(4)).

GENERAL NOTES

- Regulatory work zone speed limits should be used only for sections of construction projects where speed control is of major importance.
- Regulatory work zone speed limit signs shall be placed on supports at a 7 foot minimum mounting height.
- Speed zone signs are illustrated for one direction of travel and are normally posted for each direction of travel.
- Frequency of work zone speed limit signs should be:

40 mph and greater	0.2 to 2 miles
35 mph and less	0.2 to 1 mile
- Regulatory speed limit signs shall have black legend and border on a white reflective background (See "Reflective Sheeting" on BC(4)).
- Fabrication, erection and maintenance of the "ADVANCE SPEED LIMIT" (CW3-5) sign, "WORK ZONE" (G20-5aP) plaque and the "SPEED LIMIT" (R2-1) signs shall not be paid for directly, but shall be considered subsidiary to Item 502.
- Turning signs from view, laying signs over or down will not be allowed, unless as otherwise noted under "REMOVING OR COVERING" on BC(4).
- Techniques that may help reduce traffic speeds include but are not limited to:
 - Law enforcement.
 - Flagger stationed next to sign.
 - Portable changeable message sign (PCMS).
 - Low-power (drone) radar transmitter.
 - Speed monitor trailers or signs.
- Speeds shown on details above are for illustration only. Work Zone Speed Limits should only be posted as approved for each project.
- For more specific guidance concerning the type of work, work zone conditions and factors impacting allowable regulatory construction speed zone reduction see TxDOT form #1204 in the TxDOT e-form system.

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SHEET 3 OF 12



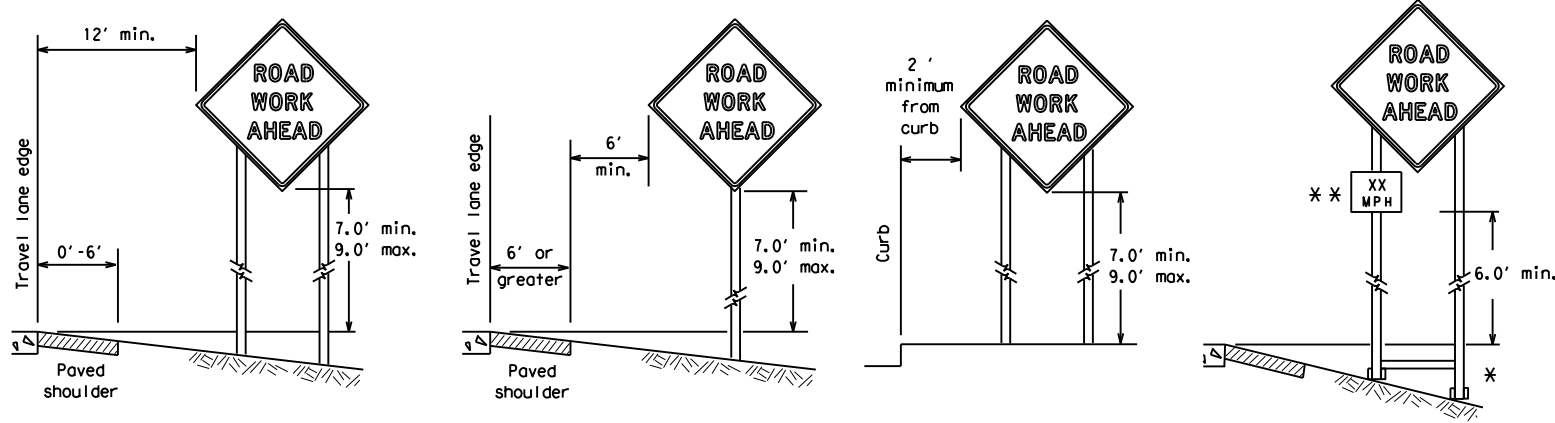
BARRICADE AND CONSTRUCTION WORK ZONE SPEED LIMIT

BC (3) - 21

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REVISIONS		0008	15	059	IH 820				
9-07	8-14	DIST	COUNTY		SHEET NO.				
7-13	5-21	FTW	TARRANT		8				

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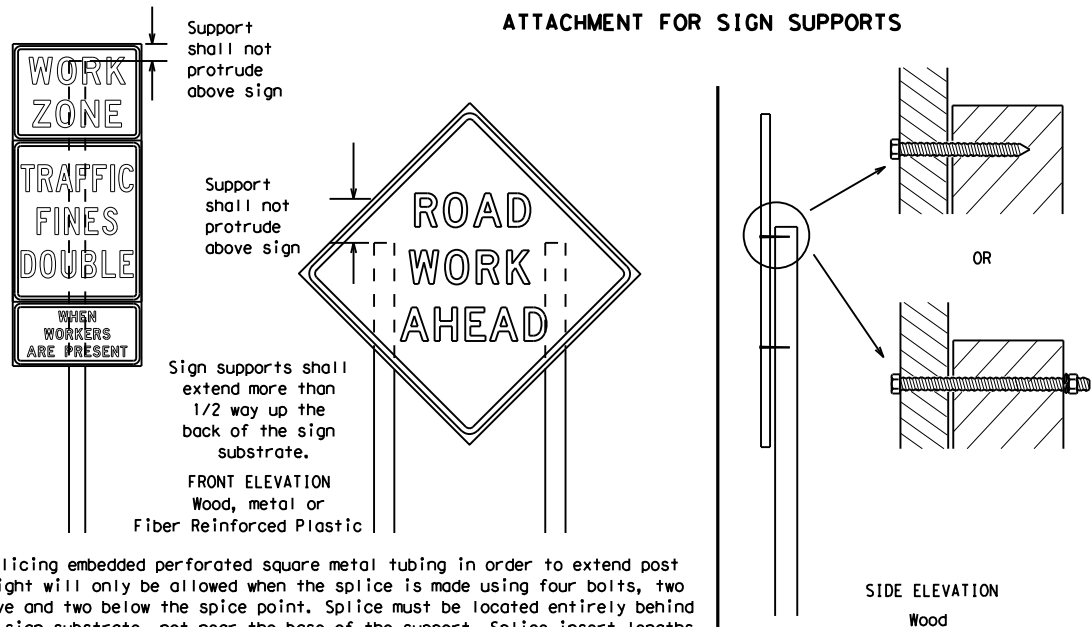
TYPICAL MINIMUM CLEARANCES FOR LONG TERM AND INTERMEDIATE TERM SIGNS



* When placing skid supports on unlevel ground, the leg post lengths must be adjusted so the sign appears straight and plumb. Objects shall NOT be placed under skids as a means of leveling.

** When plaques are placed on dual-leg supports, they should be attached to the upright nearest the travel lane. Supplemental plaques (advisory or distance) should not cover the surface of the parent sign.

ATTACHMENT FOR SIGN SUPPORTS



Nails shall NOT be allowed.
Each sign shall be attached directly to the sign support. Multiple signs shall not be joined or spliced by any means. Wood supports shall not be extended or repaired by splicing or other means.

Splicing embedded perforated square metal tubing in order to extend post height will only be allowed when the splice is made using four bolts, two above and two below the splice point. Splice must be located entirely behind the sign substrate, not near the base of the support. Splice insert lengths should be at least 5 times nominal post size, centered on the splice and of at least the same gauge material.

GENERAL NOTES FOR WORK ZONE SIGNS

- Contractor shall install and maintain signs in a straight and plumb condition and/or as directed by the Engineer.
- Wooden sign posts shall be painted white.
- Barricades shall NOT be used as sign supports.
- All signs shall be installed in accordance with the plans or as directed by the Engineer. Signs shall be used to regulate, warn, and guide the traveling public safely through the work zone.
- The Contractor may furnish either the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD). The Engineer/Inspector may require the Contractor to furnish other work zone signs that are shown in the TMUTCD but may have been omitted from the plans. Any variation in the plans shall be documented by written agreement between the Engineer and the Contractor's Responsible Person. All changes must be documented in writing before being implemented. This can include documenting the changes in the Inspector's TxDOT diary and having both the Inspector and Contractor initial and date the agreed upon changes.
- The Contractor shall furnish sign supports listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCD) for small roadside signs. Supports for temporary large roadside signs shall meet the requirements detailed on the Temporary Large Roadside Signs (TLRS) standard sheets. The Contractor shall install the sign support in accordance with the manufacturer's recommendations. If there is a question regarding installation procedures, the Contractor shall furnish the Engineer a copy of the manufacturer's installation recommendations so the Engineer can verify the correct procedures are being followed.
- The Contractor is responsible for installing signs on approved supports and replacing signs with damaged or cracked substrates and/or damaged or marred reflective sheeting as directed by the Engineer/Inspector.
- Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1 inch.
- The Contractor shall replace damaged wood posts. New or damaged wood sign posts shall not be spliced.

DURATION OF WORK (as defined by the "Texas Manual on Uniform Traffic Control Devices" Part 6)

- The types of sign supports, sign mounting height, the size of signs, and the type of sign substrates can vary based on the type of work being performed. The Engineer is responsible for selecting the appropriate size sign for the type of work being performed. The Contractor is responsible for ensuring the sign support, sign mounting height and substrate meets manufacturer's recommendations in regard to crashworthiness and duration of work requirements.
 - Long-term stationary - work that occupies a location more than 3 days.
 - Intermediate-term stationary - work that occupies a location more than one daylight period up to 3 days, or nighttime work lasting more than one hour.
 - Short-term stationary - daytime work that occupies a location for more than 1 hour in a single daylight period.
 - Short, duration - work that occupies a location up to 1 hour.
 - Mobile - work that moves continuously or intermittently (stopping for up to approximately 15 minutes.)

SIGN MOUNTING HEIGHT

- The bottom of Long-term/Intermediate-term signs shall be at least 7 feet, but not more than 9 feet, above the paved surface, except as shown for supplemental plaques mounted below other signs.
- The bottom of Short-term/Short Duration signs shall be a minimum of 1 foot above the pavement surface but no more than 2 feet above the ground.
- Long-term/Intermediate-term Signs may be used in lieu of Short-term/Short Duration signing.
- Short-term/Short Duration signs shall be used only during daylight and shall be removed at the end of the workday or raised to appropriate Long-term/Intermediate sign height.
- Regulatory signs shall be mounted at least 7 feet, but not more than 9 feet, above the paved surface regardless of work duration.

SIZE OF SIGNS

- The Contractor shall furnish the sign sizes shown on BC (2) unless otherwise shown in the plans or as directed by the Engineer.

SIGN SUBSTRATES

- The Contractor shall ensure the sign substrate is installed in accordance with the manufacturer's recommendations for the type of sign support that is being used. The CWZTCD lists each substrate that can be used on the different types and models of sign supports.
- "Mesh" type materials are NOT an approved sign substrate, regardless of the tightness of the weave.
- All wooden individual sign panels fabricated from 2 or more pieces shall have one or more plywood cleat, 1/2" thick by 6" wide, fastened to the back of the sign and extending fully across the sign. The cleat shall be attached to the back of the sign using wood screws that do not penetrate the face of the sign panel. The screws shall be placed on both sides of the splice and spaced at 6" centers. The Engineer may approve other methods of splicing the sign face.

REFLECTIVE SHEETING

- All signs shall be retroreflective and constructed of sheeting meeting the color and retro-reflectivity requirements of DMS-8300 for rigid signs or DMS-8310 for roll-up signs. The web address for DMS specifications is shown on BC(1).
- White sheeting, meeting the requirements of DMS-8300 Type A, shall be used for signs with a white background.
- Orange sheeting, meeting the requirements of DMS-8300 Type B_{FL} or Type C_{FL}, shall be used for rigid signs with orange backgrounds.

SIGN LETTERS

- All sign letters and numbers shall be clear, and open rounded type uppercase alphabet letters as approved by the Federal Highway Administration (FHWA) and as published in the "Standard Highway Sign Design for Texas" manual. Signs, letters and numbers shall be of first class workmanship in accordance with Department Standards and Specifications.

REMOVING OR COVERING

- When sign messages may be confusing or do not apply, the signs shall be removed or completely covered.
- Long-term stationary or intermediate stationary signs installed on square metal tubing may be turned away from traffic 90 degrees when the sign message is not applicable. This technique may not be used for signs installed in the median of divided highways or near any intersections where the sign may be seen from approaching traffic.
- Signs installed on wooden skids shall not be turned at 90 degree angles to the roadway. These signs should be removed or completely covered when not required.
- When signs are covered, the material used shall be opaque, such as heavy mil black plastic, or other materials which will cover the entire sign face and maintain their opaque properties under automobile headlights at night, without damaging the sign sheeting.
- Burlap shall NOT be used to cover signs.
- Duct tape or other adhesive material shall NOT be affixed to a sign face.
- Signs and anchor stubs shall be removed and holes backfilled upon completion of work.

SIGN SUPPORT WEIGHTS

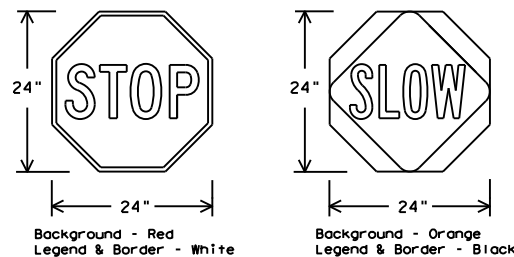
- Where sign supports require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand should be used.
- The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
- Rock, concrete, iron, steel or other solid objects shall not be permitted for use as sign support weights.
- Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
- Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall NOT be used.
- Rubber ballasts designed for channelizing devices should not be used for ballast on portable sign supports. Sign supports designed and manufactured with rubber bases may be used when shown on the CWZTCD list.
- Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign support.
- Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

FLAGS ON SIGNS

- Flags may be used to draw attention to warning signs. When used, the flag shall be 16 inches square or larger and shall be orange or fluorescent red-orange in color. Flags shall not be allowed to cover any portion of the sign face.

STOP/SLOW PADDLES

- STOP/SLOW paddles are the primary method to control traffic by flaggers. The STOP/SLOW paddle size should be 24" x 24".
- STOP/SLOW paddles shall be retroreflective when used at night.
- STOP/SLOW paddles may be attached to a staff with a minimum length of 6' to the bottom of the sign.
- Any lights incorporated into the STOP or SLOW paddle faces shall only be as specifically described in Section 6E.03 Hand Signaling Devices in the TMUTCD.



SHEETING REQUIREMENTS (WHEN USED AT NIGHT)		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	ORANGE	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND & BORDER	WHITE	TYPE B OR C SHEETING
LEGEND & BORDER	BLACK	ACRYLIC NON-REFLECTIVE FILM

CONTRACTOR REQUIREMENTS FOR MAINTAINING PERMANENT SIGNS WITHIN THE PROJECT LIMITS

- Permanent signs are used to give notice of traffic laws or regulations, call attention to conditions that are potentially hazardous to traffic operations, show route designations, destinations, directions, distances, services, points of interest, and other geographical, recreational, specific service (LOGO), or cultural information. Drivers proceeding through a work zone need the same, if not better route guidance as normally installed on a roadway without construction.
- When permanent regulatory or warning signs conflict with work zone conditions, remove or cover the permanent signs until the permanent sign message matches the roadway condition. For details for covering large guide signs see the TS-CD standard.
- When existing permanent signs are moved and relocated due to construction purposes, they shall be visible to motorists at all times.
- If existing signs are to be relocated on their original supports, they shall be installed on crashworthy bases as shown on the SMD Standard sheets. The signs shall meet the required mounting heights shown on the BC Sheets or the SMD Standards. This work should be paid for under the appropriate pay item for relocating existing signs.
- If permanent signs are to be removed and relocated using temporary supports, the Contractor shall use crashworthy supports as shown on the BC standard sheets, TLRS standard sheets or the CWZTCD list. The signs shall meet the required mounting heights shown on the BC, or the SMD standard sheets during construction. This work should be paid for under the appropriate pay item for relocating existing signs.
- Any sign or traffic control device that is struck or damaged by the Contractor or his/her construction equipment shall be replaced as soon as possible by the Contractor to ensure proper guidance for the motorists. This will be subsidiary to Item 502.

SHEET 4 OF 12



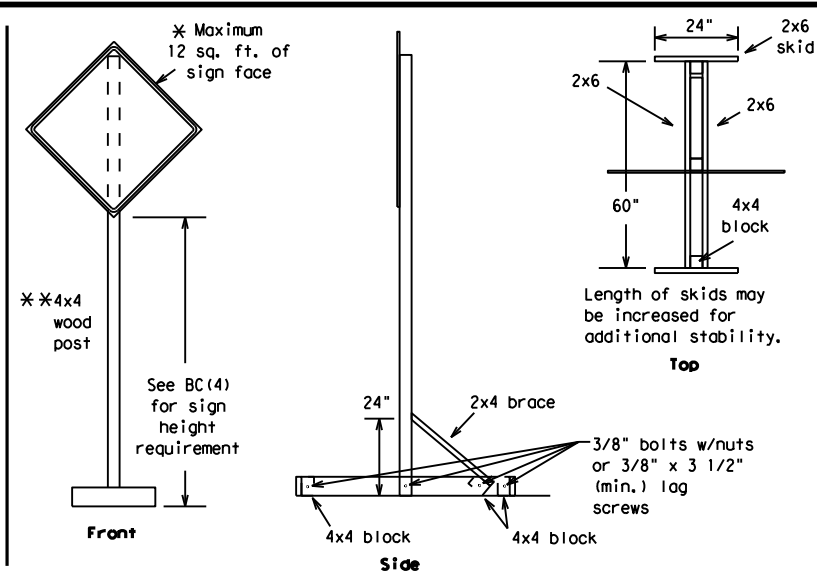
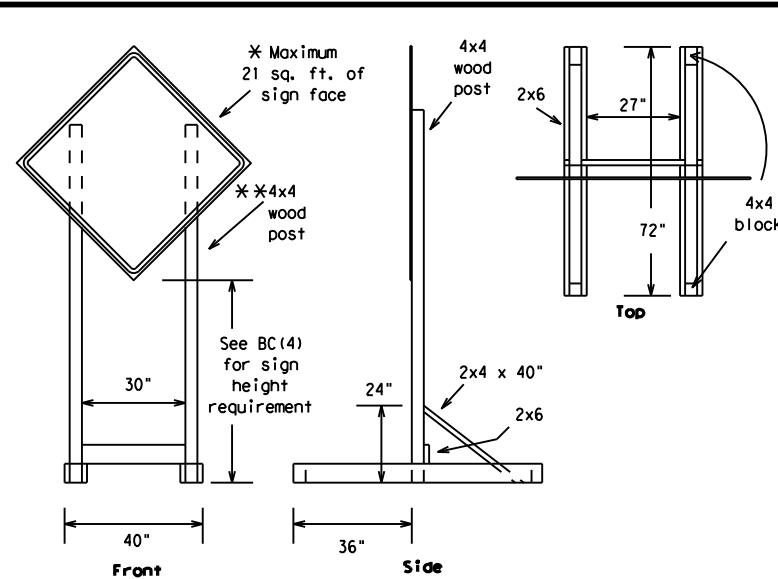
BARRICADE AND CONSTRUCTION TEMPORARY SIGN NOTES

BC (4) - 21

FILE: bc-21.dgn	DN: TxDOT	CR: TxDOT	OW: TxDOT	CK: TxDOT
© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
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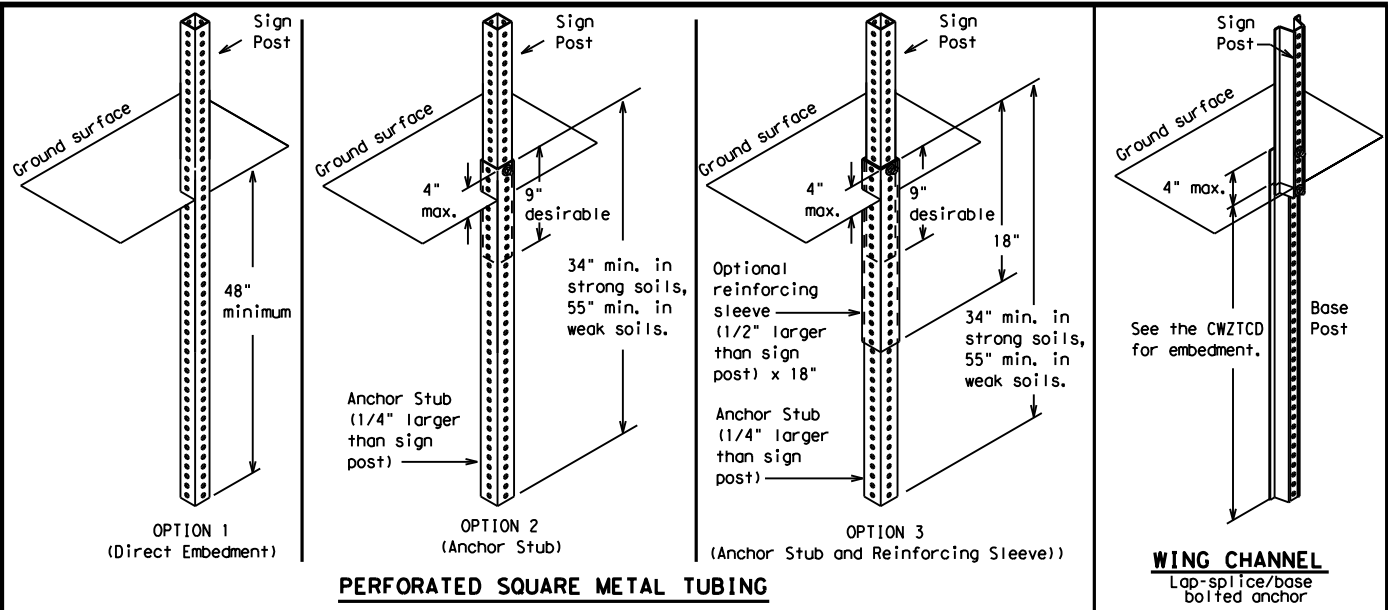
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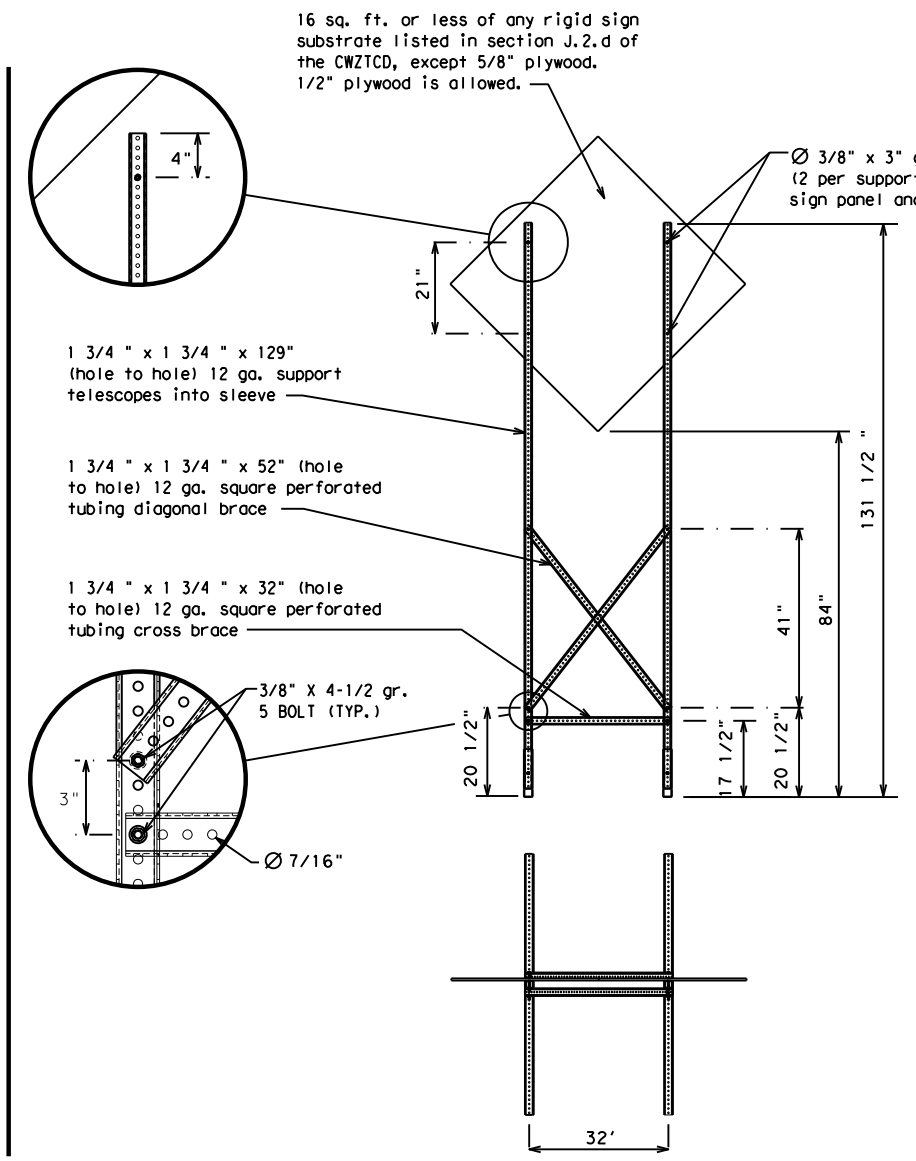
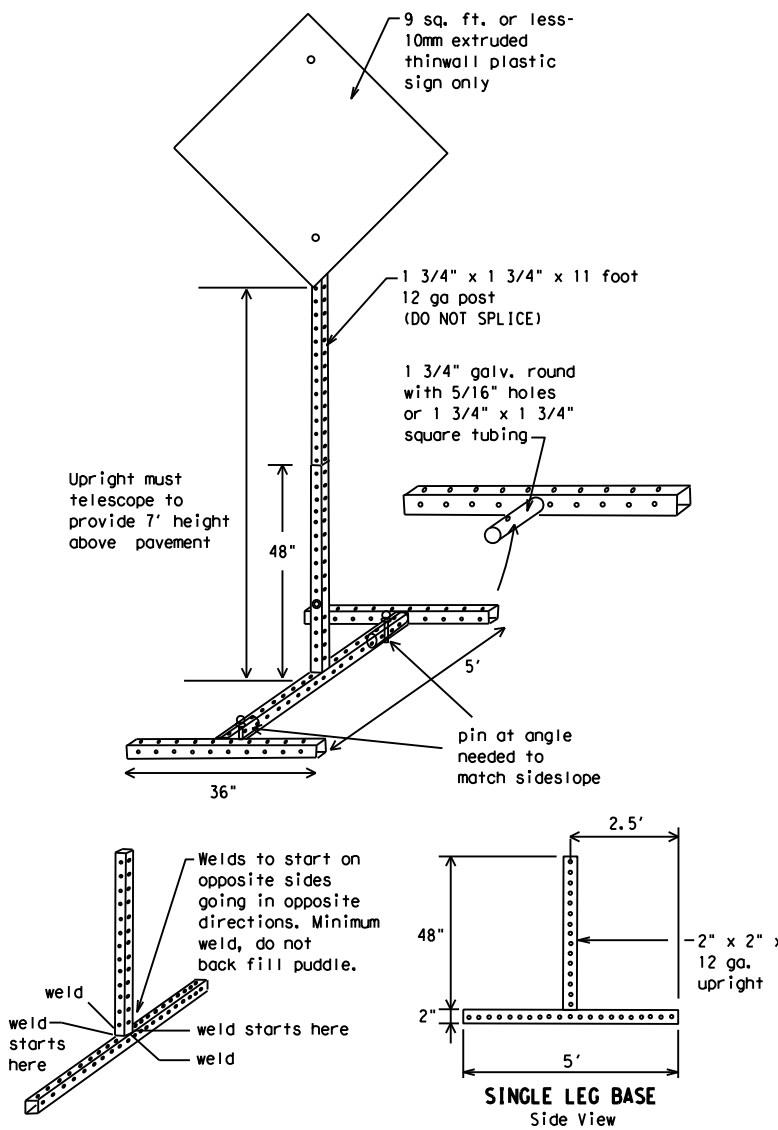
SKID MOUNTED WOOD SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS



GROUND MOUNTED SIGN SUPPORTS

Refer to the CWZTCD and the manufacturer's installation procedure for each type sign support. The maximum sign square footage shall adhere to the manufacturer's recommendation. Two post installations can be used for larger signs.



SKID MOUNTED PERFORATED SQUARE STEEL TUBING SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS

WEDGE ANCHORS
Both steel and plastic Wedge Anchor Systems as shown on the SMD Standard Sheets may be used as temporary sign supports for signs up to 10 square feet of sign face. They may be set in concrete or in sturdy soils if approved by the Engineer. (See web address for "Traffic Engineering Standard Sheets" on BC(1)).

OTHER DESIGNS
MORE DETAILS OF APPROVED LONG/INTERMEDIATE AND SHORT TERM SUPPORTS CAN BE FOUND ON THE CWZTCD LIST. SEE BC(1) FOR WEBSITE LOCATION.

- GENERAL NOTES**
- Nails may be used in the assembly of wooden sign supports, but 3/8" bolts with nuts or 3/8" x 3 1/2" lag screws must be used on every joint for final connection.
 - No more than 2 sign posts shall be placed within a 7 ft. circle, except for specific materials noted on the CWZTCD List.
 - When project is completed, all sign supports and foundations shall be removed from the project site. This will be considered subsidiary to Item 502.
- * See BC(4) for definition of "Work Duration."
 - ** Wood sign posts MUST be one piece. Splicing will NOT be allowed. Posts shall be painted white.
 - See the CWZTCD for the type of sign substrate that can be used for each approved sign support.

SHEET 5 OF 12



BARRICADE AND CONSTRUCTION TYPICAL SIGN SUPPORT

BC(5) - 21

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WHEN NOT IN USE, REMOVE THE PCMS FROM THE RIGHT-OF-WAY OR PLACE THE PCMS BEHIND BARRIER OR GUARDRAIL WITH SIGN PANEL TURNED PARALLEL TO TRAFFIC

RECOMMENDED PHASES AND FORMATS FOR PCMS MESSAGES DURING ROADWORK ACTIVITIES

(The Engineer may approve other messages not specifically covered here.)

PORTABLE CHANGEABLE MESSAGE SIGNS

- The Engineer/Inspector shall approve all messages used on portable changeable message signs (PCMS).
- Messages on PCMS should contain no more than 8 words (about four to eight characters per word), not including simple words such as "TO," "FOR," "AT," etc.
- Messages should consist of a single phase, or two phases that alternate. Three-phase messages are not allowed. Each phase of the message should convey a single thought, and must be understood by itself.
- Use the word "EXIT" to refer to an exit ramp on a freeway; i.e., "EXIT CLOSED." Do not use the term "RAMP."
- Always use the route or interstate designation (IH, US, SH, FM) along with the number when referring to a roadway.
- When in use, the bottom of a stationary PCMS message panel should be a minimum 7 feet above the roadway, where possible.
- The message term "WEEKEND" should be used only if the work is to start on Saturday morning and end by Sunday evening at midnight. Actual days and hours of work should be displayed on the PCMS if work is to begin on Friday evening and/or continue into Monday morning.
- The Engineer/Inspector may select one of two options which are available for displaying a two-phase message on a PCMS. Each phase may be displayed for either four seconds each or for three seconds each.
- Do not "flash" messages or words included in a message. The message should be steady burn or continuous while displayed.
- Do not present redundant information on a two-phase message; i.e., keeping two lines of the message the same and changing the third line.
- Do not use the word "Danger" in message.
- Do not display the message "LANES SHIFT LEFT" or "LANES SHIFT RIGHT" on a PCMS. Drivers do not understand the message.
- Do not display messages that scroll horizontally or vertically across the face of the sign.
- The following table lists abbreviated words and two-word phrases that are acceptable for use on a PCMS. Both words in a phrase must be displayed together. Words or phrases not on this list should not be abbreviated, unless shown in the TMUTCD.
- PCMS character height should be at least 18 inches for trailer mounted units. They should be visible from at least 1/2 (.5) mile and the text should be legible from at least 600 feet at night and 800 feet in daylight. Truck mounted units must have a character height of 10 inches and must be legible from at least 400 feet.
- Each line of text should be centered on the message board rather than left or right justified.
- If disabled, the PCMS should default to an illegible display that will not alarm motorists and will only be used to alert workers that the PCMS has malfunctioned. A pattern such as a series of horizontal solid bars is appropriate.

Phase 1: Condition Lists

Road/Lane/Ramp Closure List

FREEWAY CLOSED X MILE	FRONTAGE ROAD CLOSED
ROAD CLOSED AT SH XXX	SHOULDER CLOSED XXX FT
ROAD CLSD AT FM XXXX	RIGHT LN CLOSED XXX FT
RIGHT X LANES CLOSED	RIGHT X LANES OPEN
CENTER LANE CLOSED	DAYTIME LANE CLOSURES
NIGHT LANE CLOSURES	I-XX SOUTH EXIT CLOSED
VARIOUS LANES CLOSED	EXIT XXX CLOSED X MILE
EXIT CLOSED	RIGHT LN TO BE CLOSED
MALL DRIVEWAY CLOSED	X LANES CLOSED TUE - FRI
XXXXXXXXX BLVD CLOSED	

Other Condition List

ROADWORK XXX FT	ROAD REPAIRS XXXX FT
FLAGGER XXXX FT	LANE NARROWS XXXX FT
RIGHT LN NARROWS XXXX FT	TWO-WAY TRAFFIC XX MILE
MERGING TRAFFIC XXXX FT	CONST TRAFFIC XXX FT
LOOSE GRAVEL XXXX FT	UNEVEN LANES XXXX FT
DETOUR X MILE	ROUGH ROAD XXXX FT
ROADWORK PAST SH XXXX	ROADWORK NEXT FRI-SUN
BUMP XXXX FT	US XXX EXIT X MILES
TRAFFIC SIGNAL XXXX FT	LANES SHIFT *

* LANES SHIFT in Phase 1 must be used with STAY IN LANE in Phase 2.

Phase 2: Possible Component Lists

Action to Take/Effect on Travel List

MERGE RIGHT	FORM X LINES RIGHT
DETOUR NEXT X EXITS	USE XXXXX RD EXIT
USE EXIT XXX	USE EXIT I-XX NORTH
STAY ON US XXX SOUTH	USE I-XX E TO I-XX N
TRUCKS USE US XXX N	WATCH FOR TRUCKS
WATCH FOR TRUCKS	EXPECT DELAYS
EXPECT DELAYS	PREPARE TO STOP
REDUCE SPEED XXX FT	END SHOULDER USE
USE OTHER ROUTES	WATCH FOR WORKERS
STAY IN LANE *	

Location List

AT FM XXXX
BEFORE RAILROAD CROSSING
NEXT X MILES
PAST US XXX EXIT
XXXXXXXXX TO XXXXXXXX
US XXX TO FM XXXX

Warning List

SPEED LIMIT XX MPH
MAXIMUM SPEED XX MPH
MINIMUM SPEED XX MPH
ADVISORY SPEED XX MPH
RIGHT LANE EXIT
USE CAUTION
DRIVE SAFELY
DRIVE WITH CARE

** Advance Notice List

TUE-FRI XX AM-X PM
APR XX-XX X PM-X AM
BEGINS MONDAY
BEGINS MAY XX
MAY X-X XX PM - XX AM
NEXT FRI-SUN
XX AM TO XX PM
NEXT TUE AUG XX
TONIGHT XX PM-XX AM

** See Application Guidelines Note 6.

APPLICATION GUIDELINES

- Only 1 or 2 phases are to be used on a PCMS.
- The 1st phase (or both) should be selected from the "Road/Lane/Ramp Closure List" and the "Other Condition List".
- A 2nd phase can be selected from the "Action to Take/Effect on Travel, Location, General Warning, or Advance Notice Phase Lists".
- A Location Phase is necessary only if a distance or location is not included in the first phase selected.
- If two PCMS are used in sequence, they must be separated by a minimum of 1000 ft. Each PCMS shall be limited to two phases, and should be understandable by themselves.
- For advance notice, when the current date is within seven days of the actual work date, calendar days should be replaced with days of the week. Advance notification should typically be for no more than one week prior to the work.

WORDING ALTERNATIVES

- The words RIGHT, LEFT and ALL can be interchanged as appropriate.
- Roadway designations IH, US, SH, FM and LP can be interchanged as appropriate.
- EAST, WEST, NORTH and SOUTH (or abbreviations E, W, N and S) can be interchanged as appropriate.
- Highway names and numbers replaced as appropriate.
- ROAD, HIGHWAY and FREEWAY can be interchanged as needed.
- AHEAD may be used instead of distances if necessary.
- FT and MI, MILE and MILES interchanged as appropriate.
- AT, BEFORE and PAST interchanged as needed.
- Distances or AHEAD can be eliminated from the message if a location phase is used.

PCMS SIGNS WITHIN THE R.O.W. SHALL BE BEHIND GUARDRAIL OR CONCRETE BARRIER OR SHALL HAVE A MINIMUM OF FOUR (4) PLASTIC DRUMS PLACED PERPENDICULAR TO TRAFFIC ON THE UPSTREAM SIDE OF THE PCMS, WHEN EXPOSED TO ONE DIRECTION OF TRAFFIC. WHEN EXPOSED TO TWO WAY TRAFFIC, THE FOUR DRUMS SHOULD BE PLACED WITH ONE DRUM AT EACH OF THE FOUR CORNERS OF THE UNIT.

FULL MATRIX PCMS SIGNS

- When Full Matrix PCMS signs are used, the character height and legibility/visibility requirements shall be maintained as listed in Note 15 under "PORTABLE CHANGEABLE MESSAGE SIGNS" above.
- When symbol signs, such as the "Flagger Symbol" (CW20-7) are represented graphically on the Full Matrix PCMS sign and, with the approval of the Engineer, it shall maintain the legibility/visibility requirement listed above.
- When symbol signs are represented graphically on the Full Matrix PCMS, they shall only supplement the use of the static sign represented, and shall not substitute for, or replace that sign.
- A full matrix PCMS may be used to simulate a flashing arrow board provided it meets the visibility, flash rate and dimming requirements on BC(7), for the same size arrow.

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WORD OR PHRASE	ABBREVIATION	WORD OR PHRASE	ABBREVIATION
Access Road	ACCS RD	Major	MAJ
Alternate	ALT	Miles	MI
Avenue	AVE	Miles Per Hour	MPH
Best Route	BEST RTE	Minor	MNR
Boulevard	BLVD	Monday	MON
Bridge	BRDG	Normal	NORM
Canal	CANT	North	N
Center	CTR	Northbound	(route) N
Construction Ahead	CONST AHD	Parking	PKING
CROSSING	XING	Road	RD
Detour Route	DETOUR RTE	Right Lane	RT LN
Do Not	DONT	Saturday	SAT
East	E	Service Road	SERV RD
Eastbound	(route) E	Shoulder	SHLDR
Emergency	EMER	Slippery	SLIP
Emergency Vehicle	EMER VEH	South	S
Entrance, Enter	ENT	Southbound	(route) S
Express Lane	EXP LN	Speed	SPD
Expressway	EXPWY	Street	ST
XXXX Feet	XXXX FT	Sunday	SUN
Fog Ahead	FOG AHD	Telephone	PHONE
Freeway	FRWY, FWY	Temporary	TEMP
Freeway Blocked	FWY BLKD	Thursday	THURS
Friday	FRI	To Downtown	TO DWNTN
Hazardous Driving	HAZ DRIVING	Traffic	TRAF
Hazardous Material	HAZMAT	Travelers	TRVLR
High-Occupancy Vehicle	HOV	Tuesday	TUES
Highway	HWY	Time Minutes	TIME MIN
Hour(s)	HR, HRS	Upper Level	UPR LEVEL
Information	INFO	Vehicles (s)	VEH, VEHS
It Is	ITS	Warning	WARN
Junction	JCT	Wednesday	WED
Left	LFT	Weight Limit	WT LIMIT
Left Lane	LFT LN	West	W
Lane Closed	LN CLOSED	Westbound	(route) W
Lower Level	LWR LEVEL	Wet Pavement	WET PVMT
Maintenance	MAINT	Will Not	WONT

Roadway designation # IH-number, US-number, SH-number, FM-number



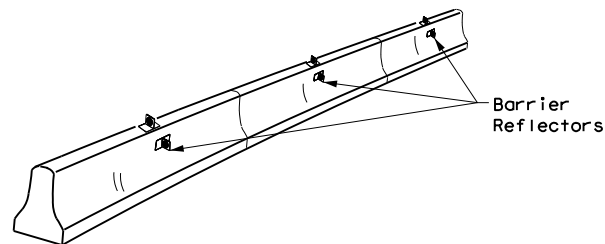
BARRICADE AND CONSTRUCTION PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)

BC (6) - 21

FILE: bc-21.dgn	DN: TxDOT	CR: TxDOT	DW: TxDOT	CK: TxDOT
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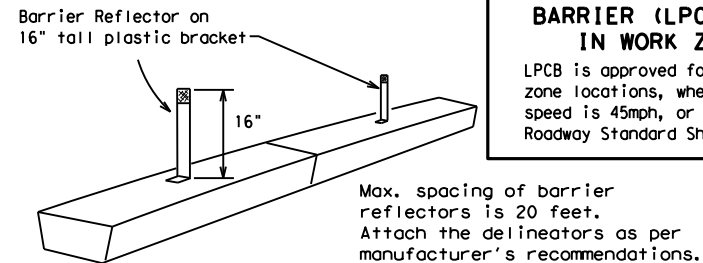
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- Barrier Reflectors shall be pre-qualified, and conform to the color and reflectivity requirements of DMS-8600. A list of prequalified Barrier Reflectors can be found at the Material Producer List web address shown on BC(1).
- Color of Barrier Reflectors shall be as specified in the TMUTCD. The cost of the reflectors shall be considered subsidiary to Item 512.



CONCRETE TRAFFIC BARRIER (CTB)

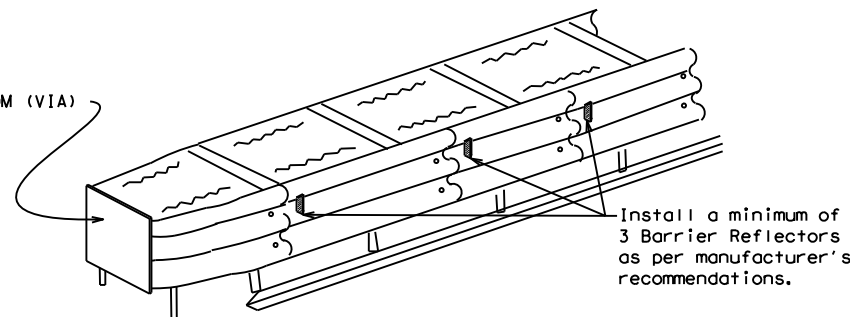
- Where traffic is on one side of the CTB, two (2) Barrier Reflectors shall be mounted in approximately the midsection of each section of CTB. An alternate mounting location is uniformly spaced at one end of each CTB. This will allow for attachment of a barrier grapple without damaging the reflector. The Barrier Reflector mounted on the side of the CTB shall be located directly below the reflector mounted on top of the barrier, as shown in the detail above.
- Where CTB separates two-way traffic, three barrier reflectors shall be mounted on each section of CTB. The reflector unit on top shall have two yellow reflective faces (Bi-Directional) while the reflectors on each side of the barrier shall have one yellow reflective face, as shown in the detail above.
- When CTB separates traffic traveling in the same direction, no barrier reflectors will be required on top of the CTB.
- Barrier Reflector units shall be yellow or white in color to match the edgeline being supplemented.
- Maximum spacing of Barrier Reflectors is forty (40) feet.
- Pavement markers or temporary flexible-reflective roadway marker tabs shall NOT be used as CTB delineation.
- Attachment of Barrier Reflectors to CTB shall be per manufacturer's recommendations.
- Missing or damaged Barrier Reflectors shall be replaced as directed by the Engineer.
- Single slope barriers shall be delineated as shown on the above detail.



LOW PROFILE CONCRETE BARRIER (LPCB) USED IN WORK ZONES

LPCB is approved for use in work zone locations, where the posted speed is 45mph, or less. See Roadway Standard Sheet LPCB.

LOW PROFILE CONCRETE BARRIER (LPCB)



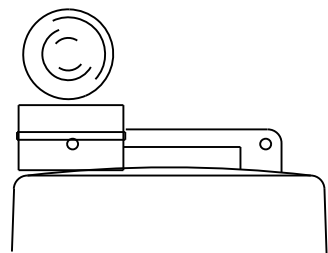
DELINEATION OF END TREATMENTS

END TREATMENTS FOR CTB'S USED IN WORK ZONES
 End treatments used on CTB's in work zones shall meet the appropriate crashworthy standards as defined in the Manual for Assessing Safety Hardware (MASH). Refer to the CWZTCD List for approved end treatments and manufacturers.

BARRIER REFLECTORS FOR CONCRETE TRAFFIC BARRIER AND ATTENUATORS

WARNING LIGHTS

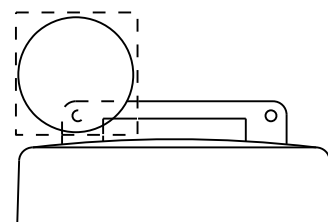
- Warning lights shall meet the requirements of the TMUTCD.
- Warning lights shall NOT be installed on barricades.
- Type A-Low Intensity Flashing Warning Lights are commonly used with drums. They are intended to warn of or mark a potentially hazardous area. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "FL". The Type A Warning Lights shall not be used with signs manufactured with Type B_{FL} or C_{FL} Sheeting meeting the requirements of Departmental Material Specification DMS-8300.
- Type-C and Type D 360 degree Steady Burn Lights are intended to be used in a series for delineation to supplement other traffic control devices. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "SB".
- The Engineer/Inspector or the plans shall specify the location and type of warning lights to be installed on the traffic control devices.
- When required by the Engineer, the Contractor shall furnish a copy of the warning lights certification. The warning light manufacturer will certify the warning lights meet the requirements of the latest ITE Purchase Specifications for Flashing and Steady-Burn Warning Lights.
- When used to delineate curves, Type-C and Type D Steady Burn Lights should only be placed on the outside of the curve, not the inside.
- The location of warning lights and warning reflectors on drums shall be as shown elsewhere in the plans.



