

INDEX OF SHEETS

SEE SHEET NO. 2

**STATE OF TEXAS
DEPARTMENT OF TRANSPORTATION**

**PLANS OF PROPOSED
STATE HIGHWAY IMPROVEMENT**

ROUTINE MAINTENANCE CONTRACT RMC638349001

US 83,ETC
MENARD

NET LENGTH OF PROJECT = 0.001 MI

LIMITS: VARIOUS HIGHWAYS IN MENARD AND GLASSCOCK COUNTIES

FOR THE CONSTRUCTION OF SIGN REPLACEMENT
CONSISTING OF SIGN REPLACEMENT

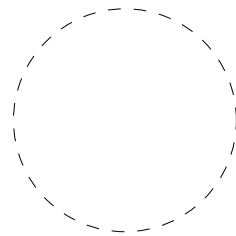
ROUTINE MAINTENANCE CONTRACT PROJECT NUMBER			
RMC638349001			
CONT	SECT	JOB	HIGHWAY
6383	49	001	US 83,ETC
DIST	COUNTY		SHEET NO.
SJT	MENARD		1

FINAL PLANS	
Letting Date:	_____
Name of Contractor:	_____
Date Work Began:	_____
Date Work Completed:	_____
Date Work Accepted:	_____
Final Contract Cost:	_____

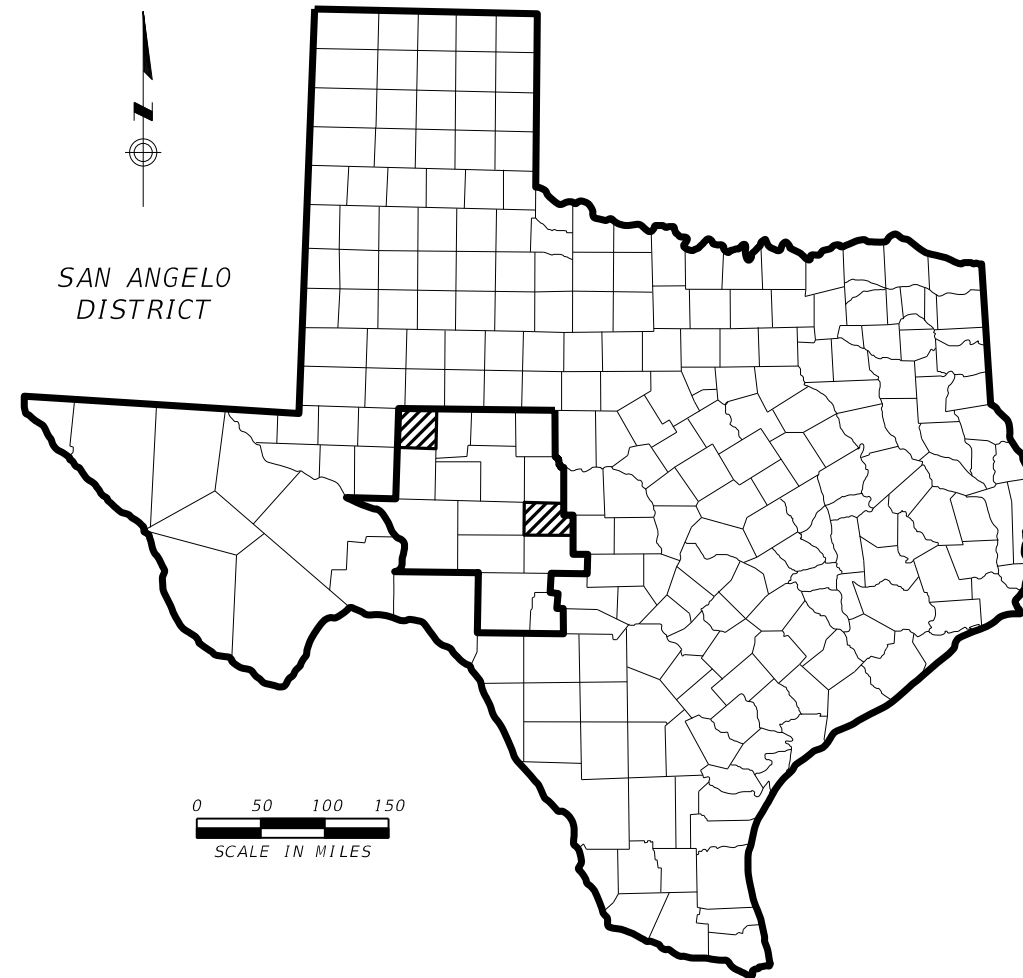
Project was built according to the Plans & Specifications.
These final plans reflect the work done and the quantities
shown thereon and on the Final Estimate are Final Quantities.

Area Engineer

Date



Summary of Change Orders:



EXCEPTIONS
NONE

EQUATIONS
NONE

RAILROAD CROSSINGS
NONE

SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION
NOVEMBER 1, 2014 AND SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS
SHALL GOVERN ON THIS PROJECT: SPECIAL LABOR PROVISIONS FOR STATE
PROJECTS (000---008).

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RECOMMENDED FOR LETTING:
DocuSigned by:

Ruben Carrasco
District Traffic Engineer

APPROVED FOR LETTING:
DocuSigned by:

[Signature], P.E.
District Director of Operations
419BB3F968D54CF...

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5	GENERAL NOTES
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77-78	REPLACE SIGNS FM 821 GLASSCOCK COUNTY
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85-86	REPLACE SIGNS RM 1357 GLASSCOCK COUNTY
87-90	REPLACE SIGNS RM 2401 GLASSCOCK COUNTY
91	REPLACE SIGNS RM 2093 GLASSCOCK COUNTY
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# 93-95	SMD(SLIP-1)-08 THRU SMD (SLIP-3)
# 96-98	TSR(3)-13 THRU TSR(5)-13
<u>ENVIRONMENTAL ISSUES</u>	
99	ENVIRONMENTAL PERMITS, ISSUES AND COMMITMENTS

THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ABOVE BY A # HAVE BEEN ISSUED BY ME AND ARE APPLICABLE TO THIS PROJECT.



23.06.2021

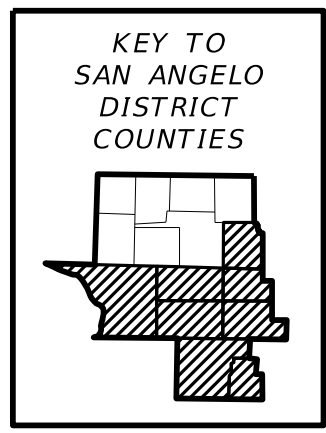
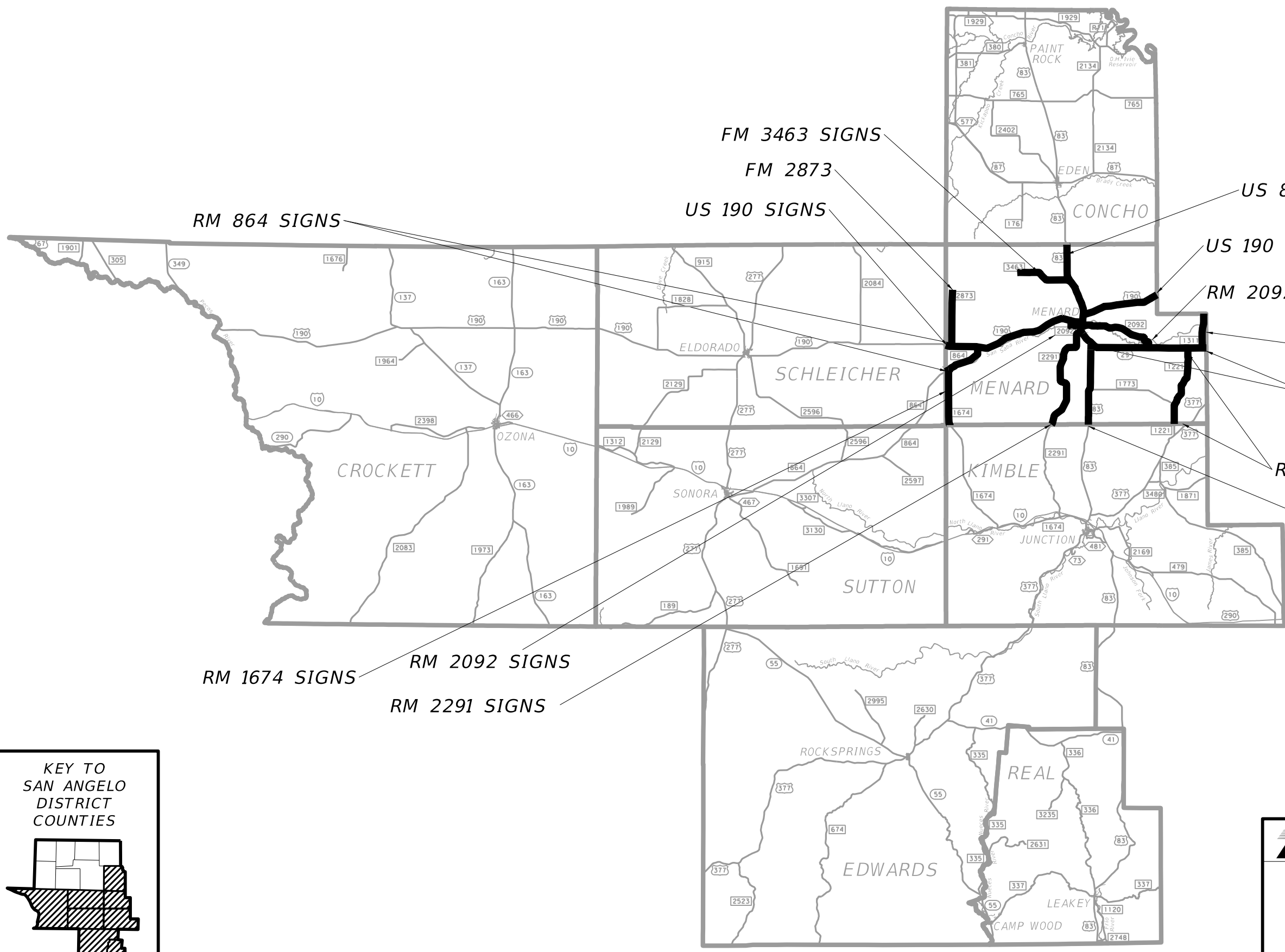
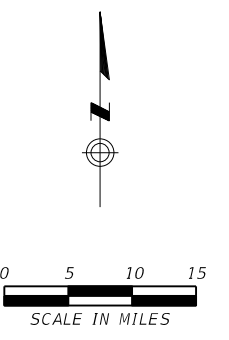


INDEX OF SHEETS

SHEET 1 OF 1 NOT TO SCALE

© TxDOT 2021 <small>SHEET ISSUED OR LAST REVISED</small> 06-21	<small>CONT</small> 6383	<small>SECT</small> 49	<small>JOB</small> 001	<small>HIGHWAY</small> US 83,ETC
	<small>DIST</small> SJT	<small>COUNTY</small> MENARD	<small>SHEET NO.</small> 2	

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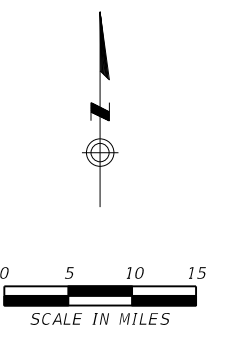


**LOCATION MAP
 JUNCTION AREA**

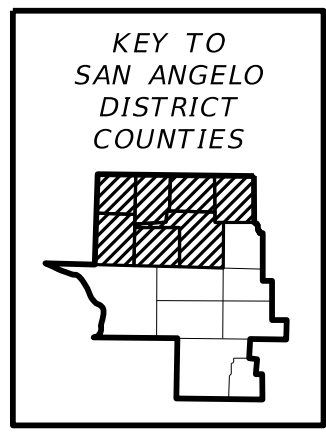
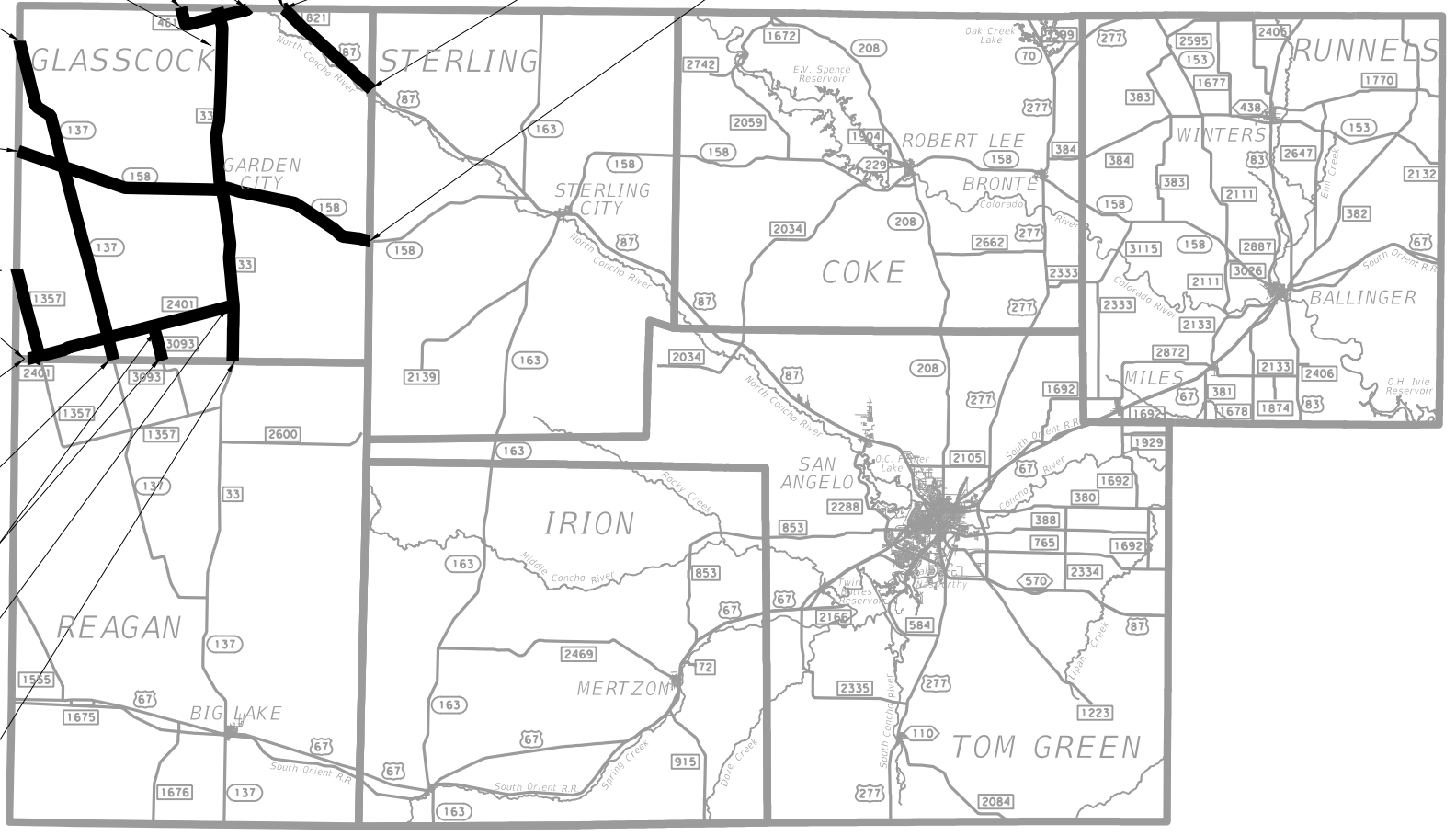
SHEET 1 OF 1 SCALE 1"=15 MILES

©TXDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83,ETC
12-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	3	

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RM 821 SIGNS
 US 87 SIGNS
 RM 461 SIGNS
 SH 158 SIGNS
 RM 33 SIGNS
 SH 137 SIGNS
 SH 158 SIGNS
 RM 1357 SIGNS
 RM 2401 SIGNS
 SH 137 SIGNS
 RM 3093 SIGNS
 RM 2401 SIGNS
 RM 33 SIGNS



23.06.2021

		San Angelo District	
<h3>LOCATION MAP SAN ANGELO AREA</h3>			
SHEET 1 OF 1		SCALE 1"=15 MILES	
©TxDOT 2021 <small>SHEET ISSUED OR LAST REVISED</small>	CONT 6383	SECT 49	JOB 001 HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 4

County: MENARD

Highway: US 83, ETC.

Control: 6383-49-001

GENERAL NOTES

The following Standard Sheets have been modified: None

Locate the project bulletin board at an approved location within the project limits such as at a field office, staging area, or stockpile, and make accessible to the public at all times. Do not remove the bulletin board from the project until approved. If a construction site notice is required for the project, post a copy at each geographically separated work location.

Will McLane, P.E.; email SJT_PreliminaryReview@txdot.gov

Contractor questions will be accepted through email, phone, and in person by the above individual.

All contractor questions will be reviewed by the Engineer. Once a response is developed, it will be posted to TxDOT's Public FTP at the following Address: <https://ftp.dot.state.tx.us/pub/txdot-info/Pre-Letting%20Responses/>

All questions submitted that generate a response will be posted through this site. The site is organized by District, Project Type (Construction or Maintenance), Letting Date, CCSJ/Project Name.

Item 6, "Control of Materials"

Access the work area from the right of way.

Item 8, "Prosecution and Progress"

Submit the sequence of work and estimated progress schedule on paper or as a Portable Document Format (PDF) electronic file compatible with Adobe Systems Incorporated "Acrobat Reader XI".

Item 9, "Measurement and Payment"

The progress payment period shall end two working days before the last working day of the month. Deliver invoices to be paid as material on hand on or before the end of the progress payment period.

Item 502, "Barricades, Signs and Traffic Handling"

The Contractor Force Account "Safety Contingency" that has been established for this project is intended to be utilized for work zone enhancements, to improve the effectiveness of the Traffic Control Plan, that could not be foreseen in the project planning and design stage. These enhancements will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent traffic management reviews on the project. The Engineer may choose to use existing bid items if it does not slow the implementation of enhancement.

County: MENARD

Highway: US 83, ETC.

Control: 6383-49-001

Item 506, "Temporary Erosion, Sedimentation, and Environmental Controls"

The project is exempt from the Texas Pollutant Discharge Elimination System (TPDES) General Permit (TXR150000). Exempt projects are those that disturb less than one acre or routine maintenance activities that maintain the original line and grade, hydraulic capacity, or original purposes of the site. No temporary erosion control measures or Storm Water Pollution Prevention Plan (SW3P) have been included in the plans.

Item 636, "Signs"

Install the prismatic sheeting for overhead signs material to within 30 degrees of the manufacturer-specified orientation.

Before removal from the project site, spray-paint (with an oil-based paint), an "X" across the face of non-salvageable signs as directed.

Item 644, "Small Roadside Sign Assemblies"

Furnish and install omni-directional sign post wrap (12 in. by 12 in. Type C retroreflective sheeting with pressure sensitive backing) on sign posts that have sign faces that do not face the predominant direction of traffic, as directed. Sign post wrap shall be yellow for signs R6-1 "ONE WAY" and shall be red for signs R1-2 "YIELD", R5-1 "DO NOT ENTER", R5-1a "WRONG WAY", and R1-1 "STOP". Place the bottom of sign post wrap a height of 4 ft. above the edge of travel lane.

Signs indicated to be mounted on the back of another sign may require punch spacing different from that shown on the Standard Sheets. Adjust punch spacing on affected signs.

The replacement supports are from the top of existing foundation support to the top of the sign. The top slip base support shall be the South Plains Style or as approved by the Engineer. This contract has no foundation work included.



CONTROLLING PROJECT ID 6383-49-001

DISTRICT San Angelo
HIGHWAY US0083

COUNTY Menard

QUANTITY SHEET

CONTROL SECTION JOB				6383-49-001		TOTAL EST.	TOTAL FINAL
PROJECT ID				A00178221			
COUNTY				Menard			
HIGHWAY				US0083			
ALT	BID CODE	DESCRIPTION	UNIT	EST.	FINAL		
	500-6001	MOBILIZATION	LS	100.00%		100.00%	
	502-6001	BARRICADES, SIGNS AND TRAFFIC HANDLING	MO	4.000		4.000	
	636-6001	ALUMINUM SIGNS (TY A)	SF	4,275.510		4,275.510	
	636-6002	ALUMINUM SIGNS (TY G)	SF	21.250		21.250	
	644-6103	REPLACE SRS & S TY10BWG(1) (P)	EA	85.000		85.000	
	644-6104	REPLACE SRS & S TY10BWG(1) (T)	EA	57.000		57.000	
	644-6106	REPLACE SRS & S TY10BWG(1) (U)	EA	8.000		8.000	
	644-6111	REPLACE SRS & S TYS80 (1) (U-WC)	EA	11.000		11.000	
	644-6112	REPLACE SRS & S TYS80 (1) (U-EXAL)	EA	3.000		3.000	
	6185-6002	TMA (STATIONARY)	DAY	75.000		75.000	
	08	SAFETY CONTINGENCY (NON-PART)	LS	1.000		1.000	

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SUMMARY OF WORKZONE TRAFFIC CONTROL ITEMS		
LOCATION	502 6001	6185 6005
	BARRICADES, SIGNS AND TRAFFIC HANDLING	TMA (MOBILE OPERATION)
	MO	DAY
Project	4	75
PROJECT TOTALS	4	75

SUMMARY OF SIGNING ITEMS							
LOCATION	636 6007	636 6008	644 6103	644 6104	644 6106	644 6111	644 6112
	REPLACE EXISTING ALUMINUM SIGNS(TY A)	REPLACE EXISTING ALUMINUM SIGNS(TY G)	REPLACE SRS & S TY10BWG (1) (P)	REPLACE SRS & S TY10BWG (1) (T)	REPLACE SRS & S TY10BWG (1) (U)	REPLACE SRS & S TYS80 (1) (U-WC)	REPLACE SRS & S TYS80 (1) (U-EXAL)
	SF	SF	EA	EA	EA	EA	EA
SIGNS US 83 MENARD COUNTY SHEET 1 OF 8	332.15		5	5	1		
SIGNS US 83 MENARD COUNTY SHEET 2 OF 8	300.90		4	2	2	2	
SIGNS US 83 MENARD COUNTY SHEET 3 OF 8	47.70						
SIGNS US 190 MENARD COUNTY SHEET 1 OF 6	299.02		1	9			
SIGNS US 190 MENARD COUNTY SHEET 2 OF 6	153.37			6			
SIGNS SH 29 MENARD COUNTY SHEET 1 OF 5	281.14		7	2	3	1	
SIGNS SH 29 MENARD COUNTY SHEET 2 OF 5	55.20		0	2	0	1	
SIGNS RM 864 & RM 1674 MENARD COUNTY SHEET 1 OF 3	210.70		4	3	1		
SIGNS FM 2873 & FM 3463 MENARD COUNTY SHEET 1 OF 2	112.20		5				
SIGNS RM 1221 MENARD COUNTY SHEET 1 OF 3	211.48		2	1	1		
SIGNS FM 1311 MENARD COUNTY SHEET 1 OF 2	80.37		3	2			
SIGNS RM 2092 MENARD COUNTY SHEET 1 OF 4	192.10		5	1			
SIGNS RM 2291 MENARD COUNTY SHEET 1 OF 4	214.26		2	5		1	
SIGNS US 87 GLASSCOCK COUNTY SHEET 1 OF 4	387.52		4	3			
SIGNS US 87 GLASSCOCK COUNTY SHEET 2 OF 4	51.34		1	1			
SIGNS SH 137 GLASSCOCK COUNTY SHEET 1 OF 3	111.92		5			1	
SIGNS SH 158 GLASSCOCK COUNTY SHEET 1 OF 4	273.12		3			2	
SIGNS FM 461 GLASSCOCK COUNTY SHEET 1 OF 2	88.26	21.25	4	2			
SIGNS FM 821 GLASSCOCK COUNTY SHEET 1 OF 2	46.62			2			
SIGNS RM 33 GLASSCOCK COUNTY SHEET 1 OF 6	269.84		11	1		1	2
SIGNS RM 33 GLASSCOCK COUNTY SHEET 2 OF 6	169.94		3			2	
SIGNS RM 1357 GLASSCOCK COUNTY SHEET 1 OF 2	62.57		4	4			
SIGNS RM 2401 GLASSCOCK COUNTY SHEET 1 OF 4	241.77		8	5			
SIGNS RM 2401 GLASSCOCK COUNTY SHEET 2 OF 4	48.20		2				1
SIGNS RM 3093 GLASSCOCK COUNTY SHEET 1 OF 2	33.82		2	1			
PROJECT TOTALS	4,275.51	21.25	85.00	57.00	8.00	11.00	3.00



23.06.2021

		San Angelo District	
<h2>QUANTITY SUMMARY</h2>			
SHEET 1 OF 1		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
			HIGHWAY US 83,ETC SHEET NO. 7

TCP NOTES:

1. USE TCP (1-1)-18 FOR ALL SIGNS IN AREAS OF SPEED LIMITS 45 MPH OR HIGHER WHERE ALL WORK VEHICLES ARE OFF THE SHOULDER OR ANY VEHICLE IS ON THE THE SHOULDER.
2. USE TCP (1-2b)-18 FOR ALL SIGNS IN AREAS OF SPEED LIMITS 45 MPH OR HIGHER WHERE ANY WORK VEHICLE IS IN THE TRAVEL LANE ON A TWO LANE HIGHWAY.
3. USE TCP (1-4a)-18 FOR ALL SIGNS IN AREAS OF SPEED LIMITS 45 MPH OR HIGHER WHERE ANY WORK VEHICLE IS IN THE TRAVEL LANE ON A FOUR LANE HIGHWAY.
4. USE TCP (1-5)-18 FOR ALL SIGNS IN AREAS OF SPEED LIMITS 45 OR HIGHER WHERE ANY WORK VEHICLES ARE IN THE LANE OF 4 LANE DIVIDED HIGHWAY.
5. USE TCP (5-1) FOR ALL SIGNS IN AREAS OF SPEED LIMITS 45 OR HIGHER WHERE ANY WORK VEHICLES ARE ON THE SHOULDER OF A FREEWAY OR EXPRESSWAY.
6. USE TCP (3-4)-13 FOR ALL SIGNS IN AREAS OF SPEED LIMITS 45 MPH OR LOWER WHERE ANY WORK VEHICLE IS IN THE TRAVEL LANE AND WORK IS LESS THAN 15 MINUTES. USE FLAGGERS TO DIRECT ANY BLOCKED TRAFFIC AROUND WORK VEHICLES.

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23.06.2021

Texas Department of Transportation		San Angelo District	
<h2 style="margin: 0;">TCP NOTES</h2>			
SHEET 1 OF 1		NOT TO SCALE	
©TxDOT 2021	CONT	SECT	JOB
SHEET ISSUED OR LAST REVISED	6383	49	001
2-20	DIST	COUNTY	SHEET NO.
SJT	MENARD	8	8

GENERAL NOTES

1. When a contractor force account "Safety Contingency" has been established for the project, it is for work zone enhancements that were unforeseen in the project planning and design stage, but would improve the effectiveness of the traffic control plan. These enhancements will be mutually agreed upon by the Engineer and the Contractor's Responsible Person based on weekly or more frequent traffic management reviews on the project. The Engineer may choose to use existing bid items if doing so does not slow implementation of work zone enhancements.
2. Shadow, lead, trail, and ramp control vehicles shown on the plans are required.
3. Use high level warning flags on advance warning signs during daytime operations.
4. Provide flaggers at such times and locations as directed to ensure the safe passage of traffic through construction areas. When flaggers are used to control traffic, furnish and install signs CW20-7 "FLAGGER SYMBOL", CW20-7aD "FLAGGER AHEAD", and CW3-4 "BE PREPARED TO STOP". Flaggers shall use 24 in. STOP/SLOW paddles.
5. Temporarily relocate existing mailbox assemblies on portable mailbox stands as shown on the plans, or as directed. Use materials conforming to the Compliant Work Zone Traffic Control Device List (CWZTCDL).
6. Prior to each work day, make provisions to exclude vehicles from parking within work areas.
7. Temporarily relocate existing permanent sign assemblies to temporary supports as shown on the plans, or as directed.
8. Omit advance warning signs and furnish and install reduced size signs CW20-1 "ROAD WORK AHEAD" mounted back to back with reduced size signs G20-2 "END ROAD WORK" signs at intersecting city streets and county roads.
9. Furnish and install signs CW20-1D "ROAD WORK AHEAD", G20-1aT "ROAD WORK ←NEXT X MILES, NEXT X MILES→", and G20-2 "END ROAD WORK" at intersecting state highways.
10. Sign and buffer spacing may be altered to fit field conditions, as directed.
11. In addition to providing a Contractor's Responsible Person and a phone number for emergency contact, have employee(s) available to respond on the project for emergencies and for taking corrective measures within 30 minutes.
12. Cones may be used as the typical channelizing device for freeway surfacing projects.
13. 28 in. tall cones will be allowed only for short duration or short term stationary operations when workers are present to maintain the devices upright and in proper location. Intermediate term stationary work areas should use drums, vertical panels, or 42 in. tall two-piece cones.
14. All construction signs and barricades placed during any phase of work shall remain in place until removal is approved by the Engineer.
15. The Engineer may direct the Contractor to furnish additional signs and barricades as required to maintain traffic flow, detours and motorist safety during construction.
16. Warning signs for long term stationary work should be mounted at 7 ft. to the bottom of the sign.
17. For long term stationary work at night, floodlights should be used to illuminate the work area and equipment crossings. Floodlights shall not produce a disabling glare condition for road users or workers.
18. All motor vehicle equipment having an obstructed view to the rear shall have a reverse signal alarm audible above the surrounding noise level.
19. Traffic control devices denoted with the triangle symbol on the plans may be omitted.
20. When sheet WZ(RS) is included in the plans, furnish and install temporary rumble strips. Do not use temporary rumble strips on freeways or expressways.
21. When sheet WZ(BRK) is included in the plans, furnish and install signs CW21-1T "GIVE US A BRAKE".
22. Flags attached to signs shown in the plans are required.
23. Signs END ROAD WORK (G20-2) may be omitted when conflicting with G20-2 signs already in place on the project.
24. The Engineer will determine advisory speeds to be shown on plaques CW13-1P.

TRUCK MOUNTED ATTENUATOR REQUIREMENTS

Provide the number of vehicles with truck mounted attenuators listed in the table below. The Contractor shall determine if multiple operations will occur at the same time, to determine the total number of truck mounted attenuators needed for the project.

TCP(1-1)	1	TCP(2-4)	0	TCP(6-1)	0
TCP(1-2)	1	TCP(2-5)	0	TCP(6-2)	0
TCP(1-3)	0	TCP(2-6)	0	TCP(6-3)	0
TCP(1-4)	1	TCP(3-1)	0	TCP(6-4)	0
TCP(1-5)	1	TCP(3-2)	0	TCP(6-5)	0
TCP(1-6)	0	TCP(3-3)	0	TCP(6-8)	0
TCP(2-1)	0	TCP(3-4)	1	TCP(6-9)	0
TCP(2-2)	0	TCP(3-5)	0	WZ(BTS-1)	0
TCP(2-3)	0	TCP(5-1)	1		
TRAFFIC CONTROL PLAN PILOT VEHICLE OPERATION					0
TRAFFIC CONTROL PLAN TWO LANE CLOSURES ON FOUR LANE UNDIVIDED HIGHWAYS					0
TRAFFIC CONTROL PLAN LANE CLOSURES WITH BARRIER					0
TRAFFIC CONTROL PLAN SHOULDER CLOSURES WITH BARRIER					0
TRAFFIC CONTROL PLAN WORK SPACE NEAR SHOULDER					0
TRAFFIC CONTROL PLAN CROSSOVER CLOSURE					0
TRAFFIC CONTROL PLAN TURNAROUND CLOSURE					0
TRAFFIC CONTROL PLAN LANE CLOSURES WITH TRAFFIC SIGNAL AND BARRIER					0
TRAFFIC CONTROL PLAN LANE CLOSURES WITH TRAFFIC SIGNAL					0

PORTABLE CHANGEABLE MESSAGE SIGN REQUIREMENTS

Provide the portable changeable message signs listed in the table below. The Contractor shall determine if multiple operations will occur at the same time, to determine the total number of portable changeable message signs needed for the project.

TCP(6-1)	0	TCP(6-3)	0	TCP(6-8)	0
TCP(6-2)	0	TCP(6-4)	0	TCP(6-9)	0
TRAFFIC CONTROL PLAN LANE CLOSURES WITH BARRIER					0
TRAFFIC CONTROL PLAN SHOULDER CLOSURES WITH BARRIER					0
TRAFFIC CONTROL PLAN LANE CLOSURES WITH TRAFFIC SIGNAL AND BARRIER					0
TRAFFIC CONTROL PLAN LANE CLOSURES WITH TRAFFIC SIGNAL					0

TYPICAL USAGE

MOBILE
Work that moves continuously or intermittently (stopping for up to approximately 15 minutes).

SHORT DURATION
Work that occupies a location up to 1 hour.

SHORT TERM STATIONARY
Daytime work that occupies a location for more than 1 hour in a single daylight period.

INTERMEDIATE TERM STATIONARY
Work that occupies a location more than one daylight period up to 3 days, or nighttime work lasting more than 1 hour.

LONG TERM STATIONARY
Work that occupies a location more than 3 days.



23.06.2021

		San Angelo District	
TRAFFIC CONTROL PLAN GENERAL REQUIREMENTS			
SHEET 1 OF 1		NOT TO SCALE	
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05-18	DIST SJT	COUNTY MENARD	SHEET NO. 9

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BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES:

- The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction pavement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.
- The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.
- The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer.
- Geometric design of lane shifts and detours should, when possible, meet the applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.
- When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.
- The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.
- All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.
- The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.
- Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES, CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.
- Traffic control devices should be in place only while work is actually in progress or a definite need exists.
- The Engineer has the final decision on the location of all traffic control devices.
- Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

WORKER SAFETY NOTES:


- Workers on foot who are exposed to traffic or to construction equipment within the right-of-way shall wear high-visibility safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel," or equivalent revisions, and labeled as ANSI 107-2004 standard performance for Class 2 or 3 risk exposure. Class 3 garments should be considered for high traffic volume work areas or night time work.
- Except in emergency situations, flagger stations shall be illuminated when flagging is used at night.

COMPLIANT WORKZONE TRAFFIC CONTROL DEVICES

- Only pre-qualified products shall be used. The "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products and their sources.
- Work zone traffic control devices shall be compliant with the Manual for Assessing safety Hardware (MASH).

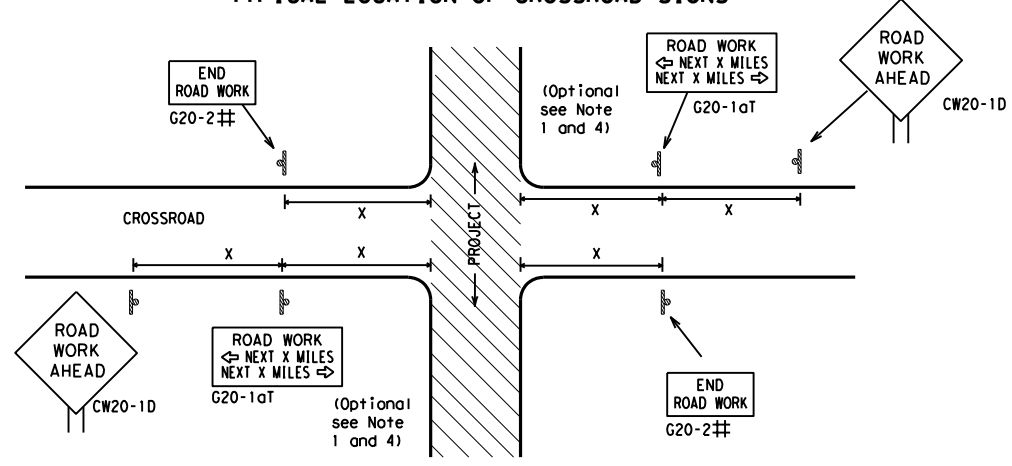
THE DOCUMENTS BELOW CAN BE FOUND ON-LINE AT http://www.txdot.gov
COMPLIANT WORK ZONE TRAFFIC CONTROL DEVICES LIST (CWZTCD)
DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS)
MATERIAL PRODUCER LIST (MPL)
ROADWAY DESIGN MANUAL - SEE "MANUALS (ONLINE MANUALS)"
STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS (SHSD)
TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD)
TRAFFIC ENGINEERING STANDARD SHEETS

SHEET 1 OF 12

 Texas Department of Transportation		Traffic Safety Division Standard	
BARRICADE AND CONSTRUCTION GENERAL NOTES AND REQUIREMENTS			
BC (1) - 21			
FILE: bc-21.dgn	DN: TxDOT	CR: TxDOT	DW: TxDOT
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9-07 8-14	DIST	COUNTY	SHEET NO.
5-10 5-21	SJT	MENARD	10

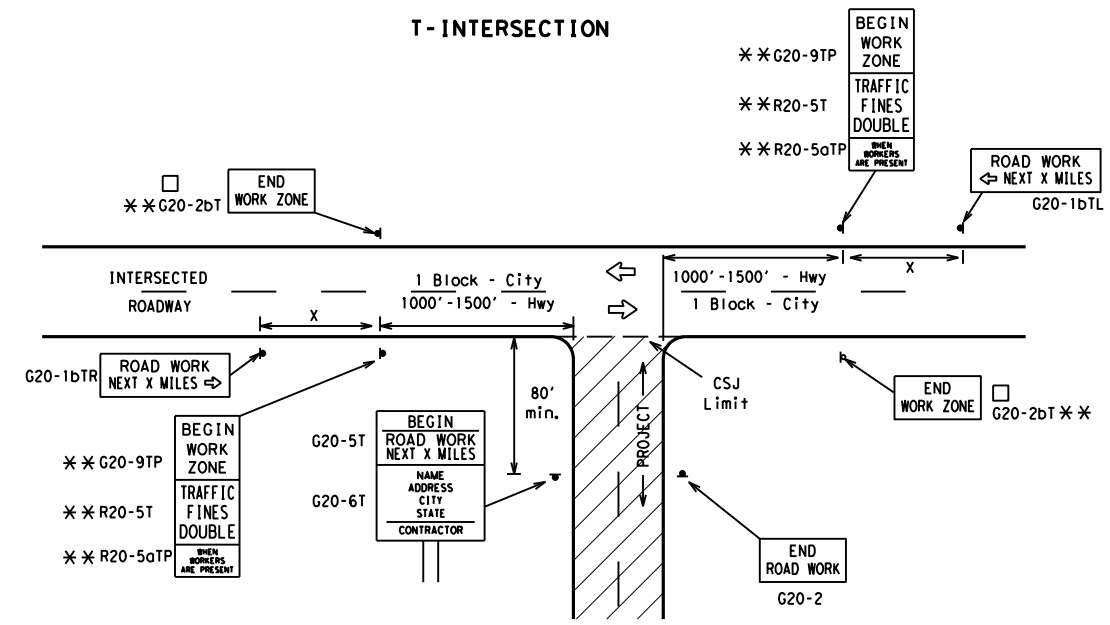
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TYPICAL LOCATION OF CROSSROAD SIGNS



- ## May be mounted on back of "ROAD WORK AHEAD" (CW20-1D) sign with approval of Engineer. (See note 2 below)
- The typical minimum signing on a crossroad approach should be a "ROAD WORK AHEAD" (CW20-1D) sign and a (G20-2) "END ROAD WORK" sign, unless noted otherwise in plans.
 - The Engineer may use the reduced size 36" x 36" ROAD WORK AHEAD (CW20-1D) sign mounted back to back with the reduced size 36" x 18" "ROAD WORK NEXT X MILES" (G20-2) sign on low volume crossroads (see Note 4 under "Typical Construction Warning Sign Size and Spacing"). See the "Standard Highway Sign Designs for Texas" manual for sign details. The Engineer may omit the advance warning signs on low volume crossroads. The Engineer will determine whether a road is low volume as per TMUTCD Part 5. This information shall be shown in the plans.
 - Based on existing field conditions, the Engineer/Inspector may require additional signs such as FLAGGER AHEAD, LOOSE GRAVEL, or other appropriate signs. When additional signs are required, these signs will be considered part of the minimum requirements. The Engineer/Inspector will determine the proper location and spacing of any sign not shown on the BC sheets, Traffic Control Plan sheets or the Work Zone Standard Sheets.
 - The "ROAD WORK NEXT X MILES" (G20-1aT) sign shall be required at high volume crossroads to advise motorists of the length of construction in either direction from the intersection. The Engineer will determine whether a roadway is considered high volume.
 - Additional traffic control devices may be shown elsewhere in the plans for higher volume crossroads.
 - When work occurs in the intersection area, appropriate traffic control devices, as shown elsewhere in the plans or as determined by the Engineer/Inspector, shall be in place.

T-INTERSECTION



CSJ LIMITS AT T-INTERSECTION

- The Engineer will determine the types and location of any additional traffic control devices, such as a flagger and accompanying signs, or other signs, that should be used when work is being performed at or near an intersection.
- If construction closes the road at a T-intersection, the Contractor shall place the "CONTRACTOR NAME" (G20-6T) sign behind the Type 3 Barricades for the road closure (see BC(10) also). The "ROAD WORK NEXT X MILES" left arrow (G20-1bTL) and "ROAD WORK NEXT X MILES" right arrow (G20-1bTR) signs shall be replaced by the detour signing called for in the plans.

TYPICAL CONSTRUCTION WARNING SIGN SIZE AND SPACING^{1,5,6}

Sign Number or Series	SIZE		SPACING	
	Conventional Road	Expressway/Freeway	Posted Speed MPH	Sign Δ Spacing "x" Feet (Apprx.)
CW20 ⁴	48" x 48"	48" x 48"	30	120
CW21			35	160
CW22			40	240
CW23			45	320
CW25			50	400
CW1, CW2, CW7, CW8, CW9, CW11, CW14	36" x 36"	48" x 48"	55	500 ²
CW3, CW4, CW5, CW6, CW8-3, CW10, CW12	48" x 48"	48" x 48"	60	600 ²
			65	700 ²
			70	800 ²
			75	900 ²
			80	1000 ²
			*	* ³

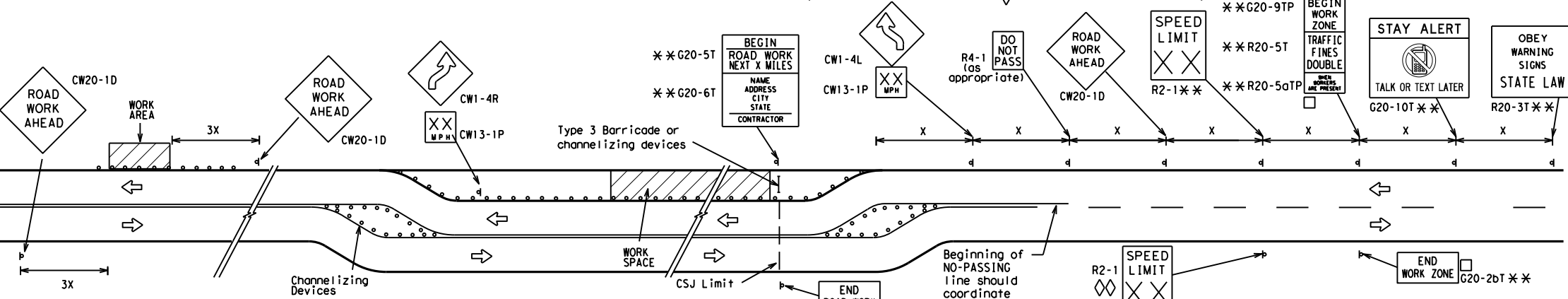
* For typical sign spacings on divided highways, expressways and freeways, see Part 6 of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) typical application diagrams or TCP Standard Sheets.

Δ Minimum distance from work area to first Advance Warning sign nearest the work area and/or distance between each additional sign.

GENERAL NOTES

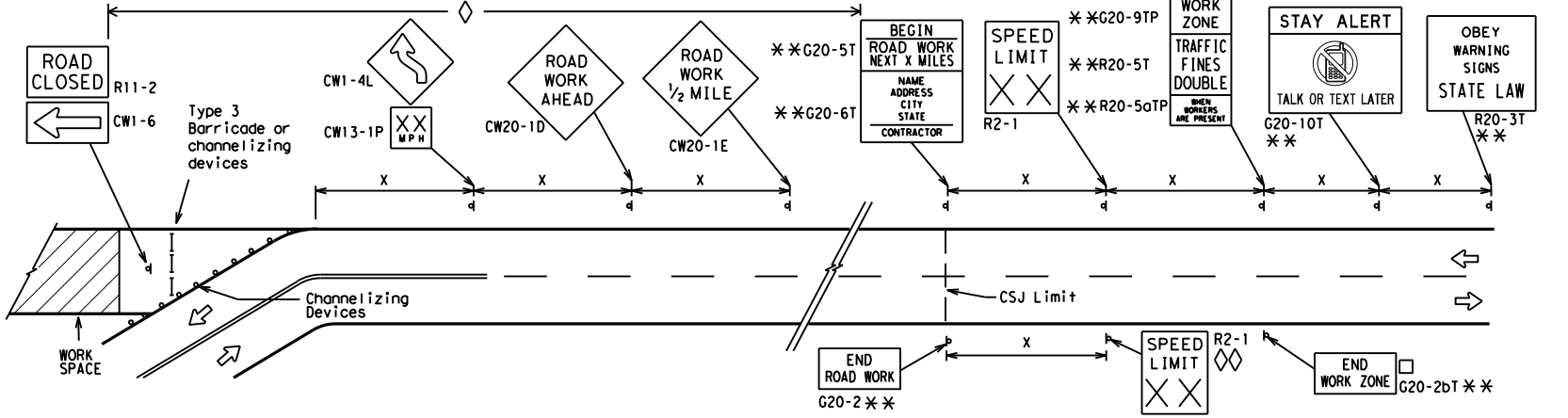
- Special or larger size signs may be used as necessary.
- Distance between signs should be increased as required to have 1500 feet advance warning.
- Distance between signs should be increased as required to have 1/2 mile or more advance warning.
- 36" x 36" "ROAD WORK AHEAD" (CW20-1D) signs may be used on low volume crossroads at the discretion of the Engineer as per TMUTCD Part 5. See Note 2 under "Typical Location of Crossroad Signs".
- Only diamond shaped warning sign sizes are indicated.
- See sign size listing in "TMUTCD", Sign Appendix or the "Standard Highway Sign Designs for Texas" manual for complete list of available sign design sizes.

WORK AREAS IN MULTIPLE LOCATIONS WITHIN CSJ LIMITS



When extended distances occur between minimal work spaces, the Engineer/Inspector should ensure additional "ROAD WORK AHEAD" (CW20-1D) signs are placed in advance of these work areas to remind drivers they are still within the project limits. See the applicable TCP sheets for exact location and spacing of signs and channelizing devices.

SAMPLE LAYOUT OF SIGNING FOR WORK BEGINNING DOWNSTREAM OF THE CSJ LIMITS



NOTES

- The Contractor shall determine the appropriate distance to be placed on the G20-1 series signs and "BEGIN ROAD WORK NEXT X MILES" (G20-5T) sign for each specific project. This distance shall replace the "x" and shall be rounded to the nearest whole mile with the approval of the Engineer. No decimals shall be used.
- The "BEGIN WORK ZONE" (G20-9TP) and "END WORK ZONE" (G20-2bT) shall be used as shown on the sample layout when advance signs are required outside the CSJ Limits. They inform the motorist of entering or leaving a part of the work zone lying outside the CSJ Limits where traffic fines may double if workers are present.
 - CSJ limit signing is required for highway construction and maintenance work, with the exception of mobile operations.
 - Area for placement of "ROAD WORK AHEAD" (CW20-1D) sign and other signs or devices as called for on the Traffic Control Plan.
 - Contractor will install a regulatory speed limit sign at the end of the work zone.

LEGEND

—	Type 3 Barricade
○ ○ ○	Channelizing Devices
■	Sign
X	See Typical Construction Warning Sign Size and Spacing chart or the TMUTCD for sign spacing requirements.

SHEET 2 OF 12



BARRICADE AND CONSTRUCTION PROJECT LIMIT

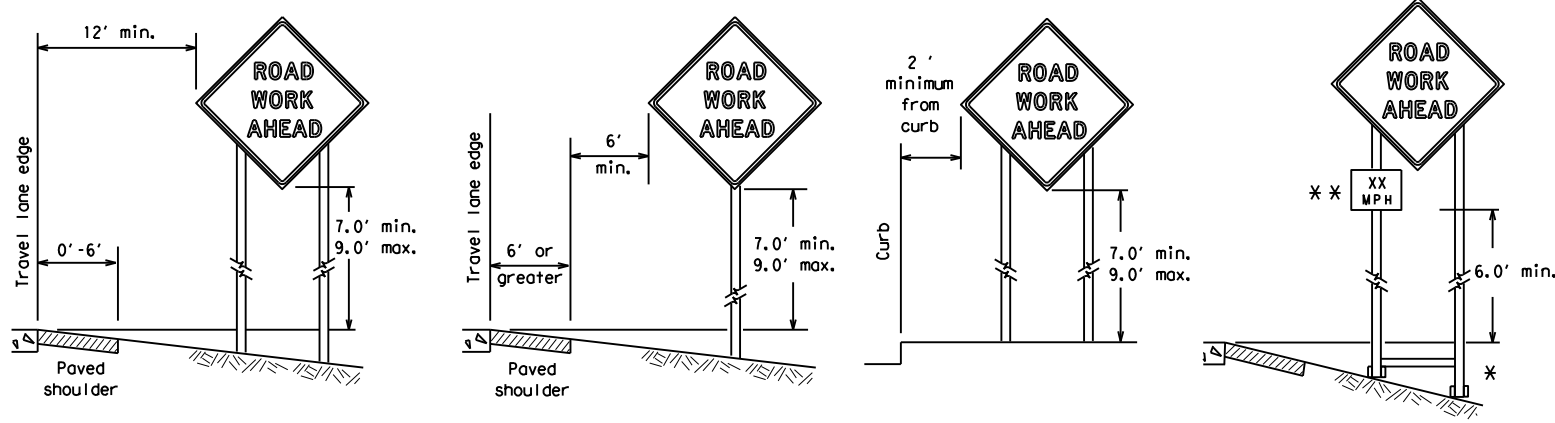
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© TxDOT November 2002	CONT	SECT	JOB	HIGHWAY
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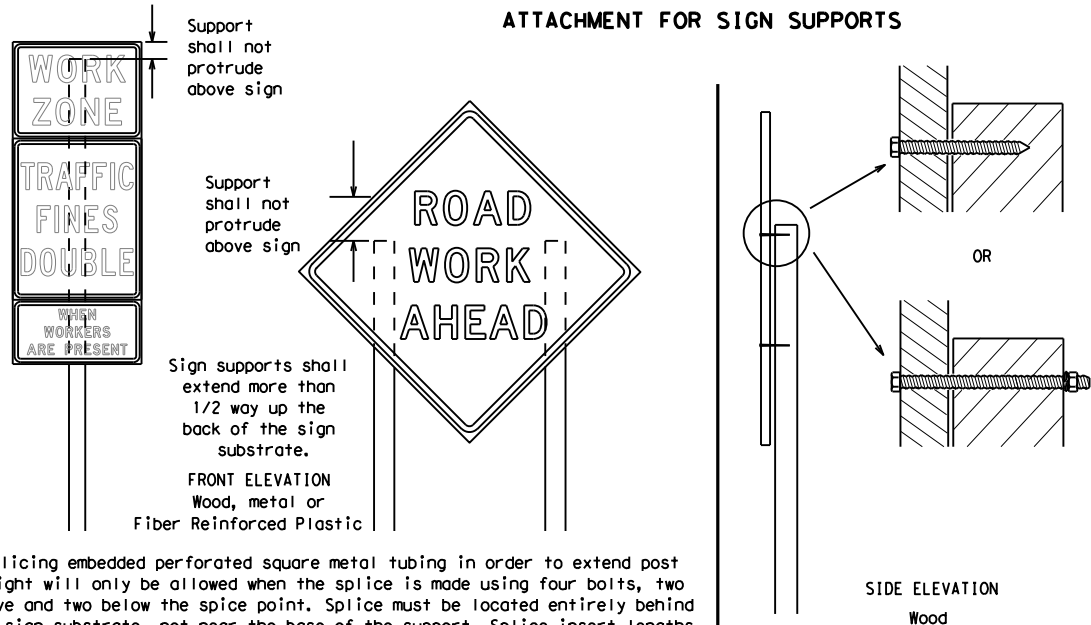
TYPICAL MINIMUM CLEARANCES FOR LONG TERM AND INTERMEDIATE TERM SIGNS



* When placing skid supports on unlevel ground, the leg post lengths must be adjusted so the sign appears straight and plumb. Objects shall NOT be placed under skids as a means of leveling.

** When plaques are placed on dual-leg supports, they should be attached to the upright nearest the travel lane. Supplemental plaques (advisory or distance) should not cover the surface of the parent sign.

ATTACHMENT FOR SIGN SUPPORTS



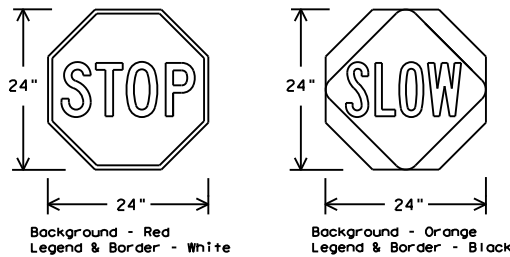
Attachment to wooden supports will be by bolts and nuts or screws. Use TxDOT's or manufacturer's recommended procedures for attaching sign substrates to other types of sign supports

Nails shall NOT be allowed.
 Each sign shall be attached directly to the sign support. Multiple signs shall not be joined or spliced by any means. Wood supports shall not be extended or repaired by splicing or other means.

Splicing embedded perforated square metal tubing in order to extend post height will only be allowed when the splice is made using four bolts, two above and two below the splice point. Splice must be located entirely behind the sign substrate, not near the base of the support. Splice insert lengths should be at least 5 times nominal post size, centered on the splice and of at least the same gauge material.

STOP/SLOW PADDLES

1. STOP/SLOW paddles are the primary method to control traffic by flaggers. The STOP/SLOW paddle size should be 24" x 24".
2. STOP/SLOW paddles shall be retroreflectORIZED when used at night.
3. STOP/SLOW paddles may be attached to a staff with a minimum length of 6' to the bottom of the sign.
4. Any lights incorporated into the STOP or SLOW paddle faces shall only be as specifically described in Section 6E.03 Hand Signaling Devices in the TMUTCD.



SHEETING REQUIREMENTS (WHEN USED AT NIGHT)		
USAGE	COLOR	SIGN FACE MATERIAL
BACKGROUND	RED	TYPE B OR C SHEETING
BACKGROUND	ORANGE	TYPE B _{FL} OR C _{FL} SHEETING
LEGEND & BORDER	WHITE	TYPE B OR C SHEETING
LEGEND & BORDER	BLACK	ACRYLIC NON-REFLECTIVE FILM

CONTRACTOR REQUIREMENTS FOR MAINTAINING PERMANENT SIGNS WITHIN THE PROJECT LIMITS

1. Permanent signs are used to give notice of traffic laws or regulations, call attention to conditions that are potentially hazardous to traffic operations, show route designations, destinations, directions, distances, services, points of interest, and other geographical, recreational, specific service (LOGO), or cultural information. Drivers proceeding through a work zone need the same, if not better route guidance as normally installed on a roadway without construction.
2. When permanent regulatory or warning signs conflict with work zone conditions, remove or cover the permanent signs until the permanent sign message matches the roadway condition. For details for covering large guide signs see the TS-CD standard.
3. When existing permanent signs are moved and relocated due to construction purposes, they shall be visible to motorists at all times.
4. If existing signs are to be relocated on their original supports, they shall be installed on crashworthy bases as shown on the SMD Standard sheets. The signs shall meet the required mounting heights shown on the BC Sheets or the SMD Standards. This work should be paid for under the appropriate pay item for relocating existing signs.
5. If permanent signs are to be removed and relocated using temporary supports, the Contractor shall use crashworthy supports as shown on the BC standard sheets, TLRs standard sheets or the CWZTCD list. The signs shall meet the required mounting heights shown on the BC, or the SMD standard sheets during construction. This work should be paid for under the appropriate pay item for relocating existing signs.
6. Any sign or traffic control device that is struck or damaged by the Contractor or his/her construction equipment shall be replaced as soon as possible by the Contractor to ensure proper guidance for the motorists. This will be subsidiary to Item 502.

GENERAL NOTES FOR WORK ZONE SIGNS

1. Contractor shall install and maintain signs in a straight and plumb condition and/or as directed by the Engineer.
2. Wooden sign posts shall be painted white.
3. Barricades shall NOT be used as sign supports.
4. All signs shall be installed in accordance with the plans or as directed by the Engineer. Signs shall be used to regulate, warn, and guide the traveling public safely through the work zone.
5. The Contractor may furnish either the sign design shown in the plans or in the "Standard Highway Sign Designs for Texas" (SHSD). The Engineer/Inspector may require the Contractor to furnish other work zone signs that are shown in the TMUTCD but may have been omitted from the plans. Any variation in the plans shall be documented by written agreement between the Engineer and the Contractor's Responsible Person. All changes must be documented in writing before being implemented. This can include documenting the changes in the Inspector's TxDOT diary and having both the Inspector and Contractor initial and date the agreed upon changes.
6. The Contractor shall furnish sign supports listed in the "Compliant Work Zone Traffic Control Device List" (CWZTCD) for small roadside signs. Supports for temporary large roadside signs shall meet the requirements detailed on the Temporary Large Roadside Signs (TLRS) standard sheets. The Contractor shall install the sign support in accordance with the manufacturer's recommendations. If there is a question regarding installation procedures, the Contractor shall furnish the Engineer a copy of the manufacturer's installation recommendations so the Engineer can verify the correct procedures are being followed.
7. The Contractor is responsible for installing signs on approved supports and replacing signs with damaged or cracked substrates and/or damaged or marred reflective sheeting as directed by the Engineer/Inspector.
8. Identification markings may be shown only on the back of the sign substrate. The maximum height of letters and/or company logos used for identification shall be 1 inch.
9. The Contractor shall replace damaged wood posts. New or damaged wood sign posts shall not be spliced.

DURATION OF WORK (as defined by the "Texas Manual on Uniform Traffic Control Devices" Part 6)

1. The types of sign supports, sign mounting height, the size of signs, and the type of sign substrates can vary based on the type of work being performed. The Engineer is responsible for selecting the appropriate size sign for the type of work being performed. The Contractor is responsible for ensuring the sign support, sign mounting height and substrate meets manufacturer's recommendations in regard to crashworthiness and duration of work requirements.
 - a. Long-term stationary - work that occupies a location more than 3 days.
 - b. Intermediate-term stationary - work that occupies a location more than one daylight period up to 3 days, or nighttime work lasting more than one hour.
 - c. Short-term stationary - daytime work that occupies a location for more than 1 hour in a single daylight period.
 - d. Short, duration - work that occupies a location up to 1 hour.
 - e. Mobile - work that moves continuously or intermittently (stopping for up to approximately 15 minutes.)

SIGN MOUNTING HEIGHT

1. The bottom of Long-term/Intermediate-term signs shall be at least 7 feet, but not more than 9 feet, above the paved surface, except as shown for supplemental plaques mounted below other signs.
2. The bottom of Short-term/Short Duration signs shall be a minimum of 1 foot above the pavement surface but no more than 2 feet above the ground.
3. Long-term/Intermediate-term Signs may be used in lieu of Short-term/Short Duration signing.
4. Short-term/Short Duration signs shall be used only during daylight and shall be removed at the end of the workday or raised to appropriate Long-term/Intermediate sign height.
5. Regulatory signs shall be mounted at least 7 feet, but not more than 9 feet, above the paved surface regardless of work duration.

SIZE OF SIGNS

1. The Contractor shall furnish the sign sizes shown on BC (2) unless otherwise shown in the plans or as directed by the Engineer.

SIGN SUBSTRATES

1. The Contractor shall ensure the sign substrate is installed in accordance with the manufacturer's recommendations for the type of sign support that is being used. The CWZTCD lists each substrate that can be used on the different types and models of sign supports.
2. "Mesh" type materials are NOT an approved sign substrate, regardless of the tightness of the weave.
3. All wooden individual sign panels fabricated from 2 or more pieces shall have one or more plywood cleat, 1/2" thick by 6" wide, fastened to the back of the sign and extending fully across the sign. The cleat shall be attached to the back of the sign using wood screws that do not penetrate the face of the sign panel. The screws shall be placed on both sides of the splice and spaced at 6" centers. The Engineer may approve other methods of splicing the sign face.

REFLECTIVE SHEETING

1. All signs shall be retroreflective and constructed of sheeting meeting the color and retro-reflectivity requirements of DMS-8300 for rigid signs or DMS-8310 for roll-up signs. The web address for DMS specifications is shown on BC(1).
2. White sheeting, meeting the requirements of DMS-8300 Type A, shall be used for signs with a white background.
3. Orange sheeting, meeting the requirements of DMS-8300 Type B_{FL} or Type C_{FL}, shall be used for rigid signs with orange backgrounds.

SIGN LETTERS

1. All sign letters and numbers shall be clear, and open rounded type uppercase alphabet letters as approved by the Federal Highway Administration (FHWA) and as published in the "Standard Highway Sign Design for Texas" manual. Signs, letters and numbers shall be of first class workmanship in accordance with Department Standards and Specifications.

REMOVING OR COVERING

1. When sign messages may be confusing or do not apply, the signs shall be removed or completely covered.
2. Long-term stationary or intermediate stationary signs installed on square metal tubing may be turned away from traffic 90 degrees when the sign message is not applicable. This technique may not be used for signs installed in the median of divided highways or near any intersections where the sign may be seen from approaching traffic.
3. Signs installed on wooden skids shall not be turned at 90 degree angles to the roadway. These signs should be removed or completely covered when not required.
4. When signs are covered, the material used shall be opaque, such as heavy mil black plastic, or other materials which will cover the entire sign face and maintain their opaque properties under automobile headlights at night, without damaging the sign sheeting.
5. Burlap shall NOT be used to cover signs.
6. Duct tape or other adhesive material shall NOT be affixed to a sign face.
7. Signs and anchor stubs shall be removed and holes backfilled upon completion of work.

SIGN SUPPORT WEIGHTS

1. Where sign supports require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand should be used.
2. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight.
3. Rock, concrete, iron, steel or other solid objects shall not be permitted for use as sign support weights.
4. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs.
5. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall NOT be used.
6. Rubber ballasts designed for channelizing devices should not be used for ballast on portable sign supports. Sign supports designed and manufactured with rubber bases may be used when shown on the CWZTCD list.
7. Sandbags shall only be placed along or laid over the base supports of the traffic control device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners. Sandbags shall be placed along the length of the skids to weigh down the sign support.
8. Sandbags shall NOT be placed under the skid and shall not be used to level sign supports placed on slopes.

FLAGS ON SIGNS

1. Flags may be used to draw attention to warning signs. When used, the flag shall be 16 inches square or larger and shall be orange or fluorescent red-orange in color. Flags shall not be allowed to cover any portion of the sign face.

SHEET 4 OF 12

Texas Department of Transportation
 Traffic Safety Division Standard

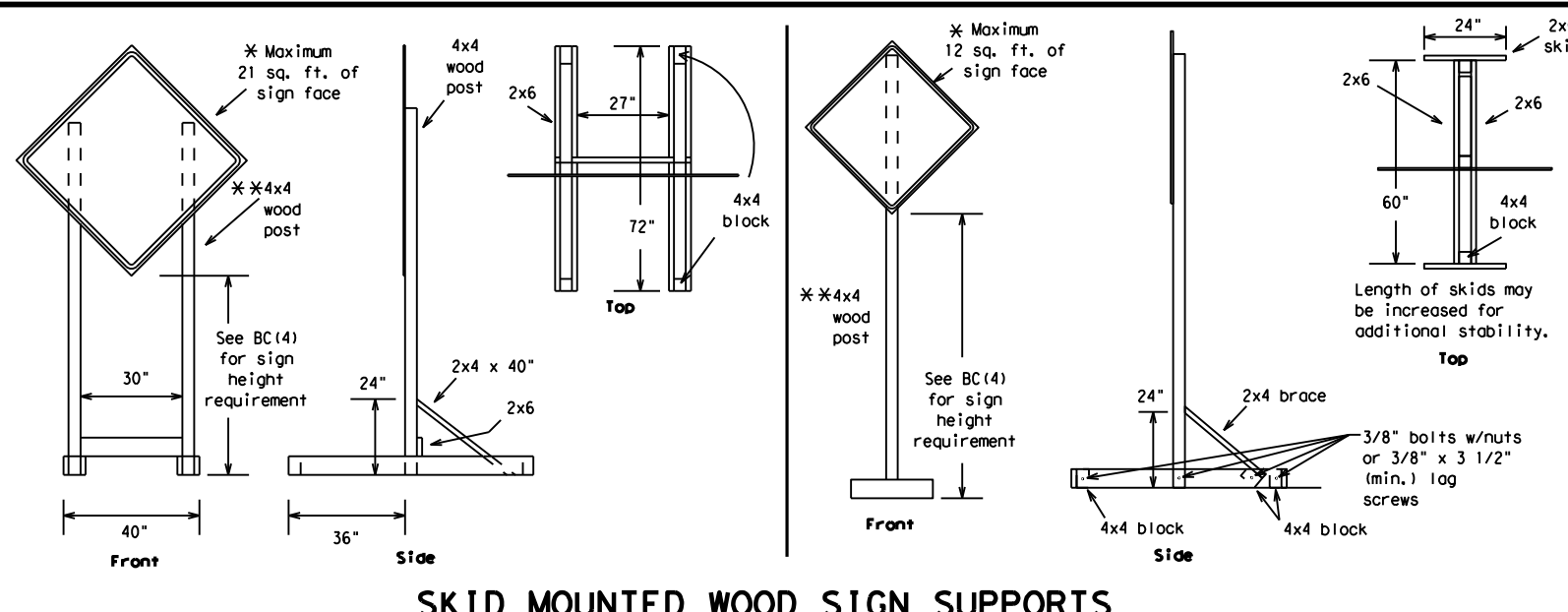
BARRICADE AND CONSTRUCTION TEMPORARY SIGN NOTES

BC (4) - 21

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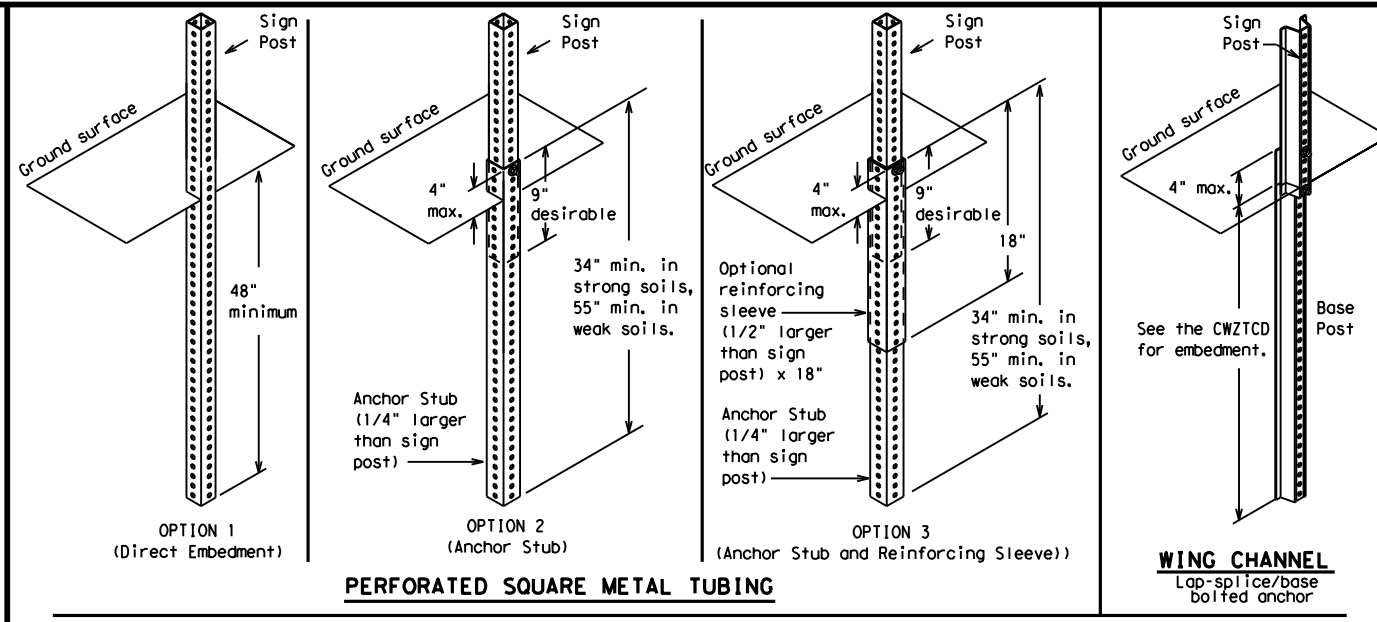
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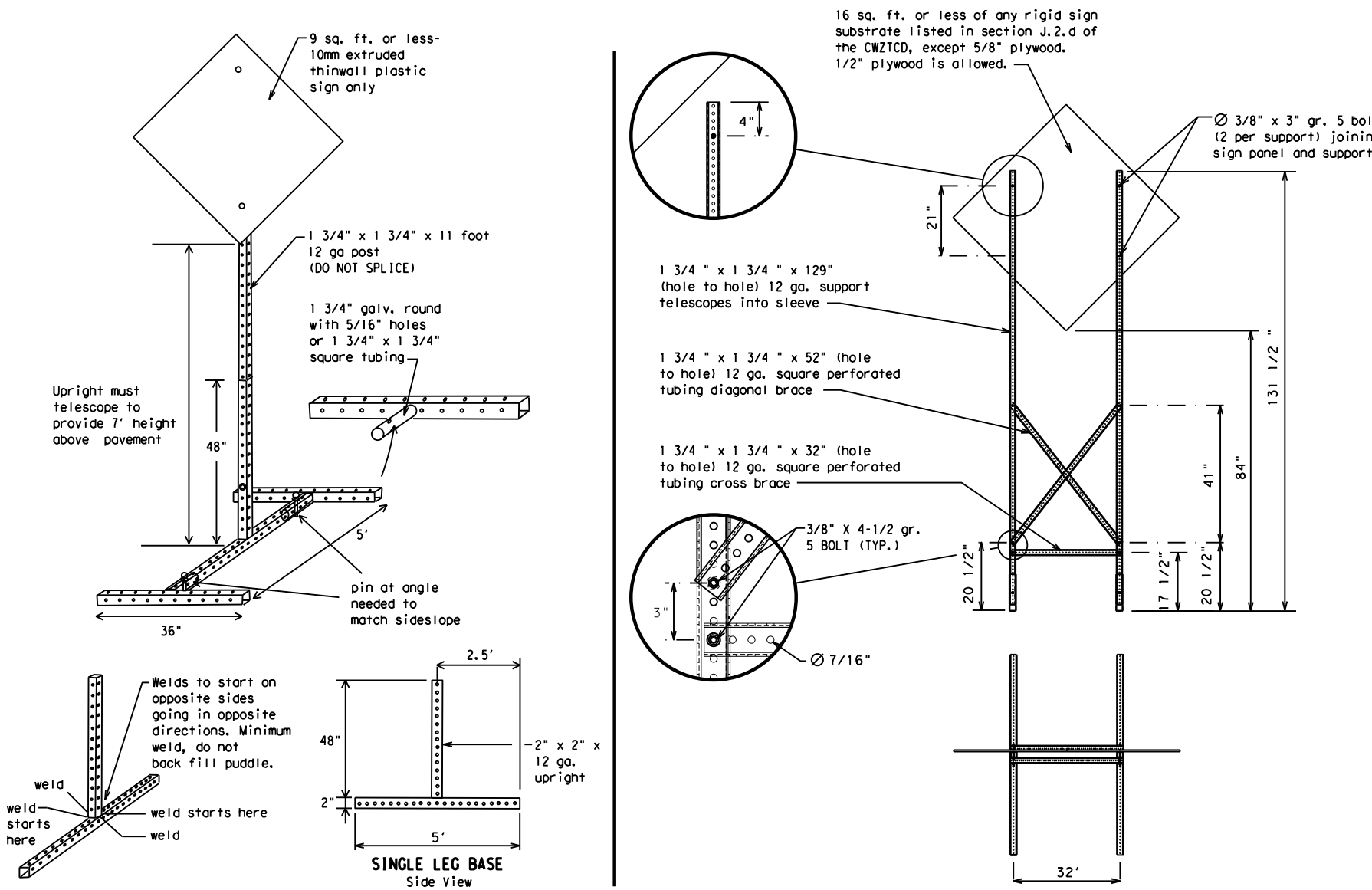
SKID MOUNTED WOOD SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS



GROUND MOUNTED SIGN SUPPORTS

Refer to the CWZTCD and the manufacturer's installation procedure for each type sign support. The maximum sign square footage shall adhere to the manufacturer's recommendation. Two post installations can be used for larger signs.



SKID MOUNTED PERFORATED SQUARE STEEL TUBING SIGN SUPPORTS

* LONG/INTERMEDIATE TERM STATIONARY - PORTABLE SKID MOUNTED SIGN SUPPORTS

WEDGE ANCHORS

Both steel and plastic Wedge Anchor Systems as shown on the SMD Standard Sheets may be used as temporary sign supports for signs up to 10 square feet of sign face. They may be set in concrete or in sturdy soils if approved by the Engineer. (See web address for "Traffic Engineering Standard Sheets" on BC(1)).

OTHER DESIGNS

MORE DETAILS OF APPROVED LONG/INTERMEDIATE AND SHORT TERM SUPPORTS CAN BE FOUND ON THE CWZTCD LIST. SEE BC(1) FOR WEBSITE LOCATION.

GENERAL NOTES

- Nails may be used in the assembly of wooden sign supports, but 3/8" bolts with nuts or 3/8" x 3 1/2" lag screws must be used on every joint for final connection.
- No more than 2 sign posts shall be placed within a 7 ft. circle, except for specific materials noted on the CWZTCD List.
- When project is completed, all sign supports and foundations shall be removed from the project site. This will be considered subsidiary to Item 502.

- * See BC(4) for definition of "Work Duration."
- ** Wood sign posts MUST be one piece. Splicing will NOT be allowed. Posts shall be painted white.
- ☐ See the CWZTCD for the type of sign substrate that can be used for each approved sign support.

SHEET 5 OF 12



BARRICADE AND CONSTRUCTION TYPICAL SIGN SUPPORT

BC(5) - 21

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WHEN NOT IN USE, REMOVE THE PCMS FROM THE RIGHT-OF-WAY OR PLACE THE PCMS BEHIND BARRIER OR GUARDRAIL WITH SIGN PANEL TURNED PARALLEL TO TRAFFIC

RECOMMENDED PHASES AND FORMATS FOR PCMS MESSAGES DURING ROADWORK ACTIVITIES

(The Engineer may approve other messages not specifically covered here.)

PORTABLE CHANGEABLE MESSAGE SIGNS

- The Engineer/Inspector shall approve all messages used on portable changeable message signs (PCMS).
- Messages on PCMS should contain no more than 8 words (about four to eight characters per word), not including simple words such as "TO," "FOR," "AT," etc.
- Messages should consist of a single phase, or two phases that alternate. Three-phase messages are not allowed. Each phase of the message should convey a single thought, and must be understood by itself.
- Use the word "EXIT" to refer to an exit ramp on a freeway; i.e., "EXIT CLOSED." Do not use the term "RAMP."
- Always use the route or interstate designation (IH, US, SH, FM) along with the number when referring to a roadway.
- When in use, the bottom of a stationary PCMS message panel should be a minimum 7 feet above the roadway, where possible.
- The message term "WEEKEND" should be used only if the work is to start on Saturday morning and end by Sunday evening at midnight. Actual days and hours of work should be displayed on the PCMS if work is to begin on Friday evening and/or continue into Monday morning.
- The Engineer/Inspector may select one of two options which are available for displaying a two-phase message on a PCMS. Each phase may be displayed for either four seconds each or for three seconds each.
- Do not "flash" messages or words included in a message. The message should be steady burn or continuous while displayed.
- Do not present redundant information on a two-phase message; i.e., keeping two lines of the message the same and changing the third line.
- Do not use the word "Danger" in message.
- Do not display the message "LANES SHIFT LEFT" or "LANES SHIFT RIGHT" on a PCMS. Drivers do not understand the message.
- Do not display messages that scroll horizontally or vertically across the face of the sign.
- The following table lists abbreviated words and two-word phrases that are acceptable for use on a PCMS. Both words in a phrase must be displayed together. Words or phrases not on this list should not be abbreviated, unless shown in the TMUTCD.
- PCMS character height should be at least 18 inches for trailer mounted units. They should be visible from at least 1/2 (.5) mile and the text should be legible from at least 600 feet at night and 800 feet in daylight. Truck mounted units must have a character height of 10 inches and must be legible from at least 400 feet.
- Each line of text should be centered on the message board rather than left or right justified.
- If disabled, the PCMS should default to an illegible display that will not alarm motorists and will only be used to alert workers that the PCMS has malfunctioned. A pattern such as a series of horizontal solid bars is appropriate.

Phase 1: Condition Lists

Road/Lane/Ramp Closure List

FREEWAY CLOSED X MILE	FRONTAGE ROAD CLOSED
ROAD CLOSED AT SH XXX	SHOULDER CLOSED XXX FT
ROAD CLSD AT FM XXXX	RIGHT LN CLOSED XXX FT
RIGHT X LANES CLOSED	RIGHT X LANES OPEN
CENTER LANE CLOSED	DAYTIME LANE CLOSURES
NIGHT LANE CLOSURES	I-XX SOUTH EXIT CLOSED
VARIOUS LANES CLOSED	EXIT XXX CLOSED X MILE
EXIT CLOSED	RIGHT LN TO BE CLOSED
MALL DRIVEWAY CLOSED	X LANES CLOSED TUE - FRI
XXXXXXXX BLVD CLOSED	

Other Condition List

ROADWORK XXX FT	ROAD REPAIRS XXXX FT
FLAGGER XXXX FT	LANE NARROWS XXXX FT
RIGHT LN NARROWS XXXX FT	TWO-WAY TRAFFIC XX MILE
MERGING TRAFFIC XXXX FT	CONST TRAFFIC XXX FT
LOOSE GRAVEL XXXX FT	UNEVEN LANES XXXX FT
DETOUR X MILE	ROUGH ROAD XXXX FT
ROADWORK PAST SH XXXX	ROADWORK NEXT FRI-SUN
BUMP XXXX FT	US XXX EXIT X MILES
TRAFFIC SIGNAL XXXX FT	LANES SHIFT *

* LANES SHIFT in Phase 1 must be used with STAY IN LANE in Phase 2.

Phase 2: Possible Component Lists

Action to Take/Effect on Travel List

MERGE RIGHT	FORM X LINES RIGHT
DETOUR NEXT X EXITS	USE XXXXX RD EXIT
USE EXIT XXX	USE EXIT I-XX NORTH
STAY ON US XXX SOUTH	USE I-XX E TO I-XX N
TRUCKS USE US XXX N	WATCH FOR TRUCKS
WATCH FOR TRUCKS	EXPECT DELAYS
EXPECT DELAYS	PREPARE TO STOP
REDUCE SPEED XXX FT	END SHOULDER USE
USE OTHER ROUTES	WATCH FOR WORKERS
STAY IN LANE *	

Location List

AT FM XXXX
BEFORE RAILROAD CROSSING
NEXT X MILES
PAST US XXX EXIT
XXXXXXXX TO XXXXXX
US XXX TO FM XXXX

Warning List

SPEED LIMIT XX MPH
MAXIMUM SPEED XX MPH
MINIMUM SPEED XX MPH
ADVISORY SPEED XX MPH
RIGHT LANE EXIT
USE CAUTION
DRIVE SAFELY
DRIVE WITH CARE

** Advance Notice List

TUE-FRI XX AM-X PM
APR XX-XX X PM-X AM
BEGINS MONDAY
BEGINS MAY XX
MAY X-X XX PM - XX AM
NEXT FRI-SUN
XX AM TO XX PM
NEXT TUE AUG XX
TONIGHT XX PM-XX AM

** See Application Guidelines Note 6.

APPLICATION GUIDELINES

- Only 1 or 2 phases are to be used on a PCMS.
- The 1st phase (or both) should be selected from the "Road/Lane/Ramp Closure List" and the "Other Condition List".
- A 2nd phase can be selected from the "Action to Take/Effect on Travel, Location, General Warning, or Advance Notice Phase Lists".
- A Location Phase is necessary only if a distance or location is not included in the first phase selected.
- If two PCMS are used in sequence, they must be separated by a minimum of 1000 ft. Each PCMS shall be limited to two phases, and should be understandable by themselves.
- For advance notice, when the current date is within seven days of the actual work date, calendar days should be replaced with days of the week. Advance notification should typically be for no more than one week prior to the work.

WORDING ALTERNATIVES

- The words RIGHT, LEFT and ALL can be interchanged as appropriate.
- Roadway designations IH, US, SH, FM and LP can be interchanged as appropriate.
- EAST, WEST, NORTH and SOUTH (or abbreviations E, W, N and S) can be interchanged as appropriate.
- Highway names and numbers replaced as appropriate.
- ROAD, HIGHWAY and FREEWAY can be interchanged as needed.
- AHEAD may be used instead of distances if necessary.
- FT and MI, MILE and MILES interchanged as appropriate.
- AT, BEFORE and PAST interchanged as needed.
- Distances or AHEAD can be eliminated from the message if a location phase is used.

PCMS SIGNS WITHIN THE R.O.W. SHALL BE BEHIND GUARDRAIL OR CONCRETE BARRIER OR SHALL HAVE A MINIMUM OF FOUR (4) PLASTIC DRUMS PLACED PERPENDICULAR TO TRAFFIC ON THE UPSTREAM SIDE OF THE PCMS, WHEN EXPOSED TO ONE DIRECTION OF TRAFFIC. WHEN EXPOSED TO TWO WAY TRAFFIC, THE FOUR DRUMS SHOULD BE PLACED WITH ONE DRUM AT EACH OF THE FOUR CORNERS OF THE UNIT.

FULL MATRIX PCMS SIGNS

- When Full Matrix PCMS signs are used, the character height and legibility/visibility requirements shall be maintained as listed in Note 15 under "PORTABLE CHANGEABLE MESSAGE SIGNS" above.
- When symbol signs, such as the "Flagger Symbol" (CW20-7) are represented graphically on the Full Matrix PCMS sign and, with the approval of the Engineer, it shall maintain the legibility/visibility requirement listed above.
- When symbol signs are represented graphically on the Full Matrix PCMS, they shall only supplement the use of the static sign represented, and shall not substitute for, or replace that sign.
- A full matrix PCMS may be used to simulate a flashing arrow board provided it meets the visibility, flash rate and dimming requirements on BC(7), for the same size arrow.