Type C Warning Light or approved substitute mounted on a drum adjacent to the travel way.

WARNING LIGHTS MOUNTED ON PLASTIC DRUMS

- Type A flashing warning lights are intended to warn drivers that they are approaching or are in a potentially hazardous area.
- Type A random flashing warning lights are not intended for delineation and shall not be used in a series.
- A series of sequential flashing warning lights placed on channelizing devices to form a merging taper may be used for delineation. If used, the successive flashing of the sequential warning lights should occur from the beginning of the taper to the end of the merging taper in order to identify the desired vehicle path. The rate of flashing for each light shall be 65 flashes per minute, plus or minus 10 flashes.
- Type C and D steady-burn warning lights are intended to be used in a series to delineate the edge of the travel lane on detours, on lane changes, on lane closures, and on other similar conditions.
- Type A, Type C and Type D warning lights shall be installed at locations as detailed on other sheets in the plans.
- Warning lights shall not be installed on a drum that has a sign, chevron or vertical panel.
- The maximum spacing for warning lights on drums should be identical to the channelizing device spacing.



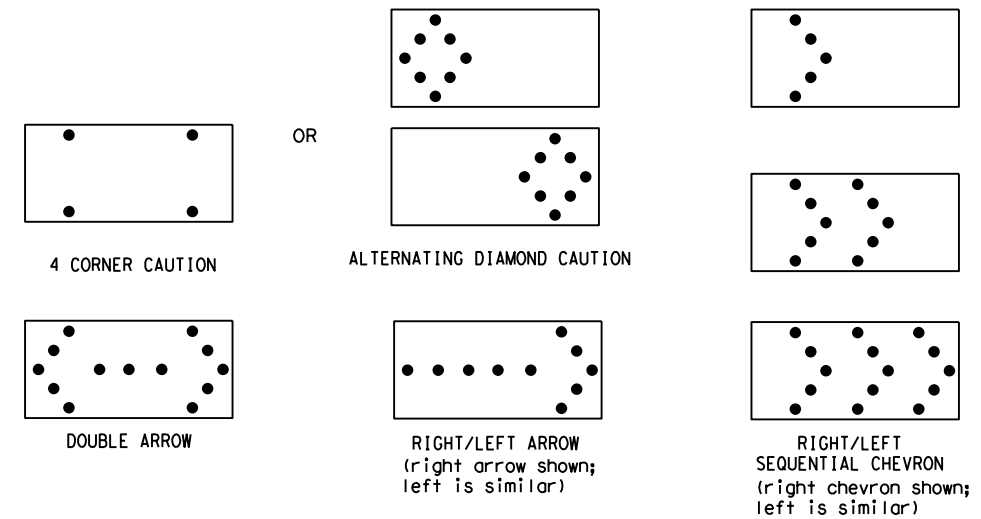
Warning reflector may be round or square. Must have a yellow reflective surface area of at least 30 square inches

WARNING REFLECTORS MOUNTED ON PLASTIC DRUMS AS A SUBSTITUTE FOR TYPE C (STEADY BURN) WARNING LIGHTS

- A warning reflector or approved substitute may be mounted on a plastic drum as a substitute for a Type C, steady burn warning light at the discretion of the Contractor unless otherwise noted in the plans.
- The warning reflector shall be yellow in color and shall be manufactured using a sign substrate approved for use with plastic drums listed on the CWZTCD.
- The warning reflector shall have a minimum retroreflective surface area (one-side) of 30 square inches.
- Round reflectors shall be fully reflectorized, including the area where attached to the drum.
- Square substrates must have a minimum of 30 square inches of reflectorized sheeting. They do not have to be reflectorized where it attaches to the drum.
- The side of the warning reflector facing approaching traffic shall have sheeting meeting the color and retroreflectivity requirements for DMS 8300-Type B or Type C.
- When used near two-way traffic, both sides of the warning reflector shall be reflectorized.
- The warning reflector should be mounted on the side of the handle nearest approaching traffic.
- The maximum spacing for warning reflectors should be identical to the channelizing device spacing requirements.

Arrow Boards may be located behind channelizing devices in place for a shoulder taper or merging taper, otherwise they shall be delineated with four (4) channelizing devices placed perpendicular to traffic on the upstream side of traffic.

- The Flashing Arrow Board should be used for all lane closures on multi-lane roadways, or slow moving maintenance or construction activities on the travel lanes.
- Flashing Arrow Boards should not be used on two-lane, two-way roadways, detours, diversions or work on shoulders unless the "CAUTION" display (see detail below) is used.
- The Engineer/Inspector shall choose all appropriate signs, barricades and/or other traffic control devices that should be used in conjunction with the Flashing Arrow Board.
- The Flashing Arrow Board should be able to display the following symbols:



- The "CAUTION" display consists of four corner lamps flashing simultaneously, or the Alternating Diamond Caution mode as shown.
- The straight line caution display is NOT ALLOWED.
- The Flashing Arrow Board shall be capable of minimum 50 percent dimming from rated lamp voltage. The flashing rate of the lamps shall not be less than 25 nor more than 40 flashes per minute.
- Minimum lamp "on time" shall be approximately 50 percent for the flashing arrow and equal intervals of 25 percent for each sequential phase of the flashing chevron.
- The sequential arrow display is NOT ALLOWED.
- The flashing arrow display is the TxDOT standard; however, the sequential chevron display may be used during daylight operations.
- The Flashing Arrow Board shall be mounted on a vehicle, trailer or other suitable support.
- A Flashing Arrow Board SHALL NOT BE USED to laterally shift traffic.
- A full matrix PCMS may be used to simulate a Flashing Arrow Board provided it meets visibility, flash rate and dimming requirements on this sheet for the same size arrow.
- Minimum mounting height of trailer mounted Arrow Boards should be 7 feet from roadway to bottom of panel.

REQUIREMENTS			
TYPE	MINIMUM SIZE	MINIMUM NUMBER OF PANEL LAMPS	MINIMUM VISIBILITY DISTANCE
B	30 x 60	13	3/4 mile
C	48 x 96	15	1 mile

ATTENTION
 Flashing Arrow Boards shall be equipped with automatic dimming devices.

WHEN NOT IN USE, REMOVE THE ARROW BOARD FROM THE RIGHT-OF-WAY OR PLACE THE ARROW BOARD BEHIND CONCRETE TRAFFIC BARRIER OR GUARDRAIL.

FLASHING ARROW BOARDS

SHEET 7 OF 12

TRUCK-MOUNTED ATTENUATORS

- Truck-mounted attenuators (TMA) used on TxDOT facilities must meet the requirements outlined in the Manual for Assessing Safety Hardware (MASH).
- Refer to the CWZTCD for the requirements of Level 2 or Level 3 TMAs.
- Refer to the CWZTCD for a list of approved TMAs.
- TMAs are required on freeways unless otherwise noted in the plans.
- A TMA should be used anytime that it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the work performance.
- The only reason a TMA should not be required is when a work area is spread down the roadway and the work crew is an extended distance from the TMA.



BARRICADE AND CONSTRUCTION ARROW PANEL, REFLECTORS, WARNING LIGHTS & ATTENUATOR

BC (7) -21

FILE:	bc-21.dgn	DN:	TxDOT	CR:	TxDOT	OW:	TxDOT	CK:	TxDOT
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GENERAL NOTES

- For long term stationary work zones on freeways, drums shall be used as the primary channelizing device.
- For intermediate term stationary work zones on freeways, drums should be used as the primary channelizing device but may be replaced in tangent sections by vertical panels, or 42" two-piece cones. In tangent sections, one-piece cones may be used with the approval of the Engineer but only if personnel are present on the project at all times to maintain the cones in proper position and location.
- For short term stationary work zones on freeways, drums are the preferred channelizing device but may be replaced in tapers, transitions and tangent sections by vertical panels, two-piece cones or one-piece cones as approved by the Engineer.
- Drums and all related items shall comply with the requirements of the current version of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Drums, bases, and related materials shall exhibit good workmanship and shall be free from objectionable marks or defects that would adversely affect their appearance or serviceability.
- The Contractor shall have a maximum of 24 hours to replace any plastic drums identified for replacement by the Engineer/Inspector. The replacement device must be an approved device.

GENERAL DESIGN REQUIREMENTS

Pre-qualified plastic drums shall meet the following requirements:

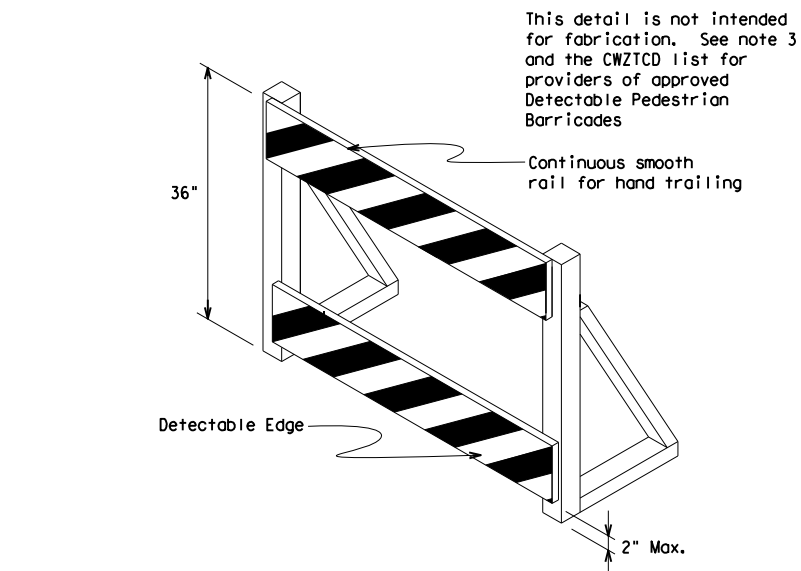
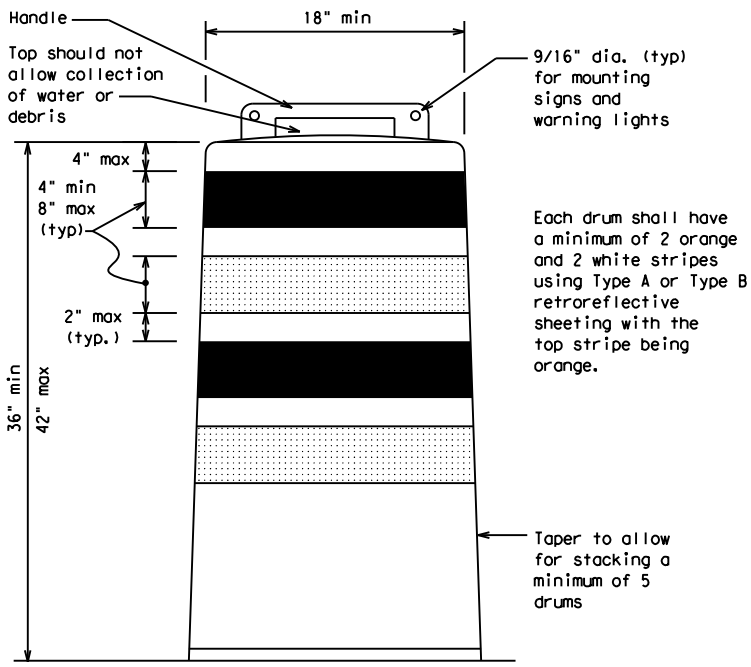
- Plastic drums shall be a two-piece design; the "body" of the drum shall be the top portion and the "base" shall be the bottom.
- The body and base shall lock together in such a manner that the body separates from the base when impacted by a vehicle traveling at a speed of 20 MPH or greater but prevents accidental separation due to normal handling and/or air turbulence created by passing vehicles.
- Plastic drums shall be constructed of lightweight flexible, and deformable materials. The Contractor shall NOT use metal drums or single piece plastic drums as channelization devices or sign supports.
- Drums shall present a profile that is a minimum of 18 inches in width at the 36 inch height when viewed from any direction. The height of drum unit (body installed on base) shall be a minimum of 36 inches and a maximum of 42 inches.
- The top of the drum shall have a built-in handle for easy pickup and shall be designed to drain water and not collect debris. The handle shall have a minimum of two widely spaced 9/16 inch diameter holes to allow attachment of a warning light, warning reflector unit or approved compliant sign.
- The exterior of the drum body shall have a minimum of four alternating orange and white retroreflective circumferential stripes not less than 4 inches nor greater than 8 inches in width. Any non-reflectORIZED space between any two adjacent stripes shall not exceed 2 inches in width.
- Bases shall have a maximum width of 36 inches, a maximum height of 4 inches, and a minimum of two footholds of sufficient size to allow base to be held down while separating the drum body from the base.
- Plastic drums shall be constructed of ultra-violet stabilized, orange, high-density polyethylene (HDPE) or other approved material.
- Drum body shall have a maximum unballasted weight of 11 lbs.
- Drum and base shall be marked with manufacturer's name and model number.

RETROREFLECTIVE SHEETING

- The stripes used on drums shall be constructed of sheeting meeting the color and retroreflectivity requirements of Departmental Materials Specification DMS-8300, "Sign Face Materials." Type A or Type B reflective sheeting shall be supplied unless otherwise specified in the plans.
- The sheeting shall be suitable for use on and shall adhere to the drum surface such that, upon vehicular impact, the sheeting shall remain adhered in-place and exhibit no delaminating, cracking, or loss of retroreflectivity other than that loss due to abrasion of the sheeting surface.

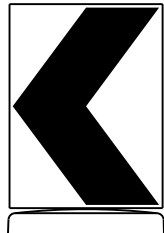
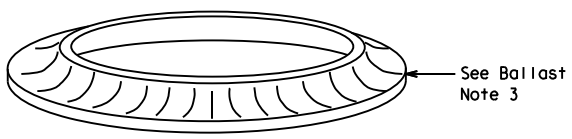
BALLAST

- Unballasted bases shall be large enough to hold up to 50 lbs. of sand. This base, when filled with the ballast material, should weigh between 35 lbs (minimum) and 50 lbs (maximum). The ballast may be sand in one to three sandbags separate from the base, sand in a sand-filled plastic base, or other ballasting devices as approved by the Engineer. Stacking of sandbags will be allowed, however height of sandbags above pavement surface may not exceed 12 inches.
- Bases with built-in ballast shall weigh between 40 lbs. and 50 lbs. Built-in ballast can be constructed of an integral crumb rubber base or a solid rubber base.
- Recycled truck tire sidewalls may be used for ballast on drums approved for this type of ballast on the CWZTCD list.
- The ballast shall not be heavy objects, water, or any material that would become hazardous to motorists, pedestrians, or workers when the drum is struck by a vehicle.
- When used in regions susceptible to freezing, drums shall have drainage holes in the bottoms so that water will not collect and freeze becoming a hazard when struck by a vehicle.
- Ballast shall not be placed on top of drums.
- Adhesives may be used to secure base of drums to pavement.

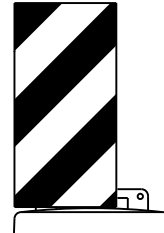


DETECTABLE PEDESTRIAN BARRICADES

- When existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility. Refer to WZ(BTS-2) for Pedestrian Control requirements for Sidewalk Diversions, Sidewalk Detours and Crosswalk Closures.
- Where pedestrians with visual disabilities normally use the closed sidewalk, a Detectable Pedestrian Barricade shall be placed across the full width of the closed sidewalk instead of a Type 3 Barricade.
- Detectable pedestrian barricades similar to the one pictured above, longitudinal channelizing devices, some concrete barriers, and wood or chain link fencing with a continuous detectable edging can satisfactorily delineate a pedestrian path.
- Tape, rope, or plastic chain strung between devices are not detectable, do not comply with the design standards in the "Americans with Disabilities Act Accessibility Guidelines (ADAAG)" and should not be used as a control for pedestrian movements.
- Warning lights shall not be attached to detectable pedestrian barricades.
- Detectable pedestrian barricades should use 8" nominal barricade rails as shown on BC(10) provided that the top rail provides a smooth continuous rail suitable for hand trailing with no splinters, burrs, or sharp edges.



18" x 24" Sign
(Maximum Sign Dimension)
Chevron CW1-8, Opposing Traffic Lane
Divider, Driveway sign D70a, Keep Right
R4 series or other signs as approved
by Engineer



12" x 24"
Vertical Panel
mount with diagonals
sloping down towards
travel way

Plywood, Aluminum or Metal sign
substrates shall NOT be used on
plastic drums

SIGNS, CHEVRONS, AND VERTICAL PANELS MOUNTED ON PLASTIC DRUMS

- Signs used on plastic drums shall be manufactured using substrates listed on the CWZTCD.
- Chevrons and other work zone signs with an orange background shall be manufactured with Type B_{FL} or Type C_{FL} Orange sheeting meeting the color and retroreflectivity requirements of DMS-8300, "Sign Face Material," unless otherwise specified in the plans.
- Vertical Panels shall be manufactured with orange and white sheeting meeting the requirements of DMS-8300 Type A or Type B. Diagonal stripes on Vertical Panels shall slope down toward the intended traveled lane.
- Other sign messages (text or symbolic) may be used as approved by the Engineer. Sign dimensions shall not exceed 18 inches in width or 24 inches in height, except for the R9 series signs discussed in note 8 below.
- Signs shall be installed using a 1/2 inch bolt (nominal) and nut, two washers, and one locking washer for each connection.
- Mounting bolts and nuts shall be fully engaged and adequately torqued. Bolts should not extend more than 1/2 inch beyond nuts.
- Chevrons may be placed on drums on the outside of curves, on merging tapers or on shifting tapers. When used in these locations, they may be placed on every drum or spaced not more than on every third drum. A minimum of three (3) should be used at each location called for in the plans.
- R9-9, R9-10, R9-11 and R9-11a Sidewalk Closed signs which are 24 inches wide may be mounted on plastic drums, with approval of the Engineer.

SHEET 8 OF 12

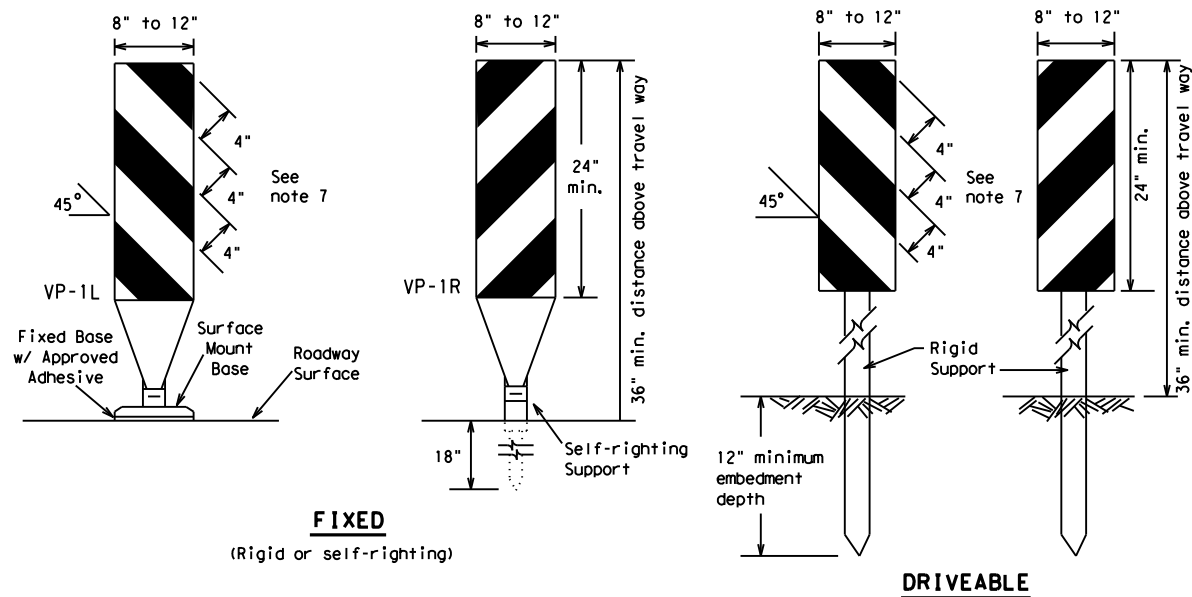


BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC(8) - 21

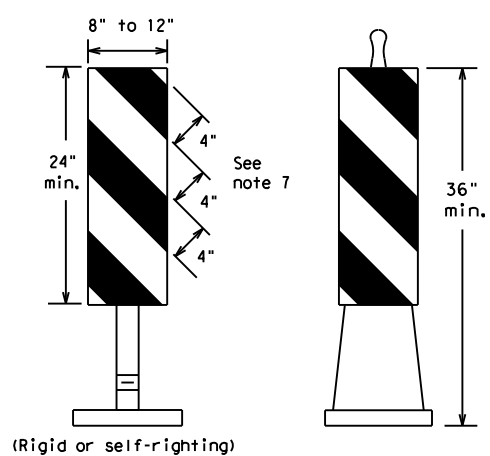
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© TxDOT	November 2002	CONT	SECT	JOB	HIGHWAY				
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7-13									

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FIXED
(Rigid or self-righting)

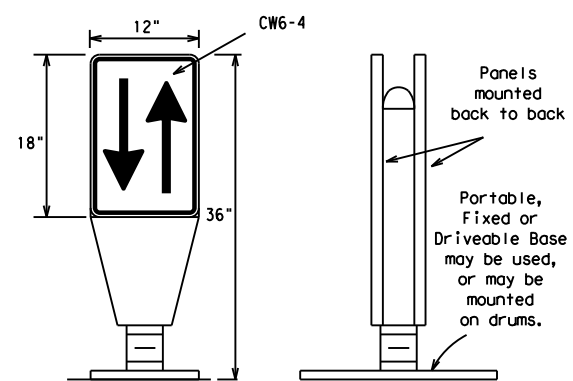
DRIVEABLE



PORTABLE

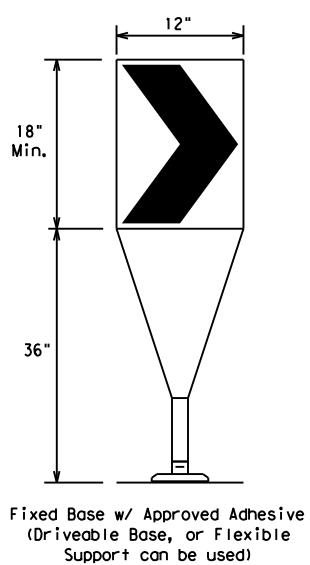
VERTICAL PANELS (VPs)

- Vertical Panels (VP's) are normally used to channelize traffic or divide opposing lanes of traffic.
- VP's may be used in daytime or nighttime situations. They may be used at the edge of shoulder drop-offs and other areas such as lane transitions where positive daytime and nighttime delineation is required. The Engineer/Inspector shall refer to the Roadway Design Manual for additional requirements on the use VP's for drop-offs.
- VP's should be mounted back to back if used at the edge of cuts adjacent to two-way two lane roadways. Stripes are to be reflective orange and reflective white and should always slope downward toward the travel lane.
- VP's used on expressways and freeways or other high speed roadways, may have more than 270 square inches of retroreflective area facing traffic.
- Self-righting supports are available with portable base. See "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Sheeting for the VP's shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300, unless noted otherwise.
- Where the height of reflective material on the vertical panel is 36 inches or greater, a panel stripe of 6 inches shall be used.



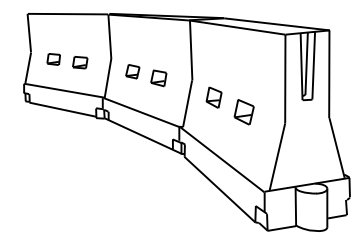
OPPOSING TRAFFIC LANE DIVIDERS (OTLD)

- Opposing Traffic Lane Dividers (OTLD) are delineation devices designed to convert a normal one-way roadway section to two-way operation. OTLD's are used on temporary centerlines. The upward and downward arrows on the sign's face indicate the direction of traffic on either side of the divider. The base is secured to the pavement with an adhesive or rubber weight to minimize movement caused by a vehicle impact or wind gust.
- The OTLD may be used in combination with 42" cones or VPs.
- Spacing between the OTLD shall not exceed 500 feet. 42" cones or VPs placed between the OTLD's should not exceed 100 foot spacing.
- The OTLD shall be orange with a black non-reflective legend. Sheeting for the OTLD shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.



- The chevron shall be a vertical rectangle with a minimum size of 12 by 18 inches.
- Chevrons are intended to give notice of a sharp change of alignment with the direction of travel and provide additional emphasis and guidance for vehicle operators with regard to changes in horizontal alignment of the roadway.
- Chevrons, when used, shall be erected on the outside of a sharp curve or turn, or on the far side of an intersection. They shall be in line with and at right angles to approaching traffic. Spacing should be such that the motorist always has three in view, until the change in alignment eliminates its need.
- To be effective, the chevron should be visible for at least 500 feet.
- Chevrons shall be orange with a black nonreflective legend. Sheeting for the chevron shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.
- For Long Term Stationary use on tapers or transitions on freeways and divided highways, self-righting chevrons may be used to supplement plastic drums but not to replace plastic drums.

CHEVRONS



LONGITUDINAL CHANNELIZING DEVICES (LCD)

- LCDs are crashworthy, lightweight, deformable devices that are highly visible, have good target value and can be connected together. They are not designed to contain or redirect a vehicle on impact.
- LCDs may be used instead of a line of cones or drums.
- LCDs shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- LCDs should not be used to provide positive protection for obstacles, pedestrians or workers.
- LCDs shall be supplemented with retroreflective delineation as required for temporary barriers on BC(7) when placed roughly parallel to the travel lanes.
- LCDs used as barricades placed perpendicular to traffic should have at least one row of reflective sheeting meeting the requirements for barricade rails as shown on BC(10). Place reflective sheeting near the top of the LCD along the full length of the device.

WATER BALLASTED SYSTEMS USED AS BARRIERS

- Water ballasted systems used as barriers shall not be used solely to channelize road users, but also to protect the work space per the appropriate Manual for Assessing Safety Hardware (MASH) crashworthiness requirements based on roadway speed and barrier application.
- Water ballasted systems used to channelize vehicular traffic shall be supplemented with retroreflective delineation or channelizing devices to improve daytime/nighttime visibility. They may also be supplemented with pavement markings.
- Water ballasted systems used as barriers shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- Water ballasted systems used as barriers should not be used for a merging taper except in low speed (less than 45 MPH) urban areas. When used on a taper in a low speed urban area, the taper shall be delineated and the taper length should be designed to optimize road user operations considering the available geometric conditions.
- When water ballasted systems used as barriers have blunt ends exposed to traffic, they should be attenuated as per manufacturer recommendations or flared to a point outside the clear zone.

If used to channelize pedestrians, longitudinal channelizing devices or water ballasted systems must have a continuous detectable bottom for users of long canes and the top of the unit shall not be less than 32 inches in height.

HOLLOW OR WATER BALLASTED SYSTEMS USED AS LONGITUDINAL CHANNELIZING DEVICES OR BARRIERS

GENERAL NOTES

- Work Zone channelizing devices illustrated on this sheet may be installed in close proximity to traffic and are suitable for use on high or low speed roadways. The Engineer/Inspector shall ensure that spacing and placement is uniform and in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Channelizing devices shown on this sheet may have a driveable, fixed or portable base. The requirement for self-righting channelizing devices must be specified in the General Notes or other plan sheets.
- Channelizing devices on self-righting supports should be used in work zone areas where channelizing devices are frequently impacted by errant vehicles or vehicle related wind gusts making alignment of the channelizing devices difficult to maintain. Locations of these devices shall be detailed elsewhere in the plans. These devices shall conform to the TMUTCD and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- The Contractor shall maintain devices in a clean condition and replace damaged, nonreflective, faded, or broken devices and bases as required by the Engineer/Inspector. The Contractor shall be required to maintain proper device spacing and alignment.
- Portable bases shall be fabricated from virgin and/or recycled rubber. The portable bases shall weigh a minimum of 30 lbs.
- Pavement surfaces shall be prepared in a manner that ensures proper bonding between the adhesives, the fixed mount bases and the pavement surface. Adhesives shall be prepared and applied according to the manufacturer's recommendations.
- The installation and removal of channelizing devices shall not cause detrimental effects to the final pavement surfaces, including pavement surface discoloration or surface integrity. Driveable bases shall not be permitted on final pavement surfaces. The Engineer/Inspector shall approve all application and removal procedures of fixed bases.

Posted Speed	Formula	Minimum Desirable Taper Lengths * *			Suggested Maximum Spacing of Channelizing Devices	
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent
30	L = WS ² / 60	150'	165'	180'	30'	60'
35		205'	225'	245'	35'	70'
40		265'	295'	320'	40'	80'
45	L = WS	450'	495'	540'	45'	90'
50		500'	550'	600'	50'	100'
55		550'	605'	660'	55'	110'
60		600'	660'	720'	60'	120'
65		650'	715'	780'	65'	130'
70		700'	770'	840'	70'	140'
75		750'	825'	900'	75'	150'
80		800'	880'	960'	80'	160'

* * * Taper lengths have been rounded off.
L=Length of Taper (FT.) W=Width of Offset (FT.)
S=Posted Speed (MPH)

SUGGESTED MAXIMUM SPACING OF CHANNELIZING DEVICES AND MINIMUM DESIRABLE TAPER LENGTHS

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BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

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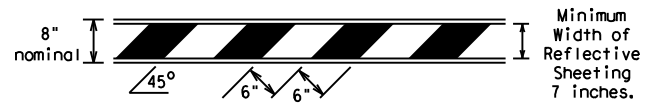
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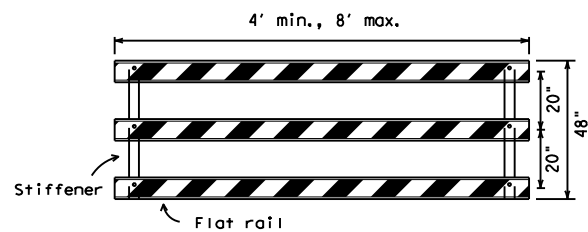
TYPE 3 BARRICADES

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTCD) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road, striping should slope downward in both directions toward the center of roadway.
4. Striping of rails, for the right side of the roadway, should slope downward to the left. For the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rails. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand bags shall not be stacked in a manner that covers any portion of a barricade rails reflective sheeting. Rock, concrete, iron, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed along or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheeting for barricades shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300 unless otherwise noted.

Barricades shall NOT be used as a sign support.



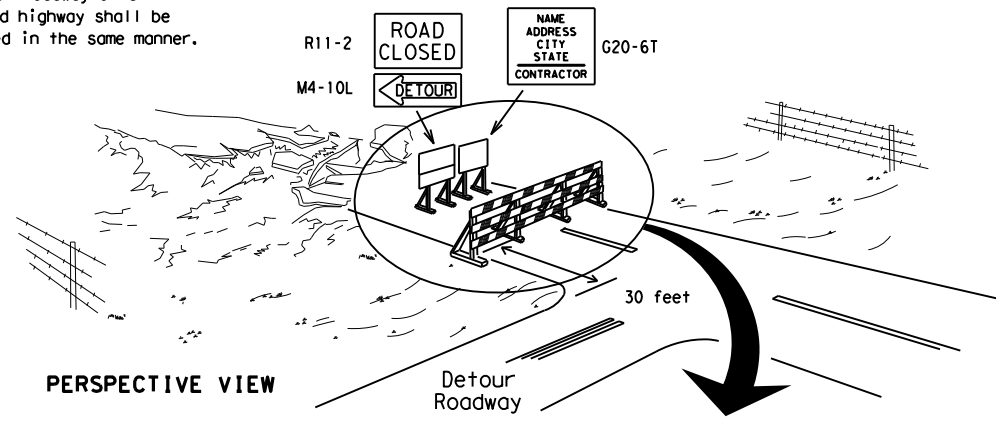
TYPICAL STRIPING DETAIL FOR BARRICADE RAIL



Stiffener may be inside or outside of support, but no more than 2 stiffeners shall be allowed on one barricade.

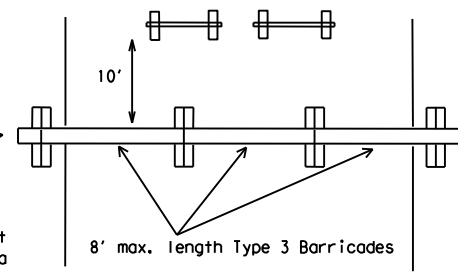
TYPICAL PANEL DETAIL FOR SKID OR POST TYPE BARRICADES

Each roadway of a divided highway shall be barricaded in the same manner.



PERSPECTIVE VIEW

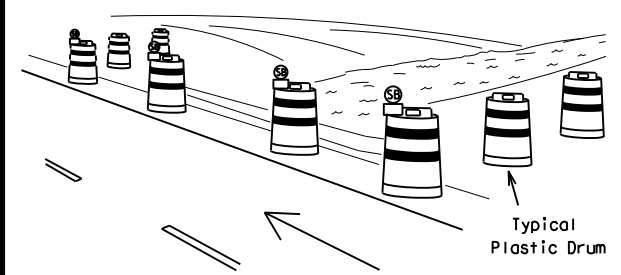
The three rails on Type 3 barricades shall be reflectorized orange and reflective white stripes on one side facing one-way traffic and both sides for two-way traffic. Barricade striping should slant downward in the direction of detour.



PLAN VIEW

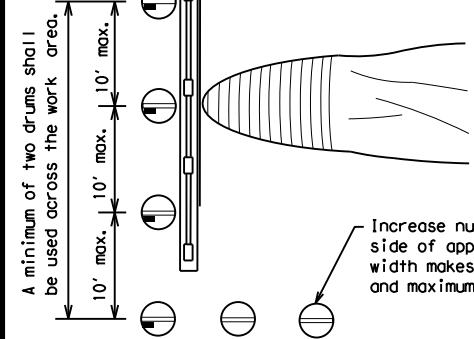
1. Signs should be mounted on independent supports at a 7 foot mounting height in center of roadway. The signs should be a minimum of 10 feet behind Type 3 Barricades.
2. Advance signing shall be as specified elsewhere in the plans.

TYPE 3 BARRICADE (POST AND SKID) TYPICAL APPLICATION



PERSPECTIVE VIEW

These drums are not required on one-way roadway



PLAN VIEW

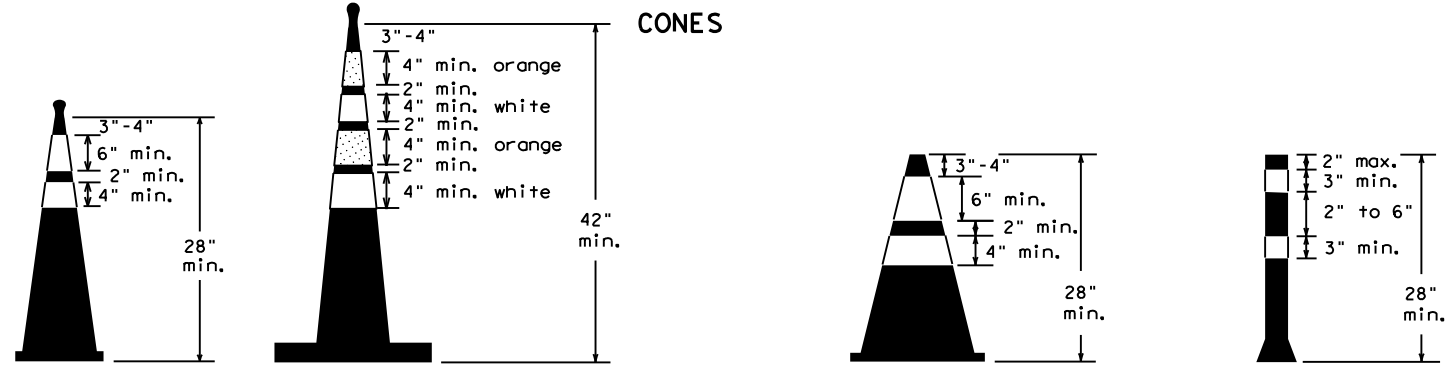
A minimum of two drums shall be used across the work area.

Increase number of plastic drums on the side of approaching traffic if the crown width makes it necessary. (minimum of 2 and maximum of 4 drums)

LEGEND	
	Plastic drum
	Plastic drum with steady burn light or yellow warning reflector
	Steady burn warning light or yellow warning reflector

1. Where positive redirection capability is provided, drums may be omitted.
2. Plastic construction fencing may be used with drums for safety as required in the plans.
3. Vertical Panels on flexible support may be substituted for drums when the shoulder width is less than 4 feet.
4. When the shoulder width is greater than 12 feet, steady-burn lights may be omitted if drums are used.
5. Drums must extend the length of the culvert widening.

CULVERT WIDENING OR OTHER ISOLATED WORK WITHIN THE PROJECT LIMITS



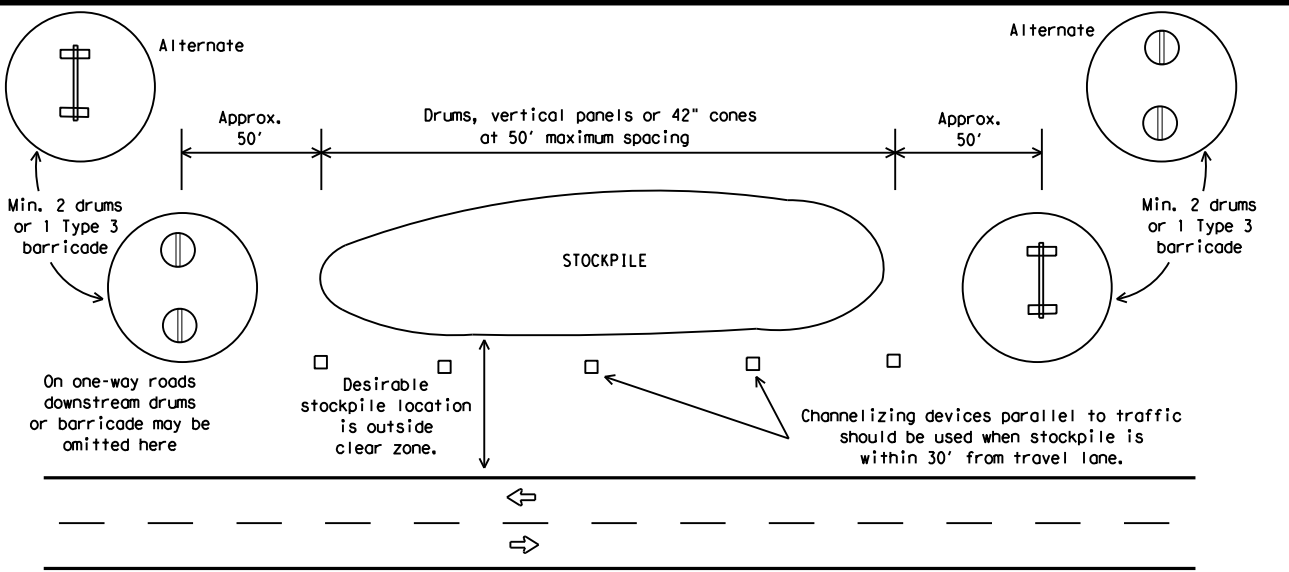
Two-Piece cones

One-Piece cones

Tubular Marker

28" Cones shall have a minimum weight of 9 1/2 lbs.
42" 2-piece cones shall have a minimum weight of 30 lbs. including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A or Type B.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined on BC(4). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone durations.
7. Cones or tubular markers used on each project should be of the same size and shape.



TRAFFIC CONTROL FOR MATERIAL STOCKPILES



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (10) - 21

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WORK ZONE PAVEMENT MARKINGS

GENERAL

- The Contractor shall be responsible for maintaining work zone and existing pavement markings, in accordance with the standard specifications and special provisions, on all roadways open to traffic within the CSJ limits unless otherwise stated in the plans.
- Color, patterns and dimensions shall be in conformance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Additional supplemental pavement marking details may be found in the plans or specifications.
- Pavement markings shall be installed in accordance with the TMUTCD and as shown on the plans.
- When short term markings are required on the plans, short term markings shall conform with the TMUTCD, the plans and details as shown on the Standard Plan Sheet WZ(STPM).
- When standard pavement markings are not in place and the roadway is opened to traffic, DO NOT PASS signs shall be erected to mark the beginning of the sections where passing is prohibited and PASS WITH CARE signs at the beginning of sections where passing is permitted.
- All work zone pavement markings shall be installed in accordance with Item 662, "Work Zone Pavement Markings."

RAISED PAVEMENT MARKERS

- Raised pavement markers are to be placed according to the patterns on BC(12).
- All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and Departmental Material Specification DMS-4200 or DMS-4300.

PREFABRICATED PAVEMENT MARKINGS

- Removable prefabricated pavement markings shall meet the requirements of DMS-8241.
- Non-removable prefabricated pavement markings (foil back) shall meet the requirements of DMS-8240.

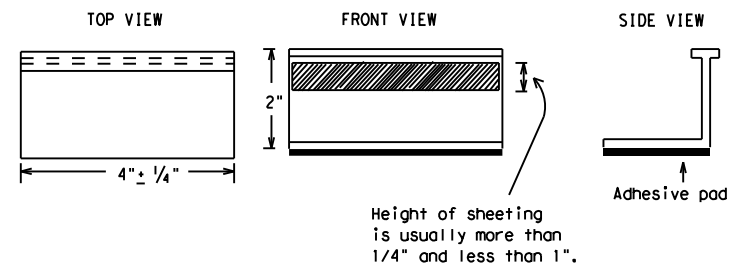
MAINTAINING WORK ZONE PAVEMENT MARKINGS

- The Contractor will be responsible for maintaining work zone pavement markings within the work limits.
- Work zone pavement markings shall be inspected in accordance with the frequency and reporting requirements of work zone traffic control device inspections as required by Form 599.
- The markings should provide a visible reference for a minimum distance of 300 feet during normal daylight hours and 160 feet when illuminated by automobile low-beam headlights at night, unless sight distance is restricted by roadway geometrics.
- Markings failing to meet this criteria within the first 30 days after placement shall be replaced at the expense of the Contractor as per Specification Item 662.

REMOVAL OF PAVEMENT MARKINGS

- Pavement markings that are no longer applicable, could create confusion or direct a motorist toward or into the closed portion of the roadway shall be removed or obliterated before the roadway is opened to traffic.
- The above shall not apply to detours in place for less than three days, where flaggers and/or sufficient channelizing devices are used in lieu of markings to outline the detour route.
- Pavement markings shall be removed to the fullest extent possible, so as not to leave a discernable marking. This shall be by any method approved by TxDOT Specification Item 677 for "Eliminating Existing Pavement Markings and Markers".
- The removal of pavement markings may require resurfacing or seal coating portions of the roadway as described in Item 677.
- Subject to the approval of the Engineer, any method that proves to be successful on a particular type pavement may be used.
- Blast cleaning may be used but will not be required unless specifically shown in the plans.
- Over-painting of the markings SHALL NOT BE permitted.
- Removal of raised pavement markers shall be as directed by the Engineer.
- Removal of existing pavement markings and markers will be paid for directly in accordance with Item 677, "ELIMINATING EXISTING PAVEMENT MARKINGS AND MARKERS," unless otherwise stated in the plans.
- Black-out marking tape may be used to cover conflicting existing markings for periods less than two weeks when approved by the Engineer.

Temporary Flexible-Reflective Roadway Marker Tabs



**STAPLES OR NAILS SHALL NOT BE USED TO SECURE
TEMPORARY FLEXIBLE-REFLECTIVE ROADWAY MARKER
TABS TO THE PAVEMENT SURFACE**

- Temporary flexible-reflective roadway marker tabs used as guidemarks shall meet the requirements of DMS-8242.
- Tabs detailed on this sheet are to be inspected and accepted by the Engineer or designated representative. Sampling and testing is not normally required, however at the option of the Engineer, either "A" or "B" below may be imposed to assure quality before placement on the roadway.
 - Select five (5) or more tabs at random from each lot or shipment and submit to the Construction Division, Materials and Pavement Section to determine specification compliance.
 - Select five (5) tabs and perform the following test. Affix five (5) tabs at 24 inch intervals on an asphaltic pavement in a straight line. Using a medium size passenger vehicle or pickup, run over the markers with the front and rear tires at a speed of 35 to 40 miles per hour, four (4) times in each direction. No more than one (1) out of the five (5) reflective surfaces shall be lost or displaced as a result of this test.
- Small design variances may be noted between tab manufacturers.
- See Standard Sheet WZ(STPM) for tab placement on new pavements. See Standard Sheet TCP(7-1) for tab placement on seal coat work.

RAISED PAVEMENT MARKERS USED AS GUIDEMARKS

- Raised pavement markers used as guidemarks shall be from the approved product list, and meet the requirements of DMS-4200.
- All temporary construction raised pavement markers provided on a project shall be of the same manufacturer.
- Adhesive for guidemarks shall be bituminous material hot applied or butyl rubber pad for all surfaces, or thermoplastic for concrete surfaces.

Guidemarks shall be designated as:
 YELLOW - (two amber reflective surfaces with yellow body).
 WHITE - (one silver reflective surface with white body).

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
TRAFFIC BUTTONS	DMS-4300
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240
TEMPORARY REMOVABLE, PREFABRICATED PAVEMENT MARKINGS	DMS-8241
TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS	DMS-8242

A list of prequalified reflective raised pavement markers, non-reflective traffic buttons, roadway marker tabs and other pavement markings can be found at the Material Producer List web address shown on BC(1).

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DATE:
FILE:

SHEET 11 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKINGS

BC(11)-21

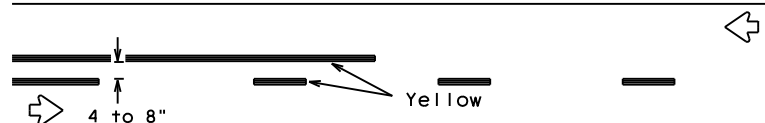
FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	DW: TxDOT	CR: TxDOT
© TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
2-98 9-07 5-21	DIST	COUNTY	SHEET NO.	
1-02 7-13	FTW	TARRANT	16	
11-02 8-14				

105

PAVEMENT MARKING PATTERNS

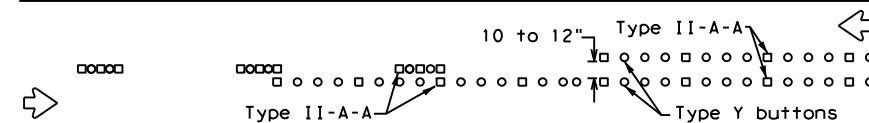


REFLECTORIZED PAVEMENT MARKINGS - PATTERN A

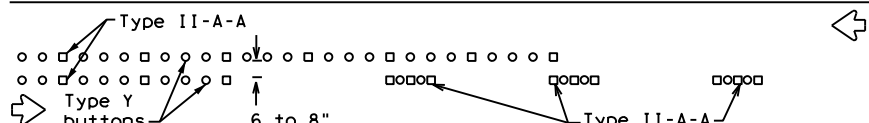


REFLECTORIZED PAVEMENT MARKINGS - PATTERN B

Pattern A is the TXDOT Standard, however Pattern B may be used if approved by the Engineer. Prefabricated markings may be substituted for reflectORIZED pavement markings.

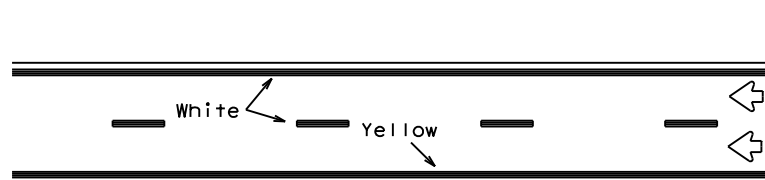


RAISED PAVEMENT MARKERS - PATTERN A



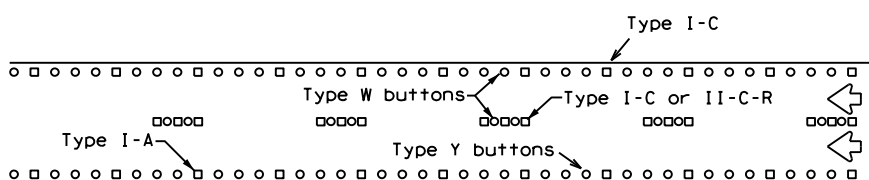
RAISED PAVEMENT MARKERS - PATTERN B

CENTER LINE & NO-PASSING ZONE BARRIER LINES FOR TWO-LANE, TWO-WAY HIGHWAYS



REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectORIZED pavement markings.



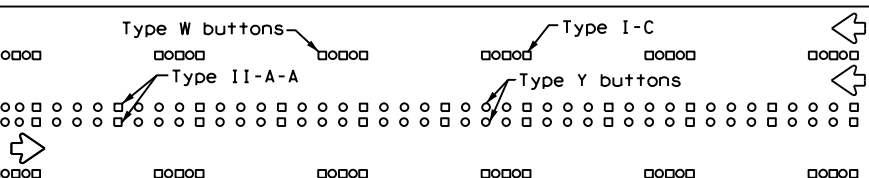
RAISED PAVEMENT MARKERS

EDGE & LANE LINES FOR DIVIDED HIGHWAY



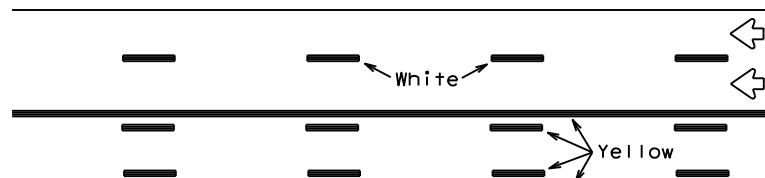
REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectORIZED pavement markings.



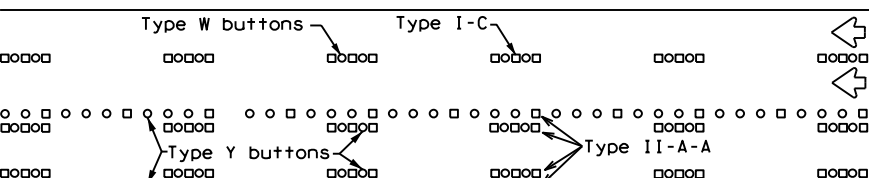
RAISED PAVEMENT MARKERS

LANE & CENTER LINES FOR MULTILANE UNDIVIDED HIGHWAYS



REFLECTORIZED PAVEMENT MARKINGS

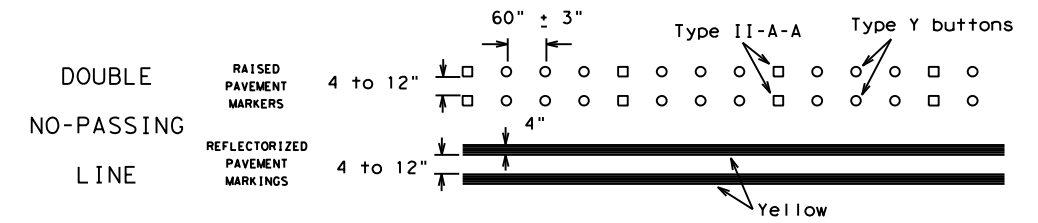
Prefabricated markings may be substituted for reflectORIZED pavement markings.



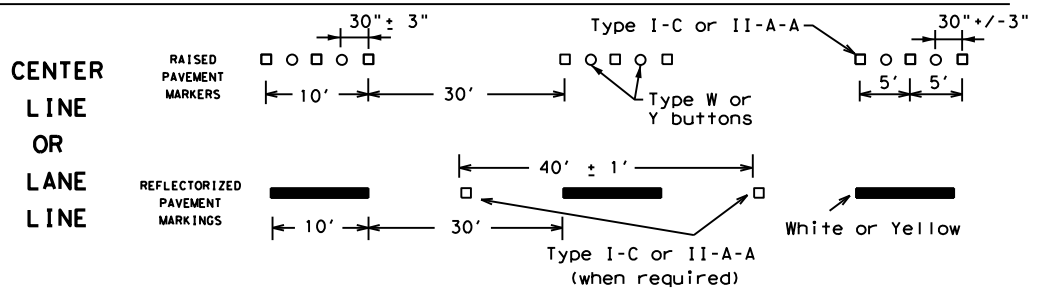
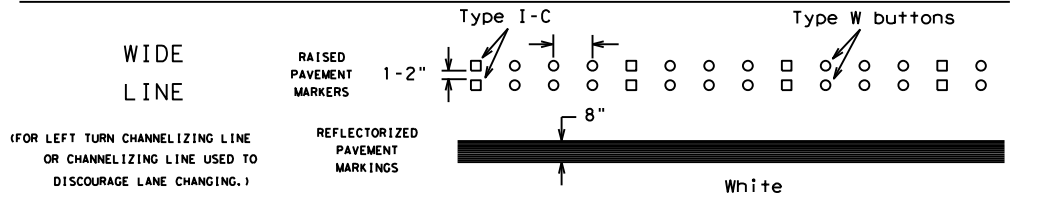
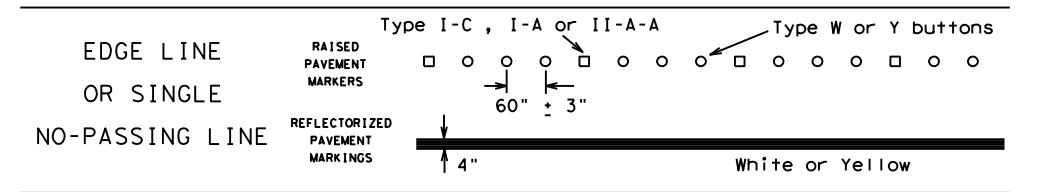
RAISED PAVEMENT MARKERS

TWO-WAY LEFT TURN LANE

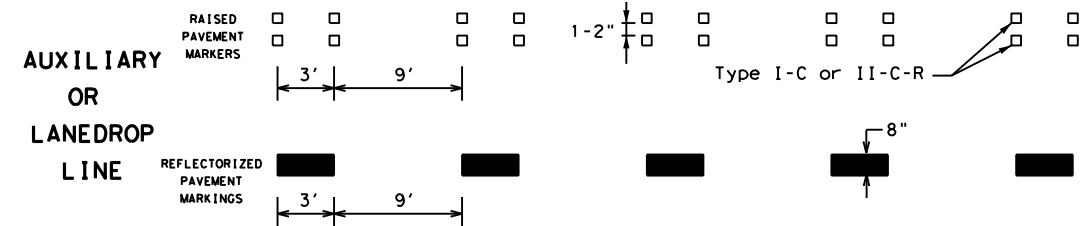
STANDARD WORK ZONE PAVEMENT MARKINGS DETAILS



SOLID LINES

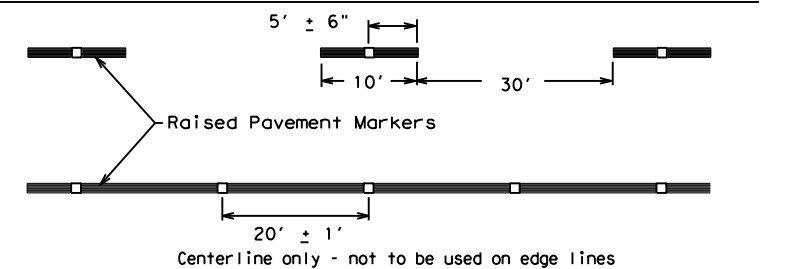


BROKEN LINES



REMOVABLE MARKINGS WITH RAISED PAVEMENT MARKERS

If raised pavement markers are used to supplement REMOVABLE markings, the markers shall be applied to the top of the tape at the approximate mid length of tape used for broken lines or at 20 foot spacing for solid lines. This allows an easier removal of raised pavement markers and tape.



SHEET 12 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKING PATTERNS

BC(12)-21

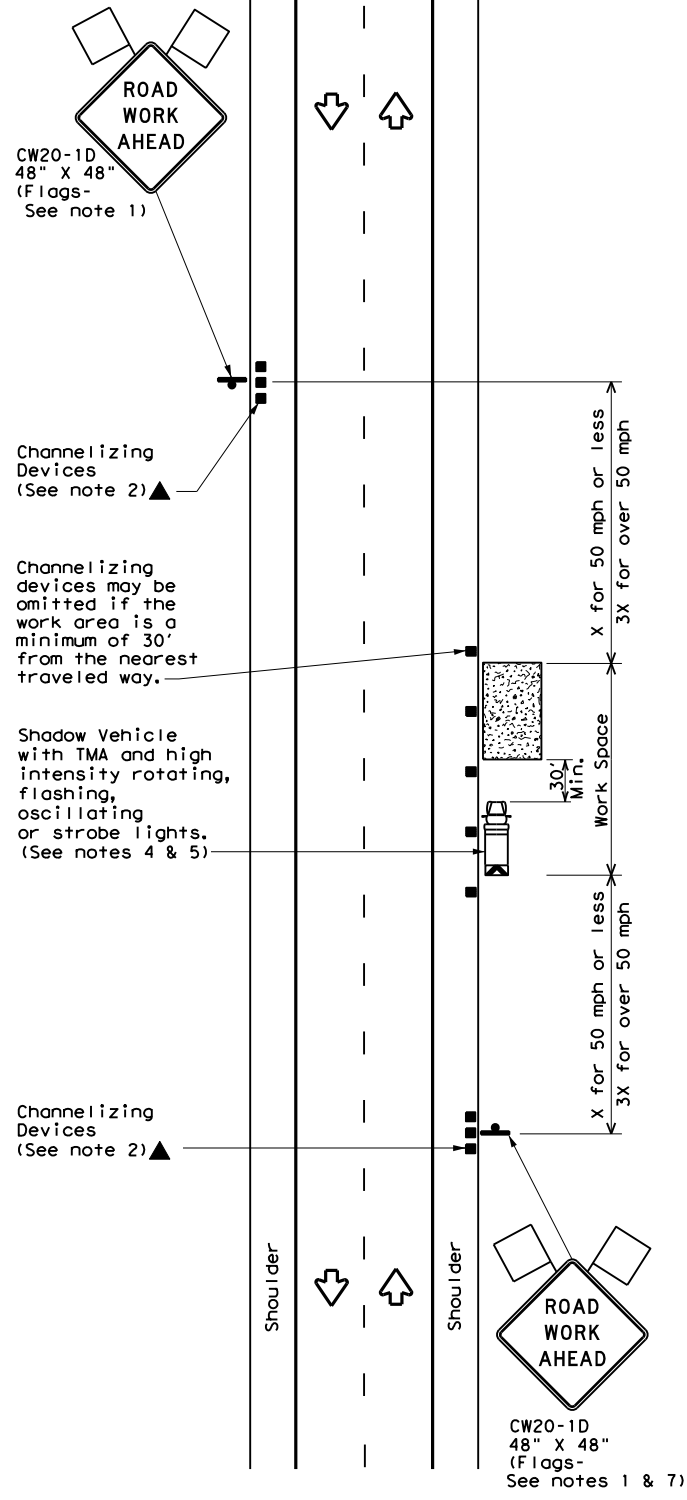
Raised pavement markers used as standard pavement markings shall be from the approved products list and meet the requirements of Item 672 "RAISED PAVEMENT MARKERS."

FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	OW: TxDOT	CK: TxDOT
©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
1-97 9-07 5-21				
2-98 7-13				
11-02 8-14	DIST	COUNTY	SHEET NO.	
	FTW	TARRANT	17	

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

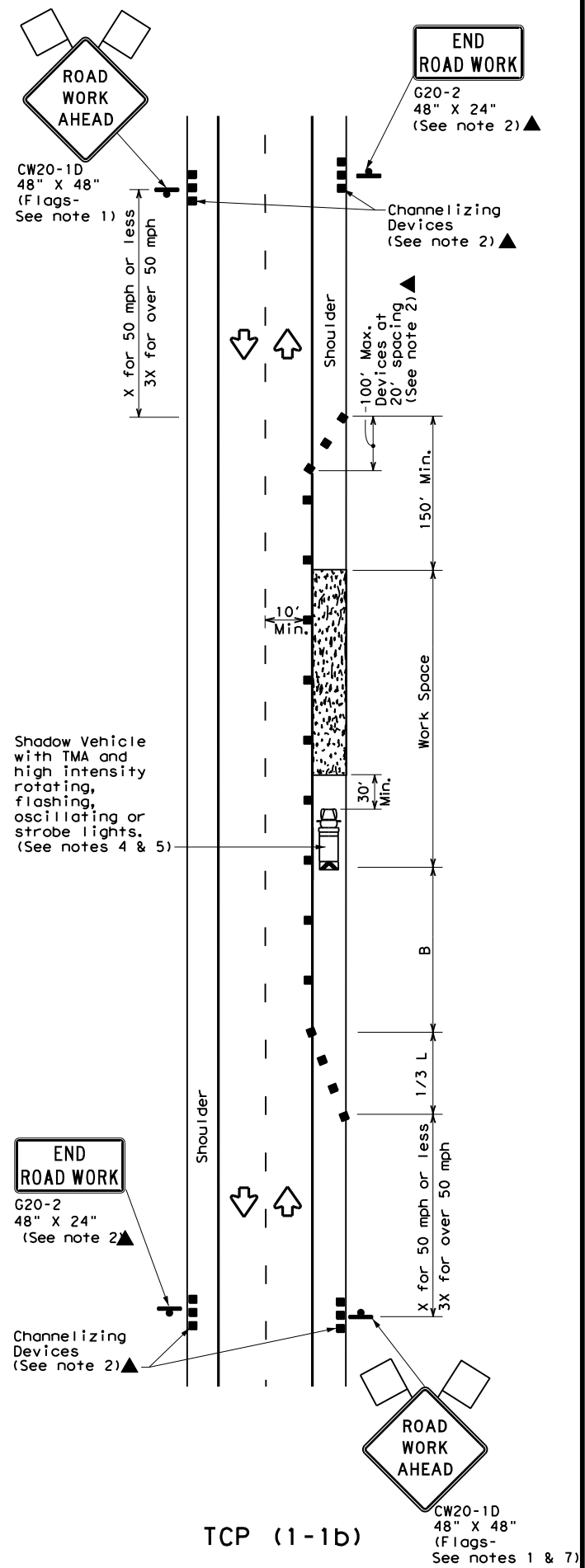
DATE: FILE:

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.



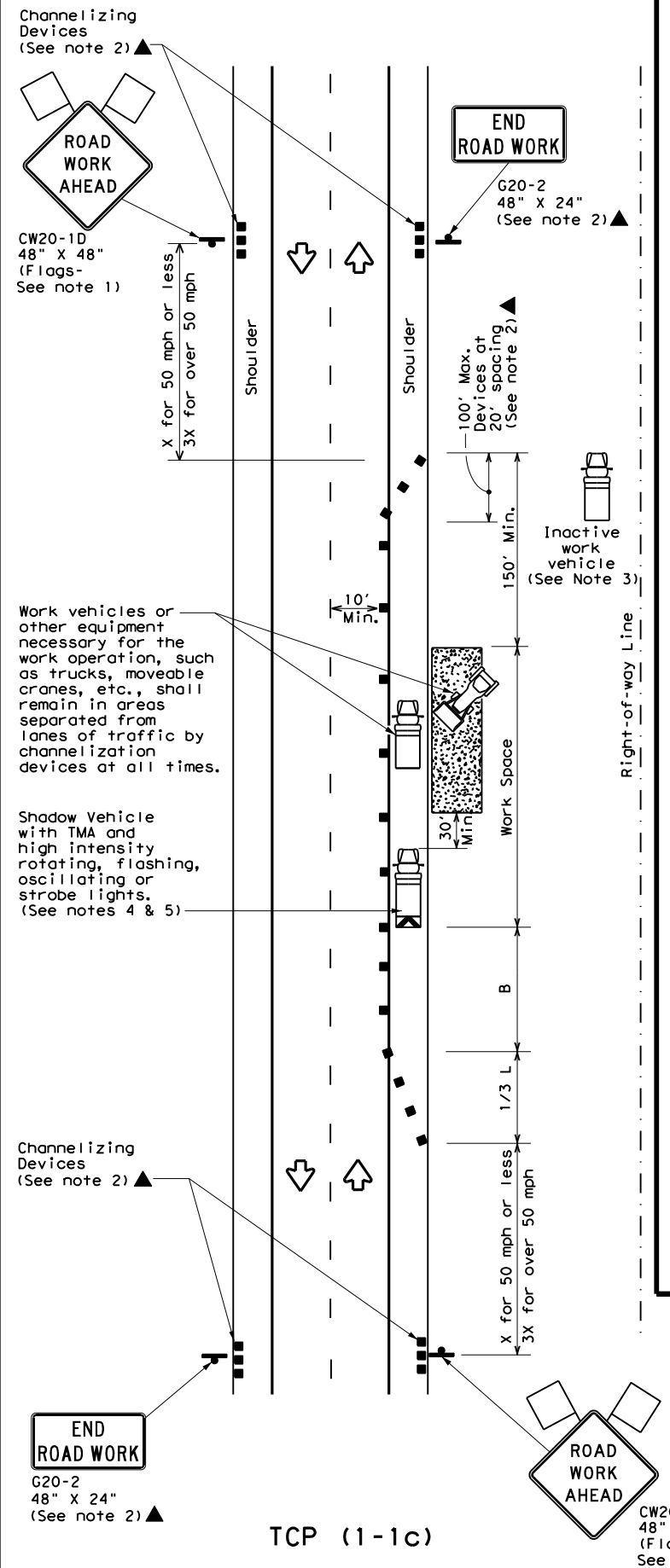
TCP (1-1a)

WORK SPACE NEAR SHOULDER
Conventional Roads



TCP (1-1b)

WORK SPACE ON SHOULDER
Conventional Roads



TCP (1-1c)

WORK VEHICLES ON SHOULDER
Conventional Roads

LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
** Taper lengths have been rounded off.
L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

- GENERAL NOTES**
- Flags attached to signs where shown are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
 - Inactive work vehicles or other equipment should be parked near the right-of-way line and not parked on the paved shoulder.
 - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
 - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.
 - See TCP(5-1) for shoulder work on divided highways, expressways and freeways.
 - CW21-5 "SHOULDER WORK" signs may be used in place of CW20-1D "ROAD WORK AHEAD" signs for shoulder work on conventional roadways.

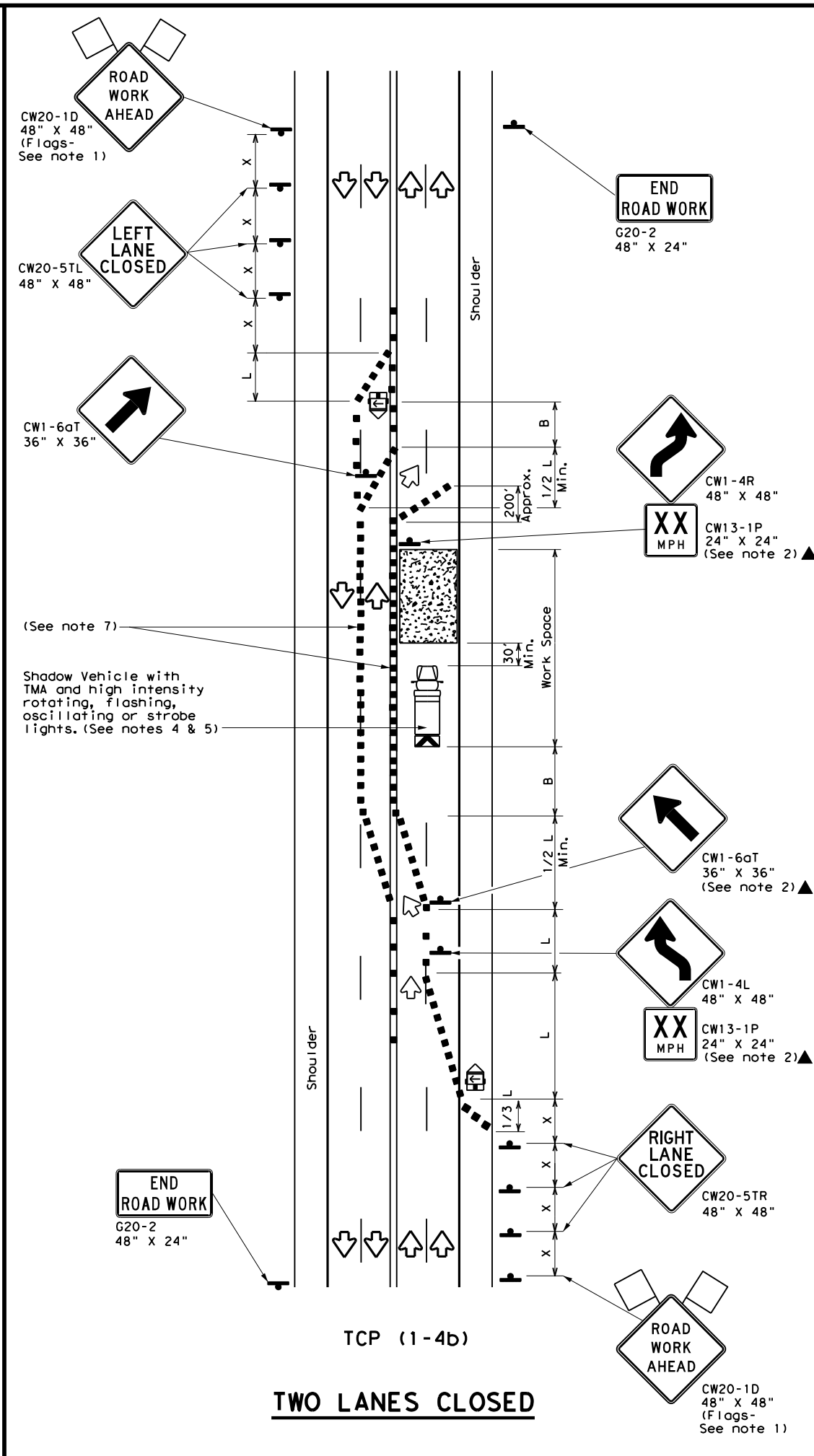
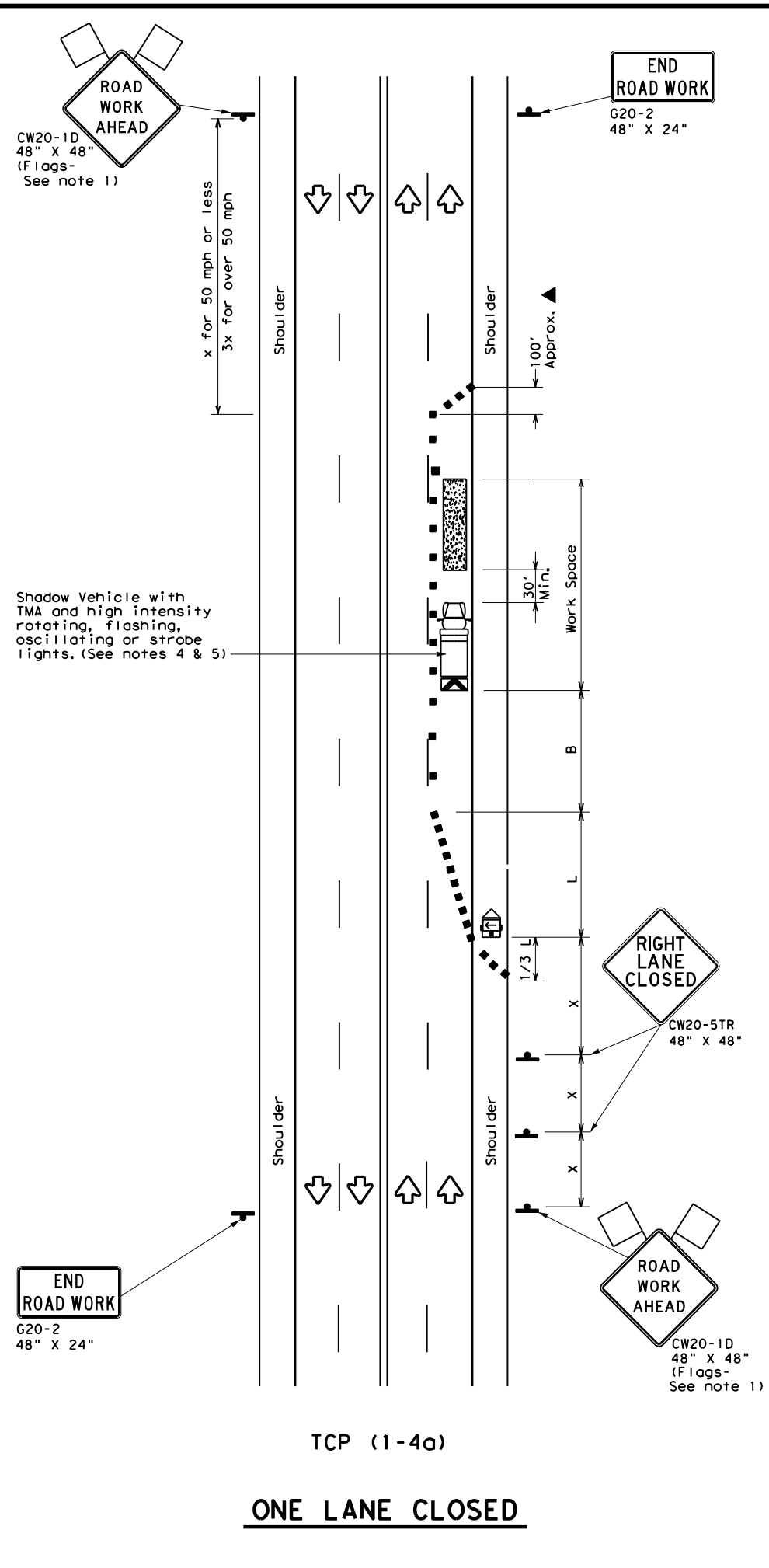
TRAFFIC CONTROL PLAN
CONVENTIONAL ROAD
SHOULDER WORK

TCP (1-1) - 18

FILE: tcp1-1-18.dgn	DN:	CK:	DW:	CK:
© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
2-94 4-98	DIST	COUNTY	SHEET NO.	
8-95 2-12	FTW	TARRANT	18	
1-97 2-18				

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DATE: FILE:



LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	$L = WS$	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

- GENERAL NOTES**
- Flags attached to signs where shown are REQUIRED.
 - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
 - The CW20-1D "ROAD WORK AHEAD" sign may be repeated if the visibility of the work zone is less than 1500 feet.
 - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
 - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.

TCP (1-4a)

6. If this TCP is used for a left lane closure, CW20-5TL "LEFT LANE CLOSED" signs shall be used and channelizing devices shall be placed on the centerline where needed to protect the work space from opposing traffic with the arrow panel placed in the closed lane near the end of the merging taper.

TCP (1-4b)

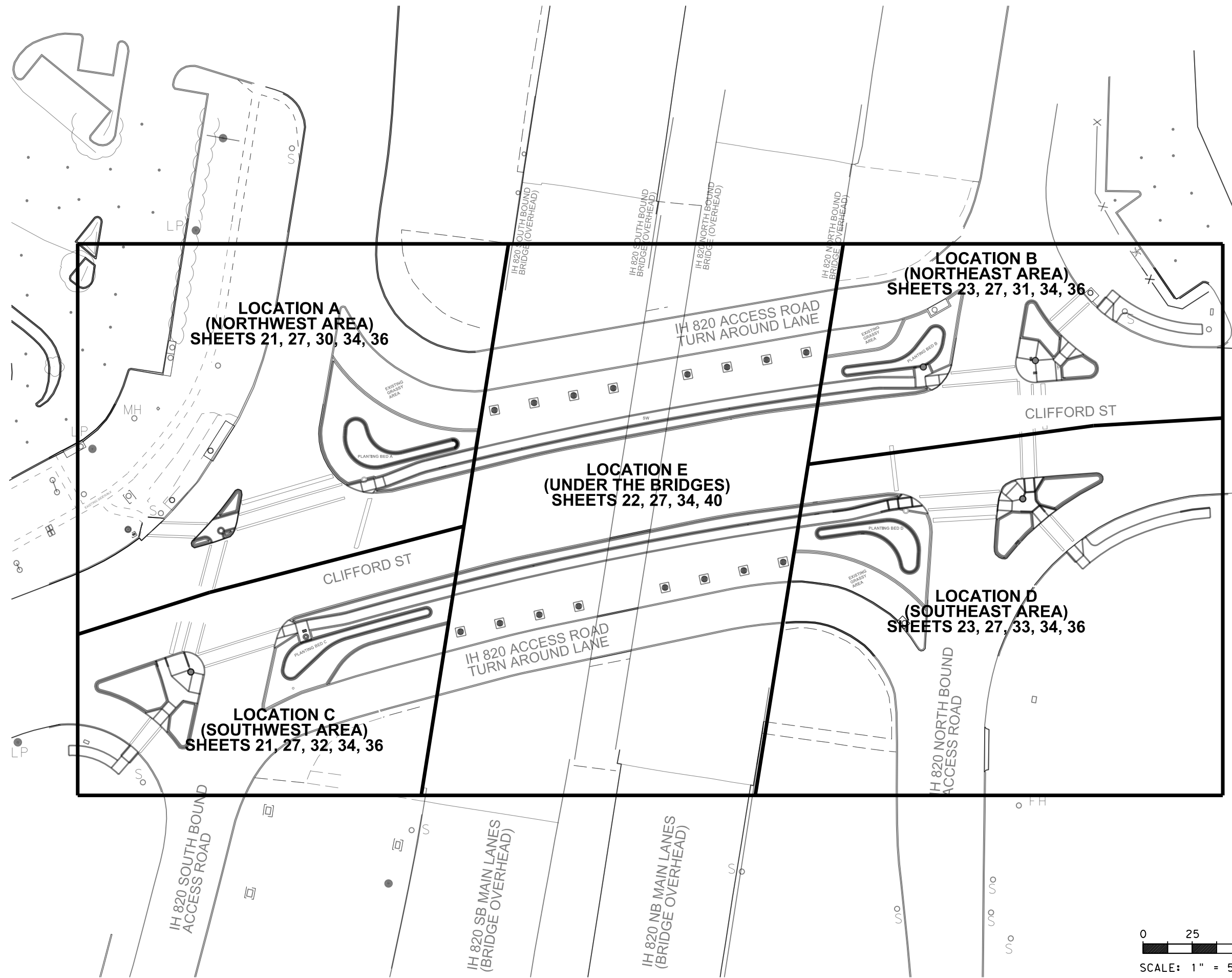
7. Where traffic is directed over a yellow centerline, channelizing devices which separate two-way traffic should be spaced on tapers at 20' or 15' if posted speeds are 35 mph or slower, and for tangent sections, at 1/2S where S is the speed in mph. This tighter device spacing is intended for the areas of conflicting markings, not the entire work zone.

Traffic Operations Division Standard

TRAFFIC CONTROL PLAN LANE CLOSURES ON MULTILANE CONVENTIONAL ROADS

TCP (1-4) - 18

FILE: tcp1-4-18.dgn	DN:	CK:	DW:	CK:
© TxDOT December 1985	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
2-94 4-98	DIST	COUNTY	SHEET NO.	
8-95 2-12	FTW	TARRANT	19	
1-97 2-18				



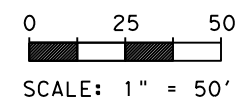
**LOCATION A
(NORTHWEST AREA)
SHEETS 21, 27, 30, 34, 36**

**LOCATION B
(NORTHEAST AREA)
SHEETS 23, 27, 31, 34, 36**

**LOCATION E
(UNDER THE BRIDGES)
SHEETS 22, 27, 34, 40**

**LOCATION D
(SOUTHEAST AREA)
SHEETS 23, 27, 33, 34, 36**

**LOCATION C
(SOUTHWEST AREA)
SHEETS 21, 27, 32, 34, 36**



James L. Godwin, Jr.
03/11/2022

IH 820 at CLIFFORD ST
PROJECT LOCATION
WHITE SETTLEMENT, TX
(SHEET 1 of 1)
















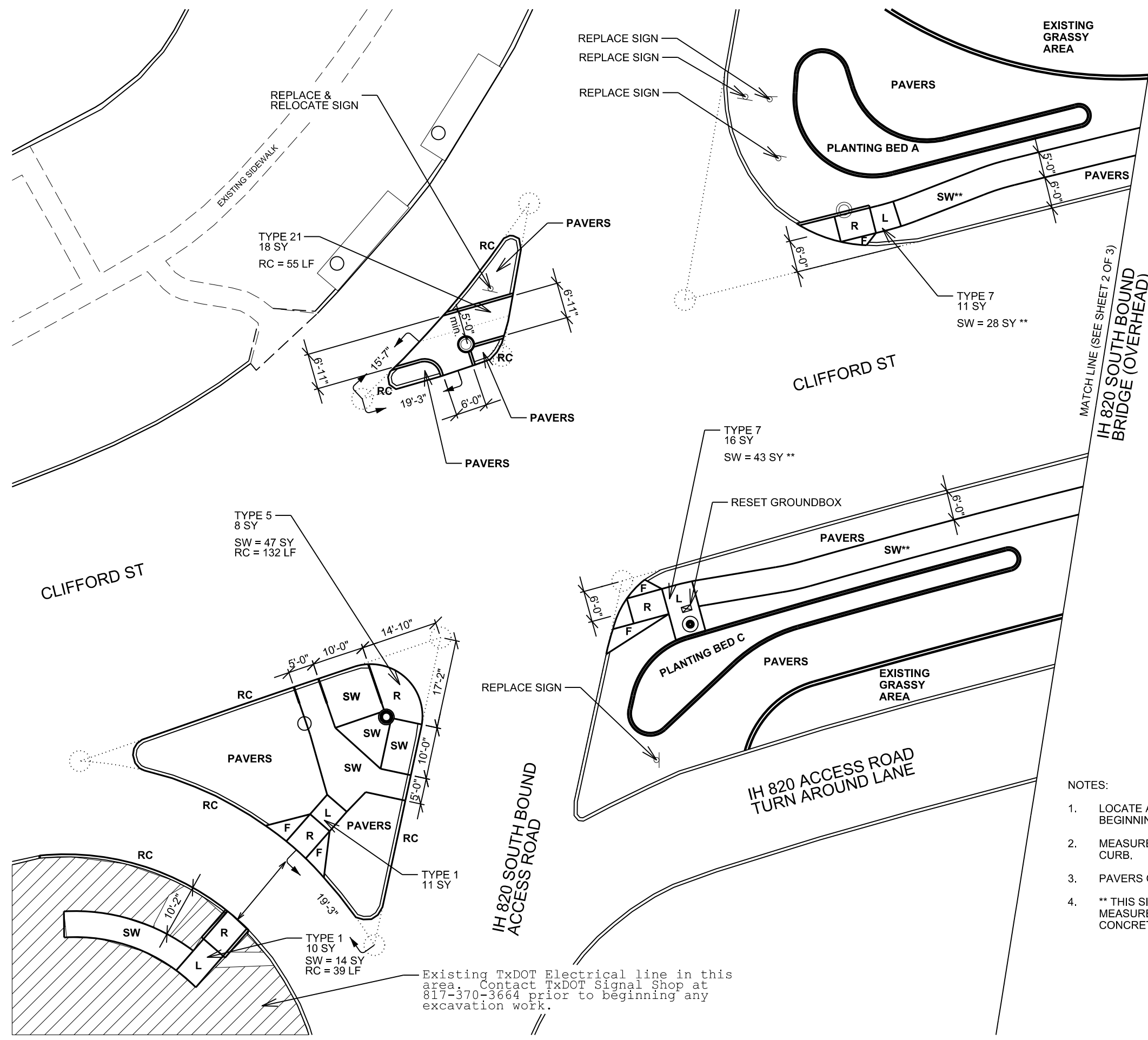
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	20	

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
104	6021	REMOVING CONC (CURB)	LF	226
529	6002	CONC CURB (TY II)	LF	226
531	6003	CONC SIDEWALKS (6")	SY	132
531	6035	CURB RAMPS	SY	74
624	6009	GROUND BOX TY D (162922)	EA	1
624	6028	REMOVE GROUND BOX	EA	1
644	6068	RELOCATE SM RD SN SUP&AM TY 10BWG	EA	4
		* CURB RAMPS (TY 1)	EA	2
		* CURB RAMPS (TY 5)	EA	1
		* CURB RAMPS (TY 7)	EA	2
		* CURB RAMPS (TY 21)	EA	1

* FOR CONTRACTOR'S INFORMATION ONLY

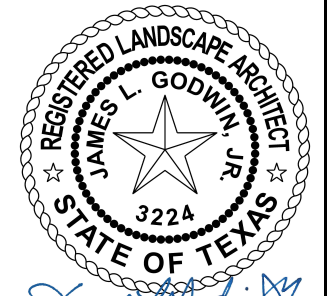
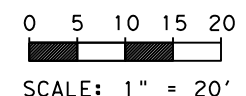
LEGEND

-  GROUND BOX
-  EXISTING BRIDGE COLUMNS
-  EXISTING PED POLE
-  SIGN
-  RAMP
-  LANDING/TURNING SPACE
-  SIDEWALK (531-6003)
-  FLARE
-  TRANSITION (531-6003)
-  LEVEL SIDEWALK (531-6003)
-  REPLACE CURB
-  TRAFFIC SIGNAL POLE
-  CURB INLET



NOTES:

- LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
- MEASURE FROM THE FACE OF CURB.
- PAVERS ON PAVER SHEET
- ** THIS SIDEWALK QUANTITY IS MEASURED TO THE EXISTING CONCRETE UNDER THE BRIDGE



James L. Godwin, Jr.
03/11/2022

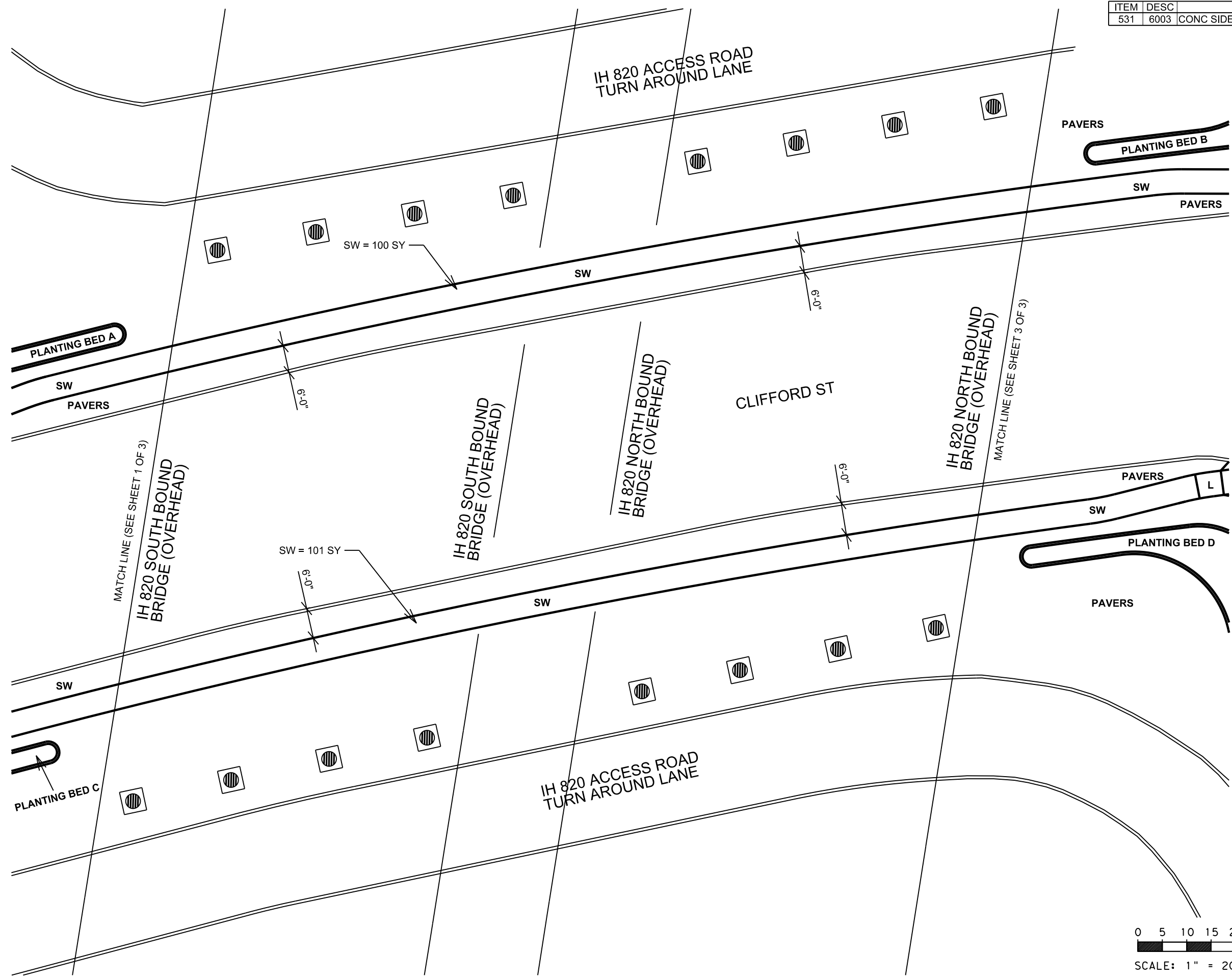
IH 820 at CLIFFORD ST
PEDESTRIAN ACCESS
LOCATION A & C
(NORTHWEST & SOUTHWEST AREAS)
WHITE SETTLEMENT, TX
(SHEET 1 of 3)



CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	21	

TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION

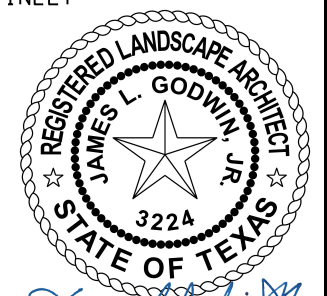
ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
531	6003	CONC SIDEWALKS (6")	SY	201



- NOTES:**
1. LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
 2. MEASURE FROM THE FACE OF CURB.
 3. PAVERS ON PAVER SHEET
 4. ** THIS SIDEWALK QUANTITY IS MEASURED TO THE EXISTING CONCRETE UNDER THE BRIDGE

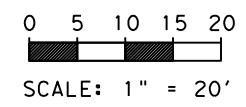
LEGEND

- GROUND BOX
- EXISTING BRIDGE COLUMNS
- EXISTING PED POLE
- SIGN
- RAMP
- LANDING/TURNING SPACE
- SW SIDEWALK (531-6003)
- FLARE
- T TRANSITION (531-6003)
- LS LEVEL SIDEWALK (531-6003)
- RC REPLACE CURB
- TRAFFIC SIGNAL POLE
- CURB INLET



James L. Godwin
 03/11/2022
 IH 820 at CLIFFORD ST

PEDESTRIAN ACCESS
 LOCATION E
 (UNDER BRIDGES)
 WHITE SETTLEMENT, TX
 (SHEET 2 of 3)



CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	22	

CKS
 DWF
 CKS
 DNF














ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
104	6021	REMOVING CONC (CURB)	LF	227
529	6002	CONC CURB (TY II)	LF	227
531	6003	CONC SIDEWALKS (6")	SY	155
531	6035	CURB RAMPS	SY	101
624	6009	GROUND BOX TY D (162922)	EA	1
624	6028	REMOVE GROUND BOX	EA	1
		* CURB RAMPS (TY 1)	EA	1
		* CURB RAMPS (TY 5)	EA	1
		* CURB RAMPS (TY 7)	EA	3
		* CURB RAMPS (TY 22)	EA	1