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WORD OR PHRASE	ABBREVIATION	WORD OR PHRASE	ABBREVIATION
Access Road	ACCS RD	Major	MAJ
Alternate	ALT	Miles	MI
Avenue	AVE	Miles Per Hour	MPH
Best Route	BEST RTE	Minor	MNR
Boulevard	BLVD	Monday	MON
Bridge	BRDG	Normal	NORM
Canal	CANT	North	N
Center	CTR	Northbound	(route) N
Construction Ahead	CONST AHD	Parking	PKING
CROSSING	XING	Road	RD
Detour Route	DETOUR RTE	Right Lane	RT LN
Do Not	DONT	Saturday	SAT
East	E	Service Road	SERV RD
Eastbound	(route) E	Shoulder	SHLDR
Emergency	EMER	Slippery	SLIP
Emergency Vehicle	EMER VEH	South	S
Entrance, Enter	ENT	Southbound	(route) S
Express Lane	EXP LN	Speed	SPD
Expressway	EXPWY	Street	ST
XXXX Feet	XXXX FT	Sunday	SUN
Fog Ahead	FOG AHD	Telephone	PHONE
Freeway	FRWY, FWY	Temporary	TEMP
Freeway Blocked	FWY BLKD	Thursday	THURS
Friday	FRI	To Downtown	TO DWNTN
Hazardous Driving	HAZ DRIVING	Traffic	TRAF
Hazardous Material	HAZMAT	Travelers	TRVLR
High-Occupancy Vehicle	HOV	Tuesday	TUES
Hour(s)	HR, HRS	Time Minutes	TIME MIN
Information	INFO	Upper Level	UPR LEVEL
It Is	ITS	Vehicles (s)	VEH, VEHS
Junction	JCT	Warning	WARN
Left	LFT	Wednesday	WED
Left Lane	LFT LN	Weight Limit	WT LIMIT
Lane Closed	LN CLOSED	West	W
Lower Level	LWR LEVEL	Westbound	(route) W
Maintenance	MAINT	Wet Pavement	WET PVMT
		Will Not	WONT

Roadway designation # IH-number, US-number, SH-number, FM-number



BARRICADE AND CONSTRUCTION PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)

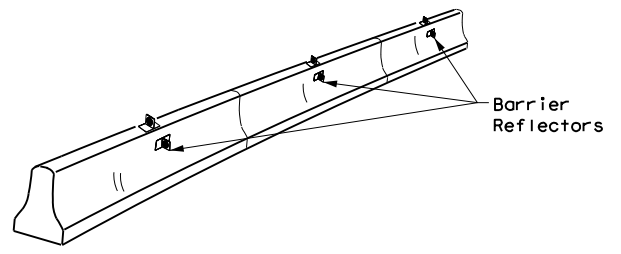
BC (6) - 21

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7-13	5-21	SJT	MENARD	15					

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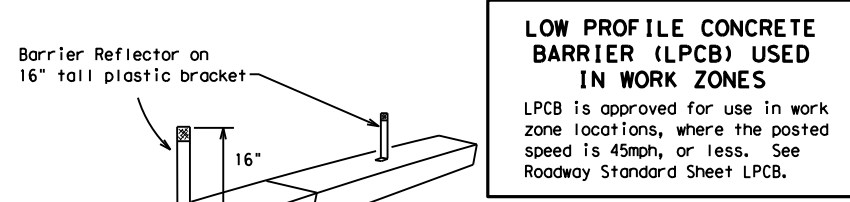
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- Barrier Reflectors shall be pre-qualified, and conform to the color and reflectivity requirements of DMS-8600. A list of prequalified Barrier Reflectors can be found at the Material Producer List web address shown on BC(1).
- Color of Barrier Reflectors shall be as specified in the TMUTCD. The cost of the reflectors shall be considered subsidiary to Item 512.



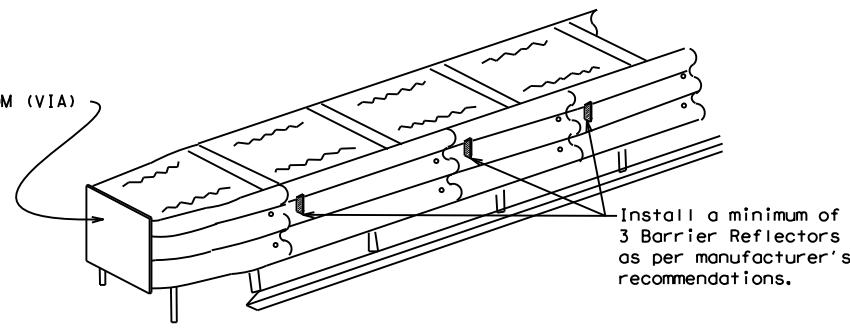
CONCRETE TRAFFIC BARRIER (CTB)

- Where traffic is on one side of the CTB, two (2) Barrier Reflectors shall be mounted in approximately the midsection of each section of CTB. An alternate mounting location is uniformly spaced at one end of each CTB. This will allow for attachment of a barrier grapple without damaging the reflector. The Barrier Reflector mounted on the side of the CTB shall be located directly below the reflector mounted on top of the barrier, as shown in the detail above.
- Where CTB separates two-way traffic, three barrier reflectors shall be mounted on each section of CTB. The reflector unit on top shall have two yellow reflective faces (Bi-Directional) while the reflectors on each side of the barrier shall have one yellow reflective face, as shown in the detail above.
- When CTB separates traffic traveling in the same direction, no barrier reflectors will be required on top of the CTB.
- Barrier Reflector units shall be yellow or white in color to match the edgeline being supplemented.
- Maximum spacing of Barrier Reflectors is forty (40) feet.
- Pavement markers or temporary flexible-reflective roadway marker tabs shall NOT be used as CTB delineation.
- Attachment of Barrier Reflectors to CTB shall be per manufacturer's recommendations.
- Missing or damaged Barrier Reflectors shall be replaced as directed by the Engineer.
- Single slope barriers shall be delineated as shown on the above detail.



LOW PROFILE CONCRETE BARRIER (LPCB) USED IN WORK ZONES
 LPCB is approved for use in work zone locations, where the posted speed is 45mph, or less. See Roadway Standard Sheet LPCB.

LOW PROFILE CONCRETE BARRIER (LPCB)



DELINEATION OF END TREATMENTS

END TREATMENTS FOR CTB'S USED IN WORK ZONES
 End treatments used on CTB's in work zones shall meet the appropriate crashworthy standards as defined in the Manual for Assessing Safety Hardware (MASH). Refer to the CWZTCD List for approved end treatments and manufacturers.

BARRIER REFLECTORS FOR CONCRETE TRAFFIC BARRIER AND ATTENUATORS

WARNING LIGHTS

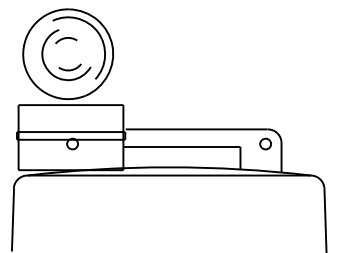
- Warning lights shall meet the requirements of the TMUTCD.
- Warning lights shall NOT be installed on barricades.
- Type A-Low Intensity Flashing Warning Lights are commonly used with drums. They are intended to warn of or mark a potentially hazardous area. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "FL". The Type A Warning Lights shall not be used with signs manufactured with Type B_{FL} or C_{FL} Sheeting meeting the requirements of Departmental Material Specification DMS-8300.
- Type-C and Type D 360 degree Steady Burn Lights are intended to be used in a series for delineation to supplement other traffic control devices. Their use shall be as indicated on this sheet and/or other sheets of the plans by the designation "SB".
- The Engineer/Inspector or the plans shall specify the location and type of warning lights to be installed on the traffic control devices.
- When required by the Engineer, the Contractor shall furnish a copy of the warning lights certification. The warning light manufacturer will certify the warning lights meet the requirements of the latest ITE Purchase Specifications for Flashing and Steady-Burn Warning Lights.
- When used to delineate curves, Type-C and Type D Steady Burn Lights should only be placed on the outside of the curve, not the inside.
- The location of warning lights and warning reflectors on drums shall be as shown elsewhere in the plans.

WARNING LIGHTS MOUNTED ON PLASTIC DRUMS

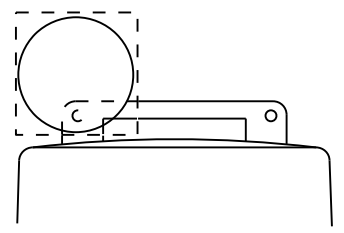
- Type A flashing warning lights are intended to warn drivers that they are approaching or are in a potentially hazardous area.
- Type A random flashing warning lights are not intended for delineation and shall not be used in a series.
- A series of sequential flashing warning lights placed on channelizing devices to form a merging taper may be used for delineation. If used, the successive flashing of the sequential warning lights should occur from the beginning of the taper to the end of the merging taper in order to identify the desired vehicle path. The rate of flashing for each light shall be 65 flashes per minute, plus or minus 10 flashes.
- Type C and D steady-burn warning lights are intended to be used in a series to delineate the edge of the travel lane on detours, on lane changes, on lane closures, and on other similar conditions.
- Type A, Type C and Type D warning lights shall be installed at locations as detailed on other sheets in the plans.
- Warning lights shall not be installed on a drum that has a sign, chevron or vertical panel.
- The maximum spacing for warning lights on drums should be identical to the channelizing device spacing.

WARNING REFLECTORS MOUNTED ON PLASTIC DRUMS AS A SUBSTITUTE FOR TYPE C (STEADY BURN) WARNING LIGHTS

- A warning reflector or approved substitute may be mounted on a plastic drum as a substitute for a Type C, steady burn warning light at the discretion of the Contractor unless otherwise noted in the plans.
- The warning reflector shall be yellow in color and shall be manufactured using a sign substrate approved for use with plastic drums listed on the CWZTCD.
- The warning reflector shall have a minimum retroreflective surface area (one-side) of 30 square inches.
- Round reflectors shall be fully reflectorized, including the area where attached to the drum.
- Square substrates must have a minimum of 30 square inches of reflectorized sheeting. They do not have to be reflectorized where it attaches to the drum.
- The side of the warning reflector facing approaching traffic shall have sheeting meeting the color and retroreflectivity requirements for DMS 8300-Type B or Type C.
- When used near two-way traffic, both sides of the warning reflector shall be reflectorized.
- The warning reflector should be mounted on the side of the handle nearest approaching traffic.
- The maximum spacing for warning reflectors should be identical to the channelizing device spacing requirements.



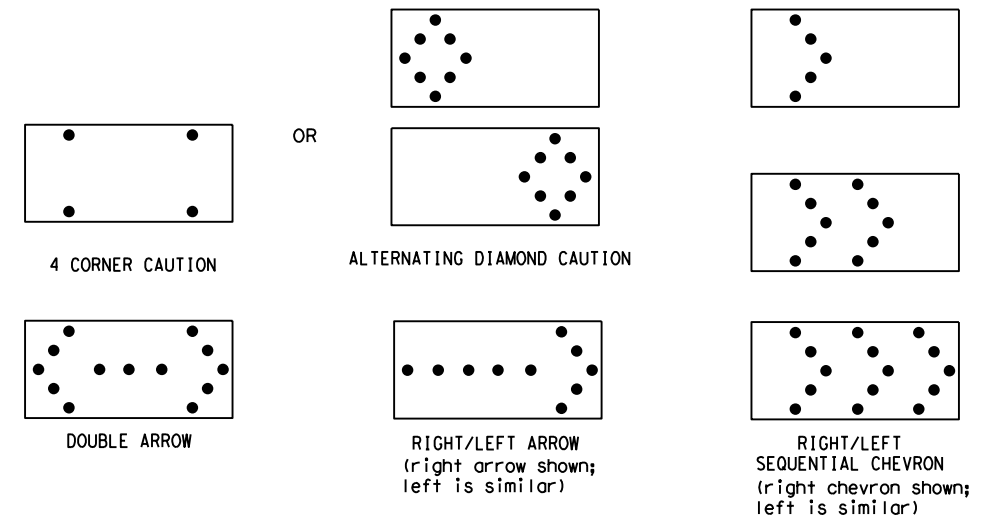
Type C Warning Light or approved substitute mounted on a drum adjacent to the travel way.



Warning reflector may be round or square. Must have a yellow reflective surface area of at least 30 square inches

Arrow Boards may be located behind channelizing devices in place for a shoulder taper or merging taper, otherwise they shall be delineated with four (4) channelizing devices placed perpendicular to traffic on the upstream side of traffic.

- The Flashing Arrow Board should be used for all lane closures on multi-lane roadways, or slow moving maintenance or construction activities on the travel lanes.
- Flashing Arrow Boards should not be used on two-lane, two-way roadways, detours, diversions or work on shoulders unless the "CAUTION" display (see detail below) is used.
- The Engineer/Inspector shall choose all appropriate signs, barricades and/or other traffic control devices that should be used in conjunction with the Flashing Arrow Board.
- The Flashing Arrow Board should be able to display the following symbols:



- The "CAUTION" display consists of four corner lamps flashing simultaneously, or the Alternating Diamond Caution mode as shown.
- The straight line caution display is NOT ALLOWED.
- The Flashing Arrow Board shall be capable of minimum 50 percent dimming from rated lamp voltage. The flashing rate of the lamps shall not be less than 25 nor more than 40 flashes per minute.
- Minimum lamp "on time" shall be approximately 50 percent for the flashing arrow and equal intervals of 25 percent for each sequential phase of the flashing chevron.
- The sequential arrow display is NOT ALLOWED.
- The flashing arrow display is the TxDOT standard; however, the sequential chevron display may be used during daylight operations.
- The Flashing Arrow Board shall be mounted on a vehicle, trailer or other suitable support.
- A Flashing Arrow Board SHALL NOT BE USED to laterally shift traffic.
- A full matrix PCMS may be used to simulate a Flashing Arrow Board provided it meets visibility, flash rate and dimming requirements on this sheet for the same size arrow.
- Minimum mounting height of trailer mounted Arrow Boards should be 7 feet from roadway to bottom of panel.

REQUIREMENTS			
TYPE	MINIMUM SIZE	MINIMUM NUMBER OF PANEL LAMPS	MINIMUM VISIBILITY DISTANCE
B	30 x 60	13	3/4 mile
C	48 x 96	15	1 mile

ATTENTION
 Flashing Arrow Boards shall be equipped with automatic dimming devices.

WHEN NOT IN USE, REMOVE THE ARROW BOARD FROM THE RIGHT-OF-WAY OR PLACE THE ARROW BOARD BEHIND CONCRETE TRAFFIC BARRIER OR GUARDRAIL.

FLASHING ARROW BOARDS

SHEET 7 OF 12

TRUCK-MOUNTED ATTENUATORS

- Truck-mounted attenuators (TMA) used on TxDOT facilities must meet the requirements outlined in the Manual for Assessing Safety Hardware (MASH).
- Refer to the CWZTCD for the requirements of Level 2 or Level 3 TMAs.
- Refer to the CWZTCD for a list of approved TMAs.
- TMAs are required on freeways unless otherwise noted in the plans.
- A TMA should be used anytime that it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the work performance.
- The only reason a TMA should not be required is when a work area is spread down the roadway and the work crew is an extended distance from the TMA.



BARRICADE AND CONSTRUCTION ARROW PANEL, REFLECTORS, WARNING LIGHTS & ATTENUATOR

BC (7) - 21

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7-13	5-21	SJT	MENARD	16					

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GENERAL NOTES

- For long term stationary work zones on freeways, drums shall be used as the primary channelizing device.
- For intermediate term stationary work zones on freeways, drums should be used as the primary channelizing device but may be replaced in tangent sections by vertical panels, or 42" two-piece cones. In tangent sections, one-piece cones may be used with the approval of the Engineer but only if personnel are present on the project at all times to maintain the cones in proper position and location.
- For short term stationary work zones on freeways, drums are the preferred channelizing device but may be replaced in tapers, transitions and tangent sections by vertical panels, two-piece cones or one-piece cones as approved by the Engineer.
- Drums and all related items shall comply with the requirements of the current version of the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD) and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Drums, bases, and related materials shall exhibit good workmanship and shall be free from objectionable marks or defects that would adversely affect their appearance or serviceability.
- The Contractor shall have a maximum of 24 hours to replace any plastic drums identified for replacement by the Engineer/Inspector. The replacement device must be an approved device.

GENERAL DESIGN REQUIREMENTS

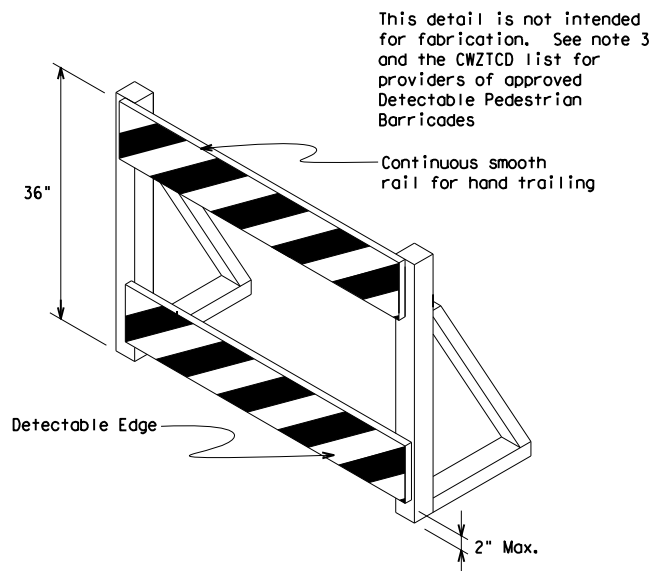
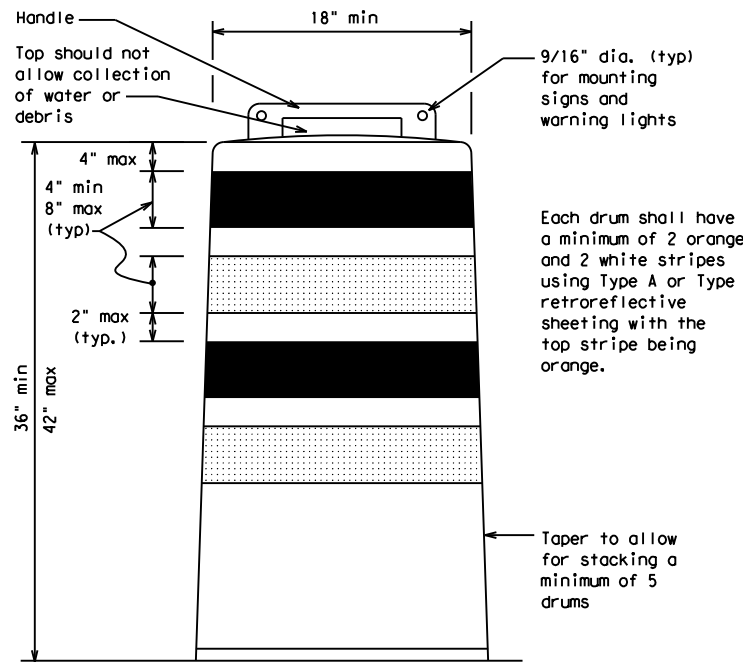
- Pre-qualified plastic drums shall meet the following requirements:
- Plastic drums shall be a two-piece design; the "body" of the drum shall be the top portion and the "base" shall be the bottom.
 - The body and base shall lock together in such a manner that the body separates from the base when impacted by a vehicle traveling at a speed of 20 MPH or greater but prevents accidental separation due to normal handling and/or air turbulence created by passing vehicles.
 - Plastic drums shall be constructed of lightweight flexible, and deformable materials. The Contractor shall NOT use metal drums or single piece plastic drums as channelization devices or sign supports.
 - Drums shall present a profile that is a minimum of 18 inches in width at the 36 inch height when viewed from any direction. The height of drum unit (body installed on base) shall be a minimum of 36 inches and a maximum of 42 inches.
 - The top of the drum shall have a built-in handle for easy pickup and shall be designed to drain water and not collect debris. The handle shall have a minimum of two widely spaced 9/16 inch diameter holes to allow attachment of a warning light, warning reflector unit or approved compliant sign.
 - The exterior of the drum body shall have a minimum of four alternating orange and white retroreflective circumferential stripes not less than 4 inches nor greater than 8 inches in width. Any non-reflectorized space between any two adjacent stripes shall not exceed 2 inches in width.
 - Bases shall have a maximum width of 36 inches, a maximum height of 4 inches, and a minimum of two footholds of sufficient size to allow base to be held down while separating the drum body from the base.
 - Plastic drums shall be constructed of ultra-violet stabilized, orange, high-density polyethylene (HDPE) or other approved material.
 - Drum body shall have a maximum unballasted weight of 11 lbs.
 - Drum and base shall be marked with manufacturer's name and model number.

RETROREFLECTIVE SHEETING

- The stripes used on drums shall be constructed of sheeting meeting the color and retroreflectivity requirements of Departmental Materials Specification DMS-8300, "Sign Face Materials." Type A or Type B reflective sheeting shall be supplied unless otherwise specified in the plans.
- The sheeting shall be suitable for use on and shall adhere to the drum surface such that, upon vehicular impact, the sheeting shall remain adhered in-place and exhibit no delaminating, cracking, or loss of retroreflectivity other than that loss due to abrasion of the sheeting surface.

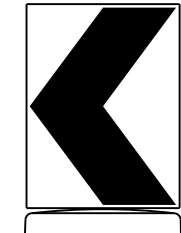
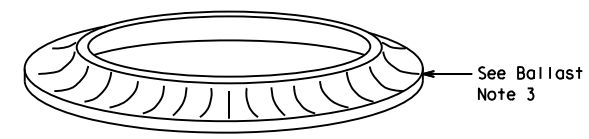
BALLAST

- Unballasted bases shall be large enough to hold up to 50 lbs. of sand. This base, when filled with the ballast material, should weigh between 35 lbs (minimum) and 50 lbs (maximum). The ballast may be sand in one to three sandbags separate from the base, sand in a sand-filled plastic base, or other ballasting devices as approved by the Engineer. Stacking of sandbags will be allowed, however height of sandbags above pavement surface may not exceed 12 inches.
- Bases with built-in ballast shall weigh between 40 lbs. and 50 lbs. Built-in ballast can be constructed of an integral crumb rubber base or a solid rubber base.
- Recycled truck tire sidewalls may be used for ballast on drums approved for this type of ballast on the CWZTCD list.
- The ballast shall not be heavy objects, water, or any material that would become hazardous to motorists, pedestrians, or workers when the drum is struck by a vehicle.
- When used in regions susceptible to freezing, drums shall have drainage holes in the bottoms so that water will not collect and freeze becoming a hazard when struck by a vehicle.
- Ballast shall not be placed on top of drums.
- Adhesives may be used to secure base of drums to pavement.

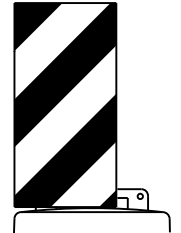


DETECTABLE PEDESTRIAN BARRICADES

- When existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility. Refer to WZ(BTS-2) for Pedestrian Control requirements for Sidewalk Diversions, Sidewalk Detours and Crosswalk Closures.
- Where pedestrians with visual disabilities normally use the closed sidewalk, a Detectable Pedestrian Barricade shall be placed across the full width of the closed sidewalk instead of a Type 3 Barricade.
- Detectable pedestrian barricades similar to the one pictured above, longitudinal channelizing devices, some concrete barriers, and wood or chain link fencing with a continuous detectable edging can satisfactorily delineate a pedestrian path.
- Tape, rope, or plastic chain strung between devices are not detectable, do not comply with the design standards in the "Americans with Disabilities Act Accessibility Guidelines (ADAAG)" and should not be used as a control for pedestrian movements.
- Warning lights shall not be attached to detectable pedestrian barricades.
- Detectable pedestrian barricades should use 8" nominal barricade rails as shown on BC(10) provided that the top rail provides a smooth continuous rail suitable for hand trailing with no splinters, burrs, or sharp edges.



18" x 24" Sign
(Maximum Sign Dimension)
Chevron CW1-8, Opposing Traffic Lane
Divider, Driveway sign D70a, Keep Right
R4 series or other signs as approved
by Engineer



12" x 24"
Vertical Panel
mount with diagonals
sloping down towards
travel way

Plywood, Aluminum or Metal sign
substrates shall NOT be used on
plastic drums

SIGNS, CHEVRONS, AND VERTICAL PANELS MOUNTED ON PLASTIC DRUMS

- Signs used on plastic drums shall be manufactured using substrates listed on the CWZTCD.
- Chevrons and other work zone signs with an orange background shall be manufactured with Type B_{FL} or Type C_{FL} Orange sheeting meeting the color and retroreflectivity requirements of DMS-8300, "Sign Face Material," unless otherwise specified in the plans.
- Vertical Panels shall be manufactured with orange and white sheeting meeting the requirements of DMS-8300 Type A or Type B. Diagonal stripes on Vertical Panels shall slope down toward the intended traveled lane.
- Other sign messages (text or symbolic) may be used as approved by the Engineer. Sign dimensions shall not exceed 18 inches in width or 24 inches in height, except for the R9 series signs discussed in note 8 below.
- Signs shall be installed using a 1/2 inch bolt (nominal) and nut, two washers, and one locking washer for each connection.
- Mounting bolts and nuts shall be fully engaged and adequately torqued. Bolts should not extend more than 1/2 inch beyond nuts.
- Chevrons may be placed on drums on the outside of curves, on merging tapers or on shifting tapers. When used in these locations, they may be placed on every drum or spaced not more than on every third drum. A minimum of three (3) should be used at each location called for in the plans.
- R9-9, R9-10, R9-11 and R9-11a Sidewalk Closed signs which are 24 inches wide may be mounted on plastic drums, with approval of the Engineer.

SHEET 8 OF 12



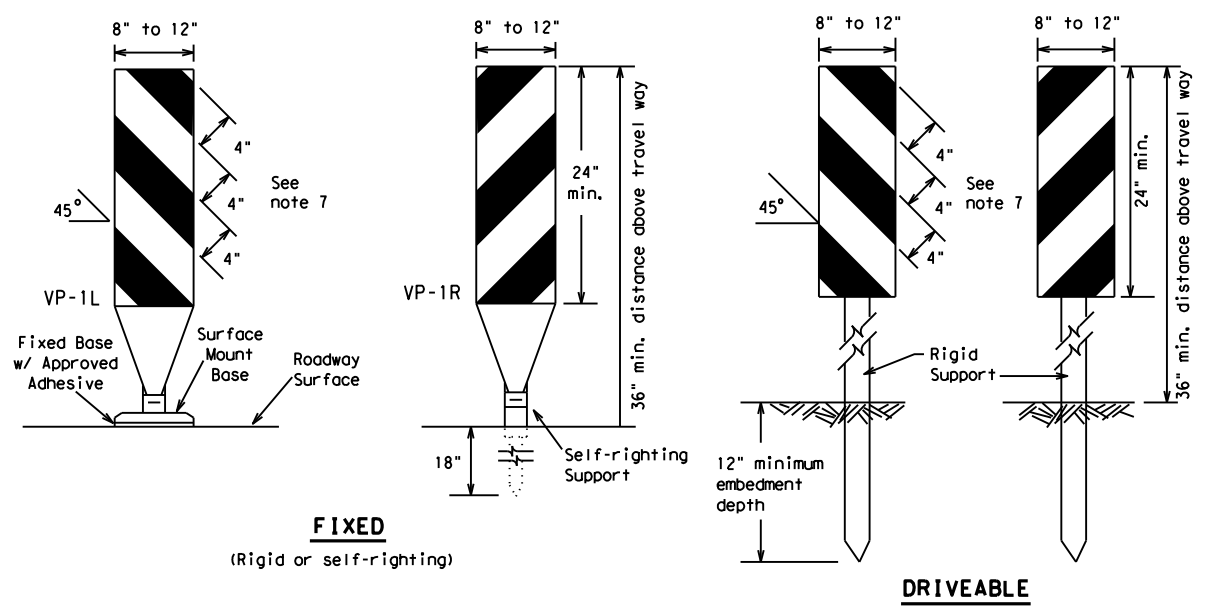
BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (8) - 21

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REVISIONS		6383	49	001	US 83, ETC				
4-03	8-14			DIST	COUNTY	SHEET NO.			
9-07	5-21			SJT	MENARD	17			
7-13									

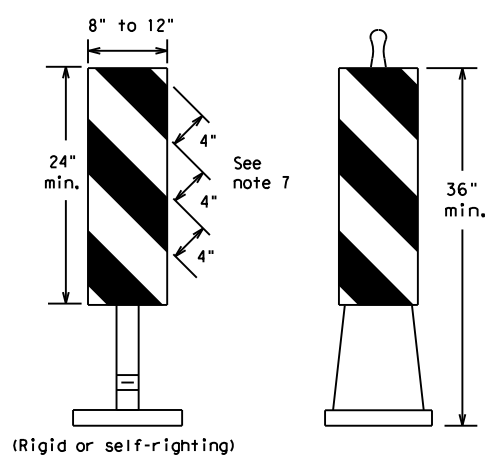
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FIXED
(Rigid or self-righting)

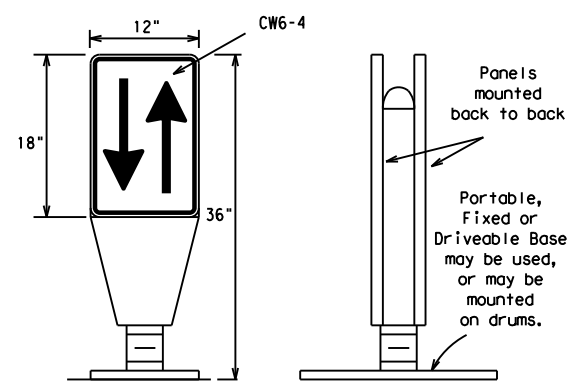
DRIVEABLE



PORTABLE

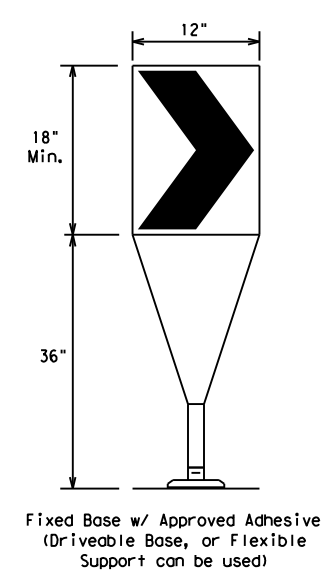
VERTICAL PANELS (VPs)

- Vertical Panels (VP's) are normally used to channelize traffic or divide opposing lanes of traffic.
- VP's may be used in daytime or nighttime situations. They may be used at the edge of shoulder drop-offs and other areas such as lane transitions where positive daytime and nighttime delineation is required. The Engineer/Inspector shall refer to the Roadway Design Manual for additional requirements on the use VP's for drop-offs.
- VP's should be mounted back to back if used at the edge of cuts adjacent to two-way two lane roadways. Stripes are to be reflective orange and reflective white and should always slope downward toward the travel lane.
- VP's used on expressways and freeways or other high speed roadways, may have more than 270 square inches of retroreflective area facing traffic.
- Self-righting supports are available with portable base. See "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- Sheeting for the VP's shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300, unless noted otherwise.
- Where the height of reflective material on the vertical panel is 36 inches or greater, a panel stripe of 6 inches shall be used.



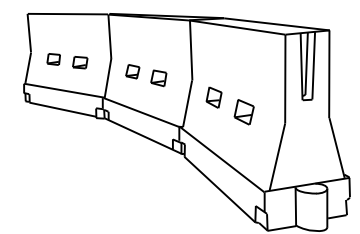
OPPOSING TRAFFIC LANE DIVIDERS (OTLD)

- Opposing Traffic Lane Dividers (OTLD) are delineation devices designed to convert a normal one-way roadway section to two-way operation. OTLD's are used on temporary centerlines. The upward and downward arrows on the sign's face indicate the direction of traffic on either side of the divider. The base is secured to the pavement with an adhesive or rubber weight to minimize movement caused by a vehicle impact or wind gust.
- The OTLD may be used in combination with 42" cones or VPs.
- Spacing between the OTLD shall not exceed 500 feet. 42" cones or VPs placed between the OTLD's should not exceed 100 foot spacing.
- The OTLD shall be orange with a black non-reflective legend. Sheeting for the OTLD shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.



- The chevron shall be a vertical rectangle with a minimum size of 12 by 18 inches.
- Chevrons are intended to give notice of a sharp change of alignment with the direction of travel and provide additional emphasis and guidance for vehicle operators with regard to changes in horizontal alignment of the roadway.
- Chevrons, when used, shall be erected on the outside of a sharp curve or turn, or on the far side of an intersection. They shall be in line with and at right angles to approaching traffic. Spacing should be such that the motorist always has three in view, until the change in alignment eliminates its need.
- To be effective, the chevron should be visible for at least 500 feet.
- Chevrons shall be orange with a black nonreflective legend. Sheeting for the chevron shall be retroreflective Type B_{FL} or Type C_{FL} conforming to Departmental Material Specification DMS-8300, unless noted otherwise. The legend shall meet the requirements of DMS-8300.
- For Long Term Stationary use on tapers or transitions on freeways and divided highways, self-righting chevrons may be used to supplement plastic drums but not to replace plastic drums.

CHEVRONS



LONGITUDINAL CHANNELIZING DEVICES (LCD)

- LCDs are crashworthy, lightweight, deformable devices that are highly visible, have good target value and can be connected together. They are not designed to contain or redirect a vehicle on impact.
- LCDs may be used instead of a line of cones or drums.
- LCDs shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- LCDs should not be used to provide positive protection for obstacles, pedestrians or workers.
- LCDs shall be supplemented with retroreflective delineation as required for temporary barriers on BC(7) when placed roughly parallel to the travel lanes.
- LCDs used as barricades placed perpendicular to traffic should have at least one row of reflective sheeting meeting the requirements for barricade rails as shown on BC(10). Place reflective sheeting near the top of the LCD along the full length of the device.

WATER BALLASTED SYSTEMS USED AS BARRIERS

- Water ballasted systems used as barriers shall not be used solely to channelize road users, but also to protect the work space per the appropriate Manual for Assessing Safety Hardware (MASH) crashworthiness requirements based on roadway speed and barrier application.
- Water ballasted systems used to channelize vehicular traffic shall be supplemented with retroreflective delineation or channelizing devices to improve daytime/nighttime visibility. They may also be supplemented with pavement markings.
- Water ballasted systems used as barriers shall be placed in accordance to application and installation requirements specific to the device, and used only when shown on the CWZTCD list.
- Water ballasted systems used as barriers should not be used for a merging taper except in low speed (less than 45 MPH) urban areas. When used on a taper in a low speed urban area, the taper shall be delineated and the taper length should be designed to optimize road user operations considering the available geometric conditions.
- When water ballasted systems used as barriers have blunt ends exposed to traffic, they should be attenuated as per manufacturer recommendations or flared to a point outside the clear zone.

If used to channelize pedestrians, longitudinal channelizing devices or water ballasted systems must have a continuous detectable bottom for users of long canes and the top of the unit shall not be less than 32 inches in height.

HOLLOW OR WATER BALLASTED SYSTEMS USED AS LONGITUDINAL CHANNELIZING DEVICES OR BARRIERS

GENERAL NOTES

- Work Zone channelizing devices illustrated on this sheet may be installed in close proximity to traffic and are suitable for use on high or low speed roadways. The Engineer/Inspector shall ensure that spacing and placement is uniform and in accordance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Channelizing devices shown on this sheet may have a driveable, fixed or portable base. The requirement for self-righting channelizing devices must be specified in the General Notes or other plan sheets.
- Channelizing devices on self-righting supports should be used in work zone areas where channelizing devices are frequently impacted by errant vehicles or vehicle related wind gusts making alignment of the channelizing devices difficult to maintain. Locations of these devices shall be detailed elsewhere in the plans. These devices shall conform to the TMUTCD and the "Compliant Work Zone Traffic Control Devices List" (CWZTCD).
- The Contractor shall maintain devices in a clean condition and replace damaged, nonreflective, faded, or broken devices and bases as required by the Engineer/Inspector. The Contractor shall be required to maintain proper device spacing and alignment.
- Portable bases shall be fabricated from virgin and/or recycled rubber. The portable bases shall weigh a minimum of 30 lbs.
- Pavement surfaces shall be prepared in a manner that ensures proper bonding between the adhesives, the fixed mount bases and the pavement surface. Adhesives shall be prepared and applied according to the manufacturer's recommendations.
- The installation and removal of channelizing devices shall not cause detrimental effects to the final pavement surfaces, including pavement surface discoloration or surface integrity. Driveable bases shall not be permitted on final pavement surfaces. The Engineer/Inspector shall approve all application and removal procedures of fixed bases.

Posted Speed	Formula	Minimum Desirable Taper Lengths * *			Suggested Maximum Spacing of Channelizing Devices	
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent
30	L = WS ² / 60	150'	165'	180'	30'	60'
35		205'	225'	245'	35'	70'
40		265'	295'	320'	40'	80'
45	L = WS	450'	495'	540'	45'	90'
50		500'	550'	600'	50'	100'
55		550'	605'	660'	55'	110'
60		600'	660'	720'	60'	120'
65		650'	715'	780'	65'	130'
70		700'	770'	840'	70'	140'
75		750'	825'	900'	75'	150'
80		800'	880'	960'	80'	160'

* * * Taper lengths have been rounded off.
 L=Length of Taper (FT.) W=Width of Offset (FT.)
 S=Posted Speed (MPH)

SUGGESTED MAXIMUM SPACING OF CHANNELIZING DEVICES AND MINIMUM DESIRABLE TAPER LENGTHS

SHEET 9 OF 12



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (9) - 21

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TYPE 3 BARRICADES

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTCD) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road, striping should slope downward in both directions toward the center of roadway.
4. Striping of rails, for the right side of the roadway, should slope downward to the left. For the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rails. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand bags shall not be stacked in a manner that covers any portion of a barricade rails reflective sheeting. Rock, concrete, iron, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed along or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheeting for barricades shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300 unless otherwise noted.

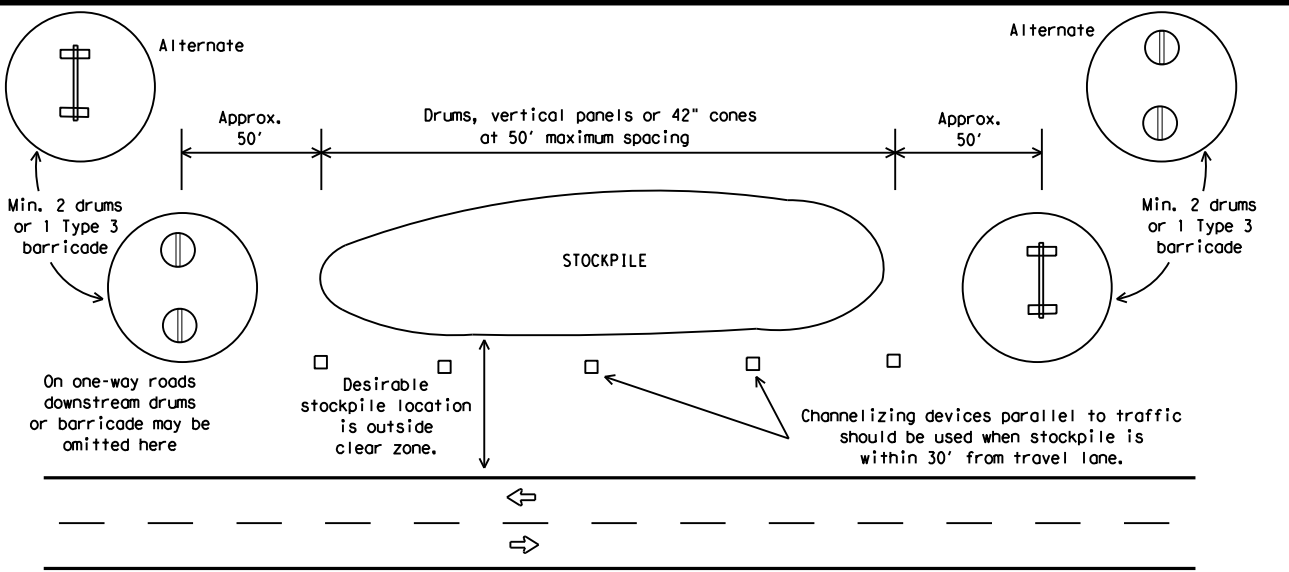
Barricades shall NOT be used as a sign support.



TYPICAL STRIPING DETAIL FOR BARRICADE RAIL

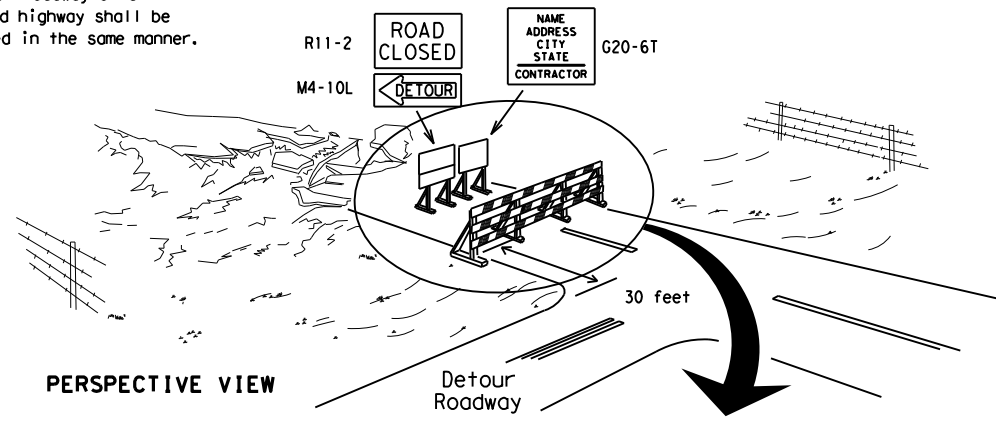


TYPICAL PANEL DETAIL FOR SKID OR POST TYPE BARRICADES



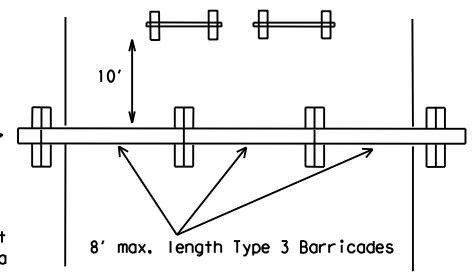
TRAFFIC CONTROL FOR MATERIAL STOCKPILES

Each roadway of a divided highway shall be barricaded in the same manner.



PERSPECTIVE VIEW

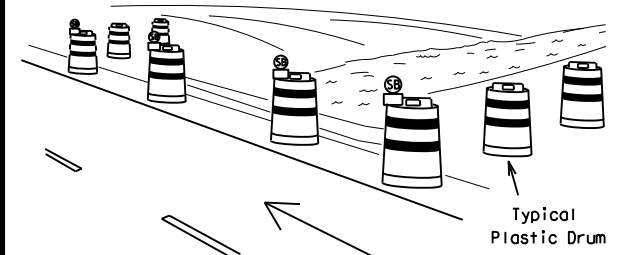
The three rails on Type 3 barricades shall be reflectorized orange and reflective white stripes on one side facing one-way traffic and both sides for two-way traffic. Barricade striping should slant downward in the direction of detour.



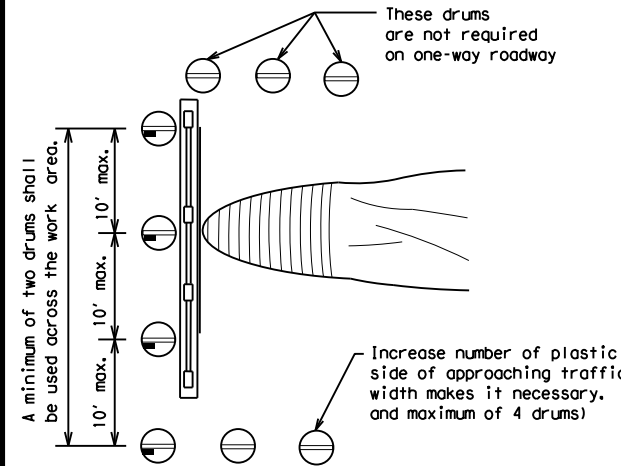
PLAN VIEW

1. Signs should be mounted on independent supports at a 7 foot mounting height in center of roadway. The signs should be a minimum of 10 feet behind Type 3 Barricades.
2. Advance signing shall be as specified elsewhere in the plans.

TYPE 3 BARRICADE (POST AND SKID) TYPICAL APPLICATION



PERSPECTIVE VIEW

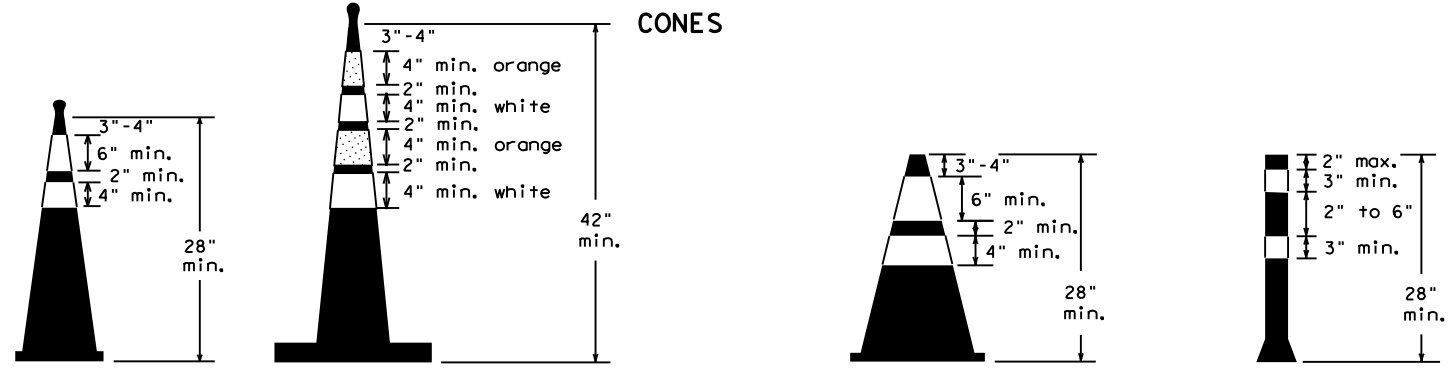


PLAN VIEW

CULVERT WIDENING OR OTHER ISOLATED WORK WITHIN THE PROJECT LIMITS

1. Where positive redirection capability is provided, drums may be omitted.
2. Plastic construction fencing may be used with drums for safety as required in the plans.
3. Vertical Panels on flexible support may be substituted for drums when the shoulder width is less than 4 feet.
4. When the shoulder width is greater than 12 feet, steady-burn lights may be omitted if drums are used.
5. Drums must extend the length of the culvert widening.

LEGEND	
	Plastic drum
	Plastic drum with steady burn light or yellow warning reflector
	Steady burn warning light or yellow warning reflector



Two-Piece cones

One-Piece cones

Tubular Marker

28" Cones shall have a minimum weight of 9 1/2 lbs.
42" 2-piece cones shall have a minimum weight of 30 lbs. including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A or Type B.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined on BC(4). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone durations.
7. Cones or tubular markers used on each project should be of the same size and shape.



BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (10) - 21

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7-13	5-21	SJT		MENARD					19

WORK ZONE PAVEMENT MARKINGS

GENERAL

- The Contractor shall be responsible for maintaining work zone and existing pavement markings, in accordance with the standard specifications and special provisions, on all roadways open to traffic within the CSJ limits unless otherwise stated in the plans.
- Color, patterns and dimensions shall be in conformance with the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
- Additional supplemental pavement marking details may be found in the plans or specifications.
- Pavement markings shall be installed in accordance with the TMUTCD and as shown on the plans.
- When short term markings are required on the plans, short term markings shall conform with the TMUTCD, the plans and details as shown on the Standard Plan Sheet WZ(STPM).
- When standard pavement markings are not in place and the roadway is opened to traffic, DO NOT PASS signs shall be erected to mark the beginning of the sections where passing is prohibited and PASS WITH CARE signs at the beginning of sections where passing is permitted.
- All work zone pavement markings shall be installed in accordance with Item 662, "Work Zone Pavement Markings."

RAISED PAVEMENT MARKERS

- Raised pavement markers are to be placed according to the patterns on BC(12).
- All raised pavement markers used for work zone markings shall meet the requirements of Item 672, "RAISED PAVEMENT MARKERS" and Departmental Material Specification DMS-4200 or DMS-4300.

PREFABRICATED PAVEMENT MARKINGS

- Removable prefabricated pavement markings shall meet the requirements of DMS-8241.
- Non-removable prefabricated pavement markings (foil back) shall meet the requirements of DMS-8240.

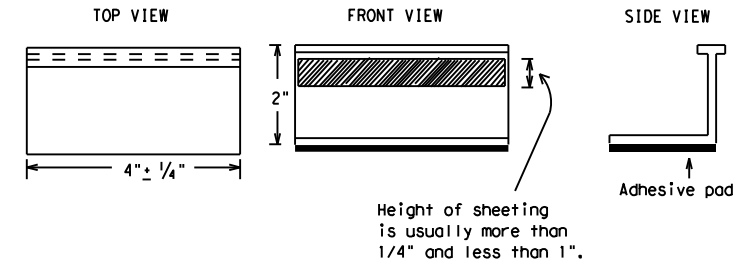
MAINTAINING WORK ZONE PAVEMENT MARKINGS

- The Contractor will be responsible for maintaining work zone pavement markings within the work limits.
- Work zone pavement markings shall be inspected in accordance with the frequency and reporting requirements of work zone traffic control device inspections as required by Form 599.
- The markings should provide a visible reference for a minimum distance of 300 feet during normal daylight hours and 160 feet when illuminated by automobile low-beam headlights at night, unless sight distance is restricted by roadway geometrics.
- Markings failing to meet this criteria within the first 30 days after placement shall be replaced at the expense of the Contractor as per Specification Item 662.

REMOVAL OF PAVEMENT MARKINGS

- Pavement markings that are no longer applicable, could create confusion or direct a motorist toward or into the closed portion of the roadway shall be removed or obliterated before the roadway is opened to traffic.
- The above shall not apply to detours in place for less than three days, where flaggers and/or sufficient channelizing devices are used in lieu of markings to outline the detour route.
- Pavement markings shall be removed to the fullest extent possible, so as not to leave a discernable marking. This shall be by any method approved by TxDOT Specification Item 677 for "Eliminating Existing Pavement Markings and Markers".
- The removal of pavement markings may require resurfacing or seal coating portions of the roadway as described in Item 677.
- Subject to the approval of the Engineer, any method that proves to be successful on a particular type pavement may be used.
- Blast cleaning may be used but will not be required unless specifically shown in the plans.
- Over-painting of the markings SHALL NOT BE permitted.
- Removal of raised pavement markers shall be as directed by the Engineer.
- Removal of existing pavement markings and markers will be paid for directly in accordance with Item 677, "ELIMINATING EXISTING PAVEMENT MARKINGS AND MARKERS," unless otherwise stated in the plans.
- Black-out marking tape may be used to cover conflicting existing markings for periods less than two weeks when approved by the Engineer.

Temporary Flexible-Reflective Roadway Marker Tabs



**STAPLES OR NAILS SHALL NOT BE USED TO SECURE
TEMPORARY FLEXIBLE-REFLECTIVE ROADWAY MARKER
TABS TO THE PAVEMENT SURFACE**

- Temporary flexible-reflective roadway marker tabs used as guidemarks shall meet the requirements of DMS-8242.
- Tabs detailed on this sheet are to be inspected and accepted by the Engineer or designated representative. Sampling and testing is not normally required, however at the option of the Engineer, either "A" or "B" below may be imposed to assure quality before placement on the roadway.
 - Select five (5) or more tabs at random from each lot or shipment and submit to the Construction Division, Materials and Pavement Section to determine specification compliance.
 - Select five (5) tabs and perform the following test. Affix five (5) tabs at 24 inch intervals on an asphaltic pavement in a straight line. Using a medium size passenger vehicle or pickup, run over the markers with the front and rear tires at a speed of 35 to 40 miles per hour, four (4) times in each direction. No more than one (1) out of the five (5) reflective surfaces shall be lost or displaced as a result of this test.
- Small design variances may be noted between tab manufacturers.
- See Standard Sheet WZ(STPM) for tab placement on new pavements. See Standard Sheet TCP(7-1) for tab placement on seal coat work.

RAISED PAVEMENT MARKERS USED AS GUIDEMARKS

- Raised pavement markers used as guidemarks shall be from the approved product list, and meet the requirements of DMS-4200.
- All temporary construction raised pavement markers provided on a project shall be of the same manufacturer.
- Adhesive for guidemarks shall be bituminous material hot applied or butyl rubber pad for all surfaces, or thermoplastic for concrete surfaces.

Guidemarks shall be designated as:
 YELLOW - (two amber reflective surfaces with yellow body).
 WHITE - (one silver reflective surface with white body).

DEPARTMENTAL MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
TRAFFIC BUTTONS	DMS-4300
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240
TEMPORARY REMOVABLE, PREFABRICATED PAVEMENT MARKINGS	DMS-8241
TEMPORARY FLEXIBLE, REFLECTIVE ROADWAY MARKER TABS	DMS-8242

A list of prequalified reflective raised pavement markers, non-reflective traffic buttons, roadway marker tabs and other pavement markings can be found at the Material Producer List web address shown on BC(1).

SHEET 11 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKINGS

BC(11)-21

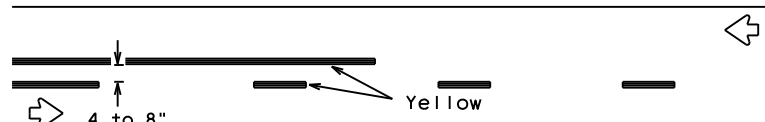
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PAVEMENT MARKING PATTERNS

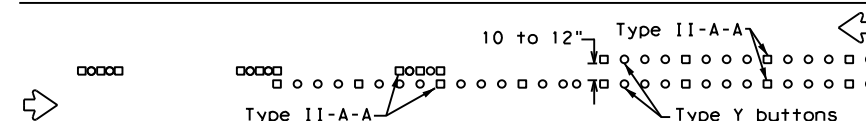


REFLECTORIZED PAVEMENT MARKINGS - PATTERN A

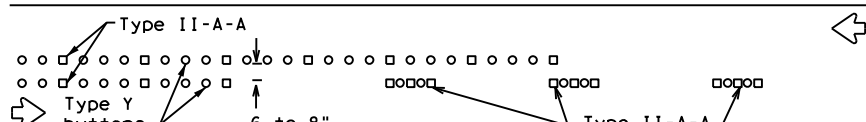


REFLECTORIZED PAVEMENT MARKINGS - PATTERN B

Pattern A is the TXDOT Standard, however Pattern B may be used if approved by the Engineer. Prefabricated markings may be substituted for reflectorized pavement markings.



RAISED PAVEMENT MARKERS - PATTERN A



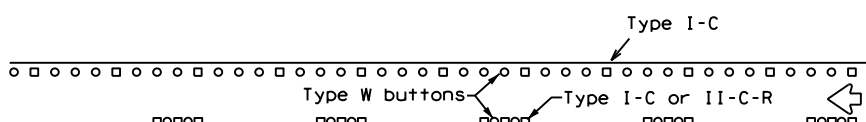
RAISED PAVEMENT MARKERS - PATTERN B

CENTER LINE & NO-PASSING ZONE BARRIER LINES FOR TWO-LANE, TWO-WAY HIGHWAYS



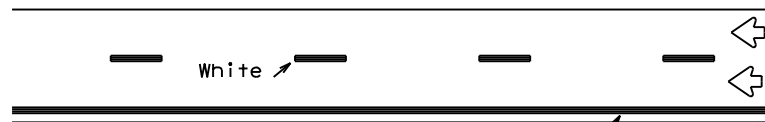
REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectorized pavement markings.



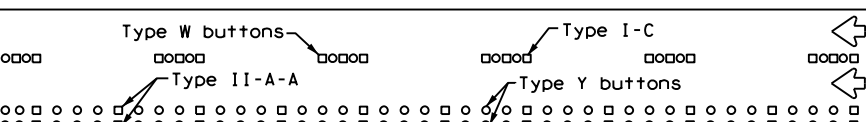
RAISED PAVEMENT MARKERS

EDGE & LANE LINES FOR DIVIDED HIGHWAY



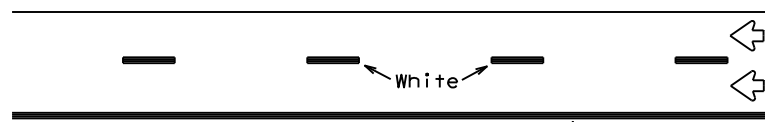
REFLECTORIZED PAVEMENT MARKINGS

Prefabricated markings may be substituted for reflectorized pavement markings.



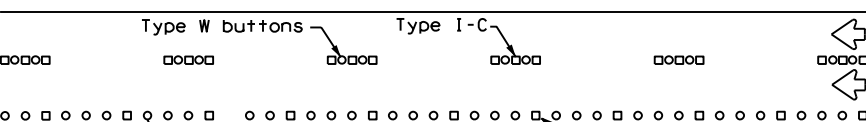
RAISED PAVEMENT MARKERS

LANE & CENTER LINES FOR MULTILANE UNDIVIDED HIGHWAYS



REFLECTORIZED PAVEMENT MARKINGS

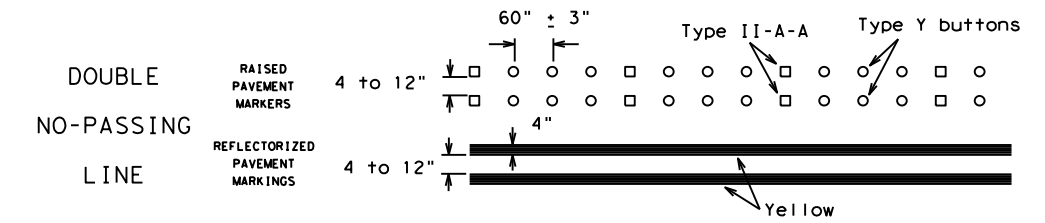
Prefabricated markings may be substituted for reflectorized pavement markings.



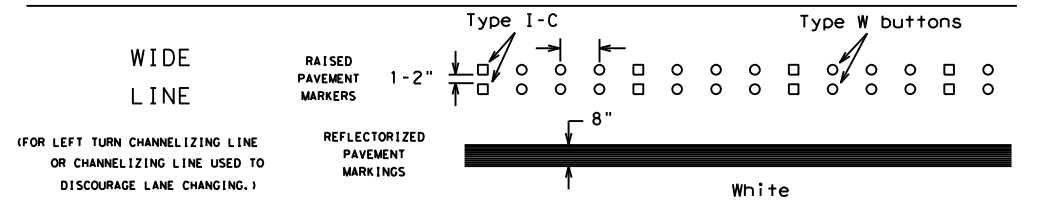
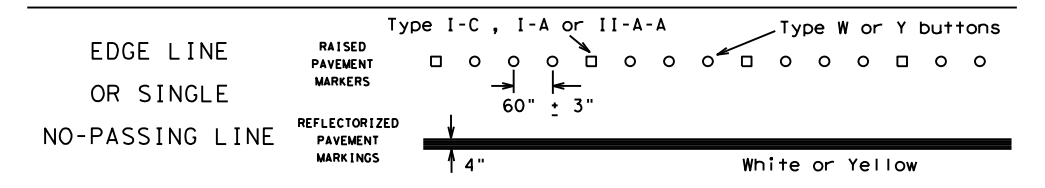
RAISED PAVEMENT MARKERS

TWO-WAY LEFT TURN LANE

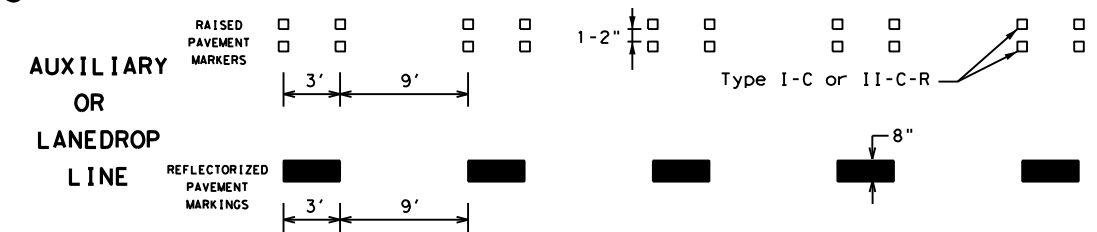
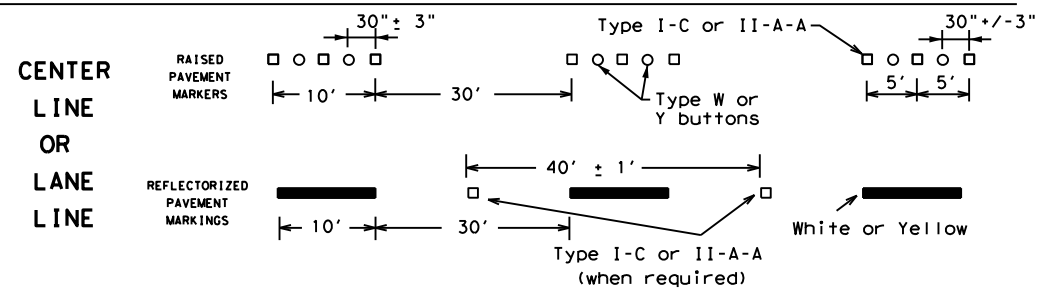
STANDARD WORK ZONE PAVEMENT MARKINGS DETAILS



SOLID LINES

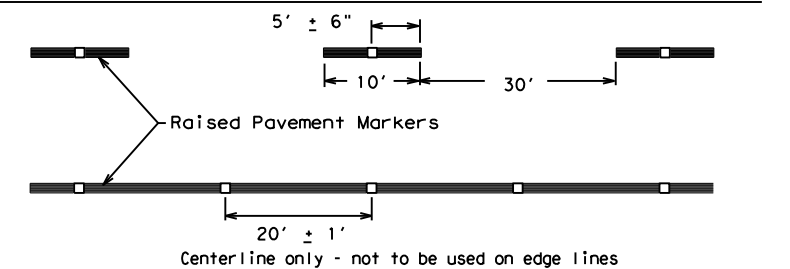


BROKEN LINES



REMOVABLE MARKINGS WITH RAISED PAVEMENT MARKERS

If raised pavement markers are used to supplement REMOVABLE markings, the markers shall be applied to the top of the tape at the approximate mid length of tape used for broken lines or at 20 foot spacing for solid lines. This allows an easier removal of raised pavement markers and tape.



SHEET 12 OF 12



BARRICADE AND CONSTRUCTION PAVEMENT MARKING PATTERNS

BC (12) - 21

FILE: bc-21.dgn	DN: TxDOT	CK: TxDOT	OW: TxDOT	CR: TxDOT
©TxDOT February 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS	6383	49	001	US 83, ETC
1-97 9-07 5-21	DIST	COUNTY	SHEET NO.	
2-98 7-13	SJT	MENARD	21	
11-02 8-14				

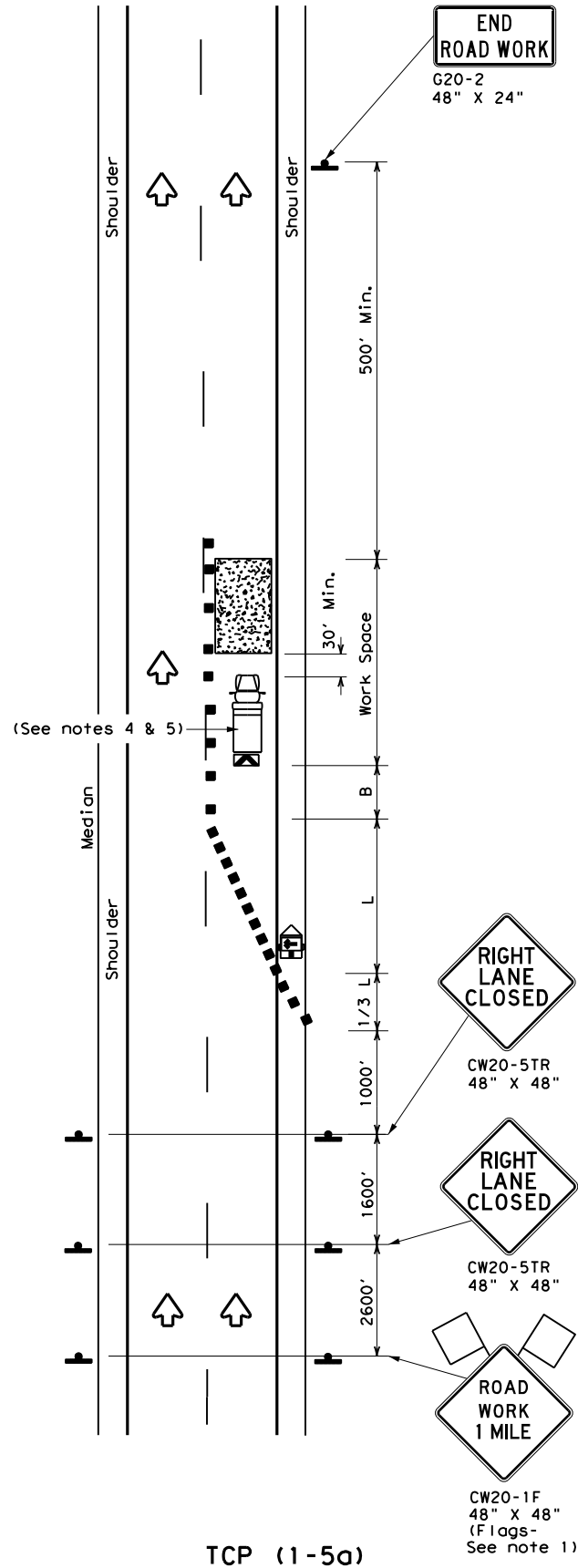
Raised pavement markers used as standard pavement markings shall be from the approved products list and meet the requirements of Item 672 "RAISED PAVEMENT MARKERS."

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TXDOT for any purpose whatsoever. TXDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

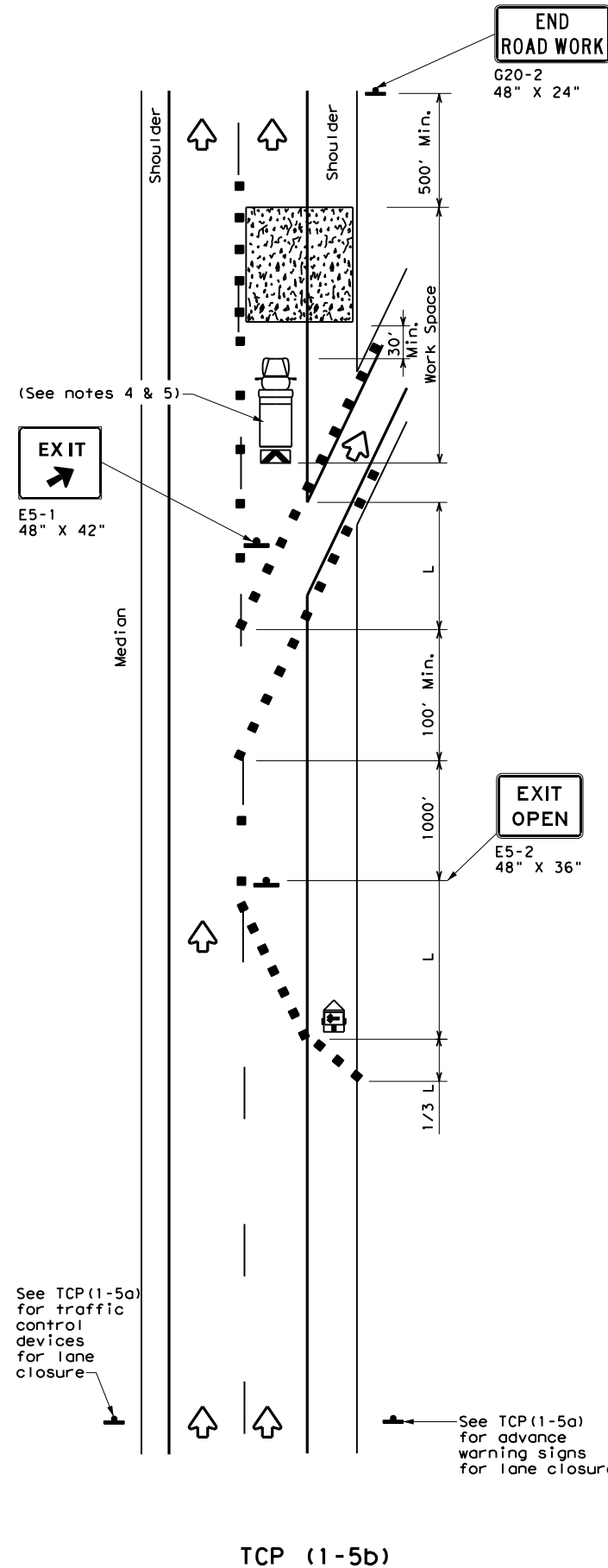
DATE: 6/23/2021 10:34:23 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT2\Documents\07 - SJT\Maintenance Projects\638349001\2 - DESIGN FILES\Plan Sheets\TCP\bc-21.dgn

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the use of this standard for purposes other than those intended by TxDOT.

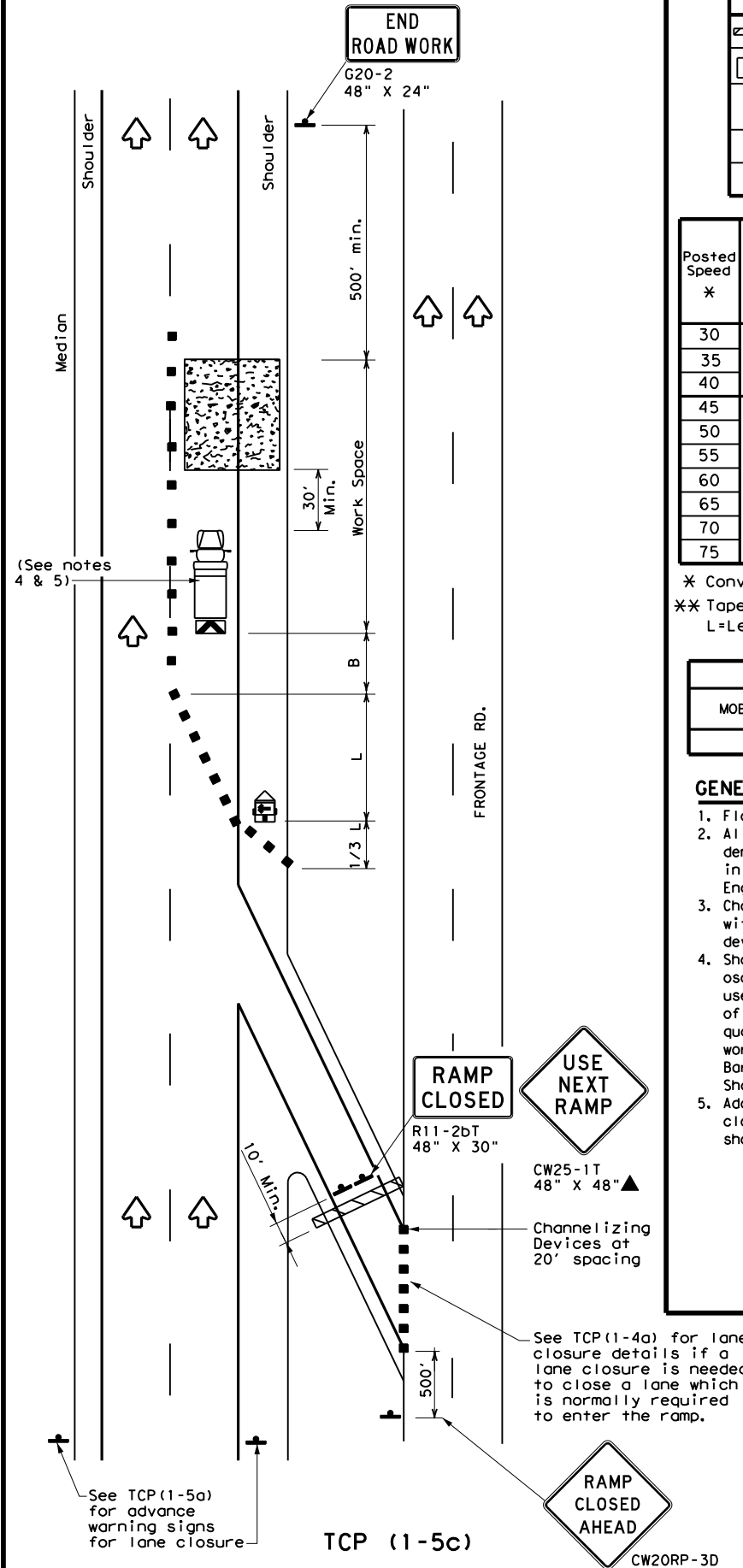
DATE: 6/23/2021 10:35:01 AM
 FILE: \\txdot.projectwiseonline.com:TXDOT12\Documents\07 - SJT\Maintenance\PHS\6383\6383-18.dgn



ONE LANE CLOSURE



LANE CLOSURE NEAR EXIT RAMP



LANE CLOSURE NEAR ENTRANCE RAMP

LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L = WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
		✓		

GENERAL NOTES

- Flags attached to signs where shown, are REQUIRED.
- All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
- Channelizing devices used to close lanes may be supplemented with the Chevron Alignment Sign placed on every other channelizing device. Chevrons may be attached to plastic drums as per BC Standards.
- Shadow Vehicle with TMA and high intensity rotating, flashing, oscillating or strobe lights. A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
- Additional Shadow Vehicles with TMAs may be positioned in each closed lane, on the shoulder or off the paved surface, next to those shown in order to protect a wider work space.

Texas Department of Transportation
 Traffic Operations Division Standard

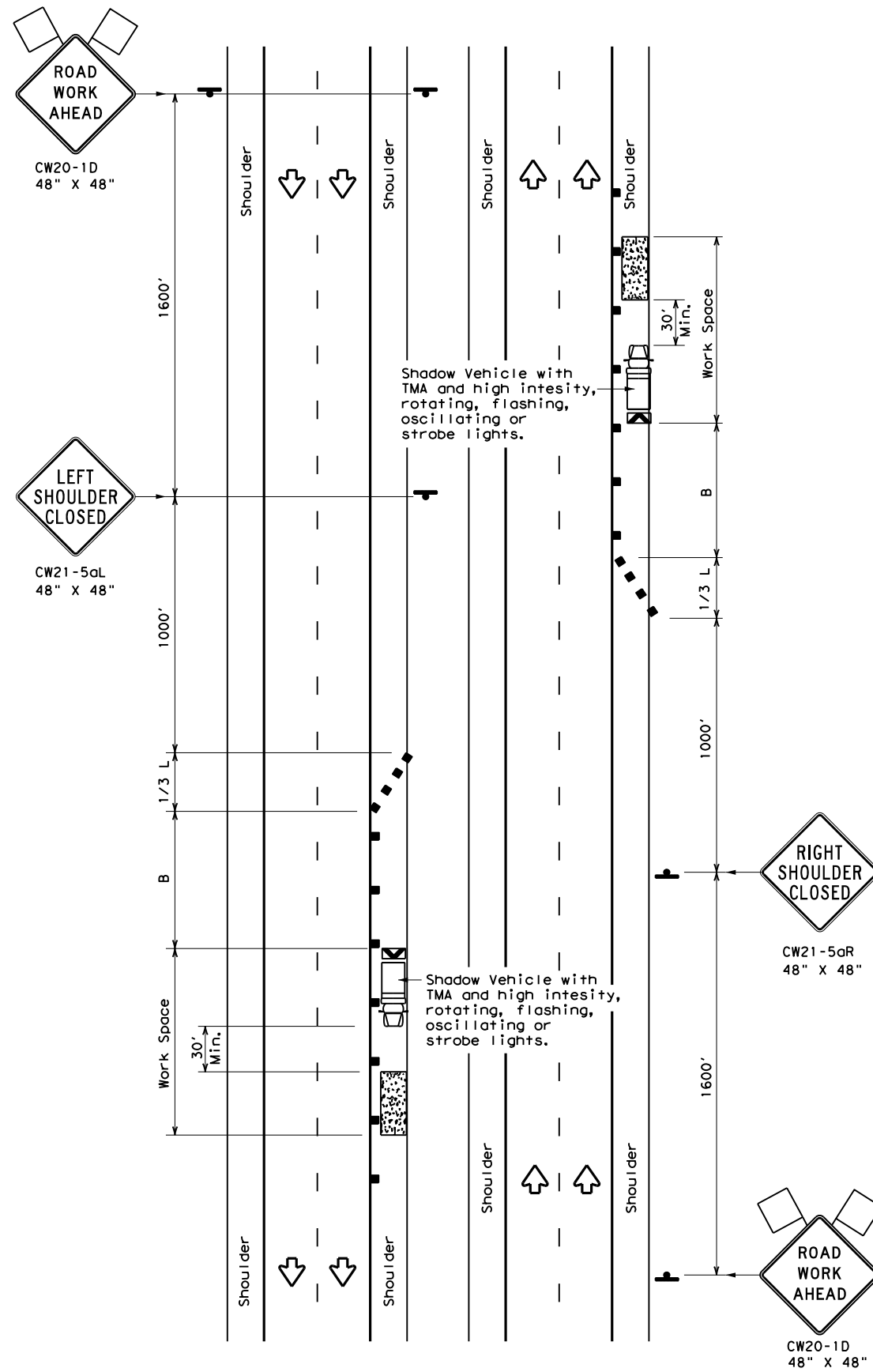
TRAFFIC CONTROL PLAN LANE CLOSURES FOR DIVIDED HIGHWAYS

TCP (1-5) - 18

FILE: tcp1-5-18.dgn	DN:	CK:	DW:	CK:
© TxDOT February 2012	CONT	SECT	JOB	HIGHWAY
2-18	6383	49	001	US 83, ETC
	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	25	

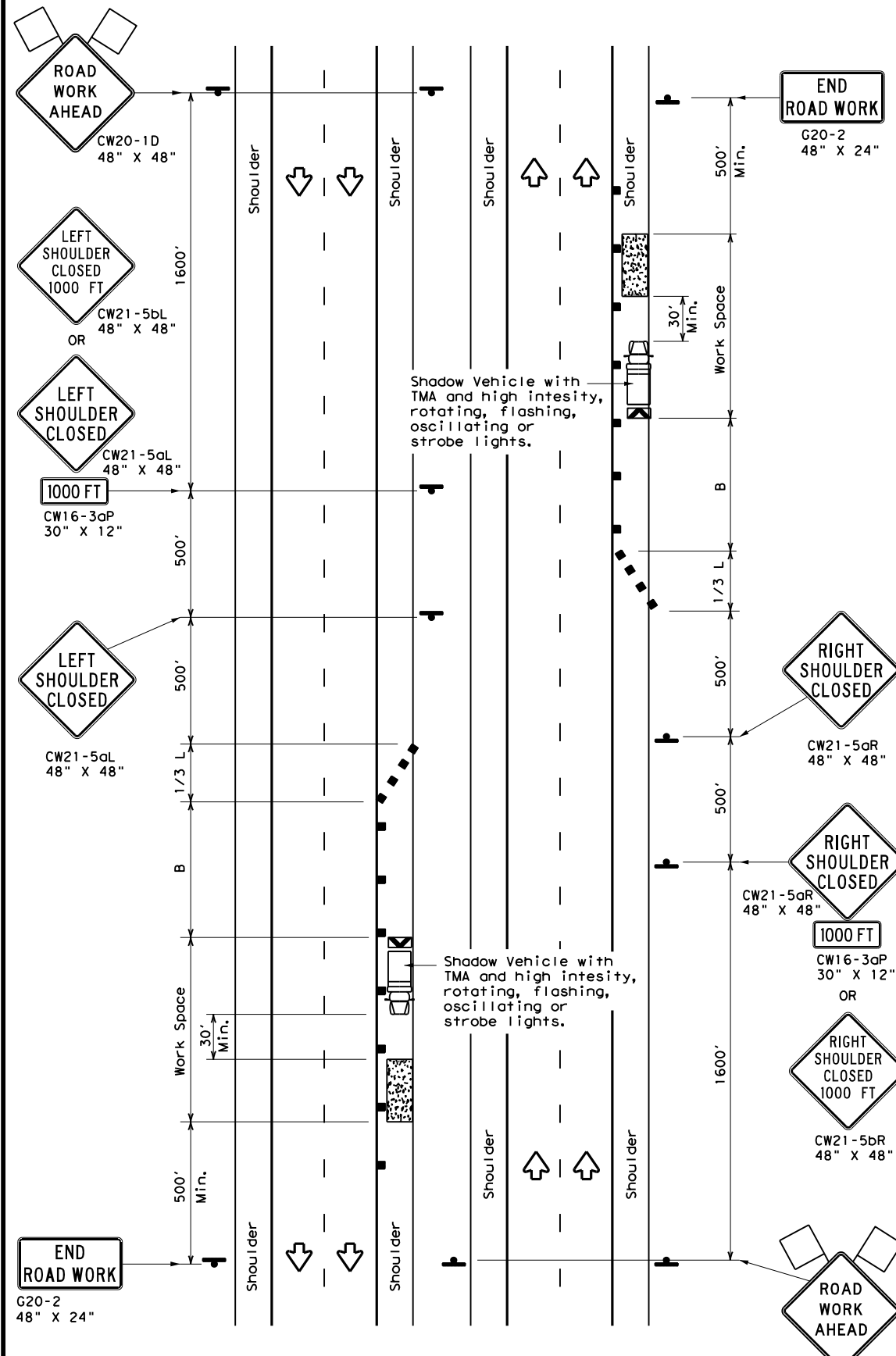
DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of units or the use of this standard for purposes other than those intended by the Texas Department of Transportation.

DATE: 6/23/2021 10:35:09 AM
 FILE: \\txdot\project\wiseonline.com\TXDOT12\Documents\07 - SJT\Maintenance\PHS\6383\6383-18\0718\TCP (5-1) - 18.dgn



TCP (5-1a)

WORK AREA ON SHOULDER



TCP (5-1b)

WORK AREA ON SHOULDER

LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent	
30	L = WS ² / 60	150'	165'	180'	30'	60'	90'
35		205'	225'	245'	35'	70'	120'
40		265'	295'	320'	40'	80'	155'
45	L = WS	450'	495'	540'	45'	90'	195'
50		500'	550'	600'	50'	100'	240'
55		550'	605'	660'	55'	110'	295'
60		600'	660'	720'	60'	120'	350'
65		650'	715'	780'	65'	130'	410'
70		700'	770'	840'	70'	140'	475'
75		750'	825'	900'	75'	150'	540'
80		800'	880'	960'	80'	160'	615'

* Conventional Roads Only
 ** Taper lengths have been rounded off.
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	TCP (5-1a)	TCP (5-1b)	TCP (5-1b)	

GENERAL NOTES

1. A Shadow Vehicle with a TMA should be used anytime it can be positioned 30' to 100' in advance of the area of crew exposure without adversely affecting the performance or quality of the work. Type 3 barricades or drums may be substituted when workers on foot are no longer present when approved by the Engineer.
2. 28" tall or taller one-piece cones will be allowed only for Short Duration or Short Term stationary operations when workers are present to maintain the devices upright and in proper location. Intermediate Term stationary work areas should use Drums, Vertical Panels or 42" tall two-piece cones.