* FOR CONTRACTOR'S INFORMATION ONLY

NOTES:

- LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
- MEASURE FROM THE FACE OF CURB.
- PAVERS ON PAVER SHEET
- ** THIS SIDEWALK QUANTITY IS MEASURED TO THE EXISTING CONCRETE UNDER THE BRIDGE

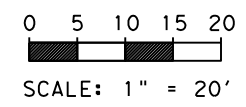
LEGEND


-  GROUND BOX
-  EXISTING BRIDGE COLUMNS
-  EXISTING PED POLE
-  SIGN
-  RAMP
-  LANDING/TURNING SPACE
-  SW SIDEWALK (531-6003)
-  F FLARE
-  T TRANSITION (531-6003)
-  LS LEVEL SIDEWALK (531-6003)
-  RC REPLACE CURB
-  TRAFFIC SIGNAL POLE
-  CURB INLET



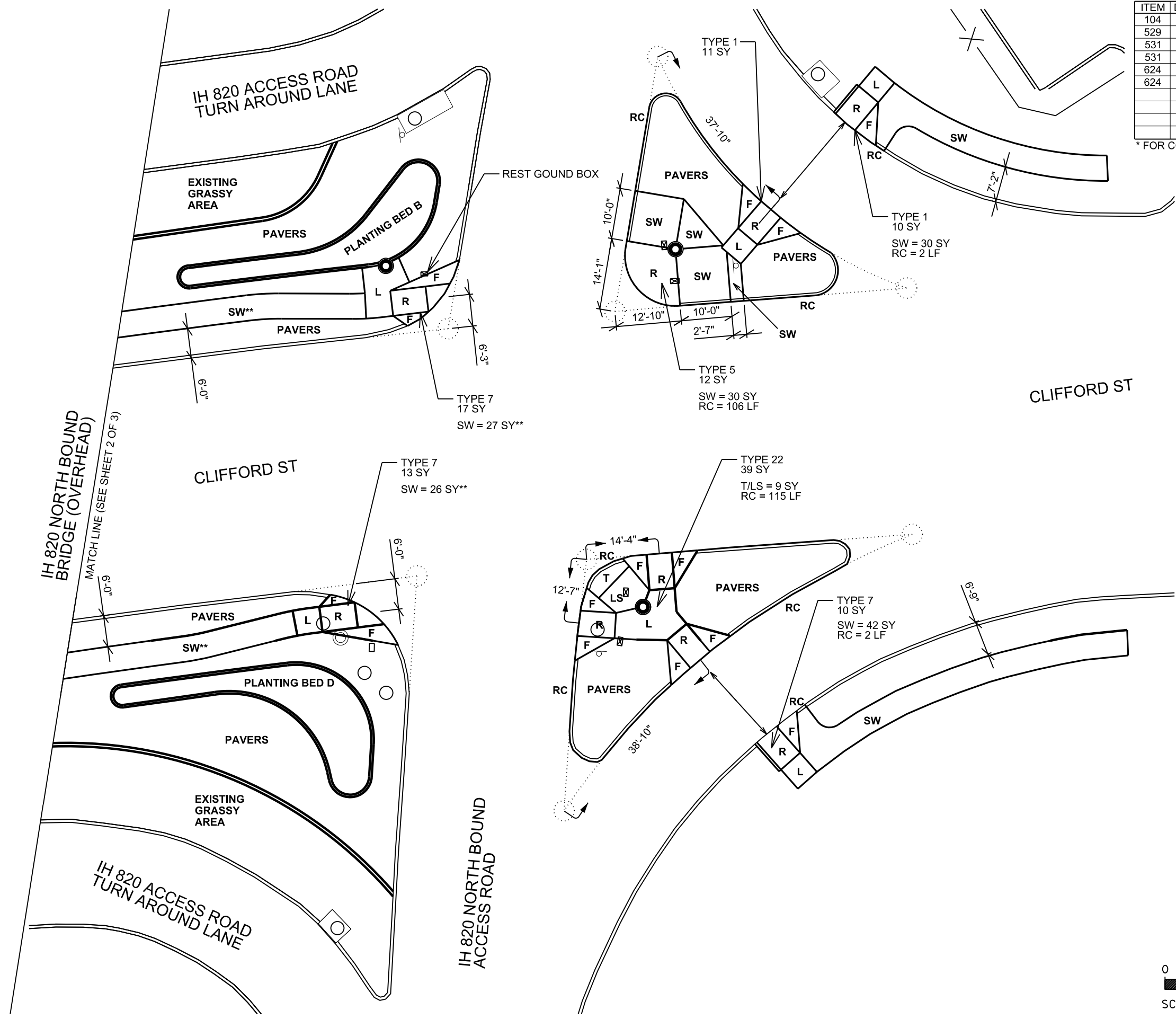
James L. Godwin
 03/11/2022

IH 820 at CLIFFORD ST
PEDESTRIAN ACCESS
 LOCATION B & D
 (NORTHEAST & SOUTHEAST AREAS)
 WHITE SETTLEMENT, TX
 (SHEET 3 of 3)

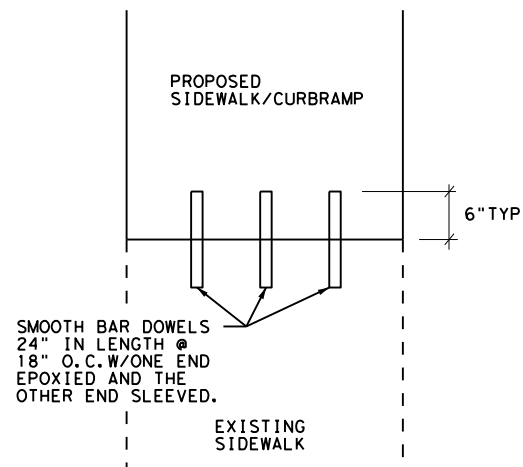


			
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	23	

TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION



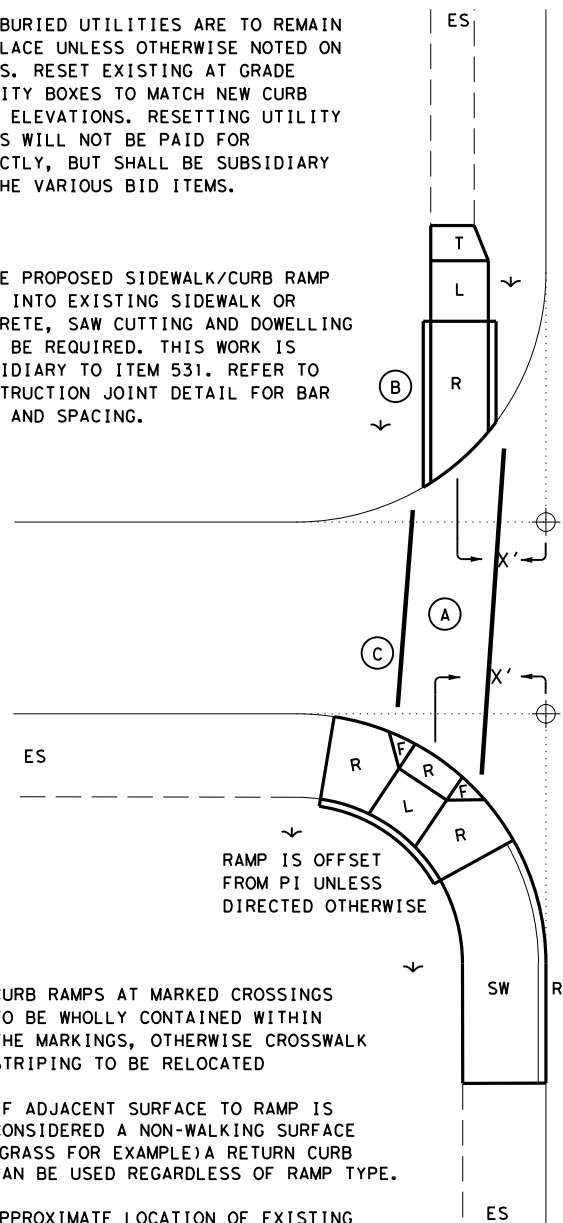
FILE: C:\Users\jgodwi1\Desktop\White Settlement\White Settlement\Standards\Sample Intersection-Special Details.dgn
 DATE: 3/11/2022 12:05:45 PM



CONSTRUCTION JOINT
 PLAN VIEW

NOTES

1. ALL BURIED UTILITIES ARE TO REMAIN IN PLACE UNLESS OTHERWISE NOTED ON PLANS. RESET EXISTING AT GRADE UTILITY BOXES TO MATCH NEW CURB RAMP ELEVATIONS. RESETTING UTILITY BOXES WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS.
2. WHERE PROPOSED SIDEWALK/CURB RAMP TIES INTO EXISTING SIDEWALK OR CONCRETE, SAW CUTTING AND DOWELLING WILL BE REQUIRED. THIS WORK IS SUBSIDIARY TO ITEM 531. REFER TO CONSTRUCTION JOINT DETAIL FOR BAR SIZE AND SPACING.



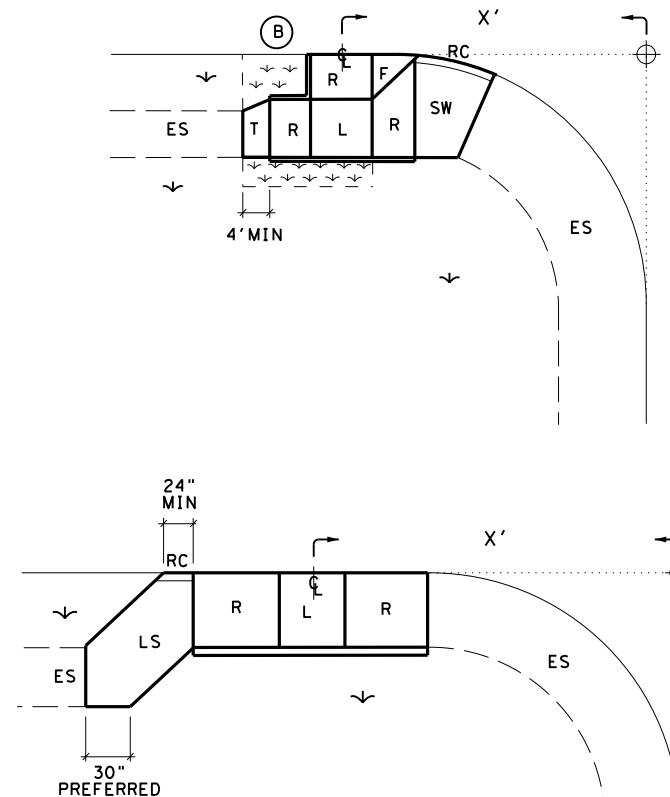
- (A) CURB RAMPS AT MARKED CROSSINGS TO BE WHOLLY CONTAINED WITHIN THE MARKINGS, OTHERWISE CROSSWALK STRIPING TO BE RELOCATED
- (B) IF ADJACENT SURFACE TO RAMP IS CONSIDERED A NON-WALKING SURFACE (GRASS FOR EXAMPLE) A RETURN CURB CAN BE USED REGARDLESS OF RAMP TYPE.
- (C) APPROXIMATE LOCATION OF EXISTING CROSSWALK MARKINGS.

SAMPLE CURB RAMP PLACEMENT
 (HORIZONTAL CONTROL)

LEGEND

- F = FLARE (10:1 OR LESS)
- R = RAMP (CROSS SLOPE NOT TO EXCEED 2%; LONGITUDINAL NOT TO EXCEED 8.33% OR 12:1)
- L = LANDING (NOT TO EXCEED 2% SLOPE IN ANY DIRECTION)
- T = TRANSITION (PAID FOR UNDER CONC SIDEWALK)
- RC = REPLACE CURB/CURB & GUTTER
- ES = SIDEWALK (EXISTING)
- X' = LENGTH MEASURED FROM PI POINT (SEE INTERSECTION SHEETS FOR DIMENSION)
- SW = SIDEWALK (NOT EXCEED 2% CROSS SLOPE)
- LS = LEVEL SIDEWALK (NOT EXCEED 2% SLOPE IN ANY DIRECTION)
- ⊕ = PI POINT MEASURED FROM TANGENTIAL CURBLINE INTERSECTION
- ↘ = EXISTING TURF

NOTE: BLOCK SOD PLACED ADJACENT TO RAMP AND/OR SIDEWALK WORK LIMITS AS REQUIRED TO RETURN SITE TO PRE-CONSTRUCTION CONDITION



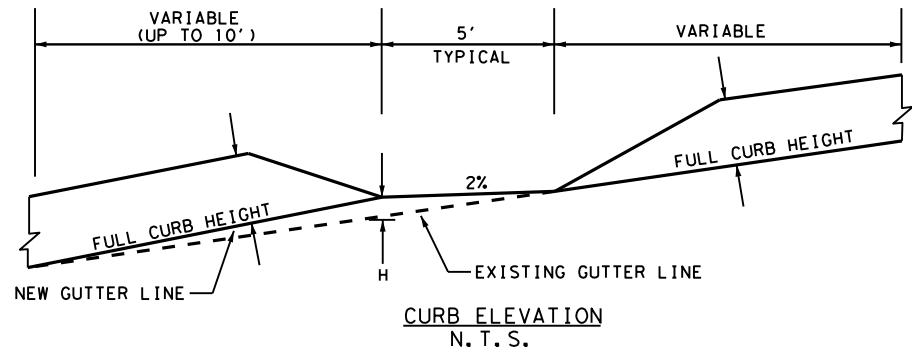
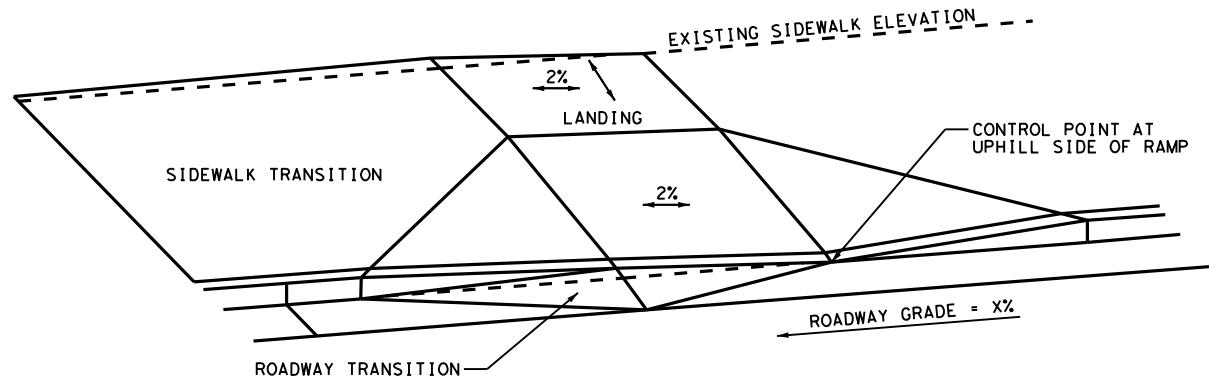
James L. Godwin
 03/11/2022

SPECIAL DETAILS

N. T. S.

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SHEET 1 OF 1			
FHWA TEXAS DIVISION	FEDERAL AID PROJECT NO.	SHEET NO.	
	SEE TITLE SHEET	24	
STATE	DISTRICT	COUNTY	
TEXAS	FTW	TARRANT	
CONTROL	SECTION	JOB	HIGHWAY NO.
0008	15	059	TH 820

ROADWAY TRANSITION

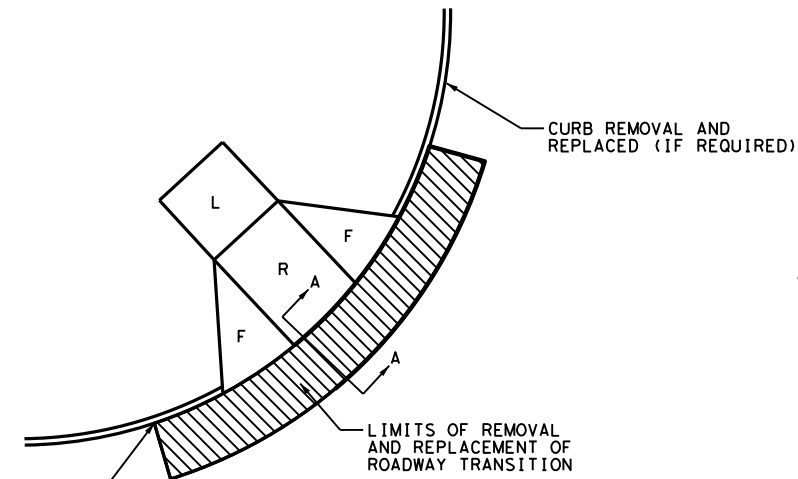


GRADE DIFFERENTIAL BETWEEN CURB RAMP CROSS SLOPE AND ROADWAY GRADE	*H
1%	0.04' (1/2")
2%	0.08' (1")
3%	0.12' (1 1/2")
4%	0.16' (2")
5%	0.20' (2 1/2")

*H = DIFFERENCE IN ELEVATION BETWEEN THE NEW GUTTER LINE AND EXISTING GUTTER LINE

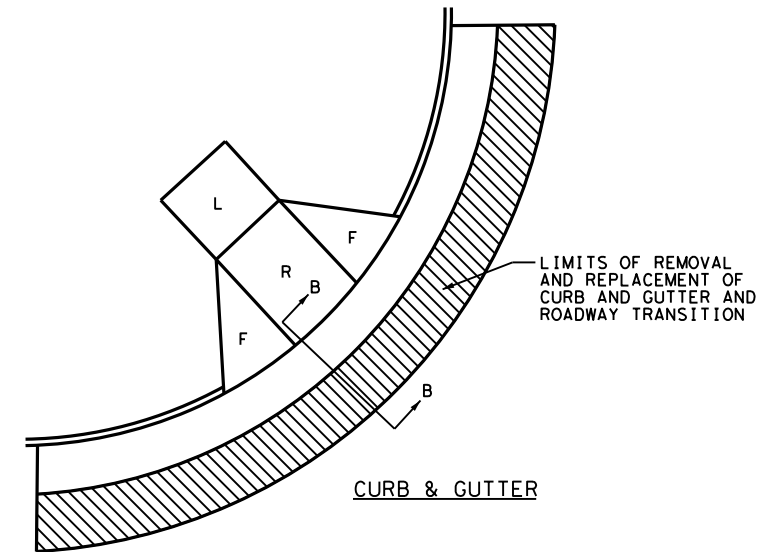
NOTES:

- UTILIZE ROADWAY TRANSITION TO TIE CROSS SLOPE OF NEWLY CONSTRUCTED CURB RAMP TO THE EXISTING ROADWAY GRADE. ROADWAY TRANSITIONS SHOULD NOT EXTEND MORE THAN 4 FEET INTO ROADWAY.
- FOR CURB SECTION, REMOVE A 2 FOOT WIDE (MIN.) BY 2 INCH DEEP SECTION OF PAVEMENT THE LENGTH OF THE TRANSITION PRIOR TO CONSTRUCTION.
- FOR CURB AND GUTTER SECTION, REMOVE CURB, GUTTER AND IF NECESSARY A SECTION OF PAVEMENT (24 INCHES MIN.) BEYOND THE GUTTER BY 6 INCHES DEEP. CONSTRUCT TRANSITION IN THE GUTTER SECTION AS SHOWN.
- CONSTRUCT FULL HEIGHT CURB AND CURB RAMP FLARES (IF REQUIRED) BASED ON NEW GUTTER LINE ELEVATIONS.
- CONSTRUCT TRANSITION FROM BOTTOM OF CURB RAMP TO ROADWAY WITH HOT-MIX ASPHALT CONCRETE AS PER PLANS AND SPECIFICATION OR AS DIRECTED.
- TRAFFIC SIGNAL LOOP DETECTORS MAY EXIST WITHIN THE ROADWAY CONSTRUCTION TRANSITION ZONE. MAINTAIN OPERATION OF LOOP DETECTORS THROUGHOUT CONSTRUCTION. REPAIR OR REPLACE ANY LOOP DETECTORS DAMAGED DURING CONSTRUCTION OPERATIONS.

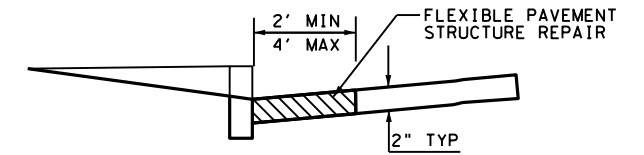


CURB REMOVAL AND REPLACEMENT (IF REQUIRED)

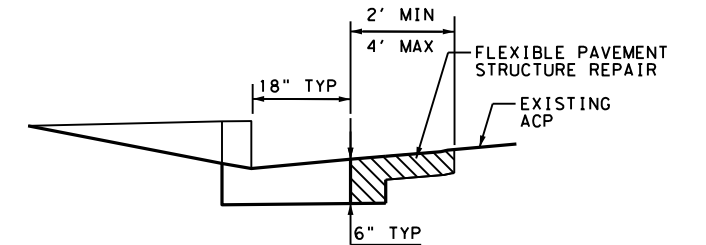
CONCRETE CURB



CURB & GUTTER



SECTION A-A



SECTION B-B



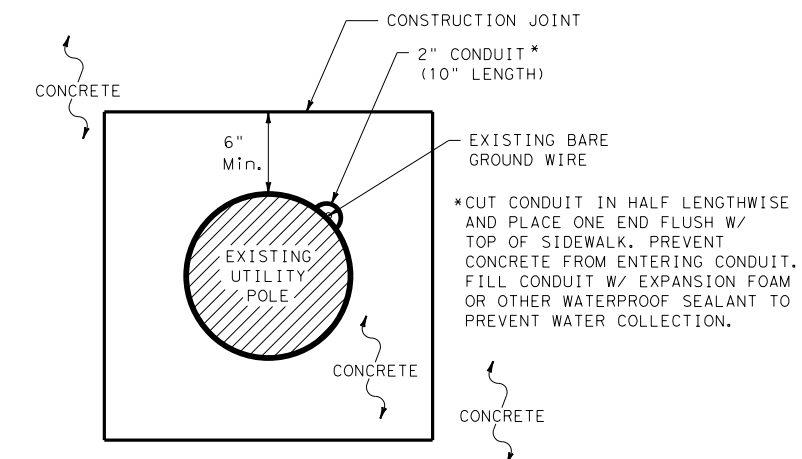
03/11/2022

ROADWAY TRANSITION DETAILS

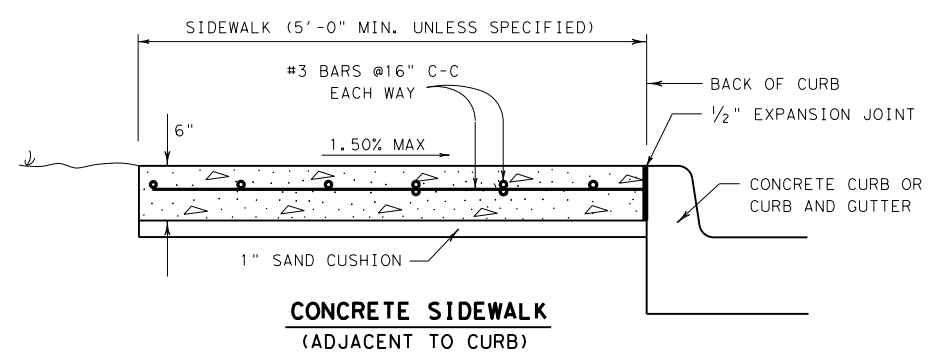
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SHEET 1 OF 1			
FHWA TEXAS DIVISION	FEDERAL AID PROJECT NO.	SHEET NO.	
	SEE TITLE SHEET	25	
STATE	DISTRICT	COUNTY	
TEXAS	FTW	TARRANT	
CONTROL	SECTION	JOB	HIGHWAY NO.
0008	15	059	IH 820

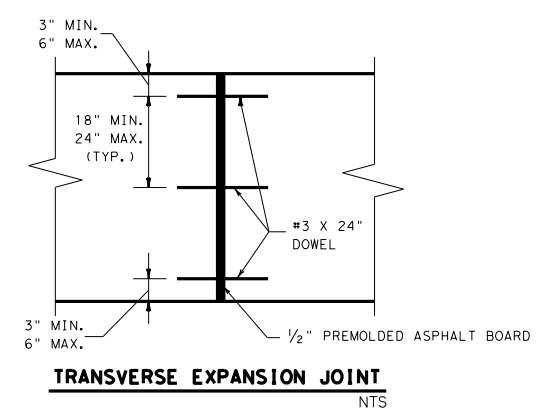
TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION



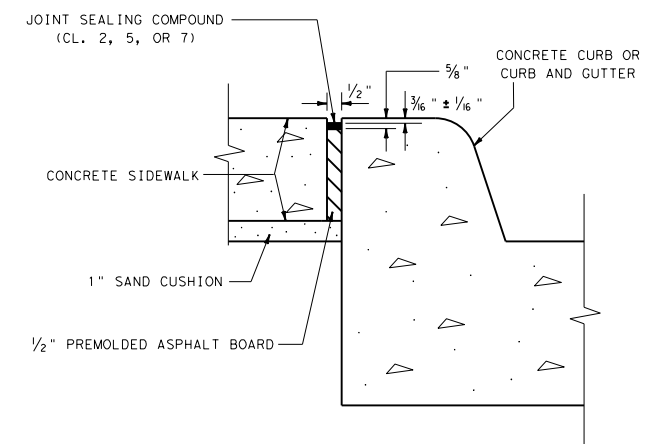
BARE GROUND WIRE PROTECTION
 SUBSIDIARY TO ITEM 531,
 SIDEWALK OR CURB RAMP NTS



CONCRETE SIDEWALK
 (ADJACENT TO CURB) NTS



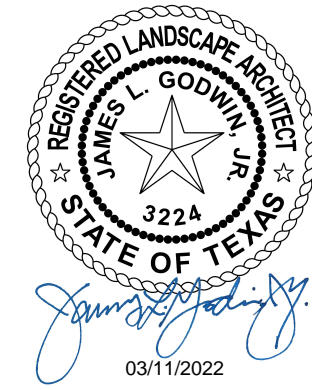
TRANSVERSE EXPANSION JOINT
 NTS



1/2" EXPANSION JOINT
 (SIDEWALK ADJACENT TO CURB) NTS

SEE PLAN SHEETS FOR LOCATIONS OF SIDEWALKS AND CURB RAMPS.

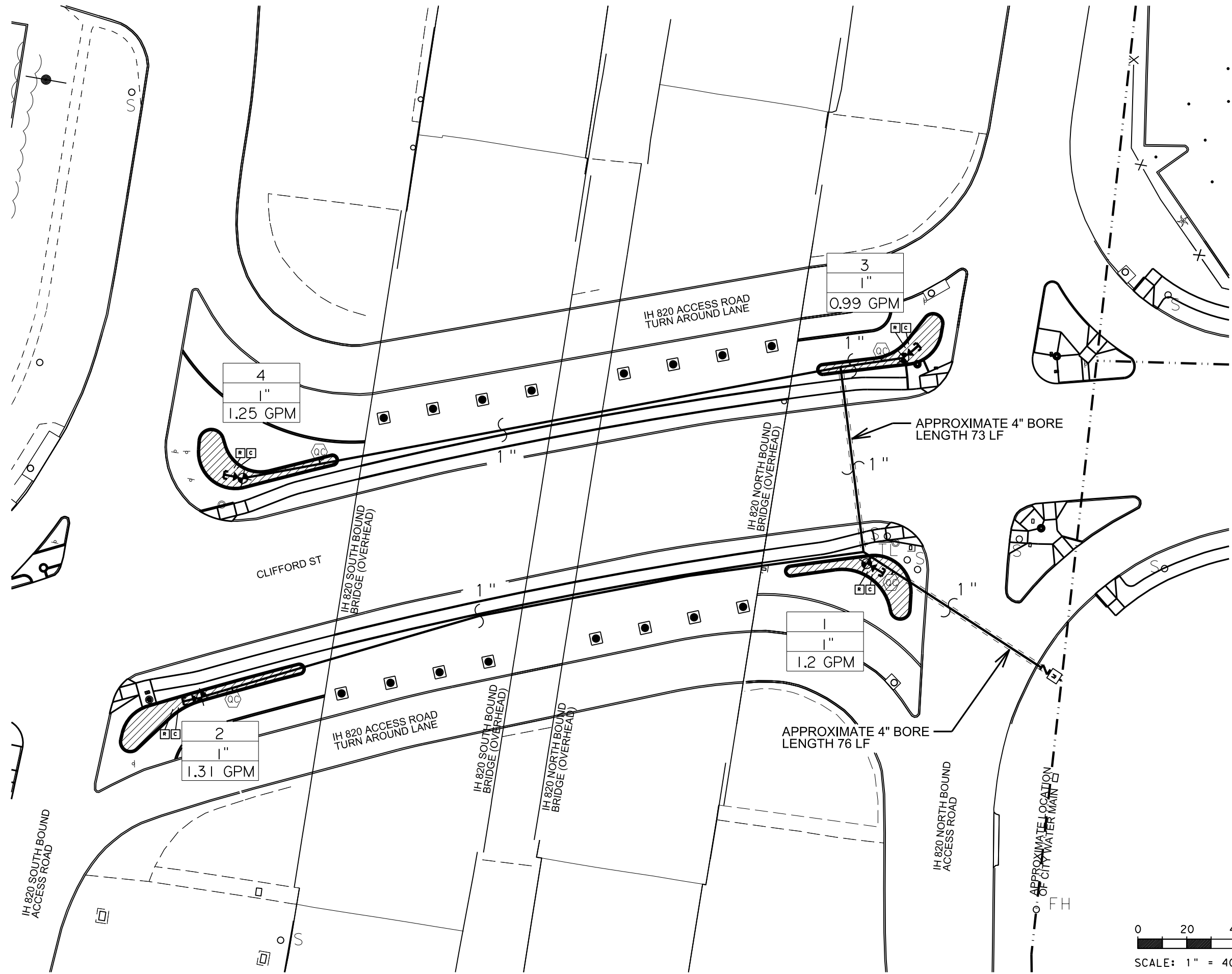
LONGITUDINAL SLOPE OF SIDEWALKS SHALL NOT EXCEED 5% EXCEPT IN CASES WHERE THE ADJACENT ROADWAY SLOPE EXCEEDS 5%. IF ROADWAY SLOPE EXCEEDS 5%, LONGITUDINAL SLOPE OF SIDEWALK MAY MATCH THAT OF ROADWAY.



Texas Department of Transportation

**MISC
CONSTRUCTION
DETAILS**

ORIGINAL DRAWING	FED. RD. DIV. NO.	PROJECT NO.	SHEET NO.
	6	SEE TITLE SHEET	26
REVISIONS:	STATE	DIST.	COUNTY
	TEXAS	FTW	TARRANT
	CONT.	SECT.	JOB HIGHWAY NO.
	0008	15	059 1H 820



IRRIGATION LEGEND:

- LATERAL PIPE
- MAINLINE
- ⊕ DRIP VALVE ASSEMBLY WITH FILTER
- ≡≡≡ BORE/SLEEVE
- [M] WATER METER
- [Z] BACKFLOW PREVENTER
- ⊥ BEGIN DRIP TUBING
- ⊕ QUICK COUPLER VALVE
- [C] CONTROLLER
- [R] RAIN/FREEZE SENSOR

VALVE INFO

ZONE	4
SIZE	1"
FLOW (GPM)	3 GPM

NOTES:

1. LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.

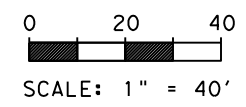


James L. Godwin, Jr.
03/11/2022

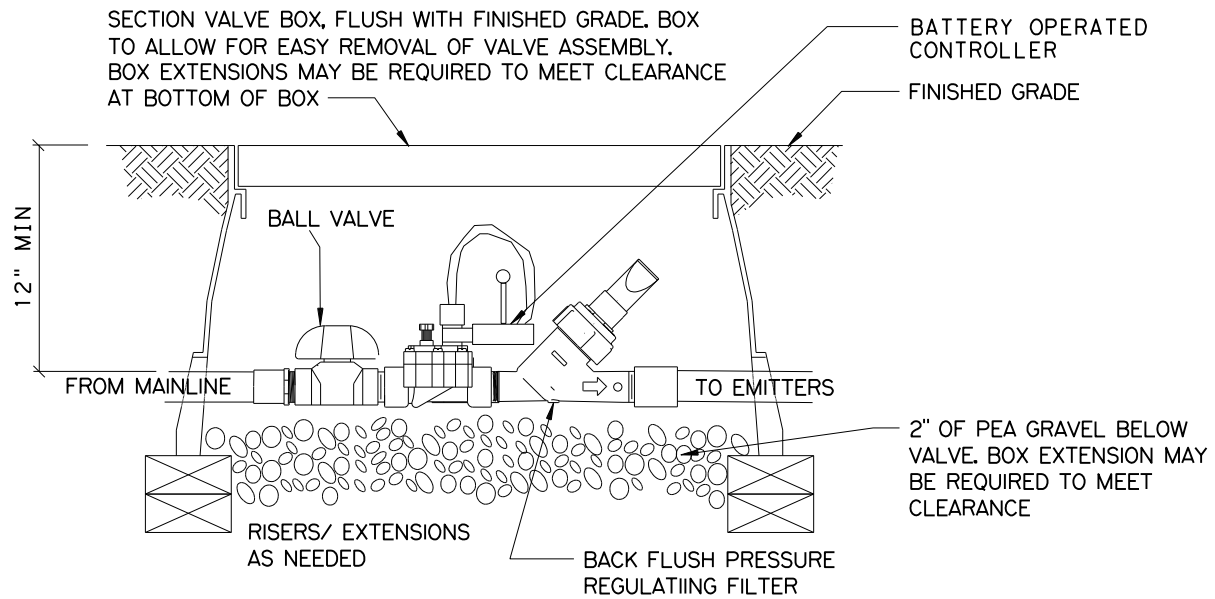
IH 820 at CLIFFORD ST

IRRIGATION PLAN

WHITE SETTLEMENT, TX
(SHEET 1 of 1)

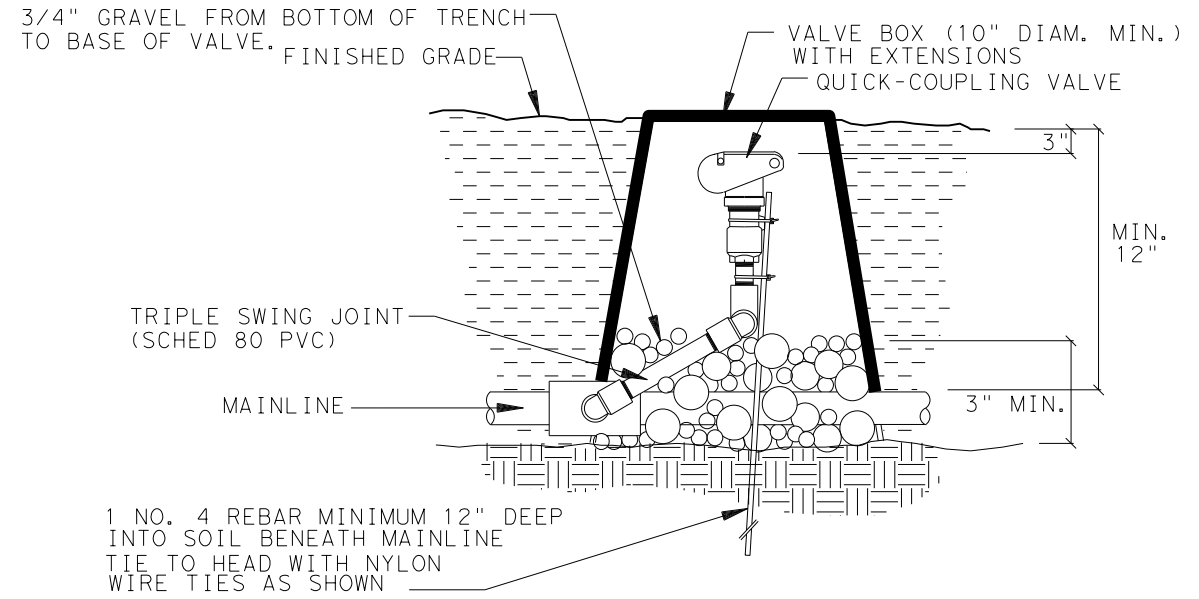


CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	27	



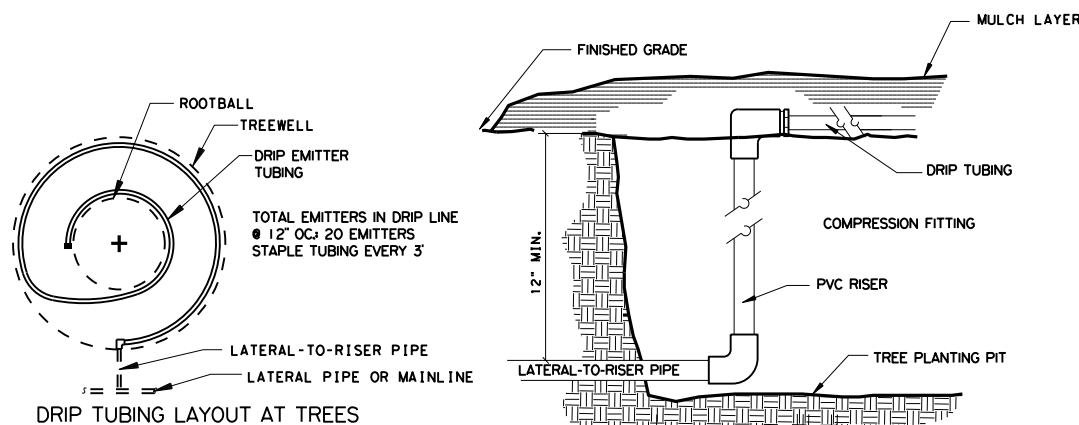
REMOTE CONTROL VALVE ASSEMBLY

NOTE: VALVES ASSEMBLY SHALL BE LOCATED NEAR THE DESIGNATED PLANTING BED AREA AS APPROVED BY ENGINEER. NTS



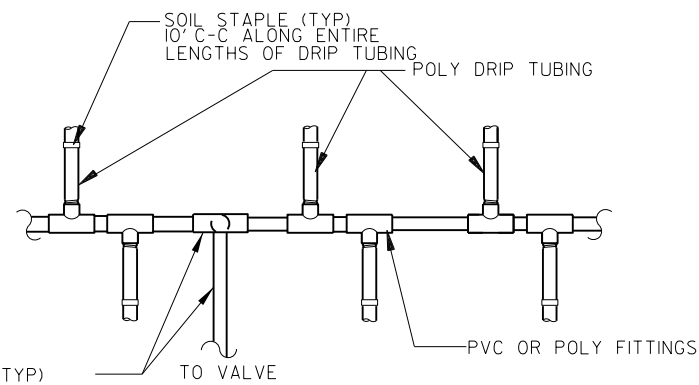
QUICK-COUPLING VALVE

NTS



DRIP TUBING LAYOUT AT TREES

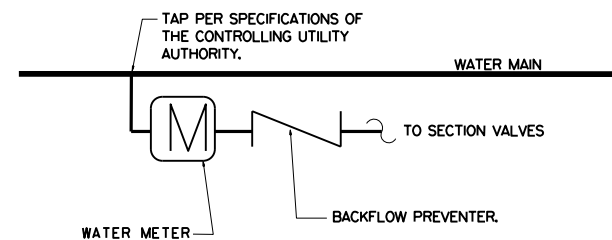
SHEET 8 AND SHEET 10 NTS



NOTE: DEPENDING ON THE MANUFACTURER TUBING, EACH DRIP SECTION SHALL HAVE AN AIR RELIEF VALVE AND FLUSH VALVE NEAR THE END OF THE LAST RUN OF TUBING.

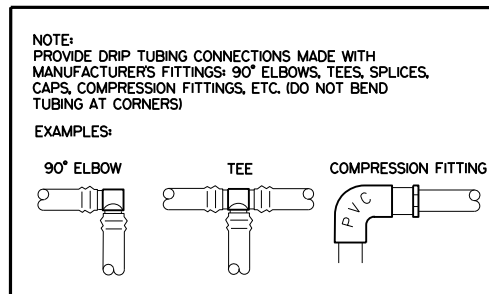
DRIP TUBING LAYOUT FOR BED AREAS

NTS

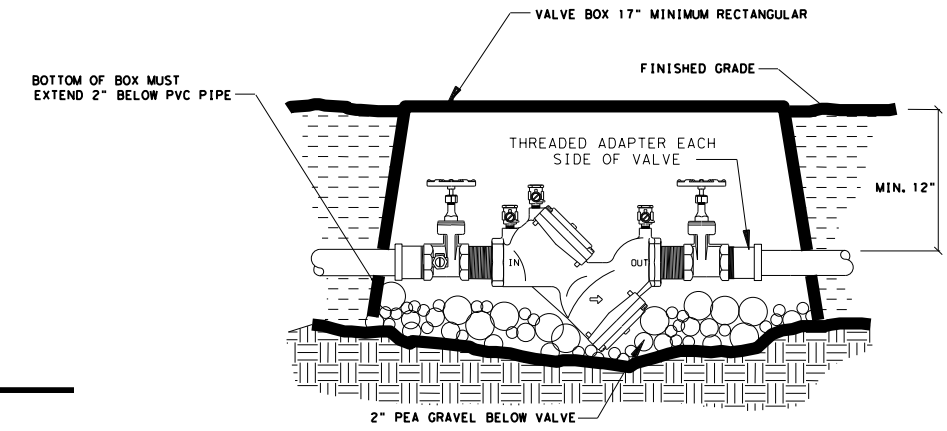


TYPICAL TAP AND METER ASSBY.

NTS



NTS



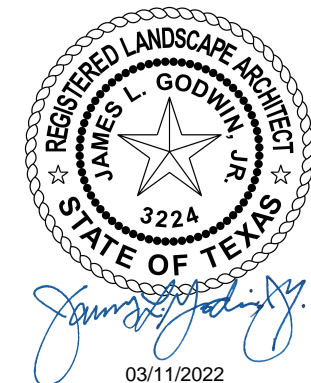
BACKFLOW PREVENTER

NTS

IH 820 at CLIFFORD ST

IRRIGATION DETAILS

WHITE SETTLEMENT, TX (SHEET 1 of 1)



CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY		SHEET NO.
FTW	TARRANT		28

TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION

GENERAL IRRIGATION NOTES:

- Reference Item 170 of the Texas Standard specifications for Construction and Maintenance of Highways, Streets and Bridges 2004 for specifications, dimensions, volumes and measurements that have been modified or not shown.
- Provide all permits, licenses, tests, and/or approvals, paying any fees and deposits and installing or arranging for all water meters and taps for installation and operation as applicable. Deposits will not be refunded. Provide water meters in the name of the entity, as provided by engineer. The entity is responsible for monthly water charges. Water meters are to remain operational and turned on through all phases of the contract to ensure plants receive required watering.
- Backflow preventers are to be placed in the name of the entity. Provide for all charges, fees, tests, backflow preventer testing at installation or annual inspection, required by local entity through all phases of the contract.
- The drawings are diagrammatic of the work to be performed. Changes may be required due to varying conditions or as directed by the engineer.
- Verify location of any underground utilities with appropriate agencies. Underground utilities (if shown) on the plans are approximate.
- See IRRIGATION DETAILS AND MATERIALS CHART for materials specifications, sizes, and requirements.

CONSTRUCTION METHODS:

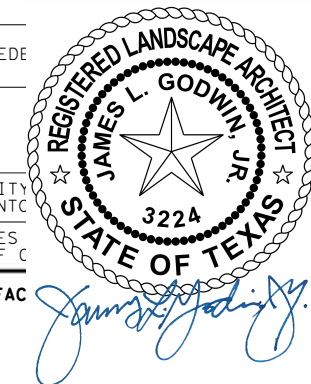
- Investigate the site conditions affecting the work and furnish offsets, fittings, and sleeves as required to meet site conditions.
- Prior to installation all irrigation valves, mainlines, quick coupler valves, dripline, etc, are to be located and approved.
- Any deviations in the piping as shown on the plans need approval, in writing, from the engineer.
- Care shall be exercised when excavating near trees. No mechanical Adjust trench path and/or excavate by hand to avoid damage to existing tree root system.
- Coordinate and verify location of signal wiring, traffic loop detector wiring, and CTMS wiring prior to beginning any work. Damage to signal wiring, loop detector wiring, CTMS wiring, any utilities not listed, and structures shall be repaired at contractor's expense.
- Any underground utilities, high mast wiring, and CTMS wiring shown on plans are approximate locations. Contact appropriate authorities to locate underground utilities, wiring and any structure.
- Dig trenches straight and support pipe continuously on bottom of trench. Install pipe to an even grade. Trench bottoms are to be clean and smooth with all organic debris and sharp objects removed. Snake pipe in trench to allow for expansion and contraction. For public safety, use plastic construction fencing, minimum 4 feet high around open excavations.
- Stake boring and sleeve locations for approval. Item 170 describes boring depth. Continuous boring and sleeves are to extend the full width of the pavement and 6' on each side thereof. Boring and sleeves are incidental to Item 170. Install bore encasement pipe the same day as the boring.
- PVC casing for bores and sleeves to consist of SCH 40 smooth wall pipe with welded joints and seams and are to be continuous. Do not exceed the diameter of casing required by the plans by more than 1" at the bore.
- Do not install pipe when air temperature is below 40 degrees Fahrenheit. Cut pipe in a manner that will insure a square cut. Remove burrs at cut ends prior to installation so that a smooth unobstructed flow will be obtained.
- Flush all water lines, valves and sprinkler bodies before installing drip tubing or sprinkler nozzles.
- Enclose all wire connections and splices in ground boxes.
- Compaction of the pipe trenches must be sufficient to limit short term settling of the backfill to no more than 1". Correct settling greater than 1" without additional compensation.

GUARANTEE AND ACCEPTANCE:

- Maintenance period. Inspect irrigation system concurrently with, and subject to the same establishment/maintenance requirement periods under Items 192 and 193. During the installation, establishment, and maintenance, perform the following activities as a minimum and to the satisfaction of the engineer:
 - Install and maintain the controller program to insure the proper distribution of water (includes replacement of any batteries).
 - Inspect, repair, and/or replace any equipment that is found defective or may be damaged by other maintenance.
 - Make any adjustments that may become necessary to ensure the proper delivery of water to the plant material.
- As-built drawings. Upon completion of the required maintenance period, the engineer will make an inspection of the project. Provide the engineer a set of as-built drawings on reproducible 11x17 film base sheets. The engineer will check base sheets to be sure they are a true record of the project conditions and will direct to correct any errors that are found. Provide drawings that show all valve locations by triangulation from a fixed object and any change to sprinkler head location from a fixed object and any change to sprinkler head location and rerouting of main and lateral lines (changes of this nature are to be approved by the engineer prior to installation).
- Operating and maintenance data. Provide instructions covering full operation, care and maintenance of the equipment, including a schedule showing length of time each valve is to be open to provide determined amount of water, and instruct the state's designated personnel in proper operation of the system.
- Test. The system is to be tested in accordance with Item 170. Test and assure the proper electrical working order of the system to the satisfaction of the engineer.

DESCRIPTION	* EXAMPLE OR EQUAL	SIZE	APPROXIMATE QUANTITIES FOR INFORMATIONAL PURPOSE ONLY
WATER METER	PER LOCAL CODE	1"	1 INSTALLED BY CITY
DRIPLINE TUBING WITH DUAL OUTLET PORTS	RAINBIRD DRIPLINE LD 0612500	1.0 GAL./HR, 12" EMITTER SPACING	APPROX. 2795 LF
	* TORO DL2000 * NETAFIM TECHLINE	1.0 GAL./HR, 18" EMITTER SPACING	APPROX. 1068 LF
PRESSURE REGULATING FILTER	RAINBIRD PRF-100-BFF	1"	4
REMOTE CONTROL VALVE	RAINBIRD PEB SERIES	1"	4 (INSTALL APPROPRIATE DC-LATCHING ACTUATOR)
BALL VALVE		1"	4
CONTROLLER SPECIFICATION	HUNTER WVC CONTROLLER WITH IN-GROUND MOISTURE SENSOR		4
	PROGRAMMER		1 PROGRAMMER IS TO BE NEW AND UNUSED
QUICK COUPLING VALVE	RAINBIRD 33DRC	3/4"	4 PROVIDE TWO (2) QUICK COUPLING KEYS TO STATE
BACKFLOW PREVENTER	FEBCO or CONBRACO PER LOCAL CODE	1	1
* AIR/VACUUM RELIEF VALVE	NETAFIM		TORO AND NETAFIM REQUIRE AIR/VACUUM RELIEF VALVES
* FLUSH VALVE	NETAFIM		
PVC CLASS 200 LATERALS		1"	1/2" 224' 1" 93'
PVC SCH40 MAINLINE PRESSURE RATED WITH TWIN GASKET COUPLINGS AND FITTINGS OR SLIP TYPE SOLVENT WELDED JOINTS		1"	1" 448' 1/2" 2160'
ABOVE GROUND PIPE INCLUDING BURIED RISERS AND SWING JOINT COMPONENTS are to be SCH 80 PVC pipe RATED FOR DIRECT SUNLIGHT EXPOSURE			AS NEEDED
FITTINGS ALL FITTINGS INCORPORATED INTO SYSTEM SHALL BE OF THE SAME TYPE, SIZE AND CLASS MATERIAL AS THE PIPE			AS NEEDED
CONTROL WIRE ALL LOW VOLTAGE CONTROL WIRE TO BE COLOR CODED. WIRE SIZES ARE TO CONFORM TO THE CONTROLLER MANUFACTURER SPECIFICATIONS FOR MAXIMUM DISTANCES FOR SPECIFIC WIRE SIZES. ALL WIRE IS TO BE SPECIFICALLY MANUFACTURED FOR DIRECT BURIAL. ALL WIRE CONNECTIONS AND SPLICES ARE TO BE MADE IN GROUND BOXES. THE SPLICE ARE TO BE COMPLETELY WATERPROOF AND BE COMPLETELY ENCAPSULATED WITHIN A KING SAFETY SEALED IRRIGATION CONNECTOR/SPLICE ENCLOSURE OR AN APPROVED EQUAL			AS NEEDED
SOLVENT CEMENT SOLVENT CEMENT IS TO BE THE TYPE RECOMMENDED BY THE PIPE MANUFACTURER			AS NEEDED
VALVE BOXES BOXES FOR SECTION VALVES, BELOW-GROUND BACKFLOW PREVENTORS, AND QUICK COUPLING VALVES ARE TO BE SHOWN ON DETAIL SHEET		10" FOR QUICK COUPLER MINIMUM 17" FOR DRIP VALVES AND BACKFLOW PREVENTER	
VALVE BOX RISERS		BOX RISER TO EXTEND BELOW VALVES AS SHOWN ON DETAIL SHEET	QUANTITY PREVENTC
BORES (SUBSIDIARY TO ITEM 170)		SCHED 40 PVC WITH SOLVENT-WELDED JOINTS	2 BORES 149 LF C

* REFERENCE TO MANUFACTURER'S TRADE NAME OR CATALOG NUMBER IS FOR THE PURPOSE OF IDENTIFICATION ONLY, FURNISH LIKE MATERIALS OF OTHER MANUFACTURER OF EQUAL QUALITY AND COMPLY WITH SPECIFICATIONS FOR THIS PROJECT AND ARE APPROVED BY THE ENGINEER.



VALVES, BELOW GROUND BACKFLOW AND ANY ACCESSORIES

IH 820 at CLIFFORD ST

IRRIGATION MATERIALS AND SPECIFICATIONS

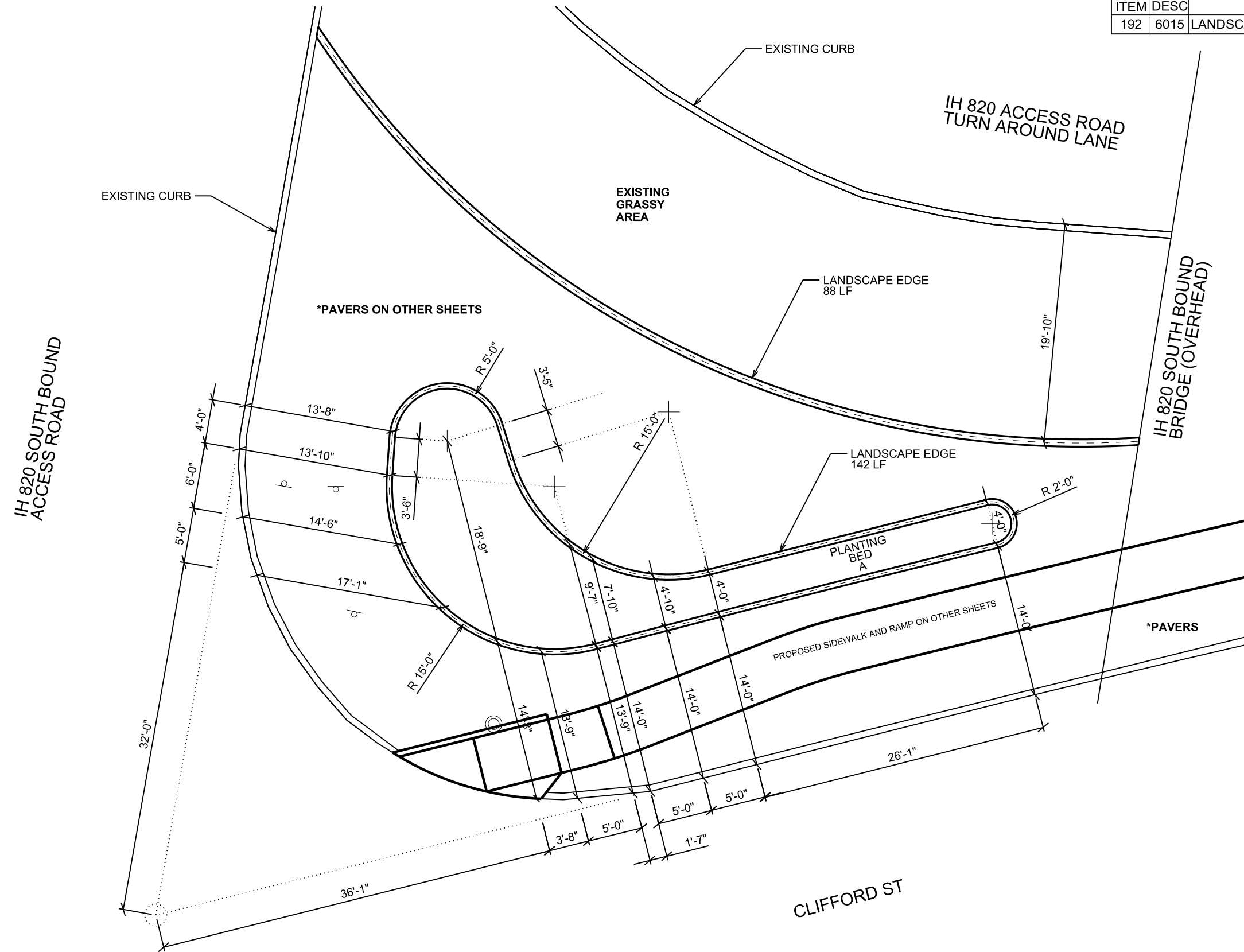
WHITE SETTLEMENT, TX
(SHEET 1 of 1)

CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	29	

ROAD DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION

03/11/2022

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
192	6015	LANDSCAPE EDGE	LF	230



NOTES:

1. LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
2. RADIUS OF CURVES MEASURED AT THE CENTERLINE OF CONCRETE LANDSCAPE EDGE.
3. MEASURE PERPENDICULAR FROM THE FACE OF CURB.

LEGEND

- ⊙ EXISTING PED POLE
- ⊐ SIGN



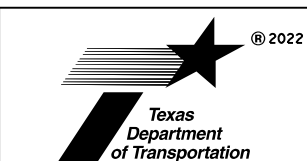
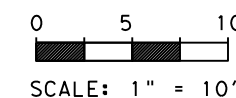
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IH 820 at CLIFFORD ST

PLANTING BED LAYOUT

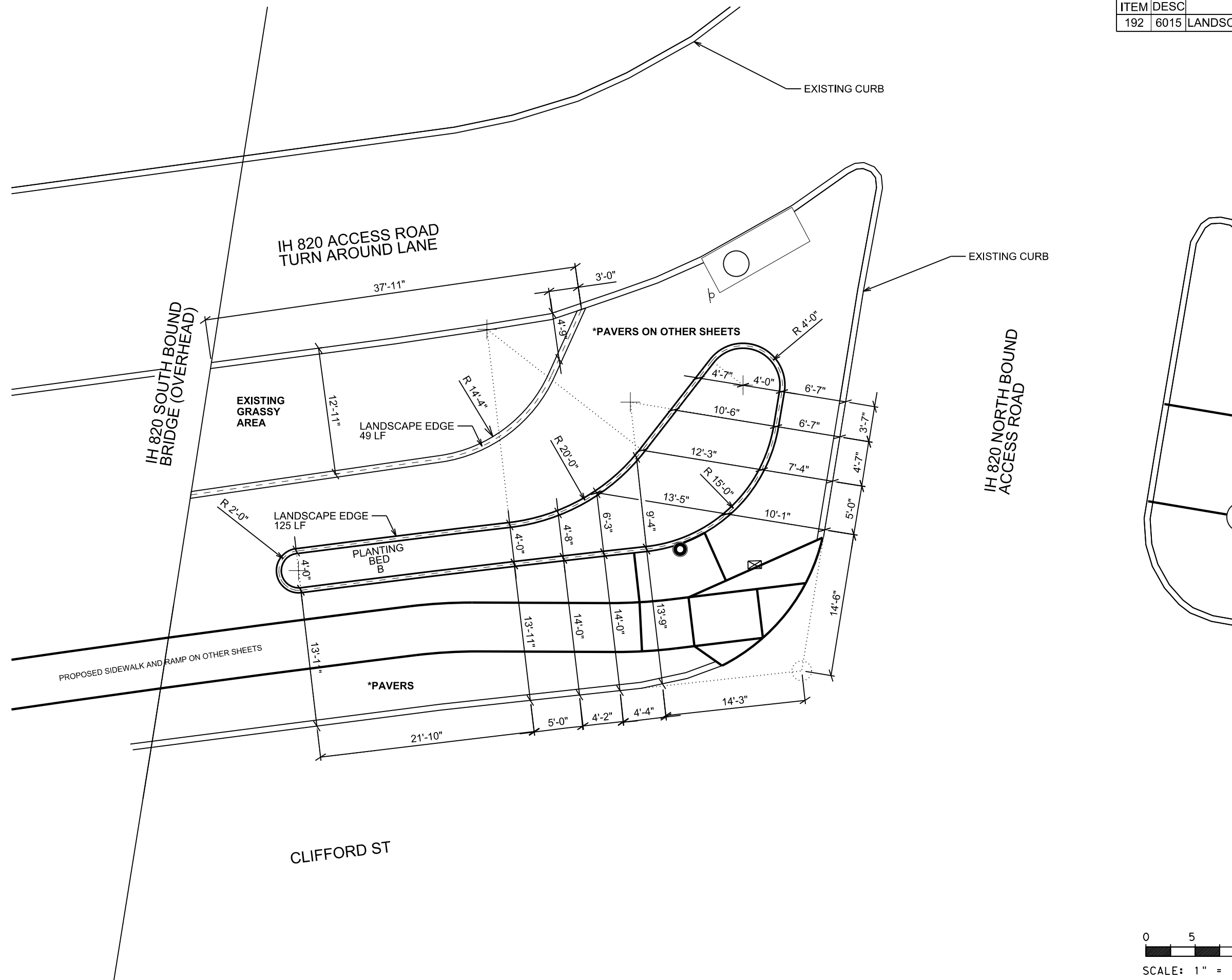
LOCATION A
(NORTHWEST AREA)

WHITE SETTLEMENT, TX
(SHEET 1 of 4)



CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	30	

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
192	6015	LANDSCAPE EDGE	LF	174

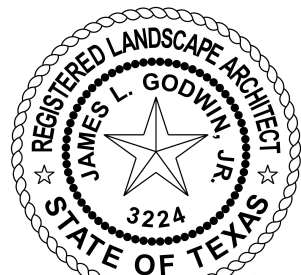
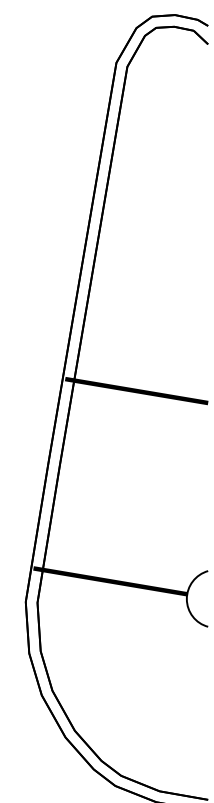


NOTES:

1. LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
2. RADIUS OF CURVES MEASURED AT THE CENTERLINE OF CONCRETE LANDSCAPE EDGE.
3. MEASURE PERPENDICULAR FROM THE FACE OF CURB.

LEGEND

- EXISTING PED POLE
- SIGN
- TRAFFIC SIGNAL POLE
- CURB INLET
- GROUND BOX



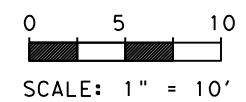
James L. Godwin, Jr.
03/11/2022

IH 820 at CLIFFORD ST

PLANTING BED LAYOUT

LOCATION B
(NORTHEAST AREA)

WHITE SETTLEMENT, TX
(SHEET 2 of 4)



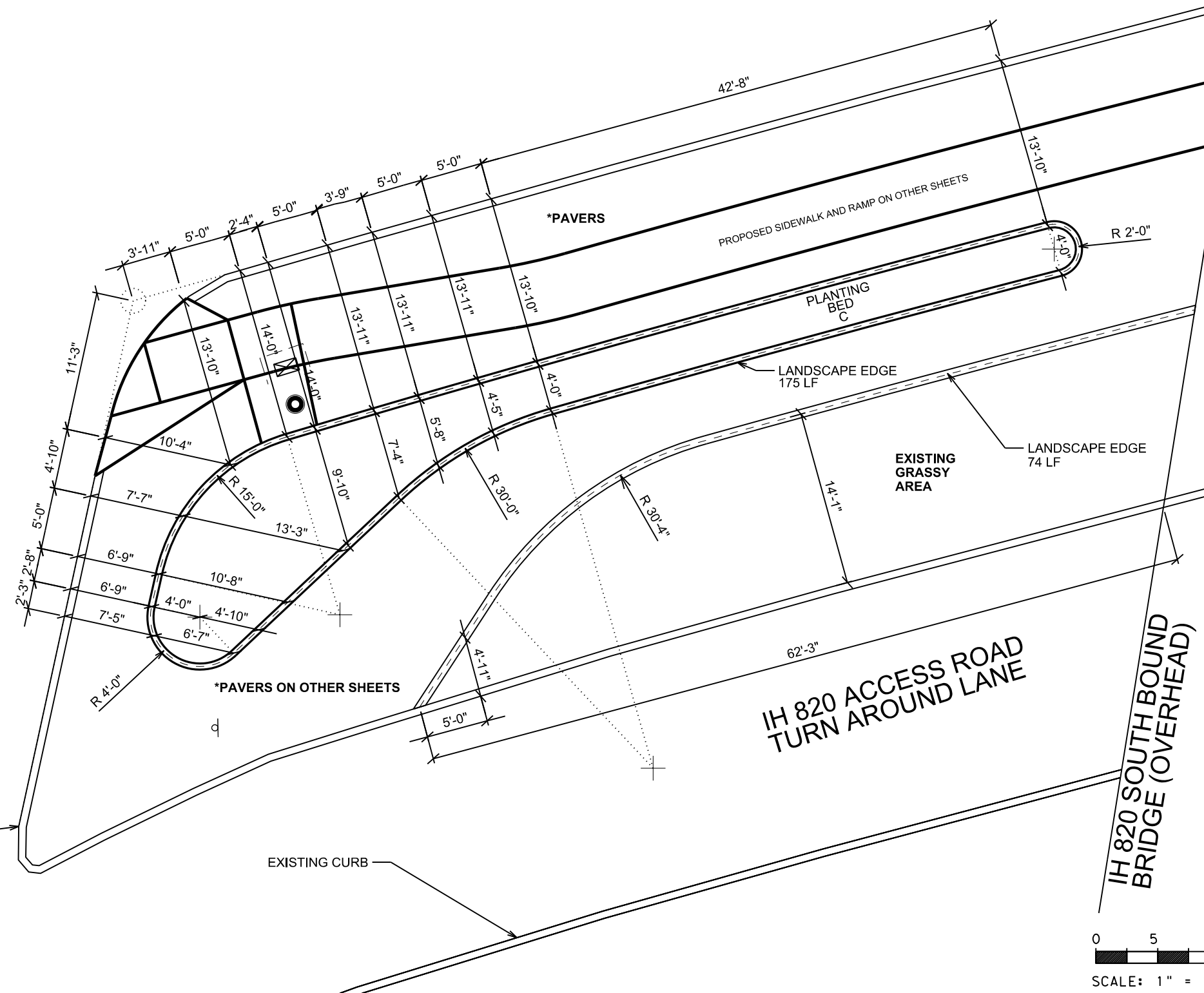
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	31	

TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
192	6015	LANDSCAPE EDGE	LF	249

CLIFFORD ST

IH 820 SOUTH BOUND
ACCESS ROAD



NOTES:

1. LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
2. RADIUS OF CURVES MEASURED AT THE CENTERLINE OF CONCRETE LANDSCAPE EDGE.
3. MEASURE PERPENDICULAR FROM THE FACE OF CURB.

LEGEND

- EXISTING PED POLE
- SIGN
- TRAFFIC SIGNAL POLE
- CURB INLET
- GROUND BOX



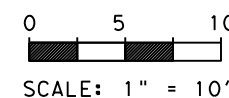
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IH 820 at CLIFFORD ST

PLANTING BED LAYOUT

LOCATION C
(SOUTHWEST AREA)

WHITE SETTLEMENT, TX
(SHEET 3 of 4)






			© 2022
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY		SHEET NO.
FTW	TARRANT		32

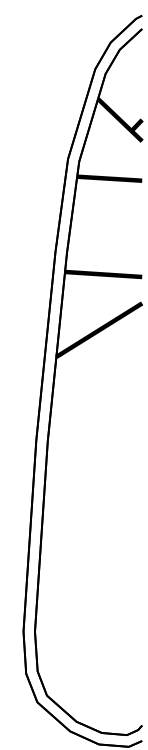
ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
192	6015	LANDSCAPE EDGE	LF	210

NOTES:

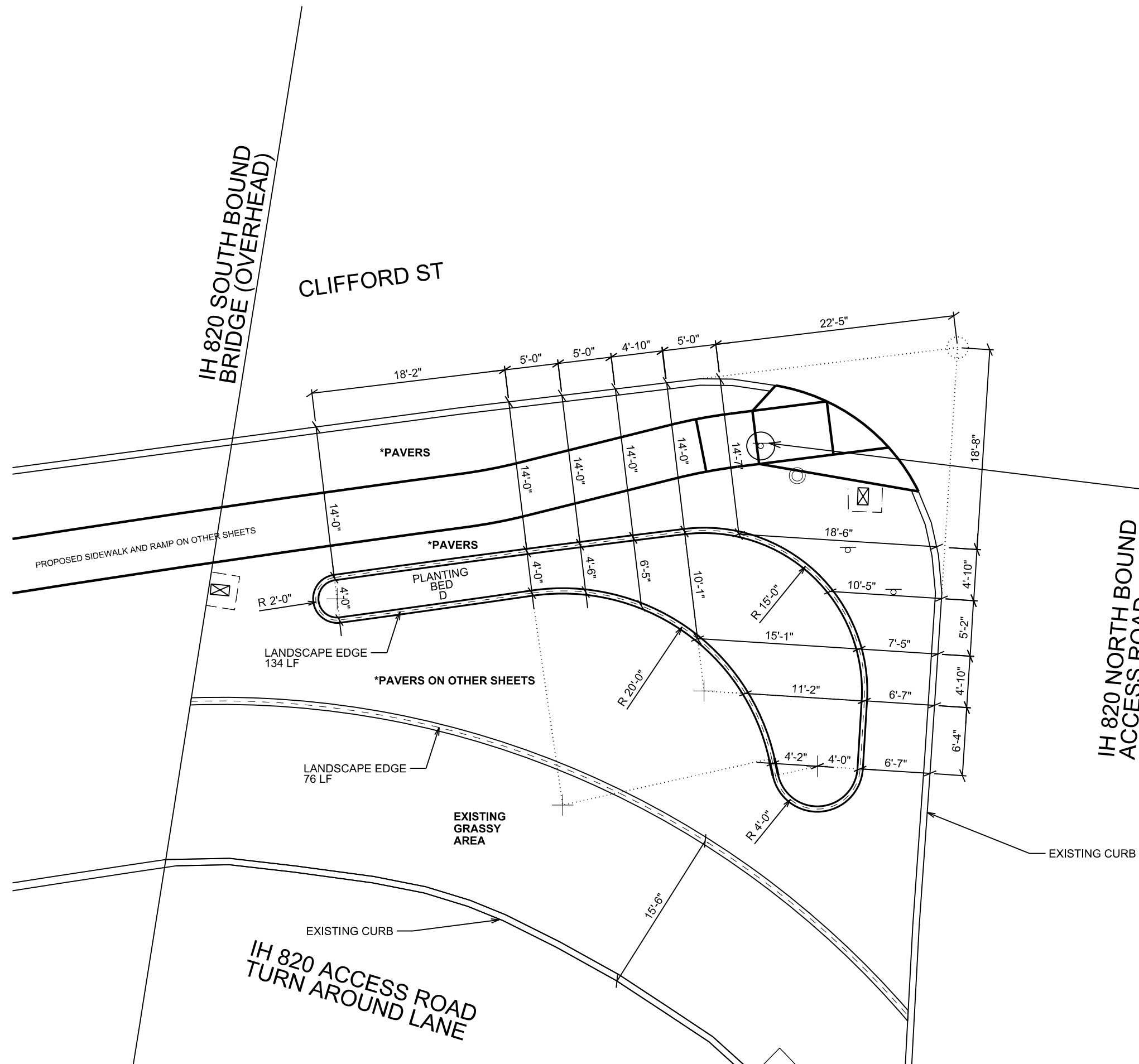
1. LOCATE ALL UTILITIES BEFORE BEGINNING CONSTRUCTION.
2. RADIUS OF CURVES MEASURED AT THE CENTERLINE OF CONCRETE LANDSCAPE EDGE.
3. MEASURE PERPENDICULAR FROM THE FACE OF CURB.

LEGEND

-  EXISTING PED POLE
-  SIGN
-  GROUND BOX

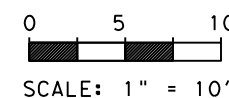



TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION



James L. Godwin, Jr.
03/11/2022

IH 820 at CLIFFORD ST
PLANTING BED LAYOUT
LOCATION D
(SOUTHEAST AREA)
WHITE SETTLEMENT, TX
(SHEET 4 of 4)



		© 2022	
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	33	

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
528	6004	LANDSCAPE PAVERS (CHARCOAL - 241) (TERRA COTTA - 979)	SY	1220

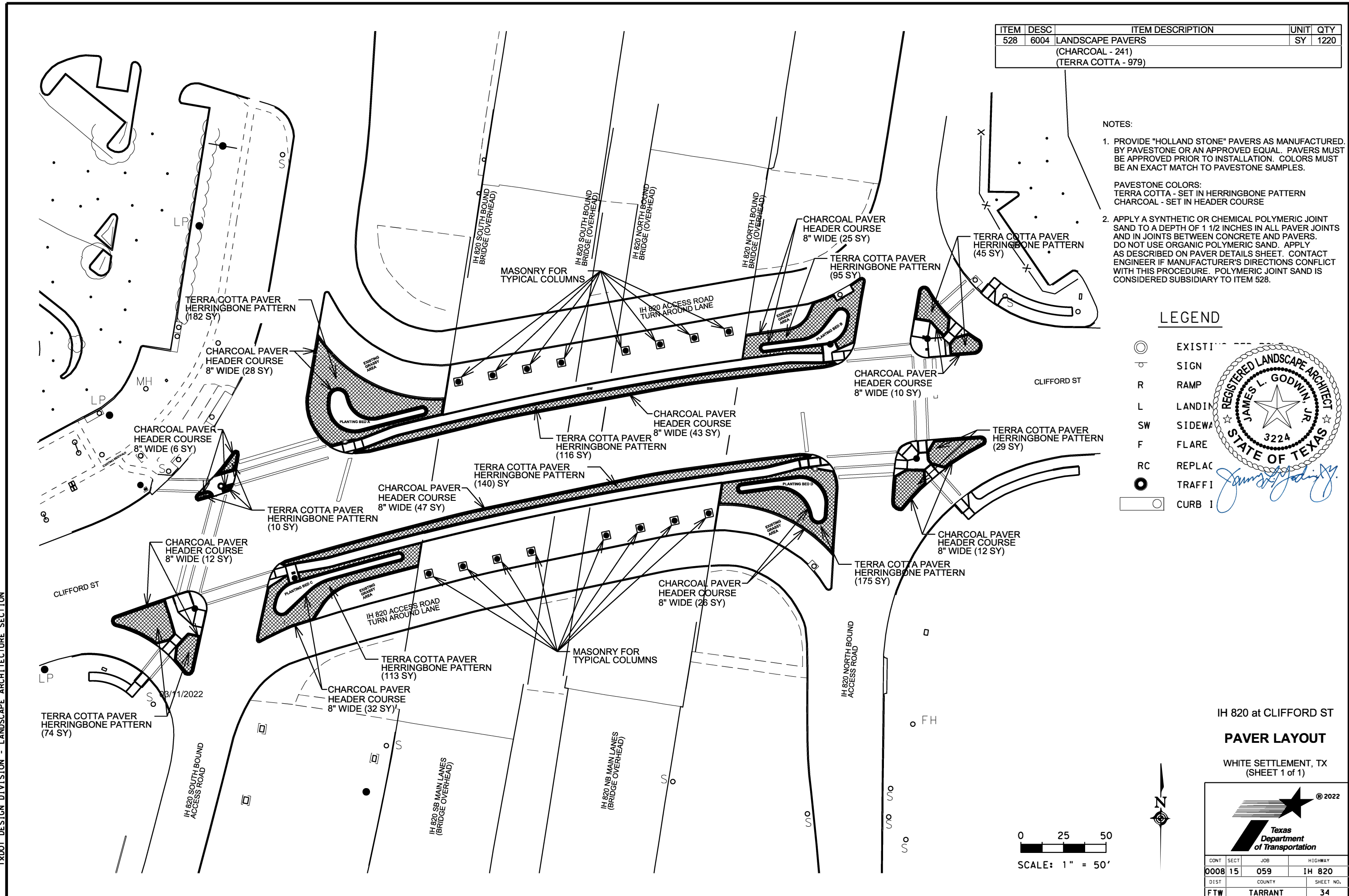
- NOTES:
- PROVIDE "HOLLAND STONE" PAVERS AS MANUFACTURED BY PAVESTONE OR AN APPROVED EQUAL. PAVERS MUST BE APPROVED PRIOR TO INSTALLATION. COLORS MUST BE AN EXACT MATCH TO PAVESTONE SAMPLES.

PAVESTONE COLORS:
TERRA COTTA - SET IN HERRINGBONE PATTERN
CHARCOAL - SET IN HEADER COURSE
 - APPLY A SYNTHETIC OR CHEMICAL POLYMERIC JOINT SAND TO A DEPTH OF 1 1/2 INCHES IN ALL PAVER JOINTS AND IN JOINTS BETWEEN CONCRETE AND PAVERS. DO NOT USE ORGANIC POLYMERIC SAND. APPLY AS DESCRIBED ON PAVER DETAILS SHEET. CONTACT ENGINEER IF MANUFACTURER'S DIRECTIONS CONFLICT WITH THIS PROCEDURE. POLYMERIC JOINT SAND IS CONSIDERED SUBSIDIARY TO ITEM 528.

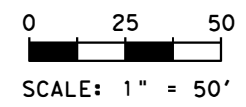
LEGEND

	EXISTING
	SIGN
	RAMP
	LANDING
	SIDEWALK
	FLARE
	REPLACEMENT
	TRAFFIC
	CURB

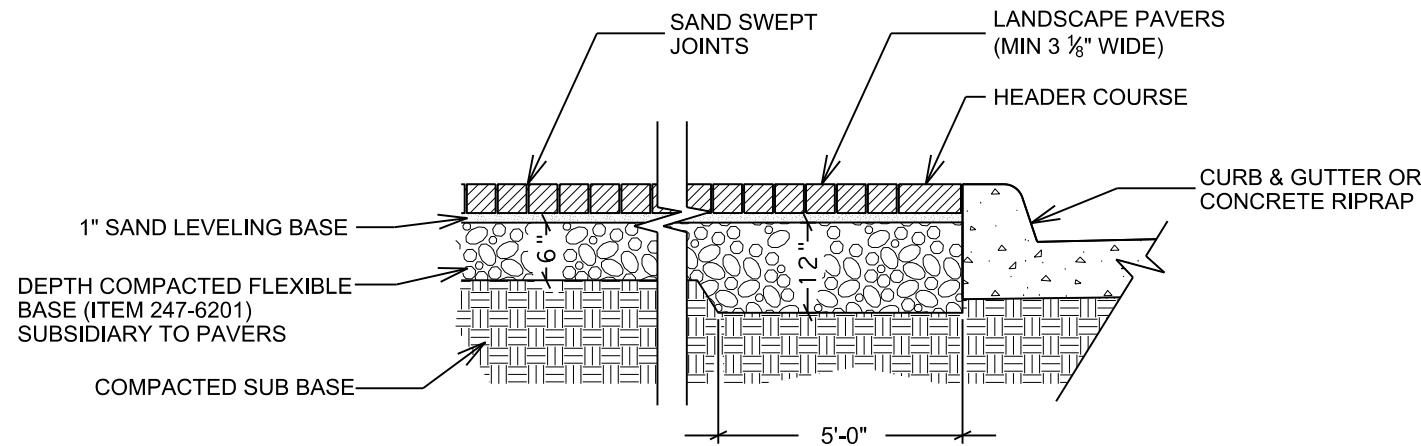
TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION



IH 820 at CLIFFORD ST
PAVER LAYOUT
 WHITE SETTLEMENT, TX
 (SHEET 1 of 1)



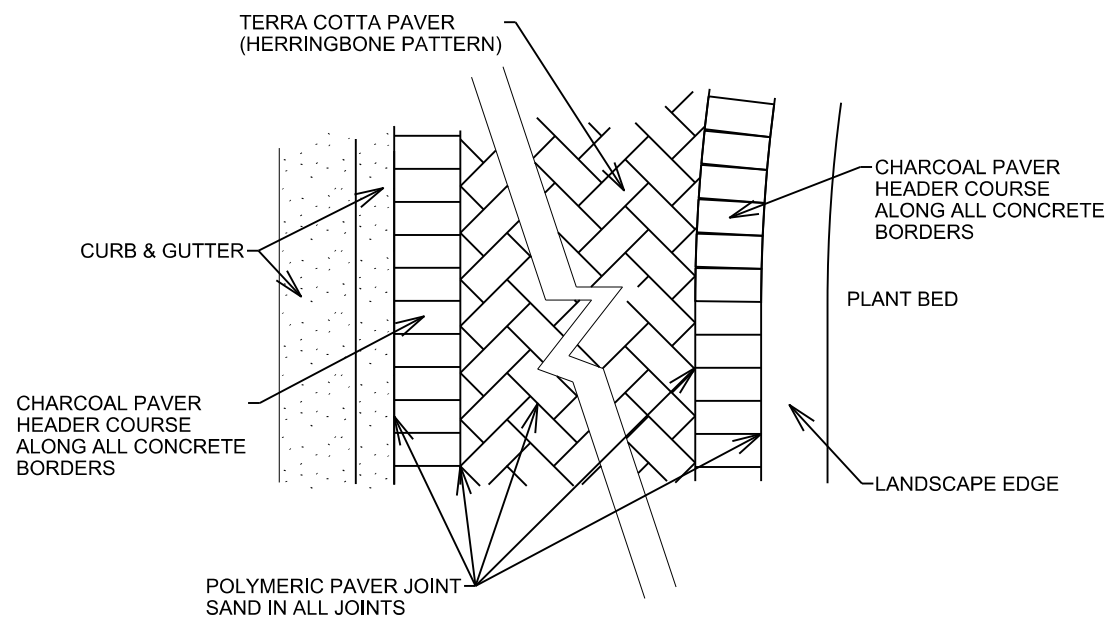
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	34	



1 LANDSCAPE PAVERS

GRADE TO DRAIN (SEE NOTES)

NTS



2 LANDSCAPE PAVERS

NTS

NOTES:

1. APPLY A SYNTHETIC OR CHEMICAL POLYMERIC JOINT SAND TO A DEPTH OF 1 1/2 INCHES IN ALL PAVER JOINTS AND IN JOINTS BETWEEN CONCRETE AND PAVERS. DO NOT USE ORGANIC POLYMERIC SAND. APPLY AS DESCRIBED BELOW. CONTACT ENGINEER IF MANUFACTURER'S DIRECTIONS CONFLICT WITH THIS PROCEDURE. POLYMERIC JOINT SAND IS CONSIDERED SUBSIDIARY TO ITEM 528.

POLYMERIC JOINT SAND APPLICATION:

1. APPLY A POLYMERIC JOINT SAND TO ALL PAVER JOINTS. USE ONLY A SYNTHETIC CHEMICAL POLYMER. ORGANIC POLYMERS WILL NOT BE ALLOWED. DO NOT APPLY IN FREEZING WEATHER. USE A TAN COLORED POLYMERIC JOINT SAND.
2. SWEEP POLYMERIC PAVER JOINT SAND INTO ALL JOINTS AFTER PAVERS HAVE BEEN LAID AND COMPACTED. REPEAT COMPACTION AND SWEEP MORE POLYMERIC SAND INTO JOINTS TO A DEPTH OF 1 1/2 INCHES. SWEEP SAND A THIRD TIME TO TOP OFF ALL JOINTS.
3. CLEAN OFF REMAINING SAND FROM PAVER SURFACES USING BROOM OR BLOWER, ONLY. ENSURE THAT ALL POLYMERIC SAND HAS BEEN REMOVED FROM SURFACES BEFORE WETTING.
4. STARTING AT THE HIGHEST POINT OF THE PAVER SURFACE, GENTLY SPRAY A WATER MIST OVER THE PAVED AREA. WORK TOWARDS THE LOWEST AREA. LIGHTLY SOAK PAVERS TO REMOVE REMAINING SAND DUST, BUT DO NOT REMOVE SAND PARTICLES FROM JOINTS.
5. ALLOW POLYMERIC SAND TO CURE FOR 10 TO 15 MINUTES. MIST AREA AGAIN WITH A LIGHT SPRAY OF WATER.
6. ENSURE THAT POLYMER SAND DOES NOT ADHERE TO TOP OF PAVERS. ALLOW FOR 24 HOURS FOR POLYMER TO ACTIVATE AND HARDEN. PROVIDE BARRIERS AND FLAGGING AS DIRECTED TO PREVENT PEDESTRIANS FROM CROSSING OVER AREA UNTIL POLYMERIC SAND HARDENS.

PAVING - SCHEDULE OF MATERIALS AND FINISHES

ITEM	DESCRIPTION	SPECIFICATION NOTES
0528-6004	LANDSCAPE PAVERS	8" HEADER COURSE, PAVESTONE, HOLLANDSTONE, COLOR: CHARCOAL OR APPROVED EQUAL
	LANDSCAPE PAVERS	HERRINGBONE PATTERN, PAVESTONE, HOLLANDSTONE, COLOR: TERRA COTTA OR EQUAL

NOTE: PROVIDE MOCK UP OF PAVING FOR APPROVAL BY DISTRICT ENGINEER

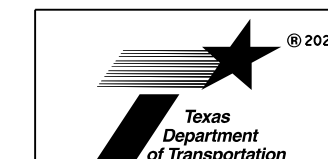


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
IH 820 at CLIFFORD ST

PAVER DETAILS

WHITE SETTLEMENT, TX
(SHEET 1 of 1)



CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	35	

PLANT KEY		
	SOTOL	5 GAL
	SPINELESS PRICKLY PEAR	5 GAL
	SKELETONLEAF GOLDENEYE	5 GAL
	RED YUCCA	5 GAL
	PINK MUHLY GRASS	5 GAL
	PERENNIALS	1 GAL

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
110	6003	EXCAVATION (SPECIAL)	CY	133.65
192	6002	PLANT MATERIAL (1-GAL)	EA	227
PERENNIALS INCLUDE: (SALVIA GREGGII - 113) (BLACKEYED SUSAN - 114)				
192	6004	PLANT MATERIAL (5-GAL)	EA	138
(SOTOL - 7) (SPINELESS PRICKLY PEAR - 9) (SKELETONLEAF GOLDENEYE - 16) (RED YUCCA - 82) (PINK MUHLY GRASS - 24)				
192	6014	PLANT SOIL MIX	CY	88.2
192	6017	VEGETATION BARRIER	SY	178
1005	6001	LOOSE AGGR FOR GROUNDCOVER (TYPE I)	CY	15
1005	6002	LOOSE AGGR FOR GROUNDCOVER (TYPE II)	CY	29.8

NOTES:

- EXCAVATE BED AREAS TO A DEPTH OF 2'-3". MARK UTILITIES AND TRAFFIC SIGNAL CONDUITS BEFORE DIGGING. HAND DIG AROUND UTILITIES AND CONDUITS.
- PLANT SPACING (APPROXIMATE):
PERENNIALS - 12" O.C.
RED YUCCA - 36" O.C.
SKELETONLEAF GOLDENEYE - 36" O.C.
PINK MUHLY GRASS - 30" O.C.
SOTOL - 48" O.C.
SPINELESS PRICKLY PEAR - 36" O.C.
- PROVIDE A PLANT SOIL MIX OF 1 PART CLEAN BROWN LOAM, 1 PART GRANITE SAND, AND 1 PART COMPOST. SLIGHTLY MOUND THE GRADE OF THE BED AREAS TO ACCOMMODATE FUTURE SETTLING. QUANTITIES SHOWN ARE CALCULATED AS VOLUME MEASUREMENTS ONLY, AND DO NOT ACCOUNT FOR MOUNDING OR SETTLING. PROVIDE ADDITIONAL MATERIAL AS NEEDED TO FILL THE LEVELS SHOWN IN THE PLANS.
- QUANTITIES SHOWN FOR VEGETATION BARRIER DO NOT ACCOUNT FOR OVERLAPS.
- LANDSCAPE AMENITY (TY 1, 2, 3): PROVIDE NATURAL SHAPED LIMESTONE BOULDERS AS SHOWN. BOULDERS ARE PAID FOR AS LANDSCAPE AMENITY TYPE 1, 2, AND 3, ACCORDING TO THE SIZED SHOWN ON THE BOULDER SIZE CHART. BURY BOTTOM OF BOULDER INTO GRADE AS NEEDED TO ATTAIN MAXIMUM AND MINIMUM HEIGHTS, AS SHOWN ON THE CHART.
- LOOSE AGGREGATE (TY 1): PROVIDE 5/8" SCREENED GRANITE GRAVEL AS DRAINAGE GRAVEL BELOW SOIL MIX. USE A FILTER FABRIC BETWEEN PLANT SOIL MIX AND DRAINAGE GRAVEL. FILTER FABRIC IS CONSIDERED SUBSIDIARY TO ITEM 192.