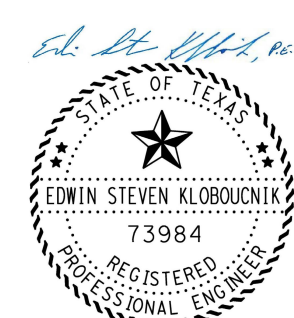
**TRAFFIC CONTROL PLAN
 SHOULDER WORK FOR
 FREEWAYS / EXPRESSWAYS**

TCP (5-1) - 18

FILE: tcp5-1-18.dgn	DN:	CK:	DW:	CK:
© TxDOT February 2012	CONT	SECT	JOB	HIGHWAY
2-18	REVISIONS	6383	49	001
	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	26	

DATE: 6/23/2021 10:35:27 AM
 FILE: pw:\xtdot\project\seonline.com\T\DOT2\Documents\07 - SJT Maintenance Projects\638349001 - 2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT US 83 MENARD COUNTY.dgn

US 83 from Concho County Line to Kimble County Line															0636-6007	0644-6103	644-6104	644-6106	644-6111
Existing Location															REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY 10 BWG (1)(U)	REPLACE SRS & S TY 580 (1)(U-WC)
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA	EA	EA	EA				
1	31.08646841	-99.82174234	Menard	US 83	SB	I-2dT	Menard County Line	54"x24"	Replace Sign & Support				1						
2	31.04738055	-99.82121439	Menard	US 83	SB	W9-1	Right Lane Ends	36"x36"	Replace Sign		9								
3	31.04601106	-99.82120614	Menard	US 83	SB	W9-2T	Lane Ends Merge Left	36"x36"	Replace Sign		9								
4	31.03949992	-99.82114077	Menard	US 83	SB	D15-11T	Next Passing Lane 3 Miles	54"x48"	Replace Sign		18								
5	31.02963428	-99.82109862	Menard	US 83	SB	D15-10T	Passing Lane 2 Miles	54"x42"	Replace Sign		15.75								
6	31.00977899	-99.81335247	Menard	US 83	SB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2								
7	31.00904852	-99.81265551	Menard	US 83	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2								
7A	31.00904852	-99.81265551	Menard	US 83	SB	M1-4	US 83	24"x24"	Replace Sign	Yes	4								
8	31.00803112	-99.81169591	Menard	US 83	SB	D2-2	Menard 6 mi Junction 37 mi	78"x30"	Replace Sign		16.25								
9	30.99254346	-99.80054473	Menard	US 83	SB	W9-1	Right Lane Ends	36"x36"	Replace Sign		9								
10	30.99123113	-99.80006774	Menard	US 83	SB	W9-2T	Lane Ends Merge Left	36"x36"	Replace Sign		9								
11	30.98539735	-99.79713029	Menard	US 83	SB	D15-11T	Next Passing Lane 1 Mile	54"x48"	Replace Sign		18								
12	30.97909181	-99.79409305	Menard	US 83	SB	I-3	Volkman Draw	42"x18"	Replace Sign & Support				1						
13	30.97070958	-99.7902058	Menard	US 83	SB	R4-3	Slower Traffic Keep Right	24"x30"	Replace Sign		5								
14	30.94600159	-99.7822031	Menard	US 83	SB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5								
15	30.94198425	-99.78229731	Menard	US 83	SB	D1-2	Menard Up Arrow Brady Left Arrow	72"x30"	Replace Sign		15								
16	30.93906101	-99.78269227	Menard	US 83	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2								
16A	30.93906101	-99.78269227	Menard	US 83	SB	M1-4	US 83	24"x24"	Replace Sign	Yes	4								
16B	30.93906101	-99.78269227	Menard	US 83	SB	M3-4	WEST	24"x12"	Replace Sign	Yes	2								
16C	30.93906101	-99.78269227	Menard	US 83	SB	M1-4	US 190	30"x24"	Replace Sign	Yes	5								
17	30.93075833	-99.78374966	Menard	US 83	SB	R2-1	Speed Limit 45 MPH	30"x36"	Replace Sign		7.5								
18	30.92534062	-99.78398388	Menard	US 83	SB	R2-1	Speed Limit 40 MPH	30"x36"	Replace Sign		7.5								
19	30.92211491	-99.78468622	Menard	US 83	SB	M3-3	SOUTH	24"x12"	Replace Sign & Support	Yes		1							
19A	30.92211491	-99.78468622	Menard	US 83	SB	M1-4	US 83	24"x24"											
19B	30.92211491	-99.78468622	Menard	US 83	SB	D10-7aT	TRM 446	3"x10"											
19C	30.92211491	-99.78468622	Menard	US 83	SB	D10-7aT	TRM 446	3"x10"											
20	30.91839434	-99.78613097	Menard	US 83	SB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2								
20A	30.91839434	-99.78613097	Menard	US 83	SB	M1-6F	FM 2092	24"x24"	Replace Sign	Yes	4								
21	30.91685398	-99.78661262	Menard	US 83	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2								
22	30.9106541	-99.78841897	Menard	US 83	SB	R2-1	Speed Limit 40 MPH	30"x36"	Replace Sign		7.5								
23	30.90843611	-99.78843168	Menard	US 83	SB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2								
23A	30.90843611	-99.78843168	Menard	US 83	SB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4								
24	30.90819455	-99.78843442	Menard	US 83	SB	W11-12T	Watch For Emergency Vehicles	36"x36"	Replace Sign		9								
26	30.90701731	-99.78839111	Menard	US 83	SB	M6-2L	Up Left Diagonal Arrow/White	21"x15"	Replace Sign	Yes	2.2								
26A	30.90701731	-99.78839111	Menard	US 83	SB	M6-2R	Up Right Diagonal Arrow/White	21"x15"	Replace Sign	Yes	2.2								
27	30.90626483	-99.78823384	Menard	US 83	SB	R2-1	Speed Limit 55 MPH	30"x36"	Replace Sign & Support			1							
28	30.90601774	-99.78815916	Menard	US 83	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2								
28A	30.90601774	-99.78815916	Menard	US 83	SB	M1-4	US 83	24"x24"	Replace Sign	Yes	4								
29	30.90575782	-99.78804557	Menard	US 83	SB	I-3	Harris Hollow	36"x18"	Replace Sign & Support				1						
30	30.90129632	-99.78486732	Menard	US 83	SB	R2-1	Speed Limit 65 MPH	30"x36"	Replace Sign		7.5								
31	30.89739653	-99.78172142	Menard	US 83	SB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign & Support			1							
32	30.88492336	-99.76882401	Menard	US 83	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign & Support				1						
33	30.88488687	-99.76861988	Menard	US 83	SB	R5-1	Do Not Enter	36"x36"	Replace Sign		9								
34	30.88374578	-99.76756005	Menard	US 83	SB	W1-2R	Curve Symbol	36"x36"	Replace Sign & Support	Yes		1							
34A	30.88374578	-99.76756005	Menard	US 83	SB	W13-1P	60 MPH Advisory	24"x24"											
35	30.87801875	-99.76397226	Menard	US 83	SB	W6-2	Divided Highway Symbol	36"x36"	Replace Sign & Support			1							
36	30.87607552	-99.7640288	Menard	US 83	SB	W6-3	Two Way Traffic Symbol	36"x36"	Replace Sign		9								
37	30.87321454	-99.76413456	Menard	US 83	SB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5								
38	30.84153506	-99.76527556	Menard	US 83	SB	D14-4T	This section Available for Adoption	48"x24"	Replace Sign & Support				1						
39	30.82946824	-99.7655252	Menard	US 83	SB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9								
40	30.79058434	-99.76630192	Menard	US 83	SB	W13-1P	60 MPH Advisory	24"x24"	Replace Sign	Yes	4								
41	30.78855986	-99.76633818	Menard	US 83	SB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2								
42	30.78855986	-99.76633818	Menard	US 83	SB	M1-6R	RM 1773	24"x24"	Replace Sign	Yes	4								
43	30.78558904	-99.7668599	Menard	US 83	SB	D1-1	Junction Up Arrow	78"x18"	Replace Sign & Support				1						
44	30.77339999	-99.77064224	Menard	US 83	SB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9								
45	30.72534721	-99.77116428	Menard	US 83	SB	D15-10T	Passing Lane 2 Miles	54"x42"	Replace Sign		15.75								
46	30.71372755	-99.77127309	Menard	US 83	SB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9								
47	30.7511666	-99.77072735	Menard	US 83	NB	W9-2T	Lane Ends Merge Left	36"x36"	Replace Sign		9								
48	30.76867526	-99.77052494	Menard	US 83	NB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9								
49	30.776644	-99.77040759	Menard	US 83	NB	W1-4R	Curve Symbol	36"x36"	Replace Sign	Yes	9								
49A	30.776644	-99.77040759	Menard	US 83	NB	W13-1P	60 MPH Advisory	24"x24"	Replace Sign	Yes	4								
50	30.77941559	-99.77036268	Menard	US 83	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2								
50A	30.77941559	-99.77036268	Menard	US 83	NB	M1-6R	RM 1773	21"x15"	Replace Sign	Yes	4								
TOTAL											332.15	5	5	1	0				



23.06.2021

San Angelo District

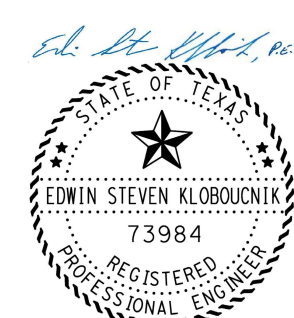
REPLACE SIGNS
 US 83
 MENARD COUNTY

SHEET 1 OF 8
NOT TO SCALE

2021 <small>SHEET ISSUED OR LAST REVISED</small>	<small>CONT</small> 6383 <small>SECT</small> 49	<small>JOB</small> 001 <small>COUNTY</small> MENARD	<small>HIGHWAY</small> US 83, ETC <small>SHEET NO.</small> 28
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DATE: 6/23/2021 10:35:36 AM FILE: \\pwwork\project\seonline\DOT2\Documents\07 - SJT Maintenance\Projects\638349001 - 2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT US 83 MENARD COUNTY-2.dgn

US 83 from Concho County Line to Kimble County Line											0636-6007	0644-6103	644-6104	644-6106	644-6111
Existing Location											REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY 10 BWG (1)(U)	REPLACE SRS & S TY 580 (1)(U-WC)
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA	EA	EA	EA
51	30.78212163	-99.76946498	Menard	US 83	NB	D1-1	Menard Up Arrow	66"x18"	Replace Sign & Support				1		
52	30.78586426	-99.76648195	Menard	US 83	NB	D2-2	Menard 10 mi Eden 32 mi	72"x30"	Replace Sign		15				
53	30.78952624	-99.76612697	Menard	US 83	NB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9				
54	30.81285497	-99.76564287	Menard	US 83	NB	D14-4T	This section Available for Adoption	48"x24"	Replace Sign & Support				1		
55	30.82398496	-99.76542587	Menard	US 83	NB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9				
56	30.84181183	-99.76507713	Menard	US 83	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4				
57	30.842068	-99.765057	Menard	US 83	NB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
58	30.87507776	-99.76385027	Menard	US 83	NB	W1-4R	Curve Symbol	36"x36"	Replace Sign & Support	Yes		1			
58A	30.87507776	-99.76385027	Menard	US 83	NB	W13-1P	55 MPH Advisory	24"x24"							
59	30.87570919	-99.76382468	Menard	US 83	NB	W6-1	Divided Highway Symbol	36"x36"	Replace Sign & Support			1			
60	30.87806544	-99.76383101	Menard	US 83	NB	R4-7b	Keep Right	24"x30"	Replace Sign	Yes	5				
61	30.87831788	-99.76365555	Menard	US 83	NB	W13-2	Exit 35 MPH	36"x48"	Replace Sign		12				
62	30.88240922	-99.76405009	Menard	US 83	NB	R5-1	Do Not Enter	36"x36"	Replace Sign		9				
63	30.88486936	-99.76726126	Menard	US 83	NB	R5-1	Do Not Enter	36"x36"	Replace Sign		9				
64	30.88537287	-99.76805221	Menard	US 83	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4				
65	30.88537287	-99.76805221	Menard	US 83	NB	M6-1	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2				
66	30.88622813	-99.77025193	Menard	US 83	NB	W9-2T	Lane Ends Merge Left	36"x36"	Replace Sign		9				
67	30.88803492	-99.7729215	Menard	US 83	NB	D2-2	Menard 3 mi Eden 25 mi	66"x30"	Replace Sign & Support				1		
68	30.895023	-99.77956582	Menard	US 83	NB	W3-5	Speed Limit 65 Ahead	36"x36"	Replace Sign		9				
69	30.89754106	-99.78159577	Menard	US 83	NB	R2-1	Speed Limit 65 MPH	30"x36"	Replace Sign		7.5				
70	30.90174303	-99.784962	Menard	US 83	NB	D21-1TR	Lone Star Alley Right	90"x12"	Replace Sign		7.5				
71	30.902449	-99.78555	Menard	US 83	NB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1			
72	30.90344875	-99.78635258	Menard	US 83	NB	W3-5	Speed Limit 40 Ahead	36"x36"	Replace Sign		9				
73	30.90502286	-99.78750367	Menard	US 83	NB	D1-3	Eden Up Arrow Eldorado Up Arrow Cleo Left Arrow	84"x42"	Replace Sign & Support					1	
74	30.90550306	-99.787757	Menard	US 83	NB	I-3	Harris Hollow	36"x18"	Replace Sign & Support				1		
75	30.90629846	-99.78803103	Menard	US 83	NB	R2-1	Speed Limit 40 MPH	30"x36"	Replace Sign & Support			1			
76	30.90673723	-99.78811476	Menard	US 83	NB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2				
76A	30.90673723	-99.78811476	Menard	US 83	NB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4				
76B	30.90673723	-99.78811476	Menard	US 83	NB	M6-1	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2				
77	30.90739232	-99.78819182	Menard	US 83	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2				
77A	30.90739232	-99.78819182	Menard	US 83	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4				
78	30.90910779	-99.78819714	Menard	US 83	NB	S1-1	School Crossing	36"x36"	Replace Sign	Yes	9				
78A	30.90910779	-99.78819714	Menard	US 83	NB	SW16-9P	Ahead	24"x12"	Replace Sign	Yes	2				
79	30.91319074	-99.78752127	Menard	US 83	NB	SW16-7P	Down Left Diagonal Arrow/Yellow	24"x12"	Replace Sign	Yes	2				
80	30.91450524	-99.78710922	Menard	US 83	NB	R2-1	Speed Limit 35 MPH	30"x36"	Replace Sign		7.5				
81	30.91604909	-99.78661219	Menard	US 83	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
81A	30.91604909	-99.78661219	Menard	US 83	NB	M1-6R	FM 2092	24"x24"	Replace Sign	Yes	4				
82	30.91802	-99.786051	Menard	US 83	NB	R2-1	Speed Limit 40 MPH	30"x36"	Replace Sign		7.5				
83	30.92166944	-99.78463051	Menard	US 83	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
83A	30.92166944	-99.78463051	Menard	US 83	NB	M1-4	US 190	30"x24"	Replace Sign	Yes	5				
84	30.92264757	-99.7842485	Menard	US 83	NB	R2-1	Speed Limit 40 MPH	30"x36"	Replace Sign		7.5				
85	30.92296406	-99.78410989	Menard	US 83	NB	D1-2	Eden Up Arrow Eldorado Left Arrow	84"x30"	Replace Sign		17.5				
86	30.92351739	-99.7839285	Menard	US 83	NB	W8-18	Road May Flood	36"x36"	Replace Sign		9				
87	30.92378737	-99.78387411	Menard	US 83	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2				
87A	30.92378737	-99.78387411	Menard	US 83	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4				
88	30.9254396	-99.783784	Menard	US 83	NB	R2-1	Speed Limit 45 MPH	30"x36"	Replace Sign		7.5				
89	30.93101338	-99.78354273	Menard	US 83	NB	R2-1	Speed Limit 50 MPH	30"x36"	Replace Sign		7.7				
90	30.93743765	-99.78271292	Menard	US 83	NB	M3-2	EAST	24"x12"	Replace Sign	Yes	2				
90A	30.93743765	-99.78271292	Menard	US 83	NB	M1-4	US 190	30"x24"	Replace Sign	Yes	5				
90B	30.93743765	-99.78271292	Menard	US 83	NB	M5-1	90 Degree Left Turn Arrow/White	21"x15"	Replace Sign	Yes	2.2				
91	30.94066012	-99.78222617	Menard	US 83	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2				
91A	30.94066012	-99.78222617	Menard	US 83	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4				
92	30.94601974	-99.78195214	Menard	US 83	NB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5				
93	30.94670744	-99.78197904	Menard	US 83	NB	D2-2	Eden 20 mi Ballinger 57 mi	84"x30"	Replace Sign & Support					1	
94	30.96509495	-99.78703812	Menard	US 83	NB	W9-1	Right Lane Ends	36"x36"	Replace Sign		9				
95	30.96634927	-99.78768439	Menard	US 83	NB	W9-2T	Lane Ends Merge Left	36"x36"	Replace Sign		9				
96	30.98725606	-99.79787789	Menard	US 83	NB	R4-3	Slower Traffic Keep Right	24"x30"	Replace Sign		5				
97	30.99811856	-99.80272736	Menard	US 83	NB	W9-1	Right Lane Ends	36"x36"	Replace Sign		9				
98	31.00549815	-99.80911048	Menard	US 83	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
98A	31.00549815	-99.80911048	Menard	US 83	NB	M1-6F	FM 3463	24"x24"	Replace Sign	Yes	4				
TOTAL											300.9	4	2	2	2



23.06.2021

Texas Department of Transportation
San Angelo District

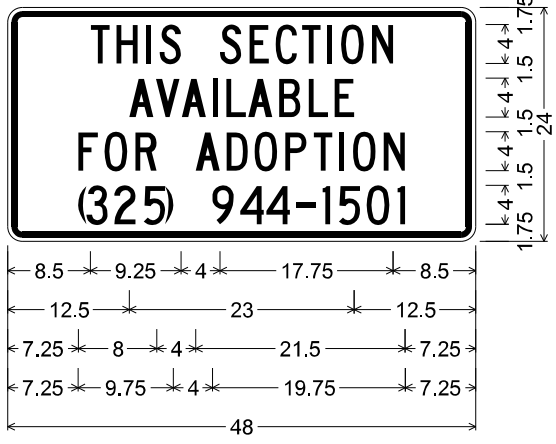
REPLACE SIGNS
 US 83
 MENARD COUNTY

SHEET 2 OF 8
NOT TO SCALE

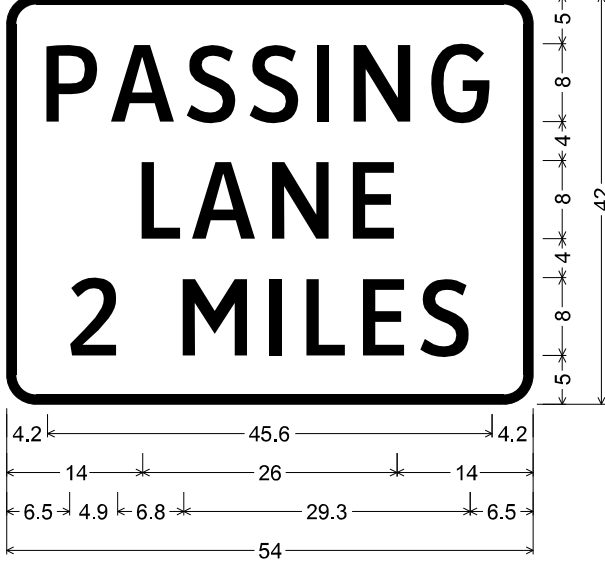
©TxDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83.ETC
12-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	29	

US 83 from Concho County Line to Kimble County Line

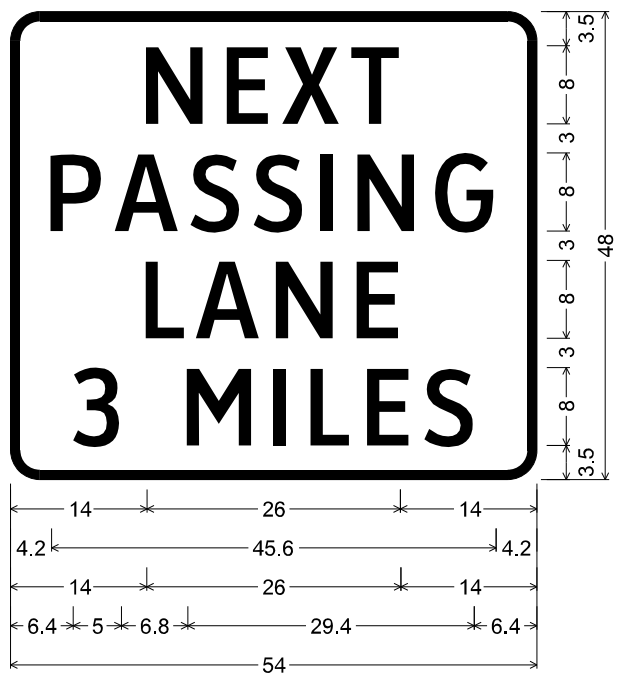
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	0636-6007	0644-6103	644-6104	644-6106	644-6111
											REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY 10 BWG (1)(U)	REPLACE SRS & S TY 580 (1)(U-WC)
											SF	EA	EA	EA	EA
99	31.00958335	-99.81292217	Menard	US 83	NB	M1-6F	FM 3463	24"x24"	Replace Sign	Yes	4				
100	31.00959007	-99.81293106	Menard	US 83	NB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2				
101	31.01020129	-99.81351265	Menard	US 83	NB	M3-1	NORTH	12"x24"	Replace Sign	Yes	2				
101A	31.01020129	-99.81351265	Menard	US 83	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4				
102	31.01152074	-99.81458187	Menard	US 83	NB	D2-2	Eden 16 mi Ballinger 53 mi	84"x30"	Replace Sign		17.5				
103	31.0551516	-99.82100445	Menard	US 83	NB	W9-1	Right Lane Ends	36"x36"	Replace Sign		9				
104	31.05652127	-99.82100282	Menard	US 83	NB	W9-2T	Lane Ends Merge Left	36"x36"	Replace Sign		9				
TOTAL											47.7	0	0	0	0



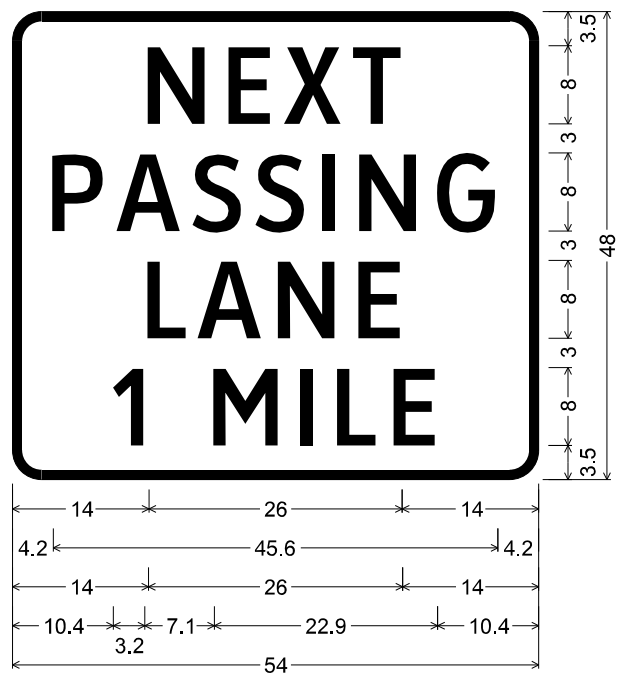
1.500" Radius, 0.625" Border, 0.375" Indent, Blue on, White;
 "THIS SECTION", C; "AVAILABLE", C;
 "FOR ADOPTION", C;
 "(325) 944-1501", C;



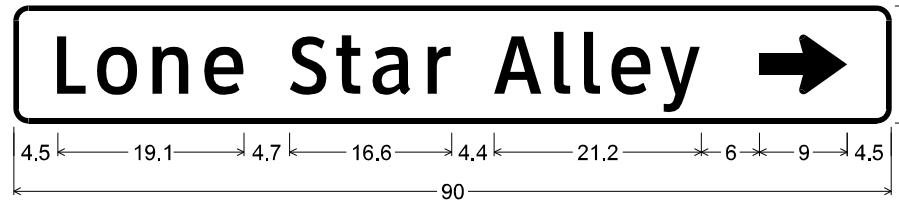
D15-10T_54x42;
 3.0" Radius, 1.0" Border, White on, Green;
 "PASSING", ClearviewHwy-3-W;
 "LANE", ClearviewHwy-3-W;
 "2 MILES", ClearviewHwy-3-W;



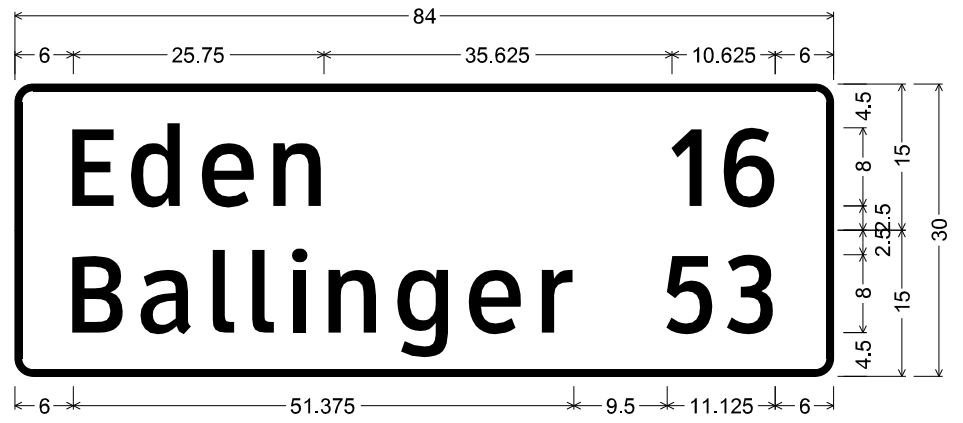
D15-11T_54x48;
 3.0" Radius, 1.0" Border, White on, Green;
 "NEXT", ClearviewHwy-3-W;
 "PASSING", ClearviewHwy-3-W;
 "LANE", ClearviewHwy-3-W;
 "3 MILES", ClearviewHwy-3-W;



D15-11T_54x48;
 3.0" Radius, 1.0" Border, White on, Green;
 "NEXT", ClearviewHwy-3-W;
 "PASSING", ClearviewHwy-3-W;
 "LANE", ClearviewHwy-3-W;
 "1 MILE", ClearviewHwy-3-W;



D21-1TR_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 "Lone Star Alley", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0';



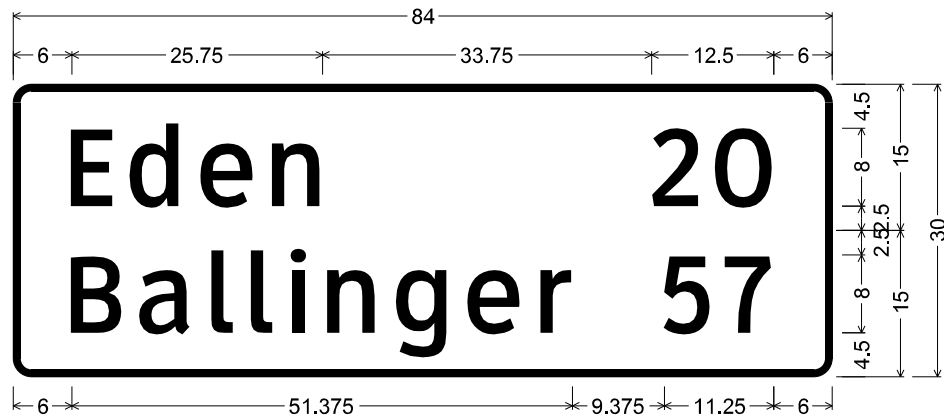
D2-2 8in;
 1.875" Radius, 0.750" Border, White on, Green;
 "Eden", ClearviewHwy-3-W; "16", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Ballinger", ClearviewHwy-3-W; "53", ClearviewHwy-3-W;



23.06.2021

		San Angelo District	
REPLACE SIGNS US 83 MENARD COUNTY			
SHEET 3 OF 8		NOT TO SCALE	
© TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83.ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 30

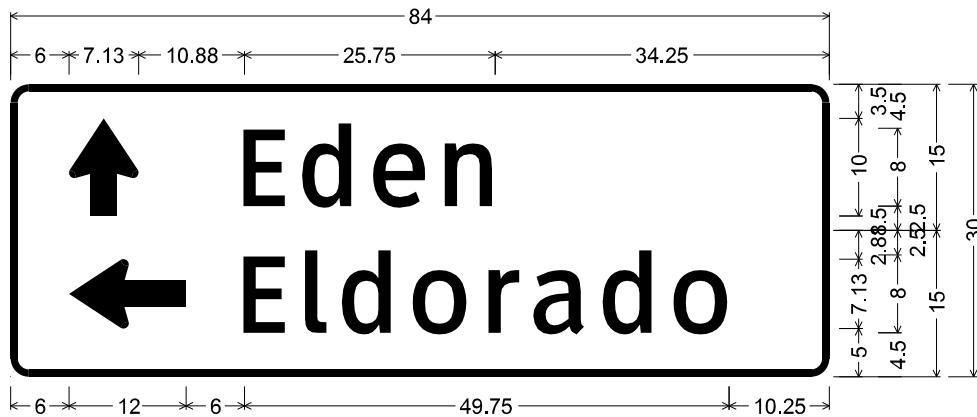
DATE: 6/23/2021 10:35:44 AM
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D2-2 8in;

1.875" Radius, 0.750" Border, White on, Green;
"Eden", ClearviewHwy-3-W; "20", ClearviewHwy-3-W;

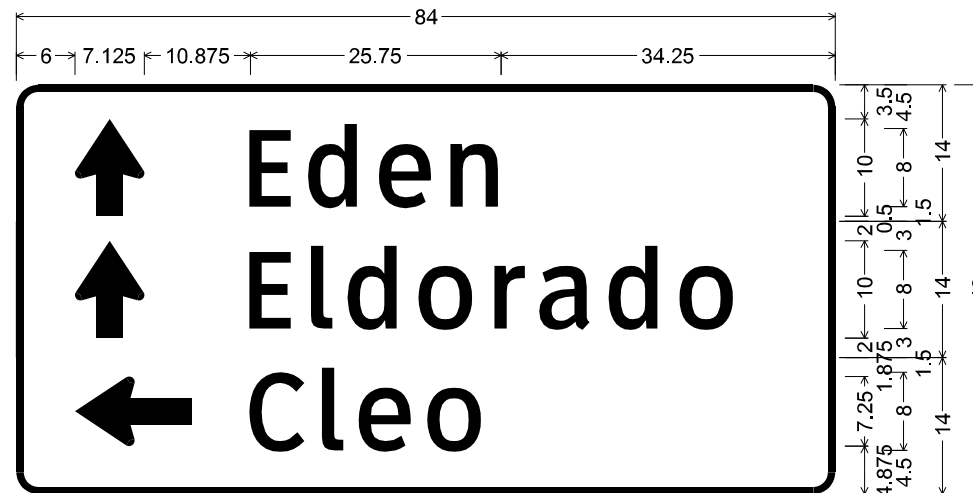
1.875" Radius, 0.750" Border, White on, Green;
"Ballinger", ClearviewHwy-3-W; "57", ClearviewHwy-3-W;



D1-2 8in UP-LT;

1.88" Radius, 0.75" Border, White on, Green;
Standard Arrow Custom 10.00" X 7.13" 90"; "Eden", ClearviewHwy-3-W;

1.88" Radius, 0.75" Border, White on, Green;
Standard Arrow Custom 12.00" X 7.13" 180"; "Eldorado", ClearviewHwy-3-W;

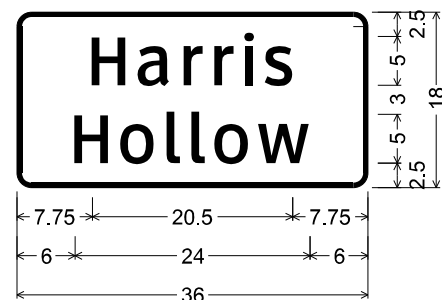


D1-3 8in UP-UP-LT;

2.250" Radius, 0.750" Border, White on, Green;
Standard Arrow Custom 10.000" X 7.125" 90"; "Eden", ClearviewHwy-3-W;

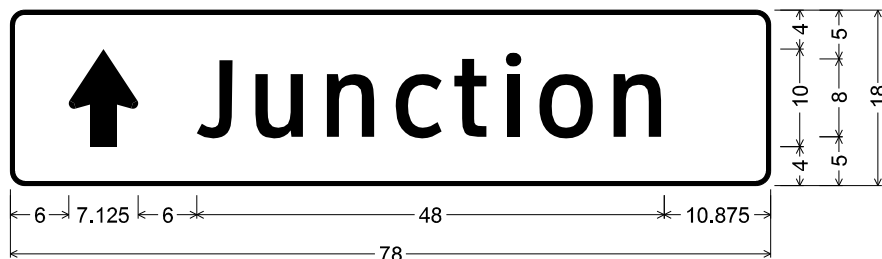
2.250" Radius, 0.750" Border, White on, Green;
Standard Arrow Custom 10.000" X 7.125" 90";
"Eldorado", ClearviewHwy-3-W;

2.250" Radius, 0.750" Border, White on, Green;
Standard Arrow Custom 12.000" X 7.125" 180"; "Cleo", ClearviewHwy-3-W;



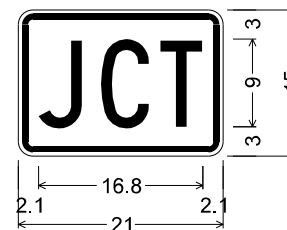
I-3 5in;

1.500" Radius, 0.500" Border, White on, Green;
"Harris", ClearviewHwy-3-W;
"Hollow", ClearviewHwy-3-W;



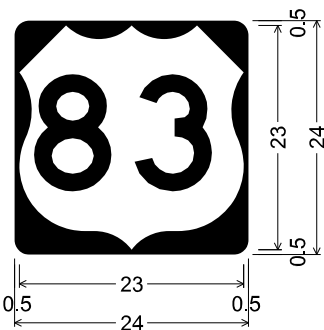
D1-1 8in UP;

1.500" Radius, 0.500" Border, White on, Green;
Standard Arrow Custom 10.000" X 7.125" 90";
"Junction", ClearviewHwy-3-W;



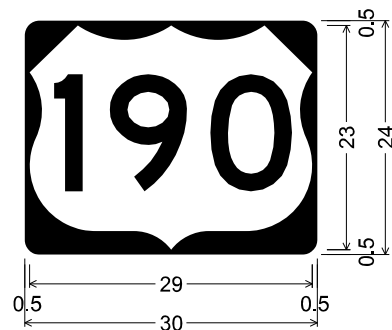
M2-1_21x15;

1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"JCT", C;



M1-4_24x24;

1.5" Radius, No border, Black;



M1-4_30x24;

1.5" Radius, No border, Black;



M1-6F_24x24;

1.5" Radius, No border, Black;
FARM, 2D;
ROAD, 2D;
2092, 4D;



M1-6F_24x24;

1.5" Radius, No border, Black;
FARM, 2D;
ROAD, 2D;
3463, 4D;



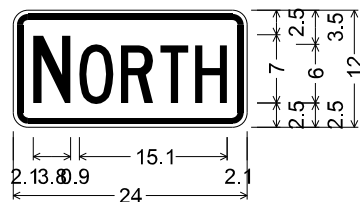
M1-6R_24x24;

1.5" Radius, No border, Black;
RANCH, 2D;
ROAD, 2D;
1773, 4D;



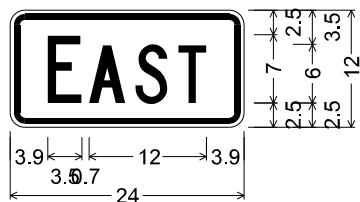
M1-6R_24x24;

1.5" Radius, No border, Black;
RANCH, 2D;
ROAD, 2D;
2291, 4D;



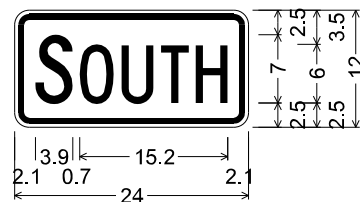
M3-1_24x12;

1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"NORTH", C 65% spacing;



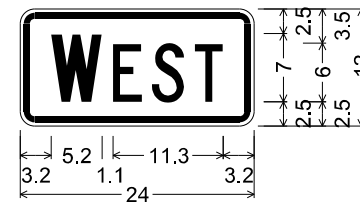
M3-2_24x12;

1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"EAST", C;



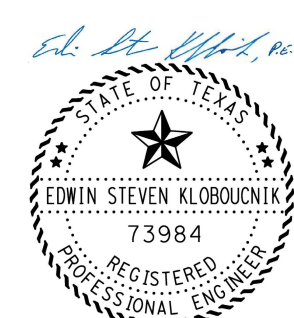
M3-3_24x12;

1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"SOUTH", C 65% spacing;



M3-4_24x12;

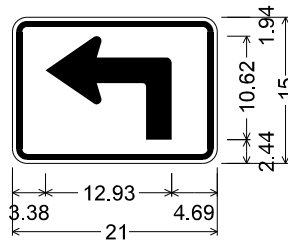
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"WEST", C;



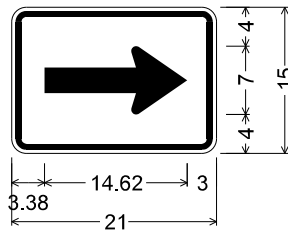
23.06.2021

		San Angelo District	
REPLACE SIGNS US 83 MENARD COUNTY			
SHEET 4 OF 8		NOT TO SCALE	
CTXDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 SECT 49 DIST SJT	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 31

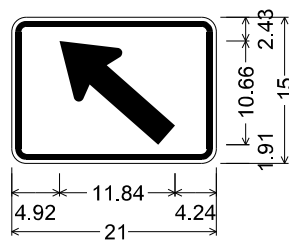
DATE:
FILE:



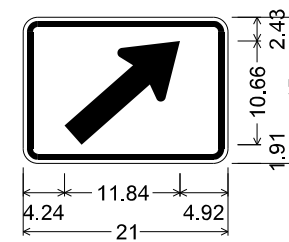
M5-1L_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M5-1;



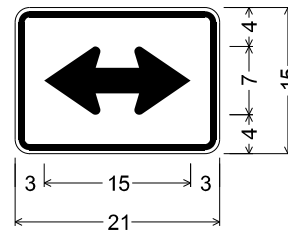
M6-1_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-1;



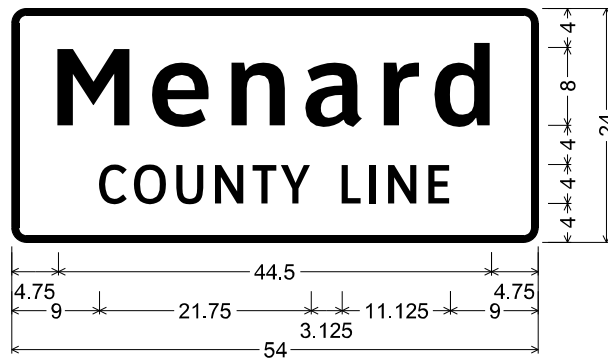
M6-2L_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-2;



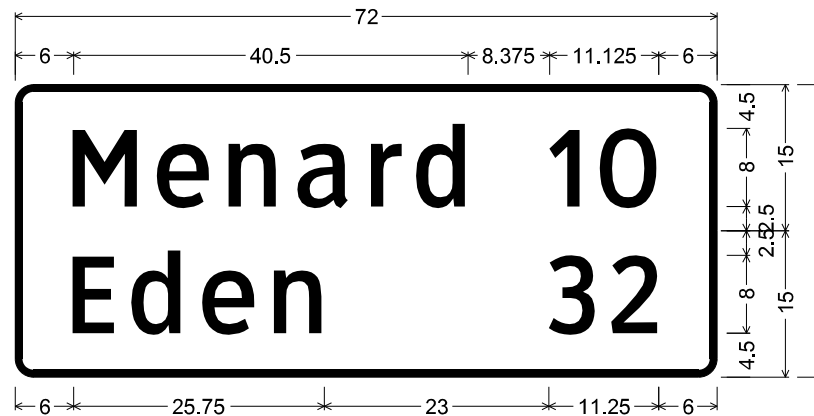
M6-2R_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-2;



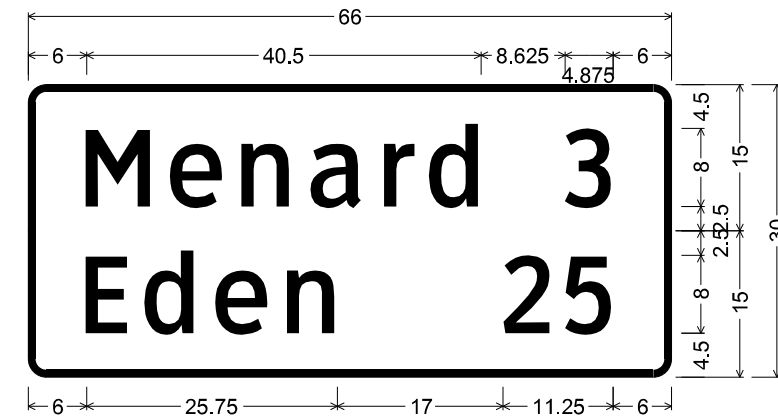
M6-4_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-4;



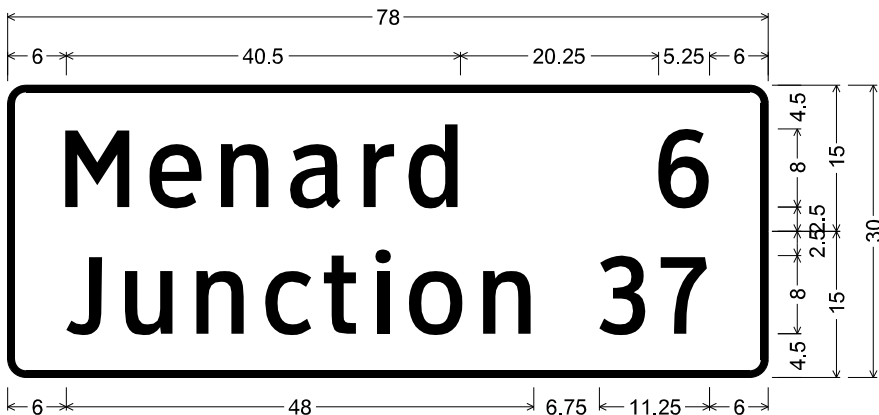
1.500" Radius, 0.750" Border, White on, Green;
"Menard", ClearviewHwy-5-W-R;
"COUNTY LINE", ClearviewHwy-3-W;



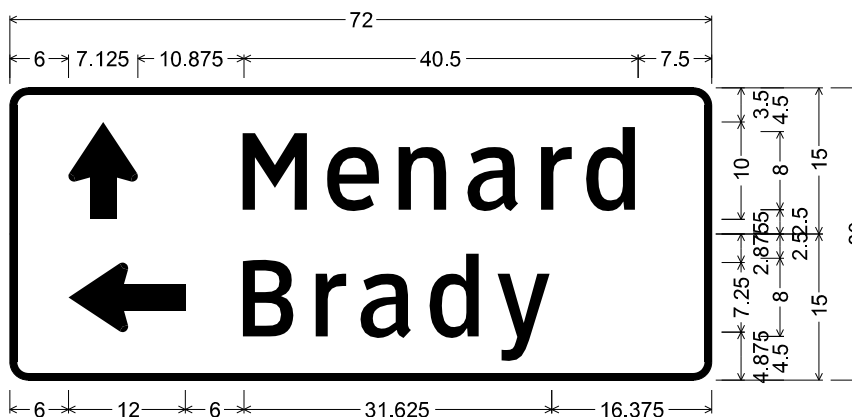
D2-2 8in;
1.875" Radius, 0.750" Border, White on, Green;
"Menard", ClearviewHwy-3-W; "10", ClearviewHwy-3-W;
1.875" Radius, 0.750" Border, White on, Green;
"Eden", ClearviewHwy-3-W; "32", ClearviewHwy-3-W;



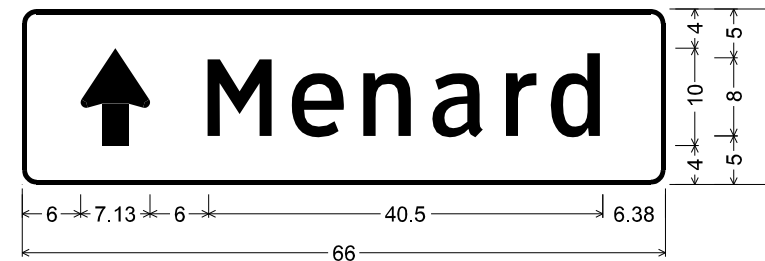
D2-2 8in;
1.875" Radius, 0.750" Border, White on, Green;
"Menard", ClearviewHwy-3-W; "3", ClearviewHwy-3-W;
1.875" Radius, 0.750" Border, White on, Green;
"Eden", ClearviewHwy-3-W; "25", ClearviewHwy-3-W;



D2-2 8in;
1.875" Radius, 0.750" Border, White on, Green;
"Menard", ClearviewHwy-3-W; "6", ClearviewHwy-3-W;
1.875" Radius, 0.750" Border, White on, Green;
"Junction", ClearviewHwy-3-W; "37", ClearviewHwy-3-W;



D1-2 8in UP-LT;
1.875" Radius, 0.750" Border, White on, Green;
Standard Arrow Custom 10.000" X 7.125" 90';
"Menard", ClearviewHwy-3-W;
1.875" Radius, 0.750" Border, White on, Green;
Standard Arrow Custom 12.000" X 7.125" 180';
"Brady", ClearviewHwy-3-W;



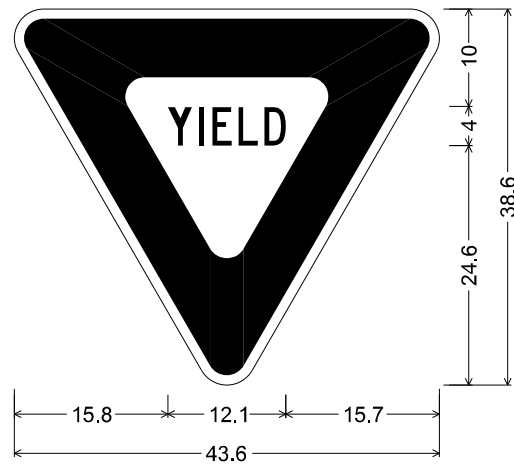
D1-1 8in UP;
1.50" Radius, 0.50" Border, White on, Green;
Standard Arrow Custom 10.00" X 7.13" 90';
"Menard", ClearviewHwy-3-W;



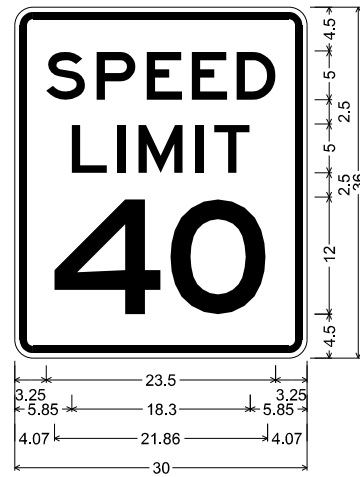
23.06.2021

		San Angelo District	
REPLACE SIGNS US 83 MENARD COUNTY			
SHEET 5 OF 8		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 SECT 49 DIST SJT	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 32

DATE:
FILE:



R1-2_48x48;
48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
"YIELD", C specified length;



R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"40", E specified length;



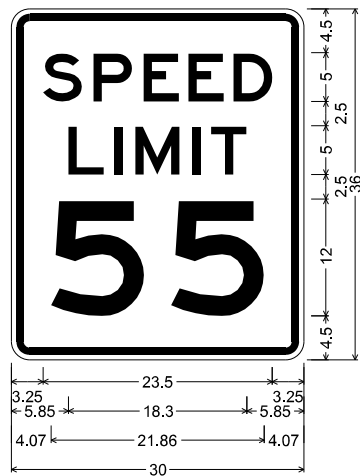
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"45", E specified length;



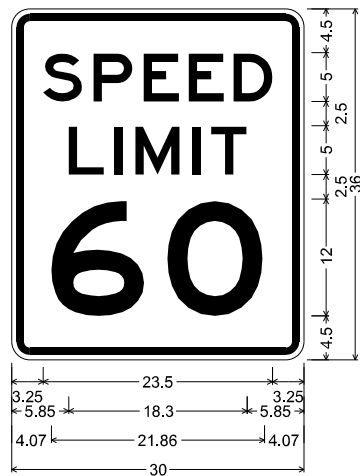
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"35", E specified length;



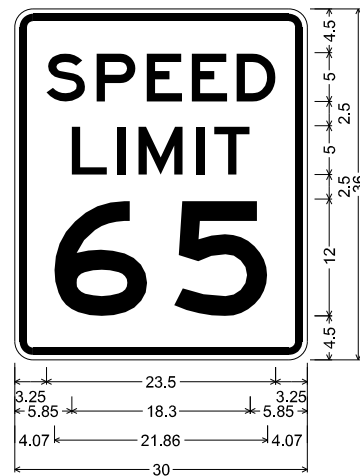
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"50", E specified length;



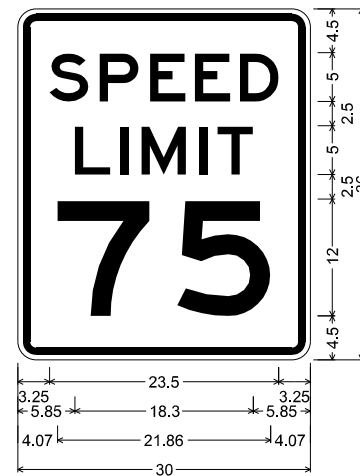
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"55", E specified length;



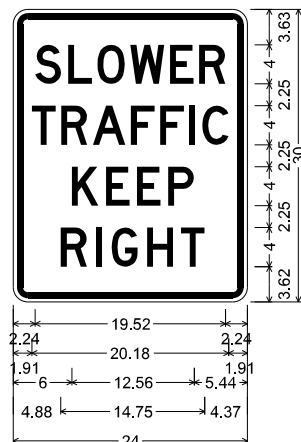
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"60", E specified length;



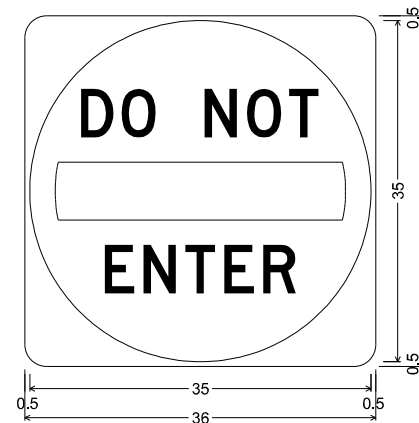
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"65", E specified length;



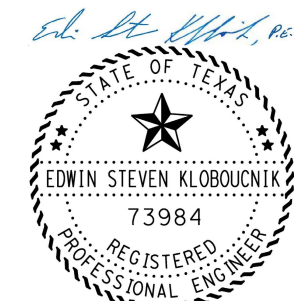
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"75", E specified length;



R4-3_24x30;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
"SLOWER", D 75% spacing;
"TRAFFIC", D 75% spacing;
"KEEP", D specified length;
"RIGHT", D specified length;



R5-1_36x36;
2.25" Radius, No border, White;



23.06.2021

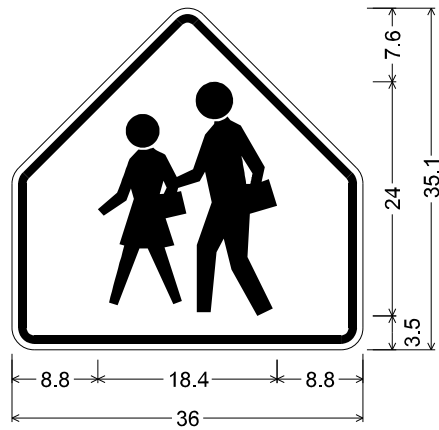
Texas Department of Transportation San Angelo District

**REPLACE SIGNS
US 83
MENARD COUNTY**

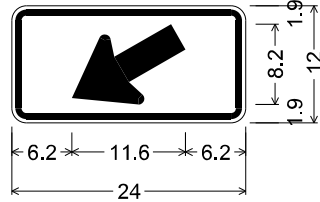
SHEET 6 OF 8 NOT TO SCALE

CTxDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83,ETC
12-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	33	

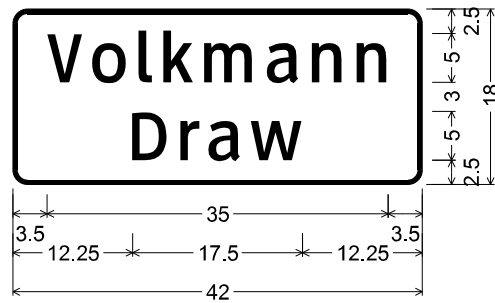
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FILE:



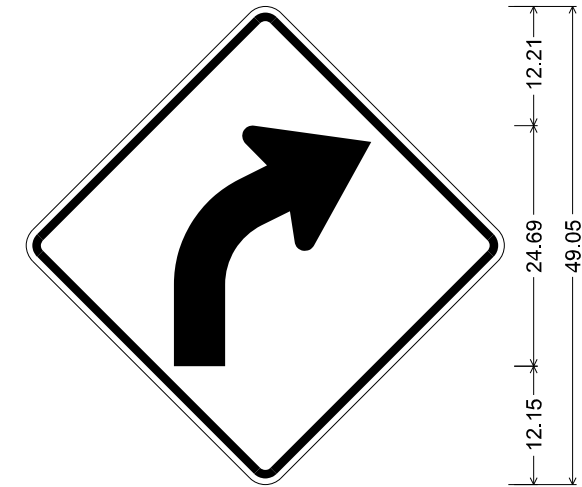
S1-1_36x36;
36.0" base to corner 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Bright yellow green;



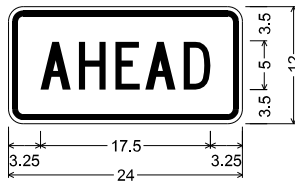
SW16-7PL_24x12;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, Bright yellow green;
Standard Arrow Custom 12.6" X 8.1" 210';



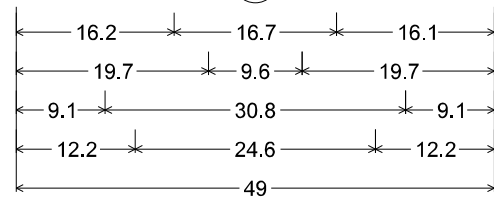
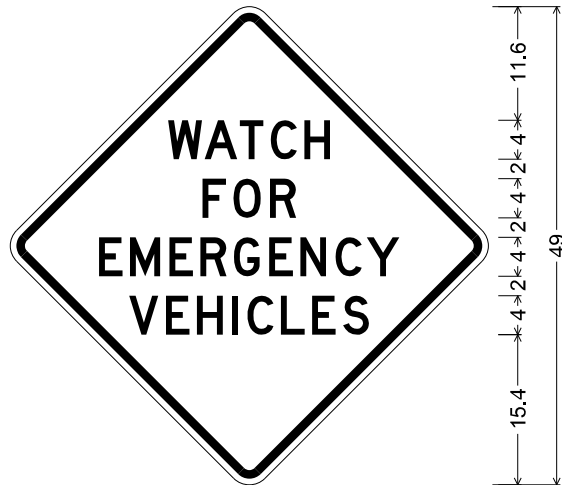
I-3 5in;
1.500" Radius, 0.500" Border, White on, Green;
"Volkmann", ClearviewHwy-3-W;
"Draw", ClearviewHwy-3-W;



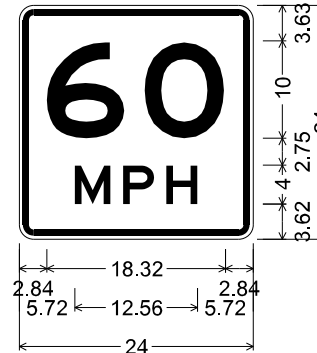
W1-2R_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Arrow W1-2;



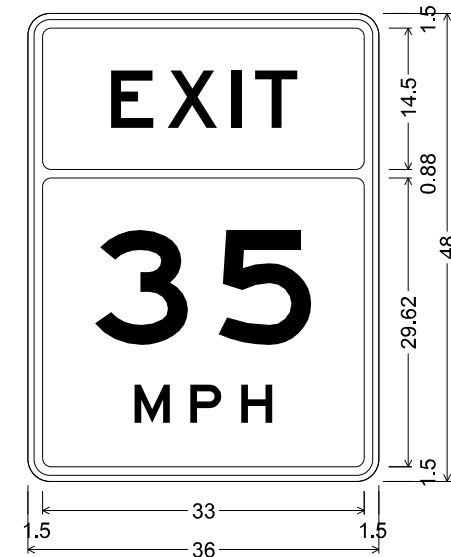
SW16-9P_24x12;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Bright yellow green;
"AHEAD", C specified length;



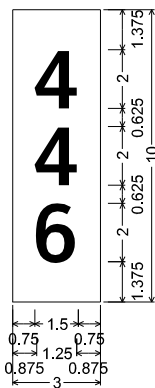
W11-12T_36x36;
36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
"WATCH", D; "FOR", D;
"EMERGENCY", D; "VEHICLES", D;



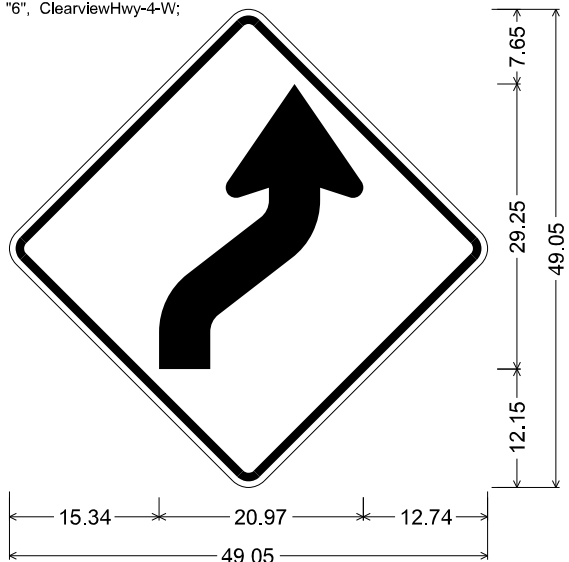
W13-1P_24x24;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Yellow;
"60", E;
"MPH", E 121% spacing;



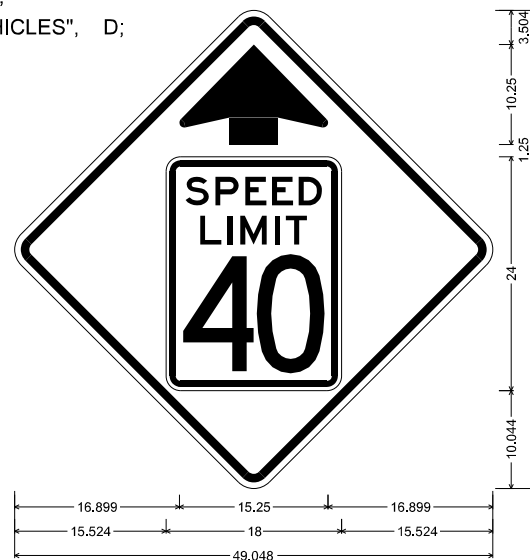
W13-2_36x48;
2.25" Radius, 0.63" Border, Yellow on, Black;
Rounded Rectangle 0.88" Radius;
Rounded Rectangle 0.88" Radius;



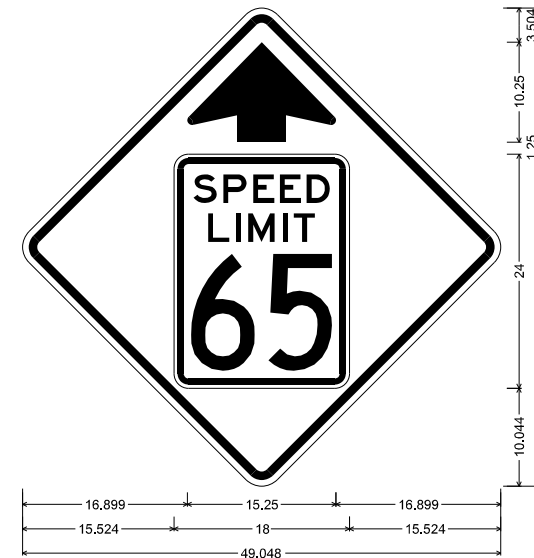
No border, White on, Green;
"4", ClearviewHwy-4-W;
"4", ClearviewHwy-4-W;
"6", ClearviewHwy-4-W;



W1-4R_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Arrow W1-4;



W3-5_36x36;
36.000" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
Down Arrow Custom - 10.250" 90';



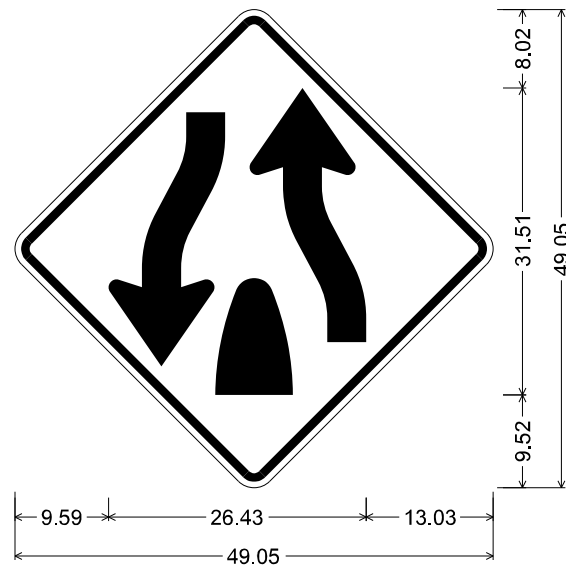
W3-5_36x36;
36.000" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
Down Arrow Custom - 10.250" 90';



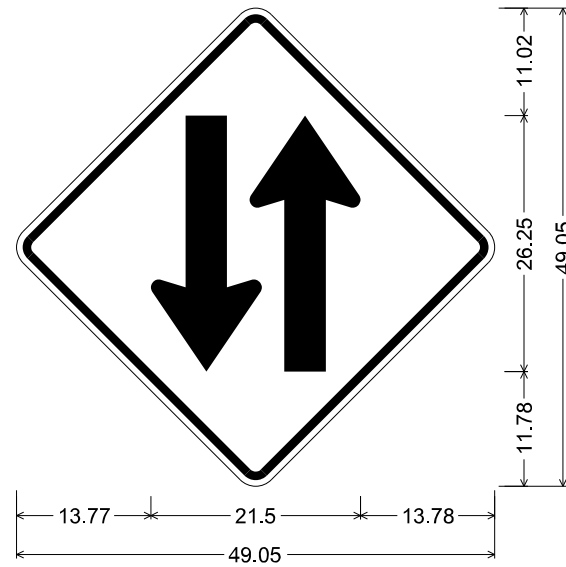
23.06.2021

		San Angelo District	
REPLACE SIGNS US 83 MENARD COUNTY			
SHEET 7 OF 8		NOT TO SCALE	
2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83, ETC SHEET NO. 34
12-19			

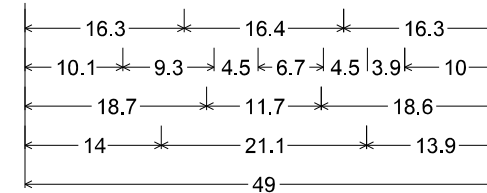
DATE:
FILE:



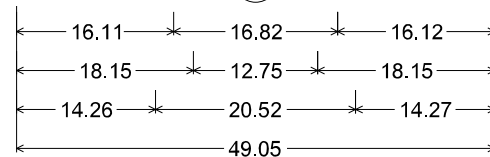
W6-2_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Symbol W6-2;



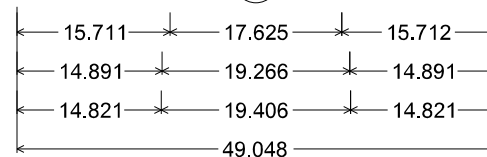
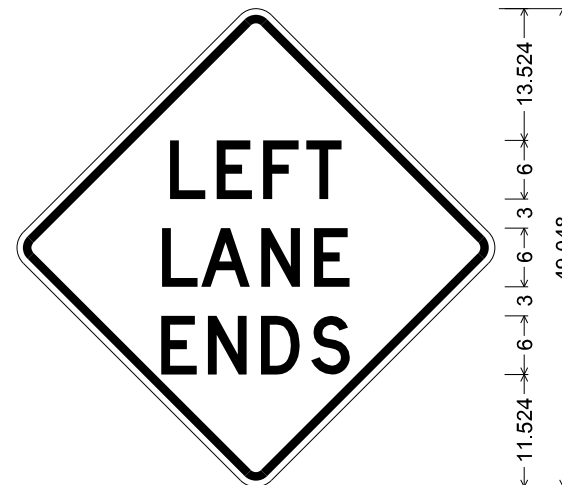
W6-3_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Symbol W6-3;



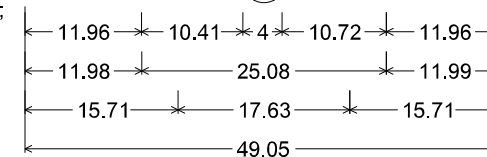
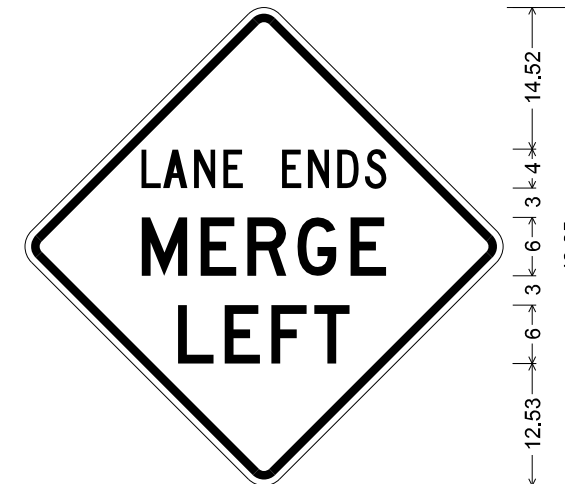
W8-13aT_36x36;
36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
"BRIDGE", C 80% spacing;
"MAY ICE IN", C 80% spacing;
"COLD", C 80% spacing;
"WEATHER", C 80% spacing;



W8-18_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
"ROAD", C; "MAY", C; "FLOOD", C;



W9-1L_36x36;
36.000" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
"LEFT", D; "LANE", D; "ENDS", D;



W9-2TL_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
"LANE ENDS", C 98% spacing;
"MERGE", D; "LEFT", D;



23.06.2021



REPLACE SIGNS
US 83
MENARD COUNTY

SHEET 8 OF 8		NOT TO SCALE		
©TxDOT	2021	CONT	SECT	HIGHWAY
SHEET ISSUED OR LAST REVISED		6383	49	001
12-19		DIST	COUNTY	SHEET NO.
		SJT	MENARD	35

DATE: 6/23/2021 10:36:38 AM
 FILE: \\pwwork\project\seonline.com\T\DOT2\Documents\07 - SJT Maintenance Projects\638349001 - 2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT US 190 MENARD COUNTY.dgn

US 190 from McCulloch County Line to Schleicher County Line											0636-6007	0644-6103	644-6104
Existing Location											REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1XP)	REPLACE SRS & S TY 10 BWG (1XT)
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree			
											SF	EA	EA
1	30.98090524	-99.60353349	Menard	US 190	WB	I-2dT	Menard County Line	54"x24"	Replace Sign		9		
2	30.96213488	-99.68295244	Menard	US 190	WB	M1-4	US 190	30"x24"	Replace Sign	Yes	5		
3	30.96151394	-99.68911139	Menard	US 190	WB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
4	30.95618898	-99.7335424	Menard	US 190	WB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
5	30.946616	-99.757634	Menard	US 190	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
6	30.94376103	-99.76769113	Menard	US 190	WB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
7	30.94294607	-99.77062148	Menard	US 190	WB	I-3	Volkman Draw	42"x18"	Replace Sign & Support				1
8	30.94230655	-99.77306303	Menard	US 190	WB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5		
9	30.94147094	-99.77616376	Menard	US 190	WB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
10	30.92343853	-99.78450557	Menard	US 190	WB	R2-1	Speed Limit 35 MPH	30"x36"	Replace Sign & Support			1	
11	30.92424117	-99.78766105	Menard	US 190	WB	D71-FT	Texas Forts Trail	42"x24"	Replace Sign		7		
12	30.92441406	-99.7889624	Menard	US 190	WB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5		
13	30.92533046	-99.8013995	Menard	US 190	WB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5		
14	30.93258332	-99.83461481	Menard	US 190	WB	I-3	Maynor Branch	36"x18"	Replace Sign & Support				1
15	30.93181317	-99.83734933	Menard	US 190	WB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
16	30.92022005	-99.86841048	Menard	US 190	WB	I-3	Coghlan Branch	36"x18"	Replace Sign & Support				1
17	30.91149407	-99.88321438	Menard	US 190	WB	D21-1TL	Four Mile Rd Left Arrow	78"x12"	Replace Sign		6.5		
18	30.91070903	-99.88455245	Menard	US 190	WB	M1-4	US 190	30"x24"	Replace Sign	Yes	5		
19	30.90451	-99.908793	Menard	US 190	WB	D21-1TR	Waddell Ln Right Arrow	72"x12"	Replace Sign		6		
20	30.90425038	-99.91515053	Menard	US 190	WB	W11-3	Deer crossing	36"x36"	Replace Sign		9		
21	30.87307977	-100.0350958	Menard	US 190	WB	D1-3	Eldorado Up Arrow Ft McKavett Left Arrow Sonora Left Arrow	96"x42"	Replace Sign		28		
22	30.87350959	-100.0382177	Menard	US 190	WB	D7-2TL	Ft McKavett State Historical Site Left Arrow	96"x24"	Replace Sign & Support				1
23	30.8738733	-100.0409637	Menard	US 190	WB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2		
24	30.87396307	-100.0416244	Menard	US 190	WB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5		
25	30.87411103	-100.0438977	Menard	US 190	WB	I-3	Brown Hollow	30"x18"	Replace Sign & Support				1
26	30.8747295	-100.0834663	Menard	US 190	WB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2		
27	30.87507719	-100.1050935	Menard	US 190	WB	I-3	Mulligan Draw	42"x18"	Replace Sign & Support				1
28	30.8773382	-100.1168669	Menard	US 190	EB	M1-4	US 190	30"x24"	Replace Sign	Yes	5		
28A	30.8773382	-100.1168669	Menard	US 190	EB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21		
28B	30.8773382	-100.1168669	Menard	US 190	EB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21		
29	30.87565844	-100.1103238	Menard	US 190	EB	I-3	Mulligan Draw	42"x18"	Replace Sign & Support				1
30	30.874909	-100.1043412	Menard	US 190	EB	D1-1	Menard Up Arrow	66"x18"	Replace Sign		8.25		
31	30.87483689	-100.0999555	Menard	US 190	EB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5		
32	30.8744711	-100.075983	Menard	US 190	EB	I-3	Brown Hollow	30"x18"	Replace Sign & Support		3.75		1
33	30.87400459	-100.0468717	Menard	US 190	EB	M2-1	JCT	21"x15"	Replace Sign	Yes			
33A	30.87400459	-100.0468717	Menard	US 190	EB	M1-6R	RM 864	24"x24"	Replace Sign	Yes			
33	30.8739474	-100.0437237	Menard	US 190	EB	D1-3	Menard Up Arrow Ft Mckavett Right Arrow Sonora Right	96"x42"	Replace Sign		28		
34	30.87376935	-100.0412338	Menard	US 190	EB	M3-4	WEST	24"x12"	Replace Sign	Yes	2		
34A	30.87376935	-100.0412338	Menard	US 190	EB	M1-6R	RM 864	24"x24"	Replace Sign	Yes	4		
34B	30.87376935	-100.0412338	Menard	US 190	EB	M6-1	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2		
35	30.87348336	-100.0391641	Menard	US 190	EB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5		
36	30.87112972	-100.0218042	Menard	US 190	EB	D21-1TR	Dunagan Rd Right Arrow	78"x12"	Replace Sign		6.5		
37	30.870963	-100.021006	Menard	US 190	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
38	30.88211926	-99.98562638	Menard	US 190	EB	M1-4	US 190	30"x24"	Replace Sign	Yes	5		
39	30.88706412	-99.97796654	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
40	30.9037664	-99.92442724	Menard	US 190	EB	I-3	Clear Creek	30"x18"	Replace Sign & Support				1
41	30.90377733	-99.92404457	Menard	US 190	EB	M1-4	US 190	30"x24"	Replace Sign	Yes	5		
42	30.90394867	-99.91930472	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
43	30.90404651	-99.91695275	Menard	US 190	EB	D21-1TR	Bois D Arc Rd Right Arrow	84"x12"	Replace Sign		7		
44	30.904118	-99.914624	Menard	US 190	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
TOTAL											299.02	1	9



23.06.2021

San Angelo District

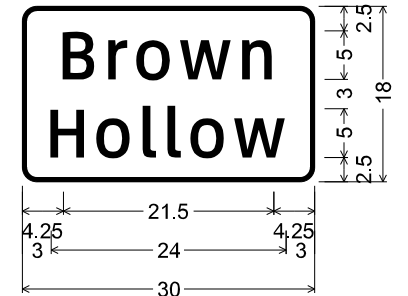
REPLACE SIGNS US 190 MENARD COUNTY

SHEET 1 OF 6
NOT TO SCALE

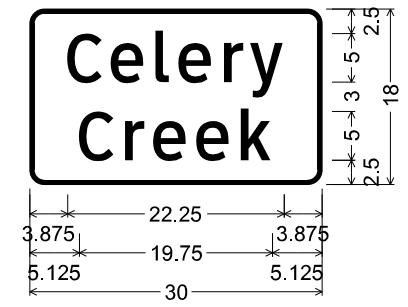
CTxDOT	2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED		6383	49	001	US 83.ETC
12-19		DIST	COUNTY	SHEET NO.	
		SJT	MENARD	36	

DATE: 6/23/2021 10:36:48 AM
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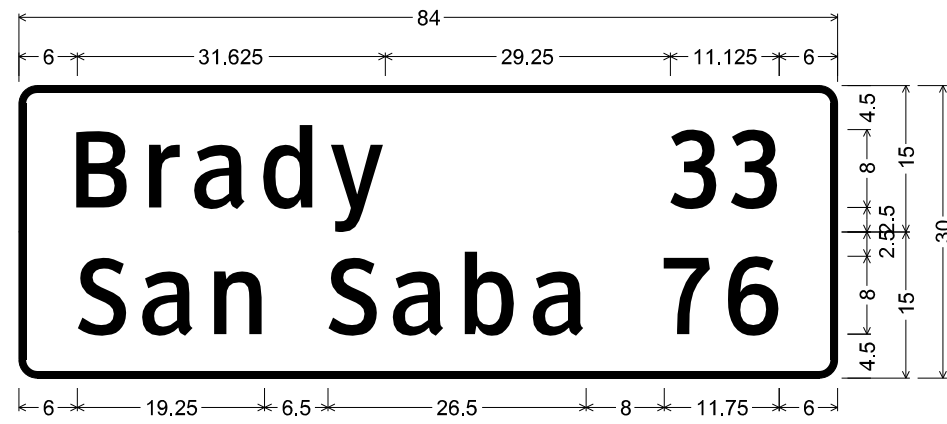
US 190 from McCulloch County Line to Schleicher County Line										0636-6007	0644-6103	644-6104	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree			
45	30.90981248	-99.88576553	Menard	US 190	EB	D21-1TR	Four Mile Rd Right Arrow	78"x12"	Replace Sign		SF	EA	EA
46	30.91694516	-99.87368377	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		6.5		
47	30.91846963	-99.87110203	Menard	US 190	EB	I-3	Coghlan Branch	36"x18"	Replace Sign & Support		9		1
48	30.92284867	-99.86251177	Menard	US 190	EB	M1-4	US 190	30"x24"	Replace Sign	Yes	5		
48A	30.92284867	-99.86251177	Menard	US 190	EB	D10-7aT	TRM 416	3"x10"	Replace Sign	Yes	0.21		
48B	30.92284867	-99.86251177	Menard	US 190	EB	D10-7aT	TRM 416	3"x10"	Replace Sign	Yes	0.21		
49	30.93063682	-99.84073449	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
50	30.93149414	-99.83787726	Menard	US 190	EB	I-3	Maynor Branch	36"x18"	Replace Sign & Support				1
51	30.924966	-99.796809	Menard	US 190	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
52	30.92503839	-99.79643237	Menard	US 190	EB	I-3	Celery Creek	30"x18"	Replace Sign & Support				1
53	30.92477816	-99.79296203	Menard	US 190	EB	R2-1	Speed Limit 50 MPH	30"x36"	Replace Sign		7.5		
54	30.92446275	-99.79063125	Menard	US 190	EB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
55	30.92419988	-99.78857362	Menard	US 190	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2		
55A	30.92419988	-99.78857362	Menard	US 190	EB	M1-4	US 190	30"x24"	Replace Sign	Yes	4		
55B	30.92419988	-99.78857362	Menard	US 190	EB	M5-1L	90 Degree Left Turn Arrow/White	21"x15"	Replace Sign	Yes	2.2		
56	30.92403327	-99.7873662	Menard	US 190	EB	R2-1	Speed Limit 35 MPH	30"x36"	Replace Sign		7.5		
57	30.92379956	-99.78554758	Menard	US 190	EB	R4-7	Keep Right	24"x30"	Replace Sign	Yes	5		
57A	30.92379956	-99.78554758	Menard	US 190	EB	R4-7b	Keep Right Symbol	24"x30"	Replace Sign	Yes	5		
58	30.92277879	-99.78458085	Menard	US 190	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2		
59	30.9223848	-99.78464235	Menard	US 190	EB	R1-2	Yield Sign	48"x48"x48"	Replace Sign & Support				1
60	30.93982483	-99.78158846	Menard	US 190	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2		
60A	30.93982483	-99.78158846	Menard	US 190	EB	M1-4	US 190	30"x24"	Replace Sign	Yes	4		
61	30.94012593	-99.78046437	Menard	US 190	EB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5		
62	30.94057512	-99.77880603	Menard	US 190	EB	D2-2	Brady 33 mi San Saba 76 mi	84"x30"	Replace Sign		17.5		
63	30.94189673	-99.77400776	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
64	30.94272647	-99.7710376	Menard	US 190	EB	I-3	Volkman Draw	42"x18"	Replace Sign & Support				1
65	30.95360597	-99.73924553	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
66	30.95472733	-99.73656627	Menard	US 190	EB	I-3	Scalp Creek	30"x18"	Replace Sign & Support				1
67	30.96069531	-99.69577525	Menard	US 190	EB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9		
68	30.96102193	-99.69268849	Menard	US 190	EB	I-3	Chapman Draw	42"x18"	Replace Sign		5.25		
TOTAL											153.37	0	6



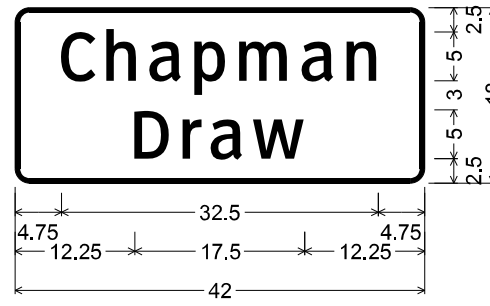
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Brown", ClearviewHwy-3-W;
 "Hollow", ClearviewHwy-3-W;



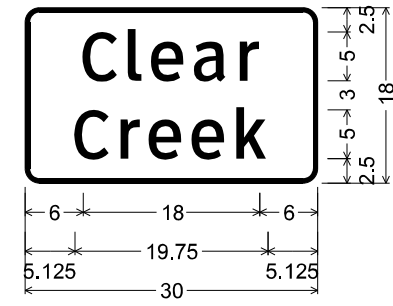
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Celery", ClearviewHwy-3-W;
 "Creek", ClearviewHwy-3-W;



D2-2 8in;
 1.875" Radius, 0.750" Border, White on, Green;
 "Brady", ClearviewHwy-3-W; "33", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "San Saba", ClearviewHwy-3-W; "76", ClearviewHwy-3-W;



I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Chapman", ClearviewHwy-3-W;
 "Draw", ClearviewHwy-3-W;



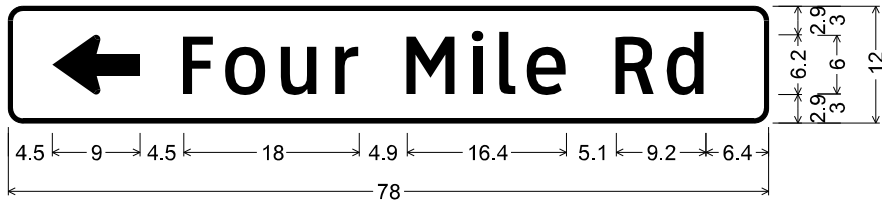
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Clear", ClearviewHwy-3-W;
 "Creek", ClearviewHwy-3-W;



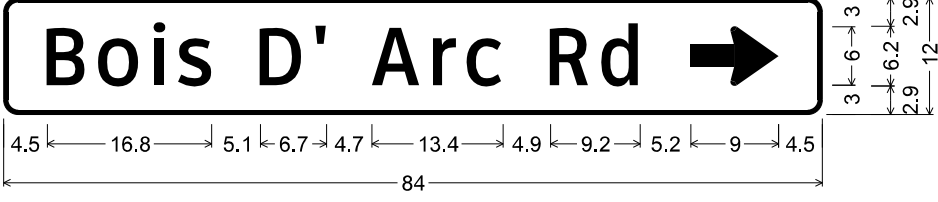
23.06.2021

Texas Department of Transportation		San Angelo District	
REPLACE SIGNS US 190 MENARD COUNTY			
SHEET 2 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
HIGHWAY US 83,ETC		SHEET NO. 37	

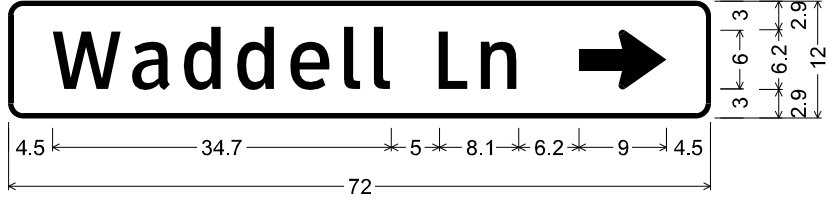
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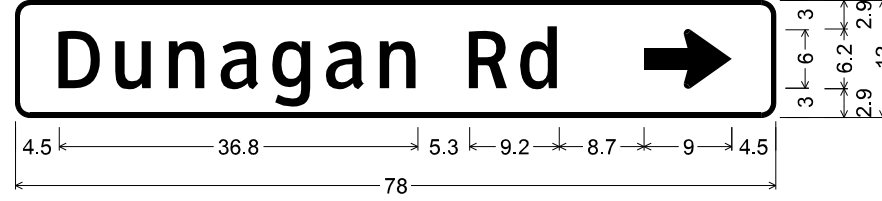
D21-1TL_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 Standard Arrow Custom 9.0" X 6.1" 180';
 "Four Mile Rd", ClearviewHwy-3-W;



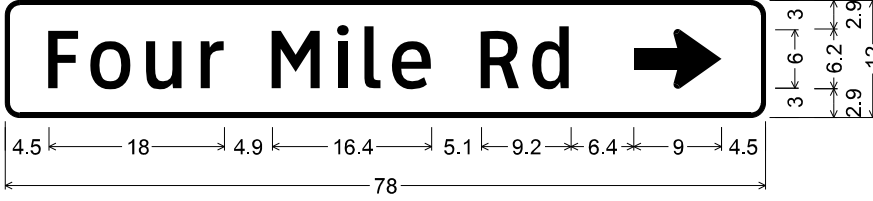
D21-1TR_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 "Bois D' Arc Rd", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0';



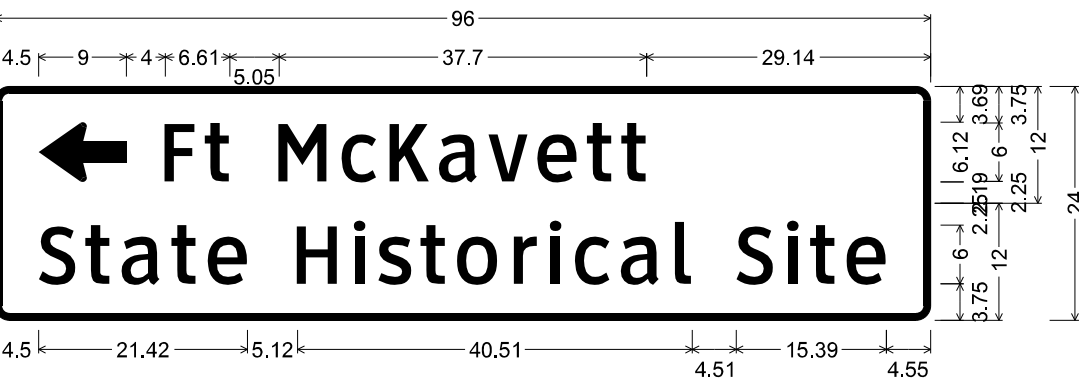
D21-1TR_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 "Waddell Ln", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0';



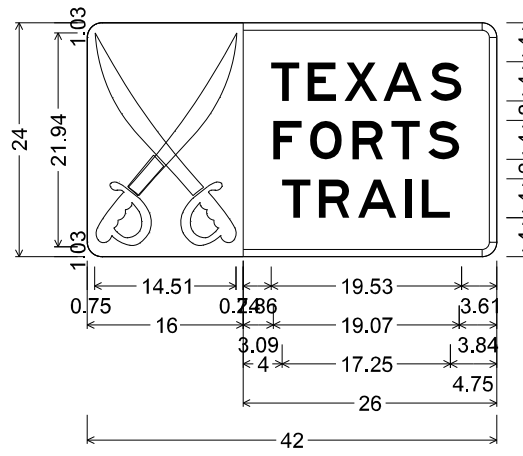
D21-1TR_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 "Dunagan Rd", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0';



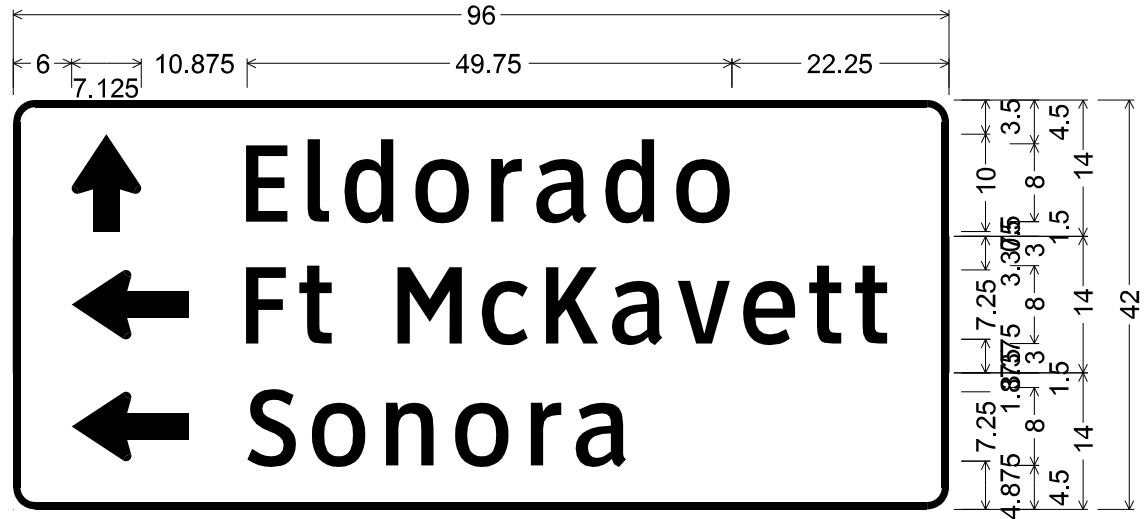
D21-1TR_VARx12;
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 "Four Mile Rd", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0';



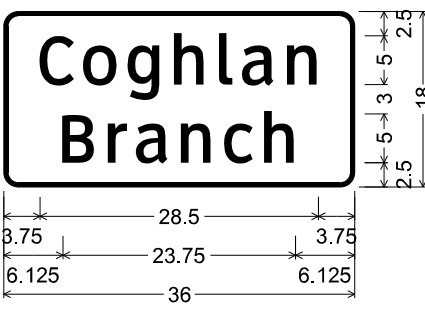
D7-2TL_VARx24;
 1.50" Radius, 0.75" Border, White on, Brown;
 Standard Arrow Custom 9.00" X 6.13" 180'; "Ft McKavett ", ClearviewHwy-3-W;
 1.50" Radius, 0.75" Border, White on, Brown;
 "State Historical Site", ClearviewHwy-3-W;



D71-FT_42X24;
 1.50" Radius, 0.01" Border, Black on, White;
 Symbol D71-FT;
 1.50" Radius, 0.75" Border, White on, Blue;
 "TEXAS", E; "FORTS", E;
 "TRAIL", E;



D1-3 8in UP-LT-LT;
 2.250" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 10.000" X 7.125" 90';
 "Eldorado", ClearviewHwy-3-W;
 2.250" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180';
 "Ft McKavett", ClearviewHwy-3-W;
 2.250" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180';
 "Sonora", ClearviewHwy-3-W;



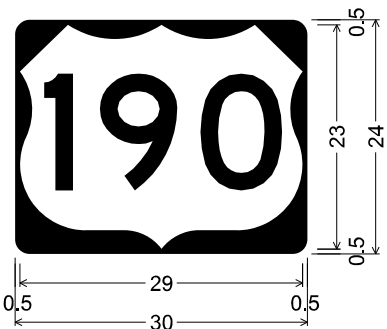
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Coghlan", ClearviewHwy-3-W;
 "Branch", ClearviewHwy-3-W;



23.06.2021

		San Angelo District	
REPLACE SIGNS US 190 MENARD COUNTY			
SHEET 3 OF 6		NOT TO SCALE	
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DIST SJT	COUNTY MENARD	SHEET NO. 38	

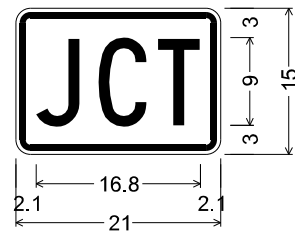
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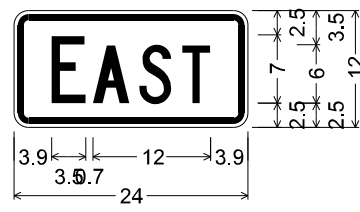
M1-4_30x24;
1.5" Radius, No border, Black;



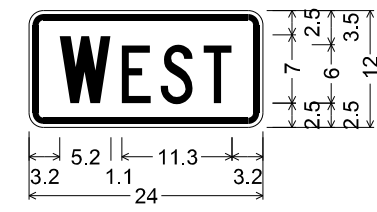
M1-6R_24x24;
1.5" Radius, No border, Black;
RANCH, 2D;
ROAD, 2D;
864, 5D;



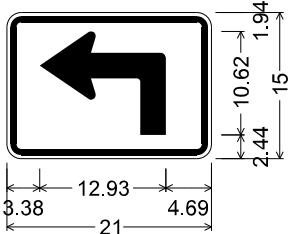
M2-1_21x15;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"JCT", C;



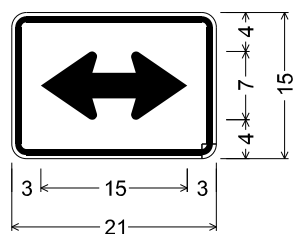
M3-2_24x12;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"EAST", C;



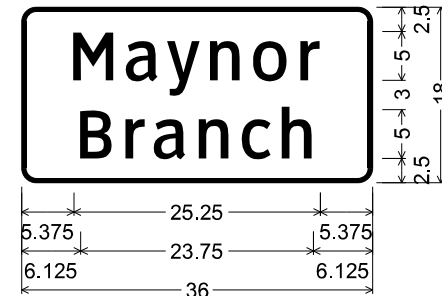
M3-4_24x12;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"WEST", C;



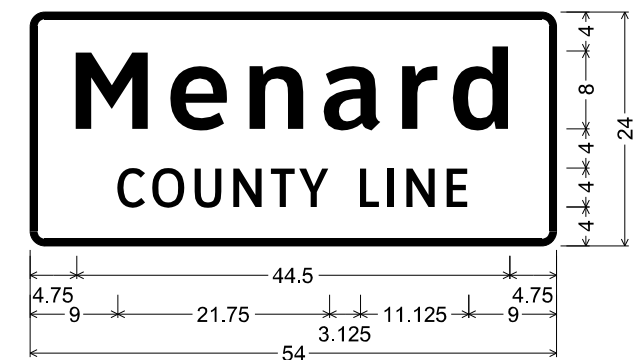
M5-1L_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M5-1;



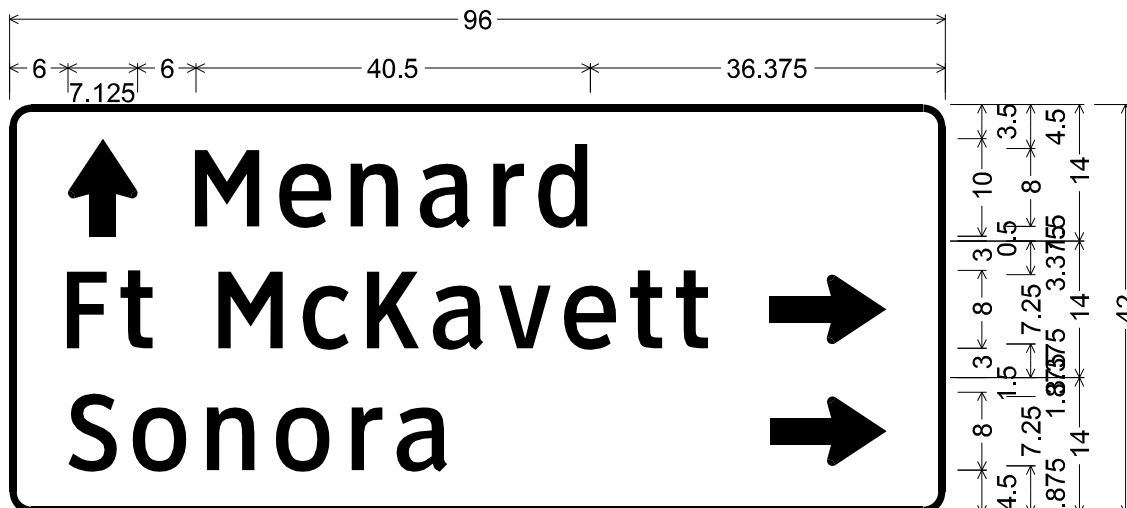
M6-4_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-4;



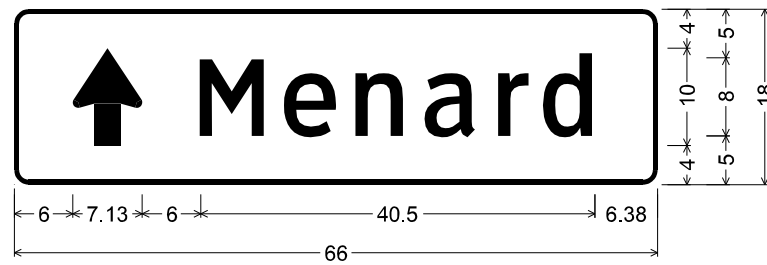
I-3 5in;
1.500" Radius, 0.500" Border, White on, Green;
"Maynor", ClearviewHwy-3-W;
"Branch", ClearviewHwy-3-W;



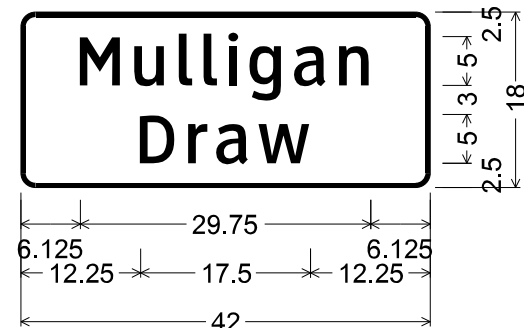
I-3 5in;
1.500" Radius, 0.750" Border, White on, Green;
"Menard", ClearviewHwy-5-W-R;
"COUNTY LINE", ClearviewHwy-3-W;



D1-3 8in UP-LT-LT;
2.250" Radius, 0.750" Border, White on, Green;
Standard Arrow Custom 10.000" X 7.125" 90';
"Menard", ClearviewHwy-3-W;
2.250" Radius, 0.750" Border, White on, Green;
"Ft McKavett", ClearviewHwy-3-W;
Standard Arrow Custom 12.000" X 7.125" 0';
2.250" Radius, 0.750" Border, White on, Green;
"Sonora", ClearviewHwy-3-W;
Standard Arrow Custom 12.000" X 7.125" 0';



D1-1 8in UP;
1.50" Radius, 0.50" Border, White on, Green;
Standard Arrow Custom 10.00" X 7.13" 90';
"Menard", ClearviewHwy-3-W;



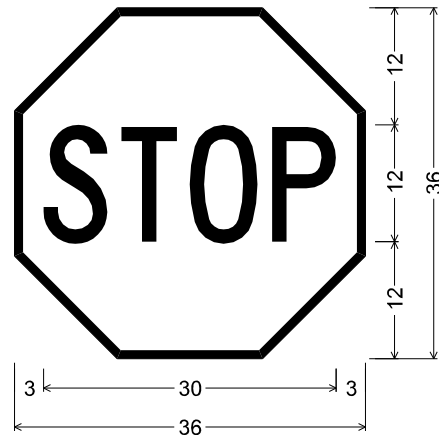
I-3 5in;
1.500" Radius, 0.500" Border, White on, Green;
"Mulligan", ClearviewHwy-3-W;
"Draw", ClearviewHwy-3-W;



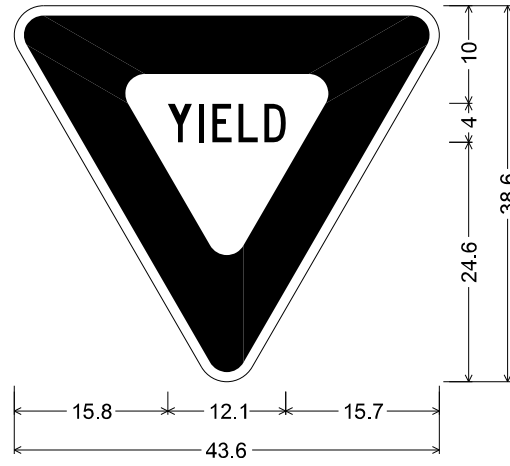
23.06.2021

		San Angelo District	
REPLACE SIGNS US 190 MENARD COUNTY			
SHEET 4 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 DIST SJT	SECT 49 COUNTY MENARD	JOB 001 HIGHWAY US 83,ETC SHEET NO. 39

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R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



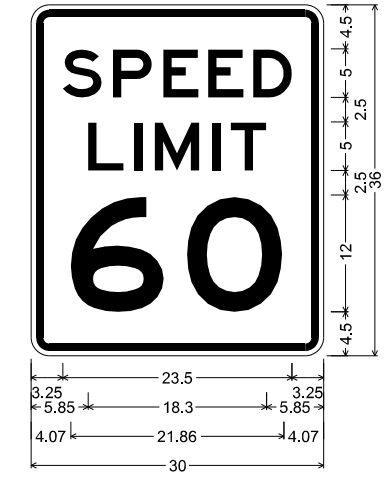
R1-2_48x48;
 48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
 "YIELD", C specified length;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "35", E specified length;



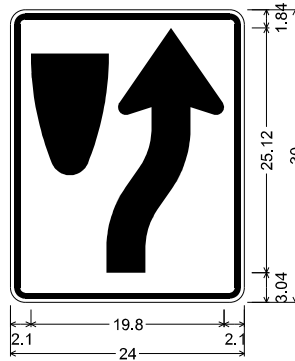
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "50", E specified length;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "60", E specified length;



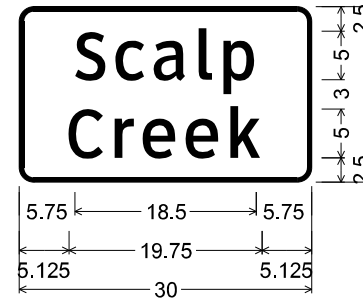
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



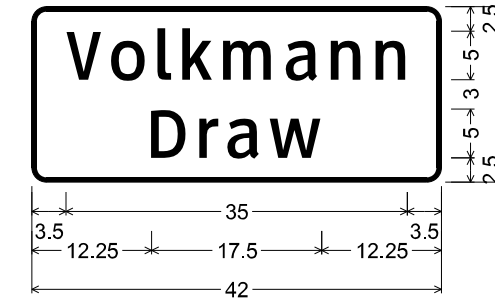
R4-7_24x30;
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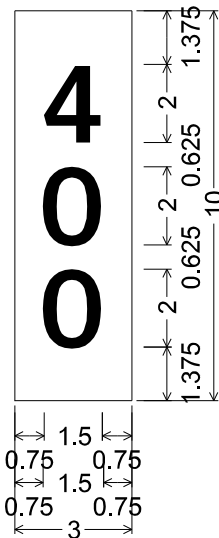
R4-7b_24x30;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 "KEEP", D;
 Standard Arrow Custom 14.25" X 7.00" 30";
 "RIGHT", D;



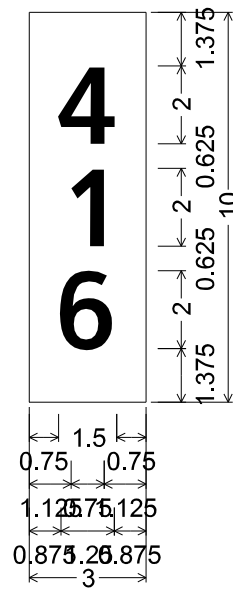
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Scalp", ClearviewHwy-3-W;
 "Creek", ClearviewHwy-3-W;



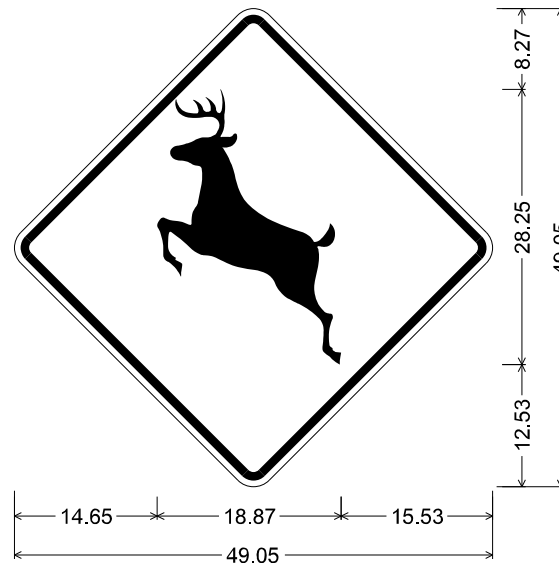
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Volkmann", ClearviewHwy-3-W;
 "Draw", ClearviewHwy-3-W;



No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "1", ClearviewHwy-4-W;
 "6", ClearviewHwy-4-W;



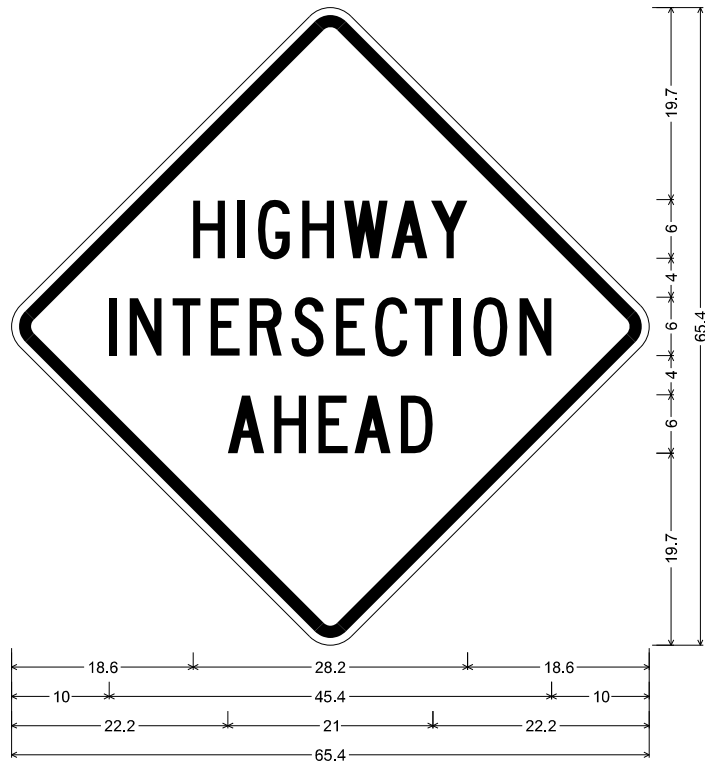
W11-3_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W11-3;



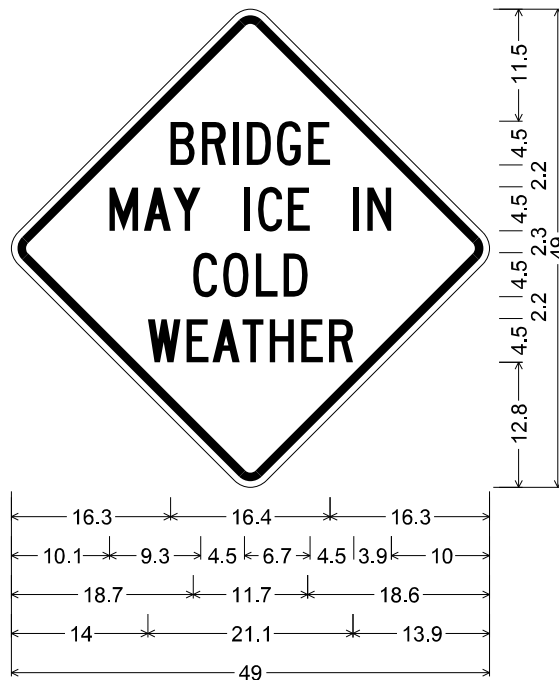
23.06.2021

		San Angelo District	
REPLACE SIGNS US 190 MENARD COUNTY			
SHEET 5 OF 6		NOT TO SCALE	
6/23/2021 6383	2021 49	001	US 83.ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 40

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W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



W8-13aT_36x36;
 36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
 "BRIDGE", C 80% spacing;
 "MAY ICE IN", C 80% spacing;
 "COLD", C 80% spacing;
 "WEATHER", C 80% spacing;

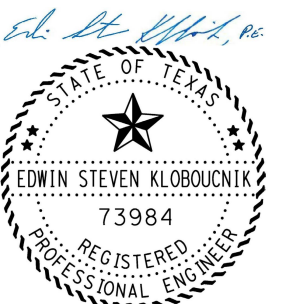


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		San Angelo District	
REPLACE SIGNS US 190 MENARD COUNTY			
SHEET 6 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 41

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SH 29 from US 83 to Mason County Line Existing Location											0636-6007	0644-6103	644-6104	644-6106	644-6111
SIGN	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY 10 BWG (1)(U)	REPLACE SRS & S TY 580 (1)(U-WC)
1	30.87958033	-99.76175637	Menard	SH 29	EB	R6-1R	One Way	54"x18"	Replace Sign		SF	EA	EA	EA	EA
2	30.88113549	-99.76406392	Menard	SH 29	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2				
3	30.88113549	-99.76406392	Menard	SH 29	EB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
3A	30.88113549	-99.76406392	Menard	SH 29	EB	D10-7aT	TRM 416	3"x10"	Replace Sign	Yes	0.21				
3B	30.88113549	-99.76406392	Menard	SH 29	EB	D10-7aT	TRM 416	3"x10"	Replace Sign	Yes	0.21				
4	30.878565	-99.76074066	Menard	SH 29	EB	W9-2TL	Lane Ends Merge Left	36"x36"	Replace Sign		9				
5	30.87773708	-99.75963367	Menard	SH 29	EB	W6-3	Two Way Traffic Symbol	36"x36"	Replace Sign		9				
6	30.87625869	-99.75759525	Menard	SH 29	EB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5				
7	30.87040753	-99.65452055	Menard	SH 29	EB	W8-13aT	Bridge May Ice in Cold	36"x36"	Replace Sign		9				
8	30.87022815	-99.63526089	Menard	SH 29	EB	D10-7aT	TRM 424	3"x10"	Replace Sign	Yes	0.21				
8A	30.87022815	-99.63526089	Menard	SH 29	EB	D10-7aT	TRM 424	3"x10"	Replace Sign	Yes	0.21				
9	30.87018754	-99.63149777	Menard	SH 29	EB	W2-1aT	Highway Intersection	48"x48"	Replace Sign		16				
10	30.87011781	-99.62459231	Menard	SH 29	EB	M1-6F	FM 2092	24"x24"	Replace Sign	Yes	4				
10A	30.87011781	-99.62459231	Menard	SH 29	EB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2				
11	30.87008512	-99.62282174	Menard	SH 29	EB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign & Support			1			
12	30.87007676	-99.62111427	Menard	SH 29	EB	D2-2	Mason 26 mi Llano 60 mi	66"x30"	Replace Sign		13.75				
13	30.86924817	-99.57116738	Menard	SH 29	EB	D21-1TR	Garvin Ln Right Arrow	66"x12"	Replace Sign		5.5				
14	30.869193	-99.572926	Menard	SH 29	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
15	30.8691129	-99.56846244	Menard	SH 29	EB	I-3	Sleepy Hollow	36"x18"	Replace Sign & Support				1		
16	30.8691142	-99.56799914	Menard	SH 29	EB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
17	30.86900653	-99.5438183	Menard	SH 29	EB	W3-5	Speed Limit 60 Ahead	36"x36"	Replace Sign		9				
18	30.86908275	-99.53982554	Menard	SH 29	EB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5				
19	30.869135	-99.537747	Menard	SH 29	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
20	30.86909676	-99.53209347	Menard	SH 29	EB	D1-2	Mason Up Arrow London Right Arrow	72"x30"	Replace Sign & Support				1		
21	30.86902209	-99.52794719	Menard	SH 29	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2				
21A	30.86902209	-99.52794719	Menard	SH 29	EB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
22	30.86888234	-99.52391707	Menard	SH 29	EB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign & Support			1			
23	30.86891984	-99.51278335	Menard	SH 29	EB	D21-2T	Jacoby Ln Left Arrow Blau Ln Right Arrow	66"x24"	Replace Sign		11				
24	30.868953	-99.511915	Menard	SH 29	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
25	30.86898563	-99.49767149	Menard	SH 29	EB	D1-2	Mason Up Arrow Brady Left Arrow	66"x30"	Replace Sign & Support				1		
26	30.86898035	-99.49481984	Menard	SH 29	EB	M1-6F	FM 1311	24"x24"	Replace Sign	Yes	4				
26A	30.86898035	-99.49481984	Menard	SH 29	EB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2				
27	30.86897119	-99.49373133	Menard	SH 29	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2				
27A	30.86897119	-99.49373133	Menard	SH 29	EB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
28	30.86895991	-99.49263239	Menard	SH 29	EB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign & Support			1			
29	30.86909664	-99.48663887	Menard	SH 29	WB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5				
30	30.86910254	-99.48770753	Menard	SH 29	WB	W2-1aT	Highway Intersection	48"x48"	Replace Sign		16				
31	30.86910754	-99.48875718	Menard	SH 29	WB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
32	30.86912605	-99.49176276	Menard	SH 29	WB	D1-2	Menard Up Arrow Brady Right Arrow	66"x30"	Replace Sign & Support				1		
33	30.86917082	-99.49831109	Menard	SH 29	WB	D2-2	Menard 19 mi Eldorado 70 mi	84"x30"	Replace Sign & Support						1
34	30.86910235	-99.51115505	Menard	SH 29	WB	D21-2T	Blau Ln Left Arrow Jacoby Ln Right Arrow	66"x24"	Replace Sign		11				
35	30.869082	-99.511978	Menard	SH 29	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
36	30.86902951	-99.51862601	Menard	SH 29	WB	I-3	Five Mile Creek	42"x18"	Replace Sign & Support				1		
37	30.86903961	-99.52390375	Menard	SH 29	WB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5				
38	30.86905067	-99.52390474	Menard	SH 29	WB	I-2cT	Hext	30"x12"	Replace Sign		2.5				
39	30.86907982	-99.52497173	Menard	SH 29	WB	D1-2	Menard Up Arrow Ahead London Left Arrow	72"x30"	Replace Sign		15				
40	30.86919296	-99.52866201	Menard	SH 29	WB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2				
41	30.86919562	-99.52868456	Menard	SH 29	WB	W1-7T	Double	96"x36"	Replace Sign		24				
42	30.86923694	-99.52982675	Menard	SH 29	WB	M3-4	WEST	24"x12"	Replace Sign & Support	Yes		1			
42A	30.86923694	-99.52982675	Menard	SH 29	WB	M1-6T	TX 29	24"x24"	Replace Sign & Support						
43	30.86925521	-99.53087077	Menard	SH 29	WB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign & Support			1			
44	30.86928026	-99.53432138	Menard	SH 29	WB	M1-6T	TX 29	24"x24"	Replace Sign & Support	Yes		1			
44A	30.86928026	-99.53432138	Menard	SH 29	WB	D10-7aT	TRM 430	3"x10"	Replace Sign						
44B	30.86928026	-99.53432138	Menard	SH 29	WB	D10-7aT	TRM 430	3"x10"	Replace Sign						
45	30.869285	-99.537898	Menard	SH 29	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
46	30.8692395	-99.53982816	Menard	SH 29	WB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign & Support			1			
47	30.869155	-99.554043	Menard	SH 29	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
TOTAL											281.14	7	2	3	1

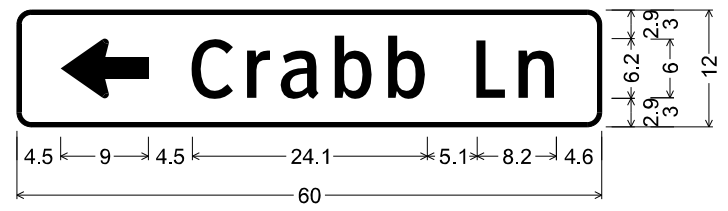


23.06.2021

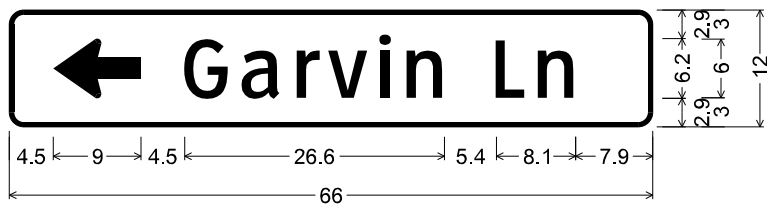
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SHEET 1 OF 5		NOT TO SCALE	
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12-19			

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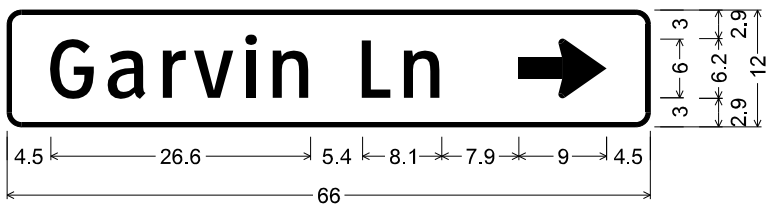
SH 29 from US 83 to Mason County Line Existing Location										0636-6007	0644-6103	644-6104	644-6106	644-6111	
SIGN	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1XP)	REPLACE SRS & S TY 10 BWG (1XT)	REPLACE SRS & S TY 10 BWG (1XU)	REPLACE SRS & S TY S80 (1XU-WC)
48	30.86934182	-99.57499029	Menard	SH 29	WB	D21-1TL	Garvin Ln Left Arrow	66"x12"	Replace Sign		SF	EA	EA	EA	EA
49	30.86980857	-99.59269185	Menard	SH 29	WB	I-3	McDougal Draw	42"x18"	Replace Sign & Support				1		
50	30.86994228	-99.60158639	Menard	SH 29	WB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
51	30.87000836	-99.6059838	Menard	SH 29	WB	D21-1TL	Crabb Ln Left Arrow	60"x12"	Replace Sign		5				
52	30.87029881	-99.62642206	Menard	SH 29	WB	R2-1	Speed Limit 75 MPH	30"x36"	Replace Sign		7.5				
53	30.87029955	-99.62747385	Menard	SH 29	WB	D2-2	Menard 11 mi Eldorado 62 mi	84"x30"	Replace Sign & Support						1
54	30.87049706	-99.64745622	Menard	SH 29	WB	W8-13aT	Bridge May Ice in Cold	36"x36"	Replace Sign		9				
55	30.87069069	-99.66884585	Menard	SH 29	WB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
56	30.87076077	-99.6771747	Menard	SH 29	WB	I-3	Menzies Creek	36"x18"	Replace Sign & Support				1		
57	30.87116556	-99.72642066	Menard	SH 29	WB	R4-3	Slower Traffic Keep Right	24"x30"	Replace Sign		5				
58	30.87122878	-99.73453877	Menard	SH 29	WB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4				
59	30.87957272	-99.76143225	Menard	SH 29	WB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
60	30.88007314	-99.76237763	Menard	SH 29	WB	R5-1	Do Not Enter	36"x36"	Replace Sign		9				
TOTAL											55.2	0	2	0	1



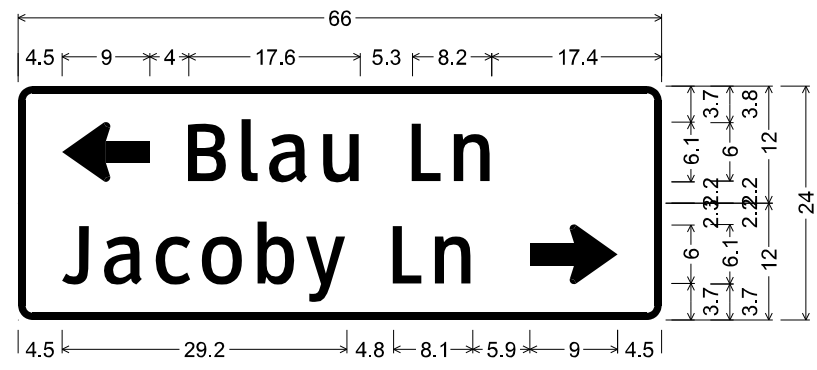
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 "Crabb Ln", ClearviewHwy-3-W;



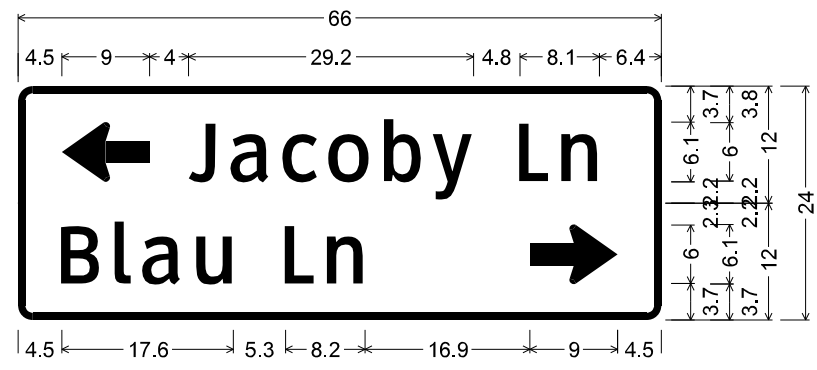
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 Standard Arrow Custom 9.0" X 6.1" 180';
 "Garvin Ln", ClearviewHwy-3-W;



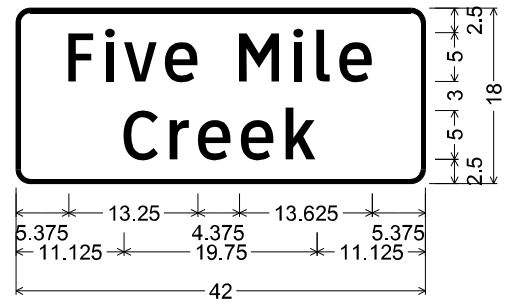
D21-1TR_VARx12;
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 "Garvin Ln", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0';



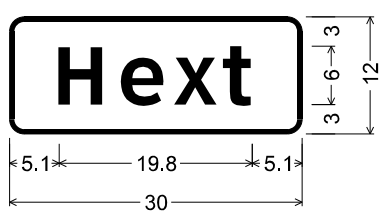
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 Standard Arrow Custom 9.0" X 6.1" 180';
 "Blau Ln", ClearviewHwy-3-W;
 1.5" Radius, 0.8" Border, White on, Green;
 "Jacoby Ln", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0';



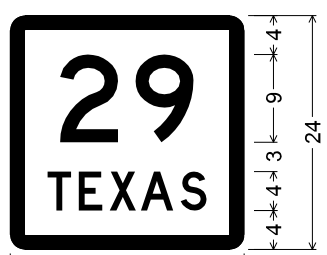
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 "Jacoby Ln", ClearviewHwy-3-W;
 1.5" Radius, 0.8" Border, White on, Green;
 "Blau Ln", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0';



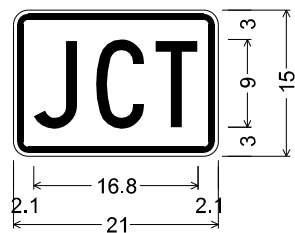
I-3 5in;
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 "Five Mile", ClearviewHwy-3-W;
 "Creek", ClearviewHwy-3-W;



I-2cT 6in;
 1.5" Radius, 0.5" Border, White on, Green;
 "Hext", ClearviewHwy-5-W-R;



M1-6T-2_24x24;
 1.5" Radius, 1.5" Border, Black on, White;
 "29", D;
 "TEXAS", D;



M2-1_21x15;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "JCT", C;



M1-6F_24x24;
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 FARM, 2D;
 ROAD, 2D;
 1311, 4D;



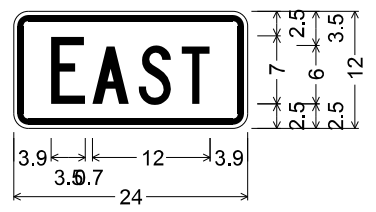
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 2092, 4D;



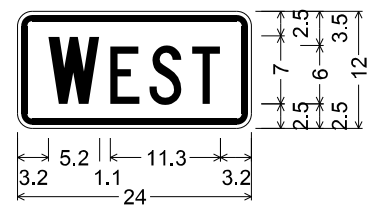
23.06.2021

Texas Department of Transportation		San Angelo District	
REPLACE SIGNS SH 29 MENARD COUNTY			
SHEET 2 OF 5		NOT TO SCALE	
©TxDOT 2021	CONT	SECT	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001
12-19	DIST	COUNTY	SHEET NO.
	SJT	MENARD	43

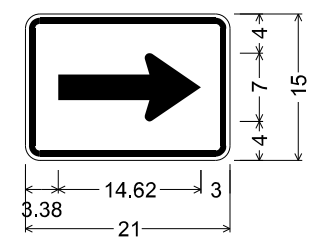
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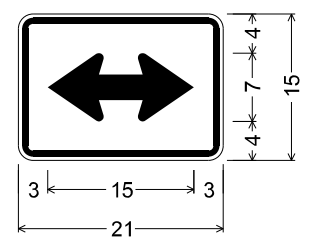
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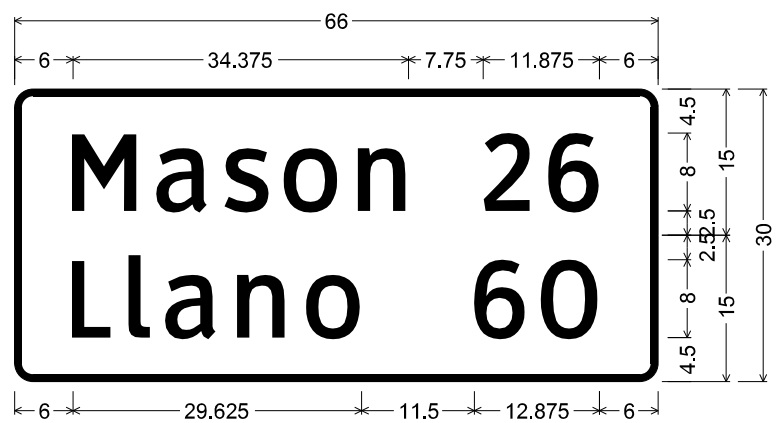
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 "WEST", C;



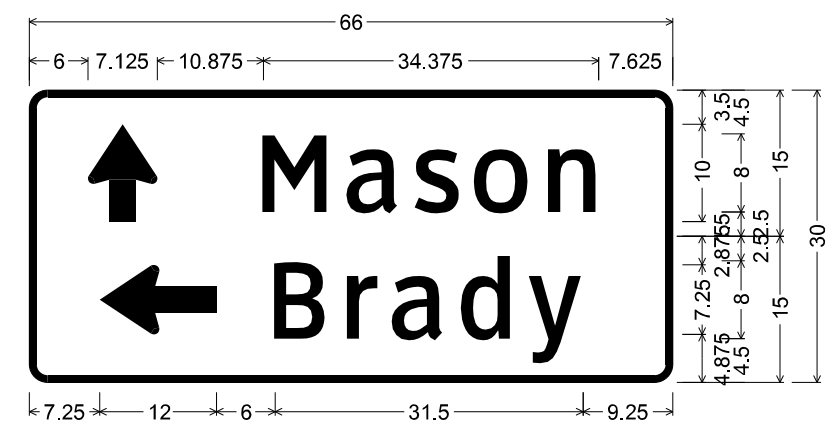
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 Arrow M6-1;



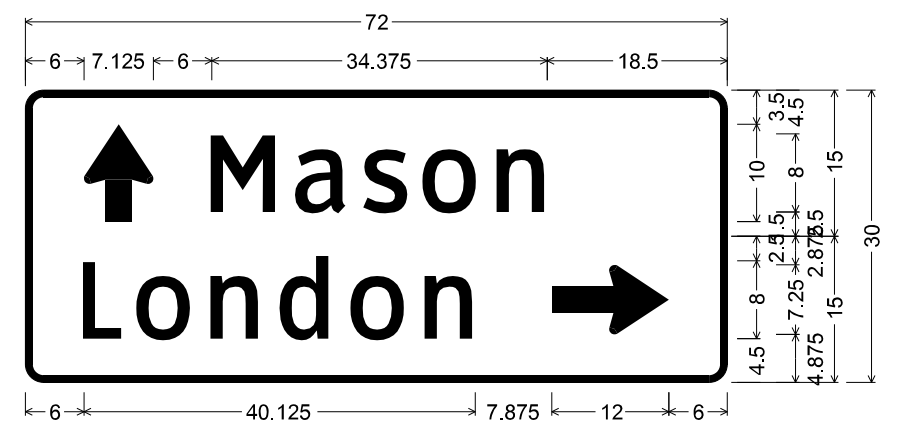
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 Arrow M6-4;



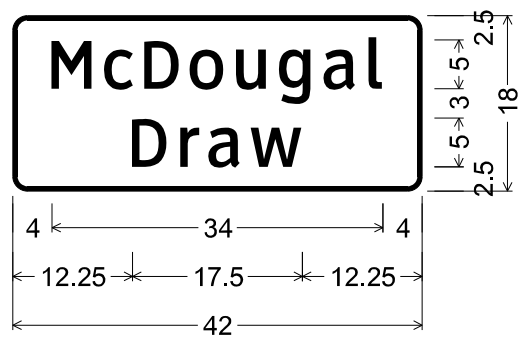
D2-2 8in;
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 "Mason", ClearviewHwy-3-W; "26", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Llano", ClearviewHwy-3-W; "60", ClearviewHwy-3-W;



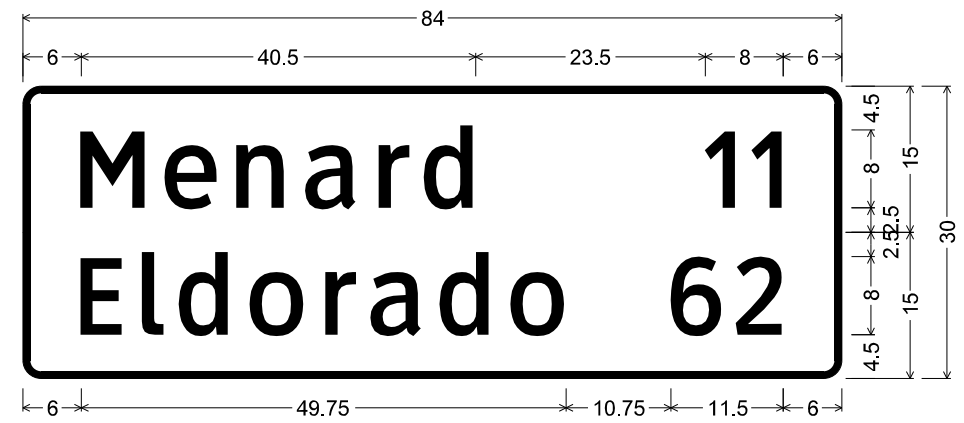
D1-2 8in UP-RT;
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 Standard Arrow Custom 10.000" X 7.125" 90';
 "Mason", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180';
 "Brady", ClearviewHwy-3-W;



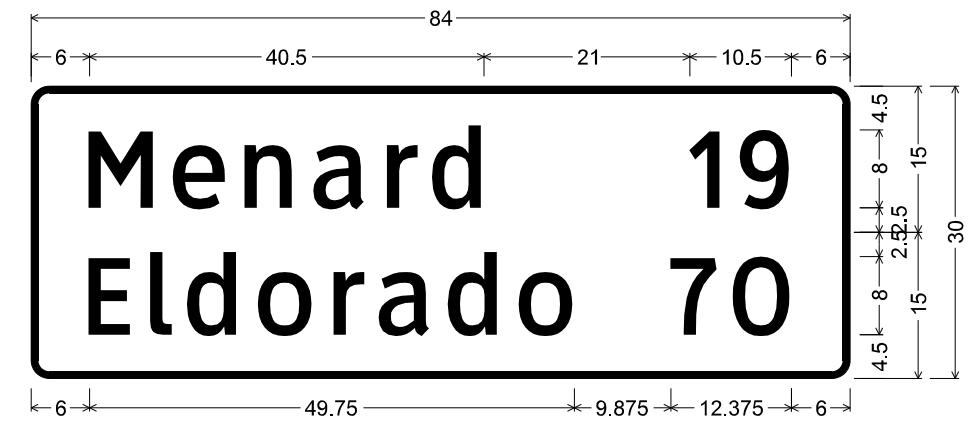
D1-2 8in UP-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 10.000" X 7.125" 90';
 "Mason", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "London", ClearviewHwy-3-W;
 Standard Arrow Custom 12.000" X 7.125" 0';



I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "McDougal", ClearviewHwy-3-W;
 "Draw", ClearviewHwy-3-W;



D2-2 8in;
 1.875" Radius, 0.750" Border, White on, Green;
 "Menard", ClearviewHwy-3-W; "11", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Eldorado", ClearviewHwy-3-W; "62", ClearviewHwy-3-W;



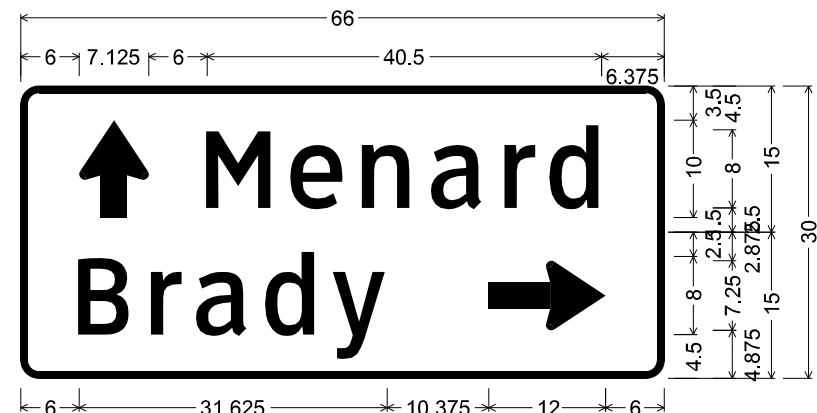
D2-2 8in;
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 "Menard", ClearviewHwy-3-W; "19", ClearviewHwy-3-W;
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 "Eldorado", ClearviewHwy-3-W; "70", ClearviewHwy-3-W;



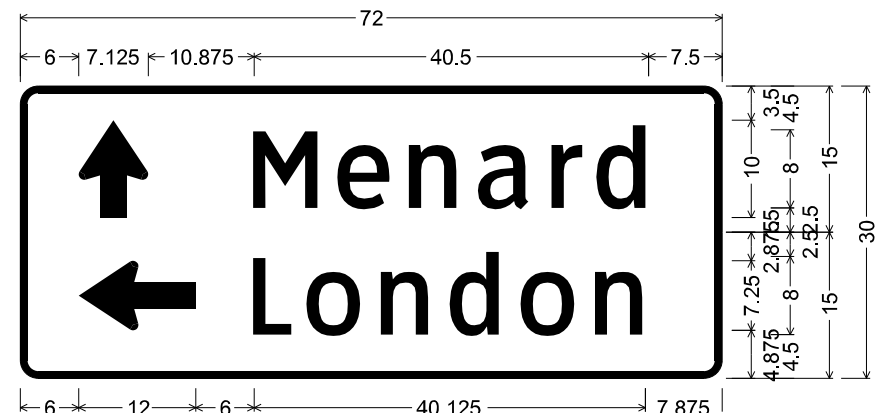
23.06.2021

		San Angelo District	
REPLACE SIGNS SH 29 MENARD COUNTY			
SHEET 3 OF 5		NOT TO SCALE	
©TxDOT 2021 <small>SHEET ISSUED OR LAST REVISED</small>	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 44

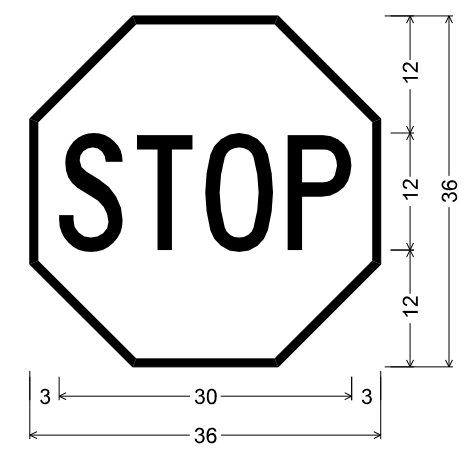
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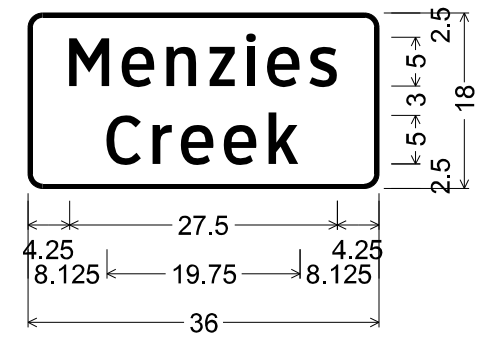
D1-2 8in UP-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 10.000" X 7.125" 90°;
 "Menard", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Brady", ClearviewHwy-3-W;
 Standard Arrow Custom 12.000" X 7.125" 0°;



D1-2 8in UP-LT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 10.000" X 7.125" 90°;
 "Menard", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180°;
 "London", ClearviewHwy-3-W;



R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



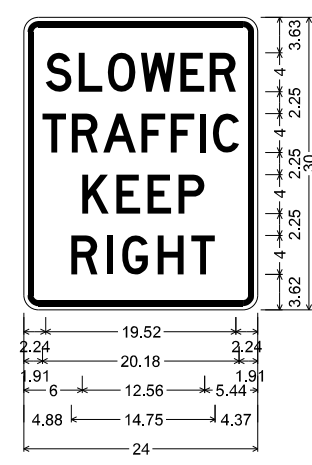
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Menzies", ClearviewHwy-3-W;
 "Creek", ClearviewHwy-3-W;



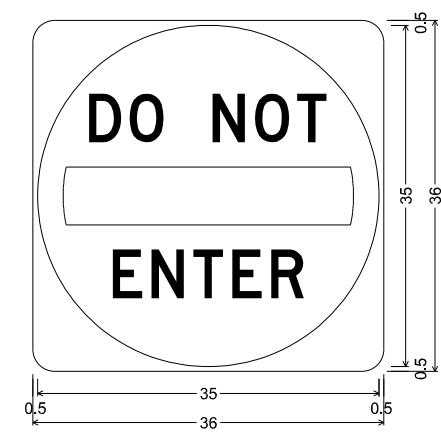
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "60", E specified length;



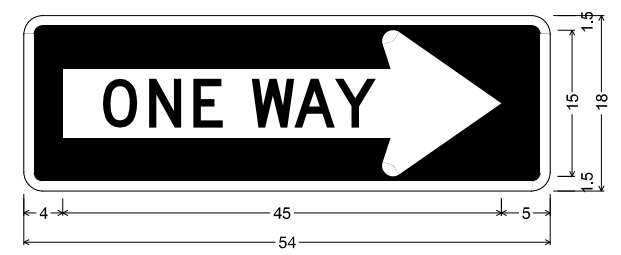
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



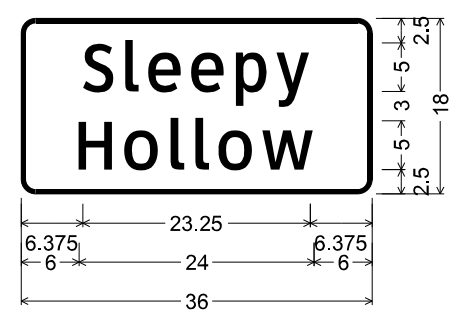
R4-3_24x30;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 "SLOWER", D 75% spacing;
 "TRAFFIC", D 75% spacing;
 "KEEP", D specified length;
 "RIGHT", D specified length;



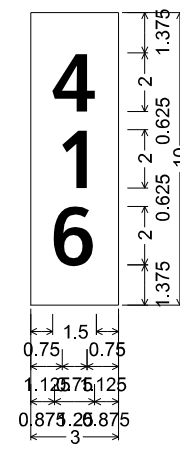
R5-1_36x36;
 2.25" Radius, No border, White;



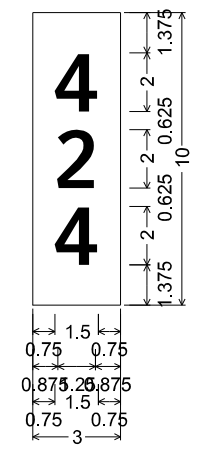
R6-1R_54x18;
 1.88" Radius, 1.00" Border, White on, Black;
 One Way;



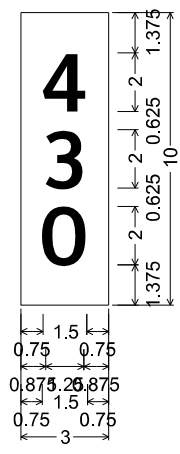
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Sleepy", ClearviewHwy-3-W;
 "Hollow", ClearviewHwy-3-W;



No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;
 "6", ClearviewHwy-4-W;



No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;



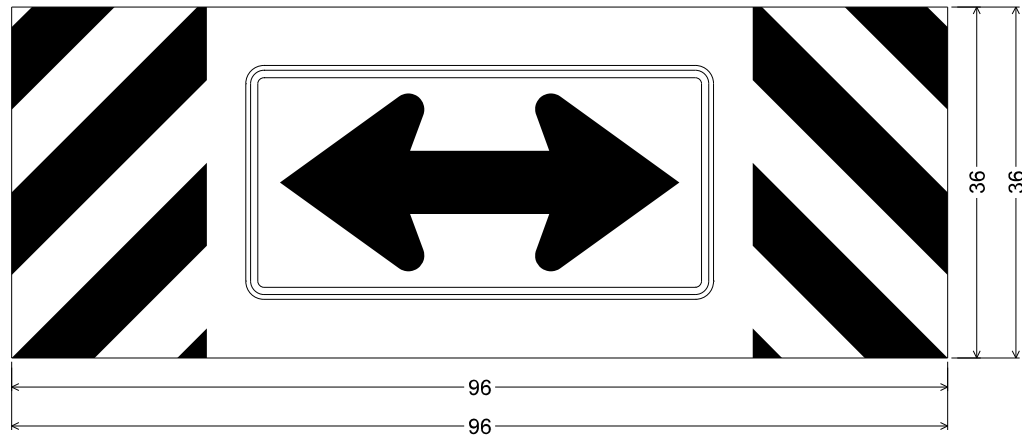
No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "3", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



23.06.2021

		San Angelo District	
REPLACE SIGNS SH 29 MENARD COUNTY			
SHEET 4 OF 5		NOT TO SCALE	
TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83.ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 45

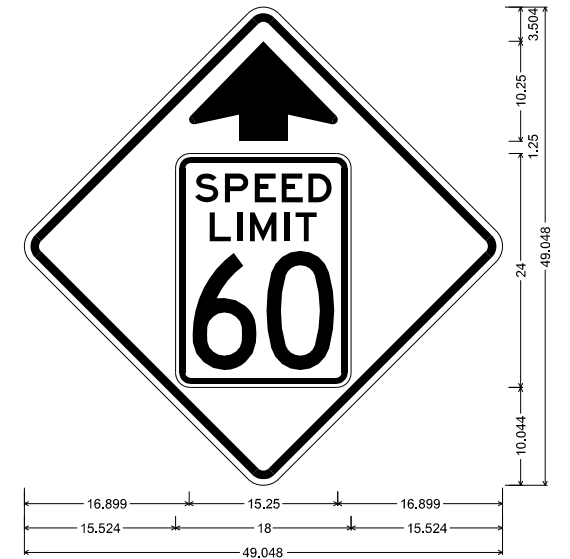
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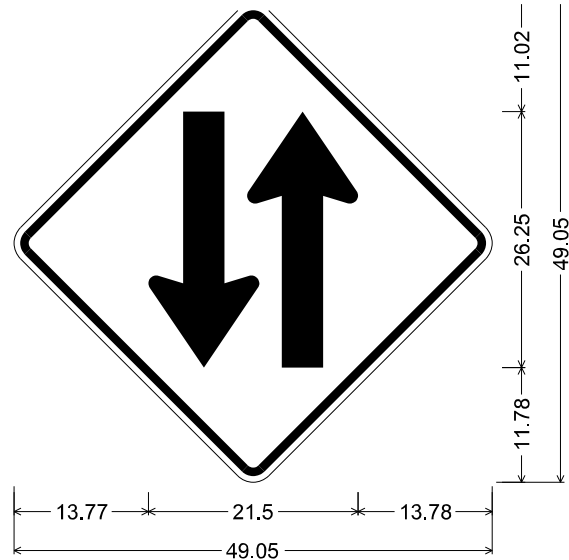
W1-7T_96x36;
 No border, Yellow;



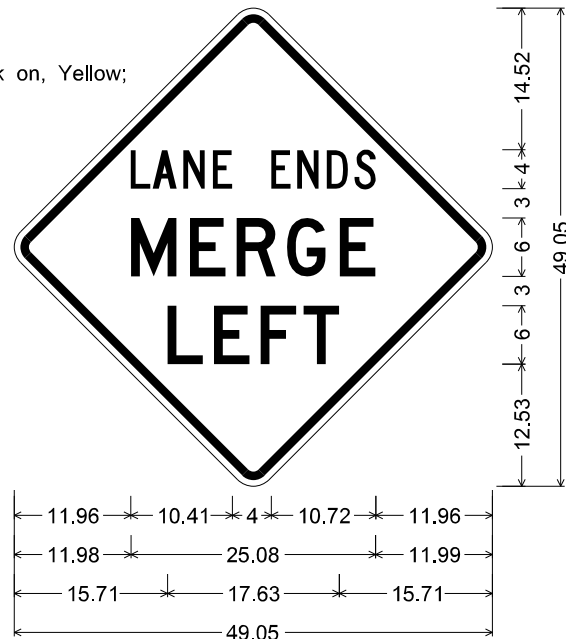
W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



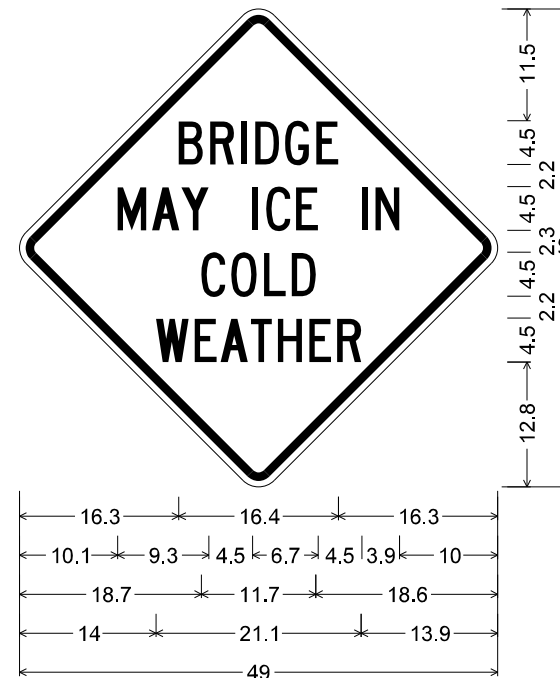
W3-5_36x36;
 36.000" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
 Down Arrow Custom - 10.250" 90°;



W6-3_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W6-3;



W9-2TL_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 "LANE ENDS", C 98% spacing;
 "MERGE", D; "LEFT", D;



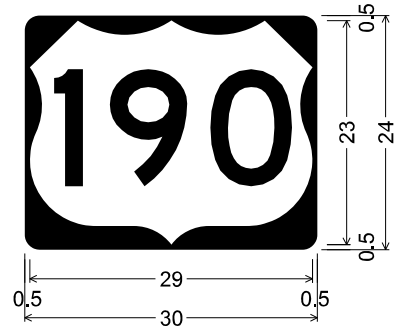
W8-13aT_36x36;
 36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
 "BRIDGE", C 80% spacing;
 "MAY ICE IN", C 80% spacing;
 "COLD", C 80% spacing;
 "WEATHER", C 80% spacing;



23.06.2021

		San Angelo District	
REPLACE SIGNS SH 29 MENARD COUNTY			
SHEET 5 OF 5		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
12-19	DIST SJT	COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 46

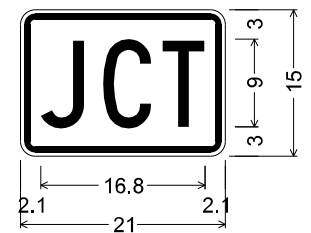
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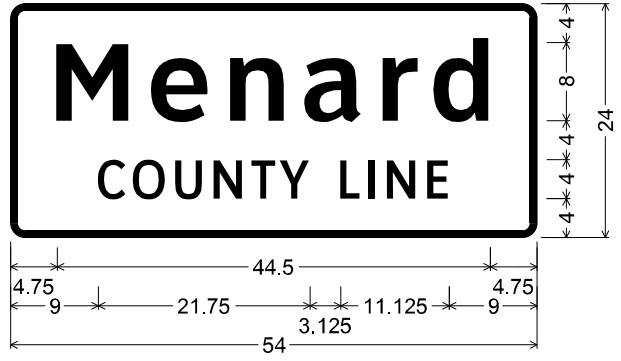
M1-4_30x24;
 1.5" Radius, No border, Black;



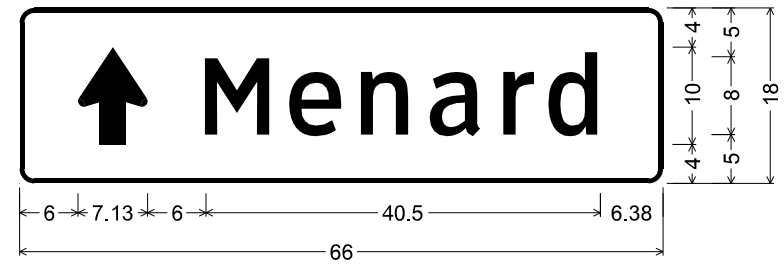
M1-6R_24x24;
 1.5" Radius, No border, Black;
 RANCH, 2D;
 ROAD, 2D;
 864, 5D;



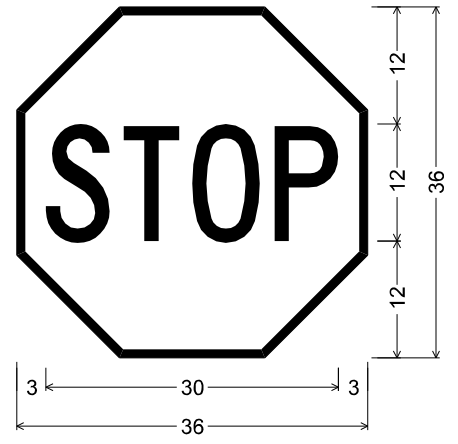
M2-1_21x15;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "JCT", C;



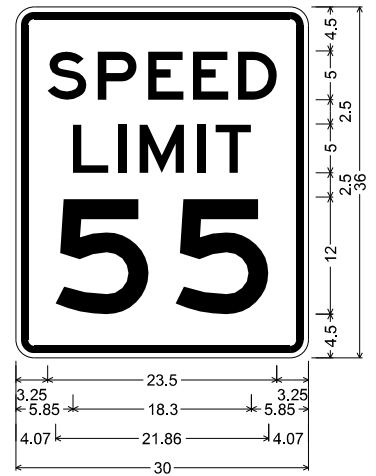
1.500" Radius, 0.750" Border, White on, Green;
 "Menard", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



D1-1 8in UP;
 1.50" Radius, 0.50" Border, White on, Green;
 Standard Arrow Custom 10.00" X 7.13" 90';
 "Menard", ClearviewHwy-3-W;



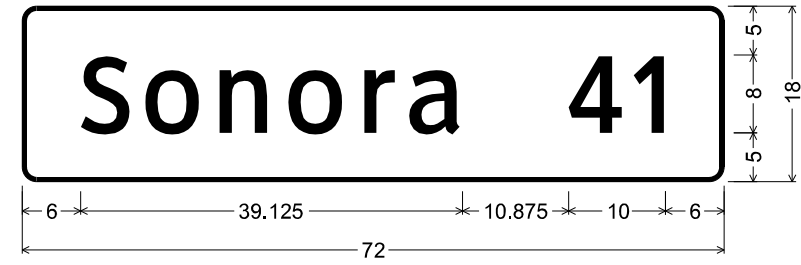
R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



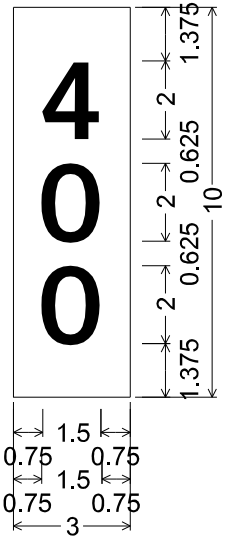
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "55", E specified length;



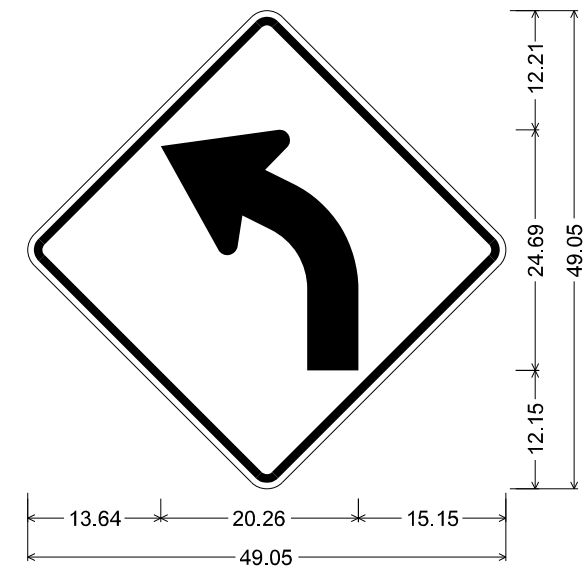
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "70", E specified length;



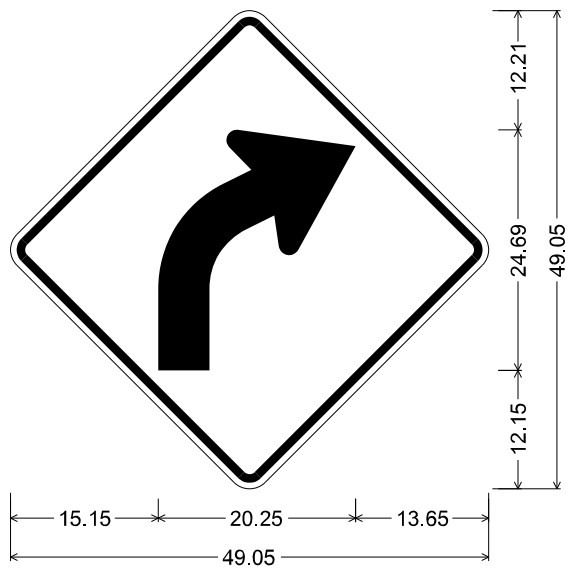
D2-1 8in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Sonora", ClearviewHwy-3-W; "41", ClearviewHwy-3-W;



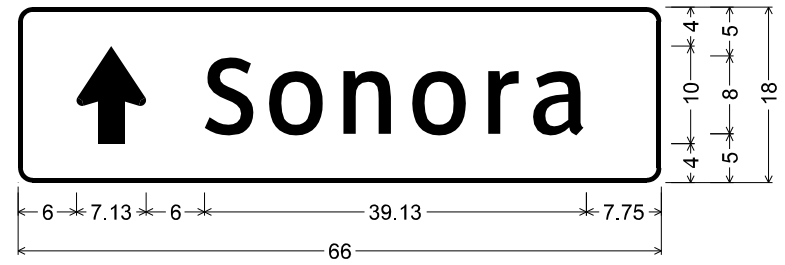
No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



W1-2L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;



W1-2R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;



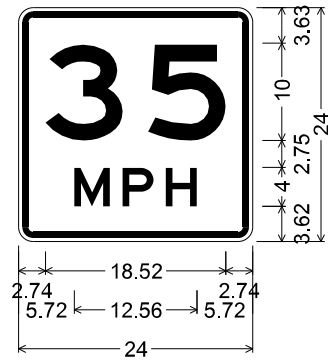
D1-1 8in UP;
 1.50" Radius, 0.50" Border, White on, Green;
 Standard Arrow Custom 10.00" X 7.13" 90';
 "Sonora", ClearviewHwy-3-W;



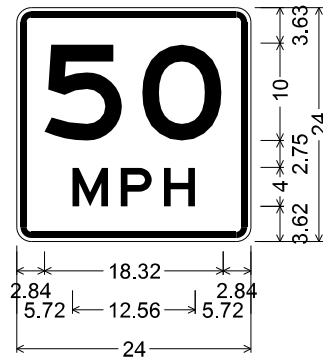
23.06.2021

		San Angelo District	
REPLACE SIGNS RM 864 & RM 1674 MENARD COUNTY			
SHEET 2 OF 3		NOT TO SCALE	
©TXDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 48
12-19		48	

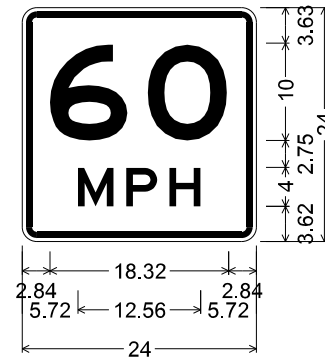
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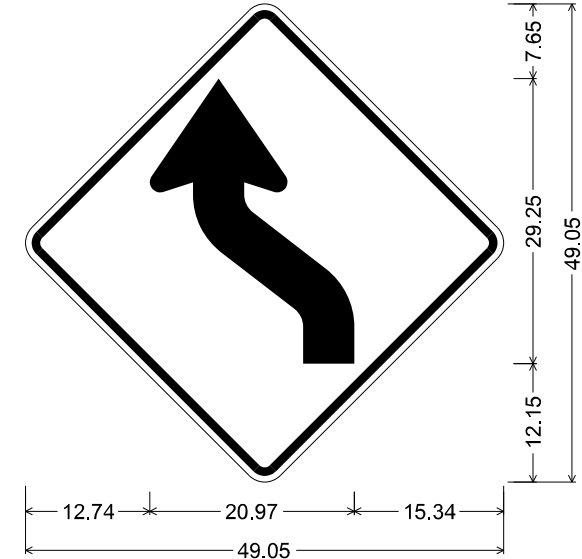
W13-1P_24x24;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Yellow;
 "35", E;
 "MPH", E 121% spacing;



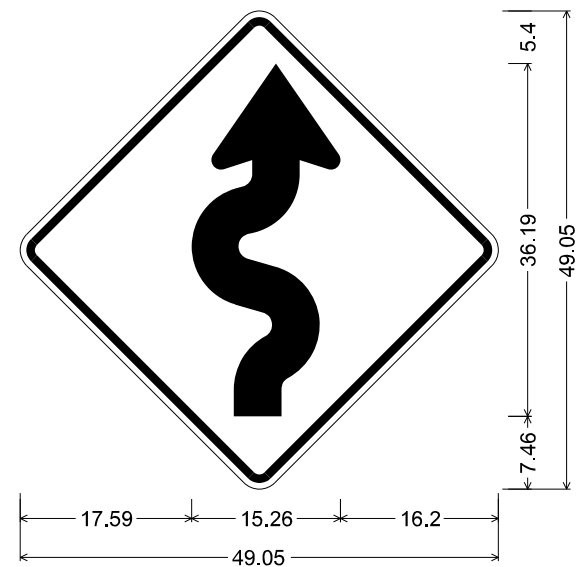
W13-1P_24x24;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Yellow;
 "50", E;
 "MPH", E 121% spacing;



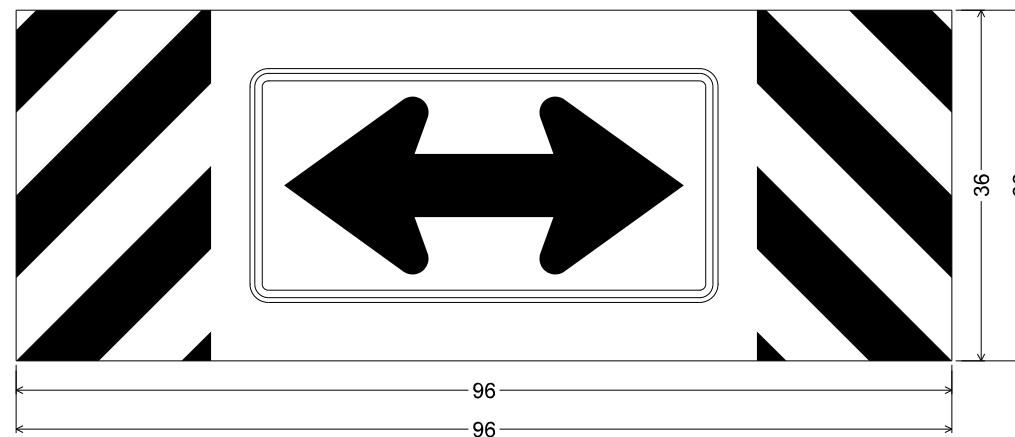
W13-1P_24x24;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Yellow;
 "60", E;
 "MPH", E 121% spacing;



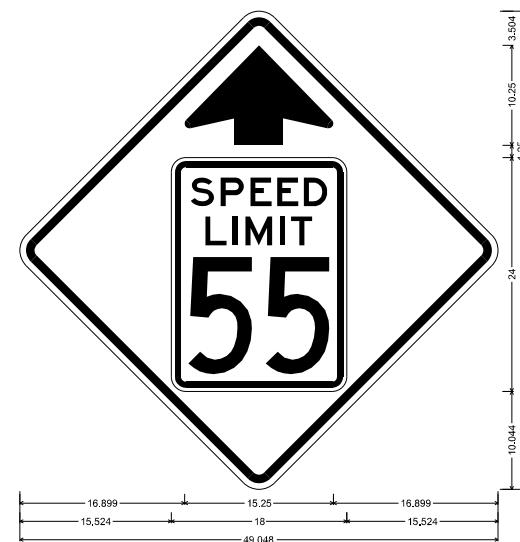
W1-4L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-4;



W1-5R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-5;



W1-7T_96x36;
 No border, Yellow;



W3-5_36x36;
 36.00" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
 Down Arrow Custom - 10.250" 90°;

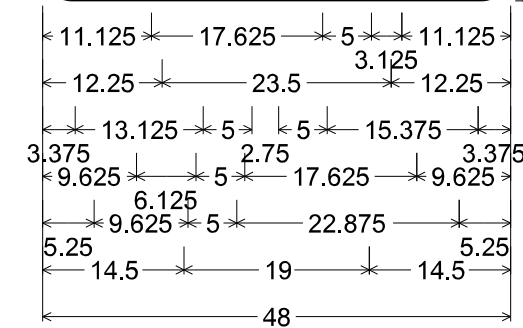
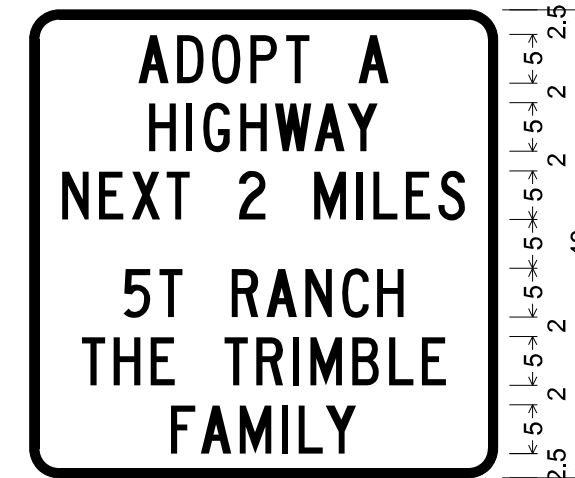


23.06.2021

		San Angelo District	
REPLACE SIGNS RM 864 & RM 1674 MENARD COUNTY			
SHEET 3 OF 3		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 DIST SJT	SECT 49 COUNTY MENARD	JOB 001 HIGHWAY US 83,ETC SHEET NO. 49

FM 2873 from US 190 to End of State Maintenance										0636-6007	0644-6103	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree		
1	30.993286	-100.099349	Menard	FM 2873	SB	M3-3	SOUTH	24"x12"	Replace Sign & Support	Yes		1
1A	30.993286	-100.099349	Menard	FM 2873	SB	M1-6F	FM 2873	24"x24"				
1B	30.993286	-100.099349	Menard	FM 2873	SB	D10-7aT	392	3"x10"				
1C	30.993286	-100.099349	Menard	FM 2873	SB	D10-7aT	392	3"x10"				
2	30.93498683	-100.101148	Menard	FM 2873	SB	M1-6F	FM 2873	24"x24"	Replace Sign	Yes	4	
3	30.88085866	-100.1016879	Menard	FM 2873	SB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16	
4	30.87649872	-100.1017362	Menard	FM 2873	SB	M1-6F	FM 2873	24"x24"	Replace Sign	Yes	4	
5	30.87510038	-100.101756	Menard	FM 2873	SB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1
6	30.93526901	-100.1010055	Menard	FM 2873	NB	D14-4T	Adopt a Highway 5T Ranch the Trimble Family Next 2 Miles	48"x48"	Replace Sign		16	
7	30.963771	-100.1005815	Menard	FM 2873	NB	D14-4T	Adopt a Highway Sultemeier Ranch Next 2 Miles	48"x48"	Replace Sign		16	
8	30.96428301	-100.1005597	Menard	FM 2873	NB	M1-6F	FM 2873	24"x24"	Replace Sign	Yes	4	
9	30.992128	-100.099262	Menard	FM 2873	NB	W1-1R	Turn Symbol	36"x36"	Replace Sign & Support			1
TOTAL											60	3