(REFER TO PLANTING DETAIL)



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03/11/2022

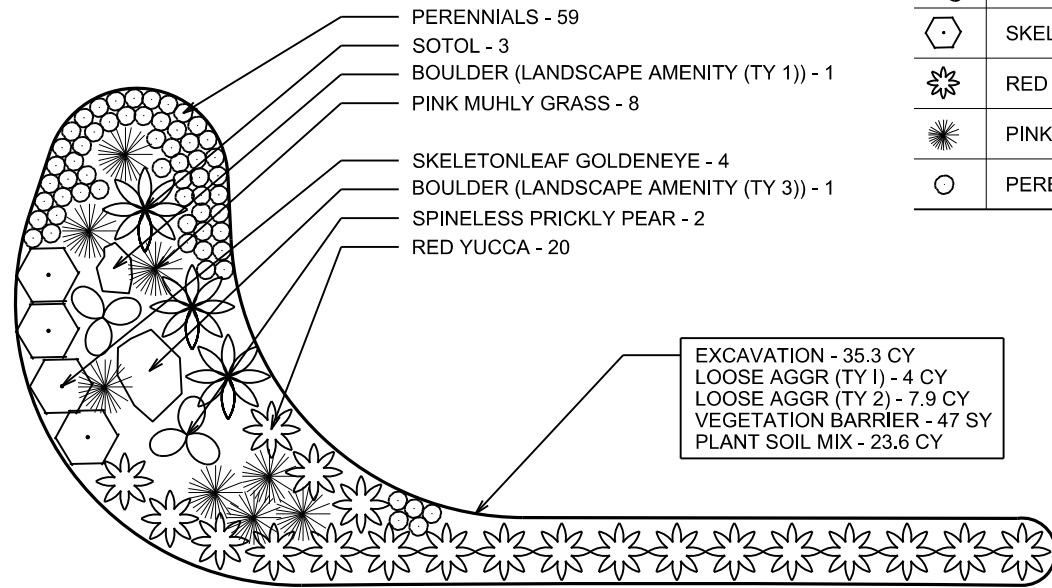
IH 820 at CLIFFORD ST

PLANTING PLAN

WHITE SETTLEMENT, TX
(SHEET 1 of 1)



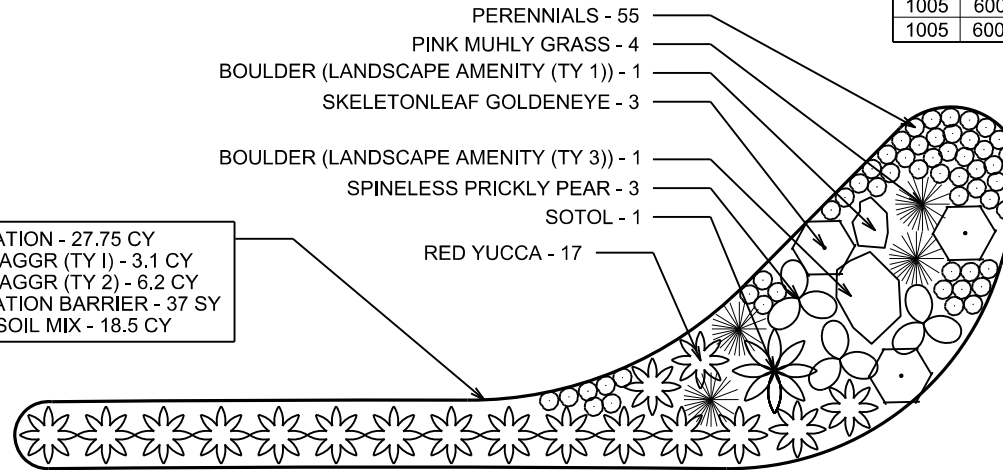
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	36	



EXCAVATION - 35.3 CY
LOOSE AGGR (TY 1) - 4 CY
LOOSE AGGR (TY 2) - 7.9 CY
VEGETATION BARRIER - 47 SY
PLANT SOIL MIX - 23.6 CY

PLANT BED A

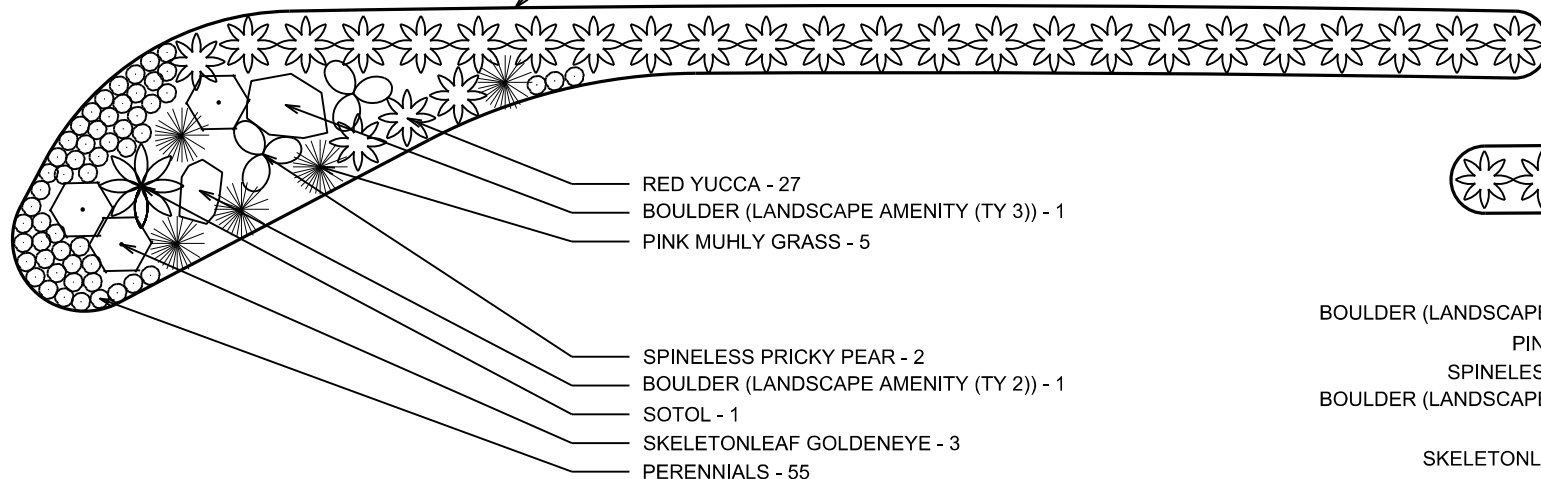
EXCAVATION - 27.75 CY
LOOSE AGGR (TY 1) - 3.1 CY
LOOSE AGGR (TY 2) - 6.2 CY
VEGETATION BARRIER - 37 SY
PLANT SOIL MIX - 18.5 CY



PLANT BED B

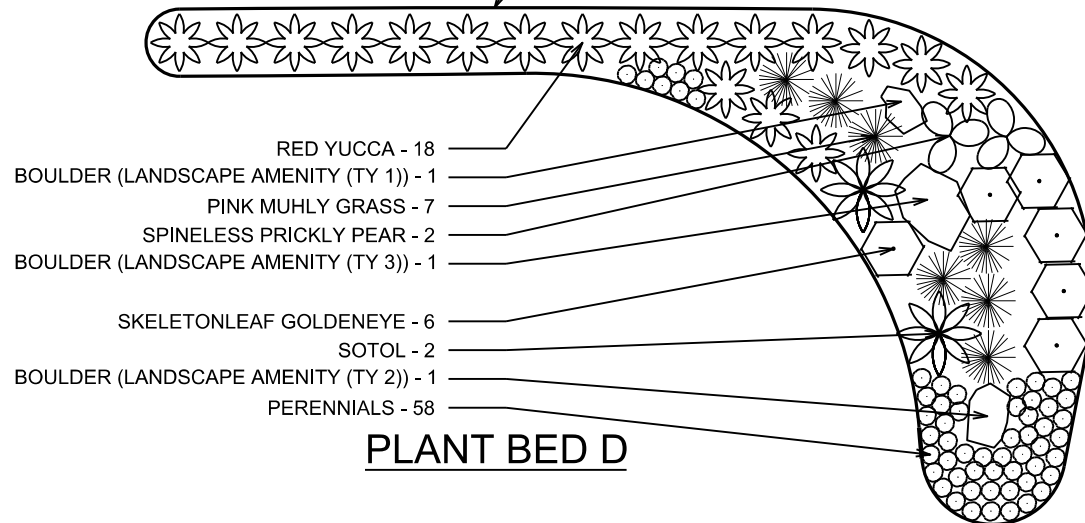
BOULDER SIZE CHART		
PROVIDE NATURAL SHAPED, LIMESTONE BOULDERS. HEIGHT ABOVE GRADE: 12" MINIMUM, 24" MAXIMUM.		
BOULDER DESCRIPTION	LENGTH	WIDTH
LANDSCAPE AMENITY (TY 1)	1.5' to 2'	2.5' to 3'
LANDSCAPE AMENITY (TY 2)	2.5' to 3'	3' to 3.5'
LANDSCAPE AMENITY (TY 3)	3' to 3.5'	4' to 4.5'

EXCAVATION - 36.8 CY
LOOSE AGGR (TY 1) - 4.1 CY
LOOSE AGGR (TY 2) - 8.2 CY
VEGETATION BARRIER - 49 SY
PLANT SOIL MIX - 23.6 CY



PLANT BED C

EXCAVATION - 33.8 CY
LOOSE AGGR (TY 1) - 3.8 CY
LOOSE AGGR (TY 2) - 7.5 CY
VEGETATION BARRIER - 45 SY
PLANT SOIL MIX - 22.5 CY



PLANT BED D



SCALE: 1" = 10'

PLANT SPECIFICATIONS

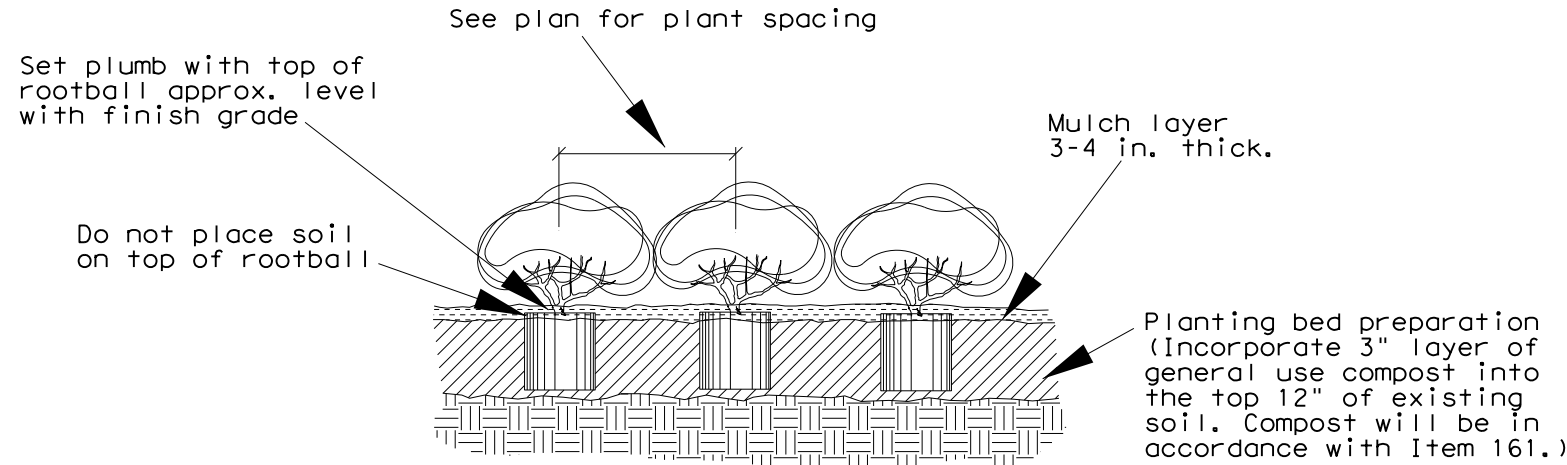
Botanical Name	Common Name	Color	Quantity	Size
Salvia greggii	Autumn Sage	Red	113	1 gal
Gloriosa Daisy	Black-eyed Susan	Yellow	114	
Dasyliirion texanum	Texas Sotol	White	7	5 gal
Opuntia cacanapa 'Ellisiana'	Spineless Prickly Pear	Yellow	9	
Viguiera stenoloba	Golden Bush Daisy	Yellow	16	
Hesperaloe parviflora	Red Yucca	Red	82	
Muhlenbergia capillaris	Pink Muhly Grass		24	

Note: Color refers to flower color.

VEGETATIVE WATERING SCHEDULE FOR:

PHASE	ITEM DESCRIPTION	FREQUENCY	RATE
Construction/installation operations, Item 192.3.7	SHRUBS	JANUARY	Same day as planting and 2 times per week with 2 days minimum between waterings
90-day Establishment period, Item 192.3.15		through DECEMBER	
Construction/installation operations, Item 192.3.7	TREES	JANUARY	Same day as planting and 2 times per week with 2 days minimum between waterings
90-day Establishment period, Item 192.3.15		through DECEMBER	

NOTES:
 Provide water necessary to meet the quality and schedule shown above. Water required is subsidiary to Item 192 and will not be paid for separately.
 Rate and frequency may be adjusted to meet site conditions and weather as approved or directed by engineer.
 Refer to Item 168.2 for water quality information.
 At the time of installation all plants are to be watered manually the same day as planting at a rate and frequency shown above.
 Stressed plant material will be rejected according to Item 192.2.2 and replaced.

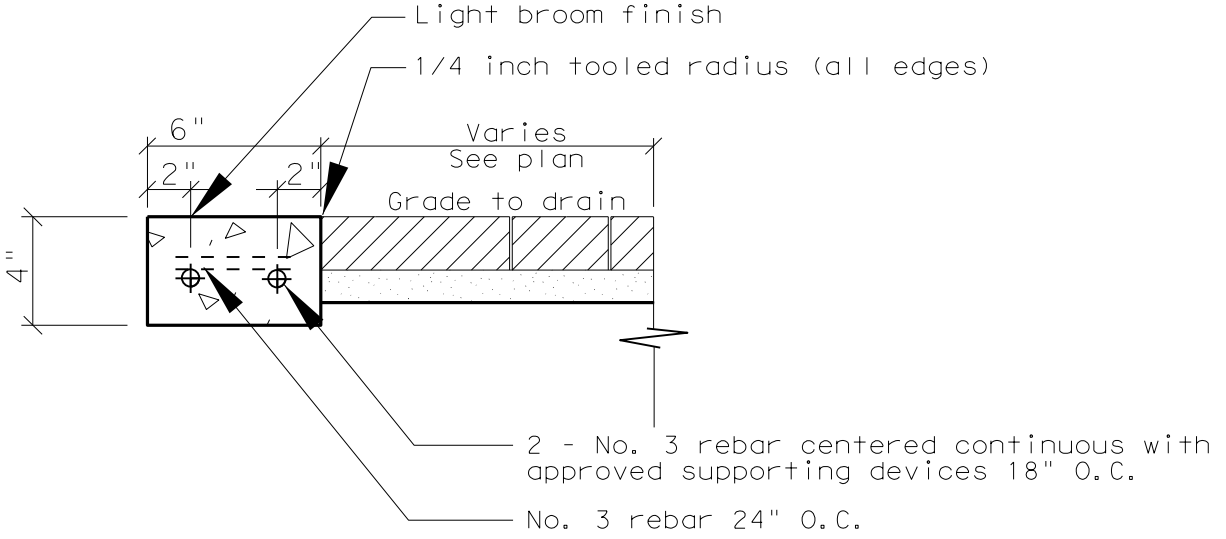


SHRUB PLANTING IN MASS BEDS

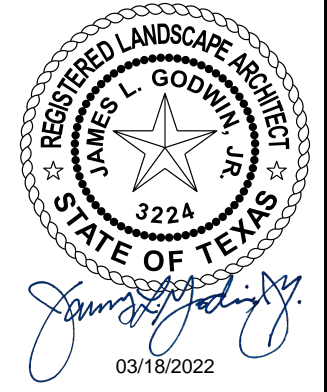
NTS

GENERAL NOTES:

1. REFERENCE ITEM 192 OF THE TEXAS STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MAINTENANCE OF HIGHWAYS, STREETS AND BRIDGES 2014 FOR SPECIFICATIONS, DIMENSIONS, VOLUMES AND MEASUREMENTS THAT HAVE BEEN MODIFIED OR NOT SHOWN.
2. PROVIDE PLANTS NURSERY-GROWN IN CONTAINERS.
3. REJECTION OF PLANTS IN ACCORDANCE WITH ITEM 192.2.2.
4. STAKE LOCATION OF TREES IN THE FIELD IN ACCORDANCE WITH ITEM 192.3.3.
5. PROVIDE FOR THE SAFE TRANSPORTATION OF PLANTS TO THE PROJECT SITE AND THE CONDITION OF PLANTS UPON ARRIVAL.
6. DO NOT STORE PLANT MATERIAL ON HARD SURFACES OR LEAVE EXPOSED TO THE SUN.
7. PROTECT THE PLANT ROOT BALLS AND WATER REGULARLY UNTIL PLANTING.
8. IF PLANTS ARE LEFT IN STORAGE OVER THE WEEKEND OR HOLIDAY, PROVIDE A MEANS OF PERIODICALLY WATERING AND INSPECTION OF CONTAINER MOISTURE.
9. PROVIDE PLANTS THAT ARE HARDY, SYMMETRICAL, TIGHT KNIT, AND SO TRAINED OR FAVORED IN DEVELOPMENT AND APPEARANCE AS TO BE SUPERIOR IN FORM, NUMBER OF BRANCHES, AND COMPACTNESS. PLANTS SHALL BE SOUND, HEALTHY AND VIGOROUS, WELL BRANCHED, DENSELY FOLIATED WHEN IN LEAF, AND SHALL HAVE HEALTHY, WELL DEVELOPED ROOT SYSTEMS.
10. ALL TREE PLANTINGS ARE TO BE MULCHED AFTER PLANTING TO THE DEPTH INDICATED IN THE DETAILS. PROVIDE SHREDDED HARDWOOD MULCH WITH A MINIMUM 3/8 " (NOT OVER 25% BY VOLUME) OF FINE PARTICLES AND DUST. PROVIDE MULCH FREE OF ANY PLASTIC, GLASS, METALS AND OTHER CONTAMINANTS (STICKS, STONES, CLAY, OR OTHER FOREIGN MATTER).



LANDSCAPE EDGE (CL B RIPRAP)



IH 820 at CLIFFORD ST
PLANTING AND ESTABLISHMENT
 WHITE SETTLEMENT, TX
 (SHEET 1 of 1)

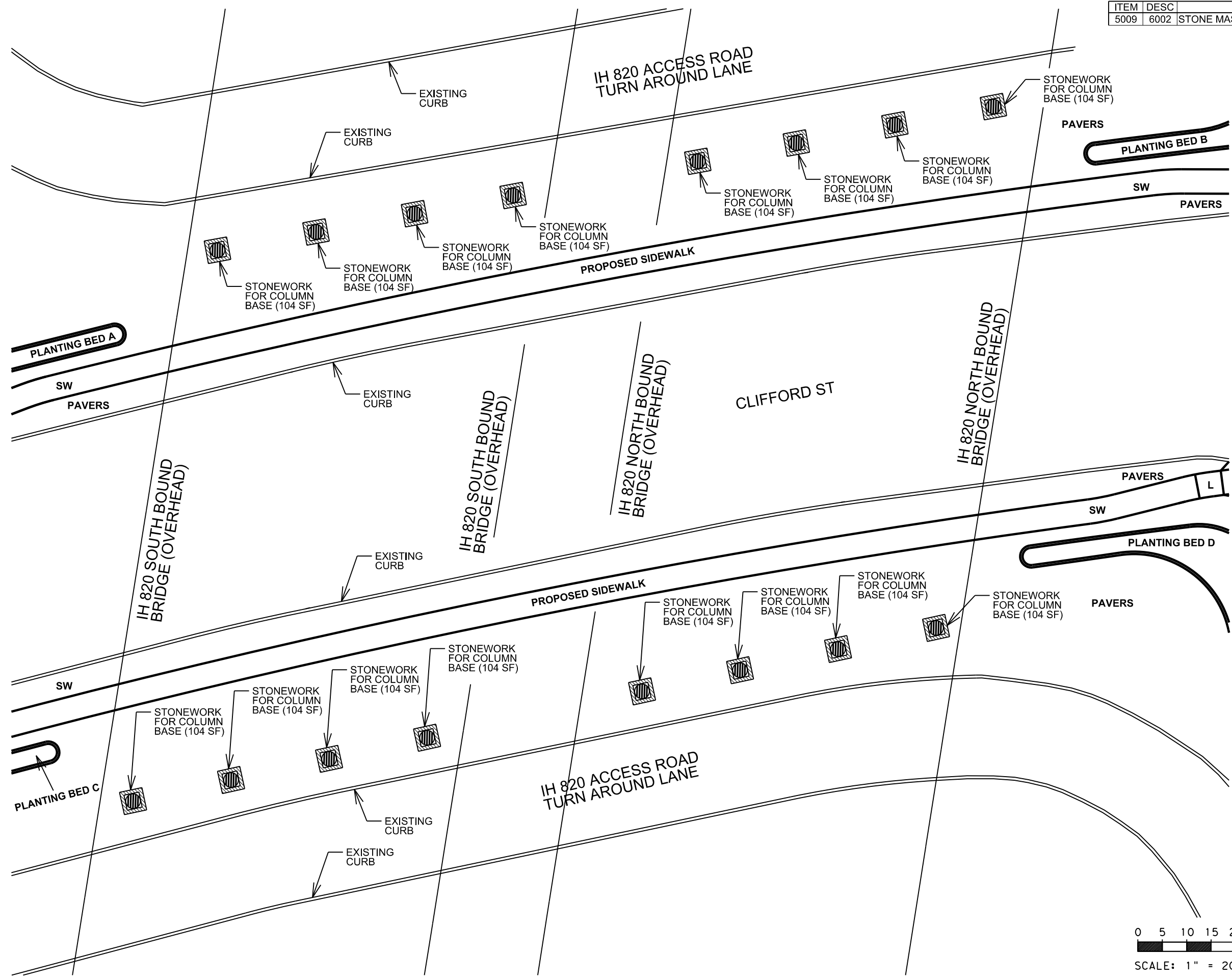
CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY		SHEET NO.
FTW	TARRANT		37

TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION

ITEM	DESC	ITEM DESCRIPTION	UNIT	QTY
5009	6002	STONE MASONRY (ROCK WALL)	SF	1664

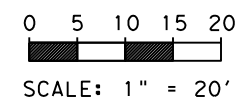
- NOTES:
- REFER TO THE STONE MASONRY DETAILS SHEETS FOR CONSTRUCTION INFORMATION AND DETAILS.
 - CONCRETE STONE, MASONRY LEDGES, REINFORCED STEEL, MASONRY TIES, EXPANSION BOLTS, MORTAR, GROUT, AND ALL MASONRY MATERIALS AND LABOR ARE CONSIDERED SUBSIDIARY TO THIS ITEM.

TxDOT DESIGN DIVISION - LANDSCAPE ARCHITECTURE SECTION

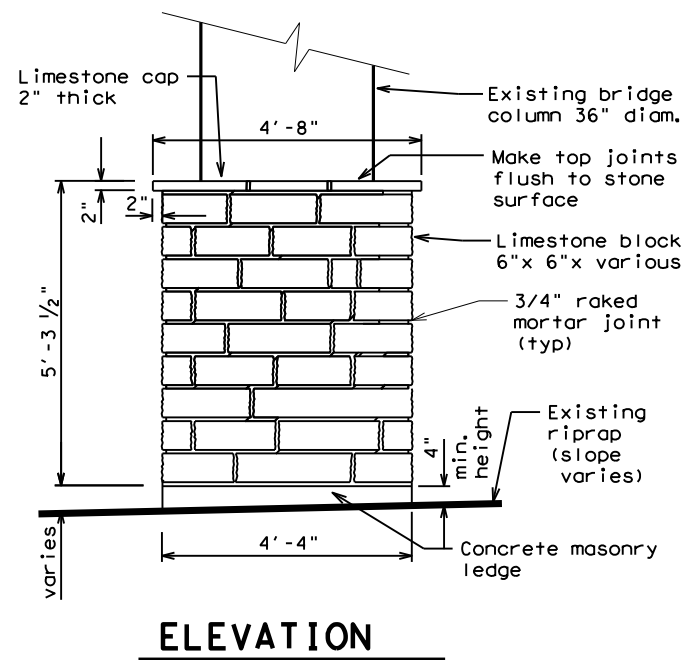
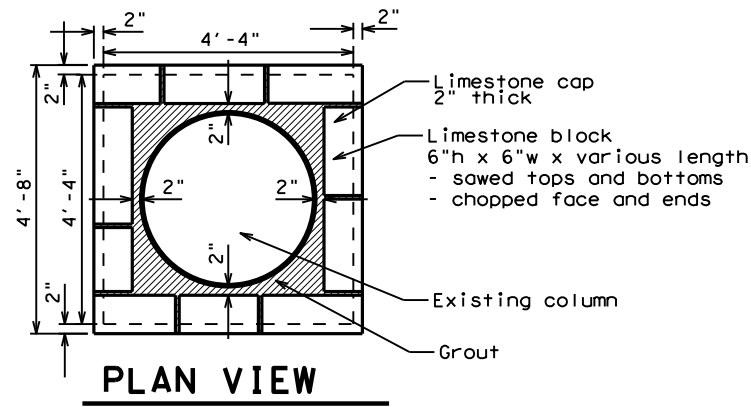


James L. Godwin, Jr.
03/11/2022

IH 820 at CLIFFORD ST
STONE MASONRY LAYOUT
LOCATION E
(UNDER BRIDGES)
WHITE SETTLEMENT, TX
(SHEET 1 of 1)



CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY	SHEET NO.	
FTW	TARRANT	38	



MASONRY - FOR TYPICAL COLUMN

NTS

NOTES:

1. Provide mortared limestone block for masonry. Limestone block shall be 6" x 6" x various lengths, with chopped (rough) sides and sawed (smooth) tops and bottoms.
2. Provide 3/4" bed joints on face of masonry wall. Use type N mortar. Rake wall joints 1/2" deep. Joints on top surface of cap stones shall be flush to top surface of the stones.
3. Wall ties shall be 22 gage, galv. corrugated steel, secured with Hilti expansion anchor bolt KB3 1/4" x 3 1/4" SS 304, or approved equal.* Place wall ties at every second horizontal bed joint, in the center of the wall face, as shown.
4. Masonry ledge shall be class B concrete.
5. Adjust height of masonry to ensure that the top of all masonry for bridge columns is in horizontal alignment.
5. Place black poly sheeting (6 mil) as a bond breaker at bottom corners of masonry. Place sheeting between masonry ledge and the bottom mortar bed, extending 12" from the corners.
6. Refer to following sheets for placement, dimensions, and sizes of reinforcing steel.
7. Stone, mortar, grout, reinforcing steel, ties, and concrete masonry ledge, are considered subsidiary to Stone Masonry, and will not be paid for separately.

* Approved equal masonry securing devices or systems must be submitted for approval prior to installation.

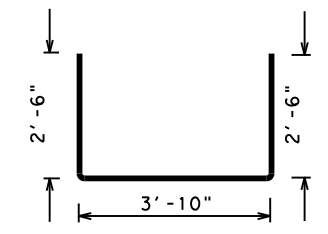


IH 820 at CLIFFORD ST
**STONE MASONRY
 DETAILS**
 WHITE SETTLEMENT, TX
 (SHEET 1 of 2)

CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY		SHEET NO.
FTW	TARRANT		39

TABLE OF ESTIMATED QUANTITIES

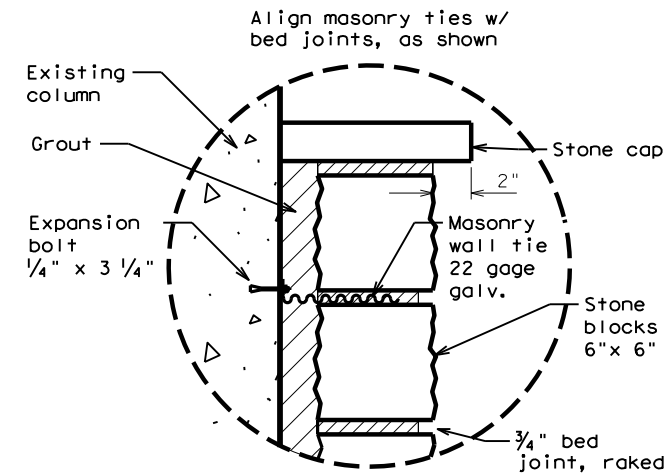
BAR	QTY.	SIZE	LENGTH	WEIGHT
A	8	#3	8'-10"	28
D1	8	#3	0'-6"	2
Reinforcing steel			Lbs.	59



BARS A

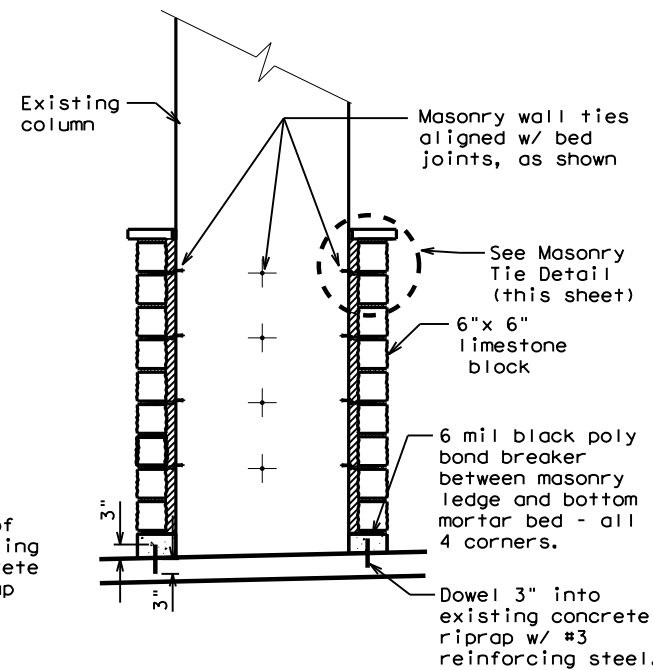
Notes:

1. Quantities shown are for the stone masonry for the full height of a typical column base.
2. Reinforcing steel shall be grade 60.
3. Reinforcing steel is considered subsidiary to stone masonry and will not be paid for separately.

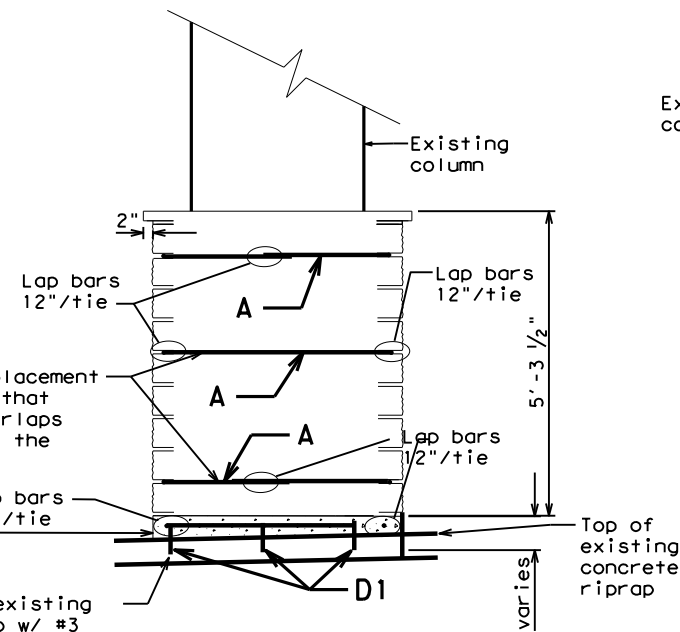


MASONRY TIE DETAIL

NTS

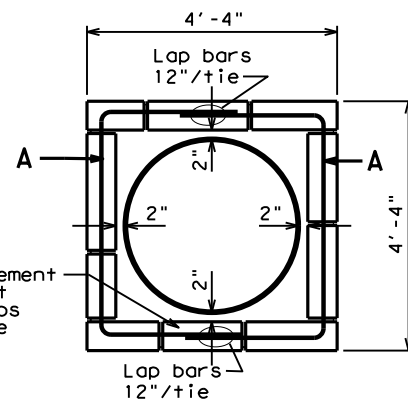


SECTION

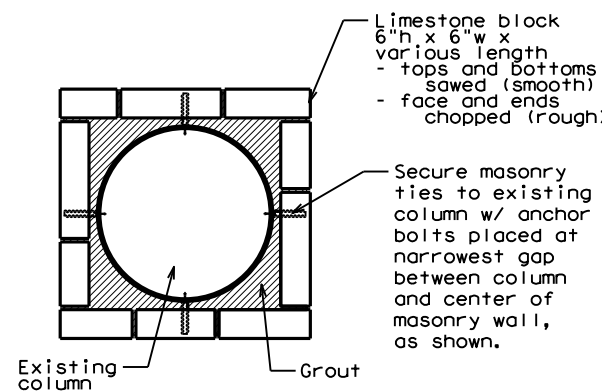


SECTION

Dowel 3" into existing concrete riprap w/ #3 reinforcing steel. One dowel at each corner and one dowel at center of each wall face.



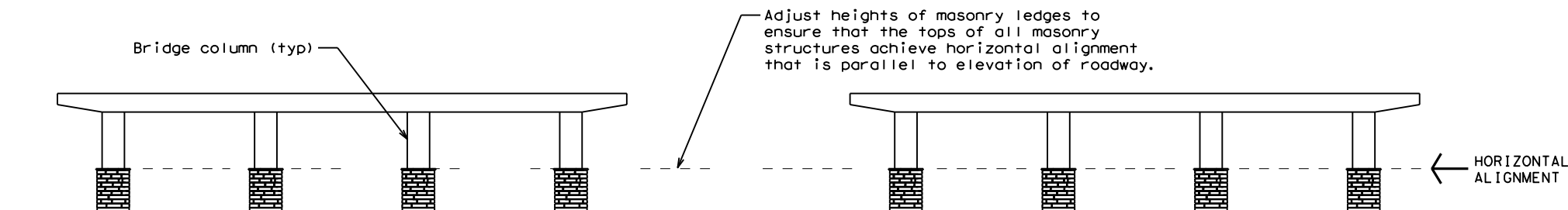
PLAN VIEW



PLAN VIEW

REINFORCING STEEL & MASONRY TIES for COLUMN BASE

NTS



HORIZONTAL ALIGNMENT OF MASONRY

NTS



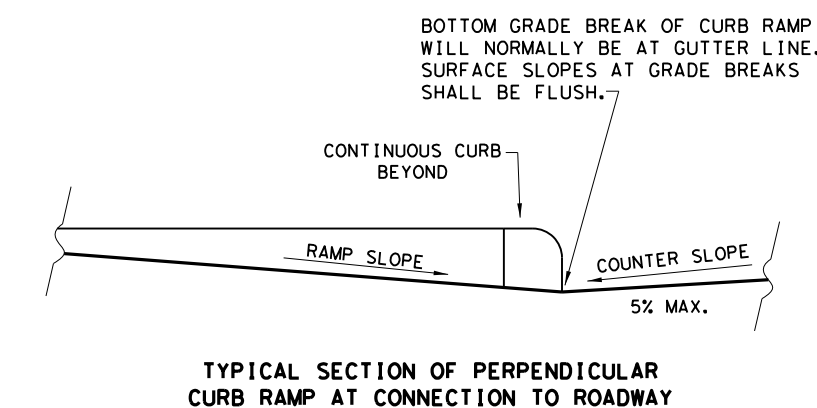
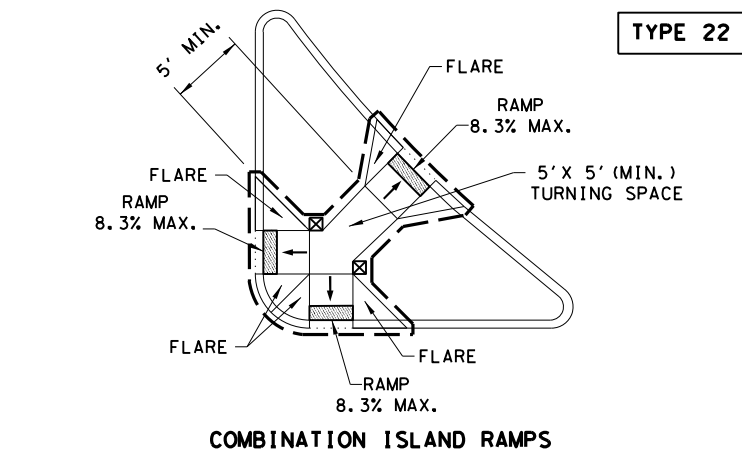
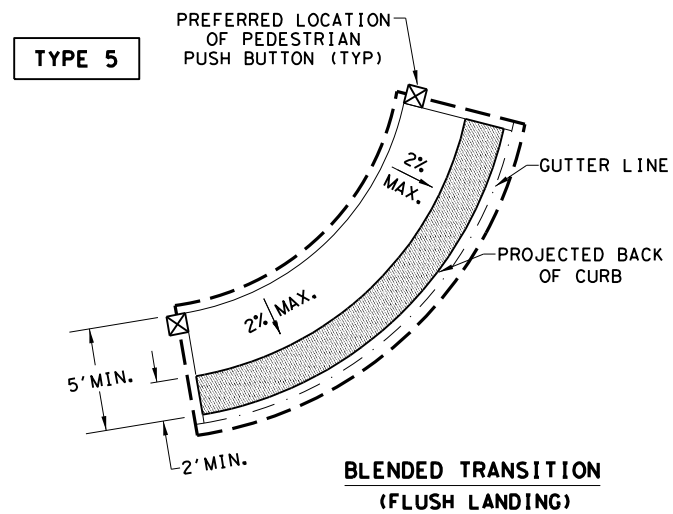
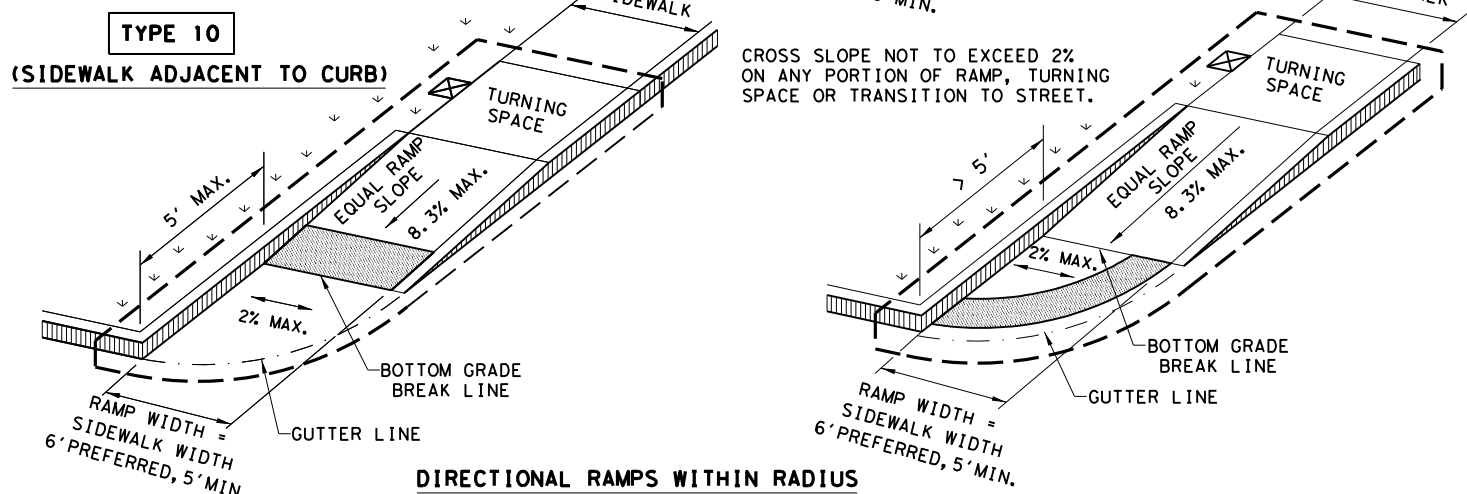
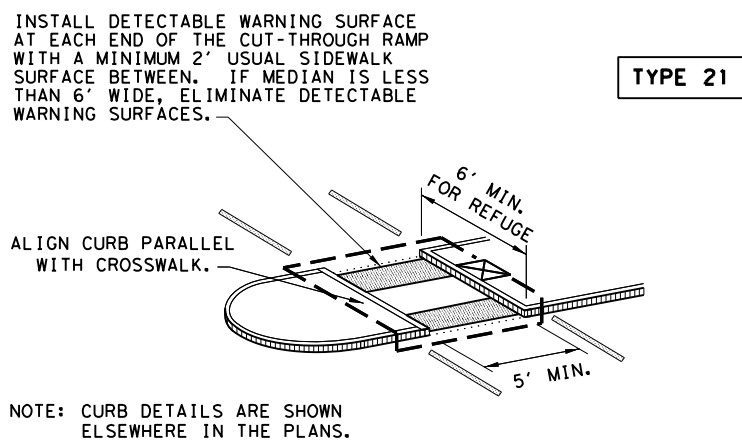
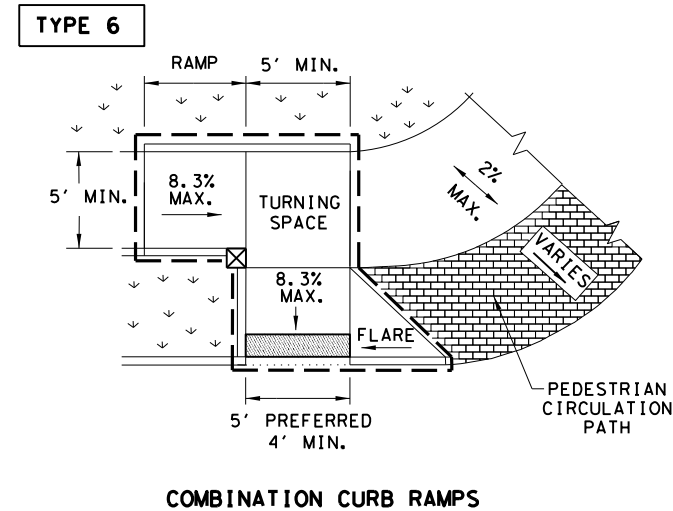
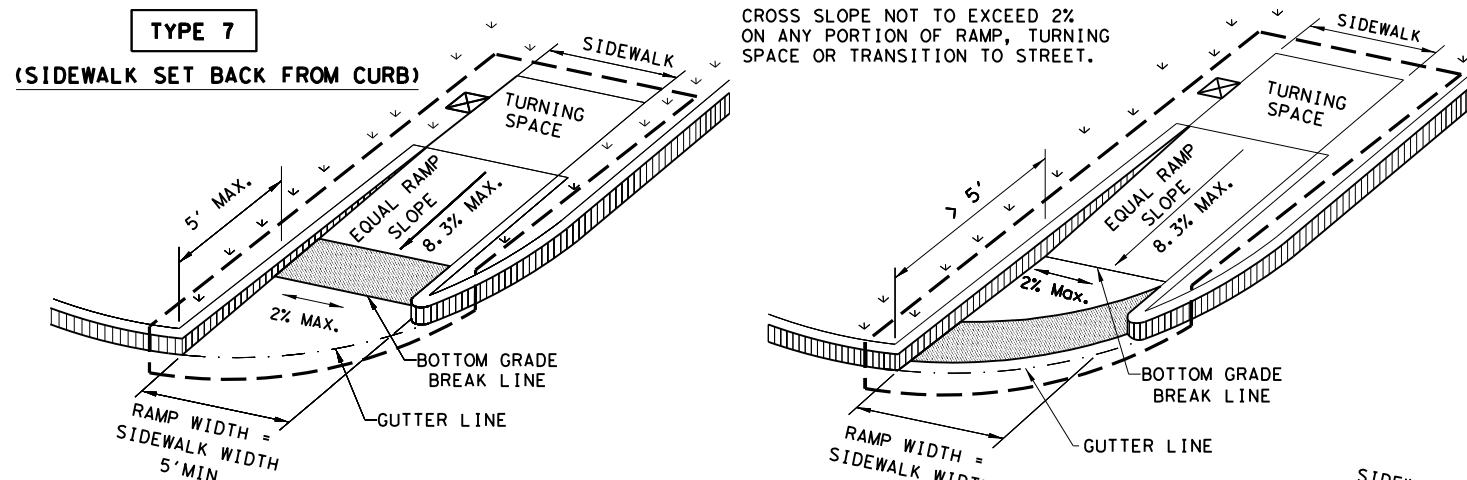
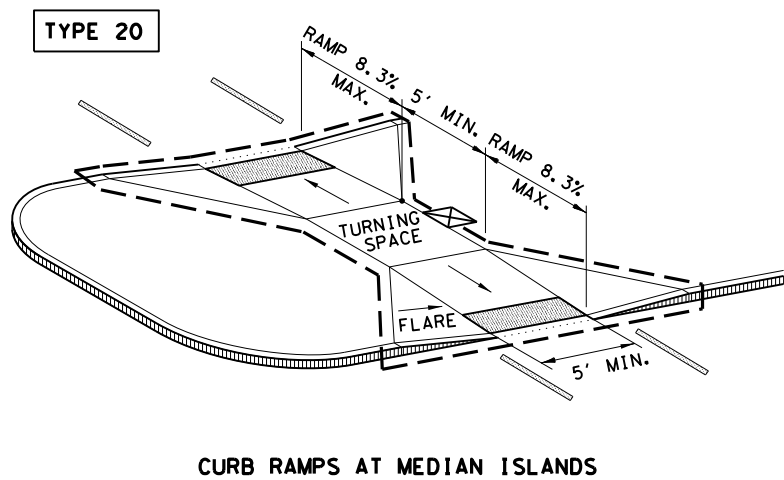
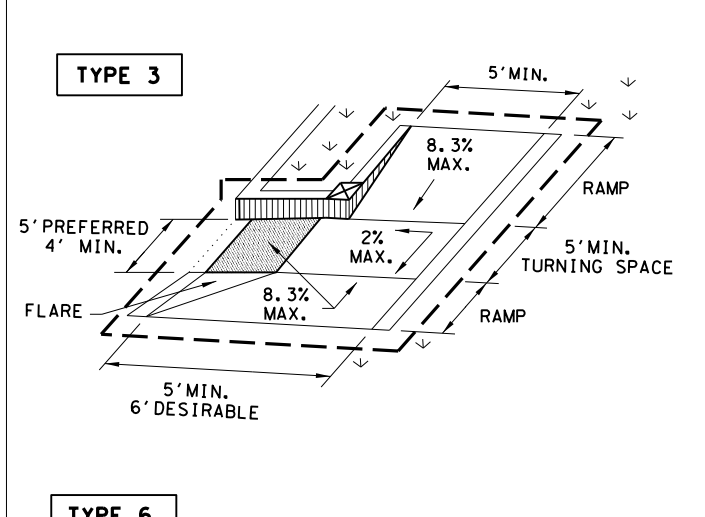
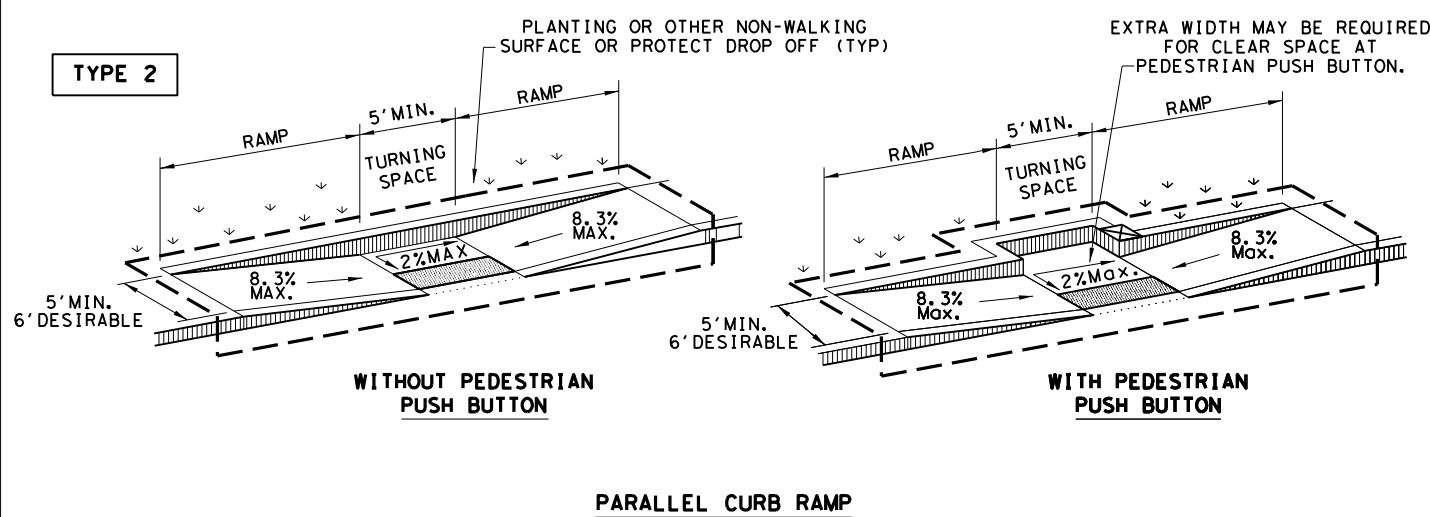
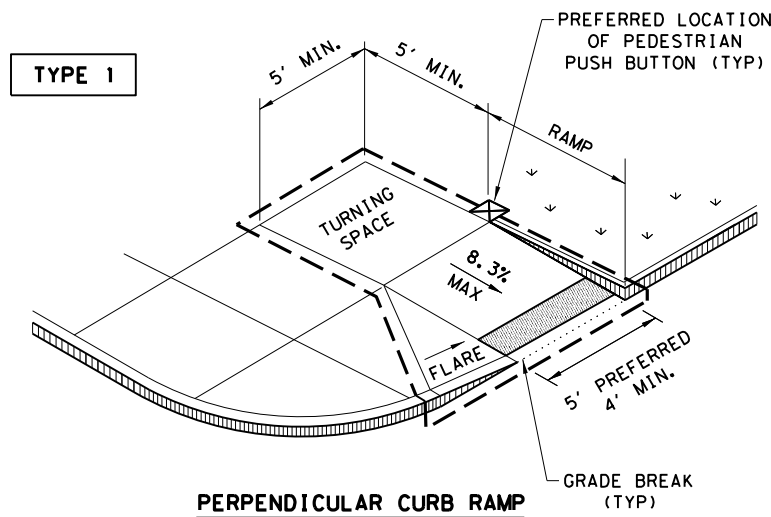
03/11/2022

IH 820 at CLIFFORD ST
STONE MASONRY DETAILS
 WHITE SETTLEMENT, TX
 (SHEET 2 of 2)

CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY		SHEET NO.
FTW	TARRANT		40

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DATE:
FILE:



NOTES / LEGEND:

SEE GENERAL NOTES ON SHEET 2 OF 4 FOR MORE INFORMATION.

DENOTES PLANTING OR NON-WALKING SURFACE NOT PART OF PEDESTRIAN CIRCULATION PATH.

DENOTES PREFERRED LOCATION OF PEDESTRIAN PUSH BUTTON IF APPLICABLE.

DETECTABLE WARNING SURFACE

GUTTER LINE

GRADE BREAK

RAMP LIMITS OF PAYMENT

SHEET 1 OF 4

Texas Department of Transportation
Design Division Standard

PEDESTRIAN FACILITIES CURB RAMPS

PED-18

FILE: ped18	DN: TxDOT	DW: VP	CK: KM	CK: PK & JG
© TxDOT: MARCH, 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
REVISED 08, 2005	DIST	COUNTY	SHEET NO.	
REVISED 06, 2012	FTW	TARRANT	41	
REVISED 01, 2018				

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DATE: FILE:

GENERAL NOTES

CURB RAMP

1. Install a curb ramp or blended transition at each pedestrian street crossing.
2. All slopes shown are maximum allowable. Cross slopes of 1.5% and lesser running should be used. Adjust curb ramp length or grade of approach sidewalks as directed.
3. Maximum allowable cross slope on sidewalk and curb ramp surfaces is 2%.
4. The minimum sidewalk width is 5'. Where the sidewalk is adjacent to the back of curb, a 6' sidewalk width is desirable. Where a 5' sidewalk cannot be provided due to site constraints, sidewalk width may be reduced to 4' for short distances. 5' x 5' passing areas at intervals not to exceed 200' are required.
5. Turning Spaces shall be 5' x 5' minimum. Cross slope shall be maximum 2%.
6. Clear space at the bottom of curb ramps shall be a minimum of 4' x 4' wholly contained within the crosswalk and wholly outside the parallel vehicular travel path.
7. Provide flared sides where the pedestrian circulation path crosses the curb ramp. Flared sides shall be sloped at 10% maximum, measured parallel to the curb. Returned curbs may be used only where pedestrians would not normally walk across the ramp, either because the adjacent surface is planted, substantially obstructed, or otherwise protected.
8. Additional information on curb ramp location, design, light reflective value and texture may be found in the latest draft of the Proposed Guidelines for Pedestrian Facilities in the Public Right of Way (PROWAG) as published by the U.S. Architectural and Transportation Barriers Compliance Board (Access Board).
9. To serve as a pedestrian refuge area, the median should be a minimum of 6' wide, measured from back of curbs. Medians should be designed to provide accessible passage over or through them.
10. Small channelization islands, which do not provide a minimum 5' x 5' landing at the top of curb ramps, shall be cut through level with the surface of the street.
11. Crosswalk dimensions, crosswalk markings and stop bar locations shall be as shown elsewhere in the plans. At intersections where crosswalk markings are not required, curb ramps shall align with theoretical crosswalks unless otherwise directed.
12. Provide curb ramps to connect the pedestrian access route at each pedestrian street crossing. Handrails are not required on curb ramps.
13. Curb ramps and landings shall be constructed and paid for in accordance with Item 531 "Sidewalks".
14. Place concrete at a minimum depth of 5" for ramps, flares and landings, unless otherwise directed.
15. Furnish and install No. 3 reinforcing steel bars at 18" o.c. both ways, unless otherwise directed.
16. Provide a smooth transition where the curb ramps connect to the street.
17. Curbs shown on sheet 1 within the limits of payment are considered part of the curb ramp for payment, whether it is concrete curb, gutter, or combined curb and gutter.
18. Existing features that comply with applicable standards may remain in place unless otherwise shown on the plans.

DETECTABLE WARNING MATERIAL

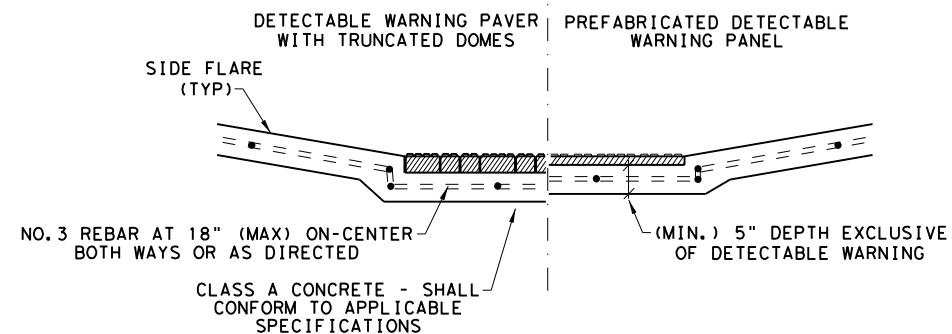
19. Curb ramps must contain a detectable warning surface that consists of raised truncated domes complying with PROWAG. The surface must contrast visually with adjoining surfaces, including side flares. Furnish and install an approved cast-in-place dark brown or dark red detectable warning surface material adjacent to uncolored concrete, unless specified elsewhere in the plans.
20. Detectable Warning Materials must meet TxDOT Departmental Materials Specification DMS 4350 and be listed on the Material Producer List. Install products in accordance with manufacturer's specifications.
21. Detectable warning surfaces must be firm, stable and slip resistant.
22. Detectable warning surfaces shall be a minimum of 24 inches in depth in the direction of pedestrian travel, and extend the full width of the curb ramp or landing where the pedestrian access route enters the street.
23. Detectable warning surfaces shall be located so that the edge nearest the curb line is at the back of curb and neither end of that edge is greater than 5 feet from the back of curb. Detectable warning surfaces may be curved along the corner radius.
24. Shaded areas on Sheet 1 of 4 indicate the approximate location for the detectable warning surface for each curb ramp type.

DETECTABLE WARNING PAVERS (IF USED)

25. Furnish detectable warning paver units meeting all requirements of ASTM C-936, C-33. Lay in a two by two unit basket weave pattern or as directed.
26. Lay full-size units first followed by closure units consisting of at least 25 percent (25%) of a full unit. Cut detectable warning paver units using a power saw.

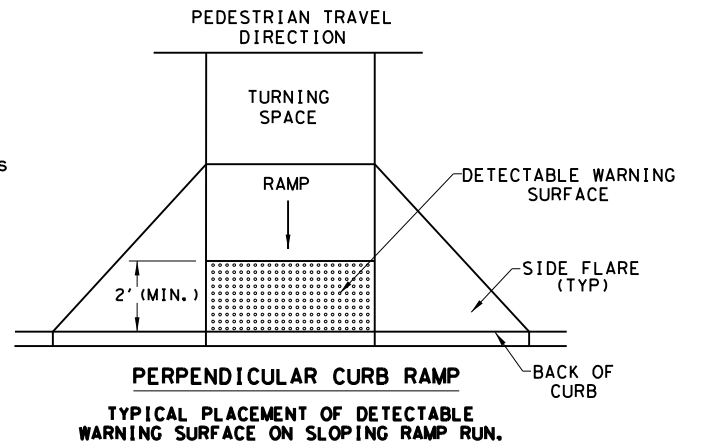
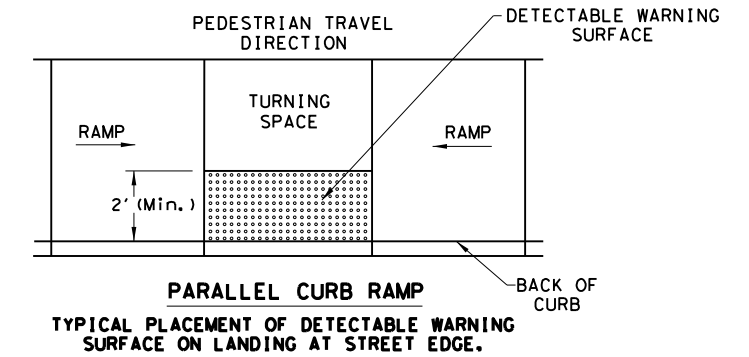
SIDEWALKS

27. Provide clear ground space at operable parts, including pedestrian push buttons. Operable parts shall be placed within unobstructed reach range specified in PROWAG section R406.
28. Place traffic signal or illumination poles, ground boxes, controller boxes, signs, drainage facilities and other items so as not to obstruct the pedestrian access route or clear ground space.
29. Street grades and cross slopes shall be as shown elsewhere in the plans.
30. Changes in level greater than 1/4 inch are not permitted.
31. The least possible grade should be used to maximize accessibility. The running slope of sidewalks and crosswalks within the public right of way may follow the grade of the parallel roadway. Where a continuous grade greater than five percent (5%) must be provided, handrails may be desirable to improve accessibility. Handrails may also be needed to protect pedestrians from potentially hazardous conditions. If provided, handrails shall comply with PROWAG R409.
32. Handrail extensions shall not protrude into the usable landing area or into intersecting pedestrian routes.
33. Driveways and turnouts shall be constructed and paid for in accordance with Item "Intersections, Driveways and Turnouts". Sidewalks shall be constructed and paid for in accordance with Item, "Sidewalks".
34. Sidewalk details are shown elsewhere in the plans.

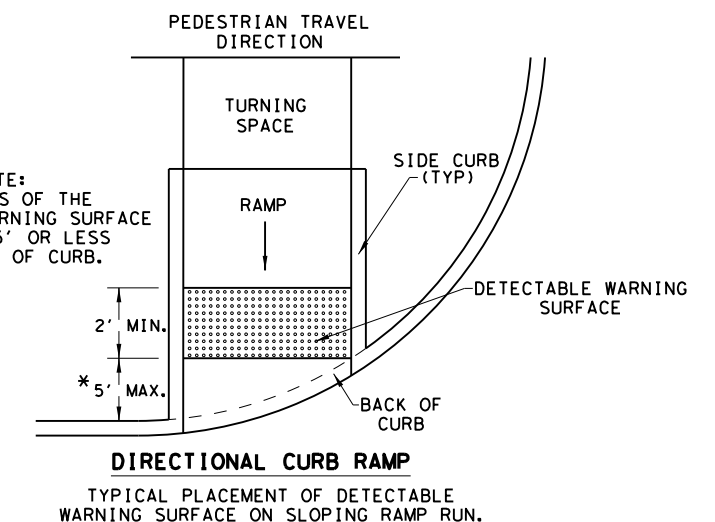


SECTION VIEW DETAIL
CURB RAMP AT DETECTIBLE WARNINGS

DETECTABLE WARNING SURFACE DETAILS



* NOTE:
 BOTH ENDS OF THE
 DETECTABLE WARNING SURFACE
 SHALL BE 5' OR LESS
 FROM BACK OF CURB.

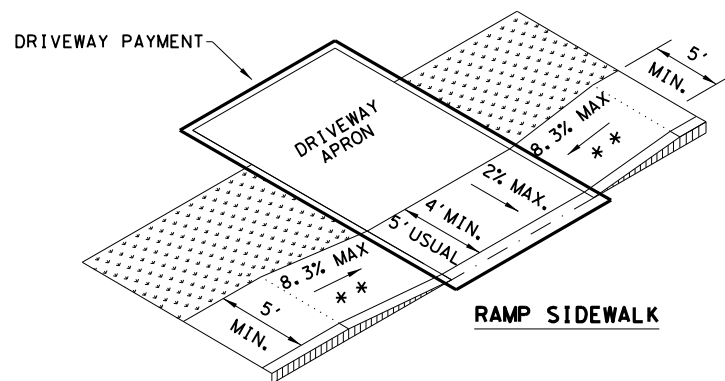
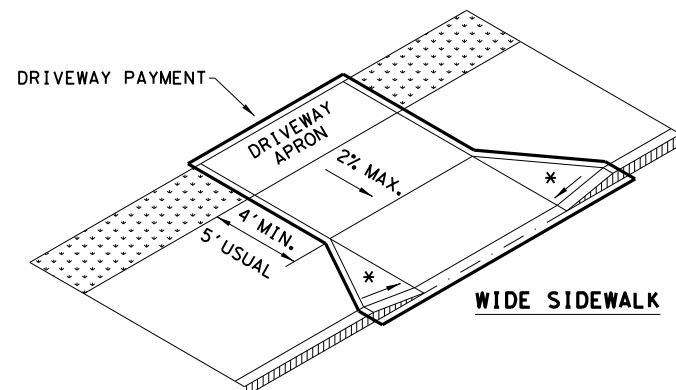
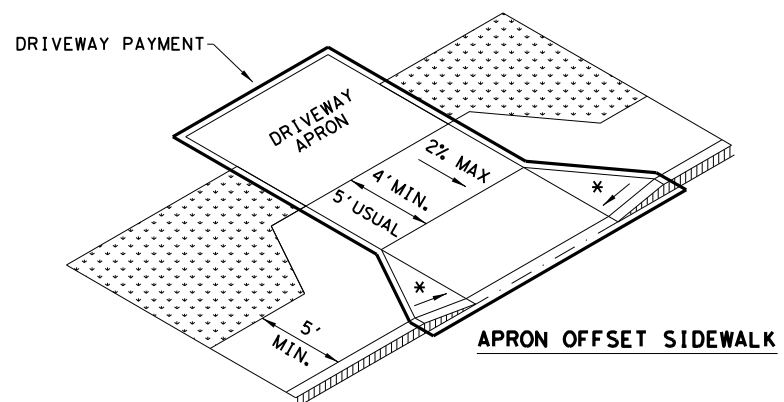
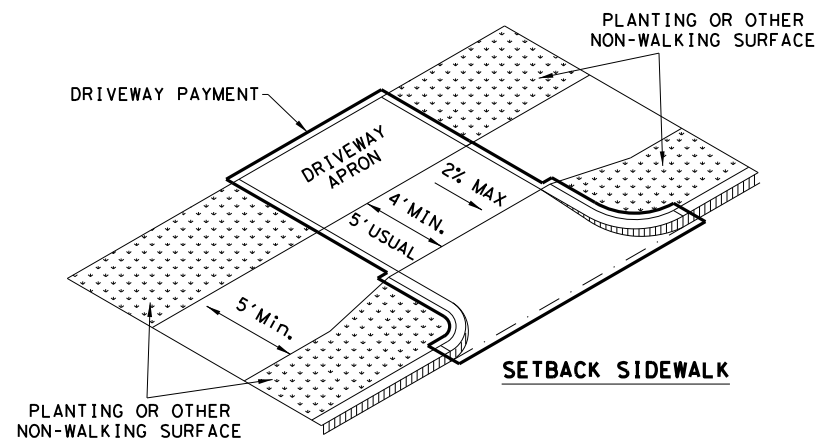


SHEET 2 OF 4

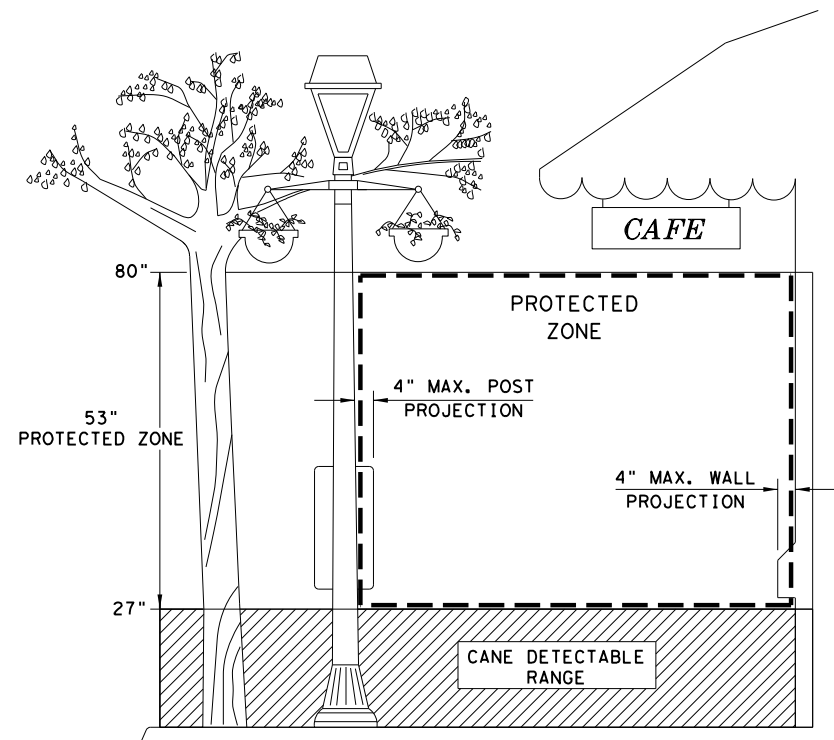
Texas Department of Transportation		Design Division Standard	
PEDESTRIAN FACILITIES CURB RAMP			
PED-18			
FILE: ped18	DN: TxDOT	DW: VP	CK: KM
© TxDOT: MARCH, 2002	CONT	SECT	JOB
REVISIONS	0008	15	059
REVISED 08, 2005	DIST	COUNTY	SHEET NO.
REVISED 06, 2012	FTW	TARRANT	42
REVISED 01, 2018			

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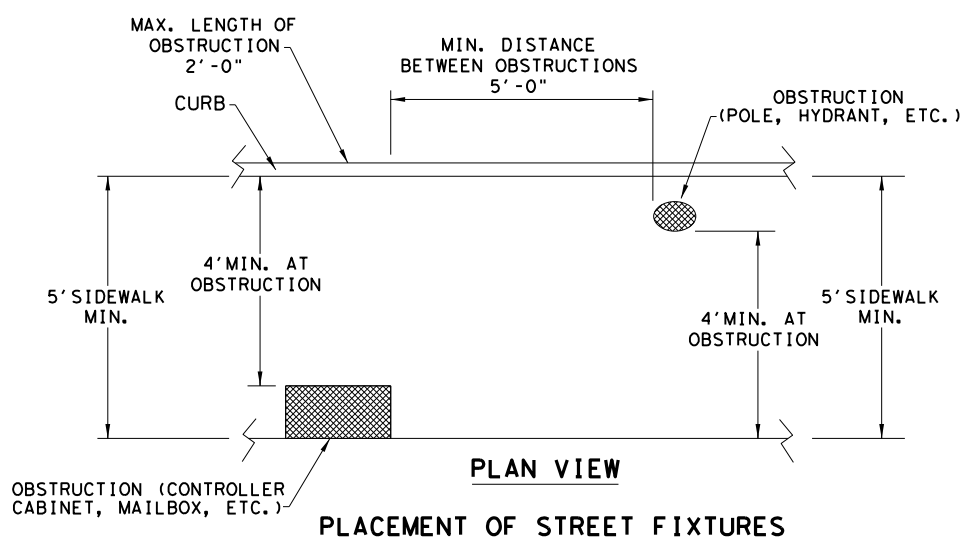
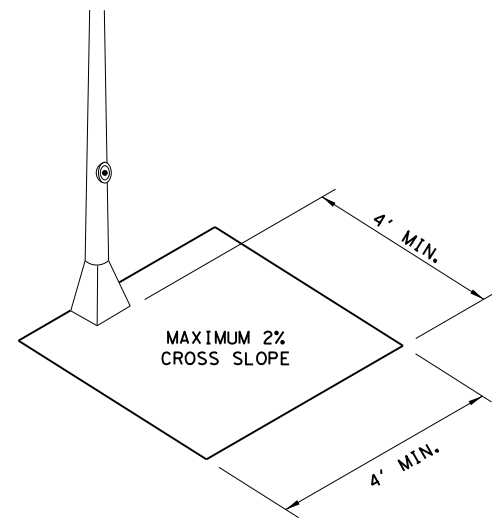
SIDEWALK TREATMENT AT DRIVEWAYS



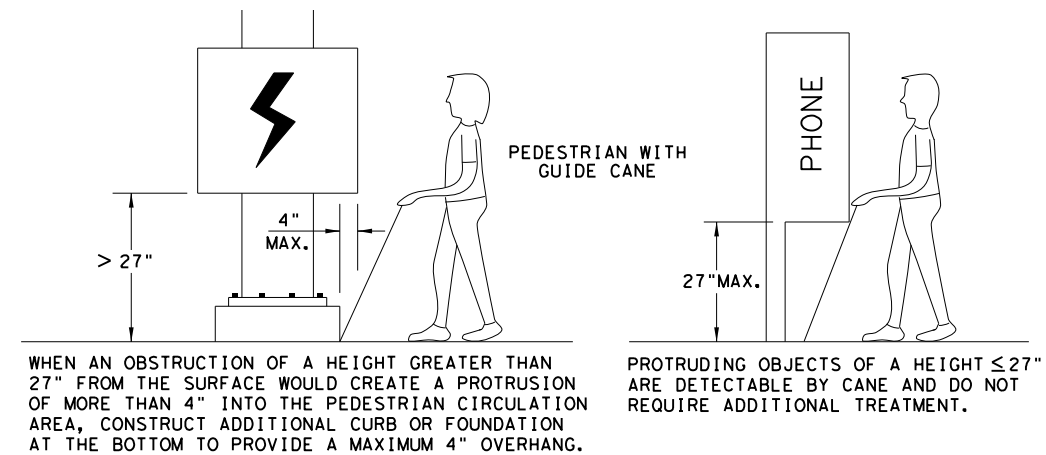
NOTES:
 * WHERE DRIVEWAYS CROSS THE PEDESTRIAN ROUTE, SIDES SHALL BE FLARED AT 10% MAX SLOPE.
 * * IF CURB HEIGHT IS GREATER THAN 6 INCHES, USE GRADE LESS THAN OR EQUAL TO 5%. HANDRAIL AND DETECTABLE WARNING ARE NOT REQUIRED.



NOTE: IN PEDESTRIAN CIRCULATION AREA, MAXIMUM 4" PROJECTION FOR POST OR WALL MOUNTED OBJECTS BETWEEN 27" AND 80" ABOVE THE SURFACE.



NOTE: ITEMS NOT INTENDED FOR PUBLIC USE. MINIMUM 4' X 4' CLEAR GROUND SPACE REQUIRED AT PUBLIC USE FIXTURES.



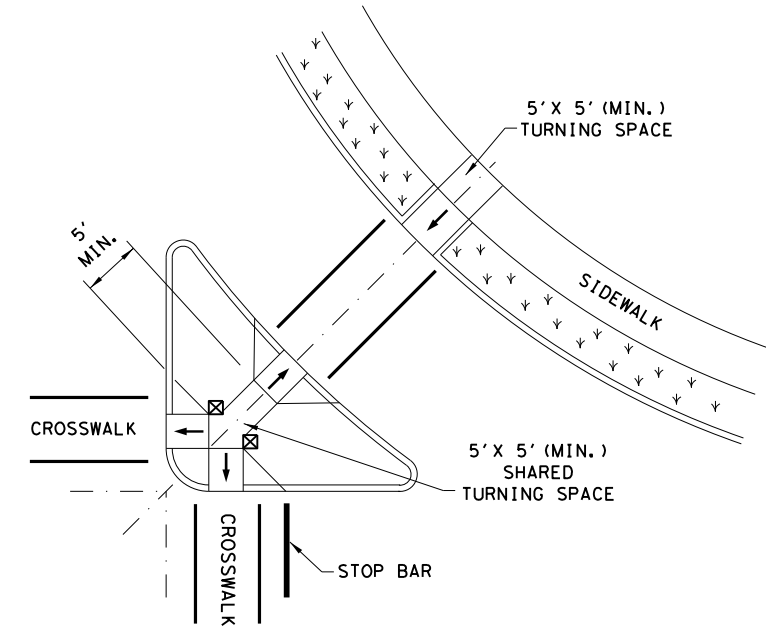
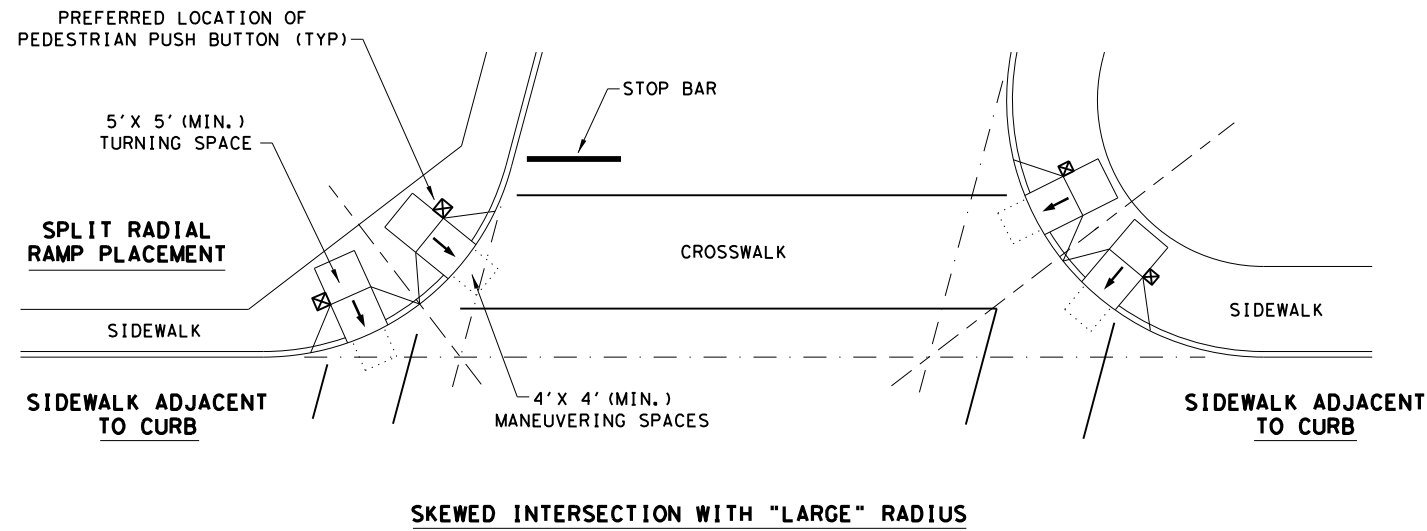
SHEET 3 OF 4

		Design Division Standard	
<h2>PEDESTRIAN FACILITIES</h2> <h3>CURB RAMPS</h3> <h1>PED-18</h1>			
FILE: ped18	DN: TxDOT	DW: VP	CK: KM
© TxDOT: MARCH, 2002	CONT	SECT	JOB
REVISIONS	0008	15	059
REVISOR: 08, 2005	DIST	COUNTY	SHEET NO.
REVISOR: 06, 2012	FTW	TARRANT	43
REVISOR: 01, 2018			

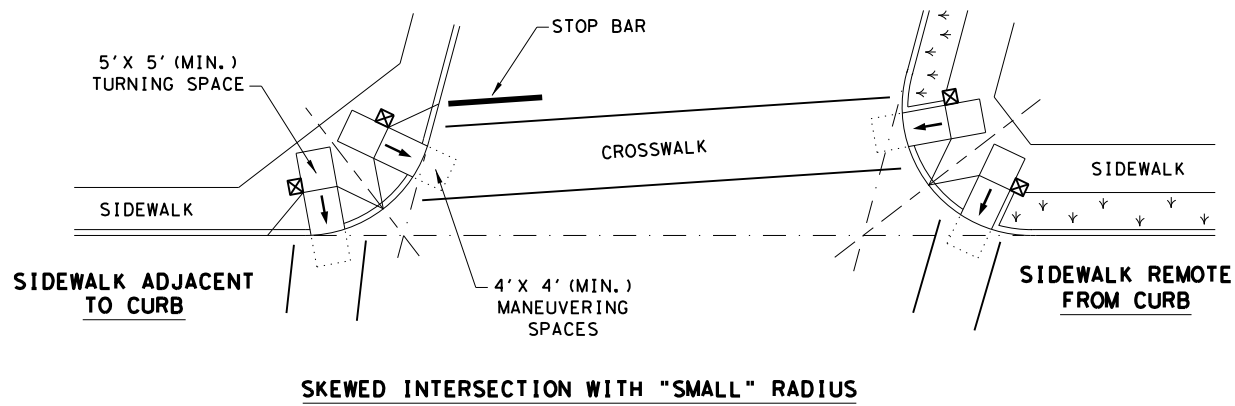
DATE:
FILE:

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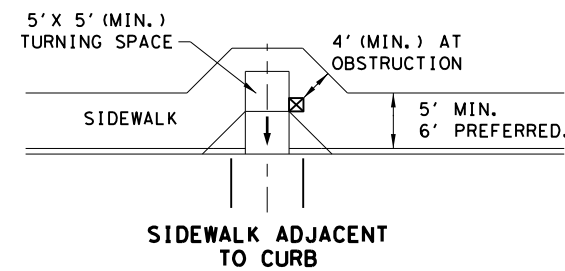
TYPICAL CROSSING LAYOUTS
SEE SHEET 1 OF 4 FOR DETAILS AND DIMENSIONS



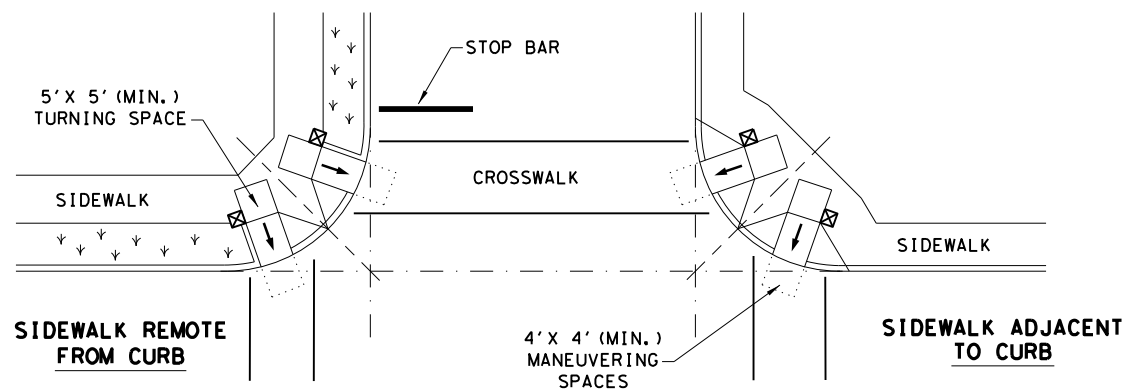
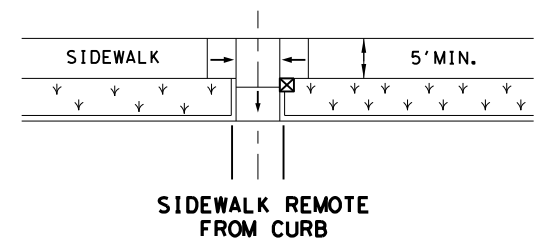
AT INTERSECTION
W/FREE RIGHT TURN & ISLAND



SKewed INTERSECTION WITH "SMALL" RADIUS



MID-BLOCK PLACEMENT
PERPENDICULAR RAMPS



NORMAL INTERSECTION WITH "SMALL" RADIUS

LEGEND:

- SHOWS DOWNWARD SLOPE. →
- DENOTES PREFERRED LOCATION OF PEDESTRIAN PUSH BUTTON (IF APPLICABLE). ☒
- DENOTES PLANTING OR NON-WALKING SURFACE NOT PART OF PEDESTRIAN CIRCULATION PATH. ↙ ↘ ↙ ↘ ↙ ↘

SHEET 4 OF 4



PEDESTRIAN FACILITIES
CURB RAMPS

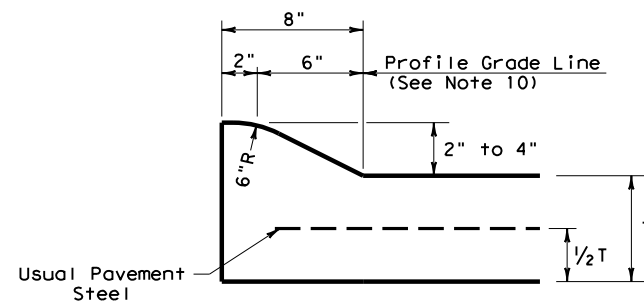
PED-18

FILE: ped18	DN: TxDOT	DW: VP	CK: KM	CK: PK & JG
© TxDOT: MARCH, 2002	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
REVISED 08, 2005	DIST	COUNTY	SHEET NO.	
REVISED 06, 2012	FTW	TARRANT	44	
REVISED 01, 2018				

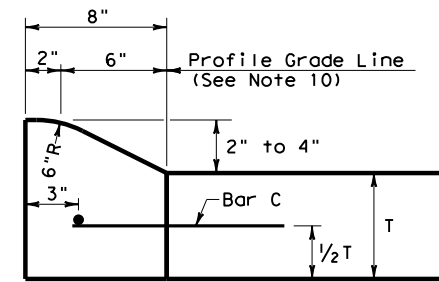
DATE:
FILE:

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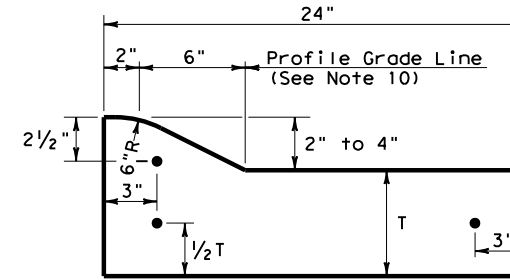
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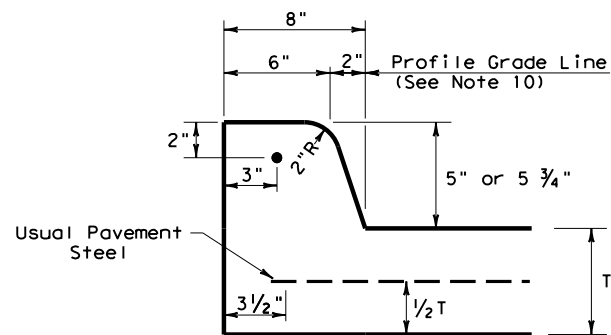
**TYPE I CURB (MONOLITHIC)
2" - 4" HEIGHT**



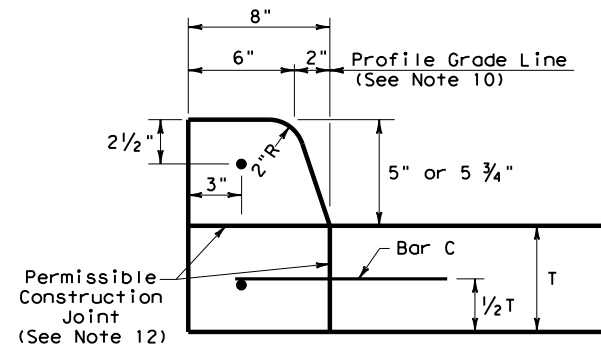
**TYPE I CURB
2" - 4" HEIGHT**



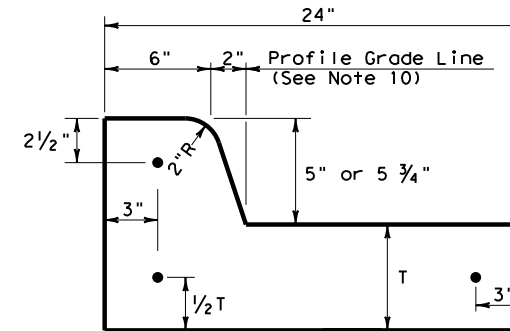
**TYPE I CURB AND GUTTER
2" - 4" HEIGHT**



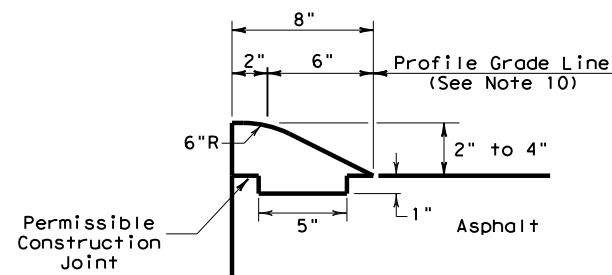
**TYPE II CURB (MONOLITHIC)
5" - 5 3/4" HEIGHT**



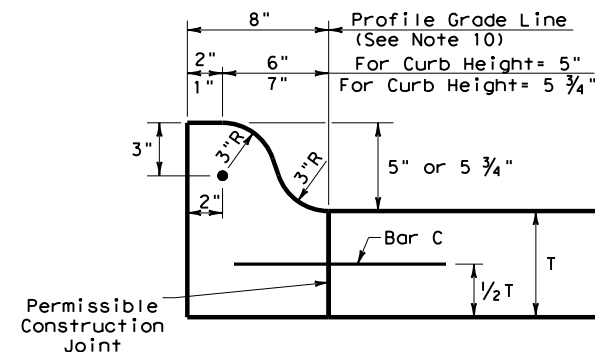
**TYPE II CURB
5" - 5 3/4" HEIGHT**



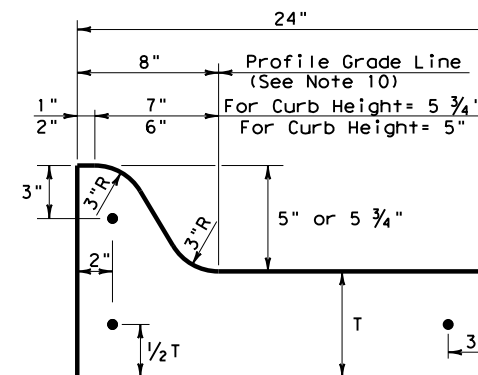
**TYPE II CURB AND GUTTER
5" - 5 3/4" HEIGHT**



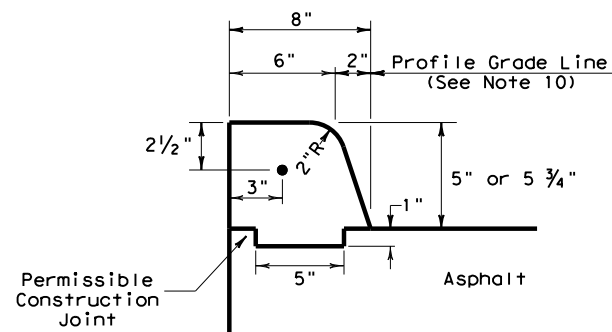
**TYPE III CURB (KEYED)
2" - 4" HEIGHT**



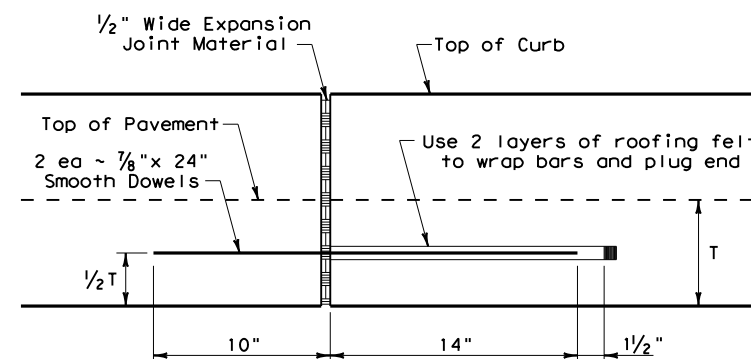
**TYPE IIa CURB
5" - 5 3/4" HEIGHT**



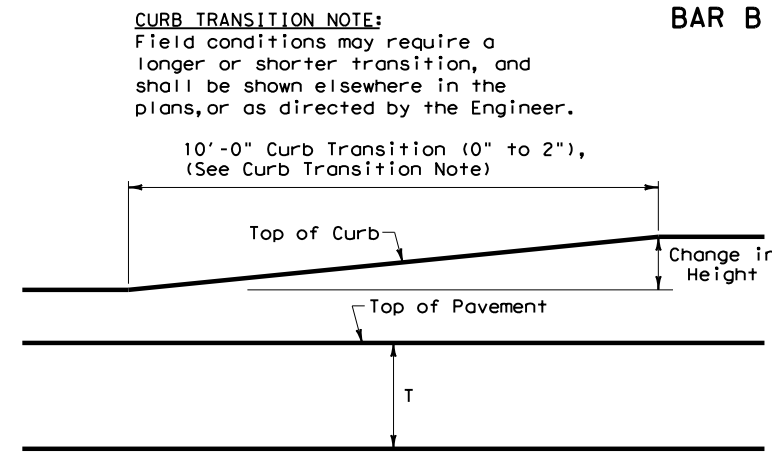
**TYPE IIa CURB AND GUTTER
5" - 5 3/4" HEIGHT**



**TYPE IV CURB (KEYED)
5" - 5 3/4" HEIGHT**



EXPANSION JOINT DETAIL

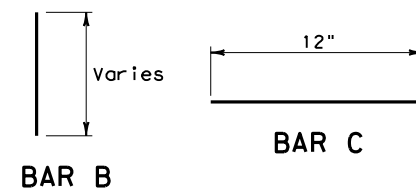


CURB TRANSITION

Note: To be paid for as Highest Curb

GENERAL NOTES

- All materials and construction shall be in accordance with Item 529, "Concrete Curb, Gutter, and Combined Curb and Gutter."
- Concrete shall be Class A.
- When reinforcing bars are used, they shall be No.4 unless otherwise shown. The use of fiber reinforced concrete in lieu of reinforcing steel is acceptable. Use fibers meeting the requirements of DMS 4550, "Fibers for Concrete," and dose fibers in accordance with Material Producers List (MPL) "Fibers for Class A and B Concrete Applications."
- Round exposed sharp edges with a rounding tool, to a minimum radius of 1/4 inch.
- All existing curbs and driveways to be removed shall be sawed or removed at existing joints.
- Where concrete curb is to be placed on existing concrete pavement, Bar B may be drilled and the grouted in place, or may be inserted into fresh concrete.
- Expansion and contraction joints shall be constructed to match pavement joints in all curbs and curb and gutter adjacent to jointed concrete pavement. Where placement of curb or curb and gutter is not adjacent to concrete pavement, expansion joints shall be provided at structures, curb returns at streets, and at locations directed by The Engineer.
- Vertical and horizontal dowel bars and transverse reinforcing bars shall be placed at four feet C-C.
- Dimension 'T' shown is the thickness of concrete pavement. When curb is installed adjacent to flexible pavement dimension 'T' is 8" maximum.
- Usual profile grade line. Refer to typical sections and plan-profile sheets for exact locations.
- One-half inch expansion joint material shall be provided where curb or curb and gutter is adjacent to sidewalk or riprap.
- When horizontal permissible construction joints are used, the longitudinal pavement steel shall be placed in accordance with pavement details shown elsewhere in the plans. Reinforcing steel for curb section shall then conform to that required for concrete curb.
- Bar B used as needed to support curb reinforcing steel during concrete placement.



CURB TRANSITION NOTE:
Field conditions may require a longer or shorter transition, and shall be shown elsewhere in the plans, or as directed by the Engineer.

				Design Division Standard	
CONCRETE CURB AND GUTTER					
CCCG-21					
FILE: cccg21.dgn	DN: TxDOT	CK: AN	DW: SS	CK: KM	
© TxDOT: FEBRUARY 2021	CONT	SECT	JOB	HIGHWAY	
REVISIONS	0008	15	059	IH 820	
	DIST	COUNTY		SHEET NO.	
	FTW	TARRANT		45	

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DATE: FILE:

I. STORMWATER POLLUTION PREVENTION-CLEAN WATER ACT SECTION 402

TPDES TXR 150000: Stormwater Discharge Permit or Construction General Permit required for projects with 1 or more acres disturbed soil. Projects with any disturbed soil must protect for erosion and sedimentation in accordance with Item 506.

List MS4 Operator(s) that may receive discharges from this project. They may need to be notified prior to construction activities.

1. CITY OF WHITE SETTLEMENT

2. No Action Required Required Action

Action No.

1. Prevent stormwater pollution by controlling erosion and sedimentation in accordance with TPDES Permit TXR 150000
2. Comply with the SW3P and revise when necessary to control pollution or required by the Engineer.
3. Post Construction Site Notice (CSN) with SW3P information on or near the site, accessible to the public and TCEQ, EPA or other inspectors.
4. When Contractor project specific locations (PSL's) increase disturbed soil area to 5 acres or more, submit NOI to TCEQ and the Engineer.

II. WORK IN OR NEAR STREAMS, WATERBODIES AND WETLANDS CLEAN WATER ACT SECTIONS 401 AND 404

USACE Permit required for filling, dredging, excavating or other work in any water bodies, rivers, creeks, streams, wetlands or wet areas.