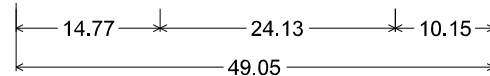
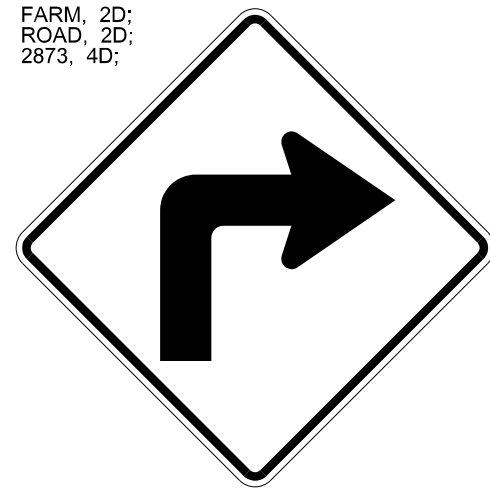
FM 3463 from US 83 to End of State Maintenance										0636-6007	0644-6103	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree		
1	31.0288456	-99.94247394	Menard	FM 3463	EB	M1-6F	FM 3463	24"x24"	Replace Sign	Yes	4	EA
2	31.02890952	-99.93936471	Menard	FM 3463	EB	R2-1	Speed Limit 70	30"x36"	Replace Sign & Support			1
3	31.01950797	-99.87976222	Menard	FM 3463	EB	M1-6F	FM 3463	24"x24"	Replace Sign	Yes	4	
4	31.0129157	-99.85657119	Menard	FM 3463	EB	D14-4T	Adopt a Highway Horny Toad Ranch Next 2 Miles	48"x48"	Replace Sign		16	
5	31.01230559	-99.81887967	Menard	FM 3463	EB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16	
6	31.01195249	-99.8179252	Menard	FM 3463	EB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2	
6A	31.01195249	-99.8179252	Menard	FM 3463	EB	M1-4	US 83	24"x24"	Replace Sign	Yes	4	
7	31.01103572	-99.81625089	Menard	FM 3463	EB	W3-1	Stop Sign Ahead	36"x36"	Replace Sign & Support			1
8	31.02900358	-99.94118585	Menard	FM 3463	WB	D21-1TU	Up Arrow Wilhelm Ln	72"x12"	Replace Sign		6	2
TOTAL											52.2	2



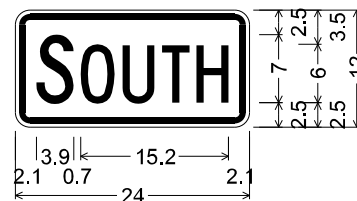
D14-4-3_48X48;
3.000" Radius, 1.000" Border, White on, Blue;
"ADOPT A", C; "HIGHWAY", C;
"NEXT 2 MILES", C;
"5T RANCH", C;
"THE TRIMBLE", C; "FAMILY", C;



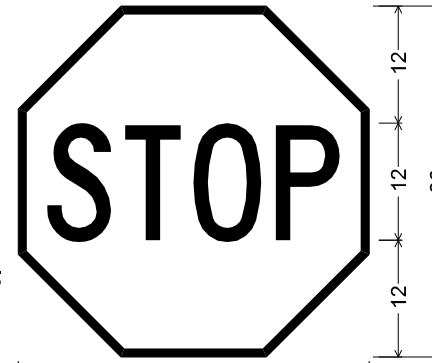
M1-6F_24x24;
1.5" Radius, No border, Black;
FARM, 2D;
ROAD, 2D;
2873, 4D;



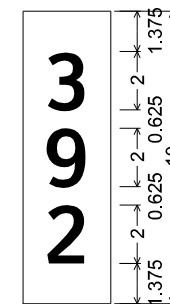
W1-1R_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Arrow W1-1;



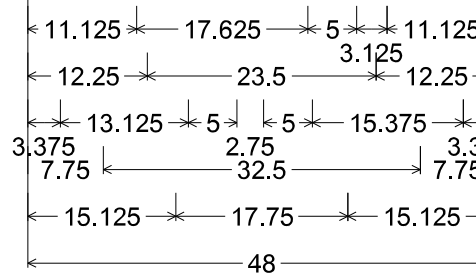
M3-3_24x12;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"SOUTH", C 65% spacing;



R1-1_36x36;
0.9" Border, White on, Red;
"STOP", C 60% spacing;



No border, White on, Green;
"3", ClearviewHwy-4-W;
"9", ClearviewHwy-4-W;
"2", ClearviewHwy-4-W;

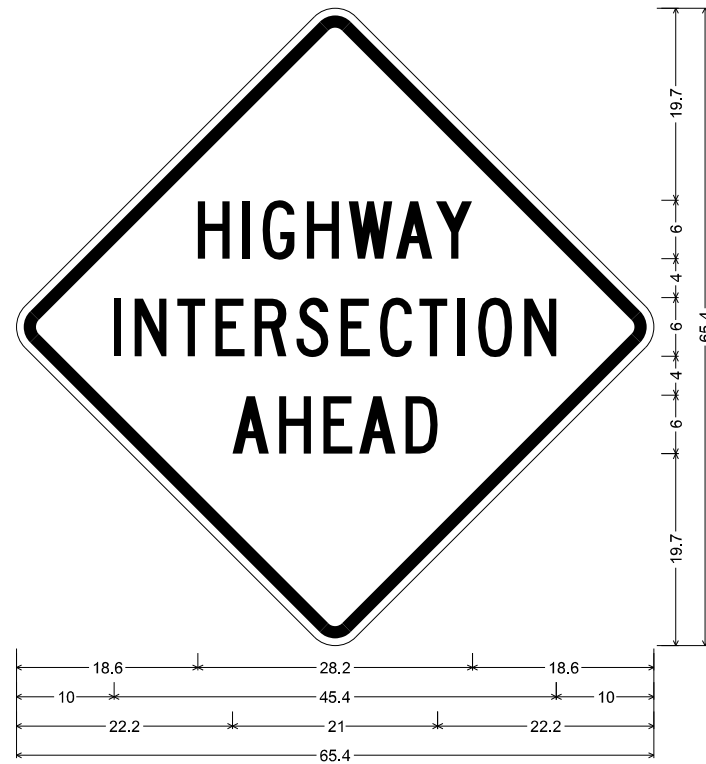


D14-4-3_48X48;
3.000" Radius, 1.000" Border, White on, Blue;
"ADOPT A", C; "HIGHWAY", C;
"NEXT 2 MILES", C;
"SULTEMEIER", C; "RANCH", C;

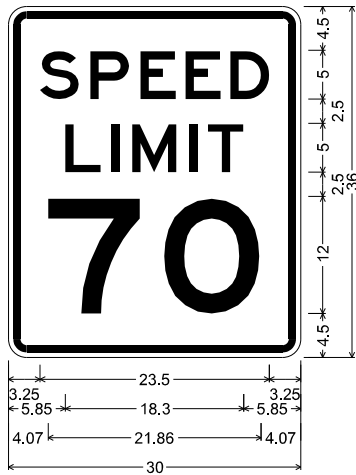


23.06.2021

Texas Department of Transportation		San Angelo District	
REPLACE SIGNS FM 2873 & FM 3463 MENARD COUNTY			
SHEET 1 OF 2		NOT TO SCALE	
©TxDOT 2021	CONT	SECT	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001
12-19	DIST	COUNTY	SHEET NO.
	SJT	MENARD	50



W2-1aT(1)_48x48;
48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
"HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



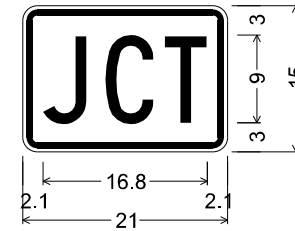
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"70", E specified length;



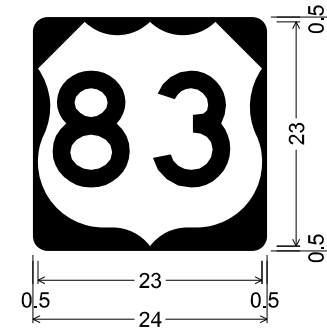
D21-1TL_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
Standard Arrow Custom 8.0" X 6.1" 90°;
"Wilhelm Ln", ClearviewHwy-3-W;



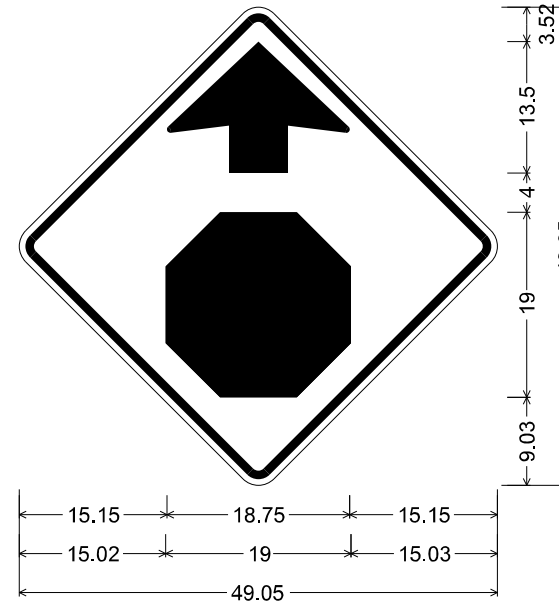
M1-6F_24x24;
1.5" Radius, No border, Black;
FARM, 2D;
ROAD, 2D;
3463, 4D;



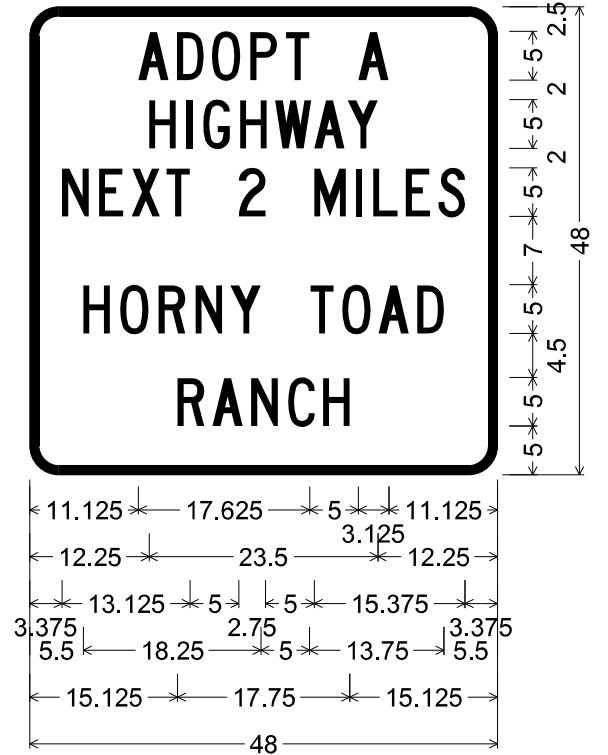
M2-1_21x15;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"JCT", C;



M1-4_24x24;
1.5" Radius, No border, Black;



W3-1_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Down Arrow Custom - 13.50" 90°;
Symbol R1-1;



D14-4-3_48X48;
3.000" Radius, 1.000" Border, White on, Blue;
"ADOPT A", C; "HIGHWAY", C;
"NEXT 2 MILES", C;
"HORNY TOAD", C; "RANCH", C;

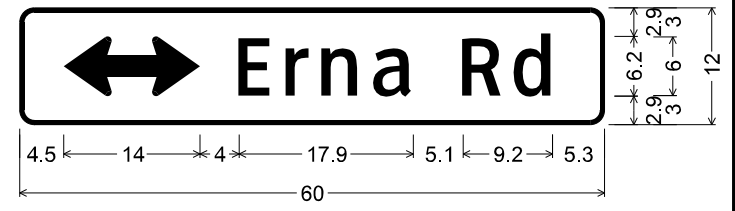


23.06.2021

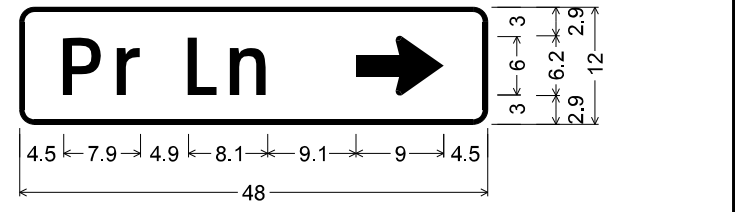
		San Angelo District	
REPLACE SIGNS FM 2873 & FM 3463 MENARD COUNTY			
SHEET 2 OF 2		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 SECT 49 DIST SJT	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 51

RM 1221 from SH 29 to Kimble County Line
Existing Location

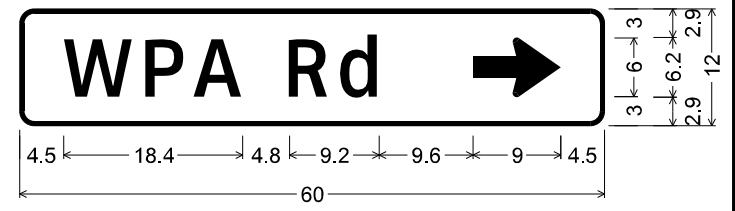
	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	0636-6007 REPLACE EXISTING ALUMINUM SIGNS (TY A)	0644-6103 REPLACE SRS & S TY 10 BWG (1XP)	644-6104 REPLACE SRS & S TY 10 BWG (1XT)	644-6106 REPLACE SRS & S TY 10 BWG (1XU)
Sign											SF	EA	EA	EA
1	30.71575967	-99.56349818	Menard	RM 1221	NB	W8-18	Road May Flood	36"x36"	Replace Sign		9			
2	30.722251	-99.563422	Menard	RM 1221	NB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
3	30.73304659	-99.56345346	Menard	RM 1221	NB	M1-6R	RM 1221	24"x24"	Replace Sign	Yes	4			
3A	30.73304659	-99.56345346	Menard	RM 1221	NB	D10-7aT	TRM 410	3"x10"	Replace Sign	Yes	0.21			
3B	30.73304659	-99.56345346	Menard	RM 1221	NB	D10-7aT	TRM 410	3"x10"	Replace Sign	Yes	0.21			
5	30.74387668	-99.56319843	Menard	RM 1221	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
5A	30.74387668	-99.56319843	Menard	RM 1221	NB	M1-6R	RM 1773	24"x24"	Replace Sign	Yes	4			
6	30.74659752	-99.56314288	Menard	RM 1221	NB	D1-1	Hext Up Arrow	54"x18"	Replace Sign & Support				1	
7	30.74739652	-99.56312118	Menard	RM 1221	NB	W1-2R	Curve Symbol	36"x36"	Replace Sign	Yes	9			
8	30.74878662	-99.56310855	Menard	RM 1221	NB	M3-4	WEST	24"x12"	Replace Sign	Yes	2			
8A	30.74878662	-99.56310855	Menard	RM 1221	NB	M6-1	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2			
9	30.75067745	-99.56245572	Menard	RM 1221	NB	M1-6R	RM 1221	24"x24"	Replace Sign	Yes	4			
10	30.75165882	-99.56171727	Menard	RM 1221	NB	D21-1TDBL	Erna Rd Double Arrow	60"x12"	Replace Sign		5			
11	30.752084	-99.561383	Menard	RM 1221	NB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
12	30.78043108	-99.54594356	Menard	RM 1221	NB	D21-1TR	WPA Rd Right	60"x12"	Replace Sign		5			
13	30.781028	-99.545907	Menard	RM 1221	NB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
14	30.80177296	-99.53773948	Menard	RM 1221	NB	W13-1P	50 MPH Advisory	24"x24"	Replace Sign	Yes	4			
15	30.84035091	-99.52943784	Menard	RM 1221	NB	M1-6R	RM 1221	24"x24"	Replace Sign	Yes	4			
15A	30.84035091	-99.52943784	Menard	RM 1221	NB	D10-7aT	TRM 402	3"x10"	Replace Sign	Yes	0.21			
15B	30.84035091	-99.52943784	Menard	RM 1221	NB	D10-7aT	TRM 402	3"x10"	Replace Sign	Yes	0.21			
16	30.86439738	-99.52904492	Menard	RM 1221	NB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4			
17	30.8662215	-99.52897492	Menard	RM 1221	NB	W3-1	Stop Sign Ahead	36"x36"	Replace Sign & Support			1		
18	30.86902574	-99.52853831	Menard	RM 1221	NB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1		
19	30.86852717	-99.52886908	Menard	RM 1221	SB	M1-6R	RM 1221	24"x24"	Replace Sign	Yes	4			
19A	30.86852717	-99.52886908	Menard	RM 1221	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21			
19B	30.86852717	-99.52886908	Menard	RM 1221	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21			
20	30.86765442	-99.52901369	Menard	RM 1221	SB	R2-1	Speed Limit 60	30"x36"	Replace Sign		7.5			
21	30.86679017	-99.52910868	Menard	RM 1221	SB	NA	Caution Road May Flood 13 Miles	72"x30"	Replace Sign & Support				1	
22	30.80979346	-99.5317778	Menard	RM 1221	SB	W1-4R	Curve Symbol	36"x36"	Replace Sign	Yes	9			
22A	30.80979346	-99.5317778	Menard	RM 1221	SB	W13-1P	50 MPH Advisory	24"x24"	Replace Sign	Yes	4			
23	30.79671593	-99.54080725	Menard	RM 1221	SB	W1-2R	Curve Symbol	36"x36"	Replace Sign		9			
24	30.76627416	-99.55363273	Menard	RM 1221	SB	W1-2L	Curve Symbol	36"x36"	Replace Sign		9			
25	30.76030717	-99.55668946	Menard	RM 1221	SB	W1-2R	Curve Symbol	36"x36"	Replace Sign		9			
26	30.75408046	-99.56012332	Menard	RM 1221	SB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
26A	30.75408046	-99.56012332	Menard	RM 1221	SB	M1-6R	RM 1773	24"x24"	Replace Sign	Yes	4			
27	30.75272947	-99.5611144	Menard	RM 1221	SB	D21-1TDBL	Erna Rd Double Arrow	60"x12"	Replace Sign		5			
28	30.7523287	-99.56141277	Menard	RM 1221	SB	W1-2L	Curve Symbol	36"x36"	Replace Sign	Yes	9			
29	30.7503343	-99.56284108	Menard	RM 1221	SB	M3-4	WEST	24"x12"	Replace Sign	Yes	2			
29A	30.7503343	-99.56284108	Menard	RM 1221	SB	M1-6R	RM 1773	24"x24"	Replace Sign	Yes	4			
29B	30.7503343	-99.56284108	Menard	RM 1221	SB	M6-1	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2			
30	30.75022975	-99.56306007	Menard	RM 1221	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign		9			
31	30.74835403	-99.56326184	Menard	RM 1221	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2			
31A	30.74835403	-99.56326184	Menard	RM 1221	SB	M1-6R	RM 1221	24"x24"	Replace Sign	Yes	4			
32	30.74745013	-99.56327173	Menard	RM 1221	SB	R2-1	Speed Limit 60	30"x36"	Replace Sign		7.5			
33	30.73771004	-99.56348741	Menard	RM 1221	SB	D21-1TR	Pr Ln Right	48"x12"	Replace Sign		4			
34	30.736898	-99.563563	Menard	RM 1221	SB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
35	30.7201678	-99.56361989	Menard	RM 1221	SB	W8-18	Road May Flood	36"x36"	Replace Sign		9			
36	30.71063266	-99.56376696	Menard	RM 1221	SB	D10-7aT	TRM 412	3"x10"	Replace Sign	Yes	0.21			
36A	30.71063266	-99.56376696	Menard	RM 1221	SB	D10-7aT	TRM 412	3"x10"	Replace Sign	Yes	0.21			
TOTAL											211.48	2	1	1



D21-1TDBL_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
Double Headed Arrow Custom - 14.0" 0';
"Erna Rd", ClearviewHwy-3-W;



D21-1TR_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
"Pr Ln", ClearviewHwy-3-W;
Standard Arrow Custom 9.0" X 6.1" 0';



D21-1TR_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
"WPA Rd", ClearviewHwy-3-W;
Standard Arrow Custom 9.0" X 6.1" 0';

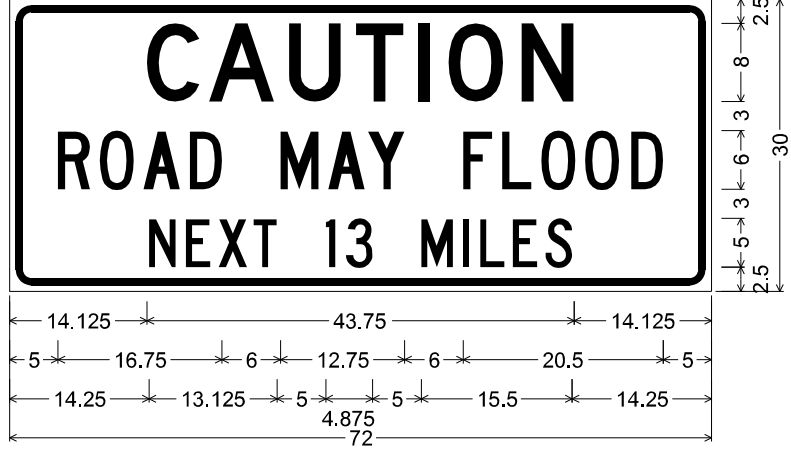


23.06.2021



REPLACE SIGNS
RM 1221
MENARD COUNTY

SHEET 1 OF 3		NOT TO SCALE	
OTxDOT	2021	CONT	SECT
SHEET ISSUED OR LAST REVISED		6383	49
12-19		JOB	001
		HIGHWAY	US 83,ETC
		DIST	COUNTY
		SJT	MENARD
		SHEET NO.	52



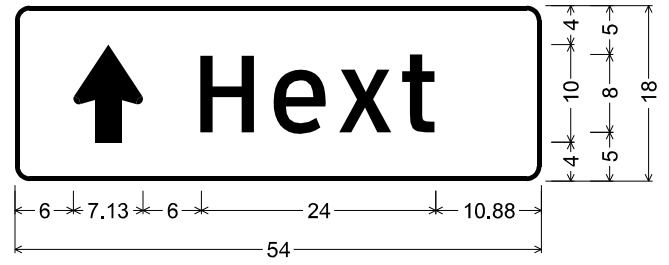
2.250" Radius, 0.750" Border, 0.625" Indent, Black on, Yellow;
"CAUTION", D; "ROAD MAY FLOOD", C;
"NEXT 13 MILES", C;



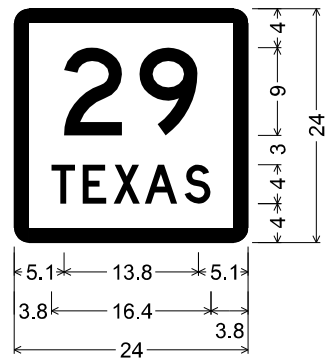
M1-6R_24x24;
1.5" Radius, No border, Black;
RANCH, 2D;
ROAD, 2D;
1221, 4D;



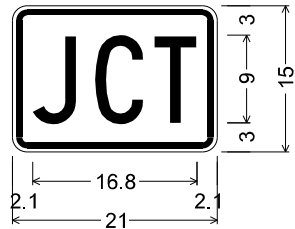
M1-6R_24x24;
1.5" Radius, No border, Black;
RANCH, 2D;
ROAD, 2D;
1773, 4D;



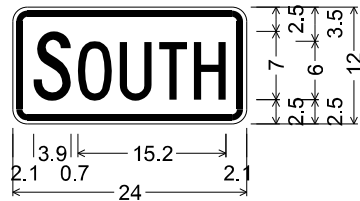
D1-1 8in UP;
1.50" Radius, 0.50" Border, White on, Green;
Standard Arrow Custom 10.00" X 7.13" 90';
"Hext", ClearviewHwy-3-W;



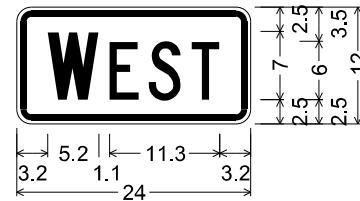
M1-6T-2_24x24;
1.5" Radius, 1.5" Border, Black on, White;
"29", D;
"TEXAS", D;



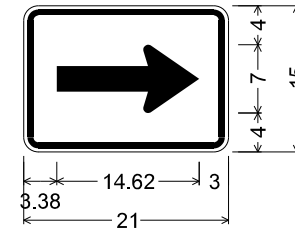
M2-1_21x15;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"JCT", C;



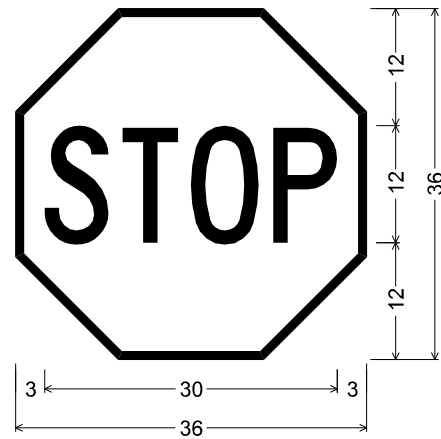
M3-3_24x12;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"SOUTH", C 65% spacing;



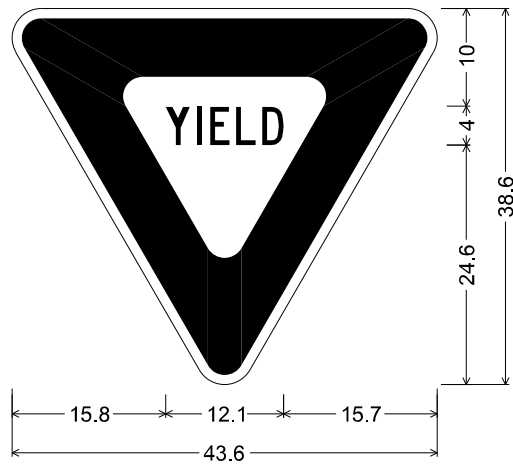
M3-4_24x12;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"WEST", C;



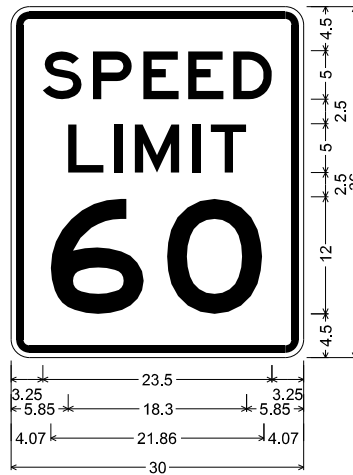
M6-1_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-1;



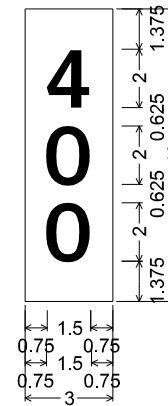
R1-1_36x36;
0.9" Border, White on, Red;
"STOP", C 60% spacing;



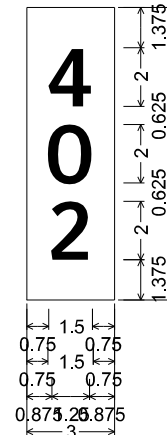
R1-2_48x48;
48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
"YIELD", C specified length;



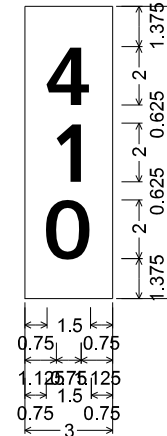
R2-1_30x36;
1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
"SPEED", E specified length;
"LIMIT", E specified length;
"60", E specified length;



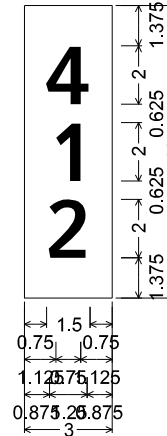
No border, White on, Green;
"4", ClearviewHwy-4-W;
"0", ClearviewHwy-4-W;
"0", ClearviewHwy-4-W;



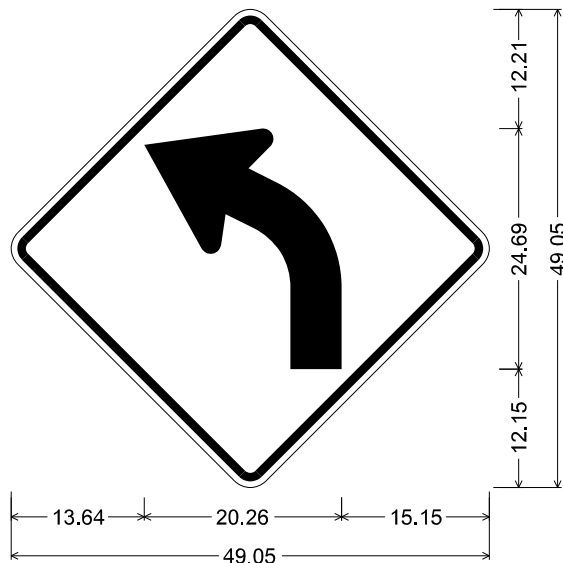
No border, White on, Green;
"4", ClearviewHwy-4-W;
"0", ClearviewHwy-4-W;
"2", ClearviewHwy-4-W;



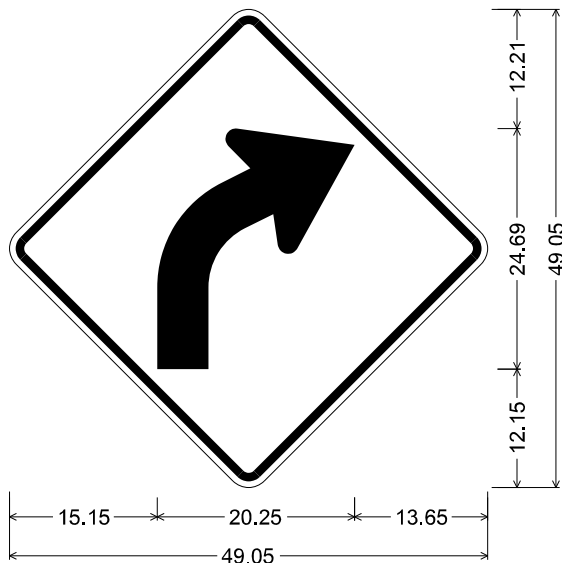
No border, White on, Green;
"4", ClearviewHwy-4-W;
"1", ClearviewHwy-4-W;
"0", ClearviewHwy-4-W;



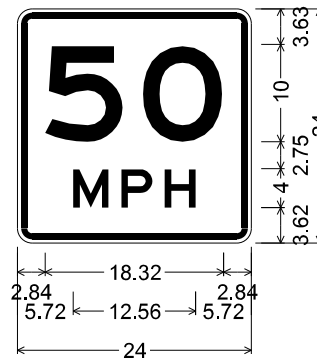
No border, White on, Green;
"4", ClearviewHwy-4-W;
"1", ClearviewHwy-4-W;
"2", ClearviewHwy-4-W;



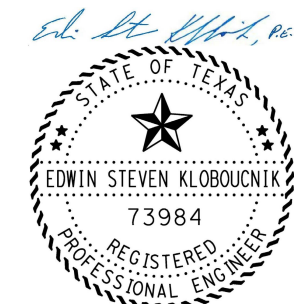
W1-2L_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Arrow W1-2;



W1-2R_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Arrow W1-2;

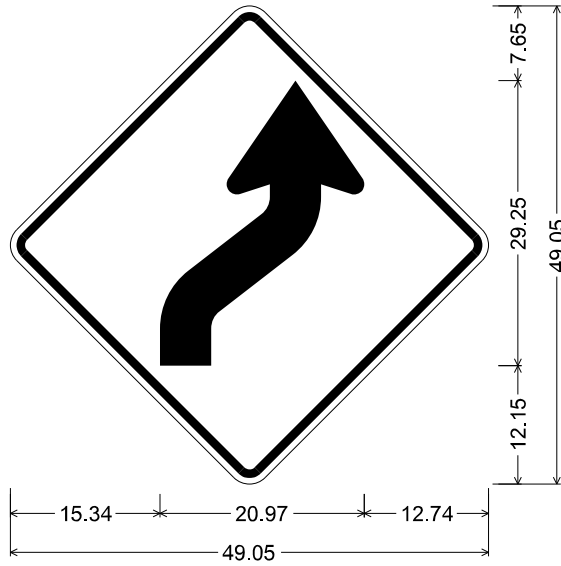


W13-1P_24x24;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Yellow;
"50", E;
"MPH", E 121% spacing;



23.06.2021

		San Angelo District	
REPLACE SIGNS RM 1221 MENARD COUNTY			
SHEET 2 OF 3		NOT TO SCALE	
© TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 DIST SJT	SECT 49 COUNTY MENARD	JOB 001 HIGHWAY US 83, ETC SHEET NO. 53

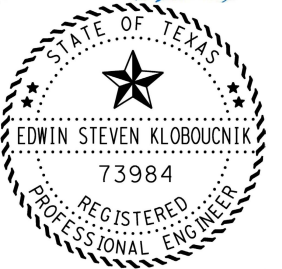


W1-4R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-4;



W8-18_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 "ROAD", C; "MAY", C; "FLOOD", C;

Ed. St. Kloboucnik, P.E.



23.06.2021

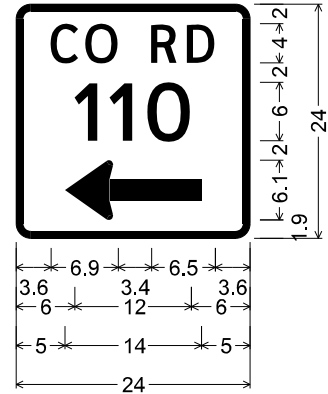


**REPLACE SIGNS
 RM 1221
 MENARD COUNTY**

SHEET 3 OF 3 NOT TO SCALE

©TxDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83,ETC
12-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	54	

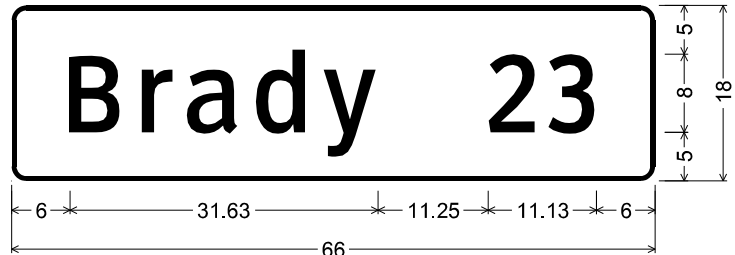
FM 1311 from SH 29 to McCulloch County Line										0636-6007	0644-6103	644-6104	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree			
1	30.94096924	-99.49034233	Menard	FM 1311	SB	M1-6F	FM 1311	24"x24"	Replace Sign	Yes	SF		
2	30.9406126	-99.49034752	Menard	FM 1311	SB	R2-1	Speed Limit 70	30"x36"	Replace Sign		7.5		
3	30.91755809	-99.49149985	Menard	FM 1311	SB	W8-18	Road May Flood	36"x36"	Replace Sign		9		
4	30.88301005	-99.49490308	Menard	FM 1311	SB	M1-6F	FM 1311	24"x24"	Replace Sign	Yes	4		
4A	30.88301005	-99.49490308	Menard	FM 1311	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21		
4B	30.88301005	-99.49490308	Menard	FM 1311	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21		
5	30.8752733	-99.49468541	Menard	FM 1311	SB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
6	30.87258988	-99.49462264	Menard	FM 1311	SB	W3-1	Stop Sign Ahead	36"x36"	Replace Sign & Support			1	
7	30.86920101	-99.49453818	Menard	FM 1311	SB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2		
8	30.86914333	-99.49448371	Menard	FM 1311	SB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1	
9	30.8691501	-99.49498697	Menard	FM 1311	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign & Support				1
10	30.87264997	-99.49446443	Menard	FM 1311	NB	D2-1	Brady 23 mi	66"x18"	Replace Sign		8.25		
11	30.90610909	-99.49395461	Menard	FM 1311	NB	D14-4T	Adopt a Highway Next 2 Miles JJ's Rocking Retreat	48"x48"	Replace Sign		16		
12	30.9095838	-99.49346542	Menard	FM 1311	NB	W8-18	Road May Flood	36"x36"	Replace Sign		9		
13	30.91122312	-99.49306021	Menard	FM 1311	NB	M1-6F	FM 1311	24"x24"	Replace Sign	Yes	4		
14	30.94020047	-99.49021894	Menard	FM 1311	NB	D20-1TL	CO RD 110	24"x24"	Replace Sign & Support			1	
15	30.94095029	-99.49018782	Menard	FM 1311	NB	I-2dT	McCulloch County Line	78"x24"	Replace Sign & Support				1
TOTAL											80.37	3	2



D20-1TL_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "110", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180';



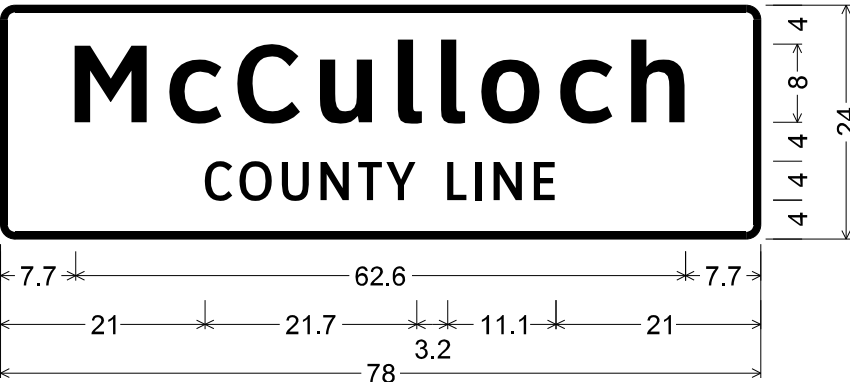
M1-6F_24x24;
 1.5" Radius, No border, Black;
 FARM, 2D;
 ROAD, 2D;
 1311, 4D;



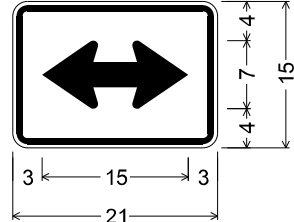
D2-1 8in;
 1.50" Radius, 0.50" Border, White on, Green;
 "Brady", ClearviewHwy-3-W; "23", ClearviewHwy-3-W;



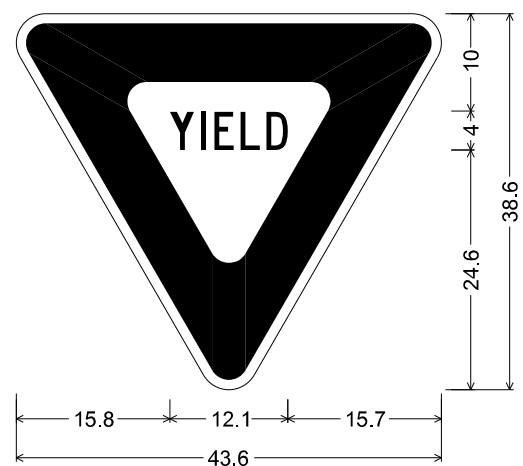
3.000" Radius, 1.000" Border, White on, Blue;
 "ADOPT A", C; "HIGHWAY", C;
 "NEXT 2 MILES", C;
 "JJ'S ROCKING", C;
 "RETREAT", C;



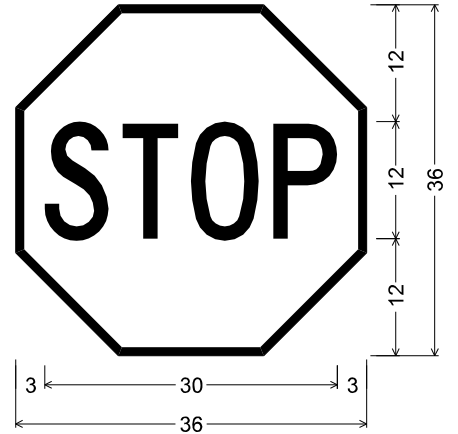
I-2dT 8in;
 1.5" Radius, 0.8" Border, White on, Green;
 "McCulloch", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



M6-4_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-4;



R1-2_48x48;
 48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
 "YIELD", C specified length;

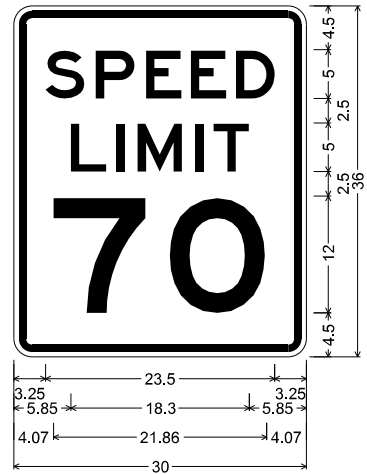


R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;

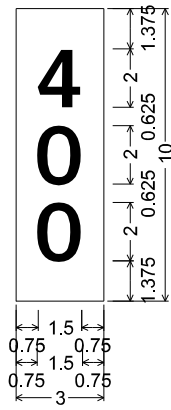


23.06.2021

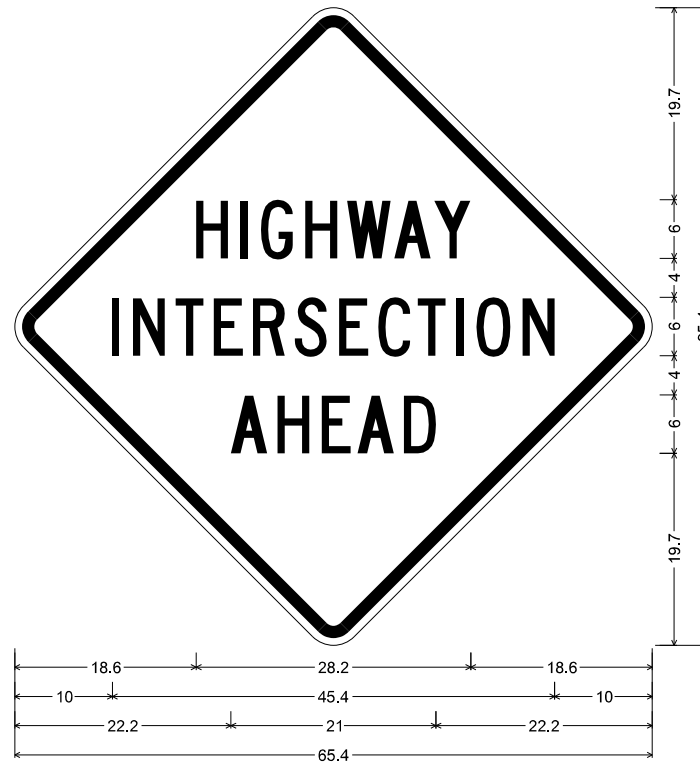
Texas Department of Transportation		San Angelo District	
REPLACE SIGNS FM 1311 MENARD COUNTY			
SHEET 1 OF 2		NOT TO SCALE	
TXDOT 2021	CONT 6383	SECT 49	JOB 001
SHEET ISSUED OR LAST REVISED		HIGHWAY US 83,ETC	
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 55



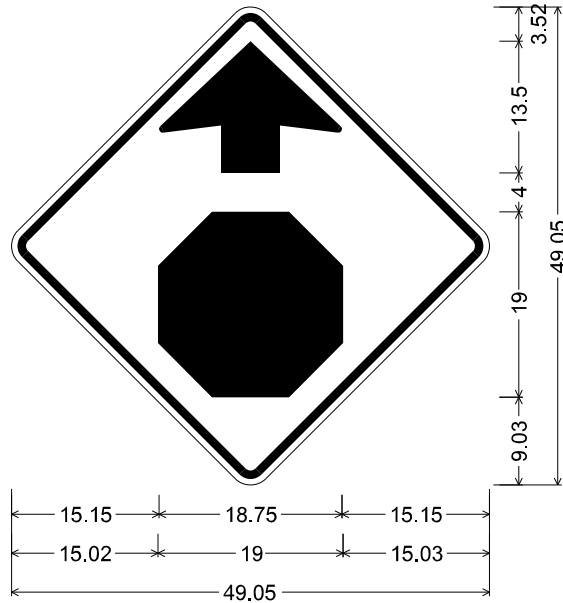
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "70", E specified length;



No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



W3-1_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Down Arrow Custom - 13.50" 90°;
 Symbol R1-1;



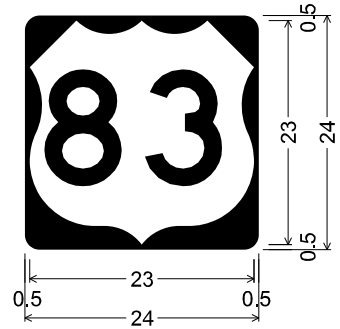
W8-18_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 "ROAD", C; "MAY", C; "FLOOD", C;



23.06.2021

		San Angelo District	
REPLACE SIGNS FM 1311 MENARD COUNTY			
SHEET 2 OF 2		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 56

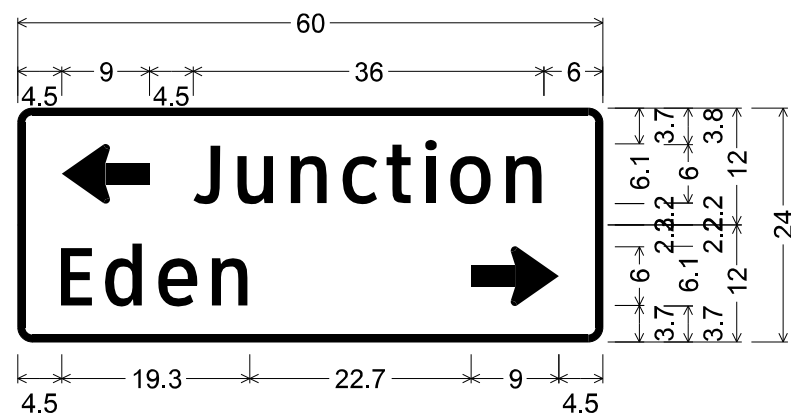
FM 2092 from SH 29 to end of State Maintenance											0636-6007	0644-6103	644-6104
Existing Location											REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1XP)	REPLACE SRS & S TY 10 BWG (1XT)
SIGN	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA	EA
1	30.916775	-99.818809	Menard	FM 2092	EB	R2-1	Speed Limit 55 MPH	30"x36"	Replace Sign & Support			1	
2	30.9189194	-99.79237568	Menard	FM 2092	EB	R2-1	Speed Limit 35 MPH	30"x36"	Replace Sign		7.5		
3	30.91875713	-99.79143935	Menard	FM 2092	EB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2		
3A	30.91875713	-99.79143935	Menard	FM 2092	EB	M1-4	US 83	24"x24"	Replace Sign	Yes	4		
4	30.91802181	-99.78884076	Menard	FM 2092	EB	R2-1	Speed Limit 30	30"x36"	Replace Sign		7.5		
5	30.91681496	-99.78380405	Menard	FM 2092	EB	SW16-7PL	Down Left Diagonal Arrow/Yellow	24"x12"	Replace Sign	Yes	2		
6	30.91652071	-99.78254238	Menard	FM 2092	EB	R2-1	Speed Limit 30 MPH	30"x36"	Replace Sign		7.5		
7	30.91667154	-99.77989243	Menard	FM 2092	EB	R2-1	Speed Limit 45 MPH	30"x36"	Replace Sign		7.5		
8	30.91814347	-99.7726367	Menard	FM 2092	EB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5		
9	30.91638483	-99.74379037	Menard	FM 2092	EB	W11-3	Deer crossing	36"x36"	Replace Sign		9		
10	30.91242021	-99.71214551	Menard	FM 2092	EB	W8-18	Road May Flood	36"x36"	Replace Sign		9		
11	30.91290406	-99.70892628	Menard	FM 2092	EB	W8-19aTP	Flood Gauge	18"x12"	Replace Sign	Yes	1.5		
11A	30.91290406	-99.70892628	Menard	FM 2092	EB	W8-19	Flood Gauge Ruler	12"x72"	Replace Sign	Yes	6		
12	30.90990379	-99.6869255	Menard	FM 2092	EB	M1-6F	FM 2092	24"x24"	Replace Sign	Yes	4		
13	30.89254836	-99.64113692	Menard	FM 2092	EB	W1-2R	Curve Symbol	36"x36"	Replace Sign		9		
14	30.87617215	-99.62456258	Menard	FM 2092	EB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
15	30.8751599	-99.62457935	Menard	FM 2092	EB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2		
15A	30.8751599	-99.62457935	Menard	FM 2092	EB	M1-6T	TX 29	24"x24"	Replace Sign	Yes	4		
16	30.87340664	-99.62461212	Menard	FM 2092	EB	W3-1	Stop Ahead	36"x36"	Replace Sign & Support			1	
17	30.87033066	-99.62473233	Menard	FM 2092	EB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1	
18	30.87293761	-99.62446476	Menard	FM 2092	WB	R2-1	Speed Limit 60 MPH	30"x36"	Replace Sign		7.5		
19	30.88815818	-99.63483068	Menard	FM 2092	WB	W1-2L	Curve Symbol	36"x36"	Replace Sign		9		
20	30.892665	-99.63775	Menard	FM 2092	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
21	30.91227671	-99.71405268	Menard	FM 2092	WB	W11-3	Deer crossing	36"x36"	Replace Sign		9		
22	30.91824129	-99.77333786	Menard	FM 2092	WB	W1-2L	Curve Symbol	36"x36"	Replace Sign		9		
23	30.91809822	-99.77584505	Menard	FM 2092	WB	R2-1	Speed Limit 45 MPH	30"x36"	Replace Sign		7.5		
24	30.91684199	-99.77981888	Menard	FM 2092	WB	R2-1	Speed Limit 30 MPH	30"x36"	Replace Sign		7.5		
25	30.91713868	-99.78417669	Menard	FM 2092	WB	D1-2	Junction Left Arrow Eden Right Arrow	60"x24"	Replace Sign & Support				1
26	30.91720364	-99.7844717	Menard	FM 2092	WB	R2-1	Speed Limit 30 MPH	30"x36"	Replace Sign		7.5		
27	30.91758891	-99.7860142	Menard	FM 2092	WB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2		
28	30.91778508	-99.78685281	Menard	FM 2092	WB	R2-1	Speed Limit 30 MPH	30"x36"	Replace Sign		7.5		
29	30.91821676	-99.78874106	Menard	FM 2092	WB	R2-1	Speed Limit 35 MPH	30"x36"	Replace Sign		7.5		
30	30.91901268	-99.79205692	Menard	FM 2092	WB	R2-1	Speed Limit 50 MPH	30"x36"	Replace Sign & Support			1	
31	30.91771902	-99.79856455	Menard	FM 2092	WB	R2-1	Speed Limit 55 MPH	30"x36"	Replace Sign & Support			1	
32	30.91689676	-99.81854388	Menard	FM 2092	WB	TX1-1T	State Maintenance Ends	24"x15"	Replace Sign	Yes	2.5		1
TOTAL											192.1	5	1



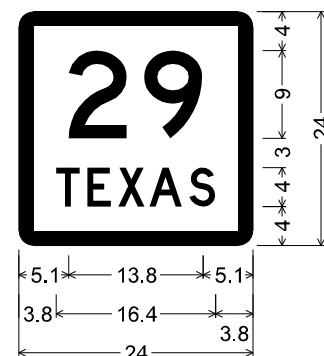
M1-4 24x24;
1.5" Radius, No border, Black;



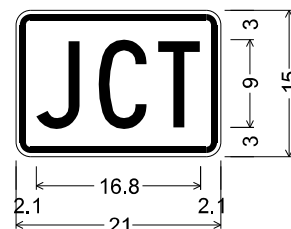
M1-6F_24x24;
1.5" Radius, No border, Black;
FARM, 2D;
ROAD, 2D;
2092, 4D;



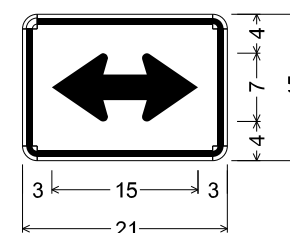
D1-2 6in LT-RT;
1.5" Radius, 0.8" Border, White on, Green;
Standard Arrow Custom 9.0" X 6.1" 180';
"Junction", ClearviewHwy-3-W;
1.5" Radius, 0.8" Border, White on, Green;
"Eden", ClearviewHwy-3-W;
Standard Arrow Custom 9.0" X 6.1" 0';



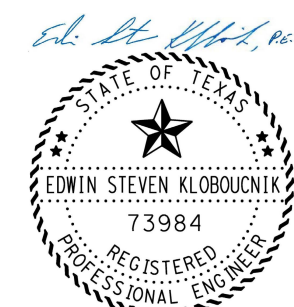
M1-6T-2_24x24;
1.5" Radius, 1.5" Border, Black on, White;
"29", D;
"TEXAS", D;



M2-1_21x15;
1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
"JCT", C;

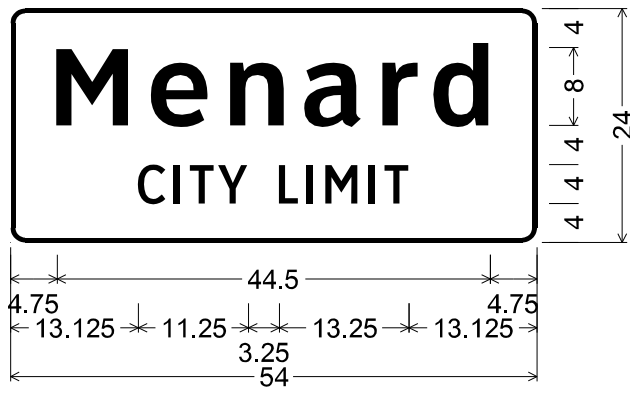


M6-4_21x15;
1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
Arrow M6-4;

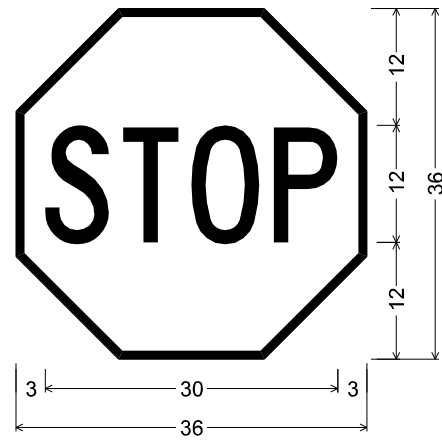


23.06.2021

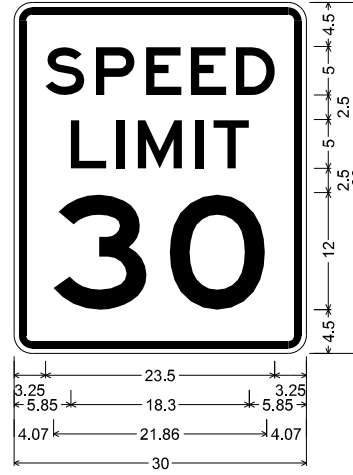
		San Angelo District	
REPLACE SIGNS FM 2092 MENARD COUNTY			
SHEET 1 OF 3		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383	SECT 49	JOB 001 HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 57



1.500" Radius, 0.500" Border, White on, Green;
 "Menard", ClearviewHwy-5-W-R;
 "CITY LIMIT", ClearviewHwy-3-W;



R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



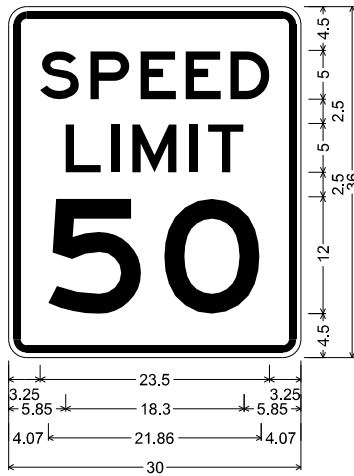
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "30", E specified length;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "35", E specified length;



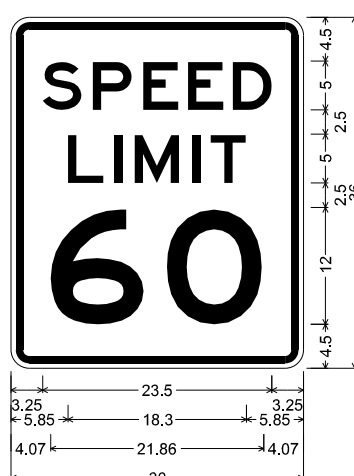
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "45", E specified length;



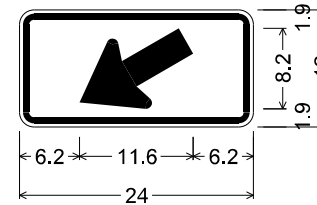
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "50", E specified length;



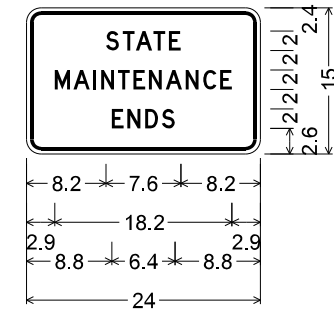
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "55", E specified length;



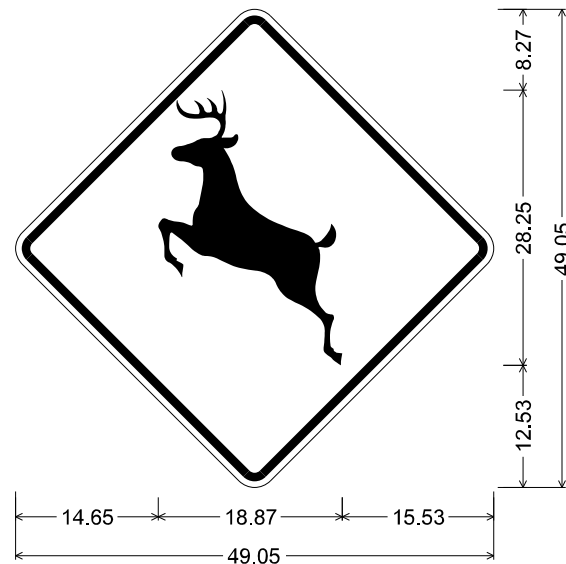
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "60", E specified length;



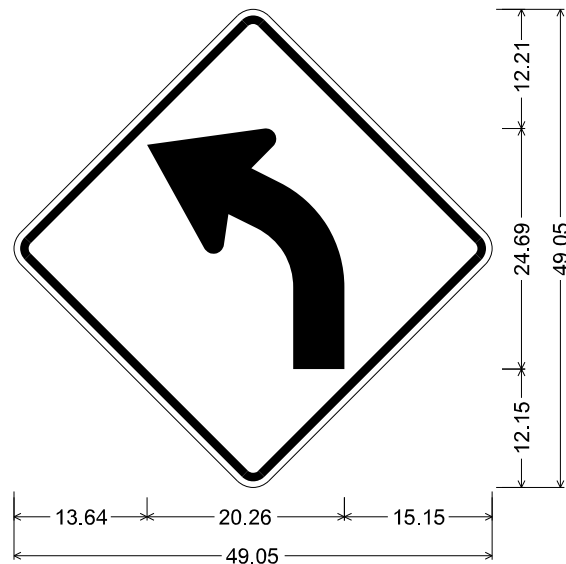
SW16-7PL_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, Bright yellow green;
 Standard Arrow Custom 12.6" X 8.1" 210';



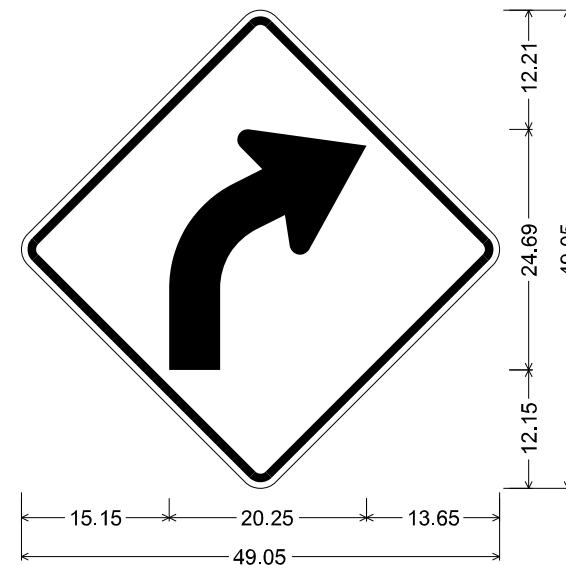
TX1-1T_24x15 ;
 1.5" Radius, 0.4" Border, 0.4" Indent, Black on, White;
 "STATE", D;
 "MAINTENANCE", D;
 "ENDS", D;



W11-3_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W11-3;



W1-2L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;

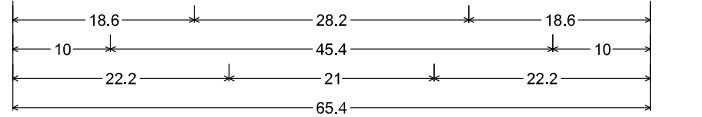


W1-2R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;

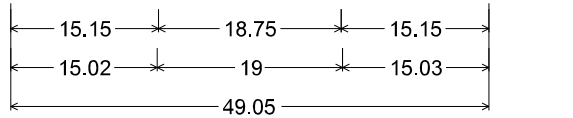
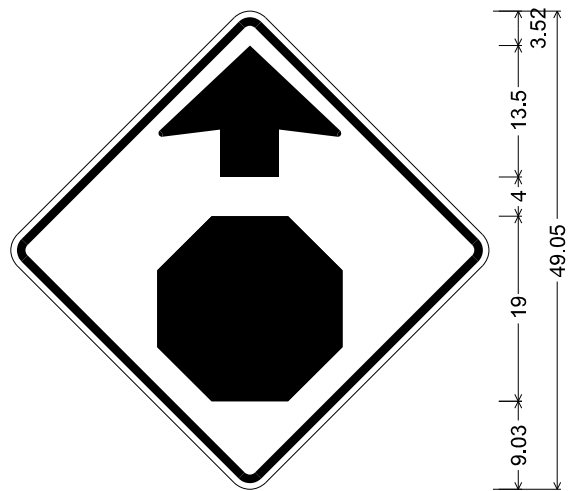


23.06.2021

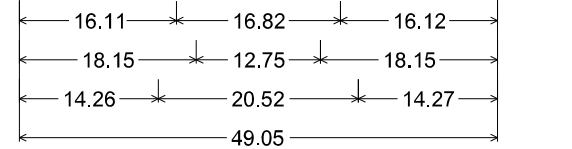
		San Angelo District	
REPLACE SIGNS FM 2092 MENARD COUNTY			
SHEET 2 OF 3		NOT TO SCALE	
CTXDOT 2021 <small>SHEET ISSUED OR LAST REVISED</small>	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 58



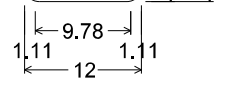
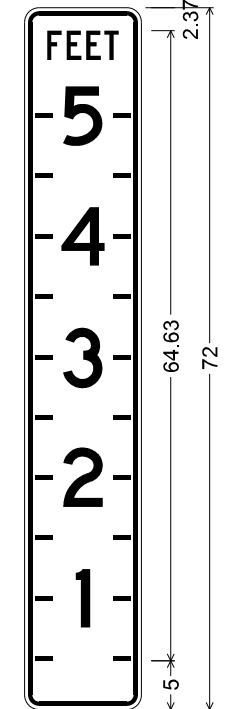
W2-1aT(1)_48x48;
48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
"HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



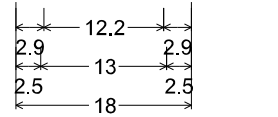
W3-1_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
Down Arrow Custom - 13.50" 90";
Symbol R1-1;



W8-18_36x36;
36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
"ROAD", C; "MAY", C; "FLOOD", C;



W8-19_12x72;
1.50" Radius, 0.44" Border, 0.38" Indent, Black on, Yellow;
Symbol W8-19;



W8-19aTP_18x12;
1.5" Radius, 0.4" Border, 0.4" Indent, Black on, Yellow;
"FLOOD", D;
"GAUGE", D;



23.06.2021



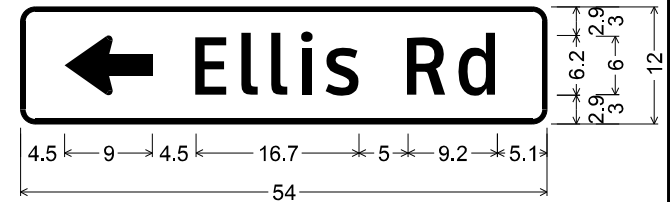
REPLACE SIGNS
FM 2092
MENARD COUNTY

SHEET 3 OF 3 NOT TO SCALE

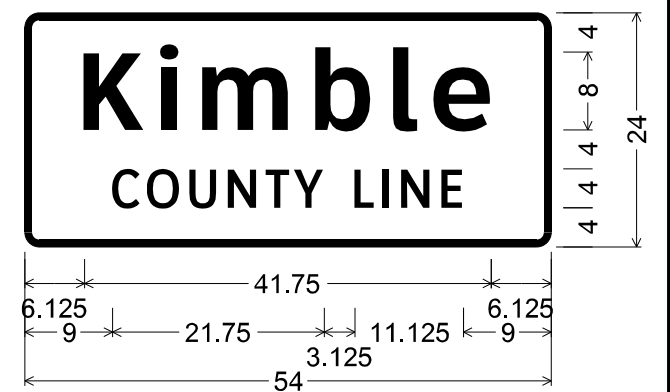
©TxDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83,ETC
12-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	59	

RM 2291 from US 83 to Kimble County Line
Existing Location

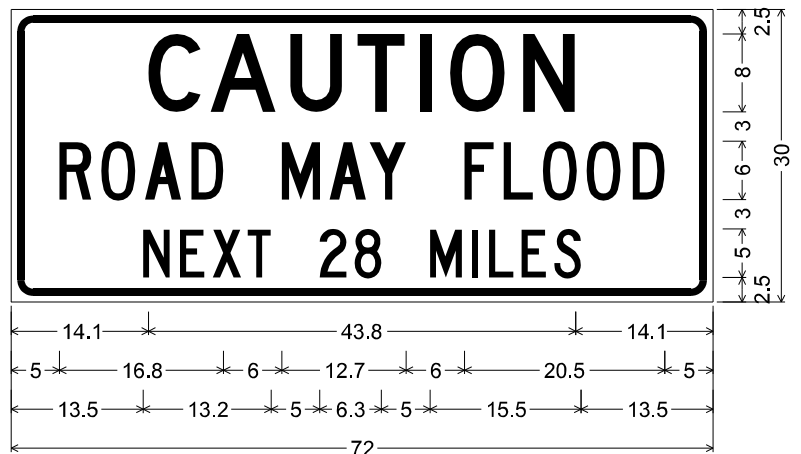
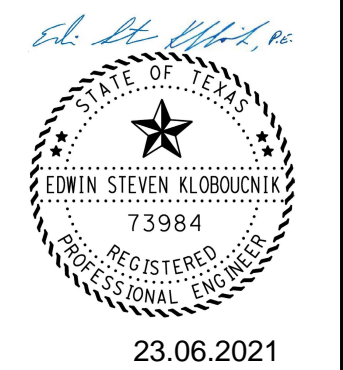
	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	0636-6007 REPLACE EXISTING ALUMINUM SIGNS (TY A)	0644-6103 REPLACE SRS & S TY 10 BWG (1XP)	644-6104 REPLACE SRS & S TY 10 BWG (1XT)	644-6111 REPLACE SRS & S TY 580 (1)(U-WC)
Sign											SF	EA	EA	EA
1	30.90691815	-99.78846311	Menard	RM 2291	SB	R6-1R	One Way	54"x18"	Replace Sign & Support				1	
2	30.90662252	-99.7887634	Menard	RM 2291	SB	R2-1	Speed Limit 45	30"x36"	Replace Sign		7.5			
3	30.9064718	-99.78889991	Menard	RM 2291	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2			
3A	30.9064718	-99.78889991	Menard	RM 2291	SB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
4	30.90559192	-99.78978454	Menard	RM 2291	SB	R2-1	Speed Limit 55	30"x36"	Replace Sign		7.5			
5	30.90481739	-99.79056993	Menard	RM 2291	SB	W1-5R	Curve Symbol	36"x36"	Replace Sign		9			
6	30.90428785	-99.79112446	Menard	RM 2291	SB	NA	Caution Road May Flood Next 28 Miles	72"x30"	Replace Sign		15			
7	30.90249478	-99.79330918	Menard	RM 2291	SB	W11-3	Deer crossing	36"x36"	Replace Sign		9			
8	30.87732244	-99.79916975	Menard	RM 2291	SB	W1-2R	Curve Symbol	36"x36"	Replace Sign		9			
9	30.87069063	-99.82126753	Menard	RM 2291	SB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
9A	30.87069063	-99.82126753	Menard	RM 2291	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21			
9B	30.87069063	-99.82126753	Menard	RM 2291	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21			
10	30.86969621	-99.82270131	Menard	RM 2291	SB	W8-18	Road May Flood	36"x36"	Replace Sign		9			
11	30.846128	-99.832267	Menard	RM 2291	SB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
12	30.74390321	-99.83447989	Menard	RM 2291	SB	D21-1TaL	Norma Spiller Ln Left Arrow	54"x24"	Replace Sign & Support				1	
13	30.71825187	-99.85714591	Menard	RM 2291	SB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
14	30.71072486	-99.86080193	Menard	RM 2291	SB	I-2dT	Kimble County Line	54"x24"	Replace Sign & Support				1	
15	30.71136102	-99.86008165	Menard	RM 2291	NB	R2-1	Speed Limit 60	Good	Replace Sign		7.5			
16	30.74020128	-99.83776934	Menard	RM 2291	NB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
17	30.74148616	-99.83595633	Menard	RM 2291	NB	D21-1TaR	Norma Spiller Ln Right Arrow	66"x24"	Replace Sign & Support				1	
18	30.742557	-99.83505	Menard	RM 2291	NB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
19	30.78117593	-99.82379321	Menard	RM 2291	NB	D14-4T	Adopt a Highway Lost Owl Ranch Next 2 Miles	48"x48"	Replace Sign		16			
20	30.79496582	-99.82406743	Menard	RM 2291	NB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
20A	30.79496582	-99.82406743	Menard	RM 2291	NB	D10-7aT	TRM 406	3"x10"	Replace Sign	Yes	0.21			
20B	30.79496582	-99.82406743	Menard	RM 2291	NB	D10-7aT	TRM 406	3"x10"	Replace Sign	Yes	0.21			
21	30.8444494	-99.83291156	Menard	RM 2291	NB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
21A	30.8444494	-99.83291156	Menard	RM 2291	NB	D10-7aT	TRM 402	3"x10"	Replace Sign	Yes	0.21			
21B	30.8444494	-99.83291156	Menard	RM 2291	NB	D10-7aT	TRM 402	3"x10"	Replace Sign	Yes	0.21			
22	30.84501358	-99.83261567	Menard	RM 2291	NB	D21-1TL	Ellis Rd Left Arrow	54"x12"	Replace Sign		4.5			
23	30.86522968	-99.82701854	Menard	RM 2291	NB	W8-18	Road May Flood	36"x36"	Replace Sign		9			
24	30.86942897	-99.82284637	Menard	RM 2291	NB	W1-2R	Curve Advisory	36"x36"	Replace Sign		9			
25	30.87180628	-99.80608593	Menard	RM 2291	NB	W1-2L	Curve Advisory	36"x36"	Replace Sign		9			
26	30.88287283	-99.79898112	Menard	RM 2291	NB	M1-6R	RM 2291	24"x24"	Replace Sign	Yes	4			
27	30.89482596	-99.7988629	Menard	RM 2291	NB	W1-5R	Curve Advisory	36"x36"	Replace Sign		9			
28	30.90246383	-99.79307617	Menard	RM 2291	NB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16			
29	30.90288722	-99.79243161	Menard	RM 2291	NB	M1-4	US 83	24"x24"	Replace Sign	Yes	4			
30	30.90340465	-99.79180818	Menard	RM 2291	NB	R2-1	Speed Limit 55	30"x36"	Replace Sign		7.5			
31	30.90441424	-99.79079258	Menard	RM 2291	NB	W3-1	Stop sign Ahead	36"x36"	Replace Sign & Support			1		
32	30.90515036	-99.79004247	Menard	RM 2291	NB	D1-3	Eden Left Junction Right Mason Right	78"x42"	Replace Sign & Support				1	
33	30.90552117	-99.78967233	Menard	RM 2291	NB	R2-1	Speed Limit 45	30"x36"	Replace Sign		7.5			
34	30.90669734	-99.78848772	Menard	RM 2291	NB	R1-2	Yield Sign	48"x48"x48"	Replace Sign & Support				1	
35	30.90655775	-99.78833142	Menard	RM 2291	NB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1		
TOTAL											214.26	2	5	1



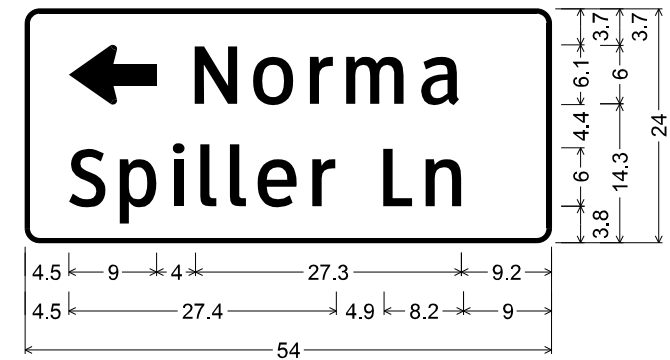
D21-1TL_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
Standard Arrow Custom 9.0" X 6.1" 180°;
"Ellis Rd", ClearviewHwy-3-W;



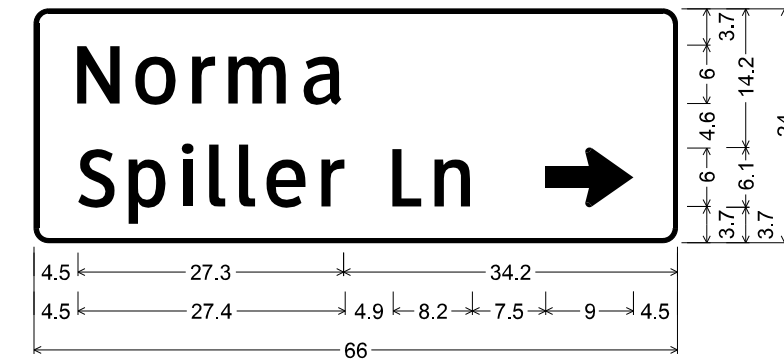
I-2d 8in;
1.500" Radius, 0.750" Border, White on, Green;
"Kimble", ClearviewHwy-5-W-R;
"COUNTY LINE", ClearviewHwy-3-W;



2.3" Radius, 0.8" Border, 0.6" Indent, Black on, Yellow;
"CAUTION", D; "ROAD MAY FLOOD", C;
"NEXT 28 MILES", C;

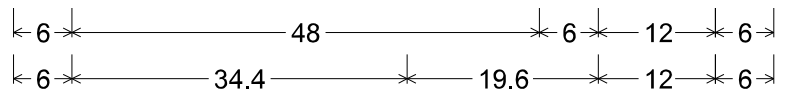
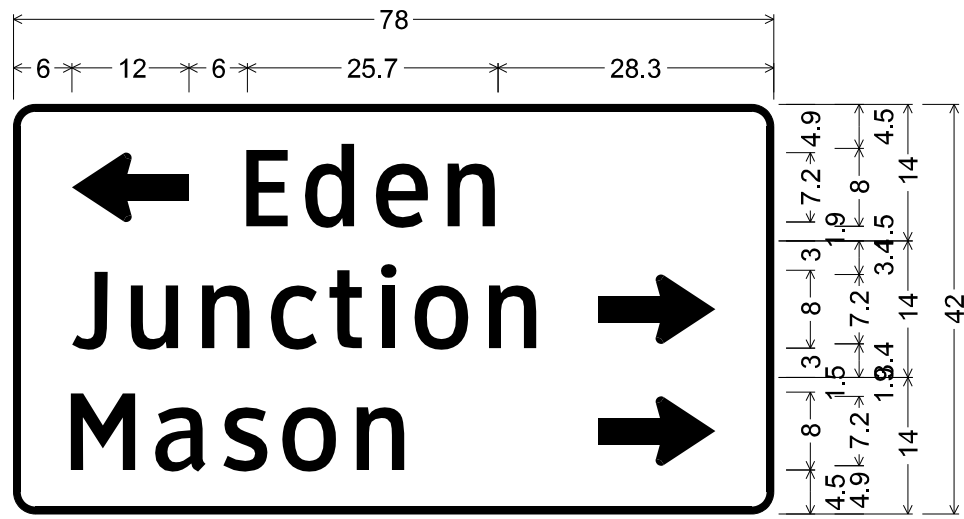


D21-1aTL_VARx24;
1.5" Radius, 0.5" Border, White on, Green;
Standard Arrow Custom 9.0" X 6.1" 180°;
"Norma", ClearviewHwy-3-W;
"Spiller Ln", ClearviewHwy-3-W;

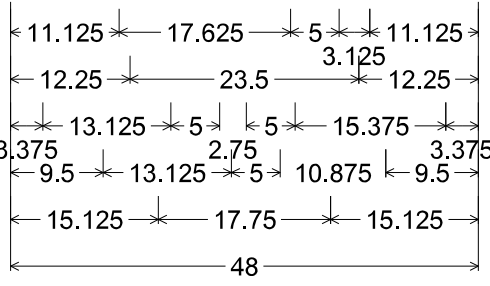


D21-1aTR_VARx24;
1.5" Radius, 0.5" Border, White on, Green;
"Norma", ClearviewHwy-3-W;
"Spiller Ln", ClearviewHwy-3-W;
Standard Arrow Custom 9.0" X 6.1" 0°;

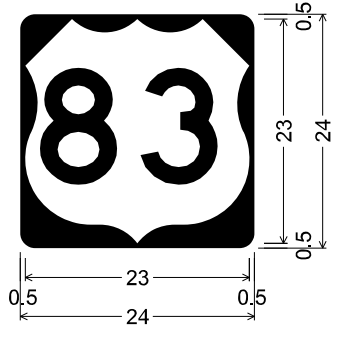
Texas Department of Transportation		San Angelo District	
REPLACE SIGNS RM 2291 MENARD COUNTY			
SHEET 1 OF 4		NOT TO SCALE	
©TXDOT 2021	CONT 6383	SECT 49	JOB 001
SHEET ISSUED OR LAST REVISED 12-19		DIST SJT	COUNTY MENARD
		HIGHWAY US 83,ETC	
		SHEET NO. 60	



D1-3 8in LT-RT-RT;
 2.3" Radius, 0.8" Border, White on, Green;
 Standard Arrow Custom 12.0" X 7.1" 180';
 "Eden", ClearviewHwy-3-W;
 2.3" Radius, 0.8" Border, White on, Green;
 "Junction", ClearviewHwy-3-W;
 Standard Arrow Custom 12.0" X 7.1" 0';
 2.3" Radius, 0.8" Border, White on, Green;
 "Mason", ClearviewHwy-3-W;
 Standard Arrow Custom 12.0" X 7.1" 0';



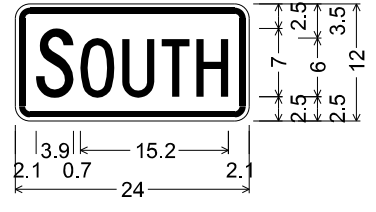
D14-4-3_48X48;
 3.000" Radius, 1.000" Border, White on, Blue;
 "ADOPT A", C; "HIGHWAY", C;
 "NEXT 2 MILES", C;
 "LOST OWL", C; "RANCH", C;



M1-4_24x24;
 1.5" Radius, No border, Black;



M1-6R_24x24;
 1.5" Radius, No border, Black;
 RANCH, 2D;
 ROAD, 2D;
 2291, 4D;



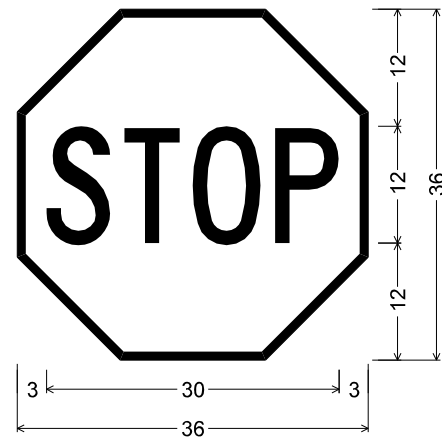
M3-3_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "SOUTH", C 65% spacing;



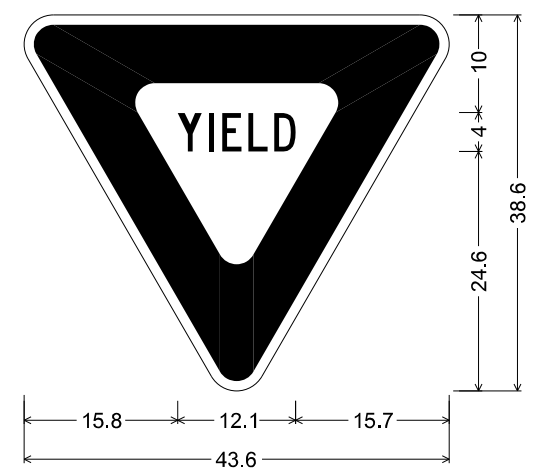
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "55", E specified length;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "60", E specified length;



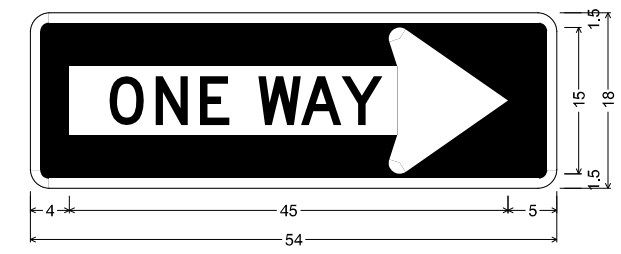
R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



R1-2_48x48;
 48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
 "YIELD", C specified length;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "45", E specified length;

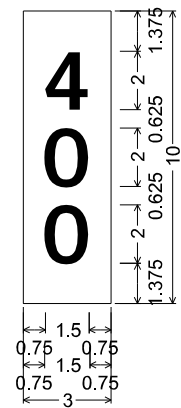


R6-1R_54x18;
 1.88" Radius, 1.00" Border, White on, Black;
 One Way;

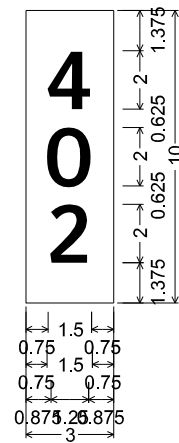


23.06.2021

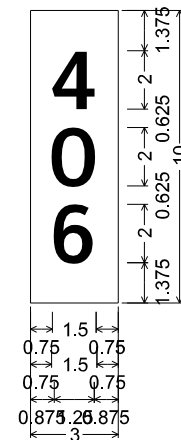
		San Angelo District	
REPLACE SIGNS RM 2291 MENARD COUNTY			
SHEET 2 OF 4		NOT TO SCALE	
TXDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 61



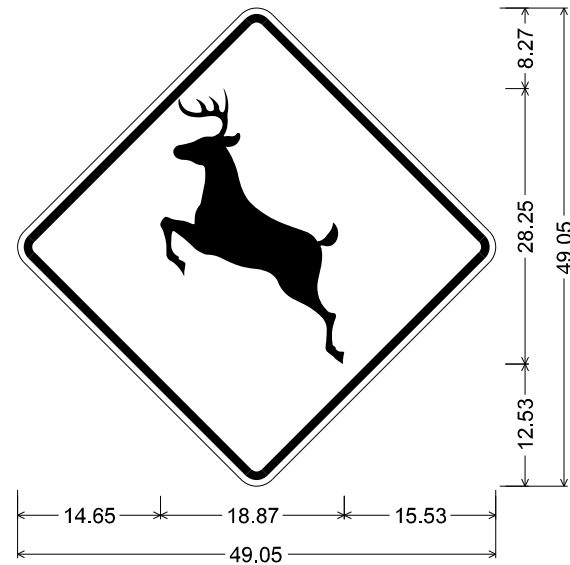
No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



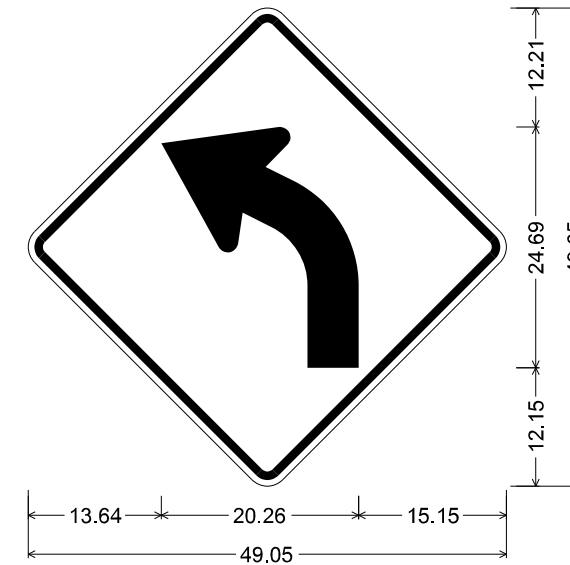
No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;



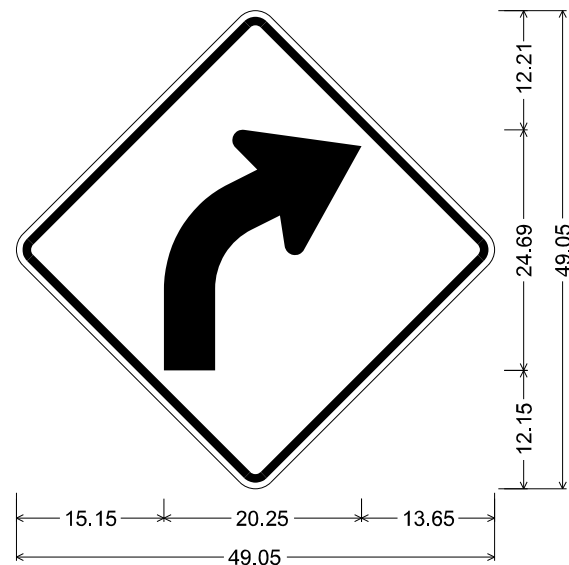
No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "6", ClearviewHwy-4-W;



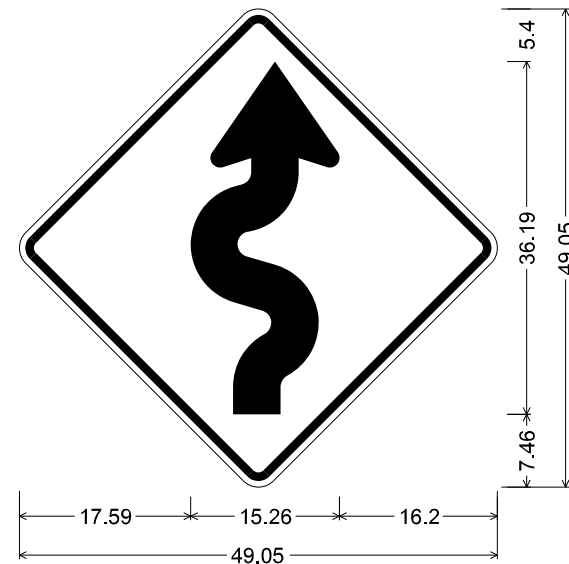
W11-3_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W11-3;



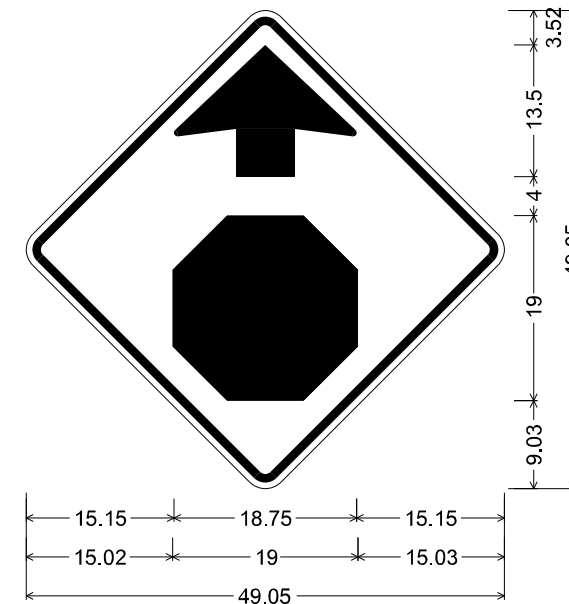
W1-2L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;



W1-2R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;



W1-5R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-5;

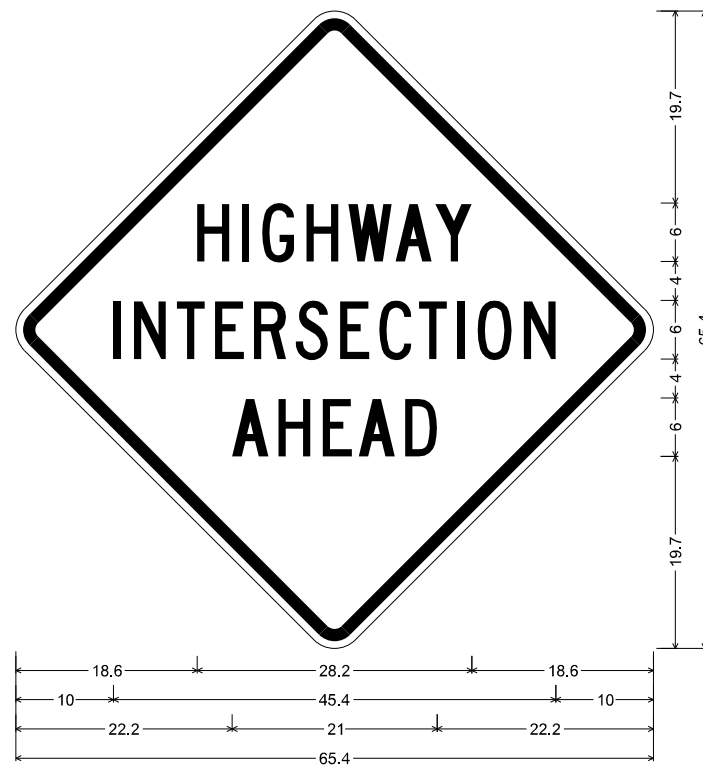


W3-1_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Down Arrow Custom - 13.50" 90°;
 Symbol R1-1;



23.06.2021

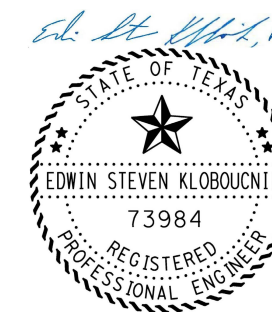
		San Angelo District	
REPLACE SIGNS RM 2291 MENARD COUNTY			
SHEET 3 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
			HIGHWAY US 83,ETC SHEET NO. 62



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



W8-18_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 "ROAD", C; "MAY", C; "FLOOD", C;

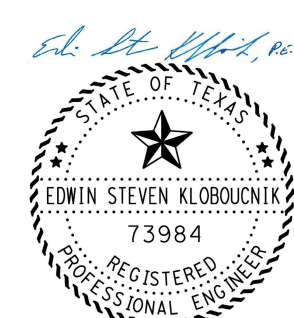


23.06.2021

		San Angelo District	
REPLACE SIGNS RM 2291 MENARD COUNTY			
SHEET 4 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
			HIGHWAY US 83,ETC SHEET NO. 63

DATE: 6/23/2021 10:41:57 AM
 FILE: pw:\xtdot\project\wisconsin\DOT2\Documents\07 - SJT\Maintenance\Projects\638349001\2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT US 87 GLASSCOCK COUNTY.dgn

US 87 from Howard County Line to Sterling County Line													0636-6007	0644-6103	644-6104
Existing Location													REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA	EA		
1	31.98859292	-101.26505	Glasscock	US 87	NB	I-2dT	Glasscock County Line	72"x24"	Replace Sign & Support	Yes			1		
2	31.98980461	-101.2666438	Glasscock	US 87	NB	M1-4	US 87	24"x24"	Replace Sign	Yes	4				
3	31.99983884	-101.2797649	Glasscock	US 87	NB	W11-10L	Truck Crossing	36"x36"	Replace Sign & Support			1			
4	32.01371875	-101.2979867	Glasscock	US 87	NB	I-3	Cannibal Draw	42"x18"	Replace Sign		5.25				
5	32.01915894	-101.3051536	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
6	32.02626651	-101.3148494	Glasscock	US 87	NB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
6A	32.02626651	-101.3148494	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
7	32.02859787	-101.3175439	Glasscock	US 87	NB	M1-4	US 87	24"x24"	Replace Sign	Yes	4				
8	32.03471498	-101.3255889	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
8A	32.03471498	-101.3255889	Glasscock	US 87	NB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
9	32.03922738	-101.331491	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
10	32.04204487	-101.3352165	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
11	32.04499674	-101.3390997	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
12	32.04532529	-101.3395277	Glasscock	US 87	NB	W11-10L	Truck Crossing	36"x36"	Replace Sign & Support			1			
13	32.04724347	-101.3420374	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
13A	32.04724347	-101.3420374	Glasscock	US 87	NB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
14	32.05159652	-101.3477591	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
15	32.05175028	-101.3479838	Glasscock	US 87	NB	R6-1R	One Way	54"x18"	Replace Sign		6.75				
16	32.05491236	-101.3525028	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
17	32.06020076	-101.3599282	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
17A	32.06020076	-101.3599282	Glasscock	US 87	NB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
19	32.06199058	-101.3626376	Glasscock	US 87	NB	D21-1TL	Overton Rd Left	72"x12"	Replace Sign		6				
20	32.06314583	-101.3642793	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
21	32.0650365	-101.3664826	Glasscock	US 87	NB	D21-1TR	Flowers Rd Right	72"x12"	Replace Sign		6				
22	32.06639265	-101.368072	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
23	32.06639265	-101.368072	Glasscock	US 87	NB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
24	32.06848465	-101.370517	Glasscock	US 87	NB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
25	32.07481363	-101.3779251	Glasscock	US 87	NB	D7-6aTL	Historical Marker 1 Mile 12671	48"x48"	Replace Sign		16				
26	32.0824215	-101.3868231	Glasscock	US 87	NB	I-3	North Concho Draw	60"x18"	Replace Sign		7.5				
27	32.08339066	-101.388076	Glasscock	US 87	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
27A	32.08339066	-101.388076	Glasscock	US 87	NB	M1-6F	FM 821	24"x24"	Replace Sign	Yes	4				
28	32.08580909	-101.3912019	Glasscock	US 87	NB	M3-2	EAST	24"x12"	Replace Sign	Yes	2				
28A	32.08580909	-101.3912019	Glasscock	US 87	NB	M1-6F	FM 821	24"x24"	Replace Sign	Yes	4				
28B	32.08580909	-101.3912019	Glasscock	US 87	NB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2				
29	32.08658465	-101.3921087	Glasscock	US 87	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2				
29A	32.08658465	-101.3921087	Glasscock	US 87	NB	M1-4	US 87	24"x24"	Replace Sign	Yes	4				
30	32.08741591	-101.3930843	Glasscock	US 87	NB	I-2dT	Howard County Line	60"x24"	Replace Sign & Support	Yes			1		
31	32.08720806	-101.3933744	Glasscock	US 87	SB	I-2dT	Glasscock County Line	72"x24"	Replace Sign & Support	Yes			1		
32	32.08618918	-101.3921618	Glasscock	US 87	SB	M3-2	EAST	24"x12"	Replace Sign	Yes	2				
32A	32.08618918	-101.3921618	Glasscock	US 87	SB	M1-6F	FM 821	24"x24"	Replace Sign	Yes	4				
33B	32.08618918	-101.3921618	Glasscock	US 87	SB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2				
34	32.08537842	-101.3912064	Glasscock	US 87	SB	R4-7	Keep Right Symbol	24"x30"	Replace Sign & Support			1			
35	32.08516262	-101.3909553	Glasscock	US 87	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2				
35A	32.08516262	-101.3909553	Glasscock	US 87	SB	M1-4	US 87	24"x24"	Replace Sign	Yes	4				
38	32.08482739	-101.3905626	Glasscock	US 87	SB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign		9				
37	32.08235504	-101.3875537	Glasscock	US 87	SB	I-3	North Concho Draw	60"x18"	Replace Sign		7.5				
38	32.07786458	-101.3822771	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
38A	32.07786458	-101.3822771	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign		6.75				
39	32.07327194	-101.376905	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
39A	32.07327194	-101.376905	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
40	32.06808702	-101.3708385	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
41	32.0676386	-101.3703349	Glasscock	US 87	SB	D21-1TL	Flowers Rd Left	72"x12"	Replace Sign		6				
42	32.06620617	-101.3686165	Glasscock	US 87	SB	R5-1	Do Not Enter	36"x36"	Replace Sign		9				
43	32.06599127	-101.3683862	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign		6.75				
44	32.06273445	-101.3646204	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
44A	32.06273445	-101.3646204	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
44B	32.06273445	-101.3646204	Glasscock	US 87	SB	R1-1	Stop Sign	36"x36"	Replace Sign	Yes	9				
45	32.05982984	-101.3602733	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
45B	32.05982984	-101.3602733	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
46	32.05486783	-101.3528955	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
46B	32.05486783	-101.3528955	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
47	32.05148682	-101.3480853	Glasscock	US 87	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign	Yes	7				
48	32.04869944	-101.3447972	Glasscock	US 87	SB	W11-10L	Truck Crossing	36"x36"	Replace Sign & Support		9	1			
49	32.04755031	-101.3432829	Glasscock	US 87	SB	M1-4	US 87	24"x24"	Replace Sign	Yes	4				
49A	32.04755031	-101.3432829	Glasscock	US 87	SB	D10-7aT	TRM 396	3"x10"	Replace Sign	Yes	0.21				
49B	32.04755031	-101.3432829	Glasscock	US 87	SB	D10-7aT	TRM 396	3"x10"	Replace Sign	Yes	0.21				
50	32.04472616	-101.3395656	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
50A	32.04472616	-101.3395656	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
51	32.03447573	-101.3260945	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75				
51A	32.03447573	-101.3260945	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75				
52	32.02642128	-101.3151491	Glasscock	US 87	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign	Yes	7				
TOTAL											387.52	4	3		



23.06.2021

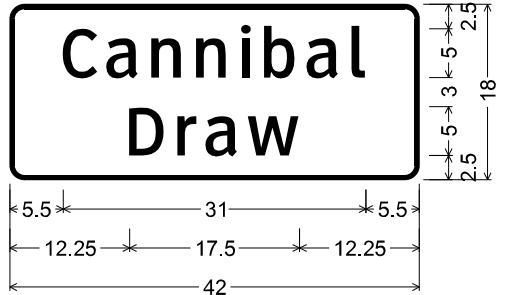
San Angelo District

REPLACE SIGNS US 87 GLASSCOCK COUNTY

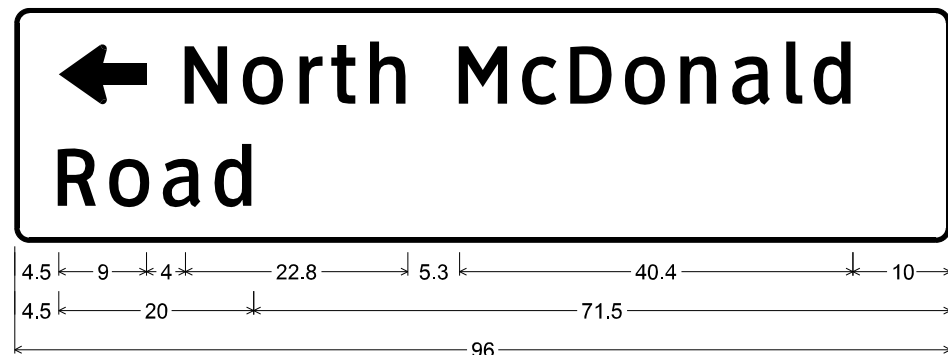
SHEET 1 OF 4
NOT TO SCALE

©TxDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83,ETC
12-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	64	

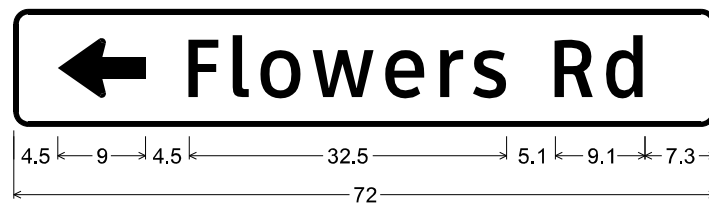
US 87 from Howard County Line to Sterling County Line Existing Location										0636-6007	0644-6103	644-6104	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1XP)	REPLACE SRS & S TY 10 BWG (1XT)
53	32.01899051	-101.3053926	Glasscock	US 87	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign	Yes	7		
54	32.01686686	-101.3030003	Glasscock	US 87	SB	I-3	Cannibal Draw	42"x18"	Replace Sign		5.25		
55	32.00888411	-101.2924631	Glasscock	US 87	SB	M1-4	US 87	24"x24"	Replace Sign	Yes	4		
55A	32.00888411	-101.2924631	Glasscock	US 87	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21		
55B	32.00888411	-101.2924631	Glasscock	US 87	SB	D10-7aT	TRM 400	3"x10"	Replace Sign	Yes	0.21		
56	32.00857732	-101.291709	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75		
56A	32.00857732	-101.291709	Glasscock	US 87	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign	Yes	7		
57	32.00310512	-101.284885	Glasscock	US 87	SB	W11-10L	Truck Crossing	36"x36"	Replace Sign & Support			1	
58	32.00249384	-101.2840934	Glasscock	US 87	SB	D21-1aTL	North Mc Donald Rd Left	96"x24"	Replace Sign & Support				1
59	32.00153602	-101.2824552	Glasscock	US 87	SB	R1-2	Yield Sign	48"x48"x48"	Replace Sign	Yes	7		
60	31.99200898	-101.2703104	Glasscock	US 87	SB	R6-1L	One Way	54"x18"	Replace Sign	Yes	6.75		
60A	31.99200898	-101.2703104	Glasscock	US 87	SB	R6-1R	One Way	54"x18"	Replace Sign	Yes	6.75		
61	31.98788901	-101.2649769	Glasscock	US 87	SB	D10-7aT	TRM 404	3"x10"	Replace Sign	Yes	0.21		
61A	31.98788901	-101.2649769	Glasscock	US 87	SB	D10-7aT	TRM 404	3"x10"	Replace Sign	Yes	0.21		
TOTAL											51.34	1	1



I-3 5in;
1.500" Radius, 0.500" Border, White on, Green;
"Cannibal", ClearviewHwy-3-W;
"Draw", ClearviewHwy-3-W;



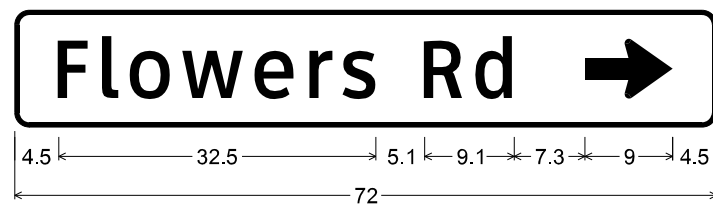
D21-1aTL_VARx24;
1.5" Radius, 0.5" Border, White on, Green;
Standard Arrow Custom 9.0" X 6.1" 180"; "North McDonald", ClearviewHwy-3-W;
"Road", ClearviewHwy-3-W;



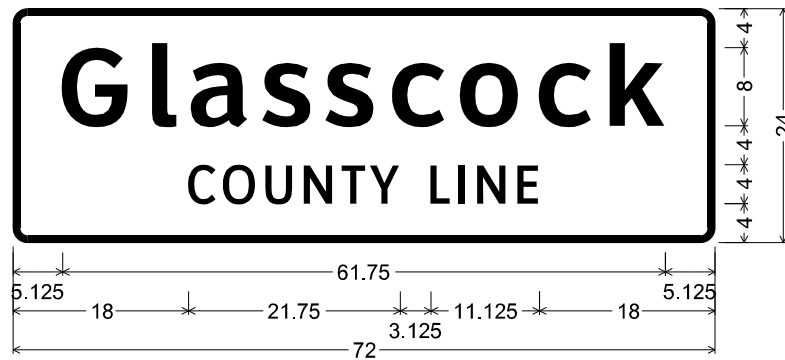
D21-1TL_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
Standard Arrow Custom 9.0" X 6.1" 180';
"Flowers Rd", ClearviewHwy-3-W;



D21-1TL_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
Standard Arrow Custom 9.0" X 6.1" 180';
"Overton Rd", ClearviewHwy-3-W;



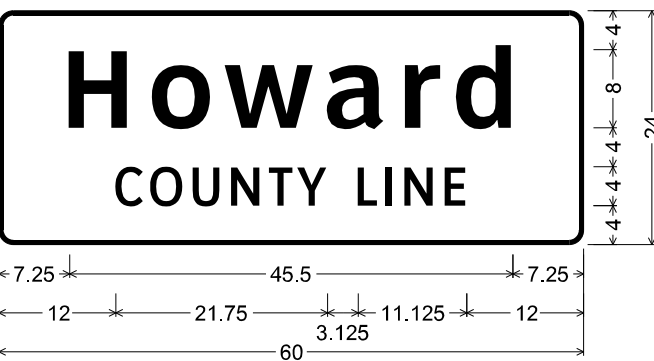
D21-1TR_VARx12;
1.5" Radius, 0.5" Border, White on, Green;
"Flowers Rd", ClearviewHwy-3-W;
Standard Arrow Custom 9.0" X 6.1" 0';



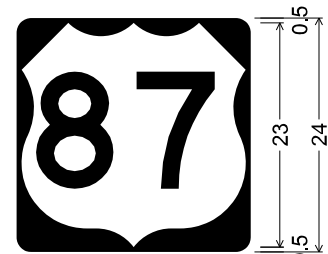
I-2d 8in;
1.500" Radius, 0.750" Border, White on, Green;
"Glasscock", ClearviewHwy-5-W-R;
"COUNTY LINE", ClearviewHwy-3-W;



3.000" Radius, 1.000" Border, White on, Brown;
"HISTORICAL", C;
"MARKER", C; "1 MILE", C;
"ON LEFT", C; "12671", C;



I-2d 8in;
1.500" Radius, 0.500" Border, White on, Green;
"Howard", ClearviewHwy-5-W-R;
"COUNTY LINE", ClearviewHwy-3-W;



M1-4_24x24;
1.5" Radius, No border, Black;



M1-6F_24x24;
1.5" Radius, No border, Black;
FARM, 2D;
ROAD, 2D;
821, 5D;

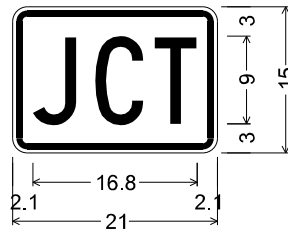


23.06.2021

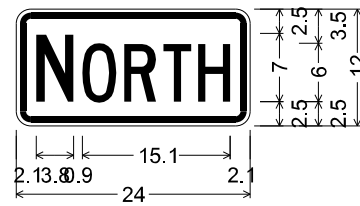
Texas Department of Transportation		San Angelo District	
REPLACE SIGNS US 87 GLASSCOCK COUNTY			
SHEET 2 OF 4		NOT TO SCALE	
CTXDDT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383 SECT 49 DIST SJT	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 65

DATE: 6/23/2021 10:42:09 AM
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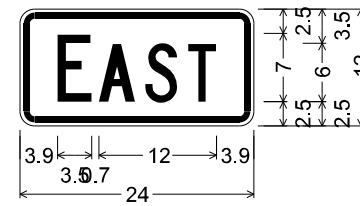
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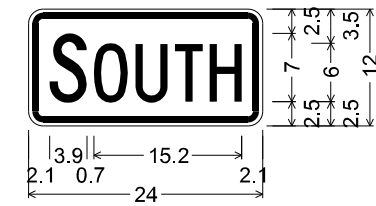
M2-1_21x15;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "JCT", C;



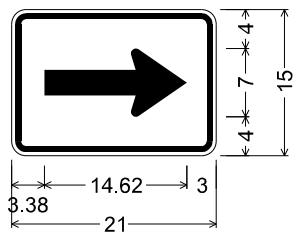
M3-1_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "NORTH", C 65% spacing;



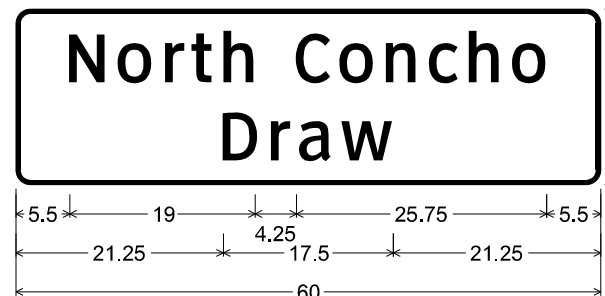
M3-2_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "EAST", C;



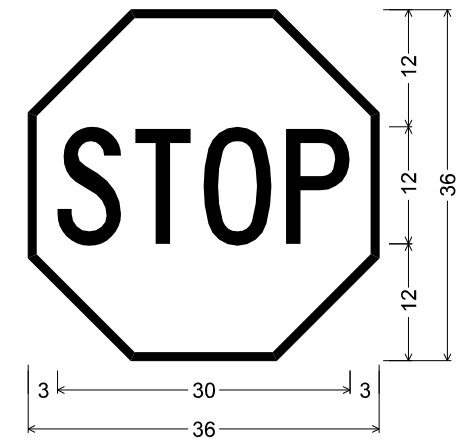
M3-3_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "SOUTH", C 65% spacing;



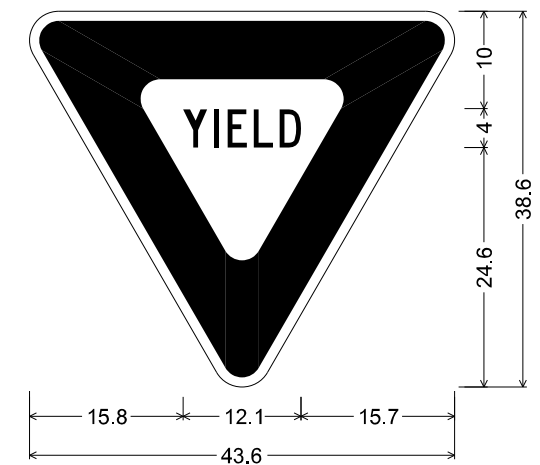
M6-1_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-1;



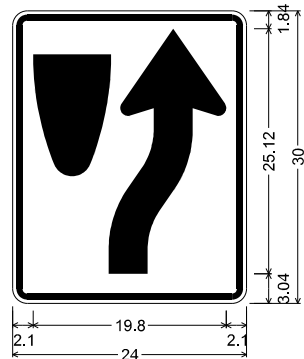
I-3 5in;
 1.500" Radius, 0.500" Border, White on, Green;
 "North Concho", ClearviewHwy-3-W;
 "Draw", ClearviewHwy-3-W;



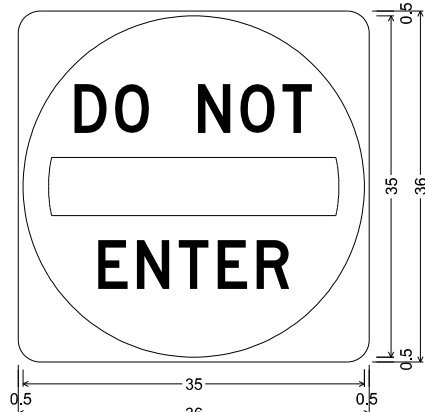
R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



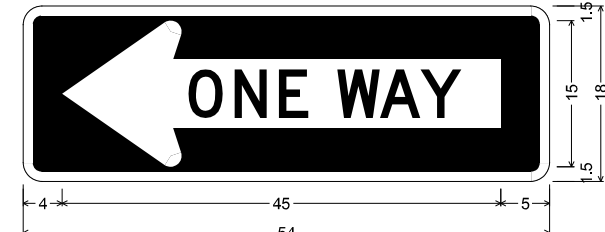
R1-2_48x48;
 48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
 "YIELD", C specified length;



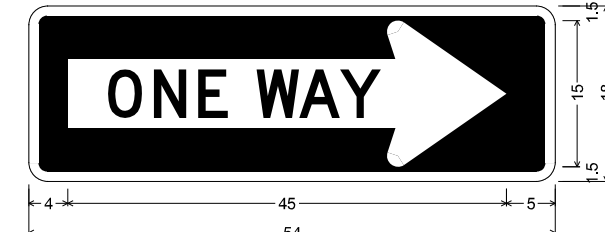
R4-7_24x30;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;



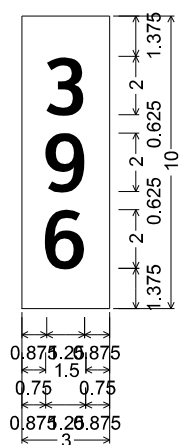
R5-1_36x36;
 2.25" Radius, No border, White;



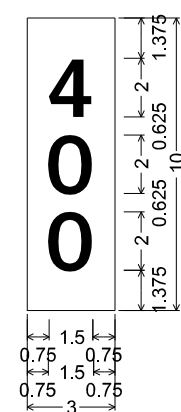
R6-1L_54x18;
 1.88" Radius, 1.00" Border, White on, Black;
 One Way;



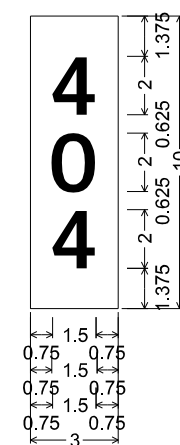
R6-1R_54x18;
 1.88" Radius, 1.00" Border, White on, Black;
 One Way;



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "9", ClearviewHwy-4-W;
 "6", ClearviewHwy-4-W;



No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



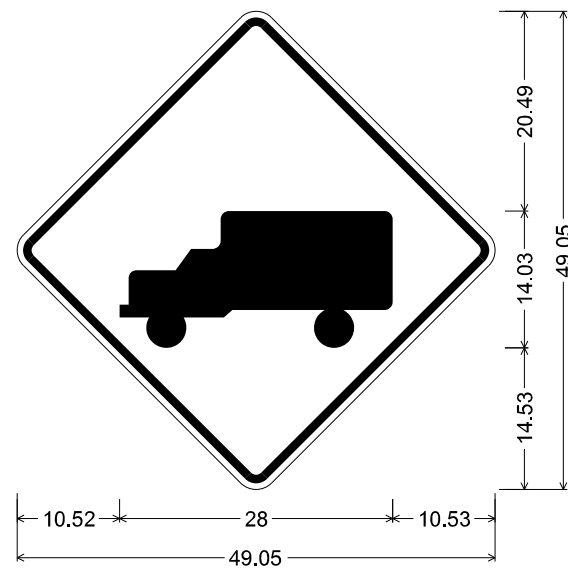
No border, White on, Green;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;



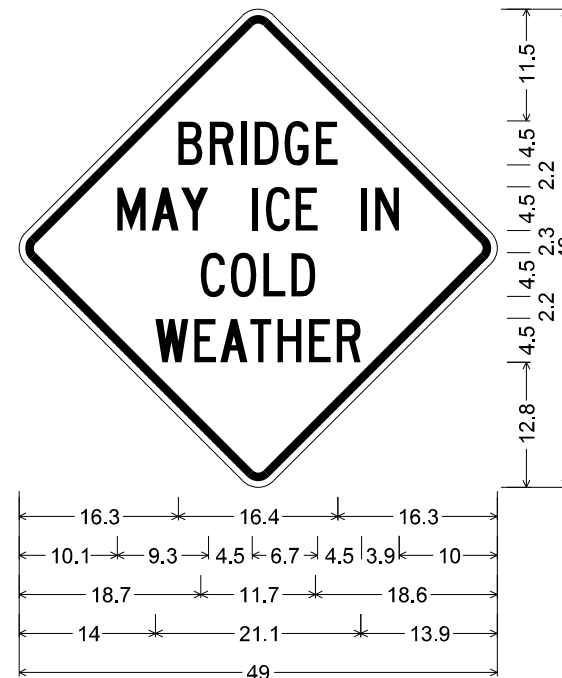
23.06.2021

		San Angelo District	
REPLACE SIGNS US 87 GLASSCOCK COUNTY			
SHEET 3 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83.ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 66

DATE: 6/23/2021 10:42:32 AM
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W11-10L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W11-10;



W8-13aT_36x36;
 36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
 "BRIDGE", C 80% spacing;
 "MAY ICE IN", C 80% spacing;
 "COLD", C 80% spacing;
 "WEATHER", C 80% spacing;

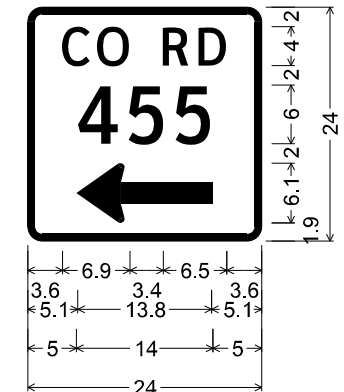


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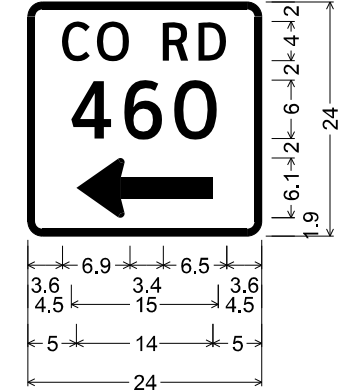
		San Angelo District	
REPLACE SIGNS US 87 GLASSCOCK COUNTY			
SHEET 4 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 67
12-19	SJT	COUNTY MENARD	SHEET NO. 67

DATE: 6/23/2021 10:42:39 AM
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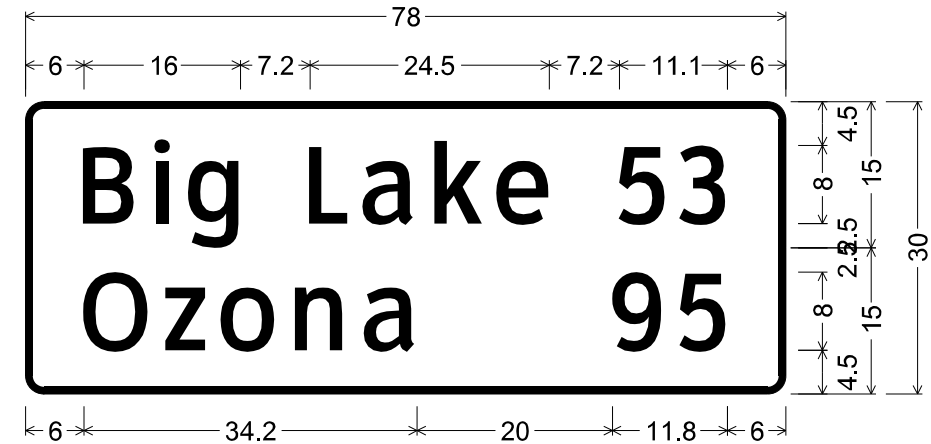
SH 137 from Midland County Line to Reagan County Line										0636-6007	0644-6103	644-6111	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY S80 (1)(U-WC)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA	EA
1	31.86490967	-101.7020999	Glasscock	SH 137	NB	W8-18	Road May Flood	36"x36"	Replace Sign		9		
2	31.88332967	-101.7079278	Glasscock	SH 137	NB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
3	31.92998241	-101.7226427	Glasscock	SH 137	NB	W8-18	Road May Flood	36"x36"	Replace Sign		9		
4	31.9328884	-101.7235703	Glasscock	SH 137	NB	W8-19aTP	Flood Gauge	18"x12"	Replace Sign	Yes	1.5		
4A	31.9328884	-101.7235703	Glasscock	SH 137	NB	W8-19	Flood Gauge Ruler	12"x72"	Replace Sign	Yes	6		
5	31.94627507	-101.7277704	Glasscock	SH 137	NB	W1-2L	Curve Symbol	36"x36"	Replace Sign & Support	Yes		1	
5A	31.94627507	-101.7277704	Glasscock	SH 137	NB	W13-1P	65 MPH	24"x24"	Replace Sign & Support	Yes		1	
6	31.96339047	-101.7501075	Glasscock	SH 137	NB	D20-1TL	CO RD 455 Left	24"x24"	Replace Sign		4		
7	31.96936204	-101.7525683	Glasscock	SH 137	NB	D20-1TL	CO RD 460 Left	24"x24"	Replace Sign		4		
8	32.01509111	-101.7668695	Glasscock	SH 137	NB	M1-6T	SH 137	24"x24"	Replace Sign	Yes	4		
9	32.04307845	-101.775883	Glasscock	SH 137	SB	I-2dT	Glasscock County Line	72"x24"	Replace Sign	Yes	12		
10	32.04308255	-101.7758644	Glasscock	SH 137	SB	M1-6T	SH 137	24"x24"	Replace Sign	Yes	4		
11A	32.04308255	-101.7758644	Glasscock	SH 137	SB	D10-7aT	TRM 328	3"x10"	Replace Sign	Yes	0.21		
11B	32.04308255	-101.7758644	Glasscock	SH 137	SB	D10-7aT	TRM 328	3"x10"	Replace Sign	Yes	0.21		
12	31.98549347	-101.7578241	Glasscock	SH 137	SB	D20-1TR	CO RD 470 Right	24"x24"	Replace Sign		4		
13	31.96908544	-101.7526775	Glasscock	SH 137	SB	W1-2L	Curve Advisory	36"x36"	Replace Sign & Support	Yes		1	
13A	31.96908544	-101.7526775	Glasscock	SH 137	SB	W13-1P	65 MPH	24"x24"	Replace Sign & Support	Yes		1	
14	31.95524585	-101.7358284	Glasscock	SH 137	SB	W1-2R	Curve Symbol	36"x36"	Replace Sign	Yes	9		
15	31.93615443	-101.7247957	Glasscock	SH 137	SB	W8-18	Road May Flood	36"x36"	Replace Sign		9		
16	31.89483101	-101.7117656	Glasscock	SH 137	SB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
17	31.88633747	-101.7091067	Glasscock	SH 137	SB	D2-2	Big Lake 53 mi Ozona 95 mi	78"x30"	Replace Sign & Support				1
18	31.88362513	-101.7082439	Glasscock	SH 137	SB	M1-6T	SH 137	24"x24"	Replace Sign	Yes	4		
19	31.88095016	-101.7073845	Glasscock	SH 137	SB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1	
TOTAL											111.92	5	1



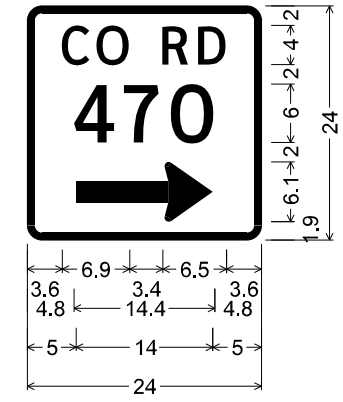
D20-1TL_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "455", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180°;



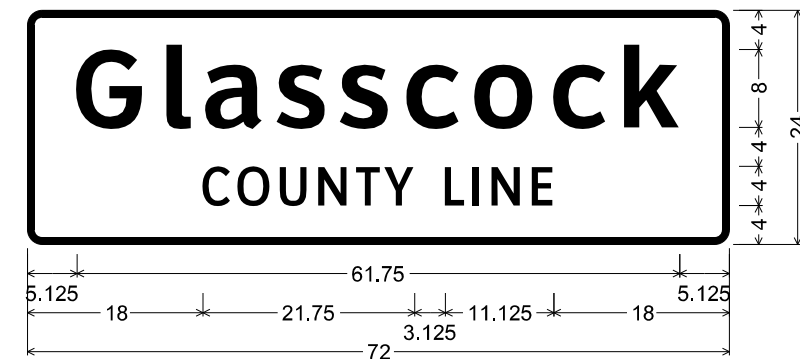
D20-1TL_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "460", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180°;



D2-2 8in;
 1.9" Radius, 0.8" Border, White on, Green;
 "Big Lake", ClearviewHwy-3-W; "53", ClearviewHwy-3-W;
 1.9" Radius, 0.8" Border, White on, Green;
 "Ozona", ClearviewHwy-3-W; "95", ClearviewHwy-3-W;



D20-1TR_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "470", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0°;



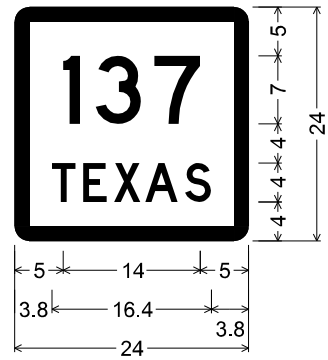
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Glasscock", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



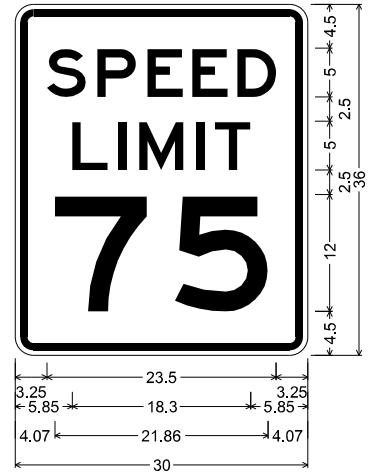
23.06.2021

		San Angelo District	
REPLACE SIGNS SH 137 GLASSCOCK COUNTY			
SHEET 1 OF 3		NOT TO SCALE	
CONT 6383	SECT 49	JOB 001	HIGHWAY US 83,ETC
SHEET ISSUED OR LAST REVISED 12-19		DIST SJT	COUNTY MENARD
		SHEET NO. 68	

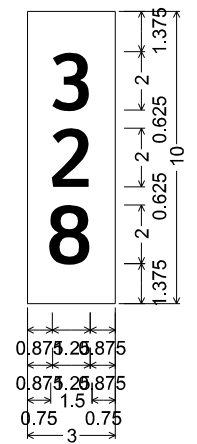
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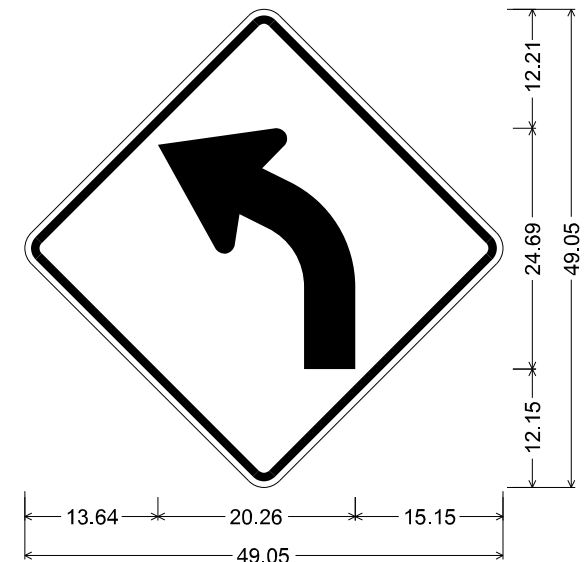
M1-6T-3_24x24;
 1.5" Radius, 1.5" Border, Black on, White;
 "137", D;
 "TEXAS", D;



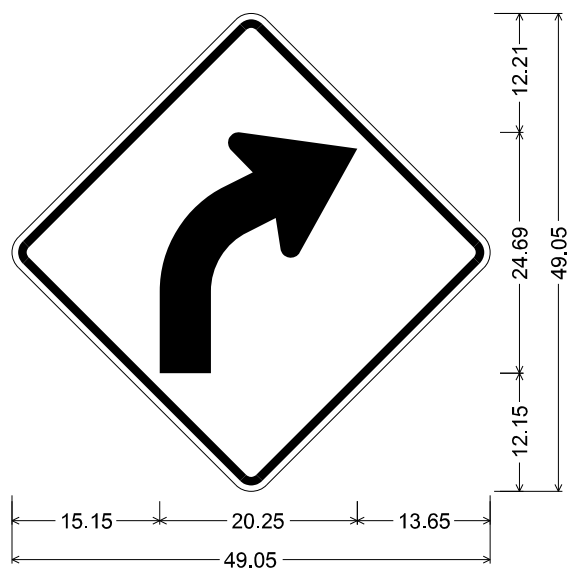
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



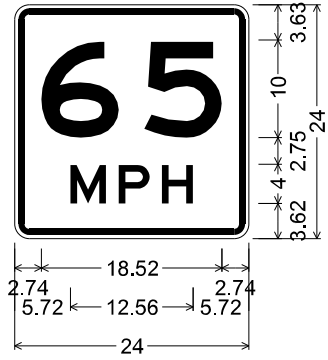
No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;
 "8", ClearviewHwy-4-W;



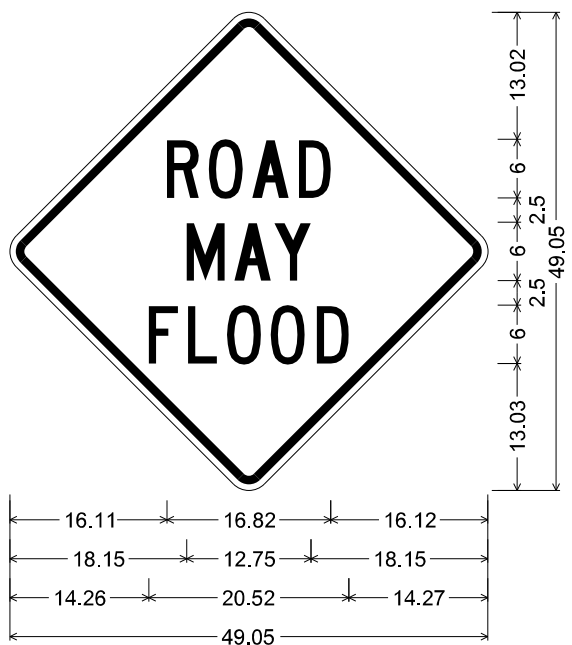
W1-2L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;



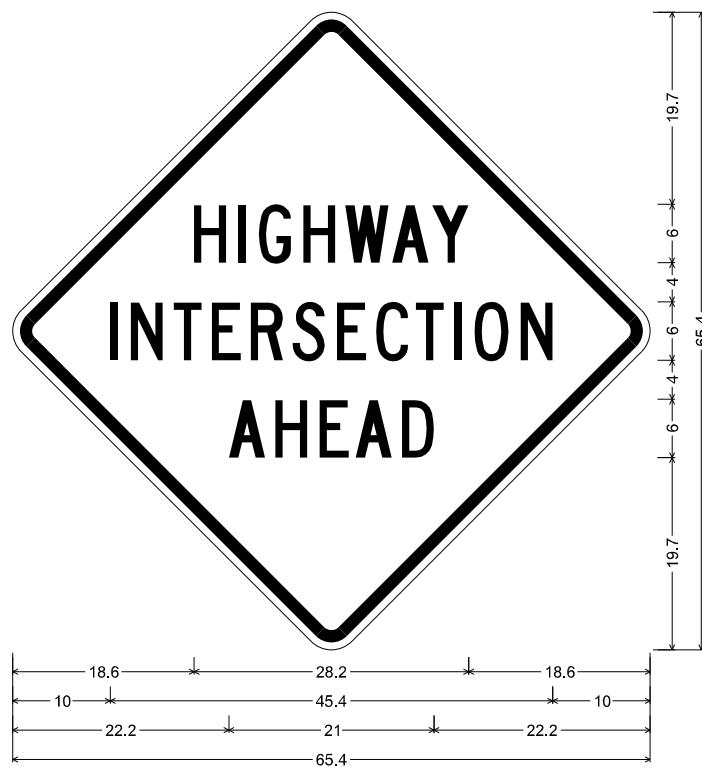
W1-2R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Arrow W1-2;



W13-1P_24x24;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, Yellow;
 "65", E;
 "MPH", E 121% spacing;



W8-18_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 "ROAD", C; "MAY", C; "FLOOD", C;



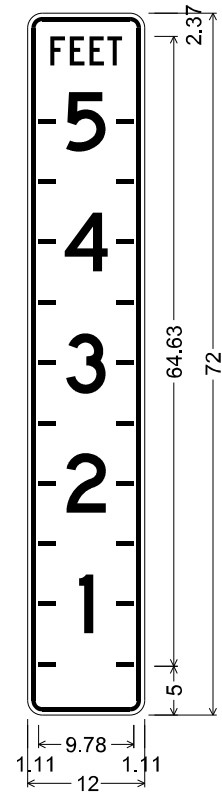
W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



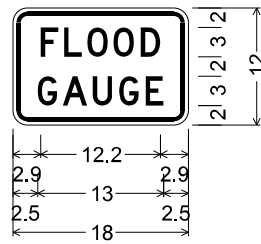
23.06.2021

		San Angelo District	
REPLACE SIGNS SH 137 GLASSCOCK COUNTY			
SHEET 2 OF 3		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001	HIGHWAY US 83,ETC
12-19	DIST SJT	COUNTY MENARD	SHEET NO. 69

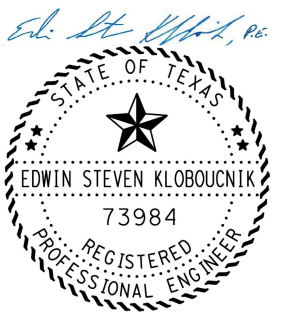
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W8-19_12x72;
 1.50" Radius, 0.44" Border, 0.38" Indent, Black on, Yellow;
 Symbol W8-19;



W8-19aTP_18x12;
 1.5" Radius, 0.4" Border, 0.4" Indent, Black on, Yellow;
 "FLOOD", D;
 "GAUGE", D;



23.06.2021



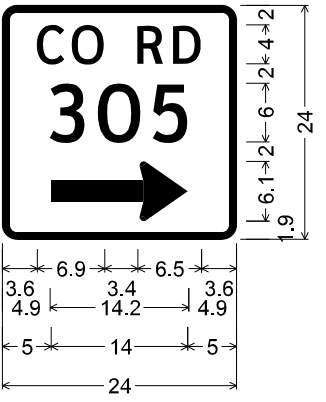
**REPLACE SIGNS
 SH 137
 GLASSCOCK COUNTY**

SHEET 3 OF 3 NOT TO SCALE

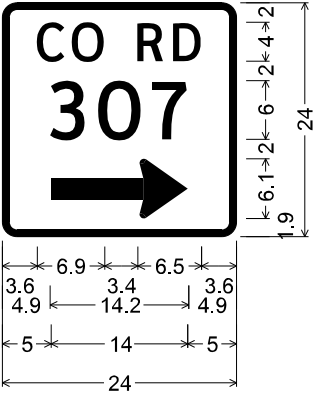
SHEET ISSUED OR LAST REVISED	CONT	SECT	JOB	HIGHWAY
	6383	49	001	US 83,ETC
12-19	DIST	COUNTY		SHEET NO.
	SJT	MENARD		70

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SH 158 from Midland County Line to Sterling County Line										0636-6007	0644-6103	644-6111	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & 5 TY 10 BWG (1)(P)	REPLACE SRS & 5 TY 580 (1)(U-WC)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree			
1	31.83361092	-101.3495635	Glasscock	SH 158	WB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	SF	EA	EA
2	31.83971751	-101.360673	Glasscock	SH 158	WB	D20-1TR	CO RD 307 Right Arrow	24"x24"	Replace Sign				
3	31.84885822	-101.3831023	Glasscock	SH 158	WB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign				
4	31.86046403	-101.4521174	Glasscock	SH 158	WB	D20-1TR	CO RD 305 Right Arrow	24"x24"	Replace Sign				
5	31.860768	-101.453502	Glasscock	SH 158	WB	R1-1	Stop Sign	36"x36"	Replace Sign				
6	31.86325817	-101.46404	Glasscock	SH 158	WB	D7-6aTL	Historical Marker 1 Mile On Left 2187	48"x48"	Replace Sign		16		
7	31.86444475	-101.4686612	Glasscock	SH 158	WB	R2-1	Speed Limit 70	30"x36"	Replace Sign		7.5		
8	31.86471518	-101.4697069	Glasscock	SH 158	WB	W3-5	Speed Limit 55 Ahead	36"x36"	Replace Sign		9		
9	31.86462944	-101.4785245	Glasscock	SH 158	WB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4		
10	31.86438568	-101.4795846	Glasscock	SH 158	WB	D1-2	Big Lake Left Big Spring Right	90"x30"	Replace Sign & Support				1
11	31.86405062	-101.4810706	Glasscock	SH 158	WB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4		
11A	31.86405062	-101.4810706	Glasscock	SH 158	WB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2		
12	31.86351171	-101.4834144	Glasscock	SH 158	WB	M3-4	WEST	24"x12"	Replace Sign	Yes	2		
12A	31.86351171	-101.4834144	Glasscock	SH 158	WB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4		
13	31.86321293	-101.4847151	Glasscock	SH 158	WB	S5-2	End School Zone	24"x30"	Replace Sign		5		
14	31.86311196	-101.4853322	Glasscock	SH 158	WB	R2-1	Speed Limit 55	30"x36"	Replace Sign		7.5		
15	31.86298599	-101.4885851	Glasscock	SH 158	WB	R2-1	Speed Limit 60	30"x36"	Replace Sign & Support			1	
16	31.86303122	-101.4889039	Glasscock	SH 158	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
17	31.86311517	-101.5047362	Glasscock	SH 158	WB	D20-1TL	CO RD 110 Left	24"x24"	Replace Sign		4		
18	31.86331321	-101.531364	Glasscock	SH 158	WB	W11-10L	Truck Crossing	36"x36"	Replace Sign		9		
19	31.86342322	-101.5463729	Glasscock	SH 158	WB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4		
19A	31.86342322	-101.5463729	Glasscock	SH 158	WB	D10-7aT	TRM 320	3"x10"	Replace Sign	Yes	0.21		
19B	31.86342322	-101.5463729	Glasscock	SH 158	WB	D10-7aT	TRM 320	3"x10"	Replace Sign	Yes	0.21		
20	31.86367271	-101.5841012	Glasscock	SH 158	WB	W11-10R	Truck Crossing	36"x36"	Replace Sign		9		
21	31.87610262	-101.6634678	Glasscock	SH 158	WB	W11-10L	Truck Crossing	36"x36"	Replace Sign		9		
22	31.89274049	-101.7228016	Glasscock	SH 158	WB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5		
23	31.87124043	-101.6470529	Glasscock	SH 158	EB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4		
24	31.86348686	-101.5927871	Glasscock	SH 158	EB	W11-10L	Truck Crossing	36"x36"	Replace Sign		9		
25	31.86315537	-101.5467558	Glasscock	SH 158	EB	W11-10L	Truck Crossing	36"x36"	Replace Sign		9		
26	31.86292021	-101.5123648	Glasscock	SH 158	EB	M1-6T	SH 158	24"x24"	Replace Sign		4		
27	31.86280295	-101.4971483	Glasscock	SH 158	EB	D7-6aTR	Historical Marker 1 Mile On Right 2187	48"x48"	Replace Sign		16		
28	31.86276871	-101.4965864	Glasscock	SH 158	EB	W1-7T	Double Arrow/Yellow/Chevron	96"x36"	Replace Sign		24		
29	31.86277028	-101.4923328	Glasscock	SH 158	EB	R2-1	Speed Limit 60	30"x36"	Replace Sign & Support			1	
30	31.86271477	-101.4889414	Glasscock	SH 158	EB	W1-7T	Double Arrow/Yellow/Chevron	96"x36"	Replace Sign & Support				1
31	31.8627312	-101.4875308	Glasscock	SH 158	EB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
32	31.863522	-101.48231	Glasscock	SH 158	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9		
33	31.86374359	-101.4812927	Glasscock	SH 158	EB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2		
33A	31.86374359	-101.4812927	Glasscock	SH 158	EB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2		
33B	31.86374359	-101.4812927	Glasscock	SH 158	EB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2		
34	31.86381303	-101.480986	Glasscock	SH 158	EB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2		
34A	31.86381303	-101.480986	Glasscock	SH 158	EB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2		
35	31.86430783	-101.4788342	Glasscock	SH 158	EB	R2-1	Speed Limit 55	30"x36"	Replace Sign & Support			1	
36	31.8652115	-101.4727587	Glasscock	SH 158	EB	R2-1	Speed Limit 70	30"x36"	Replace Sign		7.5		
37	31.86421683	-101.4687472	Glasscock	SH 158	EB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5		
38	31.85789781	-101.4382375	Glasscock	SH 158	EB	D20-1TR	CO RD 220 Right Arrow	24"x24"	Replace Sign		4		
39	31.84947639	-101.388182	Glasscock	SH 158	EB	W8-13aT	Bridge May Ice In Cold Weather	36"x36"	Replace Sign		9		
										TOTAL	273.12	3	2



D20-1TR_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "305", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0";



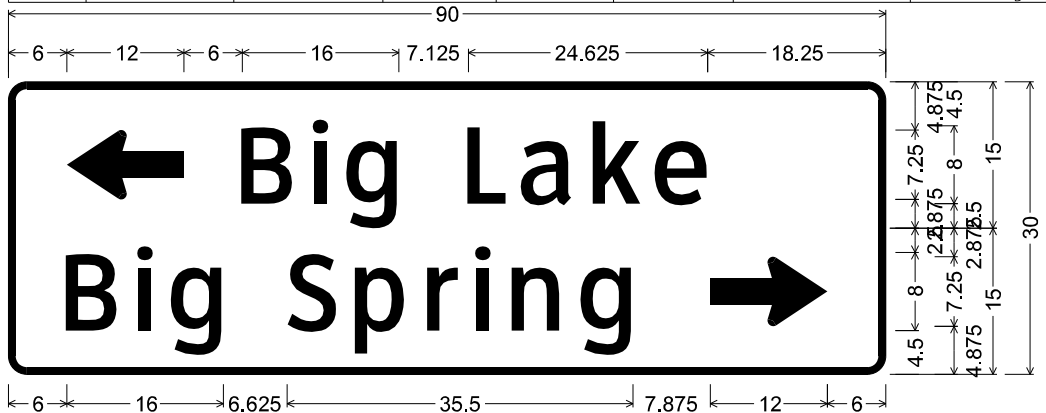
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 "CO RD", ClearviewHwy-3-W;
 "307", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0";

* TWO SLIP BASES AVAILABLE USE ONE.

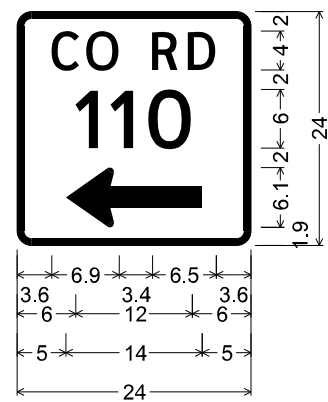


23.06.2021

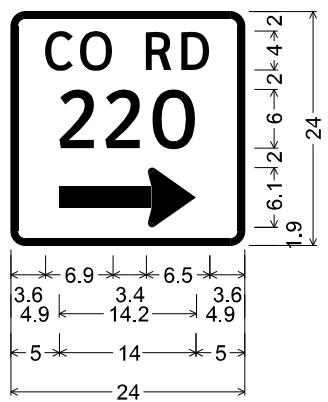
Texas Department of Transportation		San Angelo District	
REPLACE SIGNS SH 158 GLASSCOCK COUNTY			
SHEET 1 OF 4		NOT TO SCALE	
© TXDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD SHEET NO. 71



D1-2 8in LT-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180"; "Big Lake", ClearviewHwy-3-W;
 "Big Spring", ClearviewHwy-3-W; Standard Arrow Custom 12.000" X 7.125" 0";

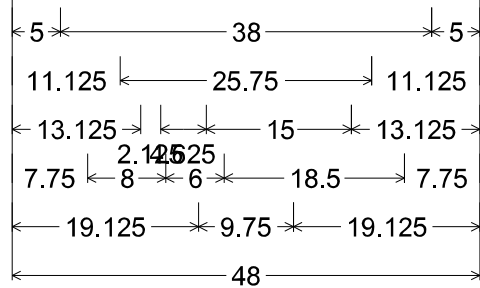


D20-1TL_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "110", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180";

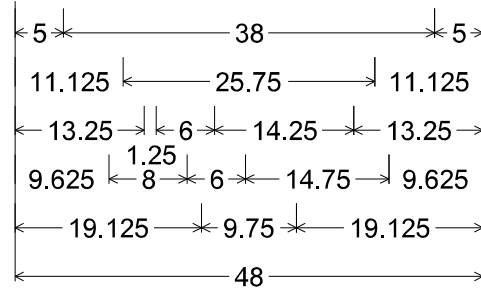


D20-1TR_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "220", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0";

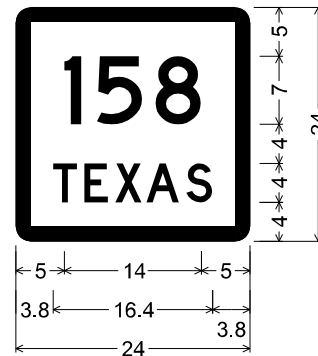
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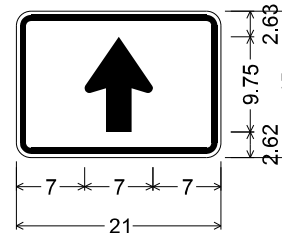
3.000" Radius, 1.000" Border, White on, Brown;
 "HISTORICAL", C;
 "MARKER", C;
 "1 MILE", C;
 "ON RIGHT", C; "2187", C;



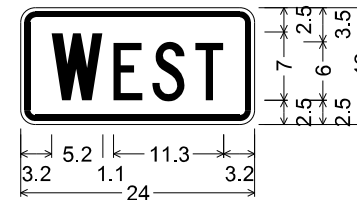
3.000" Radius, 1.000" Border, White on, Brown;
 "HISTORICAL", C;
 "MARKER", C;
 "1 MILE", C;
 "ON LEFT", C; "2187", C;



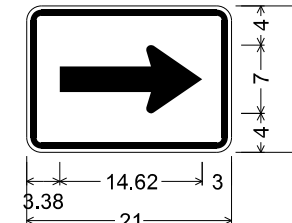
M1-6T-3_24x24;
 1.5" Radius, 1.5" Border, Black on, White;
 "158", D;
 "TEXAS", D;



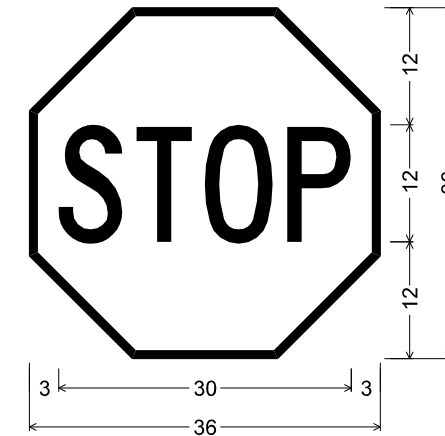
M6-3_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-3;



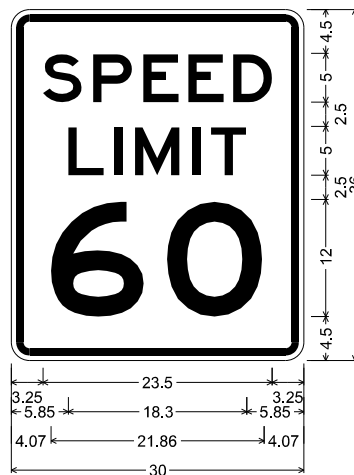
M3-4_24x12;
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 "WEST", C;



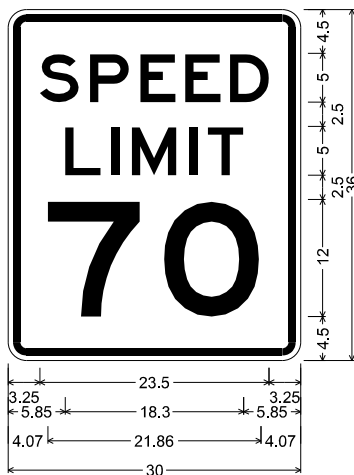
M6-1_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-1;



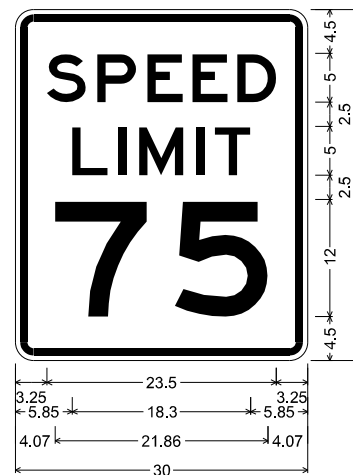
R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "60", E specified length;



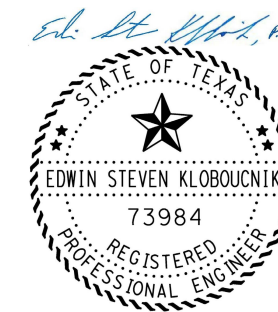
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "70", E specified length;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



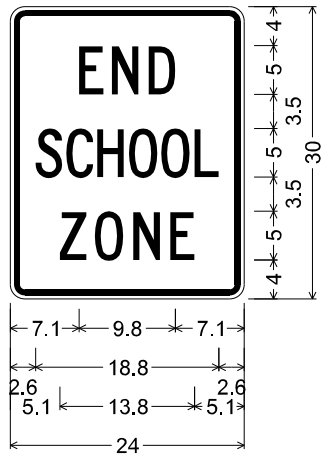
R2-1_30x36;
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 "SPEED", E specified length;
 "LIMIT", E specified length;
 "55", E specified length;



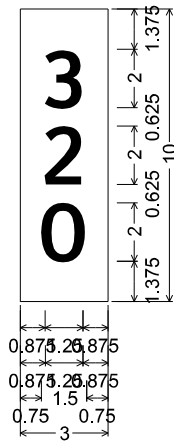
23.06.2021

		San Angelo District	
REPLACE SIGNS SH 158 GLASSCOCK COUNTY			
SHEET 2 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
HIGHWAY US 83,ETC	SHEET NO. 72		

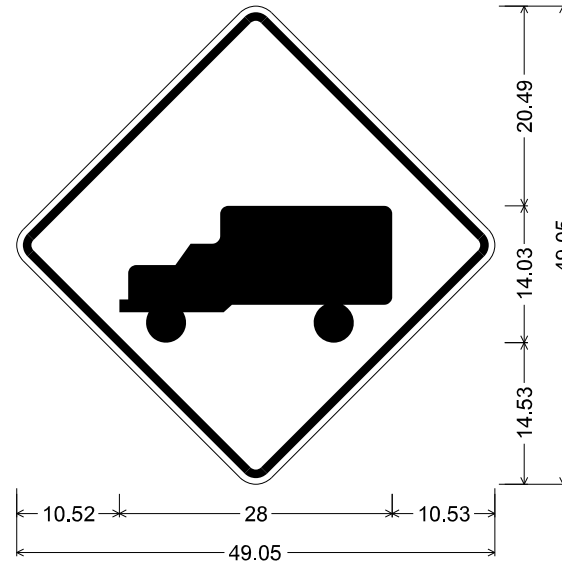
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S5-2_24x30;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "END", C;
 "SCHOOL", C 50% spacing;
 "ZONE", C;



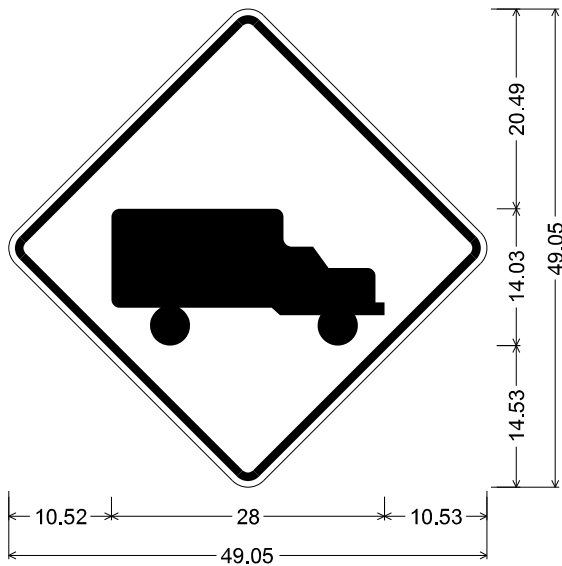
No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



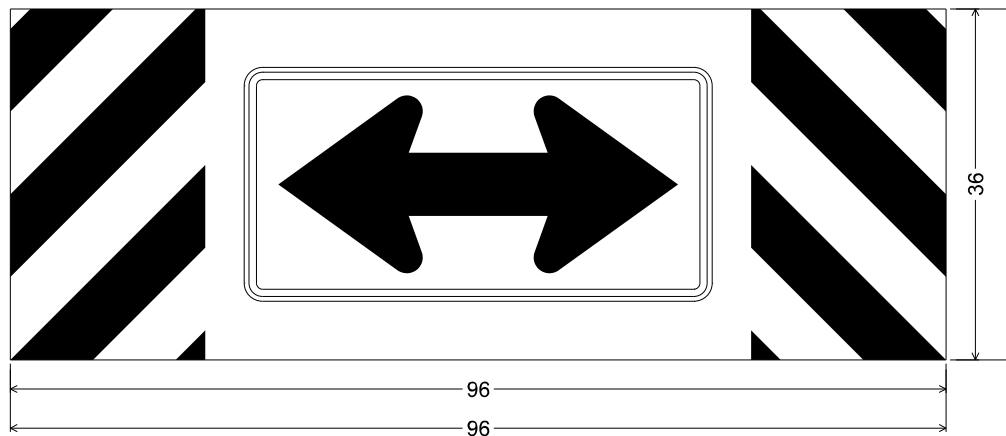
W11-10L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W11-10;



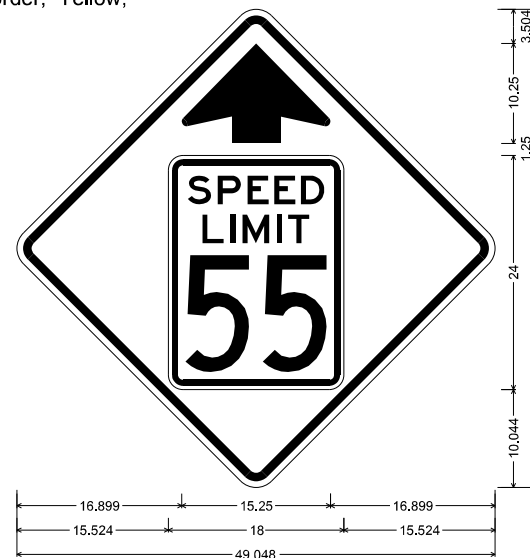
W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



W11-10R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W11-10;



W1-7T_96x36;
 No border, Yellow;



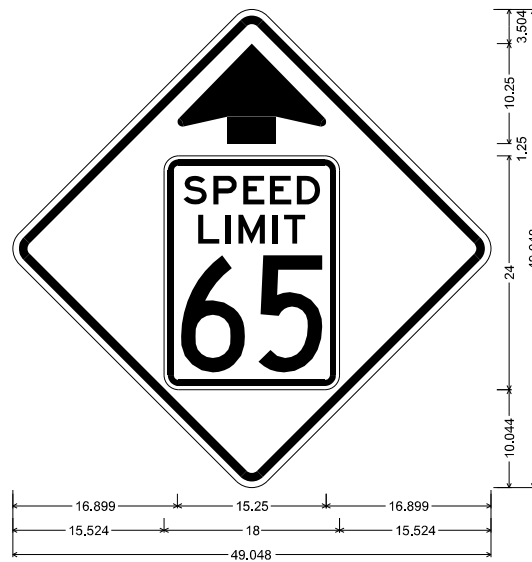
W3-5_36x36;
 36.00" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
 Down Arrow Custom - 10.250" 90°;



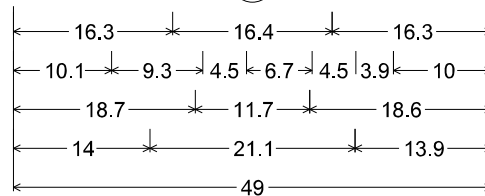
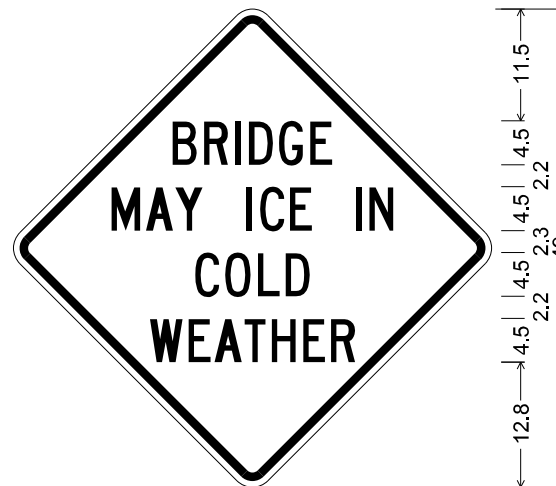
23.06.2021

		San Angelo District	
REPLACE SIGNS SH 158 GLASSCOCK COUNTY			
SHEET 3 OF 4		NOT TO SCALE	
CTxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 73
12-19		73	

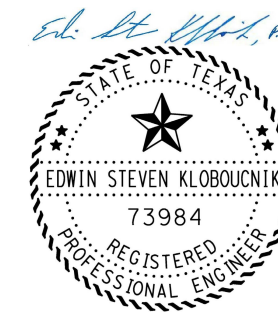
DATE: 6/23/2021 10:43:15 AM
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W3-5_36x36;
 36.000" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
 Down Arrow Custom - 10.250" 90°;



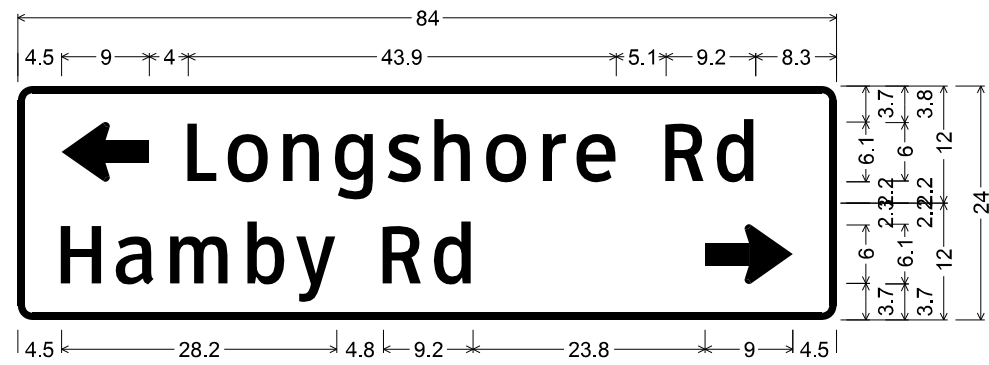
W8-13aT_36x36;
 36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
 "BRIDGE", C 80% spacing;
 "MAY ICE IN", C 80% spacing;
 "COLD", C 80% spacing;
 "WEATHER", C 80% spacing;



23.06.2021

		San Angelo District	
REPLACE SIGNS SH 158 GLASSCOCK COUNTY			
SHEET 4 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 74
12-19	SJT	COUNTY MENARD	SHEET NO. 74

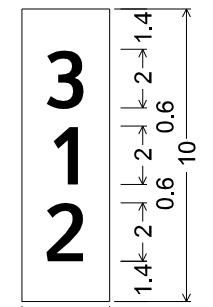
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D21-2T_VARx24;
 1.5" Radius, 0.8" Border, White on, Green;
 Standard Arrow Custom 9.0" X 6.1" 180°;
 "Longshore Rd", ClearviewHwy-3-W;
 1.5" Radius, 0.8" Border, White on, Green;
 "Hamby Rd", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0°;



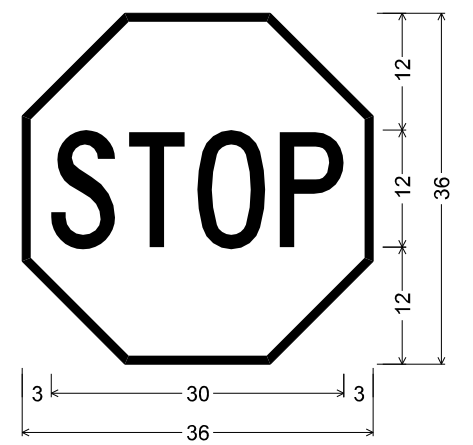
M1-6F_24x24;
 1.5" Radius, No border, Black;
 FARM, 2D;
 ROAD, 2D;
 461, 5D;



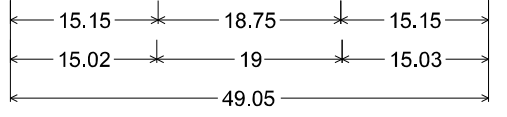
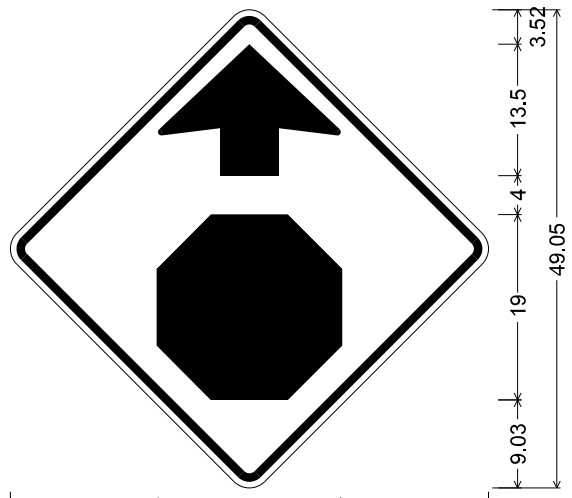
D10-7aT 3in;
 No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "1", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;



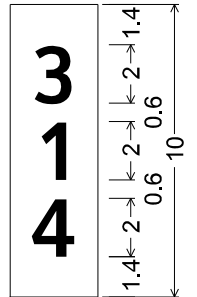
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



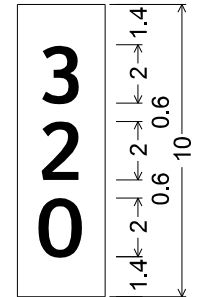
R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



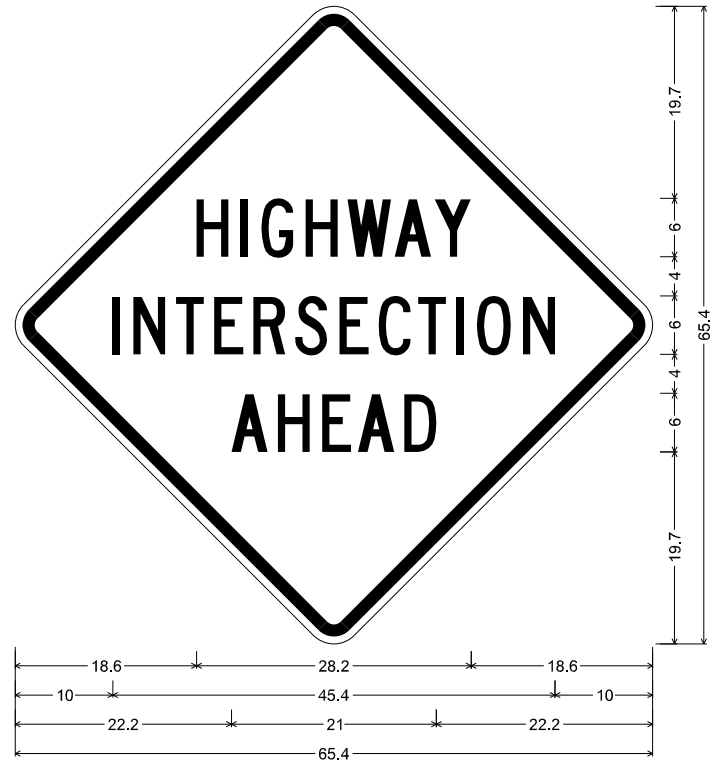
W3-1_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Down Arrow Custom - 13.50" 90°;
 Symbol R1-1;



D10-7aT 3in;
 No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "1", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;



D10-7aT 3in;
 No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;

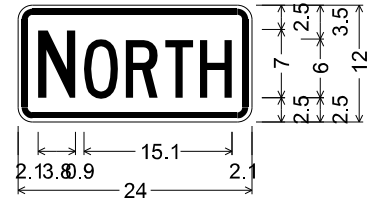


23.06.2021

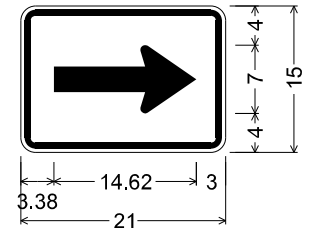
		San Angelo District	
REPLACE SIGNS FM 461 GLASSCOCK COUNTY			
SHEET 2 OF 2		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
HIGHWAY US 83,ETC	SHEET NO. 76		

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FM 821 from US 87 to Howard County Line										0636-6007	644-6104	
Existing Location										REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1XT)	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA
1	32.08605776	-101.3912581	Glasscock	FM 821	EB	R1-2	Yield Sign	48"x48"48"	Replace Sign		7	
2	32.08617983	-101.3909134	Glasscock	FM 821	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2	
2A	32.08617983	-101.3909134	Glasscock	FM 821	EB	M1-6F	FM 821	24"x24"	Replace Sign	Yes	4	
2B	32.08617983	-101.3909134	Glasscock	FM 821	EB	D10-7aT	TRM 318	3"x10"	Replace Sign	Yes	0.21	
2C	32.08617983	-101.3909134	Glasscock	FM 821	EB	D10-7aT	TRM 318	3"x10"	Replace Sign	Yes	0.21	
3	32.08714171	-101.3861838	Glasscock	FM 821	EB	I-2dT	Howard County Line	60"x24"	Replace Sign & Support	Yes		1
4	32.08729014	-101.3862232	Glasscock	FM 821	WB	I-2dT	Glasscock County Line	72"x24"	Replace Sign & Support			1
5	32.08681926	-101.3885208	Glasscock	FM 821	WB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16	
6	32.08626236	-101.3913593	Glasscock	FM 821	WB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2	
6A	32.08626236	-101.3913593	Glasscock	FM 821	WB	M1-4	US 87	24"x24"	Replace Sign	Yes	4	
6B	32.08626236	-101.3913593	Glasscock	FM 821	WB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2	
7	32.08612468	-101.3916032	Glasscock	FM 821	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9	
TOTAL											46.62	2



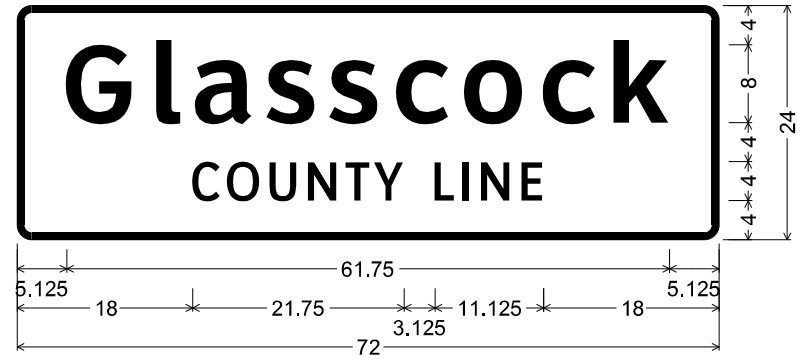
M3-1_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "NORTH", C 65% spacing;



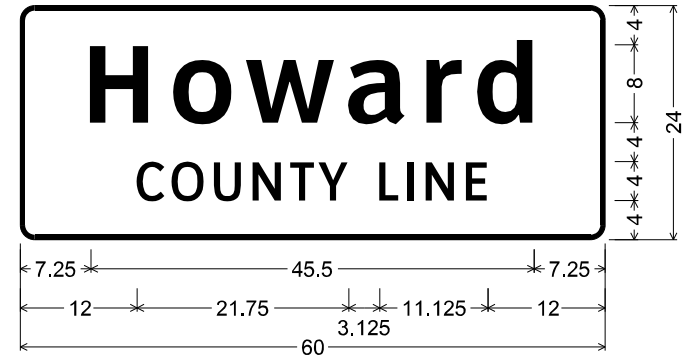
M6-1_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-1;



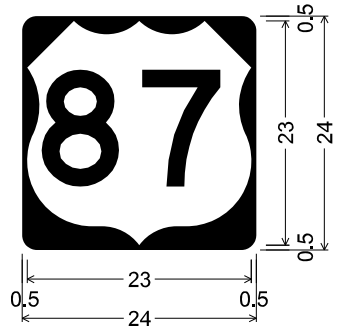
M1-6F_24x24;
 1.5" Radius, No border, Black;
 FARM, 2D;
 ROAD, 2D;
 821, 5D;



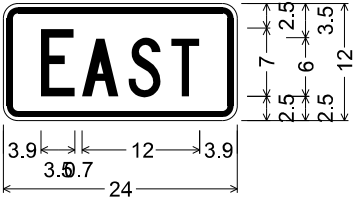
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Glasscock", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



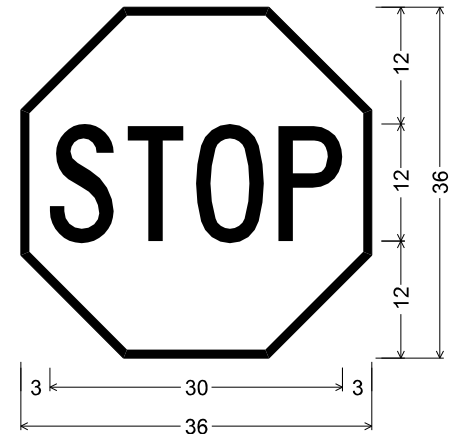
I-2d 8in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Howard", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



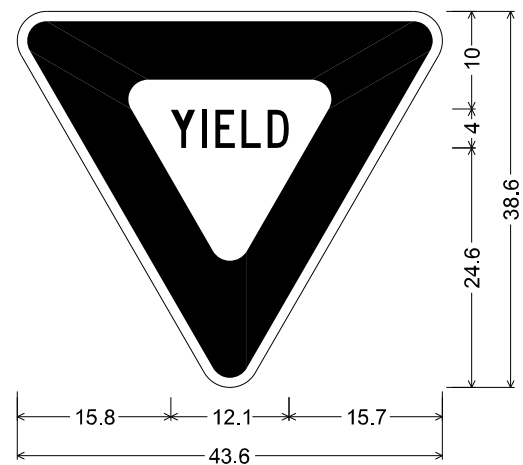
M1-4_24x24;
 1.5" Radius, No border, Black;



M3-2_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "EAST", C;



R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



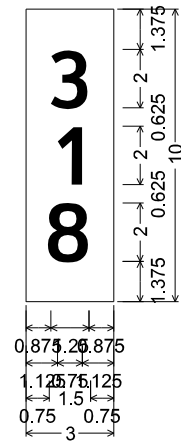
R1-2_48x48;
 48.0" corner to corner 3.0" Radius, 6.0" Border, 1.0" Indent, Red on, White;
 "YIELD", C specified length;



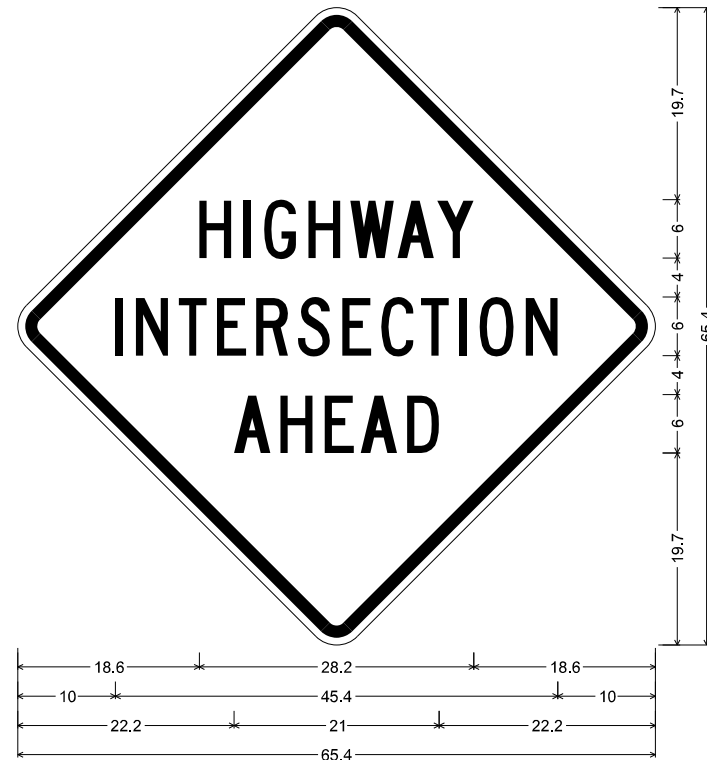
23.06.2021

		San Angelo District	
REPLACE SIGNS FM 821 GLASSCOCK COUNTY			
SHEET 1 OF 2		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
		HIGHWAY US 83,ETC	SHEET NO. 77

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No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "1", ClearviewHwy-4-W;
 "8", ClearviewHwy-4-W;



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



23.06.2021

		San Angelo District	
REPLACE SIGNS FM 821 GLASSCOCK COUNTY			
SHEET 2 OF 2		NOT TO SCALE	
© TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
HIGHWAY US 83, ETC	SHEET NO. 78		

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RM 33 from Howard County Line to Reagan County Line											0636-6007	0644-6103	644-6104	644-6111	644-6112
Existing Location											REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY S80 (1)(U-WC)	REPLACE SRS & S TY S80 (1)(U-EXAL)
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree					
1	32.08717778	-101.4899568	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	SF	EA	EA	EA	EA
1A	32.08717778	-101.4899568	Glasscock	RM 33	SB	D10-7aT	TRM 318	3"x10"	Replace Sign	Yes	0.21				
1B	32.08717778	-101.4899568	Glasscock	RM 33	SB	D10-7aT	TRM 318	3"x10"	Replace Sign	Yes	0.21				
2	32.07853168	-101.487218	Glasscock	RM 33	SB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
2A	32.07853168	-101.487218	Glasscock	RM 33	SB	M1-6F	FM 461	24"x24"	Replace Sign	Yes	4				
3	32.07687037	-101.4866937	Glasscock	RM 33	SB	M1-6F	FM 461	24"x24"	Replace Sign	Yes	4				
4	32.07575955	-101.4863457	Glasscock	RM 33	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2				
5	32.07284259	-101.4854315	Glasscock	RM 33	SB	D2-2	Garden City 15 Big Lake 62	96"x30"	Replace Sign		20				
6	32.06508889	-101.4830048	Glasscock	RM 33	SB	W1-10R	Curved T Intersection	36"x36"	Replace Sign & Support			1			
7	32.06340436	-101.4825697	Glasscock	RM 33	SB	D21-1TL	Drumright Rd Left	72"x12"	Replace Sign		6				
8	32.03685725	-101.4827303	Glasscock	RM 33	SB	W2-2L	T Intersection	36"x36"	Replace Sign		9				
9	32.01729153	-101.4831948	Glasscock	RM 33	SB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
10	31.9749903	-101.484364	Glasscock	RM 33	SB	W2-1	Cross Intersection	36"x36"	Replace Sign & Support			1			
11	31.972232	-101.484566	Glasscock	RM 33	SB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
12	31.87216052	-101.4836657	Glasscock	RM 33	SB	R2-1	Speed Limit 50	30"x36"	Replace Sign & Support			1			
13	31.86930545	-101.4829881	Glasscock	RM 33	SB	R2-1	Speed Limit 35	30"x36"	Replace Sign & Support			1			
14	31.86768537	-101.4824678	Glasscock	RM 33	SB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2				
14A	31.86768537	-101.4824678	Glasscock	RM 33	SB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4				
15	31.86657347	-101.4821259	Glasscock	RM 33	SB	W3-1	Stop Sign Ahead	36"x36"	Replace Sign & Support			1			
16	31.86477636	-101.4815994	Glasscock	RM 33	SB	D1-2	Sterling City Left Midland Right	102"x30"	Replace Sign & Support						1
18	31.8633822	-101.4811415	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
19	31.86119111	-101.4804253	Glasscock	RM 33	SB	D2-1	Big Lake 47 mi	78"x18"	Replace Sign		9.75				
20	31.86074594	-101.4802861	Glasscock	RM 33	SB	R2-1	Speed Limit 50	30"x36"	Replace Sign		7.5				
21	31.85806016	-101.4794606	Glasscock	RM 33	SB	R2-1	Speed Limit 60	30"x36"	Replace Sign		7.5				
22	31.85708133	-101.479117	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
23	31.85516157	-101.4785018	Glasscock	RM 33	SB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5				
24	31.84569109	-101.4763725	Glasscock	RM 33	SB	W8-13aT	Bridge May Ice in Cold Weather	36"x36"	Replace Sign & Support			1			
25	31.84289173	-101.4758806	Glasscock	RM 33	SB	I-3	Lacy Draw	30"x18"	Replace Sign & Support				1		
26	31.841311	-101.475691	Glasscock	RM 33	SB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
27	31.82683732	-101.4730843	Glasscock	RM 33	SB	W2-2L	T Intersection	36"x36"	Replace Sign		9				
28	31.82491502	-101.4727478	Glasscock	RM 33	SB	D20-1TL	C0 RD 220 Left Arrow	24"x24"	Replace Sign & Support			1			
29	31.81571944	-101.4711455	Glasscock	RM 33	SB	W8-13aT	Bridge May Ice in Cold	36"x36"	Replace Sign		9				
30	31.750078	-101.465685	Glasscock	RM 33	SB	R1-1	Stop Sign	36"x36"	Replace Sign		9				
31	31.72060339	-101.4620725	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
31A	31.72060339	-101.4620725	Glasscock	RM 33	SB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2				
31B	31.72060339	-101.4620725	Glasscock	RM 33	SB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2				
32	31.71978257	-101.4619707	Glasscock	RM 33	SB	M3-3	SOUTH	24"x12"	Replace Sign	Yes	2				
32A	31.71978257	-101.4619707	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
33	31.71871138	-101.4619404	Glasscock	RM 33	SB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5				
34	31.71577308	-101.461961	Glasscock	RM 33	SB	D2-1	Big Lake 37 mi	78"x18"	Replace Sign		9.75				
35	31.69057572	-101.4622277	Glasscock	RM 33	SB	W1-7T	Double Arrow/Yellow/Chevron	96"x36"	Replace Sign		24				
36	31.68392403	-101.4621826	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
37	31.65133307	-101.4624138	Glasscock	RM 33	SB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
37A	31.65133307	-101.4624138	Glasscock	RM 33	SB	D10-7aT	TRM 350	3"x10"	Replace Sign	Yes	0.21				
37B	31.65133307	-101.4624138	Glasscock	RM 33	SB	D10-7aT	TRM 350	3"x10"	Replace Sign	Yes	0.21				
38	31.65133319	-101.4624313	Glasscock	RM 33	SB	I-2dT	Reagan County Line	54"x24"	Replace Sign		9				
39	31.65133841	-101.4622355	Glasscock	RM 33	NB	I-2dT	Glasscock County Line	72"x24"	Replace Sign	Yes	12				
40	31.65482058	-101.4622106	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
41	31.68914036	-101.4619789	Glasscock	RM 33	NB	W2-2R	T Intersection	36"x36"	Replace Sign & Support			1			
42	31.670213	-101.462131	Glasscock	RM 33	NB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1			
43	31.68995473	-101.4619614	Glasscock	RM 33	NB	D20-1TR	C0 RD 280 Right Arrow	24"x24"	Replace Sign & Support			1			
44	31.69074852	-101.4618247	Glasscock	RM 33	NB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1			
45	31.72033346	-101.4618462	Glasscock	RM 33	NB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2				
46	31.72038404	-101.4618599	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
47	31.72043884	-101.4617641	Glasscock	RM 33	NB	W1-7T	Double Arrow/Yellow/Chevron	96"x36"	Replace Sign & Support				1		
48	31.72104955	-101.4619385	Glasscock	RM 33	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2				
49	31.72104955	-101.4619385	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4				
50	31.72335448	-101.4621914	Glasscock	RM 33	NB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5				
51	31.72389248	-101.462261	Glasscock	RM 33	NB	D2-2	Garden City 10 mi Big Spring 37 mi	102"x30"	Replace Sign & Support						1
52	31.74730713	-101.4651342	Glasscock	RM 33	NB	W2-1	Cross Intersection	36"x36"	Replace Sign		9				
TOTAL											269.84	11	1	1	2



23.06.2021

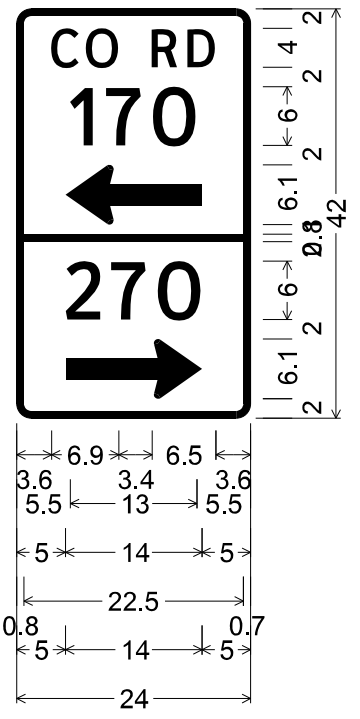
		San Angelo District	
<h2>REPLACE SIGNS</h2> <h3>RM 33</h3> <h2>GLASSCOCK COUNTY</h2>			
SHEET 1 OF 6		NOT TO SCALE	
	2021 SHEET ISSUED OR LAST REVISED	CONT: 6383 SECT: 49	JOB: 001 COUNTY: MENARD HIGHWAY: US 83, ETC SHEET NO.: 79

* TWO SLIP BASES AVAILABLE USE ONE.

DATE: 6/23/2021 10:43:51 AM
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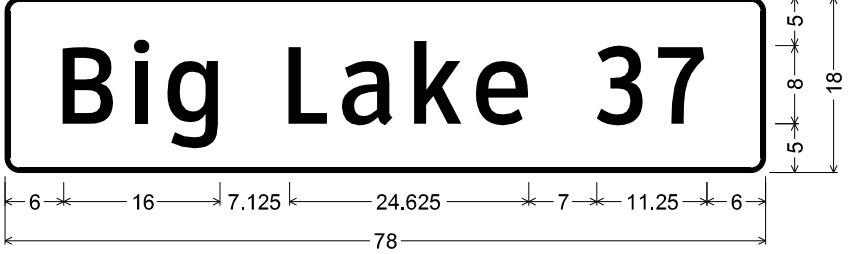
RM 33 from Howard County Line to Reagan County Line Existing Location										0636-6007	0644-6103	644-6104	644-6111	644-6112
Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY S80 (1)(U-WC)	REPLACE SRS & S TY S80 (1)(U-EXAL)
										SF	EA	EA	EA	EA
53	31.74935219	-101.4653759	Glasscock	RM 33	NB	D20-5T	CO RD 170 Left Arrow CO RD 270 Right Arrow	24"x42"	Replace Sign	7				
54	31.7705271	-101.4679806	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4			
55	31.79419959	-101.4674844	Glasscock	RM 33	NB	D20-1TL	CO RD 140 Left Arrow	24"x24"	Replace Sign		4			
56	31.79506859	-101.4673933	Glasscock	RM 33	NB	W1-7T	Double Arrow/Yellow/Chevron	96"x36"	Replace Sign & Support				1	
57	31.82118845	-101.4719193	Glasscock	RM 33	NB	W2-2R	T Intersection	36"x36"	Replace Sign		9			
58	31.82323085	-101.4722773	Glasscock	RM 33	NB	D20-1TR	CD RD 220 Right Arrow	24"x24"	Replace Sign & Support			1		
59	31.82398665	-101.4723049	Glasscock	RM 33	NB	R1-1	Stop Sign	36"x36"	Replace Sign	Yes	9			
60	31.82831975	-101.4731652	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4			
60A	31.82831975	-101.4731652	Glasscock	RM 33	NB	D10-7aT	TRM 336	3"x10"	Replace Sign	Yes	0.21			
60B	31.82831975	-101.4731652	Glasscock	RM 33	NB	D10-7aT	TRM 336	3"x10"	Replace Sign	Yes	0.21			
61	31.83851936	-101.4749415	Glasscock	RM 33	NB	W2-2L	T Intersection	36"x36"	Replace Sign & Support			1		
62	31.83983299	-101.4751685	Glasscock	RM 33	NB	W8-13aT	Bridge May Ice in Cold	36"x36"	Replace Sign		9			
63	31.8414416	-101.4753474	Glasscock	RM 33	NB	W1-7T	Double Arrow/Yellow/Chevron	96"x36"	Replace Sign & Support				1	
64	31.85197163	-101.4773414	Glasscock	RM 33	NB	W3-5	Speed Limit 60 Ahead	36"x36"	Replace Sign		9			
65	31.85519561	-101.4783387	Glasscock	RM 33	NB	R2-1	Speed Limit 60	30"x36"	Replace Sign		7.5			
66	31.8581288	-101.4792275	Glasscock	RM 33	NB	R2-1	Speed Limit 50	30"x36"	Replace Sign & Support			1		
67	31.85859454	-101.479402	Glasscock	RM 33	NB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16			
68	31.8593288	-101.4796385	Glasscock	RM 33	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
68A	31.8593288	-101.4796385	Glasscock	RM 33	NB	M1-6T	SH 158	24"x24"	Replace Sign	Yes	4			
69	31.86078085	-101.4800915	Glasscock	RM 33	NB	R2-1	Speed Limit 35	30"x36"	Replace Sign		7.5			
71	31.86486487	-101.4813652	Glasscock	RM 33	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2			
71A	31.86486487	-101.4813652	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4			
72	31.86934939	-101.4828135	Glasscock	RM 33	NB	R2-1	Speed Limit 50	30"x36"	Replace Sign		7.5			
73	31.94306372	-101.4865949	Glasscock	RM 33	NB	M1-6R	RM 33	24"x24"	Replace Sign	Yes	4			
74	31.96978766	-101.4844397	Glasscock	RM 33	NB	W2-1	Cross Intersection	36"x36"	Replace Sign		9			
75	31.97152589	-101.4843402	Glasscock	RM 33	NB	D21-2T	Newell Rd Left Arrow Sherrod Rd Right Arrow	72"x24"	Replace Sign		12			
76	32.03169339	-101.4826365	Glasscock	RM 33	NB	W2-2R	T Intersection	36"x36"	Replace Sign		9			
77	32.05996518	-101.4821426	Glasscock	RM 33	NB	W1-10L	Curved T Intersection	36"x36"	Replace Sign		9			
78	32.0616774	-101.482143	Glasscock	RM 33	NB	D21-1TR	Drumright Rd Right Arrow	84"x12"	Replace Sign		7			
79	32.07418227	-101.4856555	Glasscock	RM 33	NB	D10-7aT	TRM 324	3"x10"	Replace Sign		0.21			
79A	32.07418227	-101.4856555	Glasscock	RM 33	NB	D10-7aT	TRM 324	3"x10"	Replace Sign		0.21			
80	32.07631414	-101.4863109	Glasscock	RM 33	NB	M1-6F	FM 461	24"x24"	Replace Sign	Yes	4			
81	32.07631414	-101.4863109	Glasscock	RM 33	NB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2			
82	32.07631414	-101.4863109	Glasscock	RM 33	NB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2			
TOTAL										164.94	3	0	2	0

*

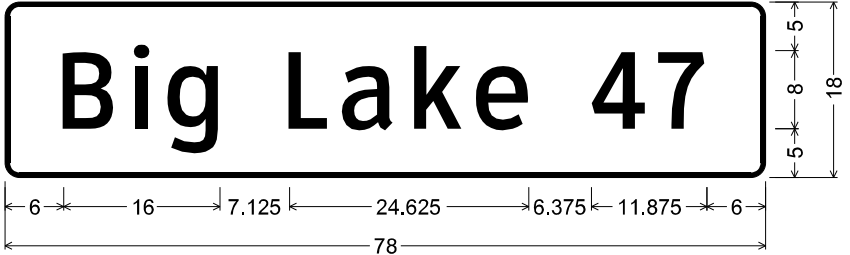


D20-5T_24x42;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "170", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180°;
 "270", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0°;

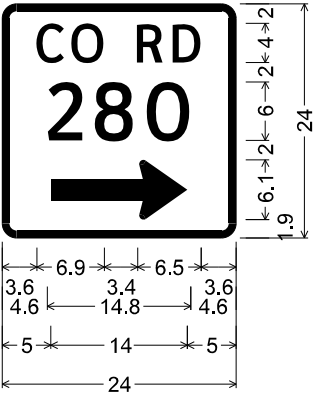
* TWO SLIP BASES AVAILABLE USE ONE.



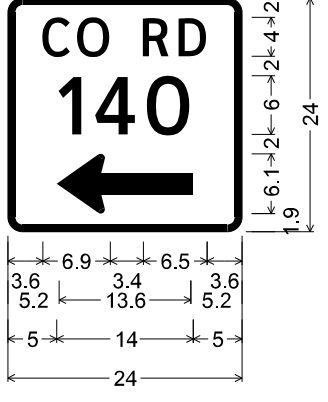
D2-1 8in;
 1.500" Radius, 0.500" Border, White on, Green;
 "Big Lake", ClearviewHwy-3-W; "37", ClearviewHwy-3-W;



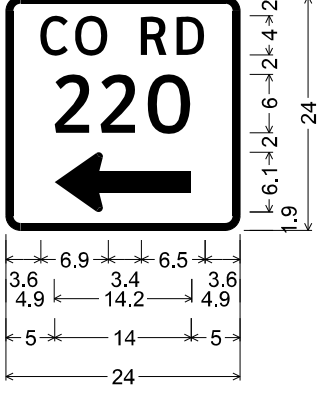
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 "Big Lake", ClearviewHwy-3-W; "47", ClearviewHwy-3-W;



D20-1TR_24x24;
 1.5" Radius, 0.8" Border, White on, Green;
 "CO RD", ClearviewHwy-3-W;
 "280", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0°;



D20-1TL_24x24;
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 "140", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180°;



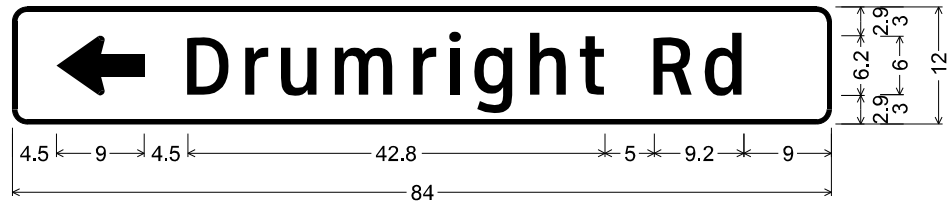
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 "220", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180°;



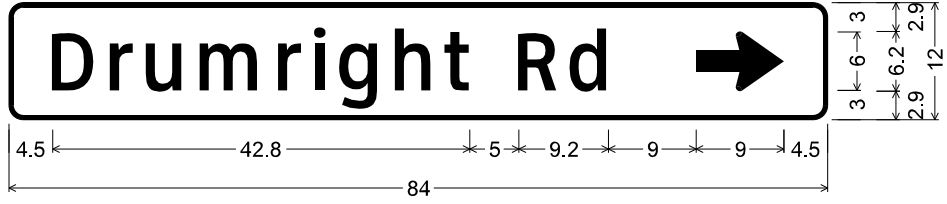
23.06.2021

Texas Department of Transportation		San Angelo District	
REPLACE SIGNS RM 33 GLASSCOCK COUNTY			
SHEET 2 OF 6		NOT TO SCALE	
©TxDOT 2021	CONT 6383	SECT 49	JOB 001
SHEET ISSUED OR LAST REVISED		HIGHWAY US 83,ETC	
12-19		DIST SJT	COUNTY MENARD
		SHEET NO. 80	

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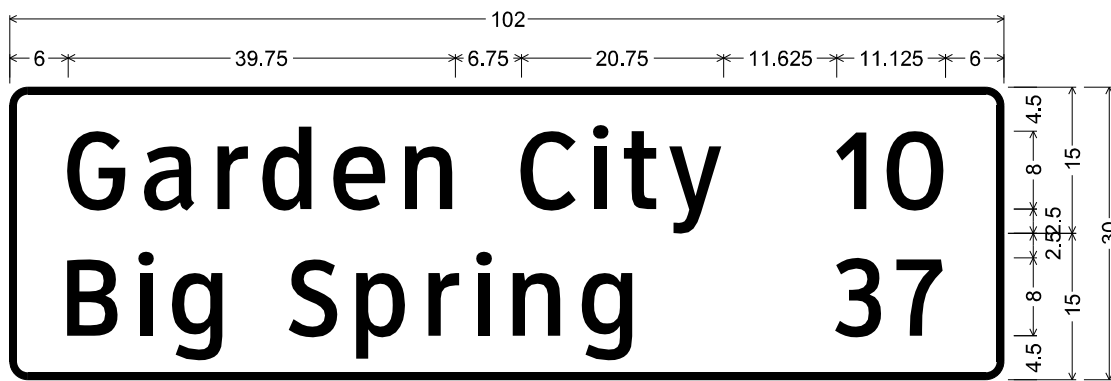
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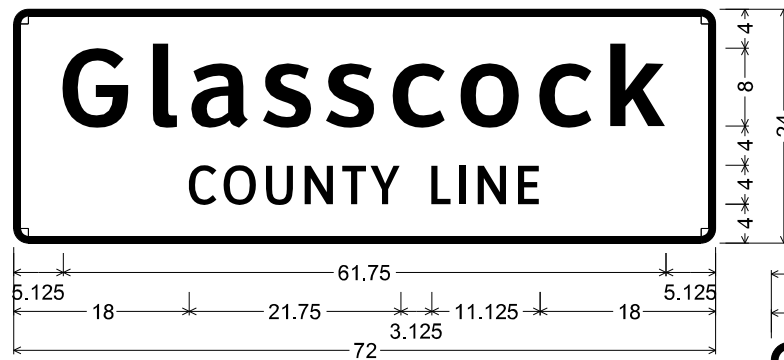
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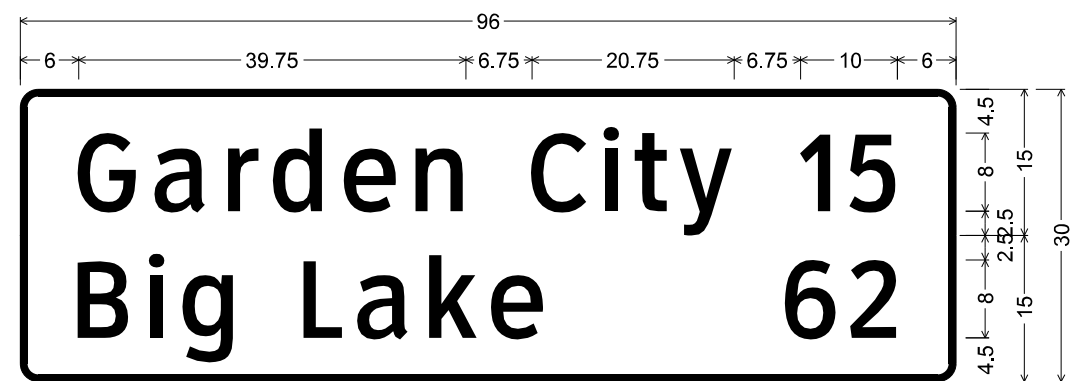
D21-2T_VARx24;
 1.5" Radius, 0.8" Border, White on, Green;
 Standard Arrow Custom 9.0" X 6.1" 180°;
 "Newell Rd", ClearviewHwy-3-W;
 1.5" Radius, 0.8" Border, White on, Green;
 "Sherrod Rd", ClearviewHwy-3-W;
 Standard Arrow Custom 9.0" X 6.1" 0°;



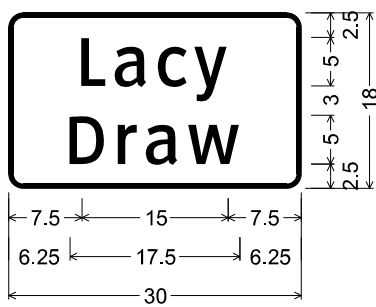
D2-2 8in;
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 "Garden City", ClearviewHwy-3-W; "10", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Big Spring", ClearviewHwy-3-W; "37", ClearviewHwy-3-W;



I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Glasscock", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



D2-2 8in;
 1.875" Radius, 0.750" Border, White on, Green;
 "Garden City", ClearviewHwy-3-W; "15", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Big Lake", ClearviewHwy-3-W; "62", ClearviewHwy-3-W;



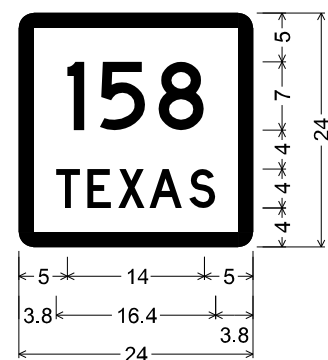
I-3 5in;
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 "Lacy", ClearviewHwy-3-W;
 "Draw", ClearviewHwy-3-W;



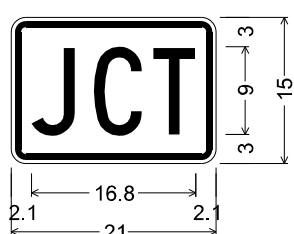
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 FARM, 2D;
 ROAD, 2D;
 461, 5D;



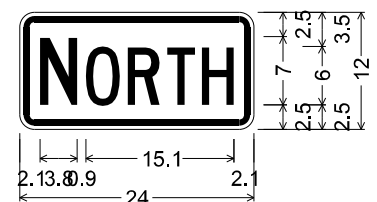
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 ROAD, 2D;
 33, 6D;



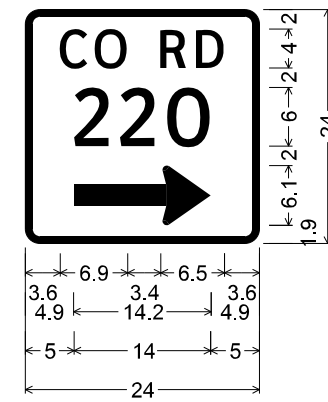
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 "158", D;
 "TEXAS", D;



M2-1_21x15;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "JCT", C;



M3-1_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "NORTH", C 65% spacing;



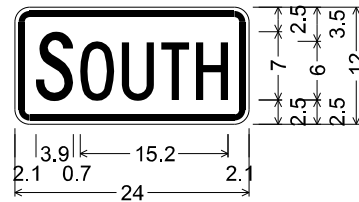
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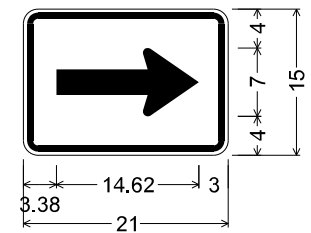
23.06.2021

		San Angelo District	
REPLACE SIGNS RM 33 GLASSCOCK COUNTY			
SHEET 3 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 DIST SJT	SECT 49 COUNTY MENARD	JOB 001 HIGHWAY US 83, ETC SHEET NO. 81
12-19			

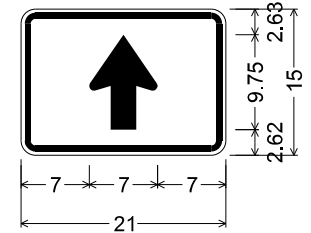
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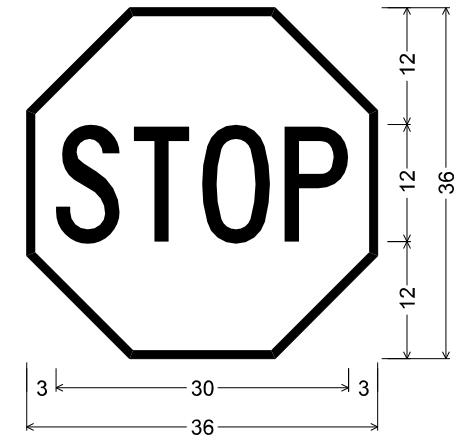
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 "SOUTH", C 65% spacing;



M6-1_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-1;



M6-3_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-3;



R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "35", E specified length;



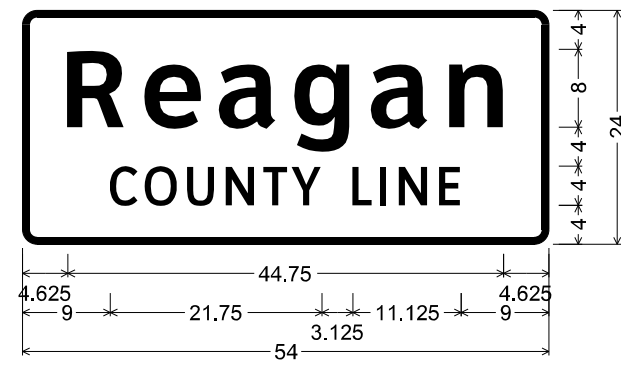
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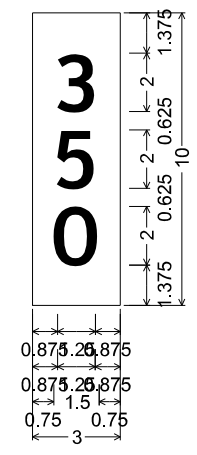
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 "LIMIT", E specified length;
 "60", E specified length;



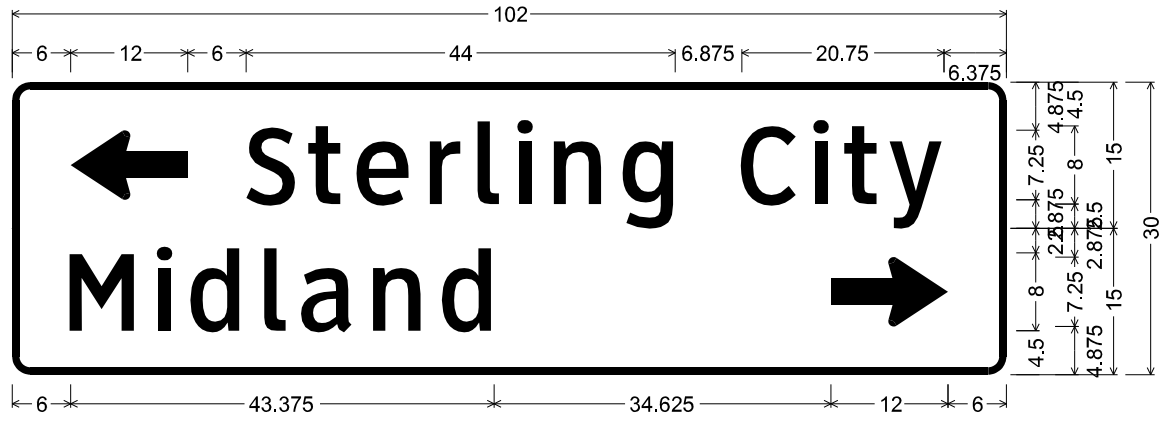
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 "LIMIT", E specified length;
 "75", E specified length;



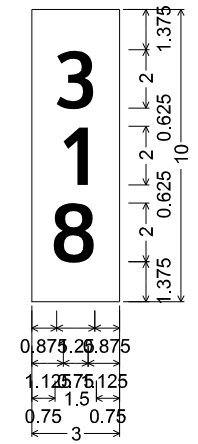
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Reagan", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



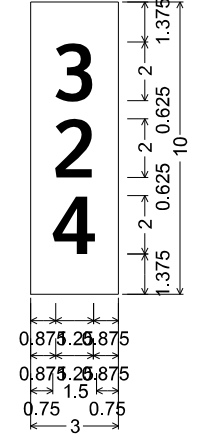
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 "3", ClearviewHwy-4-W;
 "5", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



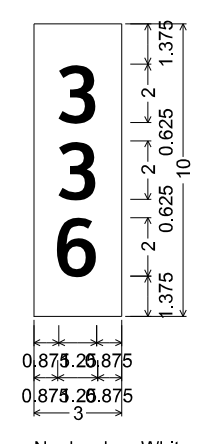
D1-2 8in LT-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180"; "Sterling City", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Midland", ClearviewHwy-3-W; Standard Arrow Custom 12.000" X 7.125" 0";



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "1", ClearviewHwy-4-W;
 "8", ClearviewHwy-4-W;



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;



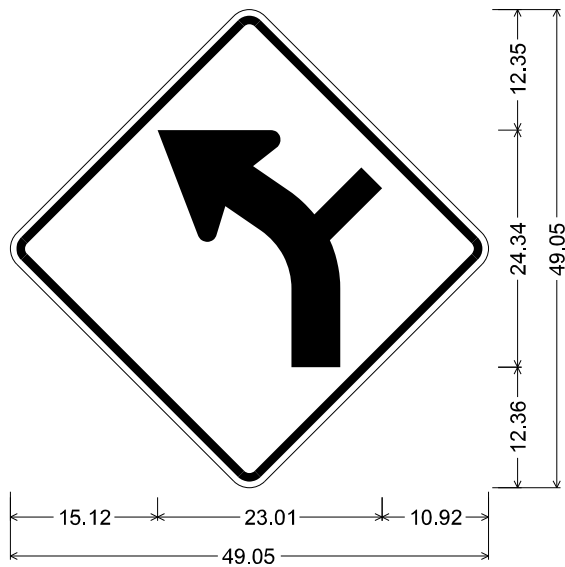
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 "6", ClearviewHwy-4-W;



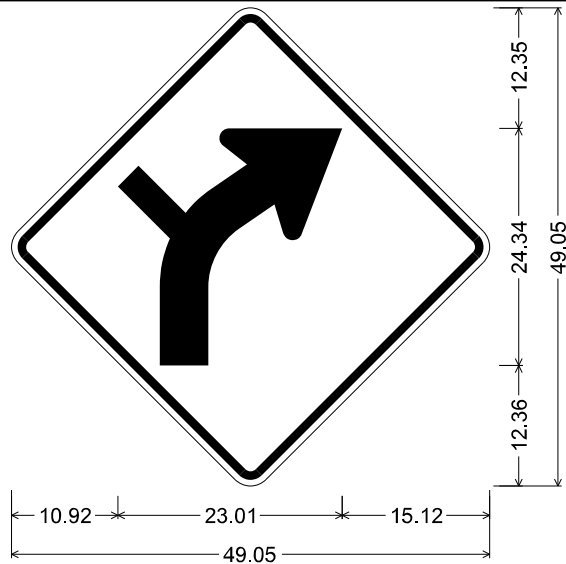
23.06.2021

REPLACE SIGNS RM 33 GLASSCOCK COUNTY			
SHEET 4 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
HIGHWAY US 83,ETC	SHEET NO. 82		

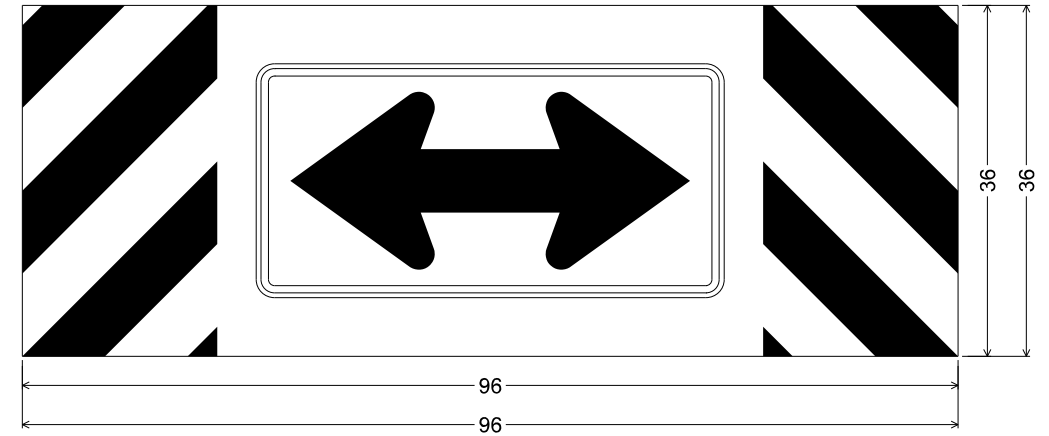
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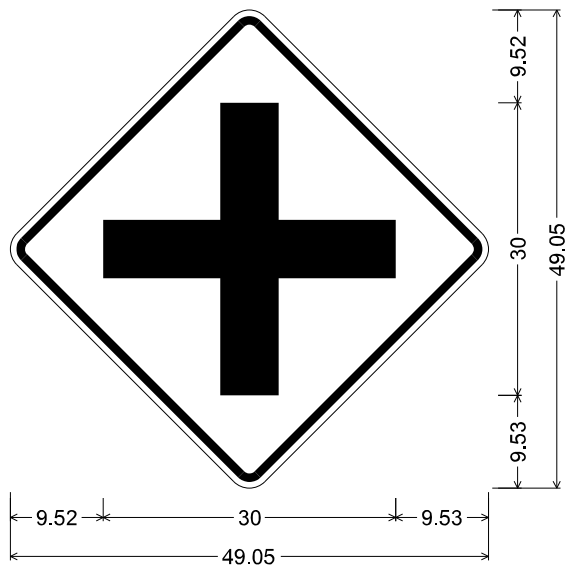
W1-10L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W1-10;



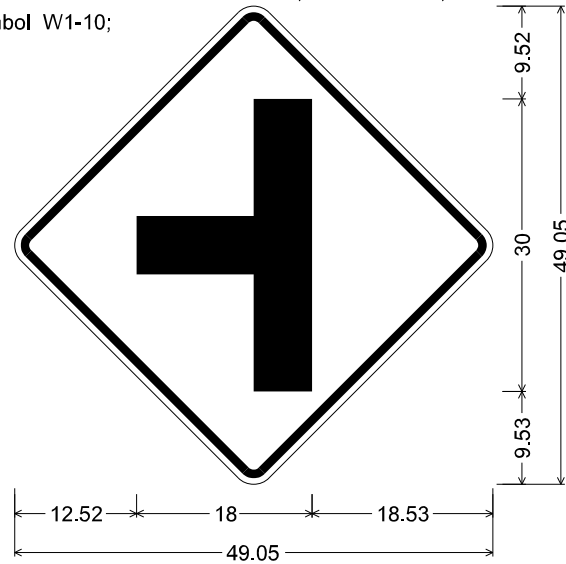
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 Symbol W1-10;



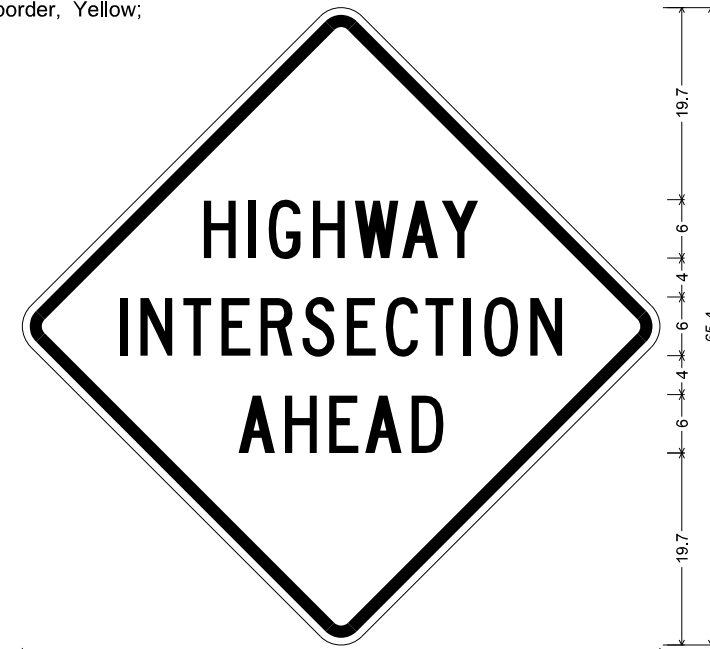
W1-7T_96x36;
 No border, Yellow;



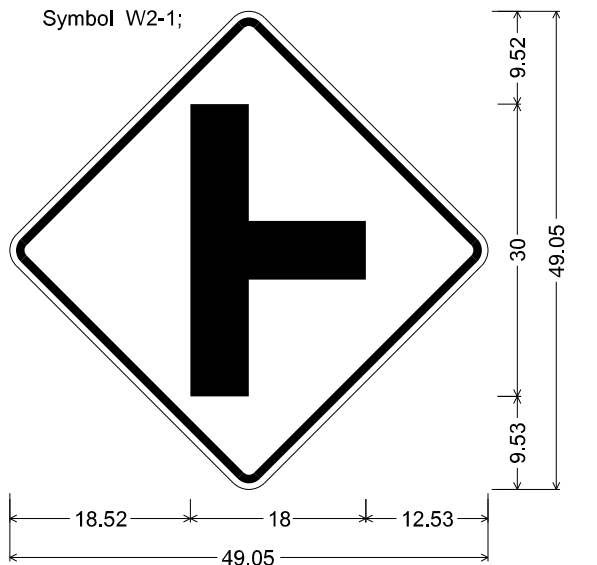
W2-1_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W2-1;



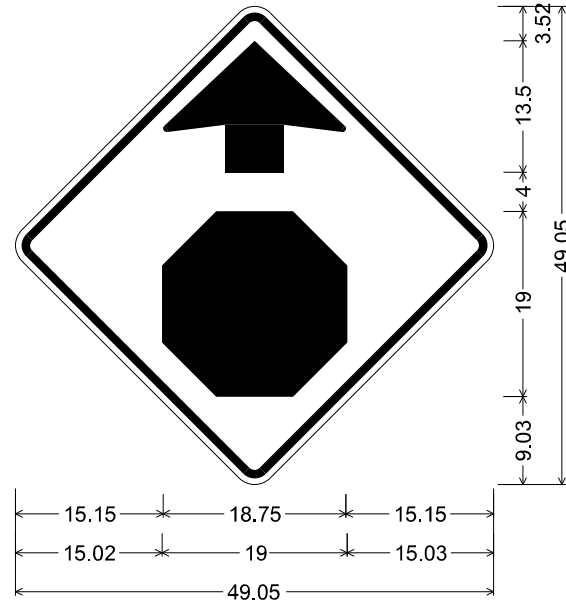
W2-2L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W2-2;



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



W2-2R_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W2-2;



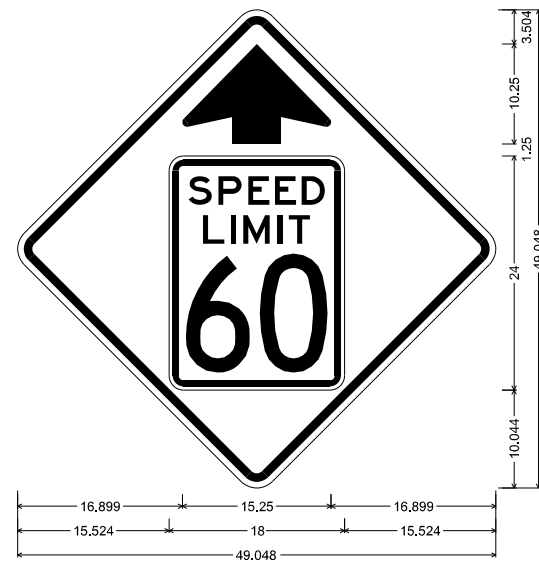
W3-1_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Down Arrow Custom - 13.50" 90';
 Symbol R1-1;



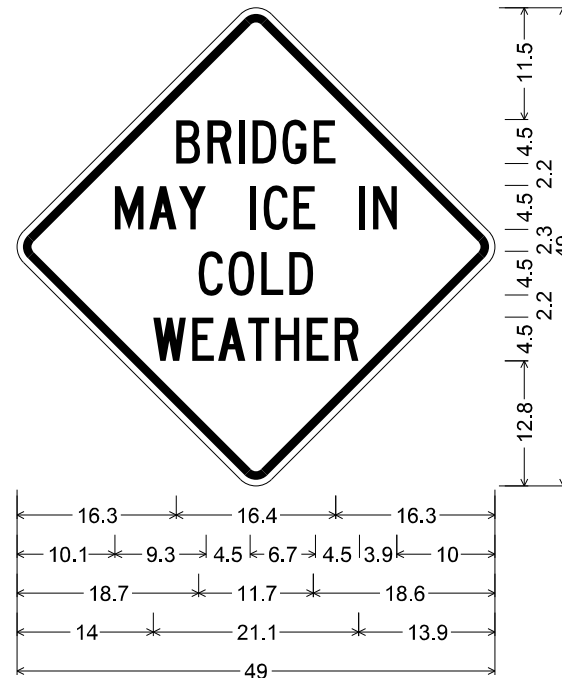
23.06.2021

		San Angelo District	
REPLACE SIGNS RM 33 GLASSCOCK COUNTY			
SHEET 5 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 83
12-19			

DATE: 6/23/2021 10:44:15 AM
 FILE: pw:\txdot\projectwiseonline.com\T\XDOT2\Documents\07 - SJT\Maintenance\Projects\638349001\2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT_RM_33_GLASSCOCK COUNTY-6.dgn



W3-5_36x36;
 36.000" across sides 2.250" Radius, 0.875" Border, 0.625" Indent, Black on, Yellow;
 Down Arrow Custom - 10.250" 90°;



W8-13aT_36x36;
 36.0" across sides 2.3" Radius, 0.9" Border, 0.6" Indent, Black on, Yellow;
 "BRIDGE", C 80% spacing;
 "MAY ICE IN", C 80% spacing;
 "COLD", C 80% spacing;
 "WEATHER", C 80% spacing;

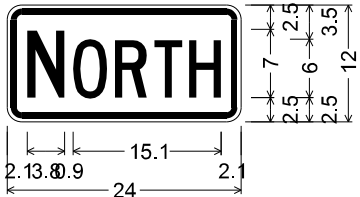


23.06.2021

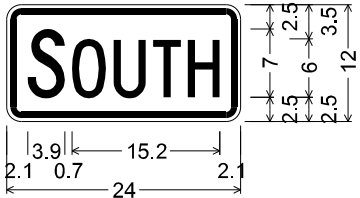
		San Angelo District	
REPLACE SIGNS RM 33 GLASSCOCK COUNTY			
SHEET 6 OF 6		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
12-19	DIST SJT	COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 84

DATE: 6/23/2021 10:44:21 AM
 FILE: \\ttdot.projectwiseonline.com\T\DOT2\Documents\07 - SJT\Maintenance Projects\638349001\2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT_RM_1357_GLASSCOCK_COUNTY.dgn

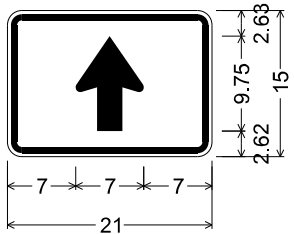
RM 1357 from Reagan County Line to Midland County Line											0636-6007	0644-6103	644-6104
Existing Location											REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & 5 TY 10 BWG (1)(P)	REPLACE SRS & 5 TY 10 BWG (1)(T)
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	SF	EA	EA
1	31.76090769	-101.7755106	Glasscock	RM 1357	SB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1	
2	31.68555496	-101.7514889	Glasscock	RM 1357	SB	W2-2L	T Intersection	36"x36"	Replace Sign		9		
3	31.66034721	-101.7430453	Glasscock	RM 1357	SB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign & Support				1
4	31.65949919	-101.742769	Glasscock	RM 1357	SB	M2-1	JCT	21"x15"	Replace Sign & Support	Yes		1	
4A	31.65949919	-101.742769	Glasscock	RM 1357	SB	M1-6R	RM 2401	24"x24"					
5	31.65693347	-101.7418946	Glasscock	RM 1357	SB	D1-1	Midkiff Right Arrow	72"x18"	Replace Sign		9		
6	31.6553983	-101.7413897	Glasscock	RM 1357	SB	M1-6R	RM 1357	24"x24"	Replace Sign	Yes	4		
6A	31.6553983	-101.7413897	Glasscock	RM 1357	SB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2		
6B	31.6553983	-101.7413897	Glasscock	RM 1357	SB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2		
7	31.65419651	-101.7409841	Glasscock	RM 1357	SB	M3-3	South	24"x12"	Replace Sign	Yes	2		
7A	31.65419651	-101.7409841	Glasscock	RM 1357	SB	M1-6R	RM 1357	24"x24"	Replace Sign	Yes	4		
8	31.65332369	-101.7406937	Glasscock	RM 1357	SB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1	
9	31.65145918	-101.7400943	Glasscock	RM 1357	SB	I-2dT	Reagan County Line	54"x24"	Replace Sign	Yes	6.75		
10	31.65145918	-101.7400943	Glasscock	RM 1357	NB	I-2dT	Glasscock County Line	72"x24"	Replace Sign & Support				1
11	31.653368	-101.740545	Glasscock	RM 1357	NB	D1-1	Midkiff Left Arrow	72"x18"	Replace Sign & Support				1
12	31.655727	-101.741342	Glasscock	RM 1357	NB	M3-1	NORTH	24"x12"	Replace Sign	Yes	2		
12A	31.655727	-101.741342	Glasscock	RM 1357	NB	M1-6R	RM 1357	24"x24"	Replace Sign	Yes	4		
13	31.656576	-101.741621	Glasscock	RM 1357	NB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1	
14	31.6820943	-101.750315	Glasscock	RM 1357	NB	D20-1TR	Stop Sign	36"x36"	Replace Sign		9		
15	31.733709	-101.766591	Glasscock	RM 1357	NB	M1-6R	RM 1357	24"x24"	Replace Sign	Yes	4		
15A	31.733709	-101.766591	Glasscock	RM 1357	NB	D10-7aT	TRM 340	3"x10"	Replace Sign	Yes	0.21		
15B	31.733709	-101.766591	Glasscock	RM 1357	NB	D10-7aT	TRM 340	3"x10"	Replace Sign	Yes	0.21		
16	31.761663	-101.775587	Glasscock	RM 1357	NB	M1-6R	RM 1357	24"x24"	Replace Sign		4		
17	31.761663	-101.775587	Glasscock	RM 1357	NB	I-2dT	Midland County Line	60"x24"	Replace Sign & Support				1
TOTAL											62.57	4	4



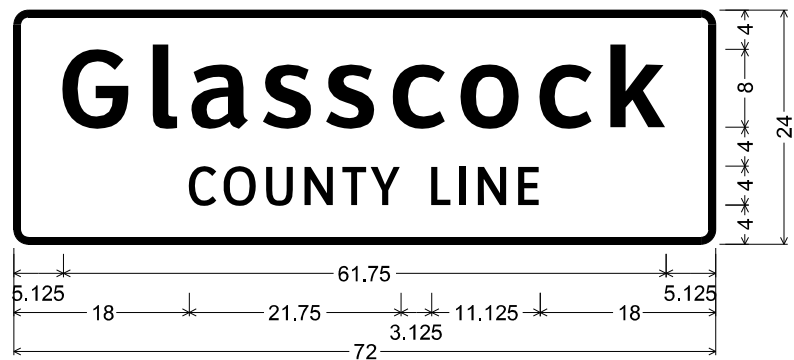
M3-1_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "NORTH", C 65% spacing;



M3-3_24x12;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "SOUTH", C 65% spacing;



M6-3_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-3;



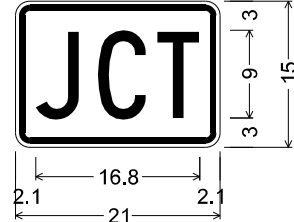
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Glasscock", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



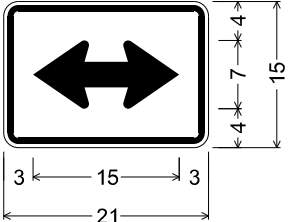
M1-6R_24x24;
 1.5" Radius, No border, Black;
 RANCH, 2D;
 ROAD, 2D;
 1357, 4D;



M1-6R_24x24;
 1.5" Radius, No border, Black;
 RANCH, 2D;
 ROAD, 2D;
 2401, 4D;



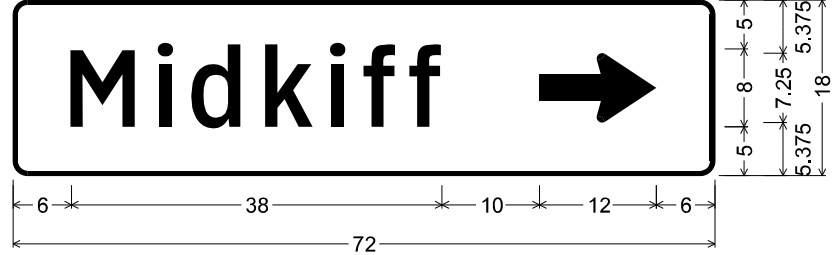
M2-1_21x15;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "JCT", C;



M6-4_21x15;
 1.50" Radius, 0.63" Border, 0.38" Indent, Black on, White;
 Arrow M6-4;



D1-1 8in LT;
 1.50" Radius, 0.50" Border, White on, Green;
 Standard Arrow Custom 12.00" X 7.13" 180';
 "Midkiff", ClearviewHwy-3-W;



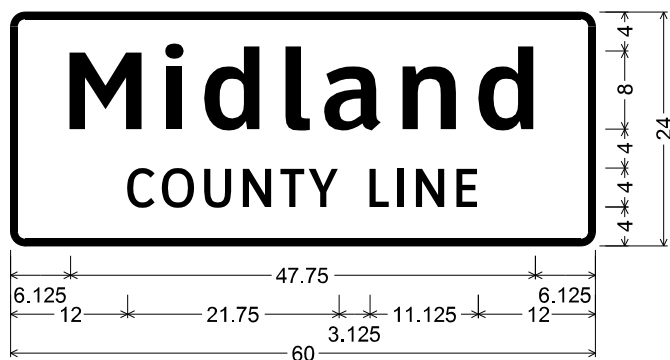
D1-1 8in RT;
 1.500" Radius, 0.500" Border, White on, Green;
 "Midkiff", ClearviewHwy-3-W;
 Standard Arrow Custom 12.000" X 7.125" 0';



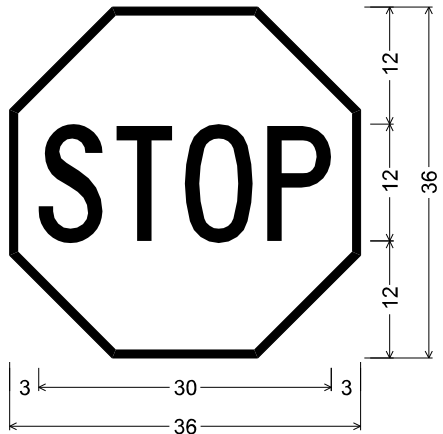
23.06.2021

Texas Department of Transportation		San Angelo District	
REPLACE SIGNS RM 1357 GLASSCOCK COUNTY			
SHEET 1 OF 2		NOT TO SCALE	
CONT	SECT	JOB	HIGHWAY
6383	49	001	US 83,ETC
SHEET ISSUED OR LAST REVISED		DIST	COUNTY
12-19		SJT	MENARD
			SHEET NO. 85

DATE: 6/23/2021 10:44:27 AM
 FILE: pw:\txdot\projectwiseonline.com\T\XDOT2\Documents\07 - SJT\Maintenance\Projects\638349001\2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT_RM_1357_GLASSCOCK_COUNTY-2.dgn



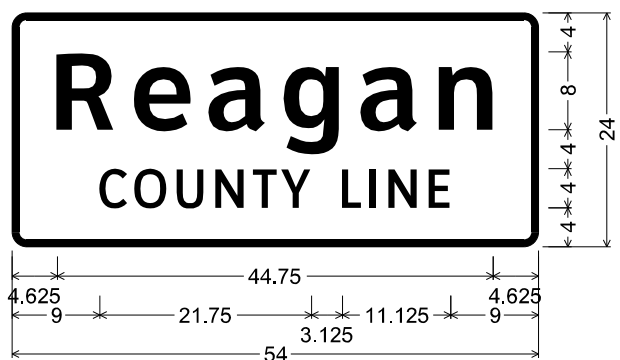
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Midland", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



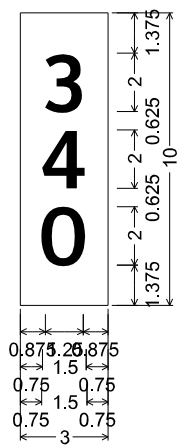
R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



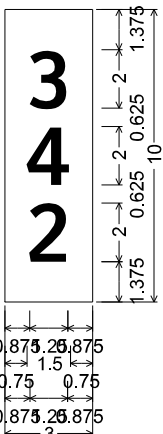
R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



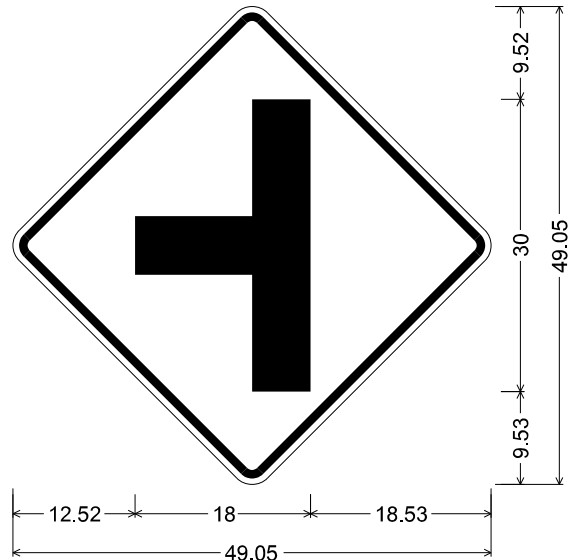
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Reagan", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



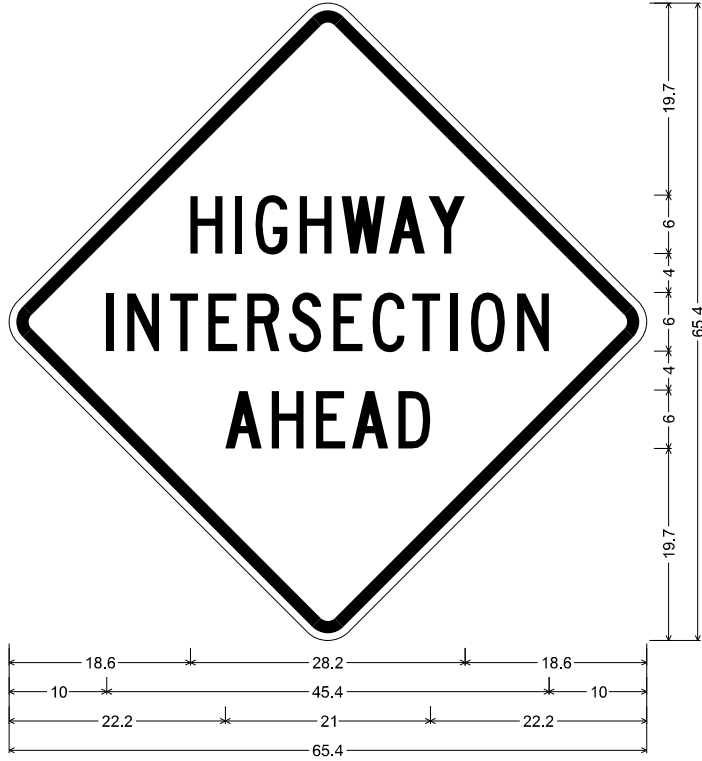
No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;
 "2", ClearviewHwy-4-W;



W2-2L_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Symbol W2-2;



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



23.06.2021

		San Angelo District	
REPLACE SIGNS RM 1357 GLASSCOCK COUNTY			
SHEET 2 OF 2		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
		HIGHWAY US 83,ETC	SHEET NO. 86

DATE: 6/23/2021 10:44:33 AM FILE: \\pwwork\proj\wis\online\T:\DOT2\Documents\07 - SJT\Maintenance\Projects\63834901\2 - DESIGN FILES\Plan Sheets\SIGNS\SIGN REPLACEMENT_RM_2401_GLASSCOCK_COUNTY.dgn

RM 2401 from RM 33 to Reagan County Line Existing Location										0636-6007	0644-6103	644-6104	644-6112	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)	REPLACE SRS & S TY S80 (1)(U-EXAL)
											SF	EA	EA	EA
1	31.72034267	-101.4626693	Glasscock	RM 2401	WB	M3-4	WEST	24"x12"	Replace Sign	Yes	2			
1A	31.72034267	-101.4626693	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
2	31.72016661	-101.4633748	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
3	31.72010083	-101.4636663	Glasscock	RM 2401	WB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1		
4	31.70553026	-101.526818	Glasscock	RM 2401	WB	D21-1TR	Paymaster Rd Right Arrow	84"x12"	Replace Sign					
5	31.70535487	-101.5278104	Glasscock	RM 2401	WB	R1-1	Stop Sign	36"x36"	Replace Sign	Yes	9			
6	31.7026744	-101.5393215	Glasscock	RM 2401	WB	W11-10L	Truck Crossing	36"x36"	Replace Sign	Yes	9			
7	31.708841	-101.511653	Glasscock	RM 2401	WB	R1-1	Stop Sign	36"x36"	Replace Sign	Yes	9			
8	31.69528086	-101.5720604	Glasscock	RM 2401	WB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
8A	31.69528086	-101.5720604	Glasscock	RM 2401	WB	M1-6R	RM 3093	24"x24"	Replace Sign	Yes	4			
9	31.69408867	-101.5769627	Glasscock	RM 2401	WB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2			
9A	31.69408867	-101.5769627	Glasscock	RM 2401	WB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2			
10	31.69406263	-101.5770712	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
10A	31.69406263	-101.5770712	Glasscock	RM 2401	WB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2			
11	31.697862	-101.560708	Glasscock	RM 2401	WB	R1-1	Stop Sign	36"x36"	Replace Sign	Yes	9			
12	31.6938766	-101.5778682	Glasscock	RM 2401	WB	M3-4	WEST	24"x12"	Replace Sign	Yes	2			
12A	31.6938766	-101.5778682	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
13	31.6901025	-101.5942223	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
14	31.68024795	-101.6375281	Glasscock	RM 2401	WB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
14A	31.68024795	-101.6375281	Glasscock	RM 2401	WB	M1-6T	SH 137	24"x24"	Replace Sign	Yes	4			
15	31.67959856	-101.6404672	Glasscock	RM 2401	WB	D1-2	Big Lake Left Arrow Stanton Right Arrow	78"x30"	Replace Sign		16.25			
16	31.67912119	-101.6425588	Glasscock	RM 2401	WB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1		
17	31.67903677	-101.6429333	Glasscock	RM 2401	WB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2			
17A	31.67903677	-101.6429333	Glasscock	RM 2401	WB	M1-6T	SH 137	24"x24"	Replace Sign	Yes	4			
17B	31.67903677	-101.6429333	Glasscock	RM 2401	WB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2			
18	31.67892381	-101.6433922	Glasscock	RM 2401	WB	M3-4	WEST	24"x12"	Replace Sign	Yes	2			
18A	31.67892381	-101.6433922	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
19	31.67868426	-101.6444119	Glasscock	RM 2401	WB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5			
20	31.67822019	-101.6464295	Glasscock	RM 2401	WB	D1-2	Midkiff 12	66"x18"	Replace Sign & Support				1	
21	31.67538996	-101.6587901	Glasscock	RM 2401	WB	W8-18	Road May Flood	36"x36"	Replace Sign		9			
22	31.67515759	-101.6598291	Glasscock	RM 2401	WB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
22A	31.67515759	-101.6598291	Glasscock	RM 2401	WB	D10-7aT	TRM 310	3"x10"	Replace Sign	Yes	0.21			
22B	31.67515759	-101.6598291	Glasscock	RM 2401	WB	D10-7aT	TRM 310	3"x10"	Replace Sign	Yes	0.21			
23	31.67177492	-101.6749754	Glasscock	RM 2401	WB	W8-18	Road May Flood	36"x36"	Replace Sign		9			
24	31.66349408	-101.7058477	Glasscock	RM 2401	WB	W2-1	Cross Intersection	36"x36"	Replace Sign	No	9			
25	31.66264641	-101.7079858	Glasscock	RM 2401	WB	D21-1TL2	Tank Farm Rd Left	84"x12"	Replace Sign & Support	Yes			1	
25A	31.66264641	-101.7079858	Glasscock	RM 2401	WB	D20-1TR	CO RD 195 Right	24"x24"						
26	31.694066	-101.577391	Glasscock	RM 2401	WB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
27	31.65636213	-101.7359063	Glasscock	RM 2401	WB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
28	31.65569172	-101.7390153	Glasscock	RM 2401	WB	D1-1	Midkiff Up Arrow	66"x18"	Replace Sign & Support				1	
29	31.65516491	-101.7414937	Glasscock	RM 2401	WB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2			
29A	31.65516491	-101.7414937	Glasscock	RM 2401	WB	M6-1L	Left Arrow/White	21"x15"	Replace Sign	Yes	2.2			
29B	31.65516491	-101.7414937	Glasscock	RM 2401	WB	M6-1R	Right Arrow/White	21"x15"	Replace Sign	Yes	2.2			
30	31.65484295	-101.7429272	Glasscock	RM 2401	WB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1		
31	31.6543916	-101.7449419	Glasscock	RM 2401	WB	D2-1	Midkiff 6 mi	66"x18"	Replace Sign		8.25			
32	31.651579	-101.7576285	Glasscock	RM 2401	WB	I-2dT	Reagan County Line	54"x24"	Replace Sign & Support				1	
33	31.65142093	-101.7575714	Glasscock	RM 2401	EB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
34	31.65366414	-101.7474698	Glasscock	RM 2401	EB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16			
35	31.65544112	-101.7393935	Glasscock	RM 2401	EB	R2-1	Speed Limit 75	30"x36"	Replace Sign		7.5			
36	31.66149803	-101.7121268	Glasscock	RM 2401	EB	W2-1	Cross Intersection	36"x36"	Replace Sign & Support			1		
37	31.66202162	-101.7098779	Glasscock	RM 2401	EB	D21-1TR	Tank Farm Rd Right	84"x12"	Replace Sign & Support	Yes			1	
37A	31.66202162	-101.7098779	Glasscock	RM 2401	EB	D20-1TL	CO RD 195 Left	24"x24"						
38	31.662306	-101.708752	Glasscock	RM 2401	EB	R1-1	Stop Sign	36"x36"	Replace Sign		9			
39	31.66778891	-101.6919779	Glasscock	RM 2401	EB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
40	31.67820169	-101.6457891	Glasscock	RM 2401	EB	W3-1	Stop Sign Ahead	36"x36"	Replace Sign & Support			1		
41	31.67843676	-101.6447741	Glasscock	RM 2401	EB	D1-2	Stanton Left Arrow Big Lake Right Arrow	78"x30"	Replace Sign		16.25			
42	31.67888125	-101.6428651	Glasscock	RM 2401	EB	R1-1	Stop Sign	36"x36"	Replace Sign & Support			1		
43	31.67894032	-101.6424898	Glasscock	RM 2401	EB	M6-3	Up Arrow/White	21"x15"	Replace Sign	Yes	2.2			
43A	31.67894032	-101.6424898	Glasscock	RM 2401	EB	M6-4	Double Arrow/White	21"x15"	Replace Sign	Yes	2.2			
44	31.67905397	-101.6419898	Glasscock	RM 2401	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2			
44A	31.67905397	-101.6419898	Glasscock	RM 2401	EB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
45	31.67928277	-101.6409819	Glasscock	RM 2401	EB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1		
46	31.67975112	-101.6389616	Glasscock	RM 2401	EB	D2-1	St Lawrence 6	96"x18"	Replace Sign & Support			1		
TOTAL											241.77	8	5	0

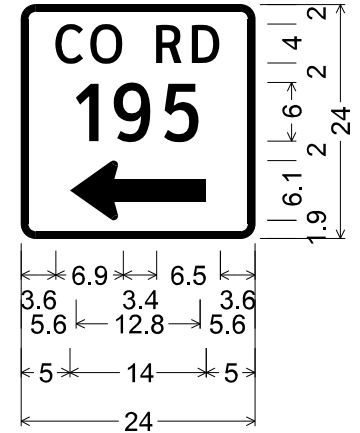


23.06.2021

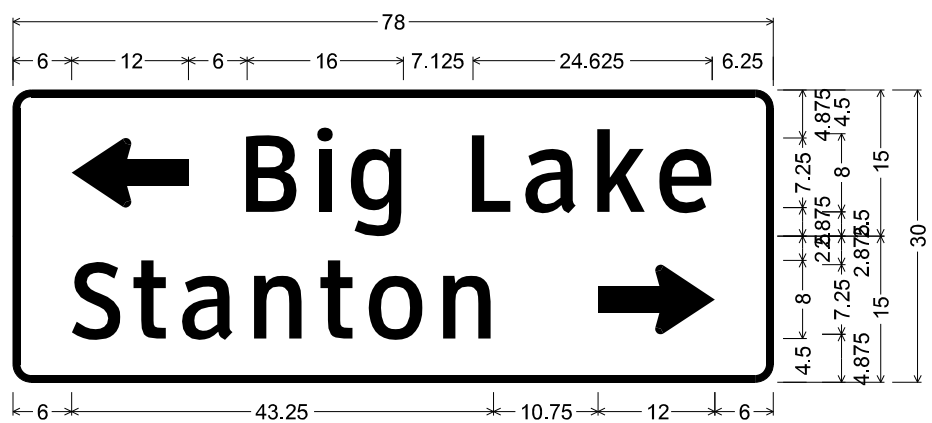
		San Angelo District																									
REPLACE SIGNS RM 2401 GLASSCOCK COUNTY																											
SHEET 1 OF 4		NOT TO SCALE																									
<table border="1" style="font-size: small;"> <tr> <td>CTxDOT</td> <td>2021</td> <td>CONT</td> <td>SECT</td> <td>JOB</td> <td>HIGHWAY</td> </tr> <tr> <td colspan="2">SHEET ISSUED OR LAST REVISED</td> <td>6383</td> <td>49</td> <td>001</td> <td>US 83,ETC</td> </tr> <tr> <td colspan="2" style="text-align: center;">12-19</td> <td>DIST</td> <td>COUNTY</td> <td colspan="2">SHEET NO.</td> </tr> <tr> <td colspan="2"></td> <td>SJT</td> <td>MENARD</td> <td colspan="2" style="text-align: center;">87</td> </tr> </table>	CTxDOT	2021	CONT	SECT	JOB	HIGHWAY	SHEET ISSUED OR LAST REVISED		6383	49	001	US 83,ETC	12-19		DIST	COUNTY	SHEET NO.				SJT	MENARD	87				
CTxDOT	2021	CONT	SECT	JOB	HIGHWAY																						
SHEET ISSUED OR LAST REVISED		6383	49	001	US 83,ETC																						
12-19		DIST	COUNTY	SHEET NO.																							
		SJT	MENARD	87																							

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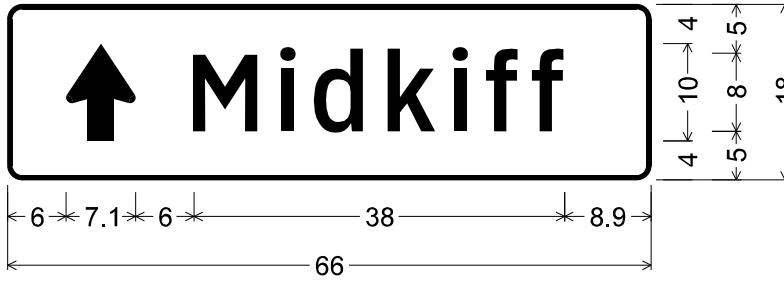
RM 2401 from RM 33 to Reagan County Line Existing Location										0636-6007	0644-6103	644-6104	644-6112	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1XP)	REPLACE SRS & S TY 10 BWG (1XT)	REPLACE SRS & S TY S80 (1XU-EXAL)
47	31.68241073	-101.6271064	Glasscock	RM 2401	EB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	SF	EA	EA	EA
48	31.69267498	-101.5823235	Glasscock	RM 2401	EB	M1-6R	RM 3093	24"x24"	Replace Sign	Yes	4			
49	31.69403731	-101.5764625	Glasscock	RM 2401	EB	M3-2	EAST	24"x12"	Replace Sign	Yes	2			
49A	31.69403731	-101.5764625	Glasscock	RM 2401	EB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	4			
50	31.69689296	-101.5641015	Glasscock	RM 2401	EB	W11-10L	Truck Crossing	36"x36"	Replace Sign	Yes	9			
51	31.70493973	-101.5286117	Glasscock	RM 2401	EB	D1-1	Paymaster Rd Left Arrow	84"x12"	Replace Sign		7			
52	31.71880537	-101.4682038	Glasscock	RM 2401	EB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16			
53	31.71907251	-101.4671008	Glasscock	RM 2401	EB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2			
54	31.71956152	-101.4650304	Glasscock	RM 2401	EB	W3-1	Stop Sign Ahead	36"x36"	Replace Sign & Support			1		
55	31.7198089	-101.4639983	Glasscock	RM 2401	EB	D1-2	Gaden City Left Big Lake Right	102"x30"	Replace Sign & Support					1
56	31.72025584	-101.4621282	Glasscock	RM 2401	EB	R1-1	Stop Sign	36"x36"	Replace Sign & Support					1
TOTAL											48.2	2	0	1



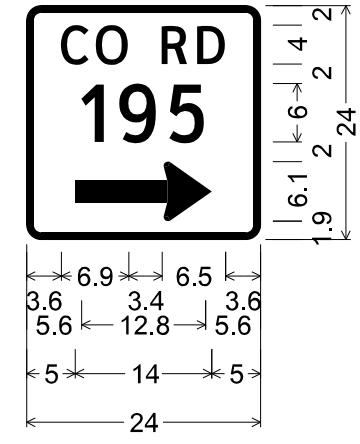
D20-1TL_24x24;
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 "195", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 180';



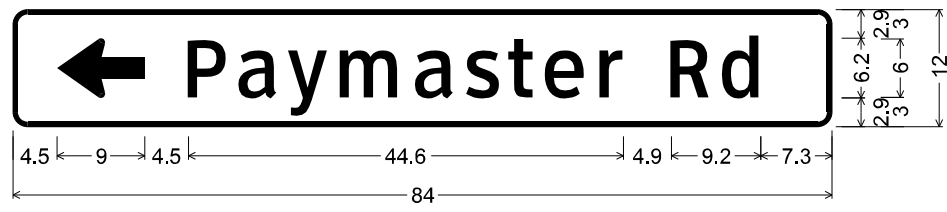
D1-2 8in LT-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180';
 "Big Lake", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Stanton", ClearviewHwy-3-W;
 Standard Arrow Custom 12.000" X 7.125" 0';



D1-1 8in UP;
 1.5" Radius, 0.5" Border, White on, Green;
 Standard Arrow Custom 10.0" X 7.1" 90';
 "Midkiff", ClearviewHwy-3-W;



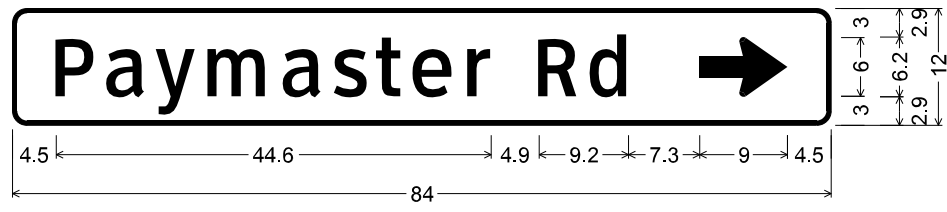
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 "CO RD", ClearviewHwy-3-W;
 "195", ClearviewHwy-3-W;
 Standard Arrow Custom 14.0" X 6.1" 0';



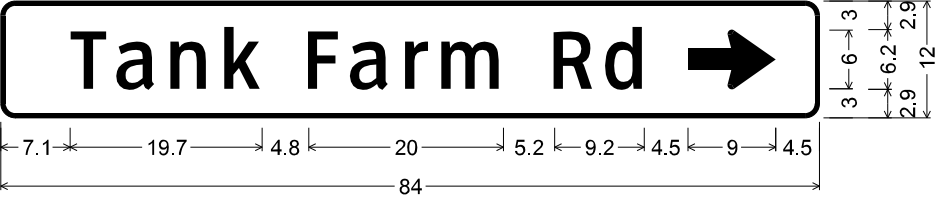
D21-1TL_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 Standard Arrow Custom 9.0" X 6.1" 180';
 "Paymaster Rd", ClearviewHwy-3-W;



D21-1TL_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 Standard Arrow Custom 9.0" X 6.1" 180';
 "Tank Farm Rd", ClearviewHwy-3-W;



D21-1TR_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 "Paymaster Rd", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0';



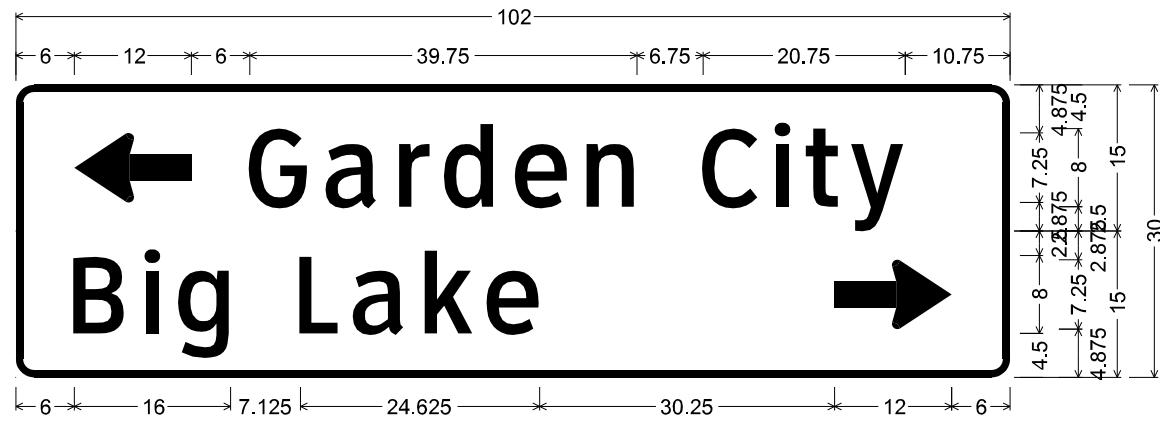
D21-1TR_VARx12;
 1.5" Radius, 0.5" Border, White on, Green;
 "Tank Farm Rd", ClearviewHwy-3-W; Standard Arrow Custom 9.0" X 6.1" 0';



23.06.2021

		San Angelo District	
REPLACE SIGNS RM 2401 GLASSCOCK COUNTY			
SHEET 2 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
HIGHWAY US 83,ETC	SHEET NO. 88		

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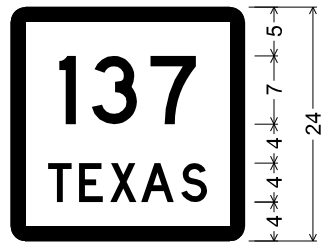
D1-2 8in LT-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180"; "Garden City", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Big Lake", ClearviewHwy-3-W; Standard Arrow Custom 12.000" X 7.125" 0';



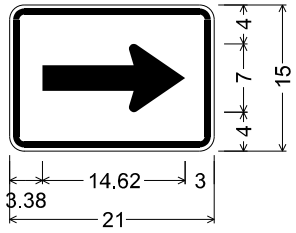
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 ROAD, 2D;
 2401, 4D;



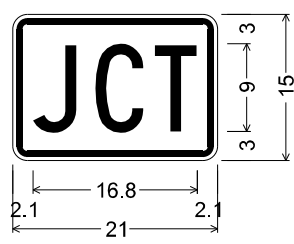
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 ROAD, 2D;
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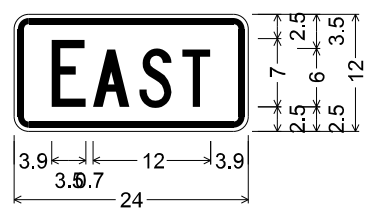
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 "137", D;
 "TEXAS", D;



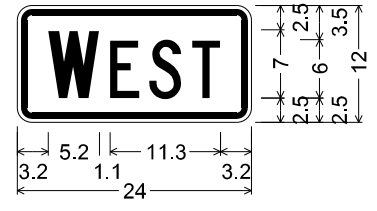
M6-1_21x15;
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 Arrow M6-1;



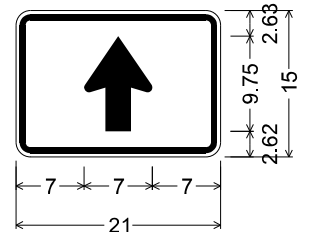
M2-1_21x15;
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 "JCT", C;



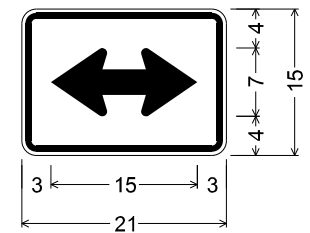
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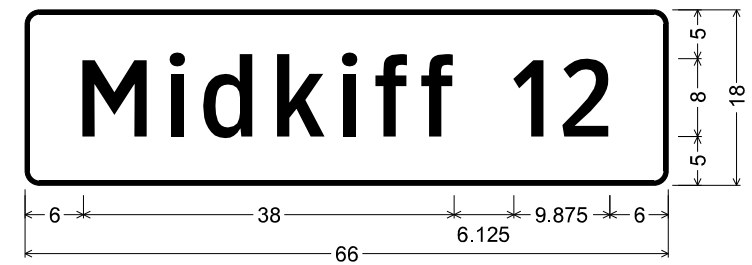
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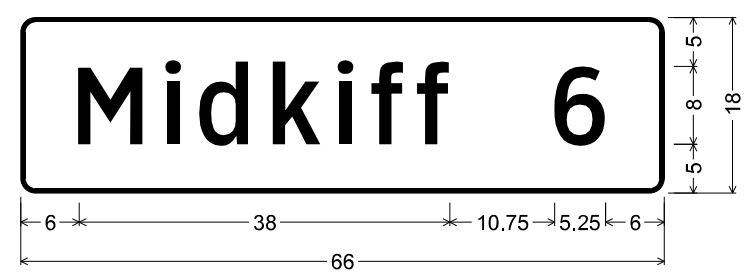
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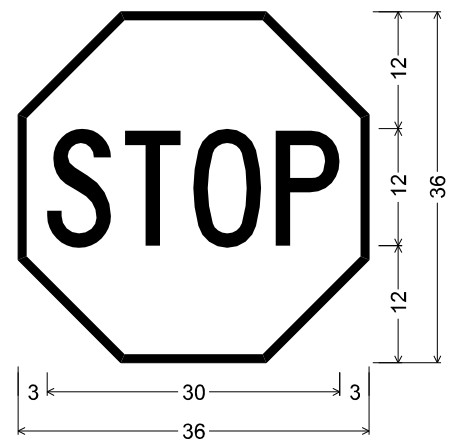
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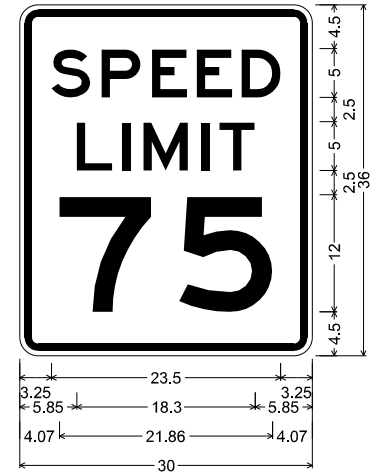
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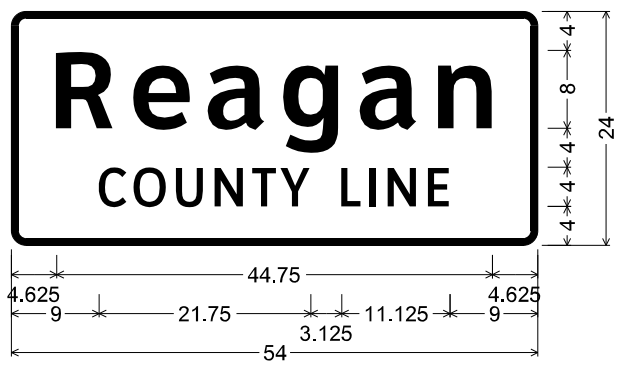
D2-1 8in;
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 "Midkiff", ClearviewHwy-3-W; "6", ClearviewHwy-3-W;



R1-1_36x36;
 0.9" Border, White on, Red;
 "STOP", C 60% spacing;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



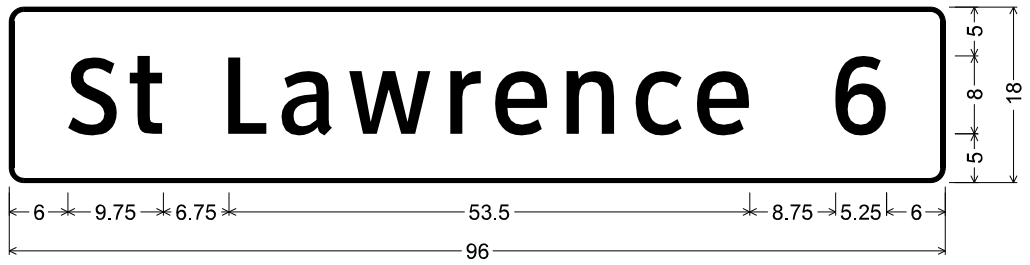
I-2d 8in;
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 "Reagan", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



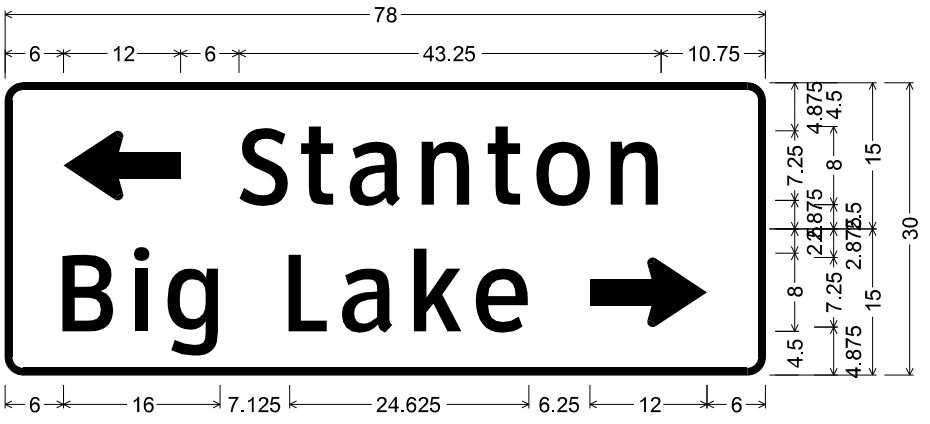
23.06.2021

		San Angelo District	
REPLACE SIGNS RM 2401 GLASSCOCK COUNTY			
SHEET 3 OF 4		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED	CONT 6383 SECT 49	JOB 001 COUNTY MENARD	HIGHWAY US 83,ETC SHEET NO. 89
12-19			

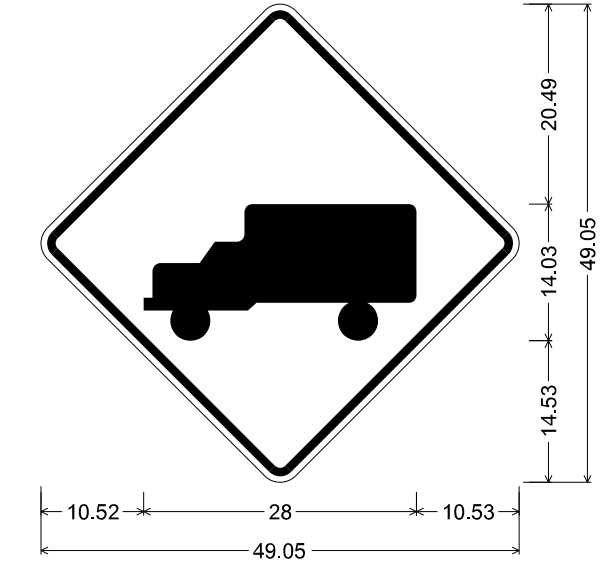
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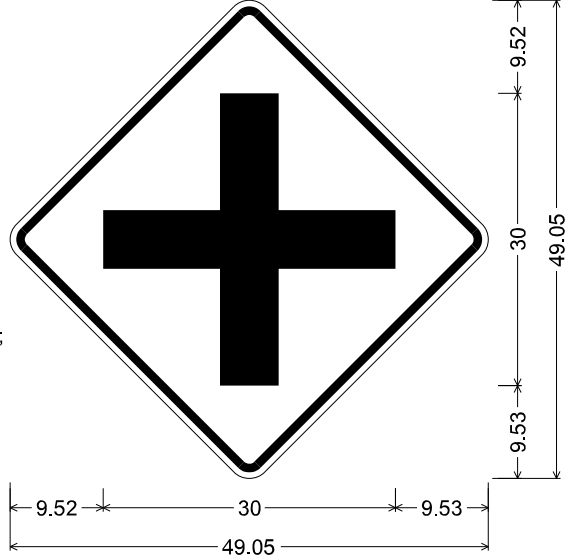
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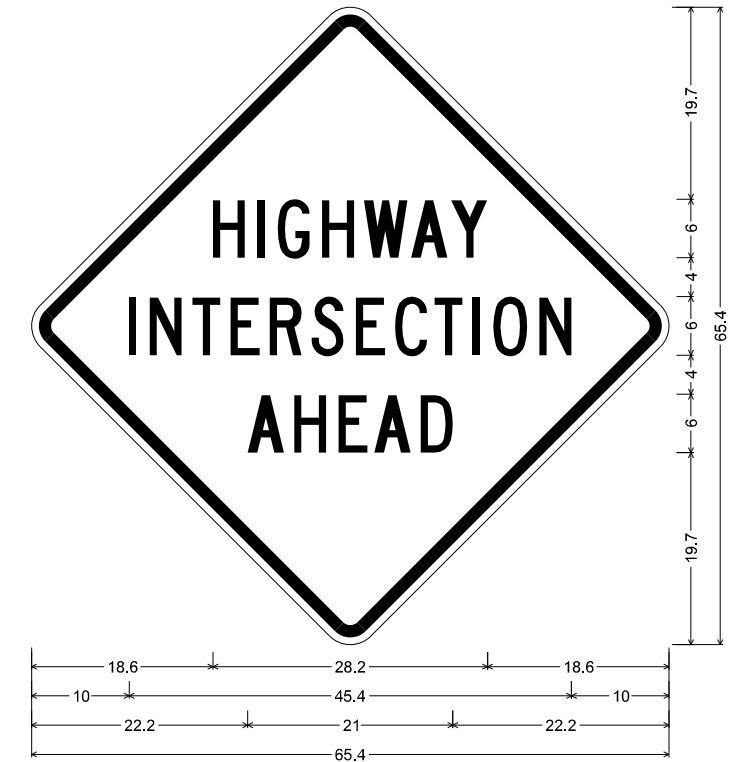
D1-2 8in LT-RT;
 1.875" Radius, 0.750" Border, White on, Green;
 Standard Arrow Custom 12.000" X 7.125" 180';
 "Stanton", ClearviewHwy-3-W;
 1.875" Radius, 0.750" Border, White on, Green;
 "Big Lake", ClearviewHwy-3-W;
 Standard Arrow Custom 12.000" X 7.125" 0';



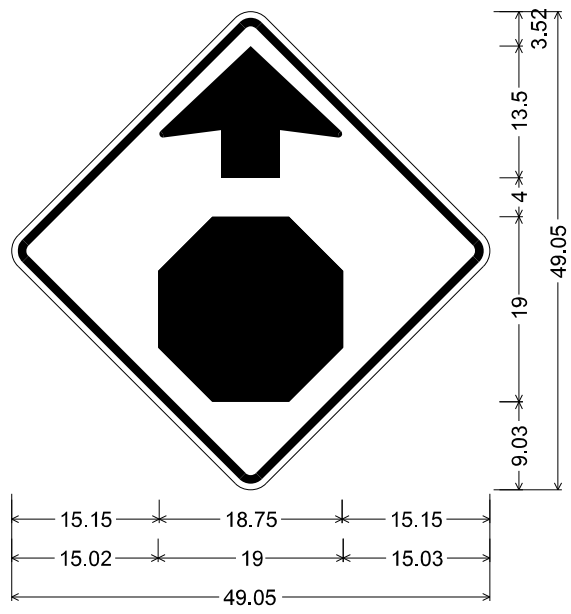
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 Symbol W11-10;



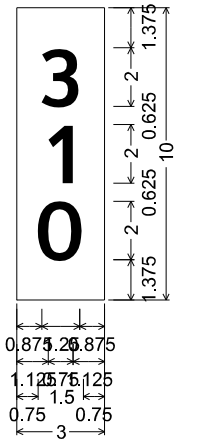
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 Symbol W2-1;



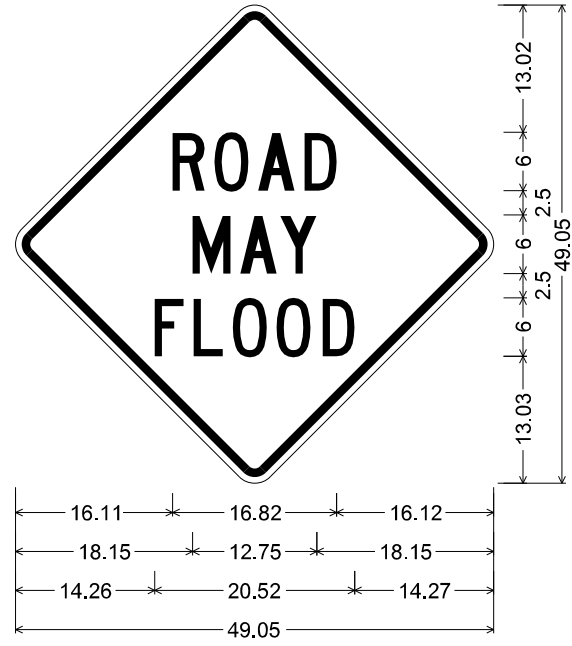
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 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



W3-1_36x36;
 36.00" across sides 2.25" Radius, 0.88" Border, 0.63" Indent, Black on, Yellow;
 Down Arrow Custom - 13.50" 90';
 Symbol R1-1;



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "1", ClearviewHwy-4-W;
 "0", ClearviewHwy-4-W;



W8-18_36x36;
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 "ROAD", C; "MAY", C; "FLOOD", C;

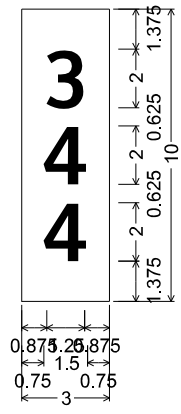


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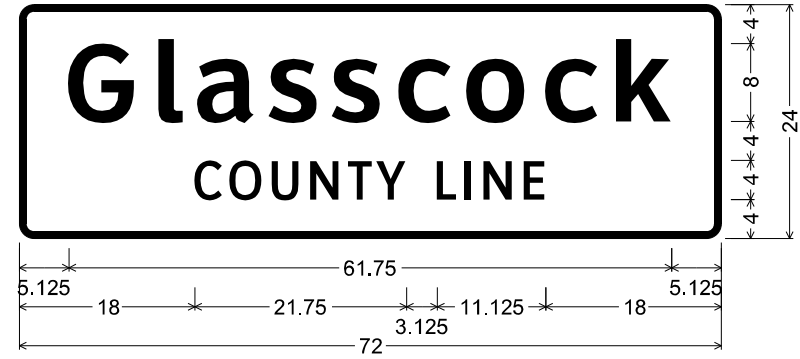
		San Angelo District	
REPLACE SIGNS RM 2401 GLASSCOCK COUNTY			
SHEET 4 OF 4		NOT TO SCALE	
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12-19		90	

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RM 3093 from Reagan County Line to RM 2401 Existing Location										0636-6007	0644-6103	644-6104	
Sign	Latitude	Longitude	County	Location	Direction	Sign Designation	Description	Size	Work Required	Sign Tree	REPLACE EXISTING ALUMINUM SIGNS (TY A)	REPLACE SRS & S TY 10 BWG (1)(P)	REPLACE SRS & S TY 10 BWG (1)(T)
											SF	EA	EA
1	31.6921527	-101.5766366	Glasscock	RM 3093	SB	R2-1	Speed Limit 75	30"x36"	Replace Sign & Support			1	
2	31.65318762	-101.5643531	Glasscock	RM 3093	SB	M1-6R	RM 3093	24"x24"	Replace Sign	Yes	4		
2A	31.65318762	-101.5643531	Glasscock	RM 3093	SB	D10-7aT	TRM 346	3"x10"	Replace Sign	Yes	0.21		
2B	31.65318762	-101.5643531	Glasscock	RM 3093	SB	D10-7aT	TRM 346	3"x10"	Replace Sign	Yes	0.21		
3	31.65318793	-101.5643632	Glasscock	RM 3093	SB	I-2dT	Reagan County Line	54"x24"	Replace Sign	Yes	9		
4	31.65323122	-101.5641671	Glasscock	RM 3093	NB	I-2dT	Glasscock County Line	72"x24"	Replace Sign & Support				1
5	31.68196928	-101.5730884	Glasscock	RM 3093	NB	M1-6R	RM 3093	24"x24"	Replace Sign & Support	Yes		1	
5A	31.68196928	-101.5730884	Glasscock	RM 3093	NB	D10-7aT	TRM 344	3"x10"					
5B	31.68196928	-101.5730884	Glasscock	RM 3093	NB	D10-7aT	TRM 344	3"x10"					
6	31.68877897	-101.5753391	Glasscock	RM 3093	NB	W2-1aT	Highway Intersection Ahead	48"x48"	Replace Sign		16		
7	31.68962926	-101.5756329	Glasscock	RM 3093	NB	M2-1	JCT	21"x15"	Replace Sign	Yes	2.2		
7A	31.68962926	-101.5756329	Glasscock	RM 3093	NB	M1-6R	RM 2401	24"x24"	Replace Sign	Yes	2.2		
TOTAL											33.82	2	1



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;



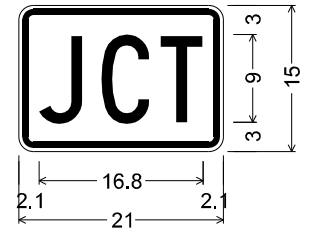
I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Glasscock", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



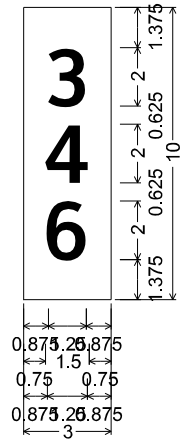
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 RANCH, 2D;
 ROAD, 2D;
 2401, 4D;



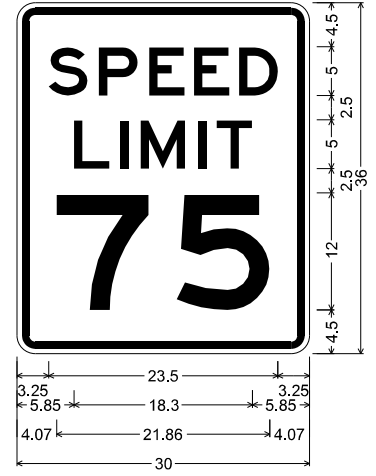
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 3093, 4D;



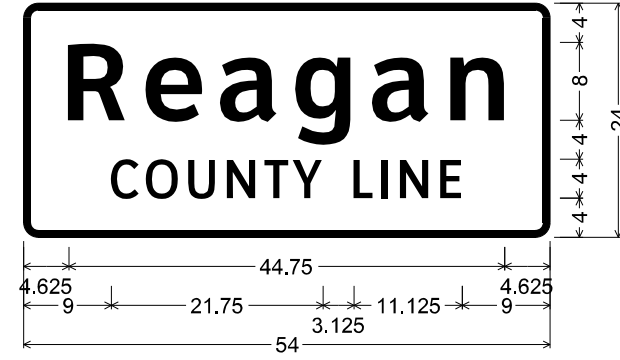
M2-1_21x15;
 1.5" Radius, 0.6" Border, 0.4" Indent, Black on, White;
 "JCT", C;



No border, White on, Green;
 "3", ClearviewHwy-4-W;
 "4", ClearviewHwy-4-W;
 "6", ClearviewHwy-4-W;



R2-1_30x36;
 1.88" Radius, 0.75" Border, 0.50" Indent, Black on, White;
 "SPEED", E specified length;
 "LIMIT", E specified length;
 "75", E specified length;



I-2d 8in;
 1.500" Radius, 0.750" Border, White on, Green;
 "Reagan", ClearviewHwy-5-W-R;
 "COUNTY LINE", ClearviewHwy-3-W;



W2-1aT(1)_48x48;
 48.0" across sides 3.0" Radius, 1.3" Border, 0.8" Indent, Black on, Yellow;
 "HIGHWAY", C; "INTERSECTION", C; "AHEAD", C;



23.06.2021

		San Angelo District	
REPLACE SIGNS RM 3093 GLASSCOCK COUNTY			
SHEET 1 OF 1		NOT TO SCALE	
©TxDOT 2021 SHEET ISSUED OR LAST REVISED 12-19	CONT 6383	SECT 49	JOB 001 COUNTY MENARD
			HIGHWAY US 83, ETC SHEET NO. 91

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SIGN SUPPORT DESCRIPTIVE CODES

(Descriptive Codes correspond to project estimate and quantities sheets)

SM RD SGN ASSM TY XXXXX(X)XX(X-XXXX)

Post Type

FRP = Fiberglass Reinforced Plastic Pipe (see SMD(FRP))
 TWT = Thin-Walled Tubing (see SMD(TWT))
 10BWG = 10 BWG Tubing (see SMD(SLIP-1) to (SLIP-3))
 S80 = Schedule 80 Pipe (see SMD(SLIP-1) to (SLIP-3))

Number of Posts (1 or 2)

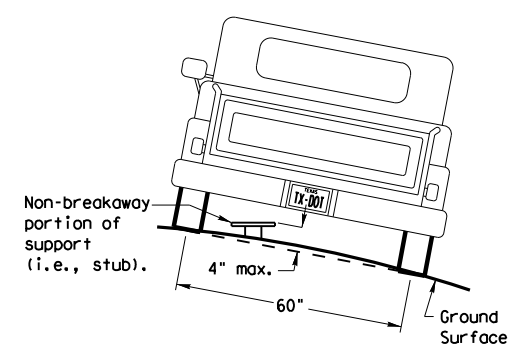
Anchor Type

UA = Universal Anchor - Concreted (see SMD(FRP) and (TWT))
 UB = Universal Anchor - Bolted down (see SMD(FRP) and (TWT))
 WS = Wedge Anchor Steel - (see SMD(TWT))
 WP = Wedge Anchor Plastic (see SMD(TWT))
 SA = Slipbase - Concreted (see SMD(SLIP-1) to (SLIP-3))
 SB = Slipbase - Bolted Down (see SMD(SLIP-1) to (SLIP-3))

Sign Mounting Designation

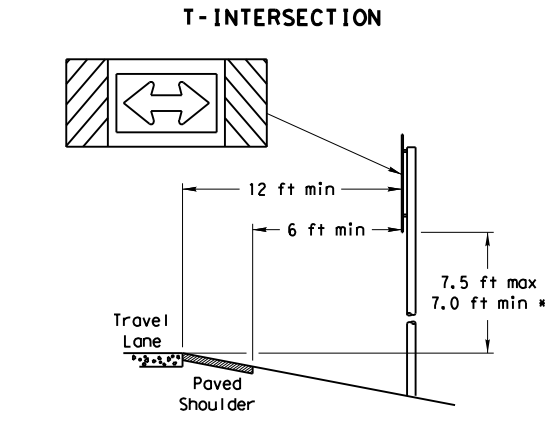
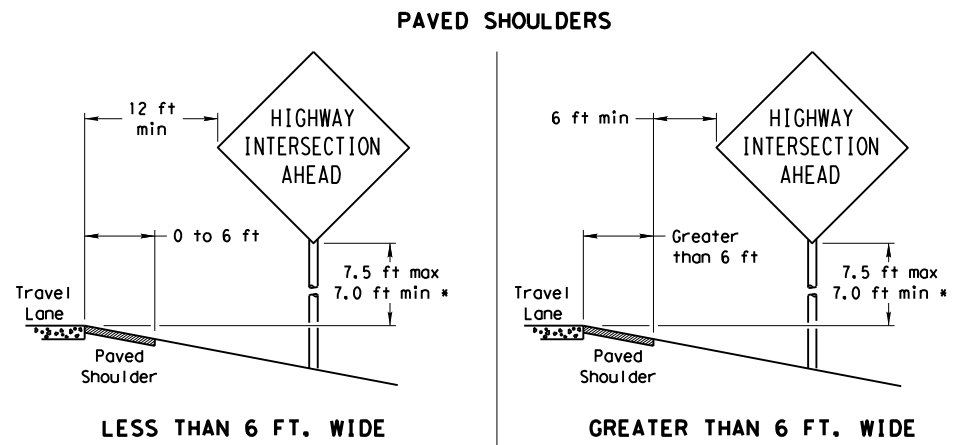
P = Prefab. "Plain" (see SMD(SLIP-1) to (SLIP-3), (TWT), (FRP))
 T = Prefab. "T" (see SMD(SLIP-1) to (SLIP-3), (TWT))
 U = Prefab. "U" (see SMD(SLIP-1) to (SLIP-3))
 IF REQUIRED
 1EXT or 2EXT = Number of Extensions (see SMD(SLIP-1) to (SLIP-3), (TWT))
 BM = Extruded Wind Beam (see SMD(SLIP-1) to (SLIP-3))
 WC = 1.12 #/ft Wing Channel (see SMD(SLIP-1) to (SLIP-3))
 EXAL = Extruded Aluminum Sign Panels (see SMD(SLIP-3))

REQUIRED CLEARANCE FOR BREAKAWAY SUPPORT



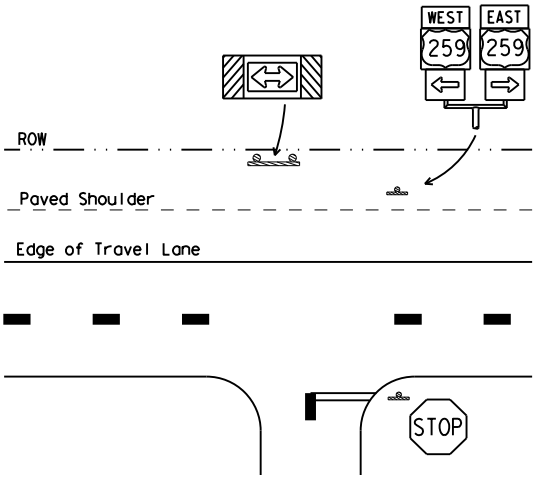
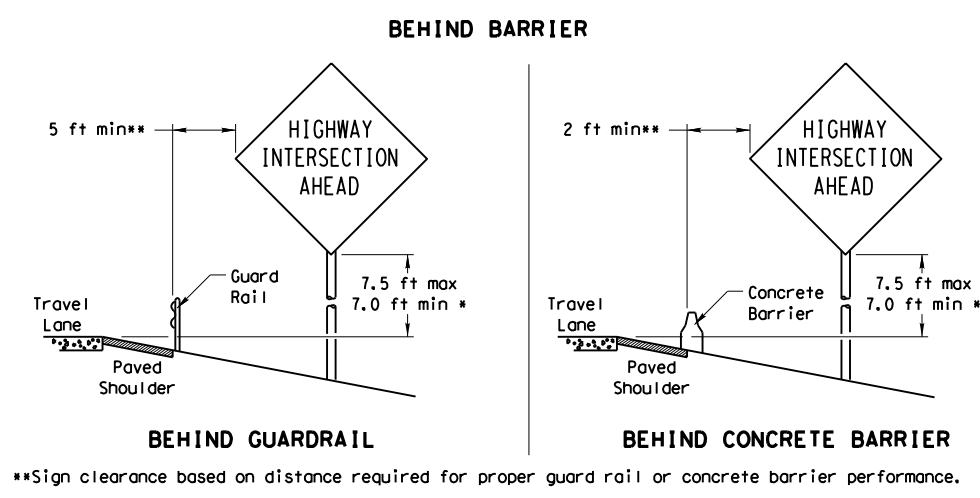
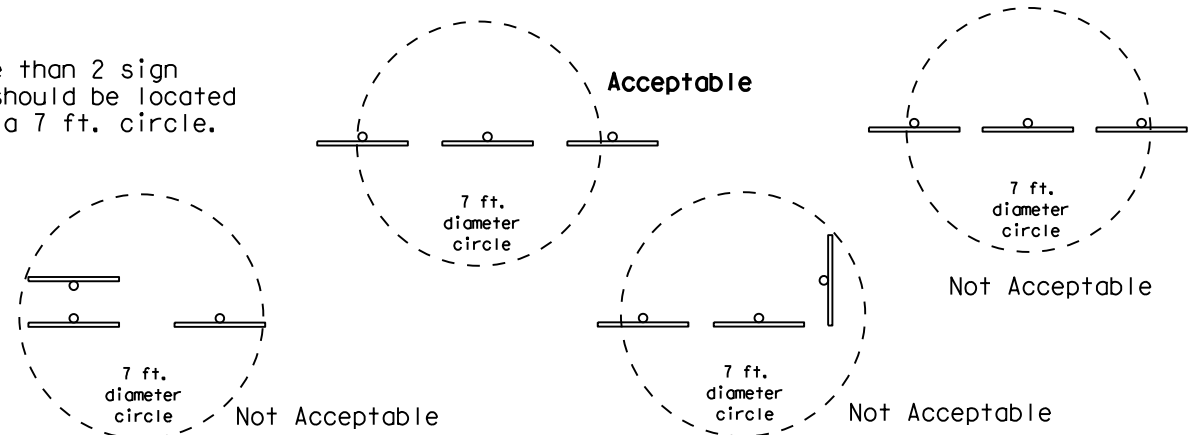
To avoid vehicle undercarriage snagging, any substantial remains of a breakaway support, when it is broken away, should not project more than 4 inches above a 60-inch chord (i.e., typical space between wheel paths).

SIGN LOCATION



When this sign is needed at the end of a two-lane, two way roadway, the right edge of the sign should be in line with the centerline of the roadway. Place as close to ROW as practical.

No more than 2 sign posts should be located within a 7 ft. circle.



* Signs shall be mounted using the following condition that results in the greatest sign elevation:

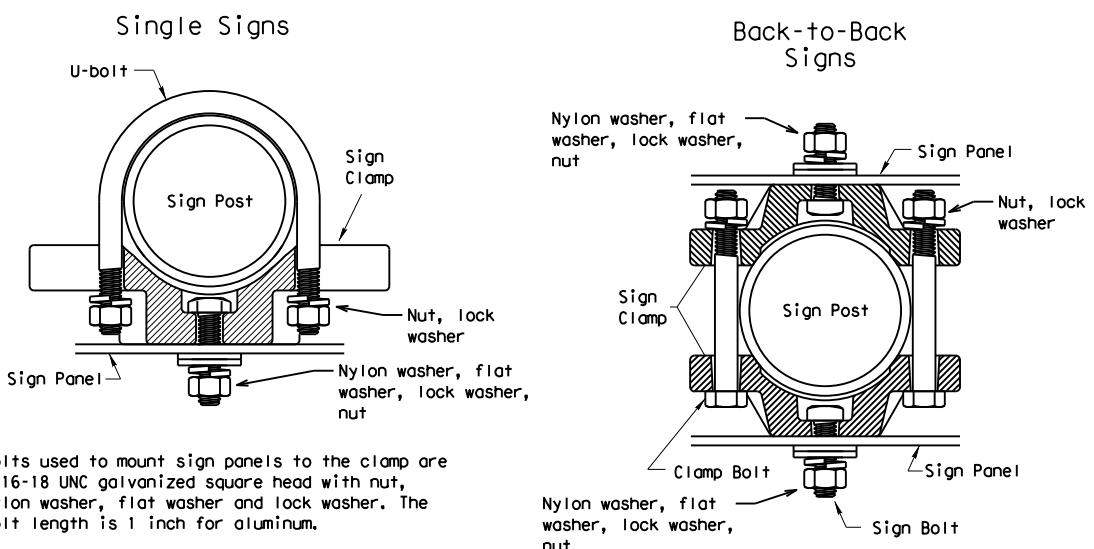
- a minimum of 7 to a maximum of 7.5 feet above the edge of the travel lane or
- a minimum of 7 to a maximum of 7.5 feet above the grade at the base of the support when sign is installed on the backslope.

The maximum values may be increased when directed by the Engineer.

See the Traffic Operations Division website for detailed drawings of sign clamps, Triangular Slipbase System components and Wedge Anchor System components.

The website address is:
<http://www.txdot.gov/publications/traffic.htm>

TYPICAL SIGN ATTACHMENT DETAIL



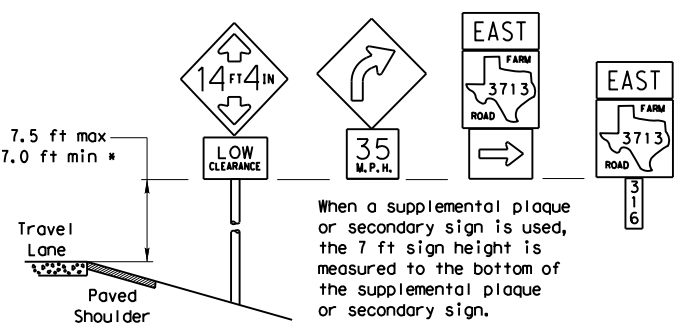
Bolts used to mount sign panels to the clamp are 5/16-18 UNC galvanized square head with nut, nylon washer, flat washer and lock washer. The bolt length is 1 inch for aluminum.

When two sign clamps are used to mount signs back-to-back, use a 5/16-18 UNC galvanized hex head per ASTM A307 with nut and helical-spring lock washer. The approximate bolt lengths for various post sizes and sign clamp types are given in the table at right. The bolt length may need to be adjusted depending upon field conditions.

Sign clamps may be either the specific size clamp or the universal clamp.

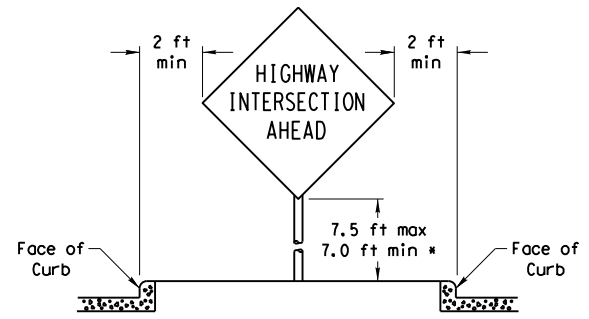
Pipe Diameter	Approximate Bolt Length	
	Specific Clamp	Universal Clamp
2" nominal	3"	3 or 3 1/2"
2 1/2" nominal	3 or 3 1/2"	3 1/2 or 4"
3" nominal	3 1/2 or 4"	4 1/2"

SIGNS WITH PLAQUES

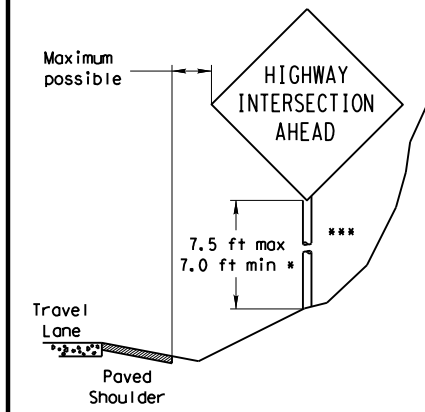


When a supplemental plaque or secondary sign is used, the 7 ft sign height is measured to the bottom of the supplemental plaque or secondary sign.

CURB & GUTTER OR RAISED ISLAND



RESTRICTED RIGHT-OF-WAY (When 6 ft min. is not possible.)



Right-of-way restrictions may be created by rocks, water, vegetation, forest, buildings, a narrow island, or other factors.

In situations where a lateral restriction prevents the minimum horizontal clearance from the edge of the travel lane, signs should be placed as far from the travel lane as practical.

*** Post may be shorter if protected by guardrail or if Engineer determines the post could not be hit due to extreme slope.



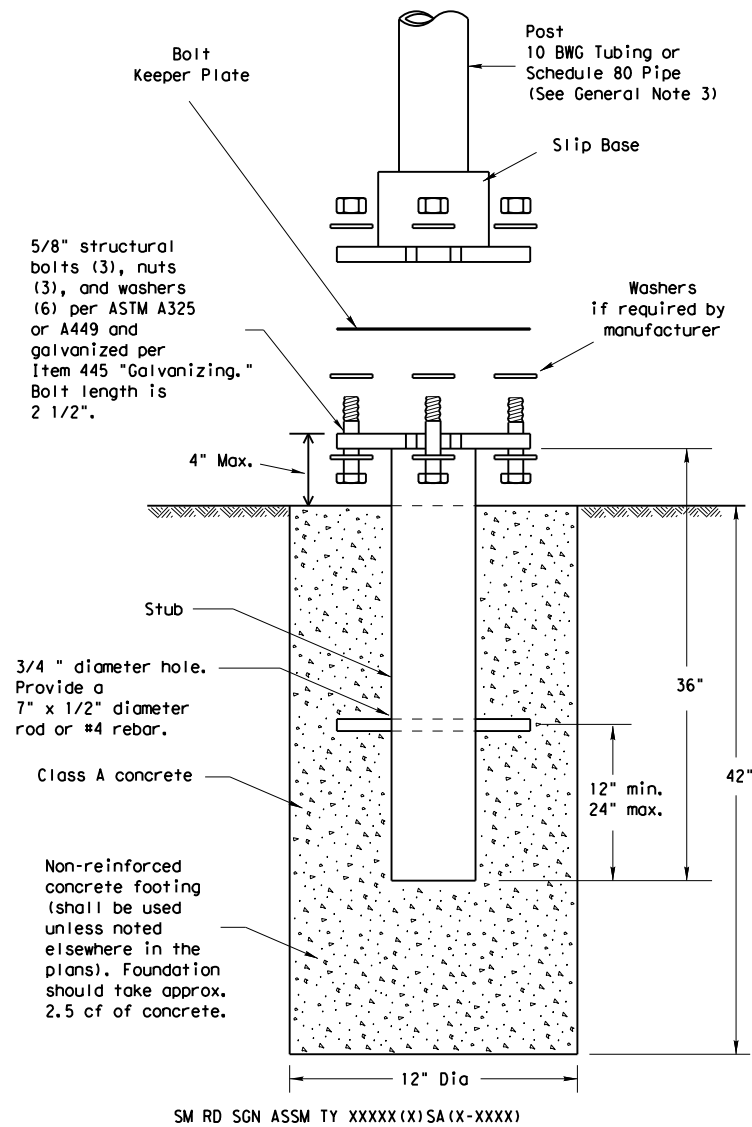
SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS GENERAL NOTES & DETAILS

SMD(GEN)-08

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		DIST	COUNTY		SHEET NO.
		SJT	MENARD		92

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TRIANGULAR SLIPBASE INSTALLATION GENERAL REQUIREMENTS



NOTE

There are various devices approved for the Triangular Slipbase System. Please reference the Material Producer List for approved slip base systems. http://www.txdot.gov/business/producer_list.htm
 The devices shall be installed per manufacturers' recommendations. Installation procedures shall be provided to the Engineer by Contractor.

GENERAL NOTES:

- Slip base shall be permanently marked to indicate manufacturer. Method, design, and location of marking are subject to approval of the TxDOT Traffic Standards Engineer.
- Material used as post with this system shall conform to the following specifications:
 - 10 BWG Tubing (2.875" outside diameter)
 - 0.134" nominal wall thickness
 - Seamless or electric-resistance welded steel tubing or pipe
 - Steel shall be HSLAS Gr 55 per ASTM A1011 or ASTM A1008
 - Other steels may be used if they meet the following:
 - 55,000 PSI minimum yield strength
 - 70,000 PSI minimum tensile strength
 - 20% minimum elongation in 2"
 - Wall thickness (uncoated) shall be within the range of 0.122" to 0.138"
 - Outside diameter (uncoated) shall be within the range of 2.867" to 2.883"
 - Galvanization per ASTM A123 or ASTM A653 G210. For precoated steel tubing (ASTM A653), recoat tube outside diameter weld seam by metallizing with zinc wire per ASTM B833.
 - Schedule 80 Pipe (2.875" outside diameter)
 - 0.276" nominal wall thickness
 - Steel tubing per ASTM A500 Gr C
 - Other seamless or electric-resistance welded steel tubing or pipe with equivalent outside diameter and wall thickness may be used if they meet the following:
 - 46,000 PSI minimum yield strength
 - 62,000 PSI minimum tensile strength
 - 21% minimum elongation in 2"
 - Wall thickness (uncoated) shall be within the range of 0.248" to 0.304"
 - Outside diameter (uncoated) shall be within the range of 2.855" to 2.895"
 - Galvanization per ASTM A123
- See the Traffic Operations Division website for detailed drawings of sign clamps and Texas Universal Triangular Slipbase System components. The website address is: <http://www.txdot.gov/publications/traffic.htm>
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.

ASSEMBLY PROCEDURE

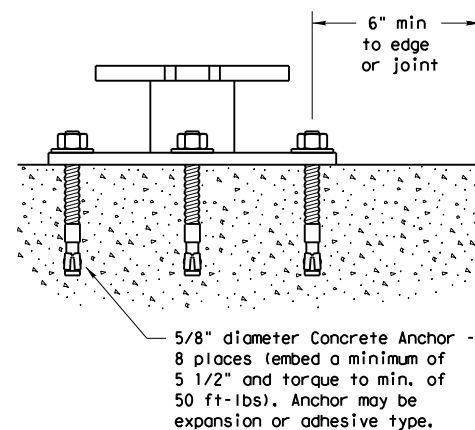
Foundation

- Prepare 12-inch diameter by 42-inch deep hole. If solid rock is encountered, the depth of the foundation may be reduced such that it is embedded a minimum of 18 inches into the solid rock.
- The Engineer may permit batches of concrete less than 2 cubic yards to be mixed with a portable, motor-driven concrete mixer. For small placements less than 0.5 cubic yards, hand mixing in a suitable container may be allowed by Engineer. Concrete shall be Class A.
- Push the pipe end of the slip base stub into the center of the concrete. Rotate the stub back and forth while pushing it down into the concrete to assure good contact between the concrete and stub. Continue to work the stub into the concrete until it is between 2 to 4 inches above the ground.
- Plumb the stub. Allow a minimum of 4 days to set, unless otherwise directed by the Engineer.
- The triangular slipbase system is multidirectional and is designed to release when struck from any direction.

Support

- Cut support so that the bottom of the sign will be 7 to 7.5 feet above the edge of the travelway (i.e., edge of the closest lane) when slip plate is below the edge of pavement or 7 to 7.5 feet above slip plate when the slip plate is above the edge of the travelway. The cut shall be plumb and straight.
- Attach sign to support using connections shown. When multiple signs are installed on the same support, ensure the minimum clearance between each sign is maintained. See SMD(SLIP-2) for clearances based on sign types.

CONCRETE ANCHOR



Concrete anchor consists of 5/8" diameter stud bolt with UNC series bolt threads on the upper end. Heavy hex nut per ASTM A563, and hardened washer per ASTM F436. The stud bolt shall have a minimum yield and ultimate tensile strength of 50 and 75 KSI, respectively. Nuts, bolts and washers shall be galvanized per Item 445, "Galvanizing." Adhesive type anchors shall have stud bolts installed with Type III epoxy per DMS-6100, "Epoxyes and Adhesives." Adhesive anchors may be loaded after adequate epoxy cure time per the manufacturer's recommendations. Top of bolt shall extend at least flush with top of the nut when installed. The anchor, when installed in 4000 psi normal-weight concrete with a 5 1/2" minimum embedment, shall have a minimum allowable tension and shear of 3900 and 3100 psi, respectively.



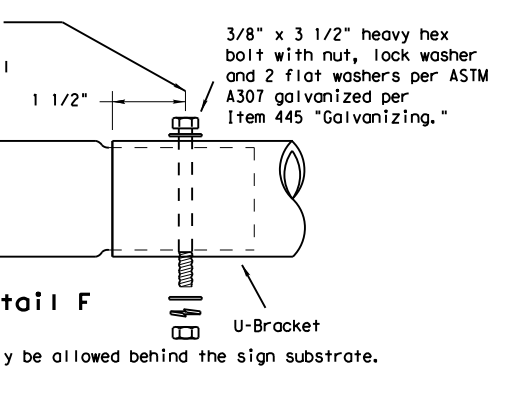
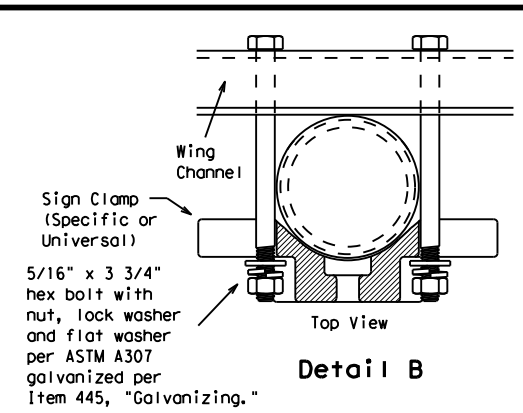
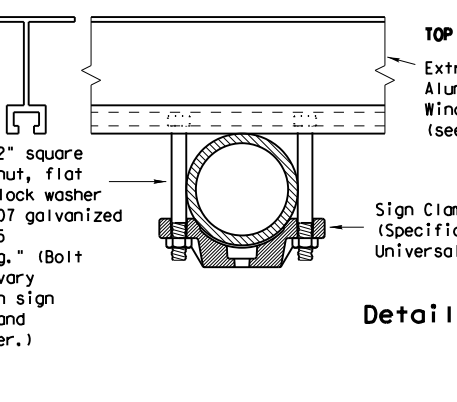
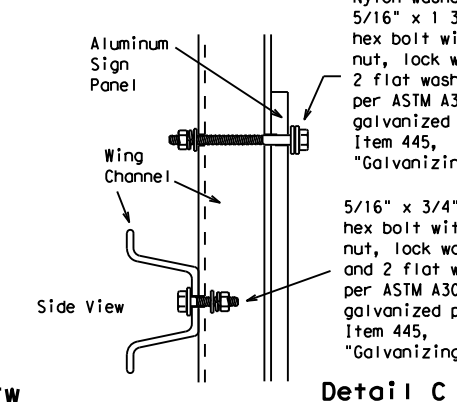
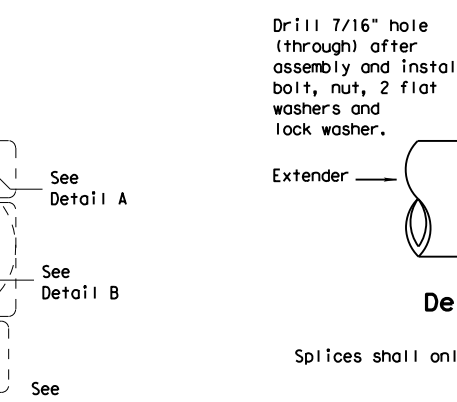
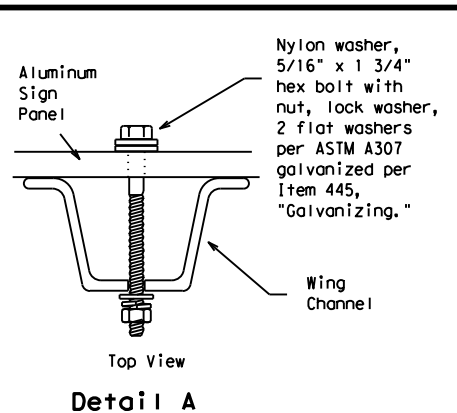
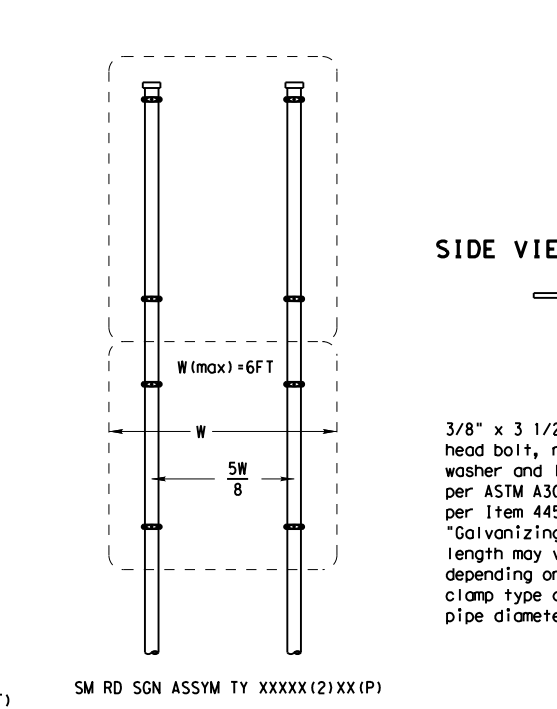
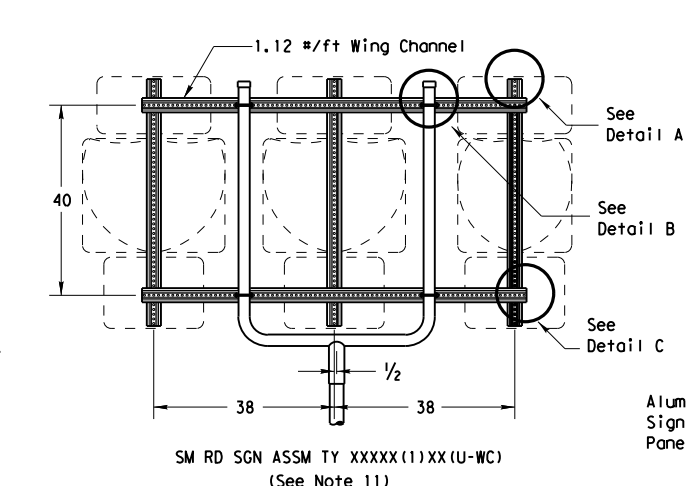
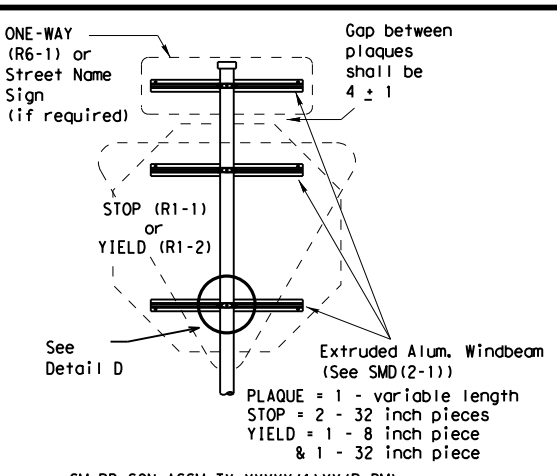
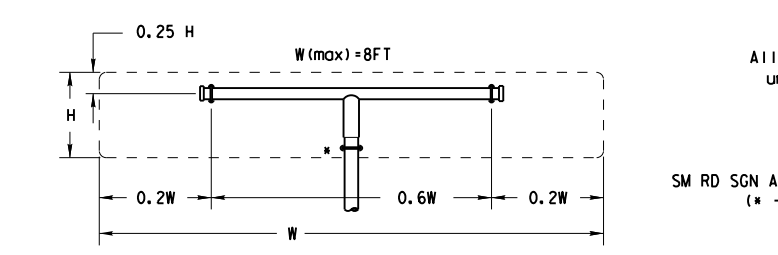
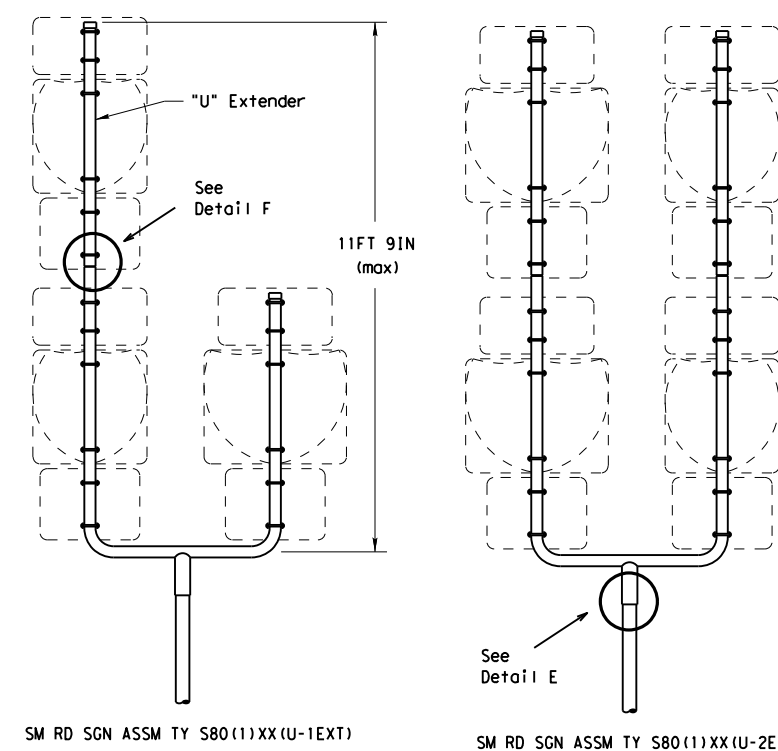
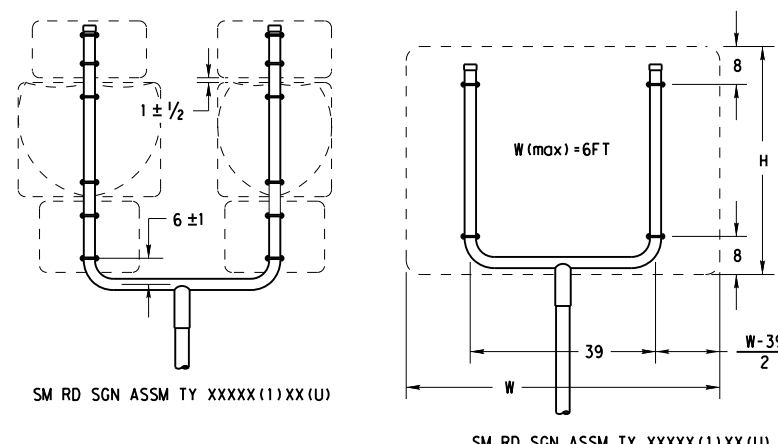
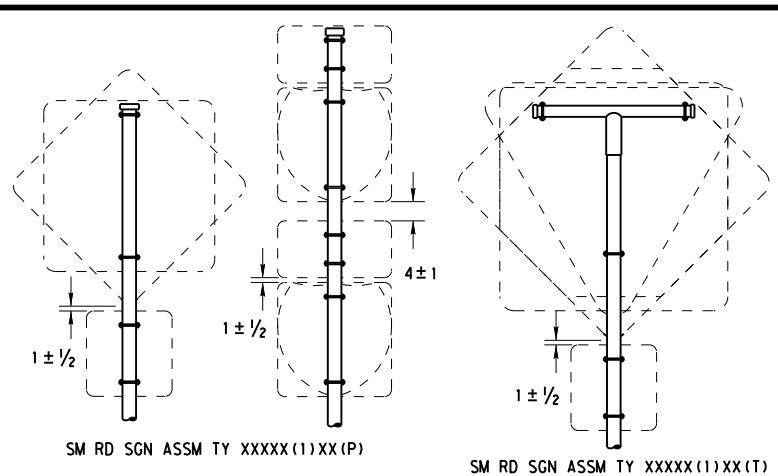
SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS TRIANGULAR SLIPBASE SYSTEM

SMD(SLIP-1)-08

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	DIST	COUNTY			SHEET NO.	
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GENERAL NOTES:

SIGN SUPPORT	# OF POSTS	MAX. SIGN AREA
10 BWG	1	16 SF
10 BWG	2	32 SF
Sch 80	1	32 SF
Sch 80	2	64 SF

- The Engineer may require that a Schedule 80 post be used in place of a 10 BWG where a sign height is abnormally high due to a fill slope.
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.
- Aluminum sign blanks shall conform to Departmental Material Specifications DMS-7110 and shall have the following minimum thicknesses: 0.080 for signs less than 7.5 sq. ft., 0.100 for signs 7.5 to 15 sq. ft., and 0.125 for signs greater than 15 sq. ft.
- Signs that require specific supports due to reasons in addition to windloading are indicated on the "REQUIRED SUPPORT" table on this sheet.
- For horizontal rectangular signs fabricated from flat aluminum, T-brackets are used for signs 24 inches or less in height. U-brackets are used for signs of greater height.
- When two triangular slipbase supports are used to support a single sign, they shall not be "rigidly" connected to each other except through the sign panel. This will allow each support to act independently when impacted by an errant vehicle.
- Wing channel shall meet ASTM A 1011 SS Gr 50 and be galvanized per ASTM A 123.
- Excess pipe, wing channel, or windbeam shall be cut off so that it does not extend beyond the sign panel (i.e., excess support shall not be visible when the sign is viewed from the front.) Repair galvanized coating at cut support ends per Item 445, "Galvanizing."
- Additional route markers may be added vertically, provided the total sign area does not exceed the maximum allowable amount per Note 1.
- Additional sign clamp required on the "T-bracket" post for 24 inch height signs. Place the clamp 3 inches above bottom of sign when possible.
- Post open ends shall be fitted with Friction Caps.
- Sign blanks shall be the sizes and shapes shown on the plans.

REQUIRED SUPPORT

SIGN DESCRIPTION	SUPPORT
	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
48-inch STOP sign (R1-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
60-inch YIELD sign (R1-2)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
48x16-inch ONE-WAY sign (R6-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
36x48, 48x36, and 48x48-inch signs	TY 10BWG(1)XX(T)
48x60-inch signs	TY S80(1)XX(T)
48x48-inch signs (diamond or square)	TY 10BWG(1)XX(T)
48x60-inch signs	TY S80(1)XX(T)
48-inch Advance School X-ing sign (S1-1)	TY 10BWG(1)XX(T)
48-inch School X-ing sign (S2-1)	TY 10BWG(1)XX(T)
Large Arrow sign (W1-6 & W1-7)	TY 10BWG(1)XX(T)

Friction caps may be manufactured from hot rolled or cold rolled steel sheets. The minimum sheet metal thickness shall be 24 gauge for all cap sizes.

The rim edges shall be reasonably straight and smooth. Caps shall be sized and formed in such a manner as to produce a drive-on friction fit and have no tendency to rock when seated on the pipe. The depth shall be sufficient to give positive protection against entrance of rainwater. They shall be free of sharp creases or indentations and show no evidence of metal fracture.

Caps shall have an electrodeposited coating of zinc in accordance with the requirements of ASTM B633 Class FE/ZN 8.

Texas Department of Transportation
 Traffic Operations Division

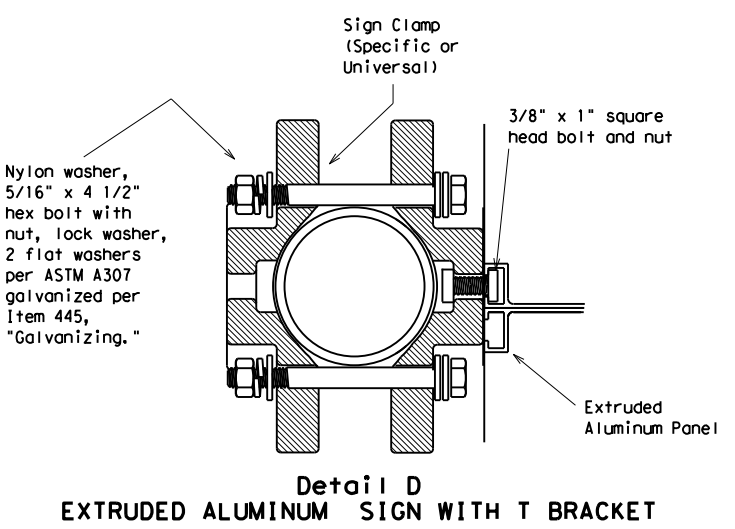
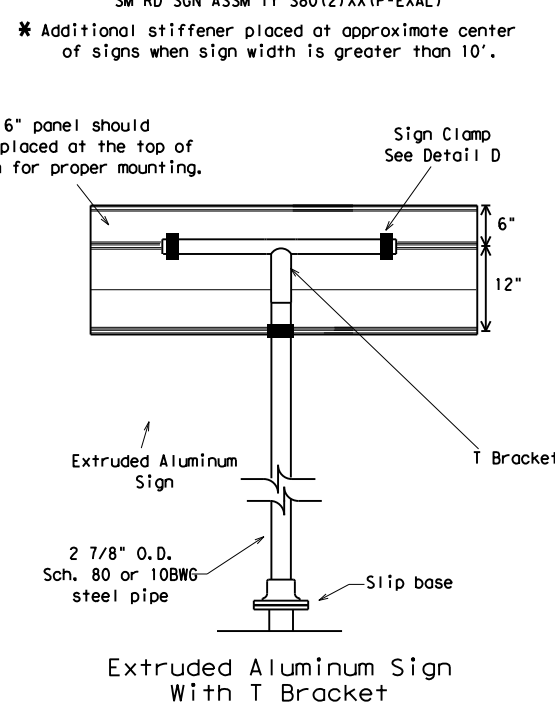
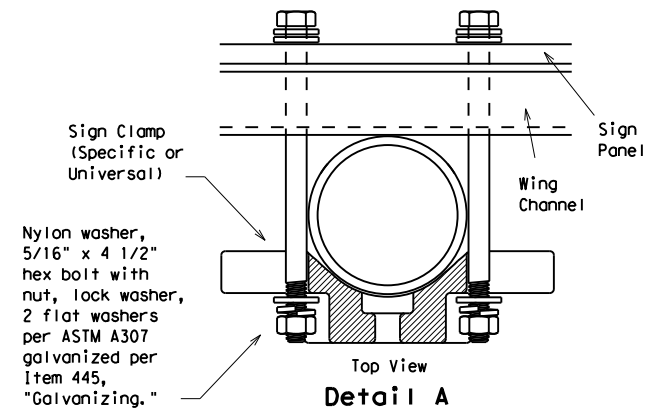
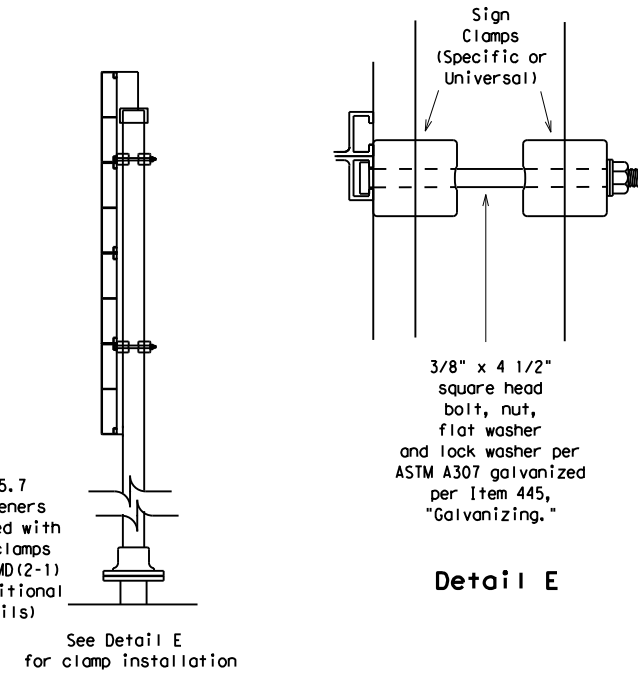
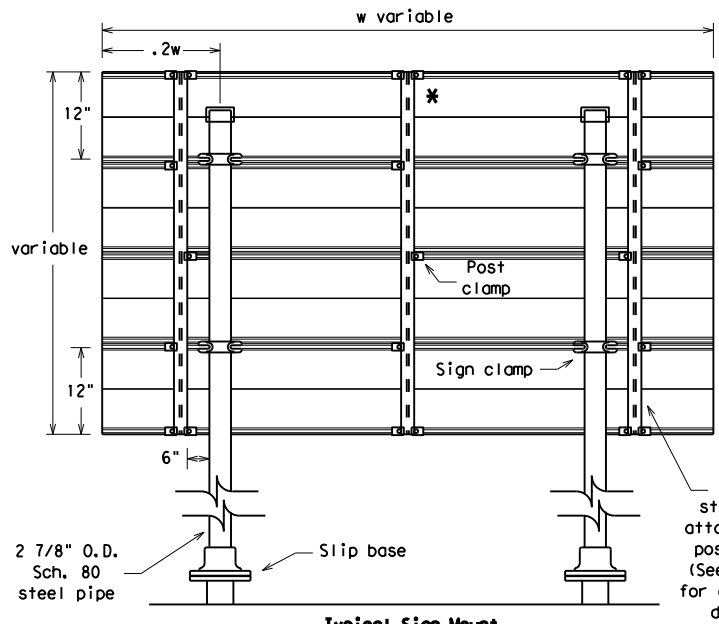
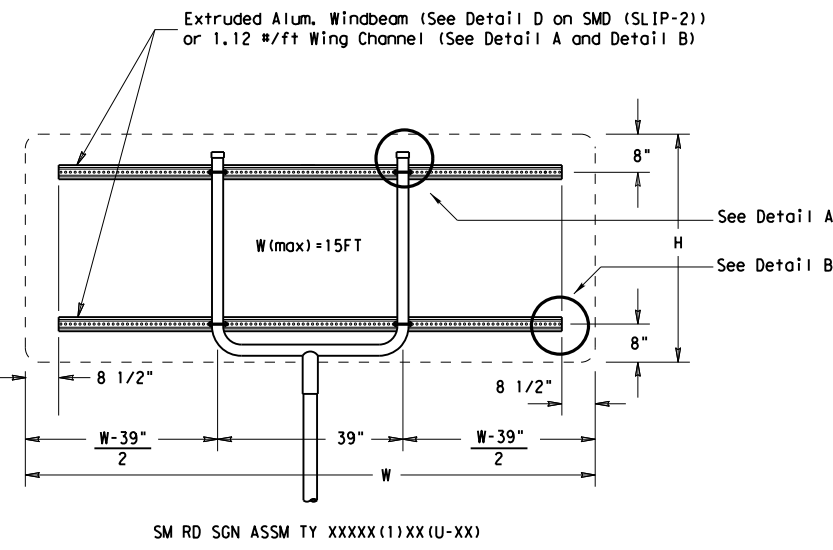
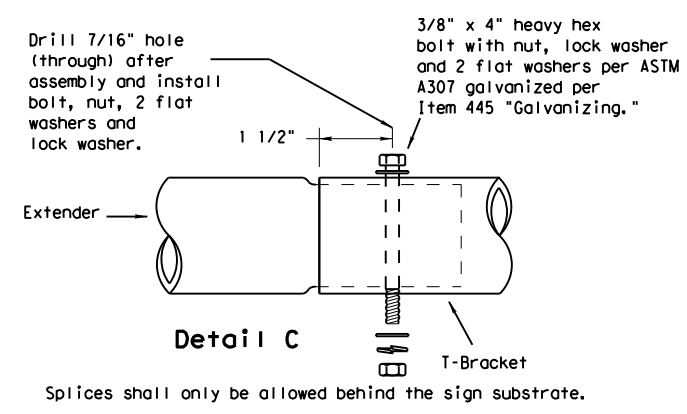
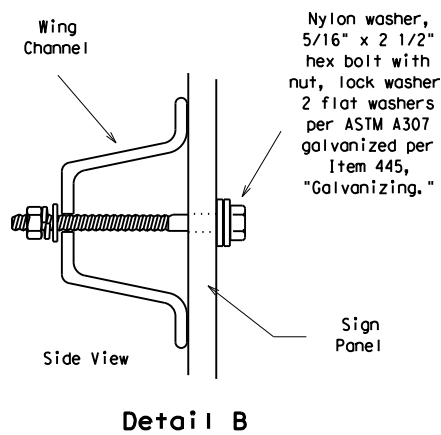
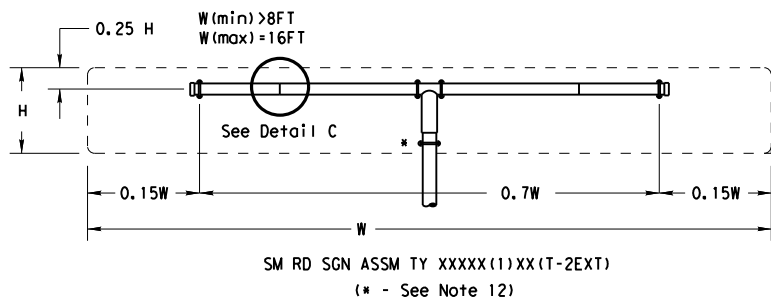
SIGN MOUNTING DETAILS
SMALL ROADSIDE SIGNS
TRIANGULAR SLIPBASE SYSTEM
SMD(SLIP-2)-08

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GENERAL NOTES:

- | SIGN SUPPORT | # OF POSTS | MAX. SIGN AREA |
|--------------|------------|----------------|
| 10 BWG | 1 | 16 SF |
| 10 BWG | 2 | 32 SF |
| Sch 80 | 1 | 32 SF |
| Sch 80 | 2 | 64 SF |
- The Engineer may require that a Schedule 80 post be used in place of a 10 BWG where a sign height is abnormally high due to a fill slope.
- Sign supports shall not be spliced except where shown. Sign support posts shall not be spliced.
- Aluminum sign blanks shall conform to Departmental Material Specifications DMS-7110 and shall have the following minimum thicknesses: 0.080 for signs less than 7.5 sq. ft., 0.100 for signs 7.5 to 15 sq. ft., and 0.125 for signs greater than 15 sq. ft.
- Signs that require specific supports due to reasons in addition to windloading are indicated on the "REQUIRED SUPPORT" table on this sheet.
- For horizontal rectangular signs fabricated from flat aluminum, T-brackets are used for signs 24 inches or less in height. U-brackets are used for signs of greater height.
- When two triangular slipbase supports are used to support a single sign, they shall not be "rigidly" connected to each other except through the sign panel. This will allow each support to act independently when impacted by an errant vehicle.
- Wing channel shall meet ASTM A 1011 SS Gr 50 and be galvanized per ASTM A 123.
- Excess pipe, wing channel, or windbeam shall be cut off so that it does not extend beyond the sign panel (i.e., excess support shall not be visible when the sign is viewed from the front.) Repair galvanized coating at cut support ends per Item 445, "Galvanizing."
- Sign blanks shall be the sizes and shapes shown on the plans.
- Additional sign clamp required on the "T-bracket" post for 24 inch high signs. Place the clamp 3 inches above bottom of sign when possible.
- Post open ends shall be fitted with Friction Caps.

REQUIRED SUPPORT		
	SIGN DESCRIPTION	SUPPORT
Regulatory	48-inch STOP sign (R1-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	60-inch YIELD sign (R1-2)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	48x16-inch ONE-WAY sign (R6-1)	TY 10BWG(1)XX(T) TY 10BWG(1)XX(P-BM)
	36x48, 48x36, and 48x48-inch signs	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
Warning	48x48-inch signs (diamond or square)	TY 10BWG(1)XX(T)
	48x60-inch signs	TY S80(1)XX(T)
	48-inch Advance School X-ing sign (S1-1)	TY 10BWG(1)XX(T)
	48-inch School X-ing sign (S2-1)	TY 10BWG(1)XX(T)
	Large Arrow sign (W1-6 & W1-7)	TY 10BWG(1)XX(T)

Texas Department of Transportation
 Traffic Operations Division

SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS TRIANGULAR SLIPBASE SYSTEM

SMD(SLIP-3)-08

© TxDOT July 2002		DN: TxDOT	CK: TxDOT	DW: TxDOT	CK: TxDOT
9-08	REVISIONS	CONT	SECT	JOB	HIGHWAY
		6383	49	001	US 83, ETC
		DIST	COUNTY		SHEET NO.
		SJT	MENARD		95

DATE: 6/23/2021 10:45:45 AM
 FILE: \\txdot.pro\project\seonline.com\T.XDOT\Documents\07 - SJT Maintenance Projects\638349001\2 - DESIGN FILES\Plan Sheets\ENVIRONMENTAL PERMITS ISSUES AND COMMITMENTS.dgn

I. STORMWATER POLLUTION PREVENTION-CLEAN WATER ACT SECTION 402

TPDES TXR 150000: Stormwater Discharge Permit or CGP required for projects with 1 or more acres disturbed soil. Projects with any disturbed soil must protect for erosion and sedimentation in accordance with Item 506.

List MS4 Operator that may receive discharges from this project. The MS4 Operator may need to be notified prior to construction activities.

1. N/A
- NO ACTION REQUIRED ACTION REQUIRED
- Prevent stormwater pollution by controlling erosion and sedimentation in accordance with TPDES Permit TXR 150000.
 - Comply with the SW3P and revise when necessary to control pollution or required by the Engineer.
 - Post CSN with SW3P information on or near the site, accessible to the public and TCEO, EPA or other inspectors.
 - When PSL's increase disturbed soil area to 5 acres or more, submit NOI to TCEO and the Engineer.

II. WORK IN OR NEAR STREAMS, WATERBODIES AND WETLANDS CLEAN WATER ACT SECTIONS 401 AND 404

USACE Permit required for filling, dredging, excavating or other work in any water bodies, rivers, creeks, streams, wetlands or wet areas.

Adhere to all of the terms and conditions associated with the following permit(s):

- No Permit Required
- Nationwide Permit 14 - PCN not Required (less than 1/10th acre waters or wetlands affected)
- Nationwide Permit 14 - PCN Required (1/10 to <1/2 acre, 1/3 in tidal waters)
- Individual 404 Permit Required
- Other Nationwide Permit Required: NWP#

The elevation of the ordinary high water marks of any areas requiring work to be performed in the waters of the US requiring the use of a nationwide permit can be found on the Bridge Layouts.

Required Actions: List waters of the U.S. that the permit applies to, the location in project, and check BMP's planned to control erosion, sedimentation and post-construction TSS.

1. N/A

BEST MANAGEMENT PRACTICES

- EROSION**
- SEEDING OR SODDING
 - MULCHING
 - SOIL RETENTION BLANKETS
 - BIODEGRADABLE EROSION CONTROL LOGS
 - DIVERSION, INTERCEPTOR, OR PERIMETER SWALES
 - DIVERSION, INTERCEPTOR, OR PERIMETER DIKES
 - TOPSOIL OR COMPOST
 - FLEXIBLE CHANNEL LINERS
 - GROUND COVER
- SEDIMENTATION**
- ROCK FILTER DAMS
 - TEMPORARY SEDIMENT CONTROL FENCES
 - TRIANGULAR FILTER DIKES
 - TOPSOIL OR COMPOST
 - BIODEGRADABLE EROSION CONTROL LOGS
 - SEDIMENT BASINS
 - SAND BAG BERMS
 - STRAW BALE DIKES
 - BRUSH BERMS
 - STORM INLET SEDIMENT TRAPS
- POST-CONSTRUCTION TSS**
- VEGETATIVE FILTER STRIPS
 - RETENTION/IRRIGATION SYSTEMS
 - EXTENDED DETENTION BASINS
 - CONSTRUCTED WETLANDS
 - WET BASINS
 - TOPSOIL OR COMPOST
 - BIODEGRADABLE EROSION CONTROL LOGS
 - VEGETATION LINED DITCHES
 - SAND FILTER SYSTEMS
 - GRASSY SWALES

III. CULTURAL RESOURCES

Refer to the Standard Specifications in the event historical issues or archeological artifacts are found during construction. Upon discovery of archeological artifacts (bones, burnt rock, flint, pottery, etc.) cease work in the immediate area and contact the Engineer immediately.

- NO ACTION REQUIRED ACTION REQUIRED

1. N/A

IV. VEGETATION RESOURCES

Preserve native vegetation to the extent practical.

Adhere to specification requirements of Items 162, 164, 192, 193, 506, 730, 751, 752 in order to comply with requirements for invasive species, beneficial landscaping, and tree/brush removal commitments.

- NO ACTION REQUIRED ACTION REQUIRED

1. Only remove woody vegetation between October 1 and March 1.

V. FEDERAL LISTED, PROPOSED THREATENED, ENDANGERED SPECIES, CRITICAL HABITAT, STATE LISTED SPECIES, CANDIDATE SPECIES AND MIGRATORY BIRDS

If any of the listed species are observed, cease work in the immediate area, do not disturb species or habitat and contact the Engineer immediately. The work may not remove active nests from bridges and other structures during nesting season of the birds associated with the nests. If caves or sinkholes are discovered, cease work in the immediate area, and contact the Engineer immediately.

- NO ACTION REQUIRED ACTION REQUIRED

1. The Migratory Bird Treaty Act of 1918 states that it is unlawful to kill, capture, collect, possess, buy, sell, trade, or transport any migratory bird, nest, young, feather, or egg in part or in whole, without a federal permit issued in accordance with the Act's policies and regulations. Migration patterns would not be affected by the proposed project. Remove non-active migratory bird nests from structures where work would be performed from September 1 through the end of February. Prevent migratory birds from building nests from March 1 to August 31. In the event that migratory birds are encountered on-site during project construction, avoid adverse impacts on protected birds, active nests, eggs, and/or young.

ABBREVIATIONS USED

- | | |
|--|--|
| BMP - Best Management Practice | NOI - Notice of Intent |
| CGP - Construction General Permit | NWP - Nationwide Permit |
| CSN - Construction Site Notice | PCN - Pre-Construction Notification |
| DSHS - Texas Department of State Health Services | PSL - Project Specific Location |
| EPA - U.S. Environmental Protection Agency | SW3P - Storm Water Pollution Prevention Plan |
| MS4 - Municipal Separate Stormwater Sewer System | TCEO - Texas Commission on Environmental Quality |
| MSDS - Material Safety Data Sheet | TPDES - Texas Pollutant Discharge Elimination System |
| | TSS - Total Suspended Solids |
| | USACE - U.S. Army Corps of Engineers |

VI. HAZARDOUS MATERIALS OR CONTAMINATION ISSUES

General (applies to all projects):

Comply with the Hazard Communication Act (the Act) for personnel who will be working with hazardous materials by conducting safety meetings prior to beginning construction and making workers aware of potential hazards in the workplace. Ensure that all workers are provided with personal protective equipment appropriate for any hazardous materials used.

Obtain and keep on-site MSDS for all hazardous products used on the project, which may include, but are not limited to the following categories: paints, acids, solvents, asphalt products, chemical additives, fuels and concrete curing compounds or additives. Provide protected storage, off bare ground and covered, for products which may be hazardous. Maintain product labeling as required by the Act.

Maintain an adequate supply of on-site spill response materials, as indicated in the MSDS. In the event of a spill, take actions to mitigate the spill as indicated in the MSDS, in accordance with safe work practices, and contact the TxDOT District spill coordinator immediately. The Contractor shall be responsible for the proper containment and cleanup of all product spills.

Contact the Engineer if any of the following are detected:

- Dead or distressed vegetation (not identified as normal)
- Trash piles, drums, canister, barrels, etc.
- Undesirable smells or odors
- Evidence of leaching or seepage of substances

Does the project involve any bridge class structure rehabilitation or replacements (bridge class structures not including box culverts)?

- YES NO

If "No", then no further action is required.

If "Yes", then TxDOT is responsible for completing asbestos assessment/inspection.

Are the results of the asbestos inspection positive (is asbestos present)?

- YES NO

If "Yes", then TxDOT must retain a DSHS licensed asbestos consultant to assist with the notification, develop abatement/mitigation procedures, and perform management activities as necessary. The notification form to DSHS must be postmarked at least 15 working days prior to scheduled demolition.

If "No", then TxDOT is still required to notify DSHS 15 working days prior to any scheduled demolition.

In either case, the Contractor is responsible for providing the date(s) for abatement activities and/or demolition with careful coordination between the Engineer and asbestos consultant in order to minimize construction delays and subsequent claims.

Any other evidence indicating possible hazardous materials or contamination discovered on site (hazardous materials or contamination issues specific to this project):

- NO ACTION REQUIRED ACTION REQUIRED

1. N/A

VII. OTHER ENVIRONMENTAL ISSUES

(Includes regional issues such as Edwards Aquifer District, etc.)

- NO ACTION REQUIRED ACTION REQUIRED

1. N/A



23.06.2021



ENVIRONMENTAL PERMITS ISSUES AND COMMITMENTS

SHEET 1 OF 1 NOT TO SCALE

©TxDOT 2021	CONT	SECT	JOB	HIGHWAY
SHEET ISSUED OR LAST REVISED	6383	49	001	US 83,ETC
11-19	DIST	COUNTY	SHEET NO.	
	SJT	MENARD	99	