The Contractor must adhere to all of the terms and conditions associated with the following permit(s):

- No Permit Required
- Nationwide Permit 14 - PCN not Required (less than 1/10th acre waters or wetlands affected)
- Nationwide Permit 14 - PCN Required (1/10 to <1/2 acre, 1/3 in tidal waters)
- Individual 404 Permit Required
- Other Nationwide Permit Required: NWP# _____

Required Actions: List waters of the US permit applies to, location in project and check Best Management Practices planned to control erosion, sedimentation and post-project TSS.

- 1.
- 2.
- 3.
- 4.

The elevation of the ordinary high water marks of any areas requiring work to be performed in the waters of the US requiring the use of a nationwide permit can be found on the Bridge Layouts.

Best Management Practices:

Erosion	Sedimentation	Post-Construction TSS
<input type="checkbox"/> Temporary Vegetation	<input type="checkbox"/> Silt Fence	<input checked="" type="checkbox"/> Vegetative Filter Strips
<input type="checkbox"/> Blankets/Matting	<input type="checkbox"/> Rock Berm	<input type="checkbox"/> Retention/Irrigation Systems
<input checked="" type="checkbox"/> Mulch	<input type="checkbox"/> Triangular Filter Dike	<input type="checkbox"/> Extended Detention Basin
<input checked="" type="checkbox"/> Sodding	<input type="checkbox"/> Sand Bag Berm	<input type="checkbox"/> Constructed Wetlands
<input type="checkbox"/> Interceptor Swale	<input type="checkbox"/> Straw Bale Dike	<input type="checkbox"/> Wet Basin
<input type="checkbox"/> Diversion Dike	<input type="checkbox"/> Brush Berms	<input type="checkbox"/> Erosion Control Compost
<input type="checkbox"/> Erosion Control Compost	<input type="checkbox"/> Erosion Control Compost	<input type="checkbox"/> Mulch Filter Berm and Socks
<input type="checkbox"/> Mulch Filter Berm and Socks	<input type="checkbox"/> Mulch Filter Berm and Socks	<input type="checkbox"/> Compost Filter Berm and Socks
<input type="checkbox"/> Compost Filter Berm and Socks	<input checked="" type="checkbox"/> Compost Filter Berm and Socks	<input type="checkbox"/> Vegetation Lined Ditches
	<input type="checkbox"/> Stone Outlet Sediment Traps	<input type="checkbox"/> Sand Filter Systems
	<input type="checkbox"/> Sediment Basins	<input type="checkbox"/> Grassy Swales

III. CULTURAL RESOURCES

Refer to TxDOT Standard Specifications in the event historical issues or archeological artifacts are found during construction. Upon discovery of archeological artifacts (bones, burnt rock, flint, pottery, etc.) cease work in the immediate area and contact the Engineer immediately.

No Action Required Required Action

Action No.

- 1.
- 2.
- 3.
- 4.

IV. VEGETATION RESOURCES

Preserve native vegetation to the extent practical. Contractor must adhere to Construction Specification Requirements Specs 162, 164, 192, 193, 506, 730, 751, 752 in order to comply with requirements for invasive species, beneficial landscaping, and tree/brush removal commitments.

No Action Required Required Action

Action No.

- 1.
- 2.
- 3.
- 4.

V. FEDERAL LISTED, PROPOSED THREATENED, ENDANGERED SPECIES, CRITICAL HABITAT, STATE LISTED SPECIES, CANDIDATE SPECIES AND MIGRATORY BIRDS.

No Action Required Required Action

Action No.

- 1.
- 2.
- 3.
- 4.

If any of the listed species are observed, cease work in the immediate area, do not disturb species or habitat and contact the Engineer immediately. The work may not remove active nests from bridges and other structures during nesting season of the birds associated with the nests. If caves or sinkholes are discovered, cease work in the immediate area, and contact the Engineer immediately.

LIST OF ABBREVIATIONS

BMP: Best Management Practice	SPCC: Spill Prevention Control and Countermeasure
CGP: Construction General Permit	SW3P: Storm Water Pollution Prevention Plan
DSHS: Texas Department of State Health Services	PCN: Pre-Construction Notification
FHWA: Federal Highway Administration	PSL: Project Specific Location
MOA: Memorandum of Agreement	TCEQ: Texas Commission on Environmental Quality
MOU: Memorandum of Understanding	TPDES: Texas Pollutant Discharge Elimination System
MS4: Municipal Separate Stormwater Sewer System	TPWD: Texas Parks and Wildlife Department
MBTA: Migratory Bird Treaty Act	TxDOT: Texas Department of Transportation
NOT: Notice of Termination	T&E: Threatened and Endangered Species
NWP: Nationwide Permit	USACE: U.S. Army Corps of Engineers
NOI: Notice of Intent	USFWS: U.S. Fish and Wildlife Service

VI. HAZARDOUS MATERIALS OR CONTAMINATION ISSUES

General (applies to all projects):

Comply with the Hazard Communication Act (the Act) for personnel who will be working with hazardous materials by conducting safety meetings prior to beginning construction and making workers aware of potential hazards in the workplace. Ensure that all workers are provided with personal protective equipment appropriate for any hazardous materials used. Obtain and keep on-site Material Safety Data Sheets (MSDS) for all hazardous products used on the project, which may include, but are not limited to the following categories: Paints, acids, solvents, asphalt products, chemical additives, fuels and concrete curing compounds or additives. Provide protected storage, off bare ground and covered, for products which may be hazardous. Maintain product labelling as required by the Act. Maintain an adequate supply of on-site spill response materials, as indicated in the MSDS. In the event of a spill, take actions to mitigate the spill as indicated in the MSDS, in accordance with safe work practices, and contact the District Spill Coordinator immediately. The Contractor shall be responsible for the proper containment and cleanup of all product spills.

Contact the Engineer if any of the following are detected:

- * Dead or distressed vegetation (not identified as normal)
- * Trash piles, drums, canister, barrels, etc.
- * Undesirable smells or odors
- * Evidence of leaching or seepage of substances

Does the project involve any bridge class structure rehabilitation or replacements (bridge class structures not including box culverts)?

Yes No

If "No", then no further action is required.

If "Yes", then TxDOT is responsible for completing asbestos assessment/inspection.

Are the results of the asbestos inspection positive (is asbestos present)?

Yes No

If "Yes", then TxDOT must retain a DSHS licensed asbestos consultant to assist with the notification, develop abatement/mitigation procedures, and perform management activities as necessary. The notification form to DSHS must be postmarked at least 15 working days prior to scheduled demolition.

If "No", then TxDOT is still required to notify DSHS 15 working days prior to any scheduled demolition.

In either case, the Contractor is responsible for providing the date(s) for abatement activities and/or demolition with careful coordination between the Engineer and asbestos consultant in order to minimize construction delays and subsequent claims.

Any other evidence indicating possible hazardous materials or contamination discovered on site. Hazardous Materials or Contamination Issues Specific to this Project:

No Action Required Required Action

Action No.

- 1.
- 2.
- 3.


VII. OTHER ENVIRONMENTAL ISSUES

(includes regional issues such as Edwards Aquifer District, etc.)

No Action Required Required Action

Action No.

- 1.
- 2.
- 3.

 Texas Department of Transportation		Design Division Standard	
ENVIRONMENTAL PERMITS, ISSUES AND COMMITMENTS EPIC			
FILE: epic.dgn	DN: TxDOT	CK: RG	DW: VP
©TxDOT: February 2015	CONT	SECT	JOB
12-12-2011 (DS) REVISIONS	0008	15	059
05-07-14 ADDED NOTE SECTION IV.	DIST	COUNTY	SHEET NO.
01-23-2015 SECTION I CHANGED ITEM 1122 TO ITEM 506, ADDED GRASSY SWALES.	FTW	TARRANT	46

A. GENERAL SITE DATA

1. PROJECT LIMITS:
 FROM 0.05 MILES N. OF CLIFFORD ST
 TO 0.05 MILES S. OF CLIFFORD ST

 PROJECT LOCATION:
 BEGIN PROJECT: R.M. 4+0.984
 END PROJECT: R.M. 5+0.086

 PROJECT COORDINATES:
 BEG. LATITUDE: 32.7653966 BEG. LONGITUDE: -97.4768893
 END LATITUDE: 32.7668342 END LONGITUDE: -97.4765968
2. PROJECT SITE MAPS:
 * PROJECT LOCATION MAP: TITLE SHEET
 * DRAINAGE PATTERNS: N/A
 * SLOPES ANTICIPATED AFTER MAJOR GRADINGS OR AREAS OF SOIL DISTURBANCE: N/A
 * LOCATION OF EROSION AND SEDIMENT CONTROLS: N/A
 * SURFACE WATERS AND DISCHARGE LOCATIONS: N/A, NONE
 * PROJECT SPECIFIC LOCATIONS: TO BE SPECIFIED BY THE PROJECT FIELD OFFICE DURING CONSTRUCTION AND LOCATED IN THE PROJECT SW3P FILE. REFERENCE ITEM #10 BELOW
3. PROJECT DESCRIPTION: LANDSCAPE DEVELOPMENT
 LANDSCAPING, WHITE SETTLEMENT GCAA
 ADA CURB RAMPS AND SIDEWALKS
4. MAJOR SOIL DISTURBING ACTIVITIES: CONSTRUCTING PLANTING BEDS, BED EDGING, PAVERS, CURB RAMPS, SIDEWALKS, IRRIGATION SYSTEMS, & TRENCHING.
5. EXISTING CONDITION OF SOIL & VEGETATIVE COVER AND % OF EXISTING VEGETATIVE COVER:
 58% VEGETATIVE COVER.
6. TOTAL PROJECT AREA: 2.29 AC
7. TOTAL AREA TO BE DISTURBED: 0.45 AC
8. WEIGHTED RUNOFF COEFFICIENT
 BEFORE CONSTRUCTION: 0.2
 AFTER CONSTRUCTION: 0.2
9. NAME OF RECEIVING WATERS: (SEGMENT NUMBER OF RECEIVING WATERS)

 LAKE WORTH, SEGMENT ID: 0807
10. PROJECT SW3P FILE: FOR PROJECTS DISTURBING ONE ACRE OR MORE, TXDOT WILL MAINTAIN AN SW3P FILE WITH ALL PERTINENT ENVIRONMENTAL DOCUMENTS, CORRESPONDENCE, ETC. AT THE PROJECT FIELD OFFICE. IF NO FIELD OFFICE IS AVAILABLE THEN THE SW3P FILE SHALL BE KEPT IN THE INSPECTOR'S TRUCK.

B. EROSION AND SEDIMENT CONTROLS

1. SOIL STABILIZATION PRACTICES:
 ___ TEMPORARY SEEDING
 PERMANENT PLANTING, SODDING, OR SEEDING
 MULCHING
 ___ SOIL RETENTION BLANKET
 ___ BUFFER ZONES
 PRESERVATION OF NATURAL RESOURCES

 OTHER: VEGETATIVE BUFFERS
2. STRUCTURAL PRACTICES:
 ___ SILT FENCES
 ___ ROCK FILTER DAMS
 ___ DIVERSION, INTERCEPTOR, OR PERIMETER DIKES
 ___ DIVERSION, INTERCEPTOR, OR PERIMETER SWALES
 ___ DIVERSION DIKE AND SWALE COMBINATIONS
 ___ PIPE SLOPE DRAINS
 ___ PAVED FLUMES
 ___ ROCK BEDDING AT CONSTRUCTION EXIT
 ___ TIMBER MATTING AT CONSTRUCTION EXIT
 ___ CHANNEL LINERS
 ___ SEDIMENT TRAPS
 ___ SEDIMENT BASINS
 ___ STORM INLET SEDIMENT TRAP
 ___ STONE OUTLET STRUCTURES
 ___ CURBS AND GUTTERS
 ___ STORM SEWERS
 ___ VELOCITY CONTROL DEVICES

 OTHER: TEMPORARY EROSION CONTROL LOGS
3. STORM WATER MANAGEMENT:

 STORM WATER DRAINAGE WILL BE PROVIDED BY GRASS-LINED SWALES & EXISTING DITCHES AND STORMWATER SYSTEMS.
 THIS SYSTEM WILL CARRY THE DRAINAGE WITHIN THE RIGHT-OF-WAY TO PROJECT SITE WHICH DRAINS TO NATURAL FACILITIES
4. STORM WATER MANAGEMENT ACTIVITIES: (SEQUENCE OF CONSTRUCTION)
 1. STABILIZE AREA FOR POTENTIAL RUNOFF.
 2. INSTALL IRRIGATION SYSTEM, INCLUDING TRENCHING & BORING.
 3. PLANT PLANT MATERIAL.
 4. INSTALL LANDSCAPE PAVERS.
 5. CONSTRUCT CURB RAMPS AND SIDEWALK.
 6. SOD DISTURBED AREAS ACCORDING TO PLANS.
5. NON-STORM WATER DISCHARGES:
 FILTER NON-STORM WATER DISCHARGES, OR HOLD RETENTION BASINS, BEFORE BEING ALLOWED TO MIX WITH STORM WATER. THESE DISCHARGES CONSIST OF NON-POLLUTED GROUND WATER, SPRING WATER, FOUNDATION AND/OR FOOTING DRAIN WATER; AND WATER USED FOR DUST CONTROL, PAVEMENT WASHING AND VEHICLE WASHWATER CONTAINING NO DETERGENTS.

C. OTHER REQUIREMENTS & PRACTICES

1. MAINTENANCE:
 MAINTENANCE WILL BE PERFORMED AS INDICATED ON FIELD INSPECTION AND MAINTENANCE REPORT FORM 2118.
2. INSPECTION:
 INSPECTION WILL BE PERFORMED AS INDICATED ON FIELD INSPECTION AND MAINTENANCE REPORT FORM 2118.
3. WASTE MATERIALS:
 ALL WASTE MATERIALS WILL BE COLLECTED, STORED AND DISPOSED OF IN A LIDDED DUMPSTER IN A LEGAL AND PROPER MANNER. NO CONSTRUCTION WASTE MATERIAL WILL BE BURIED ON SITE.
4. HAZARDOUS WASTE (INCLUDING SPILL REPORTING):
 AT A MINIMUM, ANY PRODUCTS IN THE FOLLOWING CATEGORIES ARE CONSIDERED TO BE HAZARDOUS. PAINTS, ACIDS FOR CLEANING MASONRY SURFACES, CLEANING SOLVENTS, ASPHALT PRODUCTS, CHEMICAL ADDITIVES FOR SOIL STABILIZATION, OR CONCRETE CURING COMPOUNDS AND ADDITIVES. IN THE EVENT OF A SPILL WHICH MAY BE HAZARDOUS, THE SPILL COORDINATOR MUST BE CONTACTED IMMEDIATELY.
5. SANITARY WASTE:
 ALL SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS AS NECESSARY OR AS REQUIRED BY LOCAL REGULATION BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR.

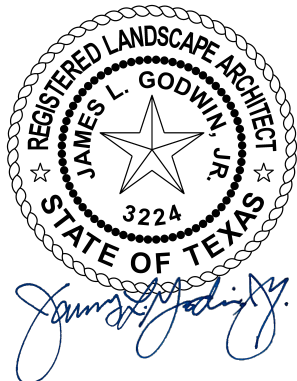
OFFSITE VEHICLE TRACKING:

- ___ HAUL ROADS DAMPENED FOR DUST CONTROL
 LOADED HAUL TRUCKS TO BE COVERED WITH TARPAULIN
 EXCESS DIRT ON ROAD REMOVED DAILY
 ___ STABILIZED CONSTRUCTION ENTRANCE

OTHER:

REMARKS: DISPOSAL AREAS, STOCKPILES AND HAUL ROADS SHALL BE CONSTRUCTED IN A MANNER THAT WILL MINIMIZE AND CONTROL SEDIMENT FROM ENTERING RECEIVING WATERS. DISPOSAL AREAS SHALL NOT BE LOCATED IN ANY WATERBODY OR STREAMBED.

CONSTRUCTION STAGING AREAS AND VEHICLE MAINTENANCE AREAS SHALL BE CONSTRUCTED TO MINIMIZE THE RUNOFF OF POLLUTANTS.



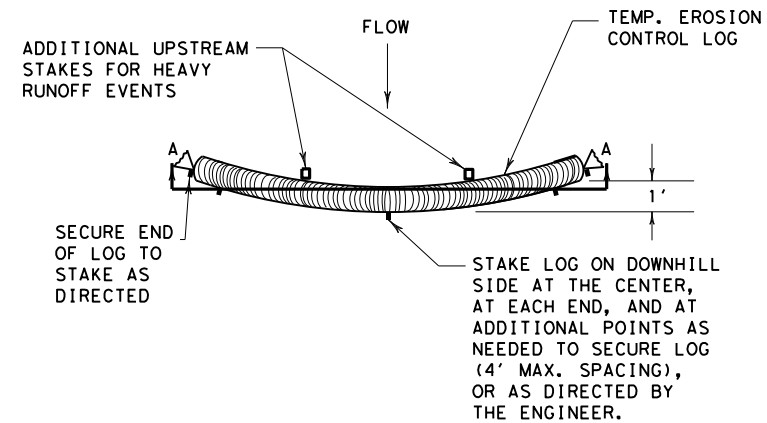
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**VA
 STORM WATER
 POLLUTION
 PREVENTION
 PLAN (SW3P)**

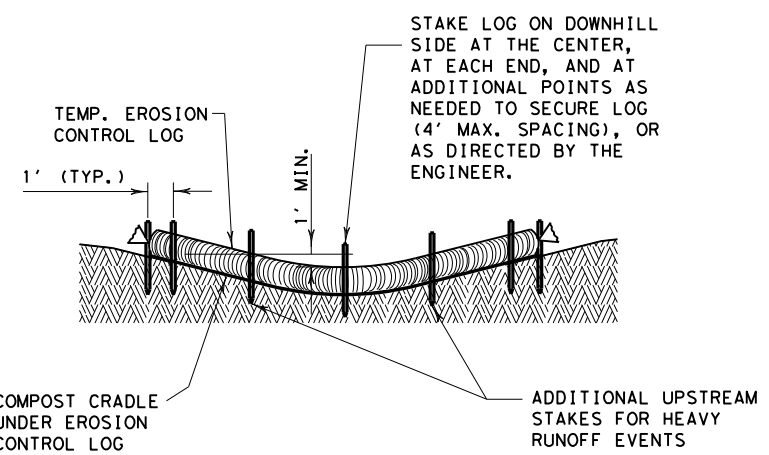


CONT	SECT	JOB	HIGHWAY
0008	15	059	IH 820
DIST	COUNTY		SHEET NO.
FTW	TARRANT		47

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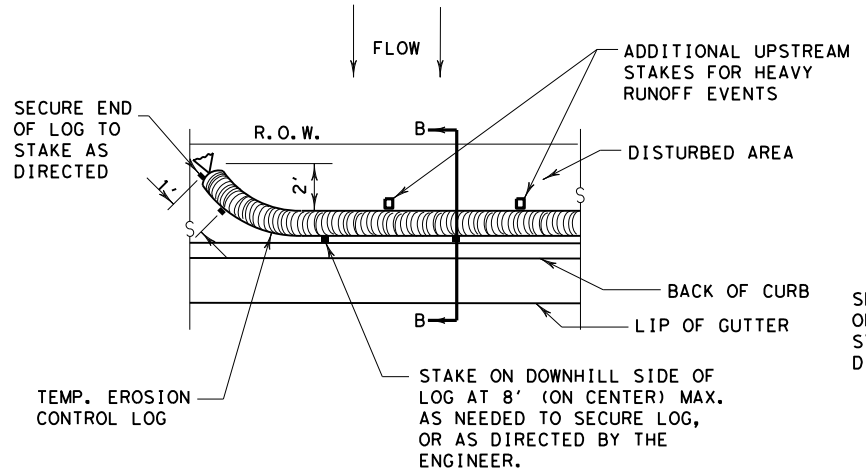
PLAN VIEW



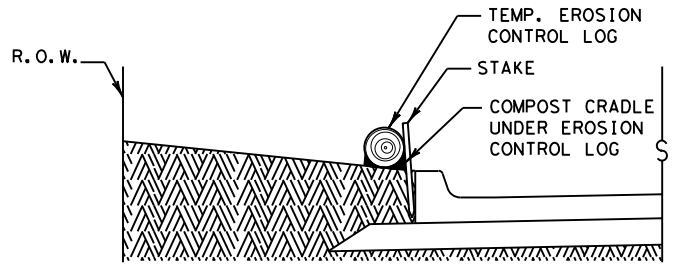
SECTION A-A

EROSION CONTROL LOG DAM

CL-D



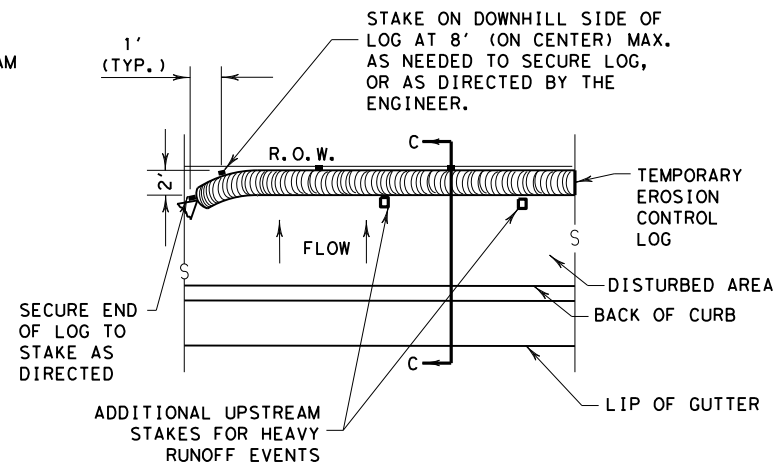
PLAN VIEW



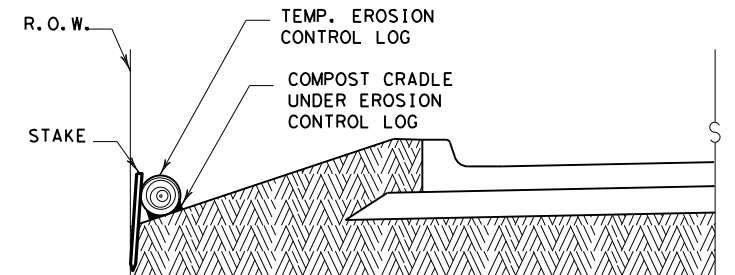
SECTION B-B

EROSION CONTROL LOG AT BACK OF CURB

CL-BOC



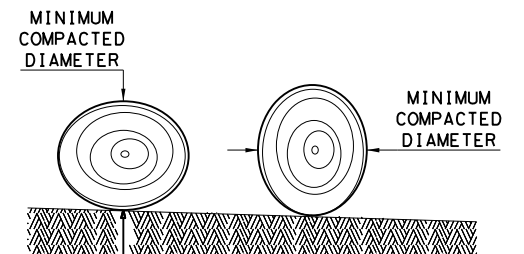
PLAN VIEW



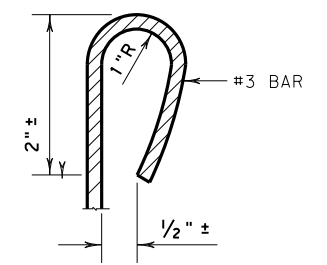
SECTION C-C

EROSION CONTROL LOG AT EDGE OF RIGHT-OF-WAY

CL-ROW



DIAMETER MEASUREMENTS OF EROSION CONTROL LOGS SPECIFIED IN PLANS



REBAR STAKE DETAIL

SEDIMENT BASIN & TRAP USAGE GUIDELINES

An erosion control log sediment trap may be used to filter sediment out of runoff draining from an unstabilized area.

Log Traps: The drainage area for a sediment trap should not exceed 5 acres. The trap capacity should be 1800 CF/Acre (0.5" over the drainage area).

Control logs should be placed in the following locations:

1. Within drainage ditches spaced as needed or min. 500' on center
2. Immediately preceding ditch inlets or drain inlets
3. Just before the drainage enters a water course
4. Just before the drainage leaves the right of way
5. Just before the drainage leaves the construction limits where drainage flows away from the project.

The logs should be cleaned when the sediment has accumulated to a depth of 1/2 the log diameter.

Cleaning and removal of accumulated sediment deposits is incidental and will not be paid for separately.

GENERAL NOTES:

1. EROSION CONTROL LOGS SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS, OR AS DIRECTED BY THE ENGINEER.
2. LENGTHS OF EROSION CONTROL LOGS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND AS REQUIRED FOR THE PURPOSE INTENDED.
3. UNLESS OTHERWISE DIRECTED, USE BIODEGRADABLE OR PHOTODEGRADABLE CONTAINMENT MESH ONLY WHERE LOG WILL REMAIN IN PLACE AS PART OF A VEGETATIVE SYSTEM. FOR TEMPORARY INSTALLATIONS, USE RECYCLABLE CONTAINMENT MESH.
4. FILL LOGS WITH SUFFICIENT FILTER MATERIAL TO ACHIEVE THE MINIMUM COMPACTED DIAMETER SPECIFIED IN THE PLANS WITHOUT EXCESSIVE DEFORMATION.
5. STAKES SHALL BE 2" X 2" WOOD OR #3 REBAR, 2'-4' LONG, EMBEDDED SUCH THAT 2" PROTRUDES ABOVE LOG, OR AS DIRECTED BY THE ENGINEER.
6. DO NOT PLACE STAKES THROUGH CONTAINMENT MESH.
7. COMPOST CRADLE MATERIAL IS INCIDENTAL & WILL NOT BE PAID FOR SEPARATELY.
8. SANDBAGS USED AS ANCHORS SHALL BE PLACED ON TOP OF LOGS & SHALL BE OF SUFFICIENT SIZE TO HOLD LOGS IN PLACE.
9. TURN THE ENDS OF EACH ROW OF LOGS UPSLOPE TO PREVENT RUNOFF FROM FLOWING AROUND THE LOG.
10. FOR HEAVY RUNOFF EVENTS, ADDITIONAL UPSTREAM STAKES MAY BE NECESSARY TO KEEP LOG FROM FOLDING IN ON ITSELF.

- LEGEND**
- CL-D EROSION CONTROL LOG DAM
 - CL-BOC EROSION CONTROL LOG AT BACK OF CURB
 - CL-ROW EROSION CONTROL LOG AT EDGE OF RIGHT-OF-WAY
 - CL-SST EROSION CONTROL LOGS ON SLOPES STAKE AND TRENCHING ANCHORING
 - CL-SSL EROSION CONTROL LOGS ON SLOPES STAKE AND LASHING ANCHORING
 - CL-DI EROSION CONTROL LOG AT DROP INLET
 - CL-CI EROSION CONTROL LOG AT CURB INLET
 - CL-GI EROSION CONTROL LOG AT CURB & GRATE INLET

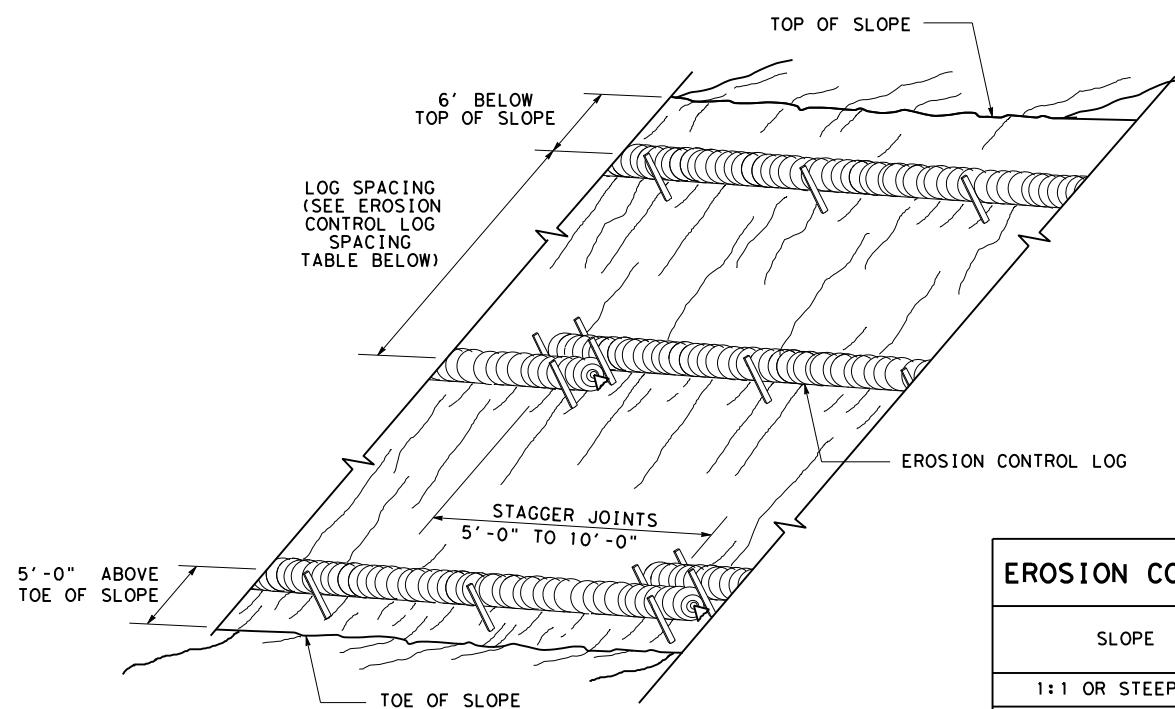
SHEET 1 OF 3

		<i>Design Division Standard</i>		
<p>TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES</p> <p>EROSION CONTROL LOG</p> <p>EC (9) - 16</p>				
FILE: ec916	DN: TxDOT	CK: KM	DW: LS/PT	CK: LS
© TxDOT: JULY 2016	CONT	SECT	JOB	HIGHWAY
REVISIONS	0008	15	059	IH 820
	DIST	COUNTY	SHEET NO.	
	FTW	TARRANT	48	

DATE: FILE:

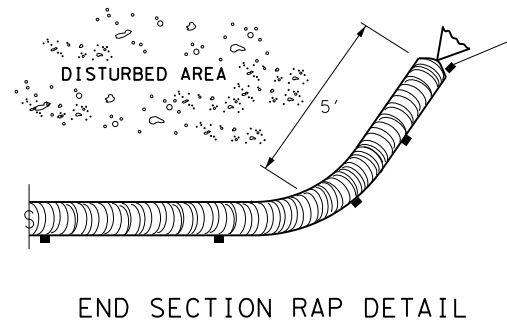
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DATE: FILE:



**EROSION CONTROL LOGS ON SLOPES
STAKE AND TRENCHING ANCHORING**

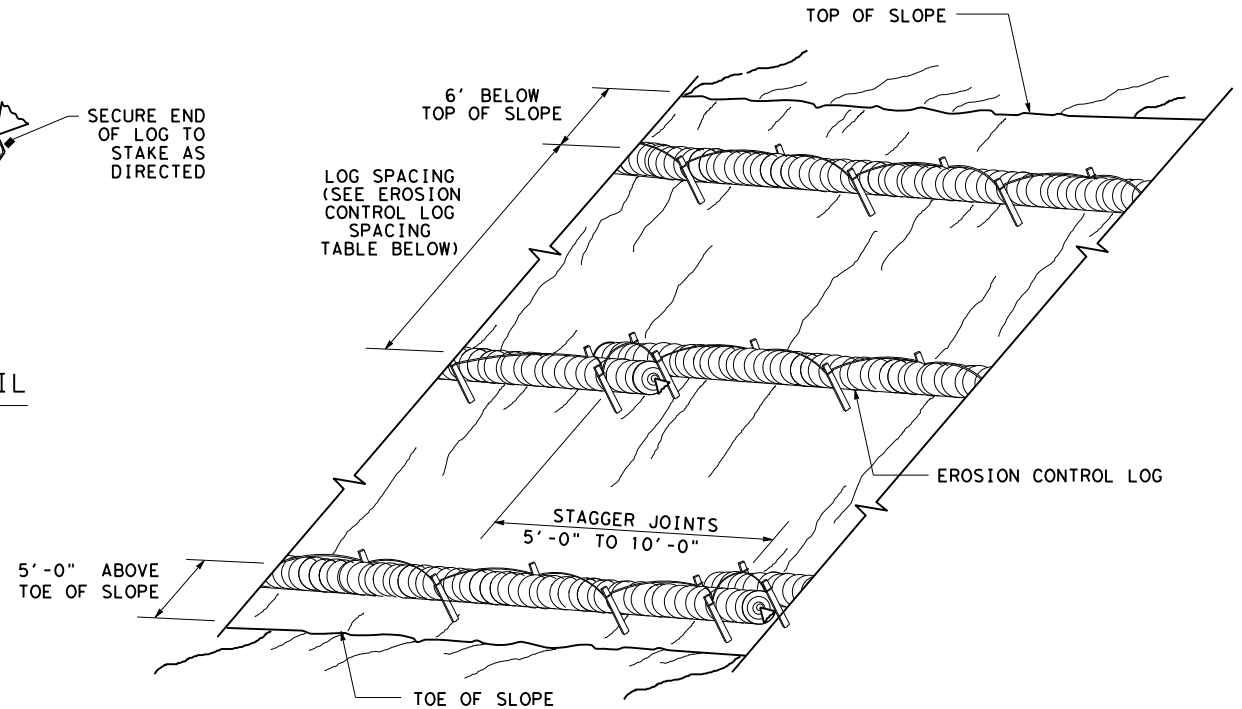
CL-SST



END SECTION RAP DETAIL

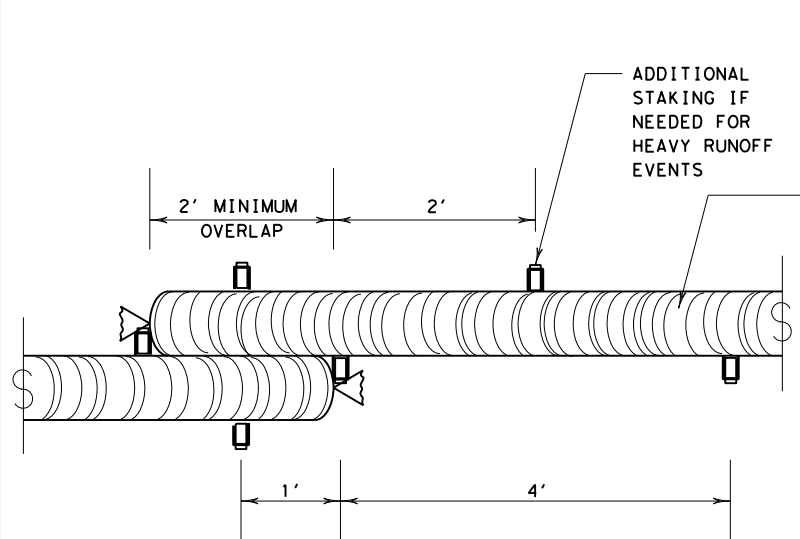
SLOPE	LOG DIAMETER			
	6"	8"	12"	18"
1:1 OR STEEPER	5'	10'	15'	20'
2:1	10'	20'	30'	40'
3:1	15'	30'	45'	60'
4:1 OR FLATTER	20'	40'	60'	80'

* ADJUSTMENTS CAN BE MADE FOR SOIL TYPE:
SOFT, LOAMY SOILS-ADJUST ROWS CLOSER TOGETHER;
HARD, ROCKY SOILS- ADJUST ROWS FARTHER APART



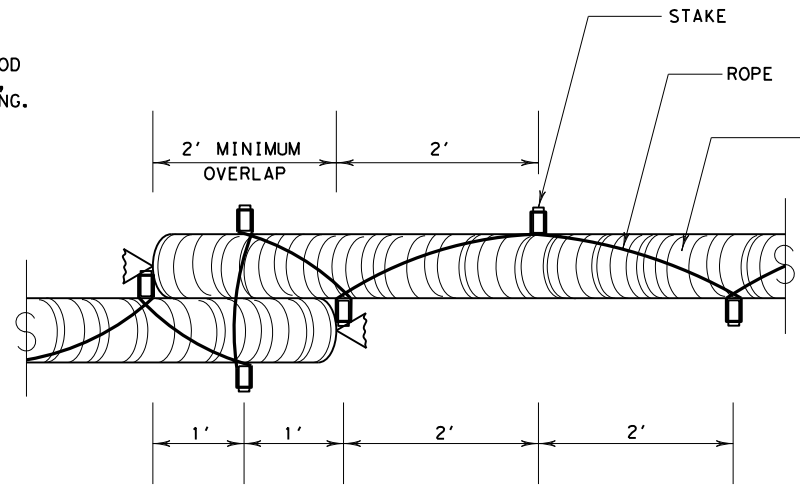
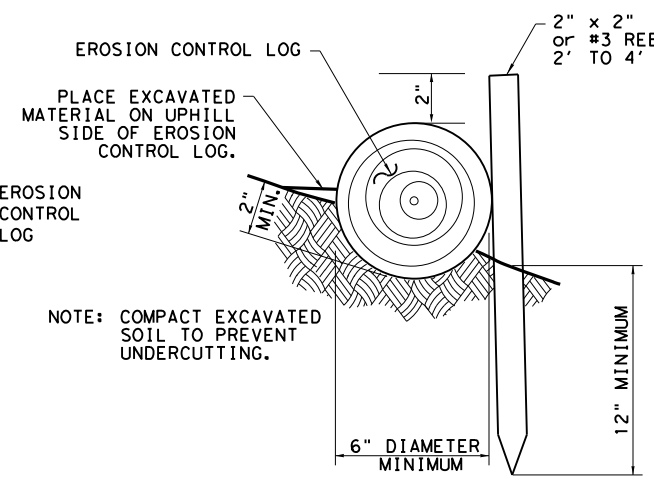
**EROSION CONTROL LOGS ON SLOPES
STAKE AND LASHING ANCHORING**

CL-SSL



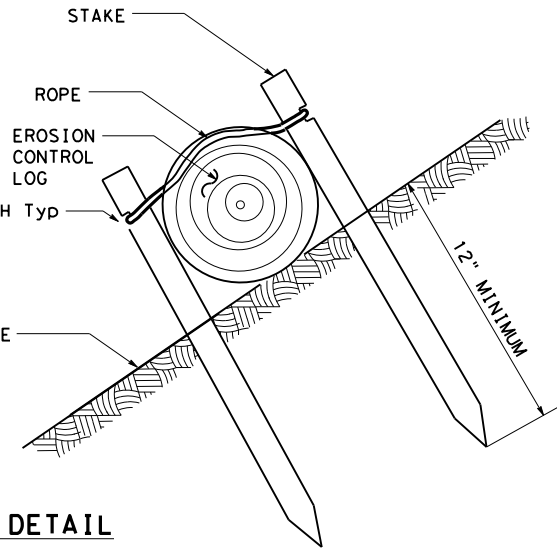
STAKE AND TRENCHING ANCHORING DETAIL

CL-SST



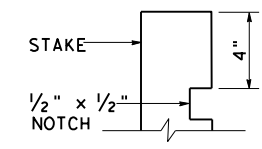
STAKE AND LASHING ANCHORING DETAIL

CL-SSL



LOG DIAMETER	DEPTH
6"	2"
8"	3"
12"	4"
18"	5"

TRENCH DEPTH TABLE

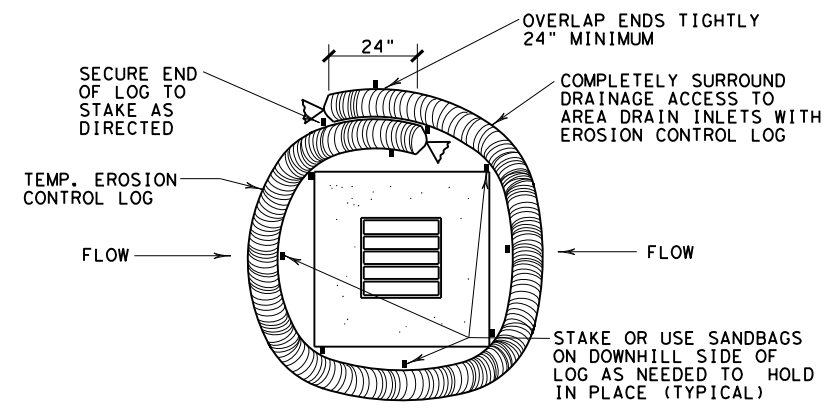


STAKE NOTCH DETAIL

SHEET 2 OF 3

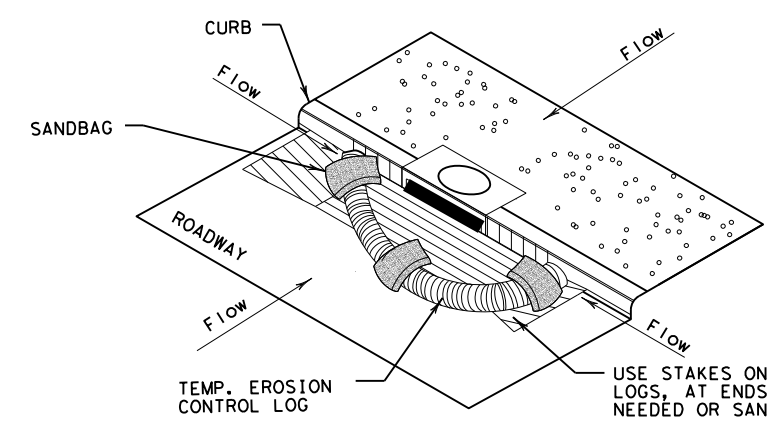
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TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec116	DN: TxDOT	CK: KM	DW: LS/PT
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REVISIONS	0008	15	059
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FTW	TARRANT	49	

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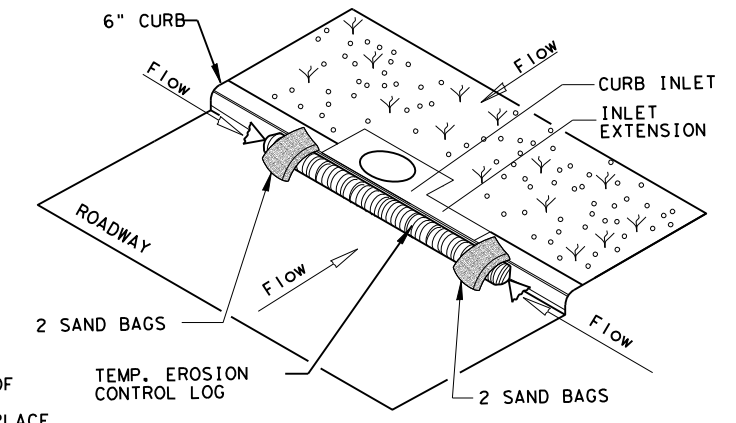
EROSION CONTROL LOG AT DROP INLET

CL-DI



EROSION CONTROL LOG AT CURB INLET

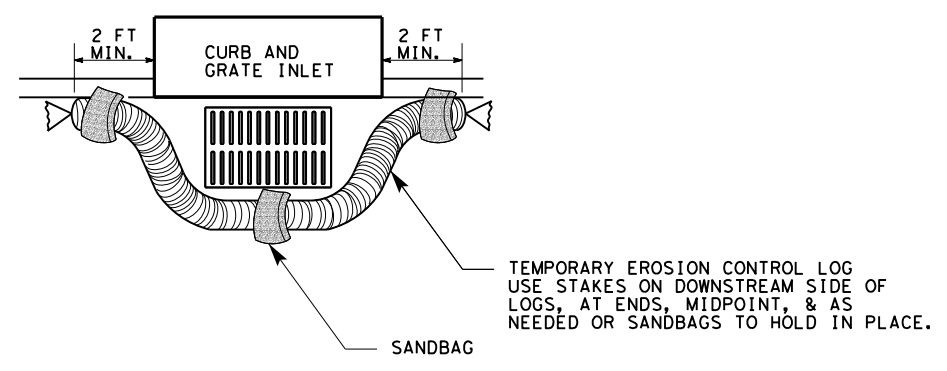
CL-CI



EROSION CONTROL LOG AT CURB INLET

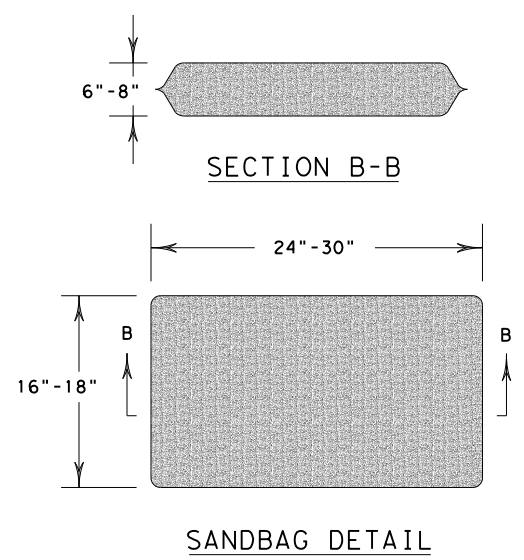
CL-CI

NOTE:
EROSION CONTROL LOGS USED AT CURB INLETS SHOULD ONLY BE USED IF THEY WILL NOT IMPEDE TRAFFIC OR FLOOD THE ROADWAY OR WHEN THE STORM SEWER SYSTEM IS NOT FULLY FUNCTIONAL.



EROSION CONTROL LOG AT CURB & GRADE INLET

CL-GI



SANDBAG DETAIL

SHEET 3 OF 3

		<i>Design Division Standard</i>	
TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES EROSION CONTROL LOG EC (9) - 16			
FILE: ec916	DN: TxDOT	CK: KM	DW: LS/PT
© TxDOT: JULY 2016	CONT: 0008	SECT: 15	JOB: 059
REVISIONS			HIGHWAY: IH 820
	DIST: FTW	COUNTY: TARRANT	SHEET NO.: 50

DATE:
FILE: